

Trainini

Full Issue

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and Prototype

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Trainini

German Magazine for Z Gauge



Fine detailing of the series 94⁵⁻¹⁷

New Wagons Rmms 33
Excellent new products

Introduction

Dear Readers,

a warm welcome to a new season of our popular magazine and all the best for 2019! With excitement and highlights we have finished 2018 and with full speed we start again.

Märklin has just delivered new model versions of its Rmms 33 stake cars, which, to our surprise, were designed in two different shapes. We are really amazed at these announcements for 2019.

But the spring novelties will only be presented in detail and with lots of background information next month as part of our trade fair report from Nuremberg. In January it's time for an extensive test of the beautiful cars that were presented to us so shortly before the end of last year.

I would like to state in advance that the first impression has been confirmed and that the traditional manufacturer from Göppingen has given us exceptionally well-designed models with great perspectives for the future. This is a clear commitment to gauge Z, as we have wished for it!

In all detail we will present the Rmms 33 to you and also look a little to the right and left how these and similar cars can be impressively showcased on the layout. After all, they provide an excellent stage to draw attention to special loads during the layout operation.

This also applies to the 94 series, which by the way is also the focus of one of our two book reviews, while the other title belongs to the freight car theme. The Märklin basic model of the Prussian tender locomotive is regarded as a model that runs well and is outstandingly realised in many aspects.

Since it also has its weaknesses, as we once pointed out in our test report, this locomotive was an ideal candidate for the steam locomotive specialist Michael Bahls. He breathed even more life into our demonstration models and brought them up to date. Only two minor modifications come from our inventory of valuable ideas.

Maybe we can whet your appetite with this article? There should be an unchanged demand for those versatile Prussian locomotives and so far they have not received any product update. If you miss that, Bahl's model trains already has an outstanding solution for you.

Traditional topics in the January issue are the review of the past twelve months and the points that enchanted, impressed and thrilled us during this time. The announcement of the new releases of the year in the categories locomotives, railcars, accessories, technology and literature is both a conclusion and a motivation.

In addition, there is also the section "complete works", in which we would like to honour deserving achievements and persons who would otherwise - wrongly - be lost in the aforementioned categories. Let us surprise you and enjoy reading the first issue of the year 2019!

Sin-Z-erly,

Holger Späing



Holger Späing
Editor-in-chief

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Cover photo:

The Wuppertaler 94 1653 is divided into a route service and makes itself useful in front of a passenger train in the Ruhr area local traffic. Until the mid-sixties, the old Prussians also played an important role in the area of BD Essen, and sister machines are also represented in their depots accordingly.

Märklin's new Rmms 33 Of times and miracles

Over the years, we have repeatedly pointed out gaps in the Z gauge wagon area. The fact that such gaps still exist today is proven by a new item from Märklin that excellently enriches the area of rolling stock. We took a very close look at it and measured it in order to classify it today in terms of role and perspective. Enjoy the extensive analysis of a very successful model.

Märklin's latest new wagon is already known from gauge 1 and it is exactly this gauge that supplied the templates for the 1:220 scale models. But before we turn to the models and would like to introduce them here in detail, we take a short look at their model history.

During the Second World War, industrial production had to be increased considerably and rationalised at the same time. In addition to weapons, locomotives and wagons were also needed to cope with the increased transport requirements. The development of the war locomotive class 52 as a simplified and material-saving successor to the peace version in the form of class 50 is well known.



As with the new Märklin models, the front stanchions of this Kbs-type wagon are inserted from DB AG stock. The side stanchions, are stored in the pockets under the floor on the long side of the wagon.

But similar developments could also be observed in the field of freight wagons, after a transport crisis had arisen with ever longer journeys from Germany to the front. The production of freight wagons, therefore, also had to be increased, at the same time reacting to scarce raw materials and labour shortages. As a result, new wagons had to be built in considerably less time and with less material input.



Motor vehicles of various types were also among the typical loads of low side and stanchion cars. Between 1950 and 1953, the DB equipped 25 wagons with heating cables for Sylt traffic in order to place them between the locomotive and the passenger coaches. In 1955 in Niebüll, a Mercedes-Benz 180 D with a pontoon body was driven onto a stake car of the type Rmmhs 31. Photo: Walter Hollnagel / Eisenbahnstiftung

Since the Reich government did not see the bottlenecks in its own omissions or wrong decisions, in March 1942 it shifted responsibility for the procurement and development of railway vehicles from the Ministry of Transport to the Reich Ministry of Armament and Ammunition, thus depriving the Reichsbahnzentralamt (Central Railway Office) of its leading role.

Many of the solutions found at that time are now indispensable in the construction of freight cars. An example of this is the front brake platform for the handbrake, which can be lowered for cars to drive over, as the Rmm(s) Ulm also showed, which is the subject of this article.

After nine test cars in 1942, series production began in 1943 and continued until the end of the war. The DR later completed further cars from the parts already produced. By zero hour, however, 12,638 of the 13,500 cars ordered from Wumag were delivered. Converted this means that 25 cars were completed daily, and each of them only took about 50 minutes on the production line.

This large number of cars, of which about one third came to DB after the end of the war, combined with their small age made them an important type of construction for the Bundesbahn, which was still in use until 1977. They were originally approved for a top speed of 90 km/h, which could be increased to 100 km/h later.

With a loaded weight of 24.5 tons, they could be used in many different ways: In addition to hay, straw and machinery, they were also used for the transport of many other cargoes. Even in times of war, these were also vehicles: In civilian service, military vehicles were replaced by brand new cars, and trucks and buses were also transported on them.

Tractors, trailers or other agricultural machinery are certainly an eye-catcher in the model. But the extraordinary is also true to the original and thus permitted, as our example of a boat transport shows. In short, low side and stanchion cars are ideal models for bringing variety to the rails.



Also many unusual transports with stake cars are documented and offer various suggestions for use on the model railway. On 29 July 1966, for example, this NS model, equipped with wooden stanchions and shelves, carried a sailboat, and the wagon became the focal point of the train, immediately. Photo: Joachim Claus / Eisenbahnstiftung

Back to the larger prototype: Since Knorr could not supply enough brake systems under war conditions, a part of the Rmm Ulm was put into service as a cable trolley, i.e., without brakes, but with continuous air conduction. When heavy road vehicles were used, permanent damage often occurred, which was due to the fact that the long beams had been modified in comparison to the peacetime versions.

The initially chosen wooden boards, which were intended to save the valuable raw material steel, were also unsuccessful. At the beginning of the construction period, the front and side flaps consisted of steel hollow sections in the upper and lower beams, between which short boards were inserted vertically. When heavy equipment drove over them, they were permanently deformed.

The Special Committee for Railway Wagons (SAE), which was responsible for the development, therefore provided the necessary steel quotas to have the side walls made entirely of steel. But also the design was changed and simplified in the course of time.

The effects and results of these rationalisations become particularly clear in comparison with the Stuttgart Rms with the same axle base and only slightly greater length: the use of steel could be reduced by 26 % to 11.0 tonnes per wagon, per ton load capacity the "Ulm" weighed only 345 kg compared with 620 kg for the "Stuttgart."

One consequence was the increasing susceptibility to damage as early as the 1950s. The corrosion-prone steels and the tight dimensioning of the profiles weakened the cars, which is why many of them were only used as Rmms 33 without stanchions.

DB had 200 of these cars prepared for piggyback transport between Mülheim (Ruhr) and Hamburg as well as Hamburg and Frankfurt (Main), in which they drove trucks and trailers overnight to their destinations. In 1955/56 another 150 wagons were converted for container traffic, 30 of which were universal wagons with the designation Rmms / BTms 33 and could be used both as stake and container wagons. The side walls of the other cars were dismantled.

Since the wagons known as Rmms 33, together with the Rms 31, were the only stanchion wagons to meet the increased traffic requirements, the numbers remained virtually unchanged until 1962. Only when the Rmms 56 and 58 from the conversion and new wagon programme were available in sufficient numbers had their end struck in their old form. When Klm 441 or X wagons were converted and classified as Xltmms 33, the last of these were still in service at DB until 1977.

Characteristics of the models

For the implementation in the scale 1:220 Märklin has taken up two of the described prototypes and designed the stake cars as DB Rmms 33 with both steel (Art. No. 82130) and wooden stanchions (82131). The inspiration for this was unmistakably the already existing models in 1 Gauge.



Fresh from the pack are the models with firmly glued forehead stanchions and two mouldings each packed in plastic bags, from which the side stanchions can be added as required to the models on the track.

The fact that these new freight cars are somehow something special can already be guessed at the sight of the package: Although this is a standard plastic can, Märklin has chosen the long version despite the rather short length of the wagon. Four-axle wagons of modern designs are usually packed in it.

The wagon lies on the left-hand side of a transparent thermoforming insert, protected by a cover produced in the same way. Above it, on the copy with article number 82130, there is a load insert from Duha which, according to the product description, replicates hay bales, but can be identified in colour as straw.

Dimensions and data of the stake wagon Rmms 33 (ex Rmms Ulm):

	Prototype	1:220	Model
Length over buffers	12.000 mm	54,5 mm	54,5 mm
with hand brake	12.100 mm	55,0 mm	54,5 mm
Length of body	10.700 mm	48,6 mm	48,6 mm
Width to side walls	2.800 mm	12,7 mm	12,8 mm
Total width	3.050 mm	13,9 mm	14,3 mm
over steps	3.090 mm	14,0 mm	14,4 mm
Height above SO (stanchions)	3.586 mm	16,3 mm	16,1 mm
Wheelbase	8.000 mm	36,4 mm	36,2 mm
Wheel diameter	unknown	---	4,5 mm
Buffer length	650 mm	3,0 mm	2,7 mm
Ø Buffer disc	370 mm	1,7 mm	1,9 mm
Tare weight	10,3 – 10,9 t	---	6 g
Permissible maximum speed	90 / 100 km/h		
Years of construction (Series)	1943 - 1945		
Quantity built (Series)	12.638 wagons*		

* until the end of the war; completion of further cars 1947/48 for the DR

In the right part of the box two plastic mouldings are inserted, from which the side stanchions can be separated and attached to the model.

Equipped in this way, it no longer fits into the packaging, which is why the stanchions have to be removed again for storage.

While the two stanchions on each front are identical and fixed on both car variants, Märklin has reproduced both pointed steel stanchions (82130) and wooden stanchions (82131) of the same width over the entire length for the sides.

Thanks to this model-correct differentiation, both cars can easily be distinguished from each other even from a distance of some distance.

The heart also leaps for joy when it comes to reducing the dimensions: not only are the proportions well met, but an electronic measuring calliper also confirms that almost all important dimensions have been adhered to exactly. Deviations from the scale conversion are in the range of the expected measuring errors.





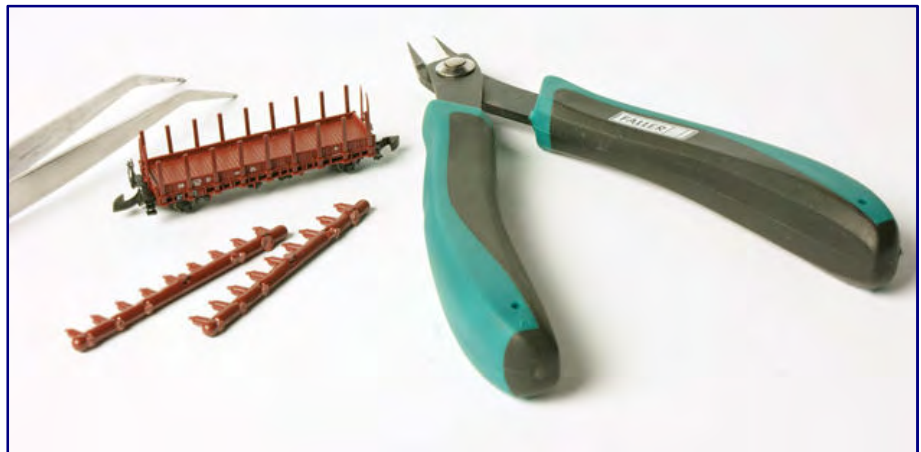
The models with steel shelves and straw load (picture on page 8 below) and with wooden stakes and handbrake (photo above) have exactly the same length in the model. Nevertheless, it is surprising how exactly Märklin was able to keep the converted model dimensions.

This shows that Märklin has done a very good job. It should also be clear that no second undercarriage will be constructed for the car with an individually attached handbrake platform and a length increased by 0.5 mm over the buffers. The eye would not be able to see this.

The structures and engravings on the wagon are also very well done. Especially a stanchion car creates some challenges due to the uneven side walls and stanchion pockets, which Märklin also has mastered convincingly.

What is striking for us, for example, is that the inner walls of the car with wooden sides could now also have grooves.

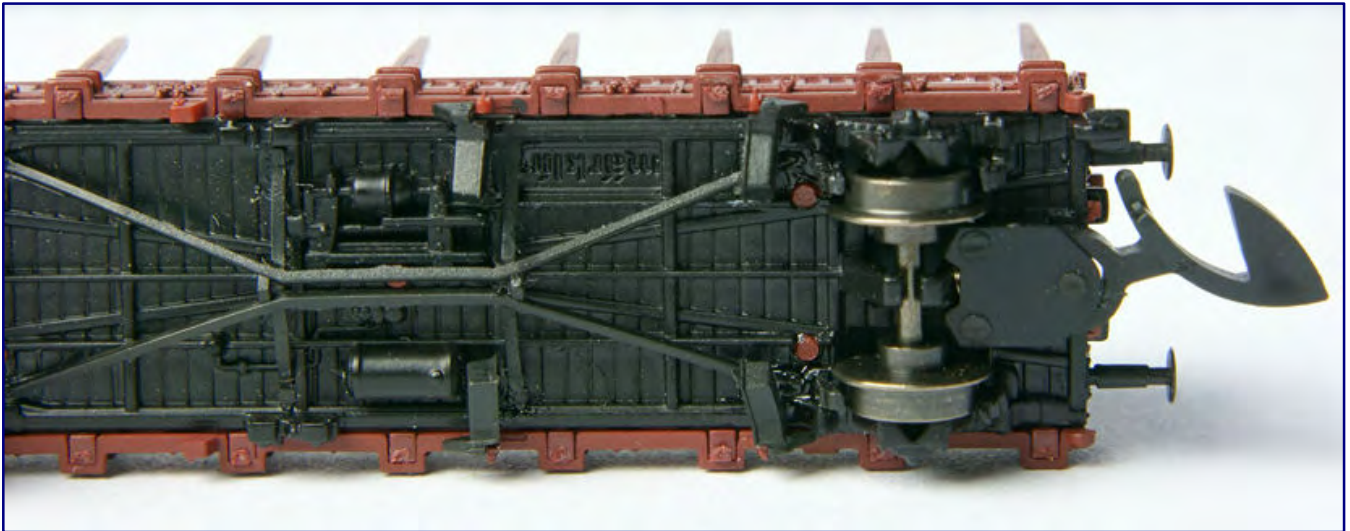
This did not exist before and invites you use this model without cargo to display the interior walls. The model also looks different from the model with metal sides, which is why the manufacturer probably chose this one for the load.



If you want to operate your wagons with stanchions, you still have a little work to do. However, the waggon no longer fits into the packaging, so these must first be removed again.

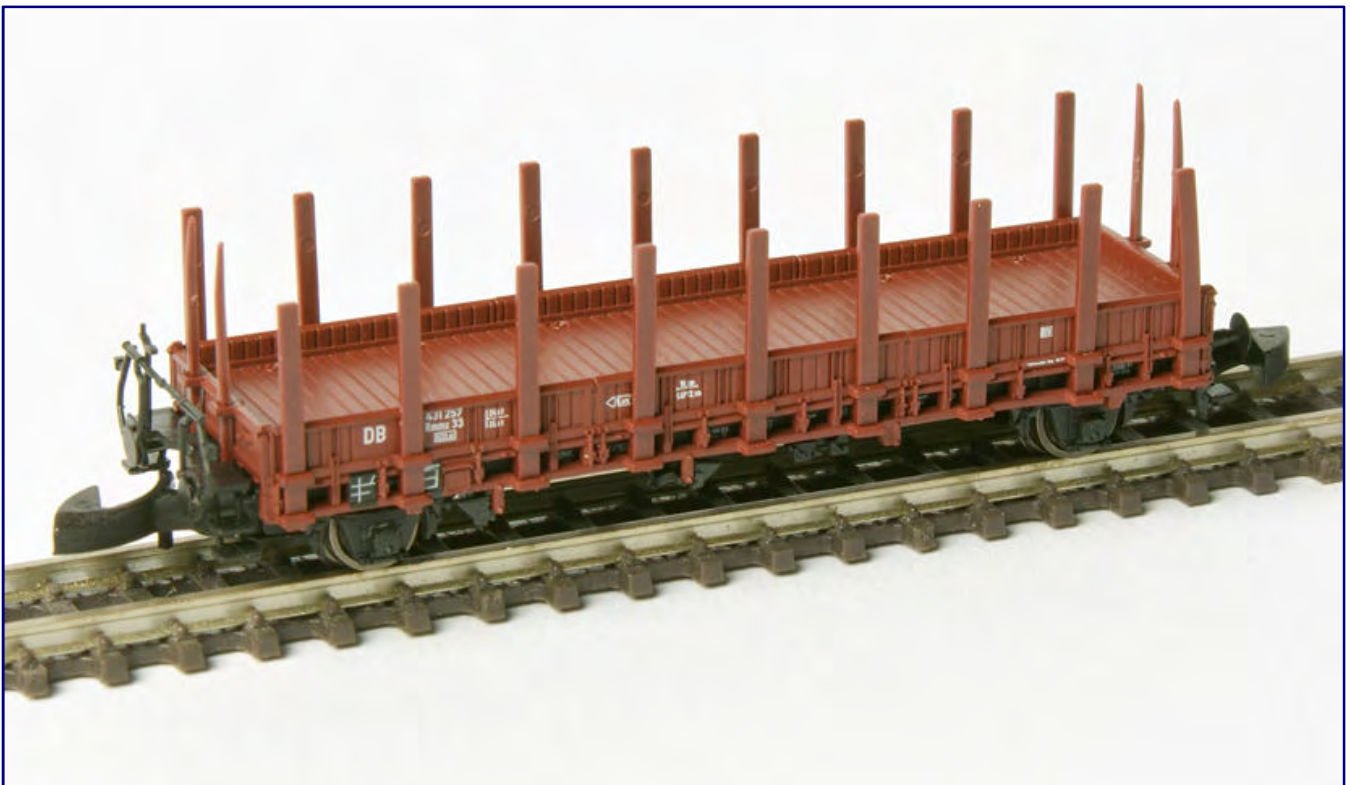
The good detailing also continues on the loading area and wagon floors. In addition to the manoeuvring steps and change-over levers for the brake system, we also find the brackets that can accommodate stanchions that can be inserted into the model.

The damper was assembled from four parts and set up individually. Unfortunately, clear traces of glue can be found in the corners of the car, which fortunately cannot be seen during operation. But this also shows that the chassis is obviously modular and will be used for other wagon types in the future.



The damper mechanism is made up of four individual parts. Unfortunately, traces of glue can be seen all too clearly near the wheels. But for the first time on a mini club model we find the brake replicas on wheel level.

Very commendable and also to be found for the first time are the brakes in the wheel plane, which will attract the attention of the viewer. Also, air tanks on the car floor were not omitted, and even the construction of the longitudinal beams, typical for the war design, was reproduced.



The excellently engraved inner sides of the wooden on-board car with handbrake platform also deserve praise: in addition to the car floor, which is structured with wooden planks, the inner walls of the on-board walls in particular also deserve praise.

The space problem in the area of the coupler box was also cleverly solved. It is just not enough for the standard shaft. However, instead of reducing the axle base, Märklin has pulled in the shaft at the rear end at an angle to a very small extent. This does not interfere with the function, but maintains the good and scale effect.

The colouring of both cars (and stanchions) is RAL 8012 red-brown, the chassis is RAL 9005 deep black, and we did not expect any other colours. The wheelsets are black nickel plated, as has been the standard from Göppingen for many years now.

The operating labelling was applied by pad printing and stands out in white. Only the chalk fields have a black background. The wagon addresses are correct in size and circumference and are also flawlessly legible. They can also be found on the longitudinal frame in an exemplary manner.



The close-up shows the excellent engravings between the stanchion holders and the brake levers as well as the flawless printing on the sides of the car and on the frame. Some models, unfortunately, have single buffers crooked. However, this can usually be corrected, since they are plugged in separately.

Only the stanchions had no printing. With them the owner abbreviation was written down with "DB" in the upper range. In order to ensure that the printing was fixed, Märklin painted over them with clear lacquer. This is again the case here and ensures a good matt appearance.

Those who want to insert the stanchions should carefully cut them out with a special side cutter (e.g., from Faller) and insert them into the holders with tweezers. They sit there very firmly and do not come out during operation, but can be removed at any time. We have good experience removing the stanchions by using light pressure on fixed stanchions with a toothpick tip from below.

Care and attention are of course required with these small models, of which every Zettie should be aware. How the stanchions are inserted correctly is also explained in a photo in the enclosed manual.

Evaluation and summary

In order to be able to place the Rmms 33 stake car in both variants appropriately in the Mini-Club range, we also have to include low side cars. After all, many of them are identical or similar in construction to the stanchion cars.

This segment had previously been served rather negligently by the large and small series, although there are many attractive themes, including circus transport. The classic in the program was and is the model

8610 from the first year of the Z-gauge. It was so full of compromises and deviations from its original that it had not been considered contemporary for two decades.

The only alternative was the short X 05 low side car, which disappeared early from DB's work train service and was no longer available for public transport. More helpful for model-oriented model railroaders was the stanchion car R 10, which was very well hit, but only with brakeman's cab remains correct compared to the model.



The new stake cars cut a good figure on the layout, regardless of whether they are used as a short block train on the branch line or as part of a longer freight train.

In the more typical Bundesbahn version with brakeman's platform, two front stanchions were missing on this side, which became necessary after dismantling the brakeman's cab and had to be retrofitted. This can be quite noticeable in the train consist. A version without handbrake is still not part of the Göppingen range.

From the point of view of many Z-gauge customers, there were gaps that had to be filled, because nothing can be loaded more beautifully than a low side or stanchion car. The most painful thing was the lack of a suitable model on modern railways from the mid-sixties onwards. The Rmms 33, as a high-speed model with a service life until 1977, remedies this very well.

It can be set up as desired in mixed freight trains, even with changing loads or unloaded, individually or in groups. Short block trains with agricultural machinery, car or truck loading are also conceivable. If you want a longer train, you should consider circus transport: For tariff reasons, only two-axle wagons were used in circus trains.

Problems are not to be expected with the new models, which will soon be supplemented by Era IV models and further Era III representatives from the 2019 novelties programme. They run well and evenly, and their weight is not discernible from the outside.

The rolling behaviour in the test was flawless and with a weight of 6 g such a wagon is not too much for the locomotive. We believe Märklin has made a good model choice, and praise their conversion in different variants and the preventing of a brakeman's cab overload, and also commend the excellent construction.



At the end of this article, the wagon with steel slats and stanchions (82130; left) and wooden boards and stanchions (82131; right) are positioned once again for direct comparison. The example on the right with handbrake platform is nominated for this new design for the best new releases of 2018.

Representing this type of car we nominate the Rmms 33 with wooden shelves (82131) for the new releases of the year 2018 in the category of cars. The result of the editorial selection can be found in a separate article in this issue.

Manufacturer of the model:
<http://www.maerklin.de>

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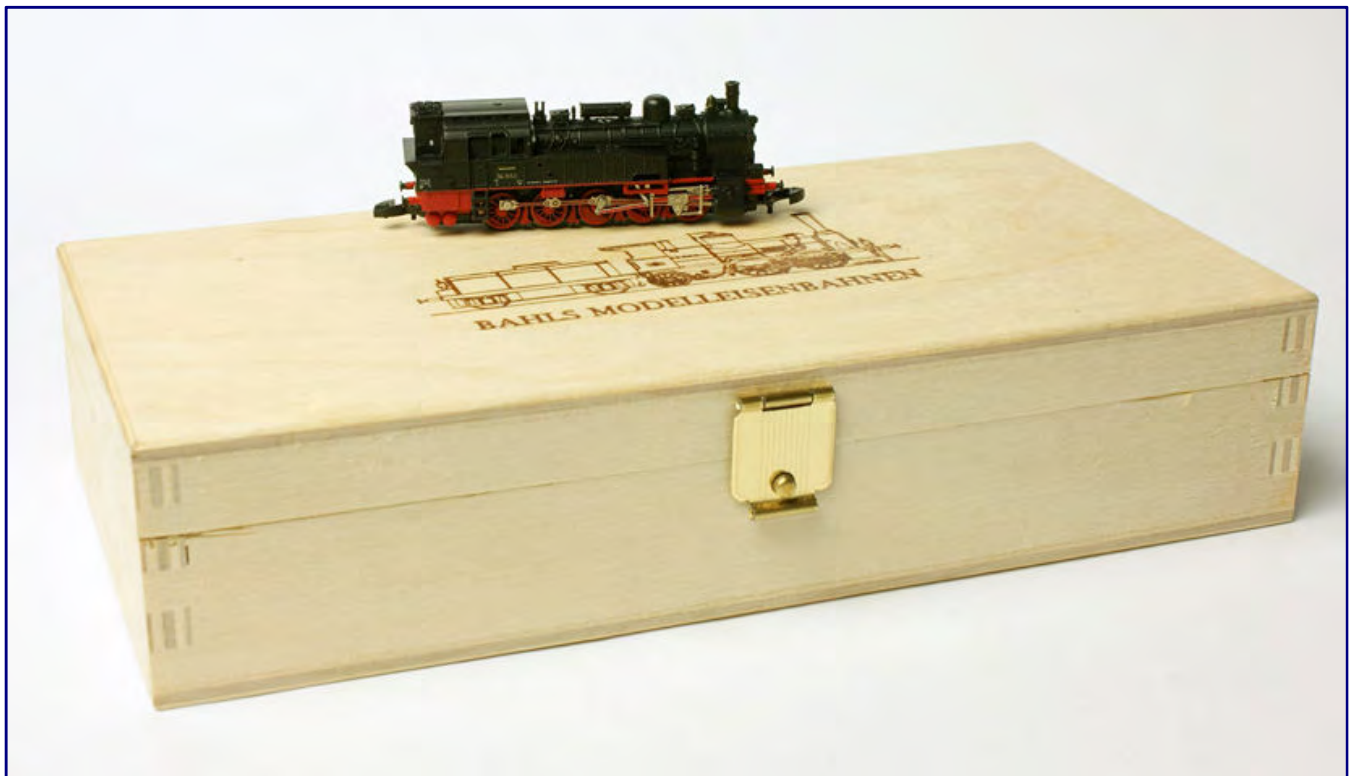
Fine detailing of the series 94⁵⁻¹⁷

Noble Prussians from Blomberg

The better is the enemy of the good. And so today, two thoroughly successful Märklin models are transformed into fine small-series gems that also meet the highest standards. The focus here is on the super range of Bahls Modelleisenbahnen for Märklin models of the 94⁵⁻¹⁷ series and Prussian T 16¹. These models are used on many installations and their reworking makes them even more visible.

It had taken five years for the announcement to become a finished model: Märklin's series 94⁵⁻¹⁷ we had presented it in detail in **Trainini**® 7/2013 as a successful implementation, but already outdated with regard to the linkage.

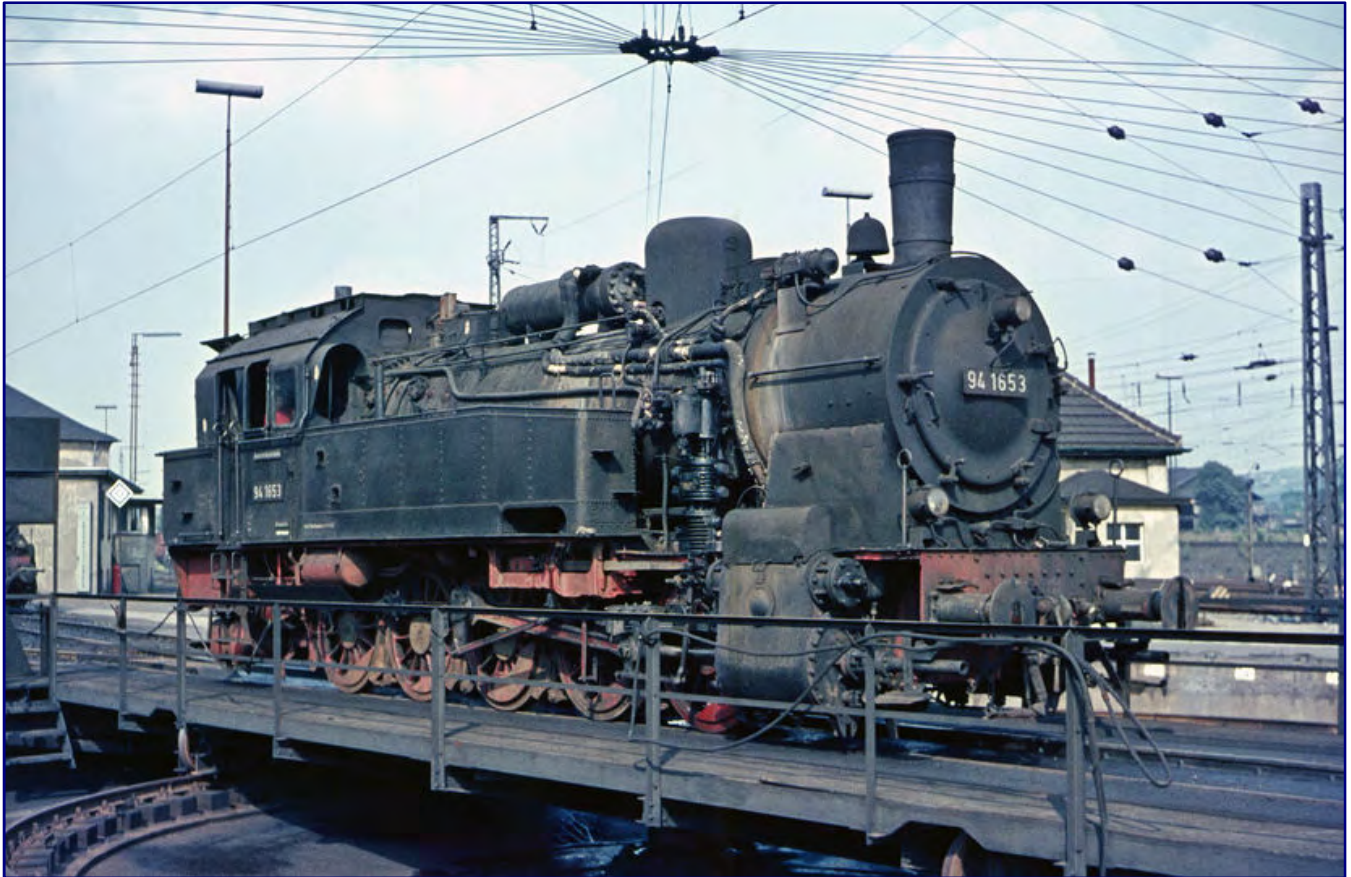
We dedicated separate articles to the big model and the small model. So we delivered a detailed evaluation of the model as well as suggestions for a prototypical use. Five years later, we picked up the miniature again at this point to show suitable superung possibilities.



Based on the Märklin models, Bahls Modelleisenbahnen have been offering finely detailed versions of the tender steam locomotive class 94⁵⁻¹⁷ under article number 5094 since around mid-2018. But we have also done some work on the models ourselves.

Bahls Modelleisenbahnen fixes the small weaknesses, which still go back to the announcement year 2008, when detail controls did not belong yet to the mass production standard. And in order to make a comparison of our two demonstration models possible at all, we are pleased to also be able to show suitable model photos of the finished models.

As a brief introduction, it should be noted that the Prussian T 16¹, the designation of origin of the 94⁵⁻¹⁷ series, was a reinforced design of its predecessor, the T 16. Since it proved itself in every respect, it was procured in large quantities and was still available in the Bundesbahn stock until the seventies.



As a model for the Epoch III model we used the 94 1653, which was photographed here on 24 July 1967, on the turntable of their home Bw Wuppertal-Vohwinkel. Photo: Wolfgang Bügel / Eisenbahnstiftung

It even survived its designated successor, the DB's new class 82 locomotive. It had long since been used not only for heavy shunting service. For almost the entire period of its active service, it also made itself useful as a push locomotive on ramps, sometimes in mainline service in front of transfer trains and even in front of commuter trains on steep stretches, such as in Dillenburg.

This universality, its bulky and characterful appearance and the good driving characteristics as a model make it popular with the Zetties. Many of them have regretted since the first appearance that they have not yet received model maintenance and have returned to the range with complete detail control and then certainly also a bell-shaped armature motor.

How much can be made out of the well running models, we want to show and explain here today. The driving characteristics of the models are characterised by smooth running and good tractive power, especially for a five-coupler.

We don't see any need for improvement here, at best the accommodation of additional weight can never does any harm. All measures that we consider necessary are purely cosmetic nature was photographed.

Our activities start with upgrading the lighting. In the 94⁵⁻¹⁷ series, incandescent lamps are still installed in plug-in sockets ex-works, some of which are covered with shrink tubing that partially shields the light emission.



On 2 April 1971 094 533-7 with P 3214 between Ewersbach and Steinbrücken is on the way on the branch line Dillenburg - Gönners - Wallau. It provided the model for an Epoch IV version of the fine detailing. Photo: Burkhard Wollny / Eisenbahnstiftung

Significantly more economical and also more effective in the low voltage range are the light-emitting diodes

Our editor Bernd Knauf, who died three years ago, had already dealt with this model and identified Märklin's LED lighting inserts of the 01 series as a suitable replacement and successfully tested them.

These can be purchased as „Beleuchtungseinheit vorn“ (“front lighting unit”) under part number E250391.

Thus they found also entrance into our sample models presented here. So that the factory lanterns, which are not exactly to scale, also look better when stationary, we have painted the light channels of one of the models with



The LED insert of the 01 series can be used to upgrade the direction-dependent front lighting. We have pulled the white shrink tube of the former bulb over the LED and the transparent insert.

white paint, and the second model serves for the time being as a basis for comparison of the lighting effect.

If, on the other hand, you generally don't want to see luminous lanterns on the front buffer beam, you can have them removed during the fine detailing by Michael Bahls and exchanged for functionless dummies. The correct replicas in scale size are then also placed on the buffer plank and enhance the appearance of the front once again significantly.



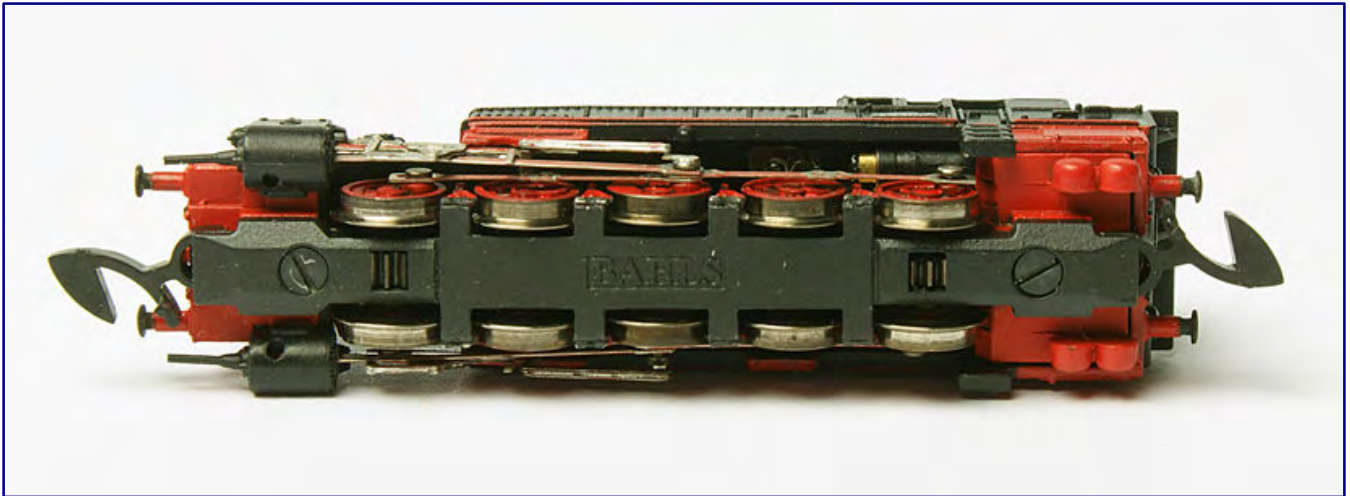
The direct comparison reveals a few differences: The rear model looks a bit better in the front area due to the white backed lower lanterns and the black set off buffer plank equipment. In addition, at 094 533-7 (rear), the chimney top was removed for operation under catenary. On the Märklin model this is only plugged in.

At the front of the locomotive we were satisfied with the correct size of the sleeve buffers with flat and arched plates, the colour of which was correctly red except for the moving parts on the prototype. The factory engravings on the coupling hook and the brake hoses have already been removed from the front and rear of one model with black paint, the other will also be done later.

Further changes to the buffer beam by Bahls Modelleisenbahnen were not necessary. With this set we now focus on the sides of the front buffer beam. It seems that there are no steps to climb up into the shunter.

However, we would like to point out that such an assumption does not have to be correct under any circumstances. This depends on the version chosen: old drawings of the Prussian T 16¹ show the locomotives without steps, which can also be proven by historical photographs.

Later, ascents have been retrofitted - we found at least two types on photographs. We cannot answer the question of whether all the models were actually equipped in this way. So the small-series manufacturer had to make a decision here that we could endorse. Michael Bahls also sets individual special requests when it comes to feasibility in the pocket.



The brake dummies are attached as a folded etched part from below (picture above), so that Michael Bahls can clearly leave his brand mark on the gearbox cover as well. The linkage is fine and detailed, but unfortunately not movable (picture below). Within fine detailing, also cylinders still standing sloped ex-works are directed.

More critical in the area of the front part are in many models problems with the function of the front coupling, because apparently the coupling shaft has a construction error regarding its dimensions. Reworking is just as much a part of the scope of services as replacing it with a fully upgraded buffer beam with coupling hook.

A correction that eliminates a second construction error is also effective: As soon as the housing is placed on the chassis and screwed on tightly, the front boiler support presses on the cylinder block so that it is visibly inclined, as is clearly indicated by the long piston rod protection tubes.

This is the biggest shortcoming of Märklin's successful model. Michael Bahls reworked the chassis block here, so that this phenomenon no longer occurs. Here, we see an important foundation, so that the modified locomotive can exert its full force at the coupling hook.

They consist of the standard program of the steam locomotive specialist, with which he has made a name for himself among his customers: In our opinion, the most visually successful replicas of steam locomotive controls are all made by Bahls.

In 2012, even before Märklin, a full-blown implementation was introduced here that can hardly be surpassed in its filigree and delicacy.

With the 94⁵⁻¹⁷ series, however, there is the problem that such a solution cannot be implemented due to the limited space in the chassis area.

Bahls Modelleisenbahnen therefore uses the technology of his earlier detail controls and presents the old Prussian with a rigid crosshead, leading lever and swinging bar.

On machines with rather small wheels, this is of little importance, especially as the coupling rods still impress with indicated lubricating vessels and bearings.

To the standard of the conversions belong also replicas of the one-sided acting brakes and larger counterweights on the driving axle.



Etched parts were also used to achieve a replica of the rear driver's cab windows that was correct in shape and size.



The larger counterweights on the driving axle and the real carbon coating of the tender also contribute to the realistic appearance. The computer number plates of 094 533-7 are from HOS-Modellbahntechnik.

It is also effective that the Bahls steam locomotive wheels first painted black and then put a second coat of red paint on the spokes, wheel tyres and axle centres. This creates an impressive depth effect and at the same time makes the spokes look more filigree. The body is reworked in matt black, with the steam bell losing its disturbing metal surface.

The back of the locomotive is also worth a look. Here, too, it was not enough to set the functionless, lower two lanterns down completely in black. Here, too, we find exchanged buffers with a true-to-original colour scheme and a real carbon layer in the tender box. In Blomberg, however, many thoughts also circled around how the driver's cab could be upgraded.



Same, but different: the comparison conveys a small hint of the diversity of the appearance of ninety-four at DB. While 94 1653 (front) bears a Bundesbahn lettering, 094 533-7 (back) has a "biscuit" that is screwed on. Also the chimney heights are different. There would have been further differentiation possibilities with the cab ventilation and coal and water boxes.

As we once noticed, the window replicas of the Prussian version without coal box elevation have been considerably better implemented, while the DRG and DB variants showed weaknesses here. With the help of etched parts no window break-through was achieved, but the finely detailed models now carry, like the bigger model, a frame recognizable as such, which changes its "face" for the positive.

The fine detailing work, whose extent always the customer individually determines, are rounded off by signage with new locomotive number. This, too, can be chosen almost freely, with Michael Bahls orienting his proposals on models documented in a suitable building condition.

We have researched our desired numbers ourselves and were able to include all four locomotive pages in our evaluation. One of the models, 94 1653, is supposed to represent the time before 1968 and instead of the DB logo, bear the lettering "Deutsche Bundesbahn." Corresponding signs in etching technology are thus part of the scope of supply of Michael Bahls.

The second model of the 094 533-7 from Bw Dillenburg reproduces the last operating condition as it was read on the locomotive after renumbering according to the registration valid from 1 January 1968. However, we deliberately refrained from reproducing the silencer of the Riggerbach counter pressure brake directly behind the smoke stack. While an etched "biscuit" is still missing to perfect the effect, new number plates can already be found on the model.



As once in Dillenburg, 094 533-7 is here with a passenger train of Donnerbüchsen baggage car and four-axle conversion cars on the way (picture above). However, it is allowed to drive local freight trains (picture below), which can then be put together as desired.

The original originals were once screen-printed and therefore no longer bore raised numerals with a metallic appearance. Their lettering was printed in white on the black plates, but the Bundesbahn did not change the ownership logo for the steam locomotives. They remained on the locomotives as long as they had not been stolen by "railway friends".



Only a few suitable steam locomotives are available in gauge Z for the Langenschwalbach passenger cars. Our 94 1653 may therefore also make itself useful in front of them and take the rather unfamiliar looking passenger coaches out on the track.

Etched plates would be pointless for a locomotive number valid since 1968, but Bahls Modell-eisenbahnen unfortunately has no printed alternatives in its program. One solution is offered by Heinz O. Schramm from HOS-Modellbahntechnik. In scale size and in correct font he is able to print locomotive signs flawlessly according to the customer's wishes.

We made use of this possibility with pleasure and were able to take our 094 533-7 back to the time when it was still used by the Dillenburg Railway Station in the passenger train service on the ramps. We are pleased to be allowed to send the model now also times with change carriages in the tow on the model railway layout.

Manufacturer of the basic model:

<http://www.maerklin.de>

Fine detailing of the basic model:

<http://www.bahls-modelleisenbahnen.de>

Printing of the computer number plates:

<http://www.hos-modellbahntechnik.de>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Baureihe 94 im Portrait

Populäre Kraftprotze

Fehlanzeige hieß es, wenn jemand nach einem Baureihenportrait der preußischen T 16 und T 16¹ suchte. In den 45 Jahren nach Erscheinen des ersten solchen Bands gab es nur ein weiteres Buch mit deutlich geringerem Umfang, doch auch das liegt schon wieder über 20 Jahre zurück. Die Erwartungen an den überarbeiteten und erweiterten EK-Band lagen folglich sehr hoch.

Hansjürgen Wenzel
Die Baureihe 94
Die preußischen Tenderloks T 16 und T 16¹

EK-Verlag GmbH
Freiburg 2018

Gebundenes Buch
Format 21,0 x 29,7 cm
464 Seiten mit 650 teilweise farbigen Abbildungen

ISBN 978-3-8446-6017-3
Best.-Nr. 6017
Preis 54,00 EUR (Deutschland)

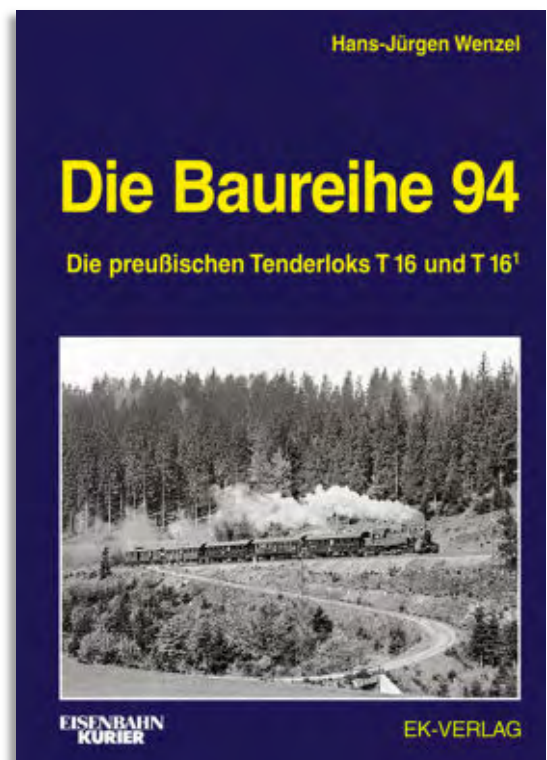
Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Kaum zu glauben ist, dass es fast zwanzig Jahre lang keinen Buchtitel mehr auf dem deutschsprachigen Markt zu kaufen gab, der sich der Entwicklung und Betriebsgeschichte der preußischen Tenderlokomotiven T 16 und T 16¹ widmete.

Immerhin gehörte doch gerade die kräftige Weiterentwicklung, die die Schwächen ihrer Vorgängerin mit großem Erfolg überwand, mit über 1.500 Exemplaren zu den bekanntesten und langlebigsten Konstruktionen aus der Länderbahnzeit. Und so ist sie auch bei den Modellbahnern stets beliebt gewesen, zumal sie auch auf der Strecke einsetzbar ist.

Ein von Hansjürgen Wenzel geschriebenes Buch war das erste Werk, das sich der Baureihe 94 derart ausführlich zuwandte. Es erschien bereits 1973 im Rahmen einer Reihe der Arbeitsgemeinschaft Eisenbahn-Kurier e.V., war aber mit 246 Seiten im Umfang noch erheblich kleiner. Das neue Buch ist mit 464 fast doppelt so dick und hat mit den üblichen Baureihenportrait-Maßen auch ein größeres Format.

Freilich haben wir diesen Urahn des heute zu besprechenden Buches damals nicht selbst lesen können, doch es fällt nicht schwer zu erahnen, dass der Zuwachs nicht allein größer wiedergegeben Bildern oder einer geänderten Schriftgröße zugeordnet werden kann. Immerhin stand die Baureihe 94⁵⁻¹⁷ bei Erscheinen der Erstauflage noch bei der Bundesbahn unter Dampf.



Da sie noch kein abgeschlossenes Kapitel im Betriebsmaschinendienst darstellte, ließ sich das Erfordernis einer Erweiterung schon früh ablesen. Dass dies nach der zweiten und letzten, unseres Wissens nach inhaltlich unveränderten, Auflage von 1980 fast vierzig Jahre dauern würde, überrascht uns im Rückblick sehr.

Umso gespannter waren wir zu Lesebeginn, was uns denn nun erwarten würde. War der Ursprungstitel noch durchweg schwarz-weiß bebildert, so enthalten die modernen EK-Werke der „blauen Reihe“ ja immer auch einen Farbteil, den wir und sicher auch alle anderen Leser sehr schätzen.

Die Wiedergabequalität auch hoch betagter Fotografien spricht beim EK-Verlag Bände, bislang hatten wir hier noch nie einen Anlass zur Klage, Werke anderer Verlage werden häufig sogar noch übertroffen. Auch hier finden wir die gewohnt gute Qualität wieder; ein Lob, das auch für die gute und aussagekräftige Bildauswahl mit immerhin 650 Motiven gilt.

Kommen wir zu den Inhalten und dem Umfang des Buches. Die preußischen Fünfkuppler gehörten zu den wichtigsten und bekanntesten Güterzug-Tenderdampflokomotiven, die auf deutschen Schienen unterwegs waren. Neben dem Verschub auf Rangierbahnhöfen bewährten sie sich sowohl vor Übergabe-Güterzügen als auch auf Steilstrecken im Zug- und Schiebedienst. Mit ihnen konnte einst der Zahnradbetrieb auf vielen Strecken durch den Reibetrieb abgelöst werden.

Die zwischen 1905 und 1924 in mehr als 1.500 Exemplaren gebauten Maschinen waren in ganz Deutschland zu finden. Die Deutsche Reichsbahn hatte sie einst als Baureihen 94²⁻⁴ und 94⁵⁻¹⁷ in ihren Bestand übernommen. Bei der DB stand die Baureihe 94⁵⁻¹⁷ noch bis 1974 im Dienst. Mehr müssen wir sicher nicht an Ausführungen verlieren, um ihre Bedeutung zu unterstreichen.

Hansjürgen Wenzel hat sein Ursprungswerk vollständig überarbeitet und ein neues Standardwerk zu den beiden darin behandelten Loktypen vorgelegt. Die Entwicklung und Technik der Maschinen wird in recht kurzem, aber angemessenem Umfang gewürdigt, auch die Verwandten und Vorgänger im Steilstreckendienst werden angerissen, soweit dies für das Portrait hilfreich ist. Ein Exkurs ist daher auch der Neubaulok Baureihe 82 der DB gewidmet, die eigentlich als Ablösung gedacht war.

Der Einsatz bei den Direktionen wird sinnvoll nach Vor- und Nachkriegszeit getrennt beschrieben, bestimmt aber wie auch bei anderen Werken den Großteil des Bandes. Nicht vergessen wurden aber auch die erhaltenen Museumslokomotiven, mit denen die Erinnerung an diese Maschinen wach gehalten wird.

Eine bedeutende Rolle in der Geschichte dieser Konstruktion spielte auch das Ausland, denn viele Exemplare gelangten durch Kriegswirren und -folgen auch zu anderen Bahnverwaltungen. Nur die an die RAG verkauften Maschinen kommen im Umfeld der an Privat- und Werkbahnen verkauften Lokomotiven für unseren Geschmack etwas (zu) kurz weg.

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Publishing pages with reference possibility:
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<https://www.eisenbahn-kurier.de>
<http://www.ekshop.de>
.....

Kräftig erweiterter Klassiker Jede Menge Güterwagen

Vor dreißig Jahren erschien der erste Band der heute legendären Güterwagen-Reihe von Stefan Carstens. Längst vergriffen und inzwischen auch überarbeitungswürdig, hat sich der Autor von einst mit Hilfe durch Paul Scheller an ein zeitgemäßes Buch gemacht, in dem viele neue Erkenntnisse verarbeitet werden konnten. Gleichzeitig wird es in seinen Strukturen an die jüngeren Bände angepasst.

Stefan Carstens – Paul Scheller
Güterwagen Band 1.1
Gedekte Wagen Länderbahn + DRB

Verlagsgruppe Bahn GmbH
Fürstenfeldbruck 2018

Gebundenes Buch
Format 21,8 x 29,7 cm
208 Seiten mit über 600 S/W- und Farbbildungen
sowie 157 Typenzeichnungen

ISBN 978-3-89610-652-0
Best.-Nr. 15088144
Preis 45,00 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Auch das an dieser Stelle zu besprechende Buch war ursprünglich als überarbeitete Neuauflage eines längst vergriffenen Vorgängerbands gedacht. Das Erstlingswerk „Güterwagen Band 1“ erschien 1989, damals noch außerhalb der VG Bahn, als Auftakttitel der heute wohl am stärksten verbreiteten Buchreihe über deutsche Güterwagen.

Einige Bücher später und längst in den Einflussbereich der Miba umgezogen, gab es auch Neuauflagen des ersten Bands. Doch hinsichtlich der Struktur hatte sich die als Standardwerk etablierte Reihe im Eigenverlag zwischenzeitlich schon weiterentwickelt.

Überarbeitungsbedarf ergab sich auch daraus, dass mit dem Fall der Mauer auch Informationen aus dem Osten und damit dem Bereich der früheren DR leichter zu beschaffen waren und sich hier verarbeiten ließen. Verändert und erweitert wurden auch die Datenspiegel zu den beschriebenen Güterwagenbauarten.

Ebenfalls nicht unwesentlich war die Erkenntnis, dass auch der im Buch mitbehandelte Modellbau längst über das Jahr des Ersterscheinens hinweggegangen war. Neue Modelle sind erschienen und sollten im Rahmen der Beschreibungen auch Eingang bezüglich Bewertung und Superungsmöglichkeiten finden.

Vorgestellt werden jetzt über 50 Modelle aus verschiedenen Entwicklungsjahrzehnten, die namhafte Modellbauer verbessert haben. Freilich umfasst das unverändert nur die Spurweite H0, Anregungen finden hier durchaus aber auch die Freunde der anderen Baugrößen.

Und so war es im November 2018 dann endlich so weit: Stefan Carstens hatte sich als Mitautor Paul Scheller zur Seite geholt und präsentierte eine umfassende Überarbeitung und Erweiterung des



Ursprungswerks, die den vorherigen Rahmen sprengte und auf zwei Bände aufzuteilen war. Dieser Band 1.1 ist das Ergebnis aller Fleißarbeiten, die Ergänzung durch Band 1.2 soll noch im Winter 2019 folgen.

Das Themenfeld dieses ersten von zwei Bänden sind die gedeckten Wagen und die meist auf gleicher Konstruktion fußenden Verschlagwagen, soweit sie von den deutschen Länder- und Staatsbahnen oder der Deutschen Reichsbahn bis 1945 gebaut wurden. Auch Beute- oder Fremdwagen, die dauerhaft in Deutschland verblieben bzw. nicht zurückgegeben wurden, haben die Autoren in dieses Buch integriert.

Auch die weitere Verwendung und Modernisierung durch die beiden deutschen Staatsbahnen der Nachkriegszeit wird mitbehandelt, solange aus den Maßnahmen keine abweichenden Bauarten entstanden oder die Wagen von Grund auf neu aufgebaut wurden.

So entstand ein Werk, das in seinem Umfang und seiner Vollständigkeit nicht mehr zu überbieten ist, obwohl es gleichzeitig klar gegenüber Band 1.2 mit Nachkriegsbauarten abgegrenzt ist. Dass es sich als neues Standardwerk etablieren wird, steht außer Frage, denn es gibt nichts Vergleichbares im deutschsprachigen Buchmarkt.

Für Modellbahner, die im Eigen- oder Umbau von Modellen tätig werden wollen, sind die 157 Zeichnungen des Bands auch in Zukunft unverzichtbar. So wurden sie neu gezeichnet und bilden wieder eine hervorragende Basis für geplante Projekte. Ihre Wiedergabe ist, wie auch die der Fotos, ohne jeden Tadel.

In den etwa 600 Fotos steckt zudem der Hauptgrund, warum auch Besitzer des Ursprungswerks zugreifen sollten und sich nicht vom alten Buch trennen müssen: Sie bieten nämlich nicht nur eine umfassende Darstellung aller Bauarten, sondern wurden überwiegend auch getauscht, um Wiederholungen zu vermeiden und besser geeignete Aufnahmen einbinden zu können, auf die vor 30 Jahren noch kein Zugriff bestand.

Noch keine Ausführungen haben wir zu den Texten geschrieben: Die Reihenfolge der Bauarten wurde geändert und neu strukturiert. Sie werden nicht mehr nach ihren UIC-Bezeichnungen abgearbeitet, sondern zunächst nach festgelegten Kriterien, orientiert an der Chronologie, zusammengefasst.

So sind Spezial-, Fremdwagen und auch Leig-Einheiten in eigenen Kapiteln zu finden, wie auch die noch nicht einheitlich gebauten Länderbahnwagen sowie die Wagen der Verbands-, Austausch und geschweißten Bauart. Davon abgehoben werden die vereinfachten Kriegsbauarten. Diese Vielfalt deutet sich bereits auf dem Buchdeckel an, auf dem der gelbe Elefantentransportwagen des Zirkus Krone strahlt.

Die gelieferten Informationen sind umfassend, tiefgreifend und gut recherchiert. Dass sie auch verständlich und kurzweilig geschrieben sind, erleichtert den Lesefluss und die Wissensaufnahme. So kommt wahre Lesefreude auf und bestätigt, dass dieser Titel seinen Preis mehr als wert ist.

Und so sind beim Lesen unsere Zweifel verflogen, ob es richtig war, wieder am Beginn der Buchreihe anzusetzen, statt zunächst die noch offenen Themen bei den Kessel- oder Tiefladewagen sowie dem kombinierten Ladungsverkehr abzuarbeiten.

Deshalb haben wir „Güterwagen Band 1.1“ auch für die Neuerscheinungen des Jahres 2018 in der Kategorie Literatur nominiert.

Publishing pages with reference possibility:
<https://shop.vgbahn.info>

The year in review 2018

Colourful and varied

For a long time now, we have also been looking back at the beginning of the year and looking at what has occupied us in the last twelve months on Z-gauge. This review will get us in the mood for what will soon be coming to us and hopefully make our model railway hearts beat faster. Also today we pause and let the past year 2018 pass in review once again.

As in previous years, we would like to start the New Year with a review of the old one. What we really relished in 2018, what demanded our patience and what anniversaries we celebrated should be remembered? This can put us in the right mood for what we are expecting and hoping for, presented in the form of new products and finally delivered.

And this is already the first anniversary of the past year: The Märklin Insider Club celebrated its 25th anniversary.

When it was launched in 1993, the Mini-Club still seemed to be a stepchild. H0 gauge was covered with exclusive products, for Zetties there was just one year old car.

It was not until the following year that they were able to enjoy a club locomotive. The box locomotive 110 155-9 with blue roof and red federal railway logo (art. no. 8834) was by no means a new design, but only a special design.

What difference does the just as blue class 141 make as Insider Model 2018, which unfortunately has not yet been delivered! Such contrasts show why the anniversary was a reason to celebrate!

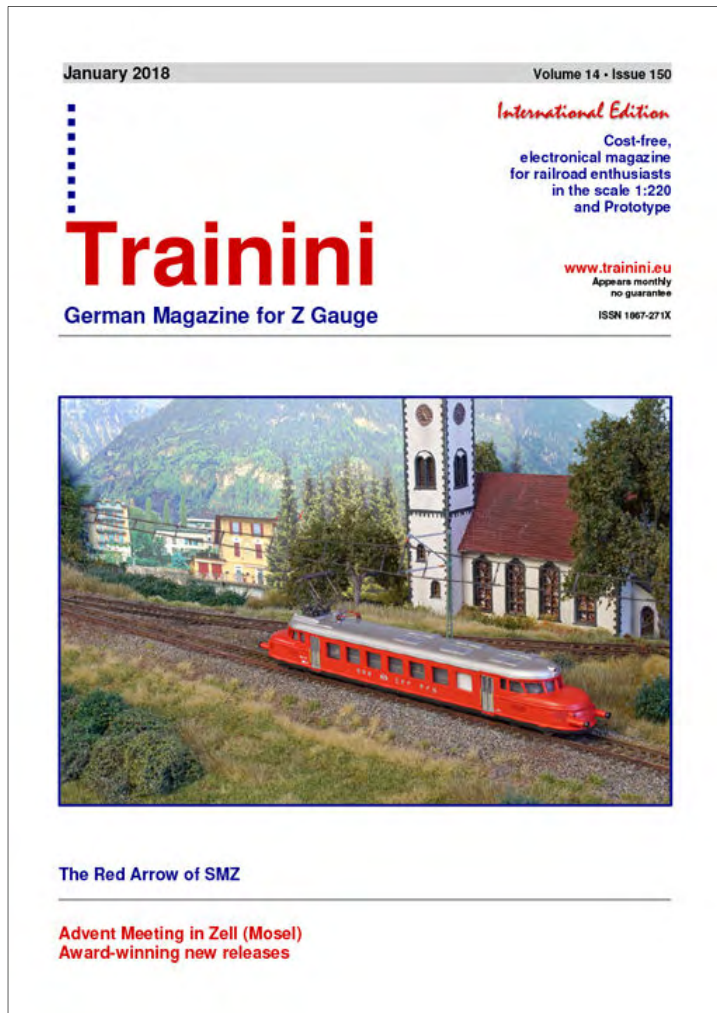
Only the secure basis of loyal customers allows new designs and technological progress, which has also been proven by the delivery of the class 80 tender steam locomotives.

With its higher weight and new drive, it brings power and driving culture to the rails, and its visual implementation also sets high standards. To illustrate this unmistakably, it only needs to be placed next to its sister series 89 from 1972.



In 2018, the Märklin Insider Club celebrated its 25th anniversary.

Märklin's pride in its customer club was evident at every trade fair in the anniversary year: The silver anniversary was toasted with Prosecco or optionally orange juice. But also our editorial staff had reason to celebrate! Starting with the January issue 2018, the *International Edition* joined the German edition of our magazine.



At the same time and shortly afterwards we were able to welcome a total of three new colleagues to the **Trainini®** team: Martin Stercken, a native speaker, took over the responsibility for the English translations, and Alexander Hock and Christoph Maier support him.

The three translators currently work on each issue in the best coordinated and disciplined way.

The praise from our readers confirms this initiative and we are happy to be able to fulfil the wishes of our readers outside the German-speaking world, which have been expressed over many years, thanks to this now established trio.

A first highlight of every New Year is always the awarding of particularly successful new shapes and innovations from the previous season. The editorial decisions for the new publications of the year are always preceded by extensive discussions in order not to forget or overlook anything important.

Among the outstanding achievements of the year 2018 were two that are closely linked to each other and, like no other media event, were well received outside our magazine: the

Z-Car system from KK Produkcja and its technical pioneer Hermann Kammler.

This makes it all the more regrettable for us, and for sure also for the excellent one, that the certificate has still not been handed over and has since been signed by all three editors. There were three reasons for this: Forgetting the certificate when leaving for Altenbeken, the postal delivery times with a view to personal absences and finally the failure to make a planned surprise appointment. But postponed is not cancelled!

Surely you will now think that we are ready for the island? You're not so wrong, because that was already the case in February 2018. We took a trip to Kniephaven and portrayed the Z-gauge layout shown on the OnTraXS! in detail, as the cover picture has already shown.

In March, Märklin's "rocking horse" was finally a big topic. The newly delivered Series 80 was not only a new design that was delivered with a new engine and successful aging, but also an innovation in terms of its production technology: brass precision casting had never existed before from Märklin.

The good response in letters to the editor is certainly also worth mentioning. We are always happy to receive incoming letters from our readers.

Not all of them are intended for publication, but they all provide us with important insights into where the shoe pinches, which topics are popular or desired, and what expectations exist at all.

Today is the right moment to thank you and encourage you all to keep going!

We would like to cite the April issue as a prime example of where such impulses lead: all three main topics came from among our readers. It doesn't matter whether a model or construction project, texts or pictures were supplied: **Trainini®** should thrive for you and with you.



After a long wait, models of the 80 series were delivered to customers. It was the first Märklin (Z) locomotive to be produced using the brass precision casting process.



In the early hours of the morning, wafts of fog are still moving over the Altenbekener Viaduct, but soon it should start very close by and be over with peace and quiet.

The undisputed highlight of the year for gauge Z was the meeting in Altenbeken. By the middle of the month, well over 1,000 people were drawn to the railway town on the Egge Mountains. And as expected, the level of the show was once again greater than the previous edition. This is to engage and encourage, because even in the small scale 1:220, cooking is done with water.

Small tips and experiences put and put many of our readers in a position to create true works of art, where before there was still the assessment: "I'll never make it". This was also proven by the diorama competition of the railway magazine, which started in the middle of the year and apparently surprised those responsible there because of the good quantitative and qualitative response.



The meeting in Altenbeken was international and multifaceted, as these two motifs show: a rural motif of a Polish participant (photo above) and a bridge over the Spree of the Berlin Z-Stammtisch (club)(photo below).

But back to the Z gauge weekend: The many conversations between like-minded people, which are often forgotten or overlooked at an exhibition, are clearly on course. The meeting close to the viaduct is a kind of family reunion with participation not just from all over Europe.

The delivery of two special Märklin novelties was close at hand. The “Heckeneilzug” car wrapping seems inconspicuous at first, but it is the inner values that count. Here all four wagons in three different designs had an interior. We regard this commitment to product improvements as a clear sign from Göppingen.

The Kittel steam railcar was also spectacular on the outside, with its sophisticated simplicity and the finest details that caused great amazement: a small-series manufacturer model at a large-series manufacturer price? The traditional manufacturer has really set new standards here.



The Kittel steam railcar (Art. No. 88145) was a great highlight among the models of the year 2018.

Our big article about the VW Beetle in June did not meet with undivided restrictions. When the tinkering time was over, we wanted to honour this car's jubilee, which has also played a big role in 1:220 scale modelling off the tracks. But the letters we received also showed that cars on model railway layouts should not be neglected.

At the same time we had placed one of our rare airplane model presentations due to the not always reliably planned delivery dates, but many readers had to miss out on the train at the beginning of summer. Both topics were accompanied by the construction report on the Dörpeder Hof by Archistories for the 1zu220 shop.

The Digital series, on the other hand, was very well received throughout the year. If no contribution could be found in a month, we immediately received reader input and questions as to when and how to proceed. We in the editorial staff then gave a lot of thought to all the feedback described in order to draw the right conclusions from it.

Two other things marked the middle of the year. On the one hand, SMZ from Austria provided an “aha” effect with the announcement of a kinematic close coupling in the model of the Rh 5147, because something like this has never existed in the 46 years of Z gauge.



SMZ attracted attention in mid-2018 with the design of a kinematic close-coupling for Z gauge. For the first time, the Rh 5047 diesel multiple-unit train from ÖBB was equipped with it. Photo: Sondermodelle Z

It was a pleasure for us to be able to count **Trainini®** among Germany's bibliographic heritage in the future. The German and international editions of our magazine were recognized as part of the German cultural heritage and thus added to the compulsory collection area of the German National Library (Frankfurt am Main / Leipzig).

Our cover story in July, in keeping with the centennial summer, sparked a holiday mood and with it a desire for the sea. The contribution of the Museumsbahn Küstenbahn Ostfriesland e.V. was dedicated to this event. (MKO), whose former Doornkaat wagon we reproduced as a model.

This topic has a special appeal, especially since the Z-friends have already turned their international attention to this troupe:

One of their annual carriages carried advertising addresses from Onno Behrends and is today on its way for the MKO in exactly the same lettering. What attracts us, on the other hand, is the already historical design with which it was once reconditioned and brought to the museum train.



In keeping with our summer holiday, we thematised the Museumsbahn Küstenbahn Ostfriesland e.V. (coastal railway museum). (MKO) with the replica of their former Doornkaat wagon.

For a short time it was on the road together with our model's prototype, but that's another story that you might find in our magazine in 2019 or 2020.

In the same issue, Christoph Maier showed what our focus topics for the year can lead to. On his small table system, Winzlingen, he successfully processed and applied the knowledge he had gained from our big series of topics in 2017.

But in 2018 we dedicated the year to technical contributions with a focus on basic knowledge for digital beginners. It was a good coincidence that Alexander Hock also presented the various Z-gauge engine concepts together with their advantages and disadvantages parallel to this series. Both of the latter authors are now members of our translation group.

The appeal of agriculture dominated August in our magazine. At harvest time we presented the equipment with which farmers could drive on the fields on a scale of 1:220. With the title photo we made it clear that some of them are also excellent loads.

Our readers or we had no idea that one month later Märklin would announce with the Rmms 33 stake car that would be better suited as a means of transport for such purposes than any other previous models of this kind.



In 2018, the Uerdingen rail bus continued to write Z-gauge history: technically revised, perfectly printed and with interior fittings, the basic model from 1973 returned to the range.

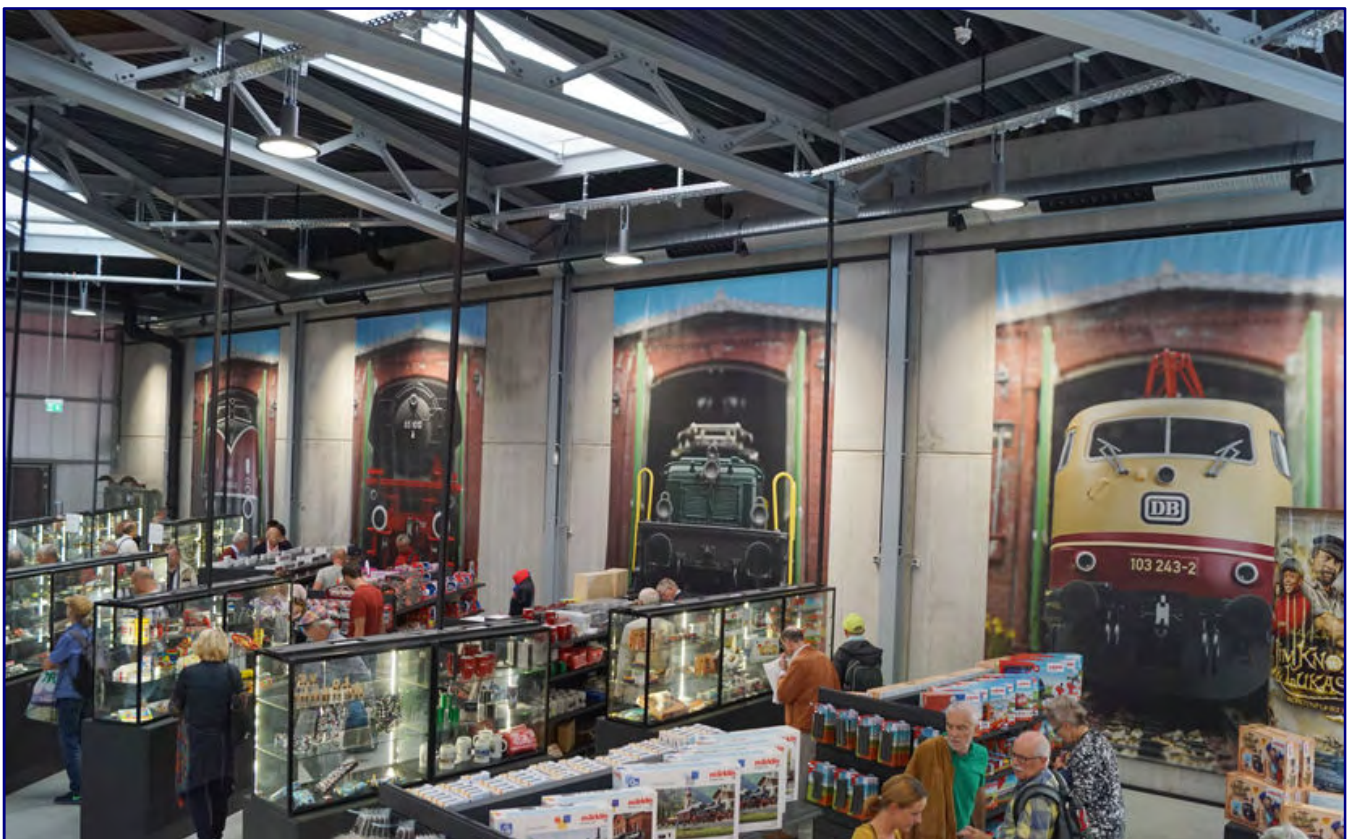
Z gauge history was written by the rail bus from Uerdingen. It appeared for the first time in 1973 and clearly outdid its relative in the HO program. Some compromises it brought, however, already at that time with itself, and also a prototype error is probably owed to the larger HO model, from which it was probably assumed.

35 years later, a comprehensively revised edition showed how far painting, printing and engine technology have developed since then. The crown probably set the scene for the youngest model with the interior decoration set off by warm-white LEDs, which shone in a smoky blue and was produced using 3D printing.

The Magigras technology, which we turned to in September, also appears innovative. The products we work with have been known for years in their own right for almost all of them. However, the composition and combination with a conductive synthetic resin is completely new.

It takes a lot of practice and skill to exhaust all the possibilities of the French invention. But it is worth it and also a lot of fun. The next step or development is trees using the same working method. After all, they were the topic of the November issue.

An important step for Märklin was the opening of the first construction phase of the new Märklineum (new Märklin museum). About a year before the planned handover of the new premises for the company museum, the large rotunda has been open since the end of September 2018, where the current program of all Märklin brands is shown, museum cars, souvenirs and special items are available, where repairs are also accepted, and spare parts are sold.



The shop and checkout area of the new Märklineum, the first construction phase of which was opened in September 2018, gives the impression of a round locomotive shed with an internal turntable.

A contact point for visitors to Göppingen is also the integrated treasure trove, in which manufacturer's seconds and leftovers from overproduction are sold. The design of the museum as a round shed with cash desk area as a centrally located turntable certainly appeals to all friends of the Märklineum.

FR Freudenreich Feinwerktechnik courageously took the step of realizing a NOHAB in small series. One of the models will also be available as an exclusive edition in the 1zu220 shop. Although it seems to be



perfectly implemented in many details, conscious competition with a Märklin product is still a daring step. The first locomotives will be delivered before the end of this month.

Our look at the freshly delivered V 200 052 from the Krauss-Maffei-Baulos in October showed us a lot of joy about the appealing design and a little bit of displeasure about an avoidable model error.

To this day, Märklin does not seem to have decided whether the mistake in the decorative line at the front will be corrected in the second edition.



But the great shortage of passenger coaches for the years before 1960 is also becoming ever clearer: What should the parade and express locomotive of the time be able to do that was not yet available in colour versions?

The 26.40-metre passenger coaches in the Märklin range can only be used in the 1960s in a way that is befitting the original image.

Our Part 7 from the series "Maintenance & Care of Small Railways" was almost a premiere: After a long time we have continued this series once again.

A brand new NOHAB (photo above; front) was a courageous decision by FR Freudenreich Feinwerktechnik, after all Märklin also has this type in its portfolio. The V 200 052 (photo below), which was delivered with a faulty decorative line guidance in the front area, made the absence of passenger coaches from their early service period painfully clear again.

The response from the readership confirms that there is a great need for such assistance in dealing with the precision mechanical models.

We were ahead of time in November with the presentation of the Raiffeisen warehouse from Märklin, designed by Modellbau Laffont. With Märklin's approval, we completed the trial assembly and gave feedback on the kit and its instructions. This honourable task certainly gave us as much pleasure as reading our impressions.

After all, the result is something very special: such a warehouse can be perfectly integrated into a model railway for shunting and loading tasks, and the hardboard kit is an absolutely contemporary and appealing building.

The portrait of the diorama "Am Mauspfad" (on the mouse path) was meant to get in the mood for our report from the Köln fair, as it was a contribution from the last edition of the diorama building competition, the results of which will be shown at this exhibition.

Our editor, Dirk Kuhlmann, used the article to present his idea of an effective presentation of the regulars' table boards of about the same size. However, these did not play a role at the Internationalen Modellbahnausstellung (International Model Railway Exhibition), which also applied to the Faszination Modellbau in Friedrichshafen. However, Z gauge was well represented at both fairs, which is why we dedicated separate reports to both events.

As a surprise, Märklin presented the Insider annual models for 2019 in November. Once again there will be a Miele freight car, but Era III will now be the perfect time for friends of the most strongly represented model period. Most did not expect the class 0310 express steam locomotive, which cuts a fine figure thanks to the new boiler and is easy to recognise in its design language and remains unmistakable.

We almost seemed to have forgotten one jubilee in the year 2018. May marked the 90th anniversary of the first ride of a Rheingold train. On 23 October 1977, the historic train set was granted the privilege of providing the rolling stock for the last scheduled steam service at the Deutsche Bundesbahn.



At the end of the year we also paid tribute to the 90th anniversary of the Rheingold train with the conversion of the 18 505 and a Märklin product overview, because the train has a long tradition there.

All in all, this resulted in a comprehensive and exciting review of German railway history, into which we also integrated the queen of steam locomotives in the form of the class 18⁴⁻⁵. Our contribution to the conversion of the 18 505 was also very well received by our readers.

With a report on beginners' experiences in the digital field, we concluded the focus of the year 2018. What remains after six parts is the realization that articles on this topic will also be available in the future.

And so we are already forging ideas as to which topics we can use to reach our readership in the new 2019 volume and we are looking forward to delighting and informing you about them. As soon as the spring new products of the large and small series manufacturers' area available to us, we will present them in detail in the next month(s).

Look forward with us to a certainly exciting New Year!

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Editorial award for special models

New products for the year 2018

When a model year is over and the upcoming new products are not yet known, the moment has come to take stock of the past period. As in prior years, our editorial team takes a hard look at what manufacturers have marketed during the last year. After long discussions, we vote, by category, on which products will receive our annual editorial awards.

The agony of choice is always a special thing. Every editor has one vote when it comes to awarding prizes to new publications in 2018. Many considerations and quality criteria are taken into account when we try to find out which of the nominated products deserves an award.

Once we are aware of all the positive characteristics of the different contenders in a given category, picking a winner can quickly turn into a difficult balancing act of weighing up the weak points of each nominated product.

It is usually the small things which make a difference, as you will see. Surprisingly, at the end of our assessment process we often agree amongst ourselves which model or product we want to award the prize for. Nevertheless, we do not take decisions lightly and we have never found it easy to arrive at a decision.



Märklin's steel-blue Z gauge version of the class E 41 012 came too late to enter the competition for the new 2018 releases: The decisive factor for inclusion in our annual awards is always the year a model goes on sale.

In this sense, we are almost glad that Märklin did not compete against itself in the locomotives category, the most prominent of all of the categories. If their class E 41 had been delivered before the end of the year and if their class V 80 had not been delayed yet again, some models which do deserve the spotlight might have been pushed into the second row.

With these introductory words, we now turn to the individual categories and announce the winners:

Locomotives Category

In our opinion, nothing really new in terms of locomotives hit the market in 2018 from the side of the small-series manufacturers. While FR Freudenreich Feinwerktechnik announced a completely, newly designed model of the NOHAB diesel locomotive, it is not scheduled for delivery until the end of January 2019.

Bahls Modelleisenbahnen also did not deliver a new locomotive of its own design last year and instead introduced two Märklin-based super-detailed models to the market: We present the class 94⁵⁻¹⁷ in detail in this issue, but did not obtain a specimen of the class 39 in time to be able to evaluate it ourselves.



The class DT 8 "Kittel" steam railcar of the Deutsche Bundesbahn (art.no. 88145) turned out to be a very impressive model, even if it did not keep everything that had been promised and announced before its release.

The situation is similar with the always extraordinary innovations of Sondermodelle Z. Here, too, no extensive testing was possible, so that we could only do some partial assessments. As for Rokuhan, there was nothing new for the European market and other models also did not meet our eligibility requirements for nominating a product.

In this category it therefore came down to a contest between two Märklin models: the class 80 steam locomotive (art. no. 81352) and the "Kittel" steam railcar DT 8 (88145). Märklin's massive investments into Z gauge innovations paid off and won it the award for the first time in six years.

The class 80 tender steam locomotive is outstanding and shows perfect driving characteristics. Moreover, its inner values and its detailed driving rod mechanism are by now also well appreciated features.

With this tiny locomotive, everything is just a little bit smaller and very tight under the body. Märklin made a brilliant move with this model and introduced, for the first time in its Z gauge range, bodies made with a precision lost mould brass casting technique.

This technique greatly improves the reproduction of very fine detail and makes for a relatively high weight of the body due to the material used. These are the features that made this model a serious contender and earned it a nomination.

Yet, in the end and by unanimous vote, the class 80 lost out to the "Kittel" steam railcar. The Kittel is a true gem and, as editor Joachim Ritter explains his choice, leaves one wondering whether it can really be the product of a large series production process. Its unconventional packaging, which does a good job in protecting all of the finely detailed bits and pieces, gained this model additional points.

The fine details include, above all, exquisite steps and the replicas of coupling hooks and brake hoses on the front buffer beam. Holger Späing highlights these features and adds that such delicate details are a novelty and that it is hard to believe that these parts are made by injection moulding.

The fact that the original use of the Kittel steam rail car was regionally confined to Southwestern Germany does not seem to diminish the demand by hobbyists, although our editorial colleague from the Black Forest is naturally particularly pleased about the release of this model. Its affordable price will have made it even more appealing to customers. All three editors agree that this has turned out to be a really fine Z gauge model of the type we would hardly have dared to dream about in years ago.

Dirk Kuhlmann praises the winner of this category as a courageous step towards taking the mass production of Z gauge models to a higher and completely different level.

Railway Cars Category

As for railway wagons, the competition also boils down to a duel: two freight cars, but from different manufacturers. On the one hand, we have from FR Freudenreich Feinwerktechnik an essentially newly constructed model of a covered Gbs type freight car in FS (Italian state railway) livery (39.441.01). The other contender is a German Federal Railway (DB) Rmms 33 type stake car (82130) from Märklin.

Of the Rmms 33, two technical versions were actually developed at the same time. Märklin released simultaneously models of the original version with wooden sides and the modernised version with metal sides. The models come with removable injection-moulded imitations of the wooden or metal stakes.



The manual handbrake platform can also be used with both models. This creates variety, and prevents repetitions, especially since such freight cars are perfectly suited for a wide variety of different types of cargo for loads.



The two models of the Rmms 33 were outstanding in the 2018 new items programme and proved to be almost perfectly implemented. They therefore rightly receive the award in the category "Cars" for the year 2018.

As this was once a widely used freight car, we gave it our unanimous vote for the best new 2018 product in this category.

We are also happy to note that Märklin is now paying closer attention to detail for those types of cars in its product range where we had previously seen smaller or larger compromises with respect to their prototypes.

Dirk Kuhlmann is therefore right to wonder: "Where else does Märklin want to take its Z gauge line-up in the future"? It will be hard to beat the level of detail (including on the underbody) already achieved.

Accessories Category

Compared to the previous categories it was much more complicated to pick nominees and agree on a winner for the accessories category. We managed in the end, but only by majority decision.

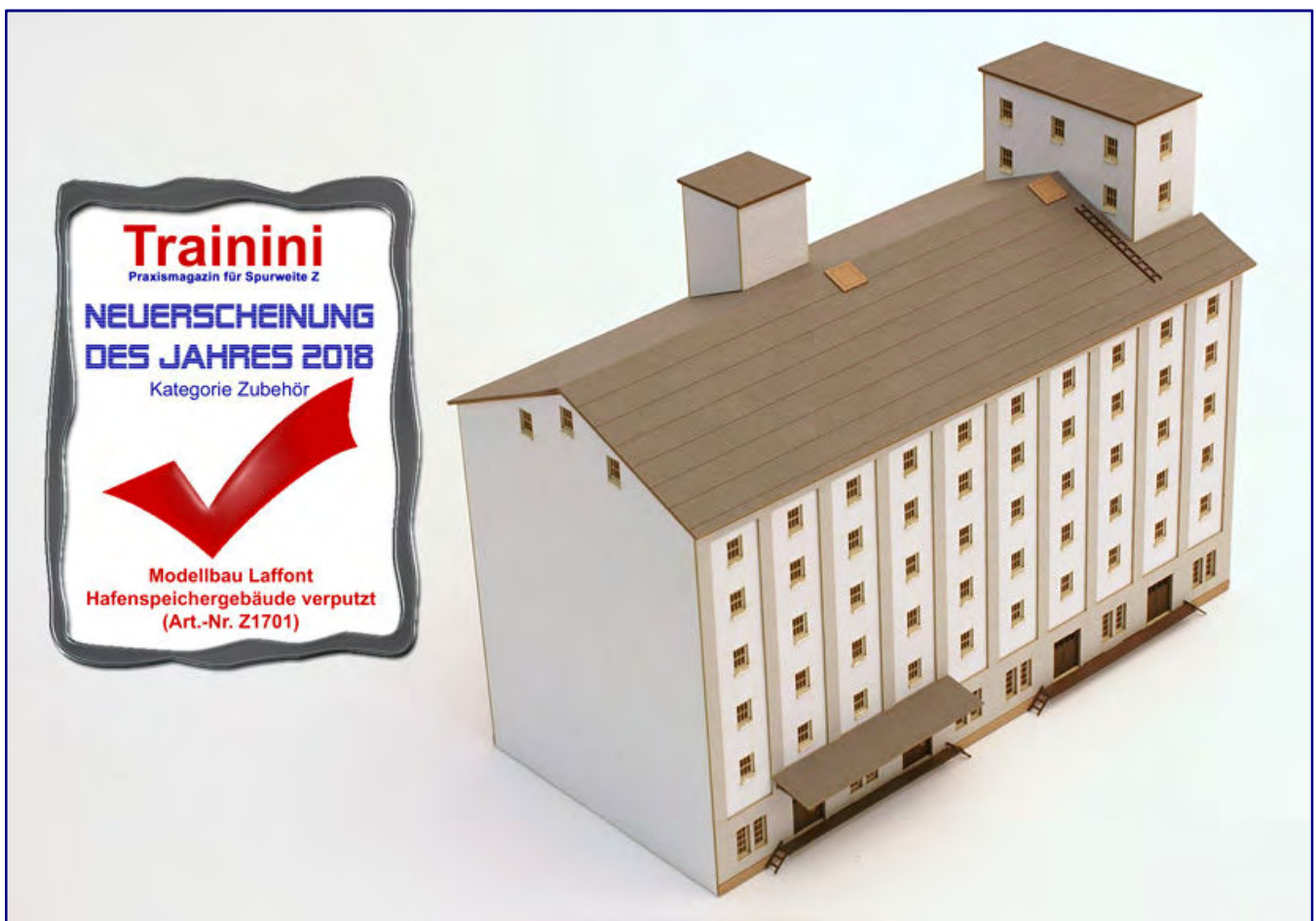
In our discussions we discarded the MF 830 combine harvester (322.018) and the yellow Caterpillar D7 bulldozer, both from Artitec. The only reason for this was that similar models had already been available on the market and that these new models did therefore not fill a major gap.



The Raiffeisen warehouse and market (89705) from Märklin also did not make the final cut, although it is an excellent model, which can also be built on slopes, a feature, which no other manufacturer has included as an option, so far.

There was also Faller's Quakenbrück Town Hall which is a prime example of how different, innovative techniques can be combined within one kit: laser cut cardboard for the building, digital printing details of the façade and 3D printing for the monument in front of the town hall.

The Samba version of the VW T1 bus from Zcustomizer also merits a mention. Unlike most other passenger car models in scale 1:220, it comes with transparent windows. The last vehicles of comparable size with these characteristics were built a long time ago. Joachim Ritter also notes that it is a dream car and a typical representative of these vintage buses.



The stucco finished harbour storage building (Z1701) serves a trend theme of gauge Z and was also flawlessly implemented. For this attractive building kit, the award in the accessories category goes to Modellbau Laffont.

However, in the end and by majority vote, the award went to the stucco finished harbour warehouse (Z1701) from Modellbau Laffont. It is impressive in its size and appearance, as well as its simple, clean and almost timeless design.

Moreover, it caters to a popular current trend of building maritime and dockside themed layouts. We expect this model to do well on the market as it fills a significant gap in the supply of Z gauge kits. Our editor Dirk Kuhlmann, for one, would have loved to include it already years ago in his layouts.

And so this year's deserved award in the accessories category goes to Stefan Laffont from Aachen!



Technology Category

Here we have often had to evaluate curiosities. Like no other category, this one is about innovations that can shape our hobby for years or even decades to come.

In 2018, we were mainly concerned with a new and very effective greening technique, to which we had introduced Magitree from Microrama as the latest offshoot.

But it stood no chance against the close couplers with guide mechanisms designed by Sondermodelle Z and installed in its model of a class RH 5147 diesel railcar.

Our unanimous vote was for this new product, which had previously been considered impossible. Model railroaders of the larger scales have long been used to it, but constructing a mechanism in the tiniest of spaces which allows carriages to be coupled close to each other and yet still be able to take curves without problems had so far seemed out of reach for Z gauge.

The ingenious inventors from Austria have turned the small world upside

down, and have made the impossible possible. "A really great feature, at last translated into Z" is the sober-sounding conclusion of Joachim Ritter.

We are also honouring this achievement because we hope that it will inspire others, above all, of course, the large series manufacturer Märklin. We are already curious to see how the market will react to this exemplary model from Wien (Vienna).

Literature Category

EK publishing house from Freiburg seems to be an annual title contender in the literature category. That's no surprise, since all of their books and films have a consistently high standard in every respect. Two of their publications made it to our 2018 shortlist.

Their DVD "Die Eisenbahn in Hamburg damals (Historic railways of Hamburg)" (8453) is the latest chapter of a new series of regionally themed documentaries which are truly inspiring and quite exciting in their own way. In addition, the film presents a lot of prototypes for which Z gauge models are available.

We were also impressed by the book "Einheitsdampflok der Deutschen Reichsbahn (Standard steam locomotives of the German Imperial Railway)" (ISBN 978-3-613-71561-5) by Transpress. After many

books of rather average quality or with slight deficiencies in the picture reproduction, this title was really a success that we have not forgotten.

Another finalist was the book, "Güterwagen Band 1.1 (Freight cars Vol. 1.1)" (978-3-89610-652-0), from the publisher VG Bahn.

Rather than being a mere re-edition of the former Vol. 1, the structure of the book was adapted to the more recent volumes of the series and expanded in such a way that old and new content had to be divided into two individual volumes.

Enough has already been said about the quality descriptions of covered freight cars up to 1945. The author's name Stefan Carstens stands for itself. Together with Paul Scheller, he has managed a seamless continuation of the other titles.

Nevertheless, in the end we decided unanimously to hand the award to another book - the second among the nominated books from EK-Verlag. "Die Baureihe 403 (The class 403 series)" (978-3-8446-6028-9) is, on the one hand, typical for this publisher's portraits of locomotives, and, on the other hand, quite an extraordinary volume.

Just flipping through the book, one cannot but notice its great layout and fantastic photographs. Anyone who, like Dirk Kuhlmann, had the privilege to travel on this train (before the Lufthansa era) will remember it for forever. The train was unique and so is this book.

Connoisseurs are aware of its close relationship to the class 420 multiple rail transit units, a result of trying to keep development costs within reasonable limits. The EK book puts the relationship between the two series into perspective, and illustrates where independent or different solutions were required. All in all, the book provides fans of the prototype with valuable information that was previously not available in such abundance.

"Complete Works" Category

Every year we are on the lookout to see which "Zettie" or which manufacturer deserves an award for their entire oeuvre. We deliberately do not aim to hand out lifetime achievement awards, because we do not want to award honours posthumously. Moreover, any winner can, in principle, win our award multiple times.





Gabriele and Michael Klingenhöfer are pillars of the Z gauge community and have been active for many years under the brand name Creativ-Modellbau Klingenhöfer (C-M-K). Over the past year, they managed to further improve the design of their animal figures which are incredibly detailed, despite their tininess, as can be seen, for example, with their elephant specimen (which is also their company logo). Photo: Klingenhöfer



Our decisive criterion for this category is a long-standing and extraordinary commitment which has set standards for the hobby. We aim to focus on overall product groups or achievements of a non-commercial type which have nevertheless influenced commercial manufacturers.

This year we have selected the couple Gabriele and Michael Klingenhöfer, better known as Creativ-Modellbau Klingenhöfer (C-M-K). This is the second time in a row that the prize goes to members of the Untereschbach Stammtisch modelling club.

With this award we would like to pay tribute to the entire programme of the finest and most scale animal figures. Even the elephant in the logo reveals what the focus of the mission is.

In 2018, Gabriele and Michael took their activities to the next level. In particular, their new the new figures of foxes, chamois, ibex and goats seem unrivalled in detail and quality.

Each of these figures would have deserved its own award, but if we would have put them into the acces-

sories category, they would have gotten lost amongst all the other products there. They are simply minuscule and can be easily overlooked.

Except on a layout that is, where they make for some eye-catching detail and signal to the observer that the model builder has gone the extra mile to produce a fine layout. Isn't it the pleasure of discovering small details which tend to captivate and inspire the viewer? Happiness can be found in little things and we hope for the same from this award.

We warmly congratulate all the winners of the 2018 awards and urge them to not let up in their efforts to delight us in the New Year with great new products, ingenious ideas and extraordinary models that further advance our small track gauge.

The webpages of our winners:

<https://www.ekshop.de>

<https://www.klingenhoefer.com>

<https://www.maerklin.de>

<https://www.modellbau-laffont.de>

<http://www.sondermodellez.at>

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Germany's largest, electronically produced magazine for Z Gauge model railroading

Trainini
German Magazine for Z Gauge

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details see imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative image is our goal. Likewise, here we note any events or meetings with a significance to Z gauge reference, if we are informed in time.

Information on the Trade Fair Report from Köln:

The buildings shown in Trainini 12/18, page 54 (on) Fig. 2 are houses from the "Maggi village". In the seventies Maggi cubes (for broth or gravy) were packed in small cardboard boxes, on which these buildings were printed in 1:220, for cutting out and collecting.



These cardboard houses on a layout shown in Cologne gave us puzzles. Roland Graesser could solve it: They belong to the "Maggi village" and were printed in the seventies on the outer carton of the well-known broth or gravy cubes.

The material of the Maggi houses was aluminium-coated and opaque. But you could make a copy on thin paper before cutting, cut out the original including the window openings and glue the copy onto the back. The houses could then be illuminated.

Roland Graesser (Canada), per E-Mail

Praise and joy over 14 years Trainini®:

I wish you and your families a Merry Christmas and a Happy New Year 2019!

In the last 2 months I have taken the trouble to browse and read all Trainini editions in the archive of 1zu220-Shop. I have to praise you for this successful magazine for Z model railroaders; it was also very helpful for me, because I have now found a few more dealers where I can stock up with accessories.

I would like to thank you very much for this achievement; I keep my fingers crossed for further successful years. I am already looking forward to the next issues.

Mario Schwarz, Augsburg

New Year greetings from Switzerland:

I think a picture of the Inter-City Experimental fits perfectly to appreciate your work, thank you very much for all the extraordinary contributions in Trainini!



With these two photos our reader thanks Trainini® and wishes all readers a good year 2019. Photo: Martin Zimmermann

The photo of the Argen bridge on Bodensee (Lake Constance) leads us into the New Year, I find the construction of the bridge very fitting for the new SMZ railcar. Happy New Year to all of you.

Martin Zimmermann (Schweiz (Switzerland)), per E-Mail

With best wishes this letter to the editor also reached us:

Thank you very much for your efforts and efforts to create a monthly edition of Trainini. I don't think there's a better advertisement for Z gauge, and no better publication.

Kyrill Makoski, per E-Mail

Alternative to Grasmaster:

At Case Hobbies (<http://case-hobbies.de/>) the Grassinator (Art.-No. ZTR-400) from Ztrains is offered as an US alternative to the local grassing equipment. This electrostatic device is very similar in shape and design to the Grasmaster from Noch, but is in no way identical to it.



Electrostatic Grassinator (Art.-No. ZTR-400). Photo: Ztrains

The main difference for the user probably lies in the transparent storage container, which is why the user can check the available filling quantity at any time. The sieve inserts supplied also include those which do not release the entire cross-section of the container and thus offer a function which is close to that of the individually available Noch fine applicator.

News from the ALAN control system:

The update 1.2.6 of the operating program, which has been available for download since the end of December 2018, corrects minor errors in the ALAN control, in particular in connection with the level change symbol.

Electronic distribution has been expanded to include LED light bulbs for locomotives, wagons and accessories, allowing the replacement of older E-5.5 threaded light bulbs or BA-5 bayonet light bulbs. The energy-saving and low-heat-emitting LEDs are available in both cold and warm white light colours. They are suitable for operating voltages from 12 to 22 volts and in ALAN operation ensure that the lights remain on even when the vehicle is stationary. The manufacturer can be contacted at <https://toy-tec.com>.

Worthwhile fairs and exhibitions in March:

On 9 and 10 March 2018 the 17th International Lahnstein Model Railway Days will take place. This exhibition is regarded as a small OnTraXS! and impresses every year anew by the exceptionally high level of the exhibits shown, which is why it is highly appreciated by connoisseurs.

Also Z gauge has traditionally a fixed place here and will again be present at this edition with 25 turbines from six European countries. The fine pieces of modelling can be seen on Saturday from 10:00, on Sunday from 11:00 in the Stadthalle am Salhofplatz (56112 Lahnstein).

Until 5 p.m. there will be ample opportunity for viewing, technical discussions and shopping at the participating dealers and small series manufacturers. Also for the physical well-being is ensured.

Only one week later the tour continues in Mannheim. The Faszination Modellbahn will take place in the Maimarkthalle after its move from 15 to 17 March 2019. The fair is open daily from 9:00 to 17:00. **Trainini®**

readers will receive an admission discount of 2 euros on the day ticket upon presentation of the advertisement in this and the next issue.

The Nuremberg fair novelties can be seen here for the first time in public and the Z gauge is to prove once again in Mannheim that it is on a par with the big tracks. With the large modular layout of the US Z Lines Europe, the US railroaders will show how large model railways can be built on a small scale.



This American-style intersection without many structures can be seen on one of the many US modules to be exhibited at Faszination Modellbahn in Mannheim. Photo: Peter Simon

Of course, long trains with multiple tractions should not be missing, and the modules designed by Peter Simon convey the “American dream.” The “Big Boy” and “Challenger” steam locomotives (even with noise decoders) are also used in front of heavy trains and show what can be done in the meantime.

Märklin deliveries at the turn of the year:

New models also came from Göppingen to the dealers at the turn of the year. The “Gotthard Panorama Express” train pack (Art. No. 81594), for example, is now also a product for friends of the Swiss railways. The Re 4/4II series ellok contained therein is now driven by a bell-shaped armature motor.

This also applies to the E 94 freight locomotive of the Deutsche Reichsbahn of the former DDR (88227), which was converted to LED lighting and is offered as part of the current museum series. Limited to 499 pieces, it is delivered in a wooden box and comes with a replica of its AEG manufacturer's board.

The three-part “Staufen-Bräu” (86603) freight car package for Era III is now also available. The cars, which have aged slightly ex works, have two different designs and, in the case of the G-10-based heat protection car, are likely to be secondary uses of an earlier museum car “Brauerei zum Rad” (Brewery at the Wheel).

Important note on Küpper loads:

As information for their customers, from New Year's Eve 2018 Spur Z Ladegut Josephine Küpper advises their customers that responses and answers may take longer than usual due to health reasons. Please understand why we would like to pass this information on to new customers at this point.

Results of the EM diorama competition:

The railway magazine has announced the winners of its diorama competition. The Z gauge was not only very well represented in terms of numbers, but also in terms of quality, as we have already stated in our trade fair report.

Nevertheless, the result of the visitors' vote is astonishing in view of the fact that in the past contributions of size H0 were always visibly preferred when voting. We therefore rate the latest result as a clear success for the small scale, its advantages of designing in the smallest space and the increased acceptance by the public.



The block position 200 by Detlef Wolf was the clear winner of the competition: For the first time in Germany, Z gauge convinced a trade fair audience of its advantages and outstanding quality.

So far, however, only the three first place winners are known which is why further surprises are not yet ruled out. Among them, three Z-scale dioramas could be found in the adult (abbreviation E) and youth (abbreviation J) categories. We congratulate the following winners on their unprecedented success:

<u>Place</u>	<u>Participant number</u>	<u>Name</u>	<u>Diorama theme</u>
1 st Place Adults	E24	Detlef Wolf	Blockstelle 220 (Block 220)
1 st Place Youth	J19	Julia Adass	Rodungsarbeiten (deforestation work)
2 nd Place Youth	J15	Marvin Schneider	Kallentaler Mühle (Mill)



Marvin Schneider is also one of the greats and proved this again impressively with the Kallentaler Mühle (mill).

All three contributions were characterised by great creativity, well thought-out structuring, cleverly applied eye-catchers and skilful landscaping. Julia Adass was also the youngest participant in the competition at the age of nine.

News from Micro-Trains:

Following the fifth postponement of the delivery date, the EMD SD40-2 diesel locomotive is to be delivered to ATSF at the beginning of February 2019. The wagon packing for this railway company will also be available at about the same time. In 2019, Micro-Trains will celebrate the 50th

Photo on the right:
Somebody should do that first! Julia Adass is only 9 years old, but has won the competition with flying colours.



anniversary of its Magnematic couplers, which are also used in gauge Z. The new Magnematic couplers will also be available in the Z gauge.

The new products from American Z Line:

AZL continues the product maintenance program for its EMD SD70M and SD70i this month with two SD70M from Union Pacific (Art.-No. 6104-2 / -3) in very small quantities. We have already described the features of the upgraded models in the last editions.



The covered AAR freight wagons from 1937 receive an increase with three deliveries for the Southern. Available are a single wagon (904312-1), a double wagon (904382-1) and a quad wagon (914312-1).



Back in the program are also the 89-foot flat cars, which are again used for military transports and are used in the Trailer Train and DOD. They are loaded with one HEMTT vehicle each M983 and M994, either in olive green with the operator code TTX (91102-1O) or sand-coloured with DODX (911024-6S).

EMD SD70M of the Union Pacific with flag design (picture above) and TTX flat car with military vehicles (picture below). Photo: AZL / Ztrack

The Amtrak Superliner passenger cars in Phase III design are now also available. There is a choice of single seat (72001-1 / -2), sleep (72002-1 / -2), dining (72003-1) and viewing cars (72004-1). Train sets for your own layout can also be put together from two packs of four (72050-1 / 72051-1), which ensure a suitable distribution of the above-mentioned wagon types.



Two packs of four can be used to assemble prototype Amtrak trains in Phase III paint. They have therefore been put together with different cars in each pack, and have different operating numbers. Photo: AZL / Ztrack

You will find further manufacturer photos of the current deliveries under <http://www.americanzline.com>.

Free admission for socially challenged people:

Also in January 2019 the Miniatur Wunderland in Hamburg repeats its action "I can't afford it." Since 2015, on selected days at the beginning of the year, the operators have offered free admission to those people who cannot afford a regular ticket.

60,000 people had already taken advantage of this opportunity before the start of this year's campaign. Nobody has to justify themselves, provide evidence or feel humiliated in any way, the decisive factor is

their own assessment of the personal economic situation. So it is sufficient to make the statement “I cannot afford it” at the cash desk in spoken or written form.

In the meantime, dozens of letters of thanks have reached the Miwula. “For example, I remember a grandmother who was there with her grandchildren, thanked them with tears and said she would still like to pay 5 euros. She simply can't afford any more,” reports Frederik Braun, visibly affected.

“Unfortunately, despite full employment in some regions, there are still many people in Germany who live under precarious conditions. For us it is a great joy that we are in the fortunate position to give these people a beautiful moment,” he continues.

More information can be found at <https://www.miniatur-wunderland.de/leisten>. The dates still offered with appearance of this expenditure are:

Montag, 28. Januar 2019 (9:00 – 19:00 Uhr),
Dienstag, 29. Januar 2019 (9:00 – 21:00 Uhr) und
Mittwoch, 30. Januar 2019 (9:00 – 19:00 Uhr).

New beer wagon from 1zu220-Shop:

The 1zu220 shop continues its Westheimer beer car series. What they have in common is the product-specific design and the underlying car type G 10. All cars have no real life prototypes, but are provided with company labelling for Era IV private cars and are designed in such a way that they can also be enjoyed by railway employees who, for example, would like to design a brewery theme.

Without prior notice at the model railway exhibition in Bad Schwartau on the weekend 19-20 January 2019 a new car was offered in beige paint and with grey aluminium roof “HOBİ Honigtrunk” (Art.-No. 98163) and was very well received.

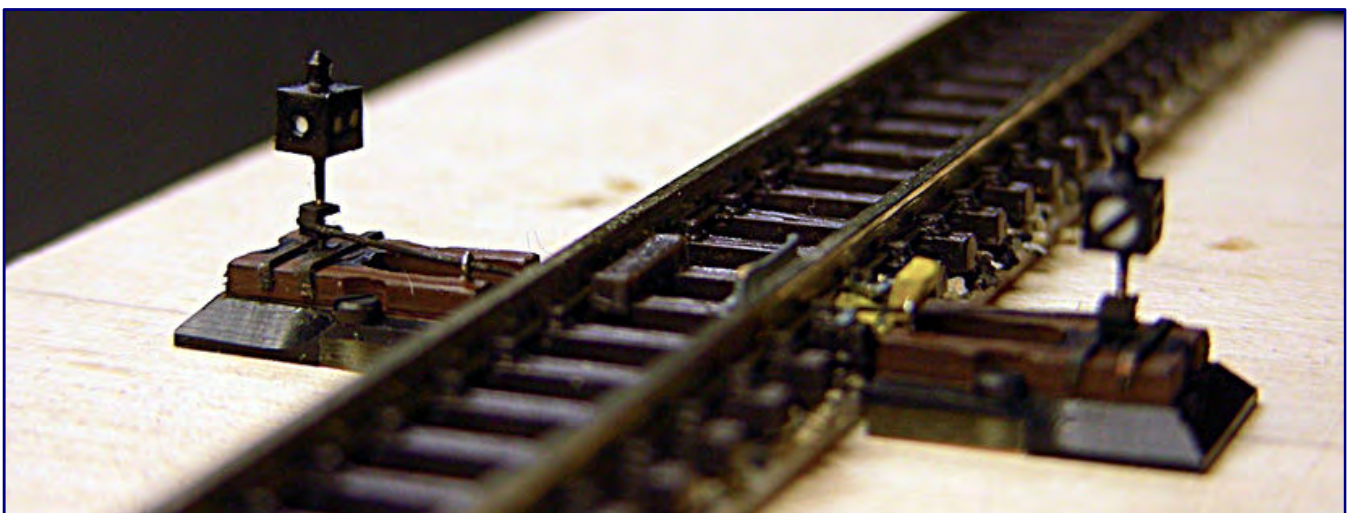
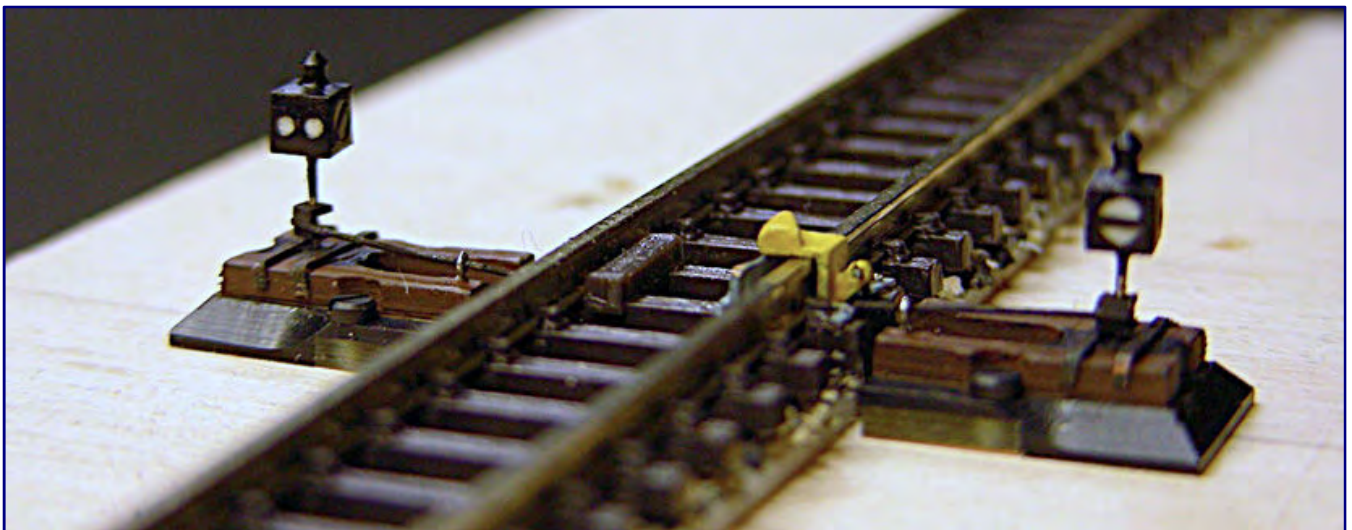
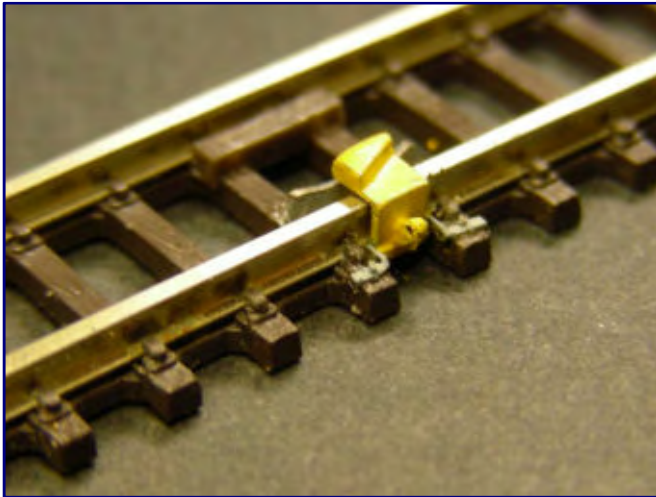


With the beige-coloured G 10 "HOBİ Honigtrunk" (Art.-No. 98163; right) from Märklin production, the 1zu220 shop continues the beer car series that has begun.

This model has also been produced and printed in a unique edition by Märklin. For all who have visited this exhibition, it is now available at <https://www.1zu220-shop.de>, while stocks last.

And it can' be ignored ...:

Götz Guddas doesn't let the fiddling go on! Our reader and Zettie, who has been very well known through exhibitions, has again designed and built at the turn of the year. The result is an extremely filigree and fully functional track barrier for his system divided into segments.



The small photo series shows the filigree individual parts (photos above left and right) including the illuminated lanterns as well as the finished track barrier in on (photo centre) and removed condition (photo below). Photo: Götz Guddas

He has chosen a model with two (movable) lanterns that signal to the locomotive driver from both directions whether the track barrier is on or off. At the push of a button, the yellow-painted locking element, which causes passing wagons or locomotives to derail and thus protects the rest of the line from a runaway train, is removed from the track in a rotary movement.

As such a self-build is unique to our knowledge; we would like to share the photos of the builder, who has also reported extensively in the forum of Z-Freunde International, with our readers.

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Trainini

German Magazine for Z Gauge



New Products from Nuremberg

News from the small series manufacturers
Hello-Kitty-Shinkansen

Introduction

Dear Readers,

do you know the typical “life” of a **Trainini®** editor? Now I sit in the often quoted quiet chamber and rack my head, which would give pleasure to the readers.

First sketches and texts are created and then... Suddenly some messages come unexpectedly by e-mail and the pen falls out of my hand. The two colleagues had already finished the report about the “Hello Kitty Shinkansen”. Already the first sentence of our report says it aptly: “Is it simply beautiful or simply kitschy?”

My initial head shaking, coupled with a certain breathlessness, gave way to repeated admiration and recognition of modern Japanese culture.

This train simply fits into this country and is also a likeable advertising medium. Besides, we've never had so much pink in a **Trainini®** issue!

But let's turn our attention to this year's Nuremberg's International Toy Fair, which was held there for the seventieth time.

As in previous years, part of our editorial team was on site to report on the new products for Z scale. Of course, we have repeatedly looked outside the box and also show you interesting products that could be useful to use across different scales.

The move to Hall 7A also had the advantage that manufacturers from the model making sector were more likely to be visited by specialist dealers and journalists from the model railway department in order to discover a lot of new things.

The new products of the small series manufacturers are of course also considered and find their worthy place here in a separate article. Let us surprise you!

The three of us from the editorial staff would just like to wish ourselves, unfortunately also repeatedly, a somewhat more committed feedback discipline on the part of many small series manufacturers. For some we are still waiting in vain for an answer...

This edition is rounded off with two book reviews about the “Boeing 727” by Motorbuch Verlag and the reference book “Triebwagen und Triebzüge” (railcars and multiple units) by Geramond. A small report about the Boeing 737-230C of Lufthansa fits finally, of course, also (exceptionally) in this frame.

By the way: Meanwhile, I can hold my pen in a steady hand again, so that we can continue with the preliminary planning for our upcoming issues. After all, the next year also needs to be well thought out and thematically structured.

Sin-Z-erly,

Dirk Kuhlmann



Dirk Kuhlmann
Editor

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We thank the West Japan Railway, Erik Frikke, Jörg Erkel and the Eisenbahnstiftung for their photo support and Jan Tappenbeck for his exhibition report.

Date of publication of the German language version of this issue: 27 February 2019

Cover photo:

Opposites that cannot be more blatant! Märklin's proud high-speed steam locomotive 03 1001 made of classic zinc die-cast construction stands next to the first parts for a VW van on rails. Its housing is made of a metal alloy plastic.

The Hello Kitty Shinkansen

Kitty on board!

Simply beautiful or rather kitschy? The answer lies in the eye of the beholder and is a matter of personal taste. Either way, the Hello Kitty Shinkansen sticks out from the crowd and is a real head-turner. It's no surprise, therefore, that it has attracted worldwide attention and is now also available as a model train.

On 13 May 2018 the Shinkansen "Neon Genesis Evangelion" completed its last observed by many cameras. That JR West Shinkansen 500, with its special livery for a Japanese combat robot animation series and its corresponding interior design, were a huge success. Initially it was to be retired from service in March 2017, but this livery was extended for another year due to great customer demand and its popularity, especially among foreign tourists.

The main reason for that will have been its unique and extremely appealing exterior design, which inspired technophile customers from the Land of the Rising Sun and beyond. Just one week after saying goodbye to this special train, its operator announced that its successor in Hello Kitty livery was to enter service on 30 June 2018.



The Hello Kitty Shinkansen entered service on 30 June 2018. Here is a shot of its end car (road number 522-7002) and middle car 527-7702. Photo: © West Japan Railway Company

Shortly afterwards, Rokuhan announced that they would reproduce this train in the scale 1:220, just like its predecessor. Opinions have been divided since then: some find the train a bit too pink and kitschy, whilst others appreciate it as an extravagant, striking and very appealing homage to an important chapter of Japanese pop-culture. In any case, it has found a large group of fans, in particular amongst girls, women and many railway enthusiasts.

For licensing reasons and to the disappointment of model railway enthusiasts outside the country, the model is, unfortunately, only available in Japan. The kitty and the bow design are owned by the Japanese company Sanrio and go back to 1974. Since then, it has turned into an unprecedented success and adorns everything from purses and bags to stationary, watches and toys.



The “Hello! Plaza” in car 1 has a shop selling Hello-Kitty-Souvenirs. Photo: © West Japan Railway Company

Strolling through the streets of Japan, one will occasionally come across young girls who dress and look like “Hello Kitty”. Over time, the character also started to appear outside Japan and is now known throughout the world. There are even dedicated Hello Kitty shops and theme parks. After previously appearing on airplanes, a train of its own was only a matter of time.

The moment arrived in the middle of 2018, when a Shinkansen with a pink roof and the characteristic bow draped over the window bands started its daily service between the Hakata and Shin-Osaka stations. The entire outside of the train promotes the cult figure. Also, a large part of the interior is decorated in a Hello Kitty fashion: Two dedicated cars made for a very special travel experience.

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A view of the sales area in car 1 (top) with still empty shelves. Note the Photo wall at the end of the car (back right) which comes with motifs of the Shimane und Tottori characters (bottom). Photos: © West Japan Railway Company

The number 1 car contains the “Hello! Plaza”, an area covered in white and pink from floor to ceiling that is used for movie screenings and showcasing souvenirs and other products. In this car, the theme of painting and products with the region through which the train travels is changed by each period, which attracts customer any number of times.

Car number 2, named “Kawaii! Room”, is a passenger car decked out with Hello Kitty motifs, including the storage areas for strollers and luggage. Moreover, it has a special area where passengers can have their photos taken next to a large Hello Kitty doll against the backdrop of a corresponding map of Japan.



The interior of car 2 (“Kawaii! Room”) is also decked out in Hello Kitty design and dominated by bows and pink coloured floors, sun screens and arm rests. Photo: © West Japan Railway Company

The seats in this car are purple, with pink armrests and pink floor. The covers of the headrests and the sun screens of the windows also bear the motif of this famous character. Regional dedicated Kitty are painted on the entire eight car bodies.

In keeping with the spirit, even the West Japanese Railways (JR West) train attendant uniforms on this train bear many feline depictions. The theme runs through the entire bullet train and finds a worthy conclusion at the opposite number 8 carriage. Surrounded by motifs of the kitten and her friends, kids can take their seats at a control desk directly behind the driver's cab and play train driver themselves.

A sign saying “Let's operate Hello Kitty Shinkansen!” and a life size photo of the driver's cab with a view of the track invites them to participate in this unique experience.

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Storage areas for strollers and baggage (top) and a photo corner with a Hello Kitty figure (bottom) in car 2. Photo: © West Japan Railway Company

It goes without saying that every passenger having travelled with this special train will become an enthusiastic fan. Japan invented high-speed rail transport, developed it to perfection and turned it into a showcase for its transport infrastructure. Those fortunate enough to travel on these trains will inevitably be fascinated by them.



Train simulator right behind the driver's cab at the far end of car 8, for kids who want to have a go at driving a train. Photo: © West Japan Railway Company

The success of the Hello Kitty bullet train has been such that the six-car "Haruka" airport train between Kansai Airport and Kyoto (Maibara) has also been operating in Hello Kitty livery since 29 January 2019. Two more multiple units with similar, but different, motifs are to follow soon.

The Rokuhan model

In **Trainini®** 4/2015 we presented a detailed overview of the prototype and model of the 8-car V-sets of the 500 series Shinkansen in standard livery, which has been running on two lines at up to 285 km/h since 2008.

Contrary to what we had assumed at the time, Rokuhan has not yet followed up with a model the original sixteen-part W-set. They did, however, quickly come out with the very appealing Project-Eva livery which was also sold in Europe and the United States.



The Hello Kitty Shinkansen model is sold in Japan (only) as a three car basic set (Art. no. T013-6) and as a complete starter set (G004-3; pictured).

At the end of November 2018 the time had finally come: Rokuhan launched a model of The Hello Kitty Shinkansen, almost half a year after the prototype entered service. The only regret is that this special looking head-turner of a train can only be sold in Japan.

Owing to reports in the media, this Shinkansen had attained, after all, certain degree of fame in Europe. Attentive customers finally discovered the news of the launch of the model on the pages of the Japanese manufacturer Rokuhan.

Although would-be customers in Europe and America, unfortunately, will not get to purchase the model for licensing reasons, we simply could not pass up on the opportunity to introduce it to the many readers of the German and international edition of our magazine.

The packaging of the train, which is divided into three sets, follows a familiar concept: basic and extension sets are offered as book cassettes in which three (basic pack; Art. No. T013-6) and five cars (extension; T013-7) are stored in vacuum formed inserts. Combined with each other, both sets allow for the formation of a prototypical Shinkansen.

The sets also contain operating instructions (only in Japanese) and theme-specific stickers. One of them depicts Hello Kitty as a train conductor and can be stuck on a transparent injection-moulded stand included in the extension set. Children will certainly be happy about this play figure.

As with the previous two editions, the three-car basic set consists of the two end carriages 1 and 8 as well as the motorised centre carriage 5. The short length of this shortened formation and the lack of pantographs make for a rather strange looking train.

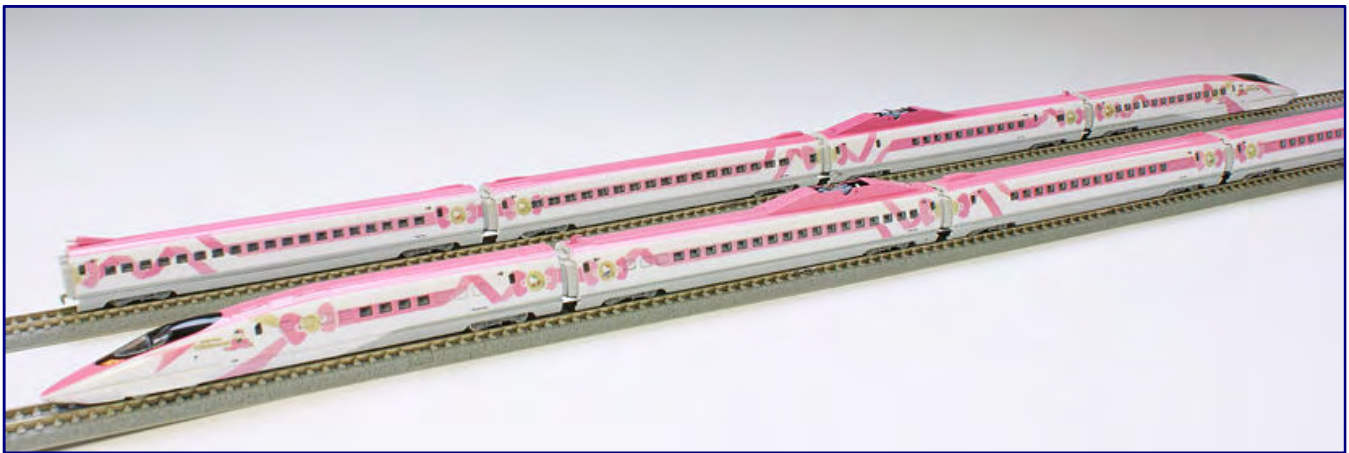
Continued on page 12



In addition to a three car train and a concrete track oval, the starter set comes with a pink re-railer with glitter inlay and a white and pink controller (top). The sticker sheet on the left can be used to decorate the controller knob or buttons and also includes a train ticket. The five car extension set (T013-7; bottom), in combination with the basic or Starter set, allows for the formation of a train of prototypical length.

The full length formation of the Hello Kitty Shinkansen model looks as follows:

Position in train sequence	Road number	Included in
End car 1	521-7002	Starter set G004-3 / basic set T013-6
Middle car 2 (with pantograph)	526-7004	Extension set T013-7
Middle car 3	527-7003	Extension set T013-7
Middle car 4	528-7002	Extension set T013-7
Middle car 5 (motorised)	525-7004	Starter set G004-3 / basic set T013-6
Middle car 6	526-7202	Extension set T013-7
Middle car 7 (with pantograph)	527-7702	Extension set T013-7
End car 8	522-7002	Starter set G004-3 / basic set T013-6



The Hello Kitty Shinkansen in its prototypical length and formation. The official numbering and order of the cars starts at the front left and ends at the back right.

It is certainly a smart move to sell also a more comprehensive starter set version (G004-3) of the basic three-car configuration. This will provide the Japanese customers with a good entry point into the world of Z scale, as the set comes with a concrete sleeper track oval consisting of 195 mm radius curves and 440 mm straight track on both sides.

Of course, the starter set also includes a special transparent pink re-railer with glitter inlay and a Hello Kitty-themed RC-02 train controller. Compared to the grey standard version, this one is made of white plastic and pink imprints.

Moreover, the set includes a sticker sheet with bow motifs and round colourful Hello Kitty illustrations that can be stuck onto the knob of the controller or onto buttons. Two of the round stickers, as well as three from the extension set, carry motifs with ribbons showing the names of Japanese cities served by the bullet train. Finally, a pink train ticket on the sheet showing a fare of 1,974 yen is also included.

Livery

Optically, the design of the train's train livery follows a uniform basic colour scheme: With the exception of the long front noses, the cars' transitions and aprons are painted in a light grey with a very slight beige tinge. The car bodies have a white base colour, which extends to the aprons at the front car and end car.



The roof of this aerodynamic train is kept in pink. The coloured band covers the entire length of the Shinkansen 500 and reaches all the way to its tip

Whilst the roof of the Type Eva project still carried the light blue colour of the standard train, this time around the special design also extends to the roof which is kept in a strong pink and thus very much contributes to the distinctive appearance of the Hello Kitty Shinkansen.

Only the driver's cab, which is set off in black, interrupts this pink colour band, which runs over the entire length of the train and which is also visible in the area of the wind deflectors around the two pantographs when viewing the train from the side.

An additional design feature is the pink-coloured horizontal ribbon covering the window areas and whose straight line folds up and down in some places. At or near the ends of the carriages we find the world famous Hello Kitty bow and, in the centre of the knot, the same city name motifs which are also included on the sticker sheet.





A horizontal ribbon covers the window areas in a straight line, interrupted by frequent upward and downward folds (top). All decorative motifs have been digitally printed onto the bodies. This is in particular evident from the halftone dots visible on the regional Hello Kitty motifs (bottom), shown here at the intersection between car 1 and 2.

In addition, both ends of the train carry a two-coloured pink arrow motif that points towards a “Hello Kitty Shinkansen” logo with a kitten in train uniform. While the three basic colours are painted onto the body, presumably using stencils, all Hello Kitty motifs and the carriage numbers were applied by digital printing.

An unmistakable sign of this are the tiny halftone dots that become visible in the window panes, ribbons and motifs when the wagons are viewed with a magnifying glass. Without magnification, the only giveaway is a minimal difference in colours that can be noted when taking a closer look at the transitions between the ribbons and the roof colour.

The colour separation edge between grey and white with this “Kitten Express” is somewhat higher than with the regular Shinkansen 500 and thus slightly above the separation edge between chassis (including apron) and car body. Therefore, a thin grey line was also printed on the body, as can also be seen under the magnifying glass.



Both end cars of the train are decorated with an arrow motif pointing towards the Hello Kitty Shinkansen logo, and a kitty in train uniform. The horizontal ribbon motif begins just behind that.

In order to protect all prints from mechanical wear, the entire train was given a coat of clear varnish. This is a welcome feature and a common practice elsewhere as well. It also gives the train exactly the right degree of glossiness that one would find with a newly delivered prototype. Our only point of criticism is that the transparent coating has turned out to be a bit too thick, which is particularly noticeable in the roof area.

Technically, the model offers exactly the same what we had already described in our review of the first edition of the Rokuhan Shinkansen: exact adherence to prototype dimensions, closed car transitions and a hook-eye coupling system that prevents the middle cars from being turned 180°. There is again an option for equipping the train with interior lighting for the end and middle cars. A new optional feature is Rokuhan's proprietary digital decoder.

Adding interior lighting is a neat and worthwhile feature as it allows for the seats to be seen through the windows. In line with the previous model, this version also has alternating yellow and red head and tail lights, depending on the direction of the train. Since the train does not have electric couplers, current is supplied through the axles of each individual car. Rokuhan has found a reliable solution for feeding electricity to the train without compromising rolling resistance.

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Top: Installing Rokuhan's optional interior lighting set is worth the effort as it allows for a view of the train's interior. This can be done in combination with Rokuhan's proprietary decoder.

Bottom: The Hello Kitty Shinkansen model comes with head and tail lights. The red double tail light behind the driver's cab window corresponds to the prototype. The two headlights are placed below the tail lights and mounted on the exterior.

Traction current is only picked up through the number 5 centre carriage. This needs to be taken into account in analogue operation of the train, especially since it requires sufficiently long sections of track if one wants to bring the train to a halt before a stop signal. One wheel on each of its inner axles is fitted with a traction tyre, diagonally offset from the wheel on the opposite side.

We are happy to repeat our experience from testing the previous version, which remain valid due to the technically unchanged new edition: At 0.3 volts, the train starts to move gently at the equivalent of 4.8 km/h. Due to the lack of breakaway torque of the bell-shaped armature motor, this is also the lowest continuous driving voltage.

The smoothly running transmission has POM gears, which also have self-lubricating properties. The light-emitting diodes at the front and rear ends are already clearly illuminated when starting up and can also be controlled with the Rokuhan train controller in such a way that they work with continuous lighting even when the train has come to a complete stop.



Nocturnal shot of the extraordinary Hello Kitty Shinkansen 500 Series of the West Japan Railway Company.

The 1:220 scale model of the Shinkansen 500 in the special Hello Kitty livery is thus ideally equipped to live up to its prototype. And it will certainly stay around far longer than the original. Although it's quite probable that its great success will buy the real Hello Kitty Shinkansen some extra time, the day will come when it will be retired from operation.

But Rokuhan's appealing and inspiring model will still be running on Z gauge tracks and keep the memory alive, but unfortunately only in the Land of the Rising Sun.

- **Manufacturer of the model (sold only in Japan):**
• <http://www.rokuhan.com>
- **Prototype references:**
• <https://www.jnto.de/aktuelles/546-hello-kitty-shinkansen>
• <https://www.jr-hellokittyshinkansen.jp/en/>
- **Prototype footage (exterior/interior):**
• <https://www.youtube.com/watch?v=VPhjBi-5tlc>
• <https://www.youtube.com/watch?v=EuOVohAUuA>

Boeing 737-200 from Herpa From baby to successful aircraft

When the Boeing 737 rose into the air for the first time, it was considered the “square” one, because length and wingspan of the original shape were almost identical. It was gladly ridiculed as a “pregnant mountain duck” because of its appearance, but today its design language has long been regarded as a classic. With the Boeing 737-200, Herpa has implemented the successful model, which is also popular on a small scale and will fit on almost any system.

Until 1967, Boeing only produced large aircraft with more than two engines, such as the four-engine 707 or the three-engine 727. At its official launch in January 1967, the Boeing 737 was initially rather smiled at and slightly mockingly referred to as the “baby Boeing.” The Boeing 737-100 celebrated its maiden flight on 9 April 1967.

The critics quickly fell silent, because the twin-engine short-haul aircraft quickly developed into a box-office hit and is today the most produced commercial aircraft in the world. Its most important competitors in the form of the Airbus A320 family are always close behind.



When the prototype of the Herpa model with the identification D-ABBE rolled past the photographer at Copenhagen Castrup Airport on 20 August 1972, the loading door on the left side can also be seen for closer inspection. Its left edge is located too centrally between the passenger door and the L, the right edge runs exactly between the “a” and the “n” of the Lufthansa lettering, which is why the seventh window of the cabin is moved forward slightly. Photo: Erik Frikke

The fact that this was possible at all is mainly thanks to Lufthansa: It saw a need for a small short-haul aircraft with a capacity of 100 passengers and urged the development of this later type of successful model. The manufacturer Boeing took a high risk because at the same time it was working on the four-engine Jumbo-Jet 747 and the supersonic aircraft Boeing 2707, which, at the time, was regarded as the future of civil aviation.

In order to keep costs under control, the well-known aircraft manufacturer used about 60 % of the structures and systems from the larger Boeing 727. The Boeing 737, for example, shares the fuselage diameter with the two types 707 and 727 (narrow fuselage). Nevertheless, the program threatened to be discontinued temporarily, as the required minimum order quantity could not be immediately reached.

When the supersonic airplane project was stopped, and also the plans for the Jumbo were changed, the responsible persons pushed forward the program for the smallest airplane, again more strongly. A typical and still valid feature was the lack of flaps on the main landing gear. This is retracted and does not disturb the aerodynamics, but weight could be saved.

The original type was the 28.63 meter long Boeing 737-100, of which only 30 were produced. 22 of them went to Lufthansa, which was both the first customer and the primary customer on February 10, 1968. A few weeks later the slightly stretched version 737-200 (991 units), which had become a successful standard model, followed, from which the 737-200C (104 units) was produced at the end of 1969.



The model (art. no. 559430), taken from a similar perspective to its prototype, reveals the only mistake in its realization: the side cargo door is missing, the asymmetry in the window band of the cabin is also not found in the model.

The C in the designation means “Convertible”: the aircraft had a large side door on the left side and could be converted from a passenger aircraft to a freighter within one day.

Lufthansa acquired a total of six aircraft of this type as Boeing 737-230C, including the Herpa model with the identification D-ABBE and the christened name “Remscheid”. The “Bravo Echo” (“BE” according to the international radio alphabet) was the first aircraft put into service on 16 December 1969, making Lufthansa the first customer (for this model) at the second time.

The combination freighters carried passengers during the day and freight at night. They remained in service until the early eighties before being transferred to Lufthansa Cargo, where they continued to operate as pure freighters. The D-ABBE “Remscheid” operated in this form for a further five years for the crane line.

Characteristics of the model

Herpa's most recent template was one of the first aircraft of this type to travel for Lufthansa. It was in service between 1968 and 1985 during Era IV. Therefore, and especially because of its extremely small size, this aircraft is well suited for model railways.



The distinctive features of the Boeing 737-200s are the shape of the vertical fin, on which Lufthansa has its "fried egg" (new logo), and the slim P&W JT8D engines projected from the rear.

It consumes little space and allows the integration of a relatively manageable portion of an airport into the model railway layout.

This provides the opportunity to integrate an (underground) airport railway station that is visible through the frame and is justified by the airport operations on the surface.

Such rail sidings developed at that time as seemingly meaningful means of transport links, for example, Düsseldorf, or also, Frankfurt (Main).

Since the model of the Boeing 737-200 (art. no. 559430) is executed at a scale of 1:200, its

dimensions seem a little too large for Z gauge, as expected. We do not find this disturbing, as we do with other models, because the aircraft seems too small for us in comparison to supply vehicles as well as figures on a scale of 1:220, in fact, however, the opposite is the case.

Herpa has reproduced all the important features of the model well and credibly. As usual in the large Wings scale, antennas are set individually at the top and bottom. Also the openings for the main landing gear are correctly reproduced without flaps.

This also applies to the characteristically slim Pratt & Whitney JT8D engines, replicas of which are fitted to the model. They protrude to the rear under the wings because they are longer than the later 737 type engines. Also the fin has the correct shape of the early versions.

Dimensions and data for the Boeing 737-200 of Lufthansa

	Prototype	1:200	1:220	Model
Length	30,53 m	152,7 mm	138,8 mm	151,6 mm
Wingspan	28,35 m	141,8 mm	141,1 mm	141,0 mm
Height	11,28 m	56,4 mm	51,3 mm	57,0 mm
Diameter of the body	3,76 m	18,8 mm	17,1 mm	18,8 mm
Weight	52.390 kg*	---	---	260 g
Cruising speed	917 km/h			
Propulsion	2 x Pratt & Whitney JT8D			
First flight	8 August 1967			

* max. take-off weight

The model also comes with a transparent base, which can be assembled from two parts and on which the aircraft can be displayed. For model railway purposes, this is, of course, irrelevant, so we only mention it here for the sake of completeness.

The heavy die-cast replica, which was painted in the correct Lufthansa colours according to the painting scheme in effect at the time, is flawlessly done. Also, the separations between the white and metallic shining fuselage, the grey underside, the blue window band, the black nose and the blue tail unit with the crane logo known as the “fried egg”, follow that scheme correctly.



Herpa attaches great importance to the design of distinctive elements such as nose and main landing gear, engines including air intake or landing gear flaps. On the Boeing 737, the retractable main landing gear is open (also in the model). Many other details are reproduced by pad printing. These include the windscreen wipers on the pilot's cockpit, which can also be seen in the photo, the landing lights on the wing bow and the various position lights.

Many details are reproduced by Herpa via pad printing. These include the windows of the cockpit and cabin (with printed frames), doors and position lights. This is also technically perfect, but unfortunately the manufacturer made a mistake: The prototype belongs to the type Boeing 737-230C and was usable as a freighter.

Despite correct registration and matching christened name, Herpa printed a Boeing 737-230, the pure passenger version. This can easily be seen on the left side, because the seventh window of the cabin was moved forward and thus arranged asymmetrically in the prototype.

Explanation of the Boeing version designations
Aircraft manufacturers use the type designation supplementary codes to differentiate between aircraft of the same type (here Boeing 737) and to make this clear to customers (airlines) in the competitive business.
The basic/original type is then often marked with the extension -100. The following abbreviations often refer to extended versions: Thus the longer versions 737-200, -300 or -400 were derived from the Boeing 737-100.
The extension can also refer to a technically advanced version with the same length, which was the case with the Boeing 737-600 and -700, for example.
Until a few years ago, the US manufacturer Boeing also used a two-digit customer number, which was also an integral part of the version designation.
Lufthansa, for example, had the customer number 30, which turned a Boeing 737-200C into a Boeing 737-230C, which technically did not differ from its siblings of the same version number.

The reason for this was the large freight door, which would otherwise have run through this window, and which is also missing. This is unfortunate, because the original photo printed on the box shows what it should have looked like.

But there is hope for interested customers: the model presented here enjoyed strong demand and the limited edition was sold out immediately ex-works. As far as we know, a follow-up edition is in preparation, which may be corrected in this respect. Usually, this will be given a different identification under the new article number, which could also be used to justify two models on the system.



Herpa's Boeing 737-200 from Lufthansa, due to its small size, its familiar appearance, and also a certain conspicuousness, is ideally suited to attract attention on the surface of the edge of the layout, and to draw attention, for example, to an underground airport railway station, which has become increasingly fashionable since the 1970s.

A Boeing 737 rarely comes alone. Today there are about 200 such planes in the air every minute at the same time, which could not better express their enormous distribution. Subsequently, the Europajet Boeing 727 has replaced it over the years as the most built commercial aircraft in the world.

Manufacturer pages and reference of the model:
<https://www.herpa.de>

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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Weiteres Nachschlagewerk

Moderne Fahrzeugvielfalt

Nach einer Übersicht über Lokomotiven, Reisezug- und Güterwagen, die heute auf Deutschlands Gleisen unterwegs sind, fehlte noch ein Typenatlas zu den modernen Triebzügen aller Art. Autor Michael Dostal war sich dessen wohl bewusst und schloss die Lücke mit einem weiteren Band. Diesen haben wir uns nun angesehen und möchten die gewonnenen Eindrücke gern an Sie weitergeben.

Michael Dostal
Typenatlas Triebwagen und Triebzüge
Deutsche Bahn und Privatbahnen

Geramond Verlag GmbH
München 2018

Taschenbuch mit Fadenheftung
Format 16,5 x 23,5 cm
160 Seiten mit ca. 200 farbigen Abbildungen

ISBN 978-3-95613-062-5
Preis 19,99 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Michael Dostal ist als Autor ein alter Bekannter: Schon häufiger haben wir hier Titel von ihm aus dem Geramond-Verlag vorgestellt. Seit Kindesbeinen interessiert er sich für die Eisenbahn, 1980 begann er auch das Fotografieren und Sammeln von Bahnmotiven. Heute verwaltet er ein Archiv von mehr als 15.000 Dias.

Neben vielen anderen Werken gehören auch Baureihen-Monographien zu seinem Schaffenswerk, darunter über die legendäre Baureihe 103 der DB und die Ludmilla der Baureihen 230 bis 242 – beide Bücher wurden auch in diesem Magazin besprochen.

Seit einiger Zeit schreibt und vervollständigt er vorrangig die Typenatlas-Reihe bei Geramond. Wer daran Freude gefunden hat, der hat bereits eine nützliche Buchserie in seiner privaten Bibliothek stehen. Doch darin fehlte bislang ein Werk über die modernen Triebwagen und Triebzüge, das wir heute nun endlich vorstellen dürfen.

Dies war zuvor insofern eine spürbare Lücke, als dass lokbespannte Züge immer weiter auf dem Rückzug sind. Triebwagen mit über den gesamten Zug verteiltem Antrieb erfordern kleinere Fahrmotoren, haben eine bessere Antriebsverteilung und dadurch auch höhere Beschleunigung. Das scheint sich auszuzahlen und weniger stark zu wiegen als die fehlende Flexibilität einer kapazitätsgerechten Zusammenstellung.



Angesichts dieser beschriebenen Bedeutung spannt der Autor ein breites Band über die große Vielfalt deutscher Triebzüge der Gegenwart. Eine klare und sinnvolle Gliederung ist ihm dabei gut gelungen. Vom ICE über Dienstfahrzeuge und auf Bahngleisen fahrenden Straßenbahnen bis hin zu Schmalspurfahrzeugen scheint nichts zu fehlen. Sogar die Schienenbusse der DB und DR, die wir doch lange auf dem Abstellgleis glaubten, tauchen hier wieder auf.

Selbst der brandneue, vierteilige Desiro HC von Siemens, eingeordnet als Baureihe 462, ist hier vertreten. Erst seit Mitte Dezember 2018 ist dieser Triebzug mit zwei doppelstöckigen Mittelwagen im RRX-Vorlaufbetrieb von Nordrhein-Westfalen aktiv: Das beweist, wie hochaktuell dieses neue Buch ist.

Sogar eigene Fotos von der Außen- und Innengestaltung hat der Verfasser einbringen können. Ähnlich verhält es sich beim Pesa-Link (Baureihen 631 bis 633), einem Dieseltriebzug aus Polen mit unverwechselbarem Gesicht, das ihm den Spitznamen „Hai“ eingebracht hat.

Trotz jahrelanger Verzögerungen bei der Zulassung sind auch von diesen Fahrzeugen Fotos aller drei Bauarten und nicht nur des Betreibers DB Regio eingeflossen. Damit scheint Michael Dostal fast schon die Betriebsaufnahme im Sauerlandnetz überholt zu haben.

Sauber abgearbeitet nach Elektrotriebzügen, Verbrennungstriebwagen, Dienstfahrzeugen und Schienenbussen erhält jedes Vorbild bis zu vier Seiten ausführlicher Beschreibung, dazu tabellarische Kurzinformationen auf einen Blick und mindestens ein aussagefähiges Foto.

Das erleichtert es dem Leser, sich gut und schnell zurechtzufinden, verschiedene Typen zu vergleichen und sich im großen Spektrum der modernen Schienenfahrzeuge zu orientieren. Eine Hilfe für so manchen Leser wird es sicher auch sein, dass das zwölfstellige Bezeichnungssystem nach dem europäischen Fahrzeugeinstellungsregister am Ende des Titels erläutert wird.

Wer die moderne Bahn liebt oder bei dem sie zumindest zum Interessengebiet einfach dazugehört, wird an diesem Buch und dieser Reihe nicht vorbeikommen. Für sie spricht übrigens auch der überzeugende Preis.

Da ist es keineswegs selbstverständlich, dass die Wiedergabe aller Fotografien hervorragend gelungen ist. Gute Vorarbeit hat der Autor hier aber schon zweifelsfrei mit deren Auswahl geleistet. Und so hat uns das Lesen und Blättern viel Spaß gemacht, das Buch empfehlen wir folglich gern zur Lektüre.

Publishing pages with reference possibility:
<https://www.geramond.de>
<https://www.verlagshaus24.de>

Geschichte der Boeing 727 Lufthansas Europajet

Was macht ein Buch über Flugzeuge in einem Modellbahnmagazin? Wir schauen rechts und links über den Tellerrand, da Flughafenabschnitte und vor der Hintergrundkulisse schwebende Maschinen in der Spur Z auch dazugehören. Dabei stießen wir, passend zu einem heutigen Modellthema, auf diesen Titel über eine nahe Verwandte der Baby-Boeing.

Wolfgang Borgmann
Die Flugzeugstars
Boeing 727

Motorbuch Verlag
Stuttgart 2018

Gebundenes Buch
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144 Seiten mit 160 überwiegend farbigen Abbildungen

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Autor Wolfgang Borgmann ist ein alter Bekannter der Luftfahrtbranche. Seit seiner Kindheit interessiert er sich für die Fliegerei, da seine in der Luftfahrt tätigen Eltern ihm diese Leidenschaft in die Wiege gelegt haben.



Nach Volontariat und Festanstellungen ging er 2000 den Weg in die Selbstständigkeit und ist seitdem als Buchautor und Verfasser von Fachartikeln bekannt. Auch beim Motorbuch-Verlag hat er schon einige Titel zur Geschichte von Flugzeugmustern, inklusive der im Modell-Artikel heute behandelten Boeing 737, und der Luftfahrt allgemein veröffentlicht.

Das Werk, das wir heute besprechen, erschien im zweiten Halbjahr 2018 und folgt dem bekannten Muster seiner Büche der Reihe „Flugzeugstars“. Spannend und fachkundig erzählt er auch die Geschichte von Entwicklung und Einsatz des Erfolgsmodells 727 aus dem Hause Boeing.

Bis zum Erscheinen der kleineren Boeing 737 war dieses dreistrahlige Kurz- und Mittelstreckenflugzeug das meistgebaute Düsenflugzeug der zivilen Luftfahrtgeschichte. Immerhin 1.832 Exemplare zählten die Auftragsbücher. Und so war die „Zwo Sieben“ auf fast allen Flughäfen der Welt anzutreffen.

Den Fluggesellschaften erschloss sie einst neue Märkte und war ein erster Vorreiter des Massenflugverkehrs. Wie die 737 spielte auch sie über viele Jahre eine wichtige Rolle bei der Lufthansa (über 50 Exemplare ab 1964) und hat daher auch eine Heimat bei einigen Modellbahnern der Spurweite Z gefunden.

Wolfgang Borgmann bedient deren Interessen in perfekter Weise. Er nähert sich dem Thema in geradezu philosophischer Weise, wenn er beschreibt, was dieses Muster ausmacht, womit es Schlagzeilen geschrieben hat und was sonst noch alles außergewöhnlich oder einmalig ist. Bevor er dann in ihren „Lebenslauf“ einsteigt, stellt er zunächst wichtige Fakten vor.

Die sich anschließende Historie arbeitet er diszipliniert und strukturiert ab. Er beschreibt die Umstände und Besonderheiten ihres Entstehens, geht auf ihren Konkurrenten „Trident“ ein, der deutlich früher am Markt war und sich nicht durchsetzen konnte, und fasst Meilensteine der 727-Geschichte zusammen.

Als (theoretisch) unmöglicher Flug ging das „Wunder von Da Nang“ während des Vietnamkriegs in die Geschichtsbücher ein, weshalb es ebenso wenig fehlen darf. Boeing und der Triebwerkhersteller Pratt & Whitney bildeten ein Erfolgsduo, wie der Autor anschaulich darlegt – maßgeblich für den Siegeszug dieses Flugzeugs. Geschlossen wird dieser Abriss von der letzten Landung auf dem Weg ins Museum.

Aber damit ist dieses Buch noch längst nicht am Ende! Durchweg hervorragend mit sorgfältig und gut ausgebildeten Bildern, denen das Alter aber meist auch anzusehen ist, arbeitet sich der Schreiber weiter voran. Immerhin verläuft die Geschichte weit über die sechziger und siebziger Jahre hinaus.

Er wirft einen ausführlichen Blick auf die Lufthansa als wichtigem Betreiber, die einstige Konzerntochter Condor eingeschlossen. Hier gibt es Lobeshymnen von Piloten, Einblicke in die Flottenplanung und nicht zuletzt den Blick auf das damals neue Lufthansa-Erscheinungsbild – immerhin dem ersten modern anmutenden der Luftfahrt. Vorgeführt wurde dies natürlich an der einst supermodernen Boeing 727. Lufthansa stellte sie und ihre Vorzüge gegenüber der Konkurrenz werblich als Europajet heraus.

Es folgen weitere deutsche und internationale Geschichten um dieses Flugzeug, teils tragischer Art, teils mit glücklichem Ausgang.

Da geht es nämlich um weitere Betreiber: Hapag-Lloyd flog die Boeing 727 im Charterverkehr, Germania ging jüngst in die Insolvenz und Jetair schrieb nur ein kurzes Kapitel.

Pan Am flog mit der „Sieben Zwo Sieben“ ins geteilte Berlin und ist wie die Mauer längst ein Abschnitt der Zeitgeschichte.

Fedex erwies sich als Rekordhalterin unter den Betreibern und auch für Delta Air Lines war es ein wichtiger Flugzeug. Noch rund 70 Maschinen sind übrigens bis heute als Frachter im Dienst.

Den Ausklang des Titels macht der Blick auf ein modernes Ölbekämpfungsflugzeug, für das die Konstruktion wie geschaffen war. Auch den vier wichtigsten Konkurrentinnen werden eigene Seiten gewidmet und darin zu ergründen versucht, warum sie sich alle nicht gegen Boeings Erfolgsmodell behaupten konnten. Dabei hilft auch eine Übersicht der wichtigsten technischen Daten im Vergleich.

Wir meinen, ein Eisenbahnfreund darf durchaus mal „fremdgehen“! Wer sich mit der Bahn befasst, beweist eine hohe technische Affinität, die auch das Interesse an Autos oder hier eben Flugzeugen weckt. Wir jedenfalls haben keine Leseminute bereut und sind heiß auf weitere Lektüre.



Hapag-Lloyd also acquired used Boeing 727s and used them in charter air traffic, shown here with Herpa models. By the way, the airline's colour of recognition was not orange, but cognac, which it attached great importance to.

Publishing pages with reference possibility:
<https://www.motorbuch.de>

New model railway appearance in Hall 7A

Anniversary in Nuremberg

In the spring of 1949, just four years after the war, a small group of people began a fair that was to become the world's largest and leading fair for toys of all kinds. The exhibition grew with the economic miracle and became well known worldwide. From 30 January to 3 February 2019, the organisers, exhibitors and guests celebrated the 70th anniversary together and looked back at the earlier fairs.

When the International Toy Fair was launched seventy years ago on a very modest scale, it was not even possible to foresee how the tiny little layout would develop. The manufacturers had previously met in Leipzig, but that was then behind a border that was becoming increasingly impermeable.

Four years after the war, there was still no European Union, no internal market, and certainly no single currency.

While the fronts between West and East hardened, the Western European states began to move closer together and hesitantly converge again.

But they had been trading internationally for centuries. Nuremberg's Lord Mayor Dr. Ulrich Maly knew how to describe this eloquently in his opening speech.

He used the two best-known Nuremberg products, the Nürnberger Eisenlebkuchen (a traditional gingerbread) and the Nürnberger Rostbratwurst (roasted sausage).



The International Toy Fair developed from humble beginnings, as this tent hall on Schoppershofstrasse shows in 1951. Photo: Spielwarenmesse eG

Via the “Fondaco dei Tedeschi” (central trading office of German merchants) in Venice, ships sailed to the Far East and imported spices. Without them, the gingerbread would probably have remained an ordinary nut cake.

Consequently, Dr. Maly explained: “And it was only the trade, which was already worldwide, that produced this typical Nuremberg speciality.” The famous sausages require pork, marjoram, salt and pepper.

“And the whole thing is packaged in sausage casings (sailings)”, he continued. “The sheep intestines come from Iran. This is an original product that can only be made if international trade works.” The International Toy Fair is also a prime example of such trade. Skilfully, he knew how to weave it into this construct and to oppose emerging nationalism with the advantages of free trade.



Nuremberg's Lord Mayor Dr. Ulrich Maly gave the opening speech at the 70th International Toy Fair.

Probably everyone in the room understood the side blow against the current EU withdrawal efforts by the United Kingdom, which in summary certainly does not bring any advantages to its citizens.

Dr. Maly described Europe as “a cultural space that has always been able to pick the best from all worlds and make something new out of it”.

This is exactly what the International Toy Fair stands for: the number of exhibitors has increased eightfold compared to the first, the number of visitors has increased sixteen fold and the number of occupied areas has increased seven and one-half fold.

One success factor might have been that “folk history is a global network of production, trade and sales”.

And with this in mind, the model railway sector appeared in 2019 for the first time after a major move in Hall 7A, where it has now been merged with the other model-making divisions. This shortened path, but a clear separation of the individual divisions was not made here.

+++ Acrylicos Vallejo +++

The Spanish paint manufacturer starts 2019 with “Chameleon Colours”. The name says it all, because the water-based effect colours impress the observer with their very different colour effects, depending on the direction of light incidence.

They do not require a clear lacquer sealant and are available in eight different colour shades at the same time. Depending on whether they are sprayed on a white or black background, they also look very different, which makes them very flexible. The most beautiful effects are achieved on a black primer, for example, blue turquoise / green.

The compilation “Ruins & Rubble” (Art.-No. 71.214) is intended for weathering houses and should also find its application in the model railway sector.

For the matt rust texture (69.821) from the Mecha-Color series presented last year, we also received the processing recommendation for brush application: This results in a particularly rough and rust typical surface, which can be thinly fogged with RAL 8012 reddish brown on freight wagons.



The Dumper (Art.-No. 322.026) is available in a fine and contemporary version from the Dutch accessories supplier (see next page). Photo: Artitec

www.acrylicosvallejo.com

+++ Artitec +++

For some years now, the Dutch resin specialist has been one of the most loyal suppliers of Z gauge accessories. And this year, too, the Zetties are expecting a fireworks display of six new products, at once.



Horse-drawn wagons were not very common in Germany (322,027). Instead, the animals were hitched directly to the tree trunks. Photo: Artitec

The Auto-Dumper (Art.-No. 322.026), also known as the Dumper, used to be a popular mini-tipper for loading tasks, while the horse drawn wagon (322.027) was used more in Germany's neighbouring countries.

The company's own tractor, as well as third-party products, are usefully supplemented by the bridge wagon (322.028) for transporting hay and scatter bales. But just unloaded and parked on the yard, this fine model will have a particularly good effect on its viewers.



The Krupp-Dolberg excavator (322.025; picture left) with backhoe is an important novelty because there have been no comparable excavators of this size and design so far. Many Zetties will also be happy about the trolley wagon (322.028 photo right) in order to be able to attach something to their tractors. Photos: Artitec

The shoe stand, horse & farrier (322.029) ensure that the most important workhorses of earlier times are well on their way. They are also a familiar sight on modern riding stables and farms with stables.

Closely related are the rope excavator (322.024), which Artitec calls the Dolberg crane, and the Krupp-Dolberg excavator (322.025) with backhoe, which differ only in their booms and shovels. They are typical excavators for construction sites of epoch III and close a large program gap here.

www.artitec.nl



The Dolberg crane (322,024) with its long mast jib will certainly not be overlooked on layouts. Photo: Artitec

+++ Badger Air-Brush +++

After the US manufacturer expanded its range of colours in recent years, this year it is once again taking care of the tools, its showpiece products. New is the "Sotar 20/20 slim" spray gun with soldered head, which eliminates the need for a gasket. The needle seal can be changed with a watchmaker's screwdriver.

The "Xtreme Arrow" offers a small flow container for those who don't need the big one, the "Xtreme Patriot 105", which appeared last year.

www.badger-airbrush.com

+++ Beli-Beco +++

Even though the Beli-Beco innovations are only presented down to nominal size N, all the innovations presented can also be used for Z gauge, as vice-CEO Stefan Bräuer once again confirmed. Since all models are handmade, they can be made shorter, if required and sometimes even with thin masts, as long as this wish is stated when ordering.

And so we Zetties benefit from eleven newly introduced lamps, which are delivered with a series resistor and protective diode for connection to 16 - 19 volts. They are all equipped with SMD LEDs (3 V operating voltage), which are installed directly into the case of the clock.



With this Beli-Beco new product (Art.-No. 155051), the Preiserlein (model figures made by Preiser) will be able to read the time even in the dark. Photo: Beli-Beco

The 19 mm high station clock is available with mast (Art.-No. 155001) or for wall mounting (155051). The clock itself is only 4 mm high and shows the typical model railway time five to five. With a height of only 2.2 mm, the square wall lamp (155551) can also be used without hesitation on 1:220 scale entrance doors.



And there are ten more lamps to choose from (from left to right): Station clock (155001), sidewalk lanterns with four screen shapes (155101, 155201, 155301 & 155401), wall lamps (155551), two whip lanterns (155601 / 155701) and two arc lamps (155601 / 155701). Photo: Beli-Beco

Smaller streets in residential areas are often equipped with lanterns on straight masts. Such models, with three different hood shapes (155101, 155301 & 155401), and as a ball light (155201) are also to be found among the new products, with which a selection for probably each region is in the selection.

Also with the whip lamps (155601 / 155701) and arc lamps (155801 / 155901) two forms are in the program in each case, with which main roads or station areas in east and west can be outfitted.

www.beli-beco.de

+++ Busch +++

This year it looks like quite a mess for the scale of 1:220 for the company from Viernheim. The concrete water pipe kit (Art. No. 7891) is intended for H0 scale and is dimensioned accordingly with a length and width of 49 x 24 mm and a height of 22 mm. However, it should also be possible to find areas of application here where such large quantities of water have to be channelled that, in exceptional cases, they can also be used in Z gauge.

The new rust spray (7010) can be used largely independently of scale. In addition to weathering in model making, it is also intended for decorative objects and finds its limits more



The concrete water pipe kit (Art. No. 7891) is too large to be used in the Z-gauge as shown in this picture. In the area of one of the hydroelectric power plants, however, it will do its job there. Photo: Busch



Caution is advised when using rust spray (7010). The rough surface on a scale of 1:220 will, in many cases, be rather out of character. Photo: Busch

in the spray head, which applies the paint coarser than a spray stylus and creates rough surfaces. The product is suitable for many materials including polystyrene and also dries quickly.

The 19 spring trees (9763) in May green, green, as well as white and pink flowering, seem to have shifted out of time. They are technically constructed like pipe cleaners, which also applies to the tree pack "70th Anniversary International Toy Fair" (9764) with deciduous and coniferous trees from 40 to 105 mm high. Both serve rather as nostalgic layout scenery.

www.busch-model.com

+++ CiLine Vitrinen- und Ladenbau +++

As a modular system, the endless display case range can now be individually and almost arbitrarily combined, dependent upon further growth.

www.ci-line.com

+++ Donau Elektronik +++

The new model railroad electrical distributor type 1 (Art. no. DM425) offers 26 terminal points, which are connected according to markings A or B and allow convenient wiring. Status LEDs on the 60 x 52 mm board indicate whether a current is present. Each terminal can be loaded with max. 6 A, while the maximum voltage is 30 V DC or AC.

shopware.donau-elektronik.de

+++ ESU electronic solutions ulm +++

The decoder manufacturer from Neu-Ulm is now sending its fifth generation of sound decoders to the field. With a 32-bit processor, they had significantly improved audio performance features. They mix up to ten audio channels with 16-bit resolution and now a sampling rate of 31.25 kHz.

Even the smallest Loksound 5 micro decoder offers an output power of 3 W and its dimensions of 21.0 x 10.6 mm at a thickness of 4.0 mm are significantly smaller than those of its predecessor, but are still slightly too large for use in Z gauge.

www.esu.eu

+++ Faller +++

Even though the Gütenbach-based model building specialist (for the time being) explicitly only mentions the H0 scale as the beneficiary of a new cooperation agreement, it may also be able to focus on the

smaller gauges, including 1:220 scale in the future: Faller has begun cooperation with Tinkertoys from Magdeburg.

Their core business is a self-developed and child-friendly construction program with which toys can be designed on screen and then printed in 3D. Faller would like to use this to have individual houses output for customers after defining the floor area, specifying the number of floors, and selecting a roof, as well as doors and windows.

In addition to the usual new products program, it should be noted that new products brochures will be released several times throughout the year in the future. The copy for the Spielwarenmesse (International Toy Fair) was therefore marked 1/2019 and surprisingly does not contain any designated Z gauge new products.



Two Z gauge new products were to be discovered at Faller at the fair, but only one of them was mentioned in the brochure as an advance notice: the Kino (cinema) Kandelhof (Art. No. 282795), which towers above all previous city townhouses.

However, the first concern is alleviated by a viewing page on which the Kino Kandelhof (Art. No. 282795) is announced in advance. This is a continuation of the laser cutting kits from recent years.

This multi-storey cinema building has already been built into the Messediorama (trade fair layout), which has been successively extended to include all the houses of the city series offered so far. Here we also discover a place of worship that seems indispensable for completing the series and is not yet mentioned in the brochure: the church of St. John the Baptist (282778), also made of hardboard.

But we also find what we are looking for in the areas of technology and design material: The stranded wire assortment (163780) offers 10 m cable material each with a cross-section of 0.04 mm² in ten colours. The strands are also available individually in red (163781), black (163782), green (163783), grey

(163784), yellow (163785), blue (163786), violet (163787), brown (163788), orange (163789) and white (163790).

Also offered are very simple trees with heights between 75 and 150 mm in four combinations: "15 deciduous trees, sorted" (181524), "15 mixed forest trees, sorted" (181529), "30 mixed forest trees, sorted" (181530) and "15 fir trees, large" (181542).

New 300 x 200 mm foliage leaves are now available in light green (181615), dark green (181616), multi-coloured (181617) and deciduous (181618). Reed (approx. 80 mm; 170715), long grass in a set of four (approx. 80 mm; 170769), light (171395) and dark (171396) tufts of grass with a length of 6 - 12 mm and light (171397), as well as dark (171398) grass strips of the same length, are also available.

www.faller.de

+++ Heki +++

Some new products from the Heki range can also be used with the Zetties without being explicitly designated as such. This applies to the mixed forest H0 (Art. No. 1957), whose 20 trees and firs are between 7 and 18 cm tall. The buyer does not have to reject a specimen if he decides in favour of the same product in the classification for N scale (1958), as the 22 specimens here are only 5 to 12 cm in size.



This mixed forest (Art.-Nr. 1958) is declared for the N scale, but its contents can also be used for the scale 1:220 without restrictions, since trees are still too small in the model. Photo: Heki

Also the 3 pollard willows (1911) we can imagine well on a Z gauge layout, because with its 5.5 cm high growth, it should be able to reproduce an old tree impression there quite well. Also, the Mediterranean

tree set H0 (1774) with 11 trees between 8 and 17 cm can be accommodated quite plausibly. Everyone who has ever seen full-grown pine trees knows this.



The trees from this composition (1774) are said to have a Mediterranean effect. It is intended by the manufacturer for H0 gauge, but we do not recognize any scale deviation for our purposes even with the pine trees. The olive trees with their silvery shimmering leaves are also quite beautiful to see. Photo: Heki

The 100 grass tufts (1825) and 10 grass strips with a length of 100 mm (1826) in the design, as flowering lavender would be suitable for this.

www.heki-kittler.de

+++ Herkat +++

The push button switches from this manufacturer are now equipped with mini LEDs in the colours red (art. no. 2281), green (2282), blue (2283), yellow (2284) and white (2285). Different LED inserts in the same colours (2271 - 2275) help to retrofit older push button switches.

The blue (2758) and white (2760) LEDs have diameters of 3 mm for individual tasks. Micro LEDs are also available in blue light colour (2744).

www.herkat.de

+++ Herpa +++

For some years now, Herpa has been announcing its innovations with a lead time of around four months in order to be able to derive reliable conclusions for production planning and quantities from customer feedback. They have therefore already been able to announce a number of things that customers will expect in the spring.

The Dietenhofen based manufacturer, whose brand name will be seventy years old in 2019, is using the International Toy Fair to announce and present the planned new products. In the field of model cars there is unfortunately nothing to report here, but among the airplanes in the scale 1:200, there are quite interesting new products to be found.



A partially printed sample of the Douglas DC-4 / C-54 Skymaster was already on display at the Herpa stand in Nuremberg, raising high hopes.

The new planes are due to appear as early as May/June. Newly designed is the Sukhoi T-50 (Art.-No. 559751), the prototype "White Shark", the first Russian stealth fighter, which will soon go into production as the SU-57.

The stretched Boeing 787-10 "Dreamliner," which was already known, might be too big for most model railway layouts. A surprise, however, was the four-engine Douglas DC-4, which was announced on the occasion of the 75th anniversary of the Allied landing in Normandy ("D-Day"), and the end of the Berlin Airlift 70 years ago.

Consequently, it initially appears as the military version of Douglas C-54M "Skymaster" (559720), with which the Americans had supplied the blocked western sectors of the divided city from Rhein-Main airfield.

Its twin-engine sister, whose design had been in existence for some time, will stand at its side. The also American Douglas C-47A "Skytrain" (559744) with the motive "Tico Belle" at the fuselage nose was involved in the 1944 air landing operation. In civilian form she flies as later Douglas DC-3 for Aer Lingus (559737).

For the Ukrainian navy, the Antonov AN-2 is in blue pattern with the identification "07 yellow" (559713), while the Airbus A220-300 flies "Latvia 100" (559690) for the civilian airBaltic. The same applies to the Aéropostale Transall C-160 (559683), which is operated by Air France.

The small Fokker 50, which also operates for KLM Cityhopper (559652), comes from Germany's neighbouring country. The four-engine aircraft that crossed the "big pond" were once only slightly larger: the reissued Lockheed L-1649A "Starliner" (558372-001) did the same for Trans World Airlines (TWA).

www.herpa.de



The old forester's lodge (art. no. 36812) is rebuilt as a single house by Kibri. Photo: Viessmann

+++ Kibri +++

The old forester's lodge (36812), which is announced by Kibri as a kit novelty, comes from an earlier kit with the former article number 6896. Judging by the brochure, the colour of the plastic for the wooden façade should be lighter than that of the buildings from the once independent production.

www.kibri.com

+++ Lenz Elektronik +++

The new LZV 200 central unit and the LH 101 handset controller are now also available as entry-level equipment for digital entry or changeover.

www.digital-plus.de

+++ Lux-Modellbau +++

The delivery of the dust witch ("Staubhexe"), which was previously announced in 2017, has been further delayed. It is now planned for release in the summer of 2019, and hopefully will win customers' approval.

www.lux-modellbau.de

+++ Märklin +++

This year in Göppingen, Märklin is celebrating several anniversaries: According to the official count, Märklin is 160 years old, the reintroduced 1 Gauge celebrates its 50th anniversary, and Minitrix can now look back on its 60 year success story.

So this year's exhibition cars were also dedicated to the anniversaries: A traffic red freight car, as it is used today for supplies to the automotive industry, was adorned with graffiti for the anniversary of N gauge (Art.-No. 15689).



Märklin's activities in 2019 will be marked by anniversaries, as can also be seen from the dealer gifts. The special Z gauge car (80129) in the foreground has different logos printed on each side with "160 Jahre Märklin".

Mini Club used the refrigerator car model (80129), which had been refined in the course of product maintenance, and which advertised the company's anniversary with the Märklin logo, a large 160 and different historical motifs printed on each side.

The most important message for the smallest track gauge is: the technical development of the last few years will continue to drive it forward.

In 2019, injection moulding with metal alloy plastic will be introduced, which is intended to help increase the weight of plastic injection-moulded housings. According to the plan, this will ensure safe power consumption and improved traction.

This process will be introduced with a new shape that would probably have been unthinkable without this technology. The tiny small car KlV 20 of the DB (88025), a first-generation VW bus on rails, will even be motorised on a scale of 1:220.



It's hard to believe that this tiny model is supposed to be powered: the KlV 20 (88025) by Märklin.

The drive is provided by a tiny bell-shaped armature motor on one of the two vehicle axles, and the second axle is also driven. Understandably, only one light function could be accommodated in this tiny vehicle. To illustrate the small dimensions, we placed a pencil tip next to the photographed prototype model.

The described procedure is also suitable for many existing forms. For example, the NoHAB diesel locomotive is reinstalled and at the same time subjected to model maintenance, in the course of which it is finally to be fitted with suitable bogie screens as in prototype, in addition to a bell-shaped armature motor.

You can choose between the green series 54 of the SNCB (88634) for epoch IV and the Hungarian M61 in epoch V lettering. On our advice, we are currently examining whether the models can also be converted to square buffers from the existing mould stock. This was not previously planned.

The Insider Club model 2019 in the form of the newly produced express train locomotive 03 1001 of the DB (88855) with disc wheels, which was already announced in Köln in November 2018, was finally on display in Nürnberg. The beautifully designed steam locomotive did not even give the impression of being a hand built master.



A first sample of the express steam locomotive class 0310 (88855) was shown in Nürnberg. And this gave the impression, at least on the outside, that it was ready for series production.

Insider Anniversary Car 2019 (80329) is, as also already known, a grey painted G1 11 covered car of the DB, rented to Miele, to be seen at the large company logo.

For the MHI, the class 86 (88962) in epoch III version of the German Federal Railway is issued as a special series. After the product has already been upgraded, with the lighting switched to LED and a detailed control system fitted, it is now also equipped with the bell-shaped armature motor. In addition, the buffer plates have been enlarged.

At the same time, a four-part "coal traffic" (86307) wagon pack with OOtz 43 (1 x) and OOtz 50 (3 x) self-discharging wagons is being published, also as a special MHI edition. The freight wagons with the lettering Erz IId and Erz IIIId are equipped with close couplings and coal load inserts.



The tender steam locomotive of class 86 appears with the Bundesbahn lettering and bell-shaped armature engine (88962; top right) and pulls a coal train (86307; top left) according to MHI's ideas. For epoch I there is a Prussian G 81 (88985) with rod buffers.

Also, epoch I does not go out completely empty, for here appears a reworked tender steam locomotive of the Prussian type G 81 (88985). It now also has the bell-shaped armature motor and is equipped with bar buffers and LED-illuminated dual-light peak signal.

A three-part freight train (86604) consisting of flammable gas wagons, a covered wagon of the bracing design type (later G 10) with loading doors that can be opened, and a beer refrigerator wagon of the brewery Loren Pfannenbergs Söhne (Zerbst) are packaged with it.

The redesigned Uerdinger rail bus was a great success. With its fine printing, bell-shaped armature drive and replica of the striking blue interior, illuminated by light-emitting diodes, it found many new admirers as an Epoch III model.

So it was only natural that he should now be provided with a model of the same quality for Era IV. The class 798 (88167) is delivered in two parts compared to its predecessor with a suitable control car class 998.

The passenger car pack (87507) dedicated to the Höllentalbahn consisting of three Donnerbüchsen Bie (2nd class) and one ABiwe (1st / 2nd class) and one Pwie (luggage car) each strongly reminds of a similar car pack for the club members a few years ago, which had only one 2nd class car less to offer.

Therefore, we will briefly discuss the design of the new car: The cars are painted bottle green using RAL 6007, have a grey aluminium roof (RAL 9007) and for the first time have pearl mouse grey window frames (RAL 7048), which is very discreet and unobtrusive. With the earlier pack, the bright yellow frames imposed too much on the viewers.

We would almost like to think that the tender steam locomotive 85 007 of the DB (88889) fits perfectly to these passenger cars. After all, they and their nine sisters were designed and built especially for the Höllental as standard locomotives. Until the electrification with 15 kV and 16 2/3 Hz frequency the nine war survivors could not be displaced from their main line.

But far from it! The beautiful steam locomotive with silver coloured boiler rings, Witte plates and buffer plate warning paint is converted into a Freiburg monument. Although it also benefited from the detailed control and brake imitations of the 44 series, the reattached smoke chamber central shutter and the locomotive number in the middle of the DB lettering according to DIN 1451 reveal its place in epoch V.

Of course, this also includes the shelter and four steel staircases, which protected them from the weather and made it easier to work. These equipment parts are realized as an architectural kit made of lasercut hard cardboard.



The rail bus was very well received by the customers with its interior fittings, fine printing, LED lighting and bell-shaped armature motor. Now it follows in two parts also for epoch IV (88167; picture above). The 85 007 actually belongs as a museum locomotive (88889; picture below) in epoch V.

In 2002 the class 143 (88438) electric locomotive was on the road in the Höllental valley, appearing for the fourth time in a traffic red version. Its switchover screw for overhead line operation was moved to the inside, and it is also supposed to impress with a printed train destination display in the driver's cab window. The model is powered by the new standard bell-shaped armature motor.

And Märklin, in view of the available tools for both design variants of the roofers, states that the right shape will also be used. Three double-decker coaches (87297) with train destination Neustadt are to be suspended from their towing hooks, which in the model are rather the system couplings.

To ensure that this is successful, Märklin is reworking the current collection of the power pickup bogie. In the future, it should guarantee a much better running and will have the white-red light change at the end of the driver's cab of type DBbzfa 761.

This year's "Höllental" theme should have become very clear so far. So it will come as no surprise that an appropriate station kit also appears in the range. Märklin has already received much praise for the station Himmelreich (89709) in its former condition before its renovation.



The architectural kit for the Himmelreich railway station is a perfect match for the thematic focus of the new product range (89709).

Its main features are the fine engraving of the brickwork, the equally successful structure of the wooden framework and the wooden studwork in the waiting area. Our magazine is already planning a report on this.

Not to be missed in the program is an Easter car (80419), which is also traditionally announced with the spring new products. This year a low side car with green base colour and Easter decorations has been used. The group of rabbits on its loading area is lasercut in hard cardboard.



Auch dieses Jahr wird es einen Osterwagen (80419) geben – und so schaut er aus.

For the right effect, the wagon is wrapped in a transparent Easter egg and delivered in an orange Easter basket with colour-coordinated Easter grass.

This brings us to the freight wagon segment, which will be served particularly extensively in 2019. The focus here is on Era III, for which the new shaped parts of the stake cars Rmms 33 are further combined to create a two-piece package (82132) with the addition of a blue Hanomag Matador flatbed vehicle.

Now the car has a brakeman's platform with steel shelves and press plate stanchions, while its brother has to manage without wooden shelves and stanchions. Within a very short time, Märklin has put an entire fleet of its award-winning model on the rails.

The X 05 low sided car with brakeman's cab (82334) is an old acquaintance. It is spiced up by a large diesel engine load from Duha. The self-unloading wagons OOtz 43 (82803) and OOtz 50 (86308) are designed and loaded like the wagons from the previously mentioned MHI package. They are therefore understood as an individual supplementary option from the standard range.

Small, but very fine, are the differences of the Aral-tank wagons (82324) of the old design. The model of a private car of the BV-Aral AG for the epoch IIIb is discontinued at the DB and captivates with its separately attached Aral diamond from the injection moulding.

The designers are now finally implementing an important model feature that had previously always been seen as printed on the boiler in a compromising manner.



Small detail, big effect: the old tank wagon (82324) receives a separately applied Aral emblem.

We also enjoyed the return of the freight silo car Kds 54 as private car "Frankenzucker" (86667) and later Ucs 908 of the Bundesbahn, to be found in a two-part freight car packing (86665). The individually sold unpainted wagons of the original edition yellowed over the years, and it was confirmed to us that the plastic construction of the new products had been painted and that this could therefore be ruled out.

The freight car package "From door to door" (82329) also opens up new perspectives: It consists of two container wagons BT 10 and a calf delivery vehicle with trailer. However, the special feature of the combination are the seven Pa containers of the open Eosakrt type with steel extension walls.



The track-laying train will be reinforced by two additional freight wagons (82425) and the two railway shacks (containers) enclosed in the package.

This is a new container type, already the third in total. This shows us that those responsible have obviously taken a liking to this particularly varied prehistory of modern container transport. For this reason, we expect further container types in the coming years and hopefully a third container carrying wagon. After all, the range still lacks a classic design with four positions.

The popular construction train theme will also be expanded further. Once again, DB Gleisbau (track construction; DBG) is growing. This time an Fcs rotary slide side unloading wagon and a Res four-axle stake wagon with corrugated side walls in typical yellow (82425) follow. It is almost half loaded with track ballast. The other part of the loading area can be occupied by one of two railway shacks, which are enclosed in the new sets.

A last, so far not mentioned jubilee is to be driven in Switzerland. The “Crocodile”, Märklin's heraldic animal, will be 100 years old. This is enough for a unique special edition of the green museum locomotive Ce 6/8III 14305 (88564) in a real wooden case.

A replica of the manufacturer's plate and a booklet as a model serve as an addition. The model itself had already undergone a model update, but now, for the first time in Z scale, it also has grey traction motor covers on the stems that are set off in prototype-like grey.



The RTS train, consisting of a class 221 diesel locomotive (88204; right) and three Eamos dump cars (82435; left) belongs to the current era VI.

Around 1971 the TEE 75 “Roland” (81593) was also on the road in Switzerland, covered with a Re 4/III of the SBB in TEE paint. To make this locomotive a perfect match for the train, it now also has round headlights in addition to the bell-shaped armature drive. Its four compartment cars (Avümz 111), large-capacity cars (Apümz 121) and dining cars (WRüm 132) are DB cars, and (finally again) red-beige around the corners.

Another new model is also dedicated to the Swiss railway enthusiasts, with which we make the leap into the railway historical present. In the three-part large-capacity sliding wall wagon package (82417) we find Habbiillnss wagons leased by Wascosa to Schweizerische Post AG.

Their car floors are made of metal and ensure good running. The yellow wagons have a white chest strip and are printed in German, French and Italian with sayings such as “Millions of letters a day”, “Life in Yellow” or “On a Journey for You”.

Graz is home to the Austrian company RTS, to which a three-part side dump truck package Eamos (82435) in orange is dedicated. The matching diesel locomotive is the series 221 (88204), painted in the same colours, based on the historical DB scheme. It cannot be seen from the outside, but it is now also driven by the bell-shaped armature motor and the lights change from white to red with a direction of travel change.

Some years back the car transport car DDm 915 (87093) jumps in far blue with red Ege cookie. The unloaded model is fitted with close couplers, as is the Bpmz 291.2 “Kinderland” (87591). Since the Kinderland, which gave the car its name, was at a fixed end, the lettering on the model did not stand diagonally opposite each other. Märklin wants to reflect this correctly on the model.

The spring new products are completed by a single-track fish belly bridge (89758) with a length of 220 mm and enclosed piers. The prototype of the hardboard construction kit is a bridge in Plettenberg



The Crocodile for the 100th anniversary of the prototype (88564) has grey engine covers, which can be seen in the side view. Here it drives for the fish belly bridge (89758) after the Plettenberg prototype.

(Sauerland region). It should captivate with fine replicas of all model characteristics including the rivet-tipped gusset plates.

www.maerklin.de

+++ MBR Model +++

This Polish company displayed at the Toy Fair new grass flock with 4.5 mm stem length in six colours: light green (Art. No. 54-0401), grass green (54-0402), forest green (54-0403), dark green (54-0404), olive green (54-0405) and beige (54-0406).

There are fine tufts of grass on a 15 x 20 cm carrier foil. Two of them have 2 mm short stalks and a verdant green (55-2001) or brownish-autumn (55-2004) shade, the others in different greens (55-2002, 55-2003 & 55-2006) and brownish-green (55-2005) are each equipped with fibres from 2 to 4.5 mm long.

With the grass mats in the 20 x 30 cm and 30 x 40 cm formats, only those with 66 mm fibre length are suitable for designing Z-scale scenes. They are offered in different shades of green, which are always combined with different coloured fibres (55-0022 to 55-0026 / 55-1022 to 55-1026).

The 10 x 15 cm flower carpets are very attractive, but due to their 12 - 15 mm height they are only suitable in tufts as larger, flowering shrubs. Available are: white (50-2001), yellow (50-2002), pink (50-2003), red (50-2004) and blue (50-2005) flowering as well as rushes (50-2006) and goldenrod (50-2007).

For the purpose which we have intended for floral designs on a scale of 1:220, the manufacturer has provided shrubs with the names "reed" (50-5008), "dry reed" (50-5009), "dark yellow" (50-5010) and "purple red" (50-5011). They each have a format of 15 x 15 cm.

www.mbrmodel.eu

+++ Microrama Model Decor +++

Microrama has always used RTS Greenkeeper flocking equipment for its impressive demonstrations. For these, the French “landscape magician” is offering new attachment boxes for storing and processing Magigrass fibres. They are transparent and allow the filling level to be checked at all times during work.

But there are also new products from the company's own production. Under the brand name Magiroc, Gwendal Theis presented a new rock design material in Nuremberg that impresses with its lightness and simple processing. The attachments are solid and hollow on the inside, which results in weight savings.

As a substructure a Styrofoam scaffold is sufficient, which is easy to build, and does not contribute much weight. If you operate a transportable layout, we will quickly appreciate this advantage.

www.microrama.eu
www.microrama.online

+++ Modellbaukompass +++

Trainini® readers continue to receive a permanent discount of 10% on orders placed in the electronics shop. In the open text block, the keyword “Trainini” is to be mentioned, so that the deduction is taken into account in the final invoice amount.

Heinz Wagner had brought a book tip with him to Nuremberg: The book “Civil Vehicles” (ISBN 978-84-09-00987-9) by Eugene Tur describes in English the effective ageing of civil vehicles with Vallejo products. In this respect, many model railroaders can certainly also benefit from a reading.

www.modellbaukompass.de

+++ Noch +++

After other accessory suppliers also jumped on the bandwagon to represent rocks with the help of paper that had to be crumpled, Noch decided to stay with the original and enter into a sales cooperation with the model construction studio Andreas Dietrich from Austria.

The latter invented the Knitterfelsen (cliff rock formations) and filed a trademark application for the name. The special feature of this original is that the rock structures are not printed on the special paper according to shades or other defined patterns. Instead, photo motifs from original photographs are used.



Here we show the “Wildspitze” (Art. No. 60302) as a representative sample of the five cliff rock formation motifs.

The company still distributes five different motifs in the formats 45 x 25.5 cm: “Großglockner” (Art. No. 60301), “Wildspitze” (60302), “Großvenediger” (60303), “Sandstein” (60304) and “Seiser Alm” (60305). Thus, a suitable structure and stone colour can be found for almost all areas of application. The finely cliff rock formations are simply glued onto an existing substructure.

In the standard tree series some new compilations appear, which are either explicitly declared for Z gauge, or at least can be used here, because we don't have to trivialize our vegetation. Here the 16 deciduous trees (24603) should be mentioned, which are only 4 to 10 cm high. But also the eight (24600) and sixteen tree (24601) packages should be applicable without any problems, which Noch, with 10 to 14 cm height, thought could be used only down to gauge TT.



The 16 deciduous trees (24603) from the standard series are 4 to 10 cm high and therefore also suitable for Z-gauge - some of them might even be a little higher. Photo: Noch

The same applies to the 16-part mixed forest (24623; 4 - 10 cm) and its larger neighbours with eight (24620) and sixteen parts (24621; each 10 - 14 cm). The 16 firs (24643) and those from the packs with eight (24640) and 16 larger trees (24641) have identical heights.

The corresponding article numbers for the snow firs in this series are: 24683, 24680 and 24681. Two tree assortments (25963 / 25964) with 100 trees each in summer design are planned for retail sale for dealers. If you are already thinking far ahead, you can also make friends with Christmas trees illuminated with yellow LEDs with a height of 5 cm (22111), 8 cm (22121) or 12 cm (22131).

Sand and gravel are available in different grain sizes, can be mixed with each other because of their coordinated colours and thus also offer creative design possibilities: "Sand fine" (09234), "Sand medium" (09235) and gravel (09237).

The well-known short grasses now have 6 mm long fibres as wild grass in the same colours: Dark green (07080), medium green (07081), brown (07082) and gold-yellow (07083).



The aging and altering of cardboard kits and other objects can be done using Noch's patina markers (61158/61159). Photo: Noch

The new patina markers can be an alternative to powdery and pasty colours. These pens each have a fine brush tip and a wide marking tip for a larger area of application. The first of two sets of six (61158) is intended for models made of plastic and hard cardboard, the second for landscape design (61159), where, in addition to rocks and walls, model waters (algae replication) are also envisaged as an area of application.

A further focus in the new product range is on so-called "Perfect Sets". They contain typical design products, including new items from recent years, and a CD with instructions.



The "Perfect Set" compilations contain well-known design products with a theme reference and instructions on a data carrier. Here you can see the package "Right & left of the tracks" (60811). Photo: Noch

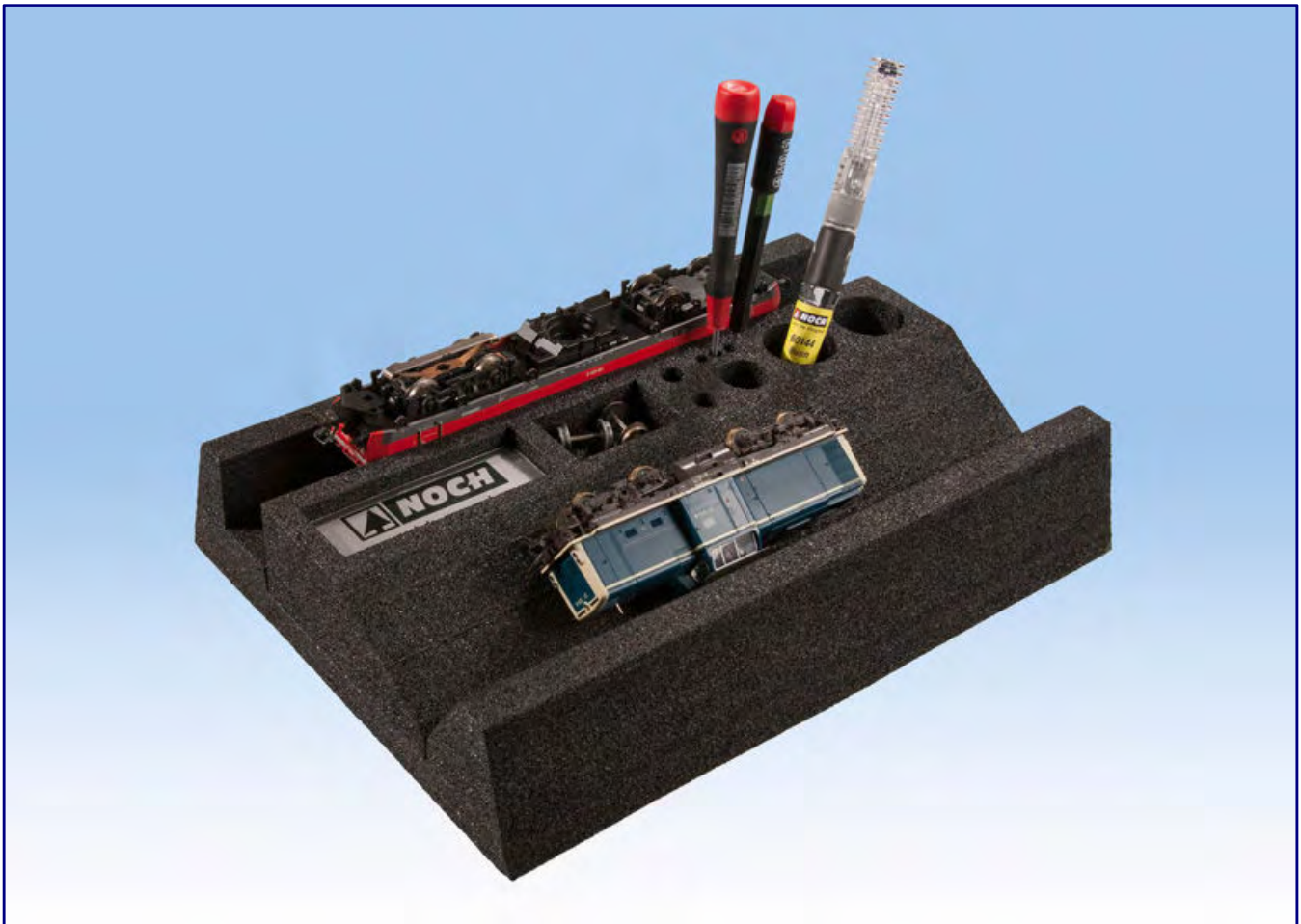
The product "Right and left of the track" (60811) with natural bushes, foliage, structure flakes and grass tufts is particularly suitable for Z-gauge purposes.

Simply "Lake" (60813) is the name of the combination of "water drops", colour granulate, wave and wave filler as well as tufts of grass.

The "Winter Landscape" (60815), in which snow glue, snowflakes, shaking can, snow paste, icicles paste and icicles are included, can also be used for Z gauge.

But the accessories supplier does not completely disregard the technical range either. With the professional locomotive train service tray (99352), the

Wangen company creates a good working base for maintenance and repairs on models. It is made of foam and does not damage the valuable treasures.



The professional locomotive train service tray (99352) proves to be helpful for the maintenance, care and repair of rolling stock. It also has a magnetic storage surface (with Noch logo) so that the smallest parts, such as coupler springs, are not lost. Photo: Noch

These can be inserted upside down, the necessary tools also find a safe place here. A magnetic contact surface protects against the loss of springs and other small metal parts. With the “model and precision oil pin” (60144), you can find the right lubricant in the same product line.

www.noch.de

+++ Roco +++

Roco offers the new CAN hub (art. no. 10804) as a supplement to the Z21 control unit. It provides two CAN outputs each with a 1 A power supply, and is ideal for large systems with many CAN stations. The connection cable for the CAN bus is included.

www.roco.cc
www.z21.eu

+++ Rokuhan +++

The Shorty chassis was shown at the International Toy Fair, and it has been well received for in-house and conversion projects. From the manufacturer's point of view, this also includes the matching bodies for Japanese models.



The three-part “Revaty” (picture above) is underway in Japan as Tobu-Limited-Express 500 and is offered by Rokuhan in two versions (7297842 / 7297843). Connecting bellows and couplings (picture below) are included in the individual packages in order to be able to couple two units to each other prototypically. Photos: Rokuhan

The modern “Revaty”, which has been on the road in Japan as the Tobu-Limited-Express 500 since 2017, is included in the distribution via Noch.

The three-car multiple-unit train is often coupled together with another three-car unit in prototype operation. Rokuhan also makes this possible in the model, as the individual packs include bellows and couplings for connecting cars. The directional lighting between two units can be switched off.

Available are the three-part trains “Revaty Kego” (7297842) and “Revaty Aizu” (7297843) as well as a starter kit (7297806) with motorised train, driving gear, re-railing guide and track oval.

www.rokuhan.de



The castle Kriebstein (Art.-No. 778) has been built on a scale of 1:250 and is recommended as background model for the Z-gauge. Photo: Schreiber-Bogen Modellbau / Aue-Verlag

+++ Schreiber-Bogen Kartonmodellbau (Aue-Verlag) +++

The Kriebstein Castle (Art. No. 778), which occupies an area of 34 x 25 cm and is 29 cm high, is built on a scale of 1:250. Only those who have a large harbour on a layout will be able to accommodate the refrigerator ship "Slovan Alstertor" (3329), also reproduced in a scale of 1:250.



For the refrigerated ship Slovan Alstertor (3329) with its striking loading crane jibs, there is also an upgrade kit (4329) with fine details for a more accurate design. Photo: Schreiber-Bogen Modellbau / Aue-Verlag

The ship dates back to a time when aesthetics were a high priority. But with its length of 56 cm it also takes up a lot of space. For the finer details and for the setup, we recommend the upgrade kit "Slovan Alstertor" (4329), so that the final result is more accurate.

www.schreiber-bogen.de

+++ Seuthe +++

A cleaning fluid that was previously available is returning to the product range. It removes dirt of all kinds.

www.seuthe-dampf.de

+++ Silhouette Modellbau +++

Following the takeover and continuation of the brand by the new owners, the Silhouette range has picked up speed. What has happened there over the year can only be guessed with your own browsing through the website. New in spring 2019 are various vegetation mats for spring, summer and forest edges in the usual fine design.

www.mininatur.de

+++ The Cool Tool +++

New control electronics with integrated motion controller and pre-installed CNC software were presented in Nürnberg. It is called TCT-Control and also offers a tool movement simulation mode.

www.thecooltool.com

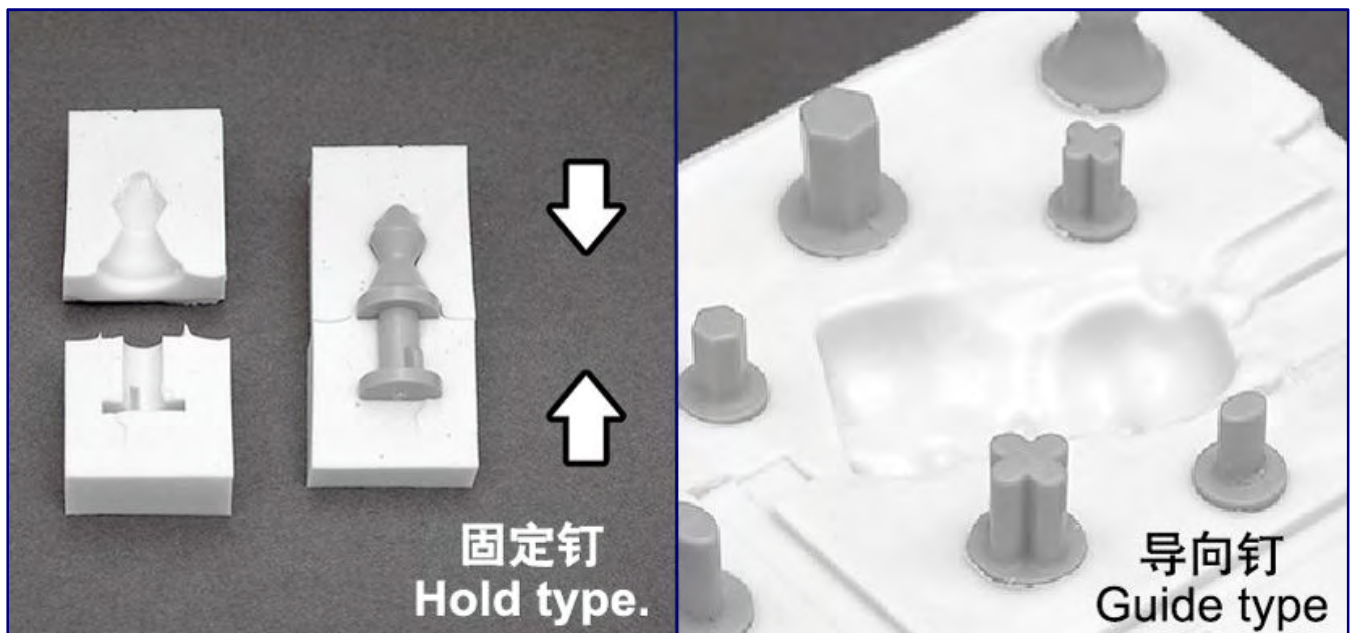
+++ Trumpeter +++

The Chinese plastic model maker has now added tools to its program, which are referred to as "Master Tools," and, of which, we would like to briefly introduce some here. These include, for example, three bending aids for etched parts (art. no. 09931 to 09933) in various sizes, craft knives with draw blades (09908), fine saws (09909) or clamps (09914) for holding painted parts.

The sandpaper holder in eight different shapes and sizes (09929) helps smooth fine model parts. Various colour palettes (09960, 09973, 09974 and 09975) are very helpful when working with paints, glues or spatulas.



The clamps (Art.-No. 09914), inserted into the base plate, hold painted parts during the drying process, but can also be used as a third hand. Photo: Trumpeter



The function of each of the 16 holding pins (09982 to 09984) in two basic moulds as a guide when assembling a multi-part mould (right) or when holding together during filling (left) is illustrated here. Photos: Trumpeter

Anyone who wants to produce multi-part silicone moulds for duplicating master models will appreciate the 16 small holding pins (height 20 mm; 09984) which are cast in and then help to place the mould parts exactly on top of each other. They are also available in two larger lengths (09983 / 09982).

The five needle files (09964) and the five diamond files (09965), each in different shapes, are always required. Their handles each have a diameter of 3 mm; the tools are 140 mm long.

This compilation represents only a small selection of the extensive program, in which further hand and also electrical tools can be found, and which can be found, however, in similar form also elsewhere.

www.trumpeter-china.com

+++ Unique Scenery Products +++

For the flocking devices "RTS Greenkeeper" distributed by this supplier, there are new, transparent swap bodies which are suitable for devices of all performance levels and which have already been described at Microrama.

www.sceneryproducts.eu

+++ Viessmann +++

The two sound modules "Street Guitarist" (Art.-No. 5577) and "Jukebox" (5578) for connection to direct or alternating current sources are interesting for animating figure scenes or a classic pub. A further module "Shooting Range" (5579) is also available.

The blue LED with soldered caps (3564) is packaged in five pieces, each 1.6 x 0.8 mm in size. Skilfully combined with light guides, they also illuminate emergency vehicles very effectively.

This also applies to the 2 Mini-LED red (5272) and 3 Mini-LED red-yellow (5273), each with micro-flashing electronics, which, however, are no longer installed in the vehicle itself, but underneath the roadway in Z gauge.

www.viessmann-modell.de

+++ Woodland Scenics +++

The in-house lighting system will be further expanded with further electronic components. The "Field System" landscape design range also includes grass fibres with lengths of 2 and 4 mm in the colours dark green (art. no. WFS613 / WFS617), medium green (WF614 / WFS618), light green (WFS615 / WFS619) and straw colour (WFS616 / WFS 620).

woodlandscenics.woodlandscenics.com



In combination with figures from C-M-K or Trafofuchs, the sound module "Street Guitarist" (Art. No. 5577) will also do its work in Z gauge. Photo: Viessmann

17. Internationale Lahnsteiner Modellbahntage

Sa. 10-17 Uhr
So. 11-17 Uhr

9. und 10.
März



Stadthalle am Salhofplatz 56112 Lahnstein

Große Tauschbörse für
Eisenbahnen, Autos usw.
Modul- und Schauanlagen
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New and news of the small series

Far away from the International Toy Fair

For many years, the International Toy Fair has been losing importance for the model railway sector. The relocation from the traditional Hall 4A to the new Hall 7A, in which all model-making sectors have been bundled, has done nothing to change this. With Micro-Trains and American Z Line, the two most important US suppliers have also never been represented in Nuremberg, that's why we are also looking to the left and right to be able to report on all the news and new products that have been announced outside the exhibition.

+++ A2 Models +++

Some years ago A2 Models attracted attention in Altenbeken and presented a very extensive car model program from the 3D printer within a very short time. But for the last two years nothing has happened in the program.

Now it is sad to know that this supplier is closing down. The sale of available models of all scales is already running with a discount of 20 %.

www.a2models.nl

+++ Álvaro Cortes +++

The Portuguese model car constructor is also burning off a firework display of innovations this year. Customers will be particularly pleased about the emergency vehicles from Germany, Austria and Switzerland, which are based on the VW Transporter T6, the E-Class from Mercedes-Benz, Porsche and others dedicated to the police and emergency services.



Various emergency vehicles form the main focus of the new car models from Portugal. The VW Transporter is also available in civilian versions. Photo: Álvaro Cortes

The VW transporter of the current generation is produced in various metallic finishes as well as black, and also in a civilian version, including a yellow version as a delivery van with windowless loading area.

A feast for the eyes is the Mercedes-Benz Actros as a car transporter, loaded with various sedans and station wagons of the traditional Swabian brand. The same truck type is also available as a wood transporter in long and short version, both with attached crane, which is movable, even in the model.



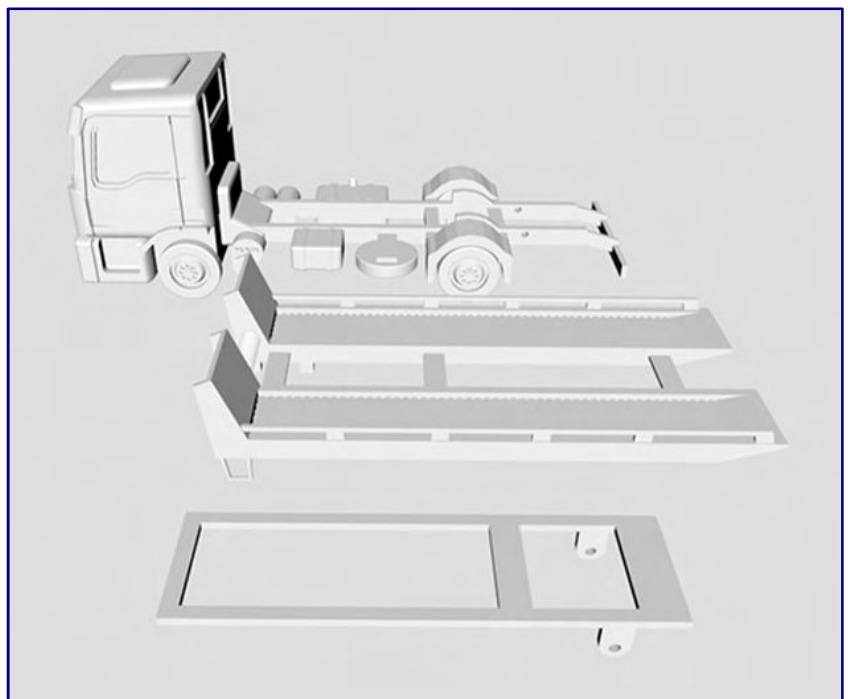
The Mercedes-Benz Actros cuts a convincing figure as a car transporter (picture above) and as a wood transporter (picture below). The loading crane is even fully movable on the model. Photos: Álvaro Cortes

America car fans will be catered for with the Chevrolet Suburban and the GMC Sierra, while the McLaren 570 S and 570 GT sports cars have their fans among the wealthy worldwide.

In addition, several 3D printing projects are underway with which Álvaro Cortes is breaking new ground in implementation: MAN Lion and Mercedes-Benz Citaro buses are currently being built here. Based on MAN, he is developing a tow truck and various truck bodies in parallel, from the flatbed with tarpaulin to the classic box body and the refrigerated box.



The GMC Sierra with closed and open tailgate (photo above) handles the interests of US railroaders. With the modern towing vehicle (photo right), one of the current 3D printing projects, those who build European models will certainly be able to do more. Photo / Illustration: Álvaro Cortes



Even for real connoisseurs of this creative designer, however, house furnishings with beds, stairs, chairs and tables (set) will be surprising, which are also waiting to be produced with 3D printing.

[Acortes\(at\)leya.com](mailto:Acortes@leya.com)
<https://www.facebook.com/My-Z-scale-models-1648628058694887/>

+++ Archistories +++

Archistories is currently developing three new products, which will be distributed exclusively by the 1zu220 shop. The universally usable track marshal 271 (Art.-No. 110181) with its red bricks is reminiscent of many buildings in Münsterland. It has an angled dormer roof and numerous fine details.

The railway underpass “Norbertusbrücke” (109181) is the sandstone coloured underpass of a railway embankment or a road. It shows a round arch passage and carries railings at the top to prevent falls. The embankment walls can be individually angled and adjusted.



All three Archistories new products are exclusive items for the 1zu220 shop: Line item 271 (art. no. 110181; top left photo), Norbertusbrücke railway underpass (109181; top right photo) and Torrstein makeshift signal box (111181; bottom photo). Photos: Archistories

There never seems to be enough signal boxes and the makeshift signal box “Bahnhof Torrstein” (111181) is completely out of the ordinary, thanks to its extraordinary appearance. It was created from a disused “Donnerbüchse”, which had a front extension for better track visibility. After all, the interior was very cramped.

In contrast to many contemporaries, the part stands on a filigree steel frame instead of a massive wall base and thus radiates a certain lightness. This was only possible with the model because signals and points from the inside were already electrically switched and thus the mechanical signal box with ropes, deflection pulleys and tension levers could be dispensed with.

This is the only reason why this model can look so aesthetic and fine, which is also ensured by the filigree details, which create a fascinatingly, realistic impression on the railway track.

www.archistories.com
www.archistories-shop.de

+++ Atelier Dietrich +++

The very successful Knitter-Felsen (cliff rock formations) with photo printing are now more widely available and more easily available, because beginning this year, they will also be distributed by Noch.

www.atelier-dietrich.at

+++ Avantgarde Modellbau +++

Unfortunately the webpages of Avantgarde Modellbau are not (any longer) accessible since some months, as already readers were surprised to find out. But Phillip Meyer, who presented his offer in Altenbeken in detail and with passion, did not stop his activities at all.



The old, rotten oak with hollowed out tree trunk, dead wood, moss and lichen growth is one of the new products 2019 at Avantgarde Modellbau. Previously a tree of this size and growth was only known as a healthy specimen. Photo: Avantgarde Modellbau

The opposite is the case, since a new workshop is to be moved into this year in order to improve the production process and thus reduce delivery times. Avantgarde Modellbau responded quickly and extensively to our request for new products.

With a view to the information gap in the net, we advise interested parties to contact us by e-mail until further notice or to alternatively call up the pages via <https://avantgarde-modellbau.beepworld.de>. This brings us to the explanations and enumerations of this year's innovations.

Here the product range is being expanded further: After the perfect replica of one of the most striking US southern state trees and a particularly detailed old oak, some trees and plants are also planned according to the Japanese model: Ginkgo (also planted in Germany as a park tree), Japanese birch, crescent fir and red pine as well as rice fields.

This awakens the desire to recreate the typical Far Eastern landscape to the right and left of the Shinkansen routes in a model. The matching trains can already be found in the Rokuhan program.



The house trellises would be revised and are now available again in even more convincing quality. Photo: Avantgarde Modellbau

The aforementioned trees in California and the Southern States are still open items from the previous year. The old, rotten oak, an outstanding solitary tree with a hollowed out trunk, deadwood, moss and lichen, was previously known only as a healthy tree.

Model maintenance is also a topic for the young year. A large part of the Finescale tree programme is being revised and given new foliage with individual leaves of the correct size. The house accessories have also been revised and are already available again.

Lavender (field strips), various flowers (different shapes and colours) and wild plants (such as lupines) as well as "ruderal vegetation" have also been added. Behind this lies everything that grows in plants and herbs in railway facilities. Not to forget wine in the form of vines. The plants mentioned are ivy, wild wine and blue rain.

Provided there is a sufficiently high level of customer interest, building models will also appear from the end of 2019/20. At the beginning, the typical Ticino stone houses ("Rustico" as shed and farmhouse) will be available as resin building kits, and finished models are planned.

www.avantgarde-modellbau.de
[motewe\(at\)gmx.de](mailto:motewe(at)gmx.de)

+++ AZL (American Z Line) +++

AZL has been a major player in the rolling stock sector since 2000, and has long been the most important supplier of vehicles based on American models. Recently, however, the Norwegian-American company has been paralyzed by the (orderly) insolvency of the Chinese supplier Affa.

According to our information, this problem has now also been overcome and the construction and production of new Z-scale models should continue soon. But even before this forced break there were still a few things that are now gradually being launched on the market.

And so the new year begins immediately with new designs, eight in number. They apply to the light passenger coaches in their different designs and are graced in the first edition to an icon of the US railway companies, the Union Pacific (UP).



In addition to the usual detailed design with separately attached parts such as antennas and standard close couplers, the interior fittings in the tails of the observation cars should also be mentioned here. Special features, reproduced in the model, are the green tinted windows of the cars following the UP prototype.

The models used were distributed over the entire UP network. Among them were also many name trains like "City of Los Angeles", "City of Portland" and "City of San Francisco".

Also worth noting is the fact that some foreign cars of the SP, C&NW as well as the Wabash have crept in, but they also wear the yellow colour of the Union Pacific.



The EMD E8A in early version (art. no. 62600-1) is a suitable locomotive for the new light passenger coaches, see photos on page 62: 4-4-2-sleeper coaches of the Southern Pacific (73008-5; top picture), observation coaches (73408-1; centre picture) and pulpit coaches (73808-1; bottom picture). Photos: AZL / *Ztrack*

The following types of cars were produced in this way: 4-4-2 sleeping cars (item numbers 73008-1 to -5), 6-6-4 sleeping cars (73108-1 to -6), observation cars (73408-1 to -3), dining cars (73508-1 / -2), luggage cars (73608-1 / -2), seating cars (73708-0 to -2), end cars with pulpit (73808-1 / -2) and post cars (73908-1 / -2).

Suitable locomotives for this are the EMD E8A in the early colour version (62600-1 / -2). AZL retained this version, which still lacked the cooling shields above the roof fans, until delivery of the new passenger cars.



The covered AAR cars now run for the Northern Pacific, here the double pack (904379-1). Photo: AZL / *Ztrack*

The second edition of the light passenger coaches is dedicated to the Canadian Via and will also follow this month: one seat (73751-0) and two post coaches (73951-1 / -2) will be delivered here. Suitable locomotives are the modern GE P42 Genesis, of which two are offered in blue (63504-1 / -2) and one in turquoise-green (63504-3) basic colours.

The EMD SD70M of the Norfolk Southern (6101-1 to -3) represents a further shape and colour variant for modern freight train traffic. They come from AZL's update program, which includes the installation of old housings on the new undercarriages, and are therefore very limited in edition.

The previous Amtrak trains can be extended with three light food (snack) (73550-7 to -9) and three baggage cars (73650-7 to -9), each in Phase IVb paint. Following Phase III, however, the multi-storey transport wagons for the "Amtrak Auto Train" (904101-1 / -2) have been assembled in four-car packs.

But there are also new freight wagon variants for ordinary trains, namely for the Northern Pacific: Here the covered AAR wagons with a length of 40' now appear as single wagons (904309-1), as well as in combinations of two (904379-1) and four (914309-1).

The 89' long flat cars from TTX are loaded with one M1126 and one M1134 wheeled tank each from the production of Z tanks. They are each sand-coloured (colour code S in the art. no.) or olive green (colour code O), the cars yellow (911010-5O / 911010-5S) or brown (911020-2O / 911020-2S). As all four possible combinations are possible, no wish remains unfulfilled here, either.

www.americanzline.com

+++ Bahls Modelleisenbahnen +++

Michael Bahls announces with the three-axle car transport car Offs 55 a new edition, which has it in itself. Its model was the first freight car designed specifically and exclusively for vehicle transport. Its basic concept was retained in the subsequent models, but these could also load higher vehicles such as the VW Bus T 1 ("Type 2") in the lower deck.



The three-axle Offs 55 car transport wagon is currently being built at Bahls Modelleisenbahnen. Delivery is scheduled to start by the end of 2019. Photo: Joachim Claus, Sammlung Eisenbahnstiftung

A revised new edition of the Gläserne Zug (series 491) is to be delivered in 2019 in various variants, including with an optional ski trailer.

The road traffic of epoch II is enhanced by two form variants of the legendary Hanomag Kommißbrot (Loaf of Rye Bread, because of its small squarish shape).

www.bahls-modelleisenbahnen.de



The unmistakable Hanomag Kommißbrot also appears at Bahls. Photo: Bahls Modelleisenbahnen

+++ Creativ-Modellbau Klingenhöfer +++

Inspired by our award in the context of the new releases of the year 2018, the duo from Creativ-Modellbau Klingenhöfer is now following suit: No less than three new animal figures in their excellent fine design and detailing are now new in the product range.



Walrus (left), man with Lama (middle) and the snow goat (right) are the three figure new items from C-M-K at the beginning of 2019. Photos: Creativ-Modellbau Klingenhöfer

For use in a zoo, the thick walrus with the striking tusks is recommended, while the man with the llama can also be found on a farm, since these other animals, like the domesticated alpaca, are also kept here as wool suppliers. Meanwhile, the snow goat in its wintery white coat feels at home in the mountains of a model railway layout.

www.klingenhoefer.com

+++ D&H Doehler und Haass Steuerungssysteme +++

The new feedback unit, as well as the decoders SD05A, SH05A and PD06, about which we reported here a year ago, are now available. All important information can be found on the D&H website.

www.doehler-haass.de

+++ EtchIT-Modellbau +++

As announced, this still very young supplier in the Z-gauge market has converted further model kits from its N-gauge range into the nominal size Z. The Z-gauge range has been extended by a new range of model kits. He took care not to simply scale down the model designs, but to optimize wall thicknesses and other critical parts of the models for 1:220 scale, which he emphasizes.



Here, you can see the construction of the Citroën Ami 8 Break (Art.-No. ET027_Z) described in the text. Photo: EtchIT-Modellbau

This also applies to the layer thickness of 3D-printed parts, which is 15 microns (0.015 mm). This minimizes the striping of layered models that is common in 3D printing.

The material used is a special resin that is cured by UV light and guarantees high stability even with fine components.

The models from etchIT-Modellbau are supplied as kits for self-assembly and painting. For the upcoming months further new products in scale 1:220 are planned.



The Fortschritt E512 (XD120_Z) combine harvester, which was built in the DDR, offers a high degree of flexibility in layout use with its detachable cutter bar and enclosed transport carriage. Photo: EtchIT-Modellbau

One new agricultural machine and three cars have to be reported. The new combine harvester has its role model in the east and is named "Fortschritt E512" (Item no. XD120_Z). The model, produced in 3D printing, consists of separate upper and lower parts to facilitate the application of a prototype-like two-colour finish. The cutter bar is removable and can be stored on the enclosed cutterbar trolley when driving on the road.

With the car models the producer usually follows the prototype only vaguely, however, because of the good implementation, the vehicles are easy to recognize.

The car based on the French model (ET027_Z) follows the design and styling of a Citroën Ami 8 Break (station wagon or kombi).

The second car model, a five-door Opel Astra F with hatchback (ET004_Z) also follows the same design as the aforementioned car: chassis and body (with seats and further interior) are manufactured as separate parts in order to achieve the highest possible level of detail and a simple colour design.



The sporty convertible (ET023b_Z) can easily be recognized as a VW Karmann-Ghia. Photo: EtchIT-Modellbau

The third new item for private transportation in Z gauge is also a 3D-print model, but a little older. It is also a car with a cult status: We are talking about the VW Karmann-Ghia (ET023b_Z) as a sporty convertible, which was built as Type 14 for Volkswagen until 1973.

Here the vehicle was produced as a single part model, to which only the four wheels are separately attached. The procedure described at the beginning allows reproduction of an exceptionally fine frame around the windscreen.

www.etchit.de

+++ Fischer +++

Fischerwerke from Waldachtal are known worldwide for their fastening technology. For decades, the famous Fischer-Dübels have ensured this. But hardly anyone in the model railway sector knows that this company now also has an adhesives division.

And it was in this sector that we became aware of the new UV adhesive (Art. No. 545866), which Fischer claims “sticks at the speed of light”. This product was developed, consisting of an adhesive bottle with a closure tip and a small UV LED device, probably with a view to the criticism of a similar competitor product due to allegedly unreliable bonding.

The Fischer alternative works differently in that it combines the light-sensitive part, which hardens only after irradiation and can develop immense adhesive strength, with a classic cyanoacrylate (superglue). This provides holding power where the UV light emitting diode cannot reach.

The in-house 30-second adhesive (545868), a cyanoacrylate-based adhesive that allows repositioning for a short time, and the power adhesive (545865), whose two components are only mixed in the tip when it is pressed out, are also interesting for model making. It remains workable for six minutes and is already hand-solid after ten minutes. After 24 hours it reaches a final strength of 170 kg/cm².

www.fischer.de

+++ Freudenreich Feinwerktechnik +++

Shortly after the publication of this issue, the NoHAB diesel locomotive TMY1150, designed exactly to scale, will be delivered in the white paint finish of Railcare. It is also available as a DCC sound version.



The ÖBB ribbed cars will follow in 2019 in the version with drop windows. Photo: FR Freudenreich Feinwerktechnik

In addition, the seven-winged ÖBB ribbed cars with drop windows will appear this year, while the new Xas 73 ballast car will receive BLS printing. In Freudenreich's main Scandinavian segment, customers can look forward to a new design: the two-axle Littera F5 passenger train baggage car of the SJ is to be offered here in the course of the year.

Shortly before the editorial deadline, the decision was made to offer a “parts set Eanos unpainted” (Art. No. 00.337.91) of the open freight car of the type Eanos-x 052. This consists of an unpainted body, the weight underbody plate, bogies of type Y 25, double-sided insulated wheels, system couplings and attachments in etching technology.

www.fr-modell.de

+++ Heißwolf Modellbahnzubehör +++

The GFR1500 DC voltage speed controller will be available in the first quarter of 2019 as a variant of the proven SFR1500. This very easy to operate speed controller exclusively supplies a finely adjustable DC voltage, which is intended for the operation of analogue models with bell-shaped armature motors.

The maximum output voltage, as well as the behaviour of the short-circuit monitoring, can be set in the configuration of the device. Overtemperature protection and fan control ensure safe operation.

Consisting of a handy operating unit and a power section, the latter is available with (Art. No. 1406) or without housing (1401; matching fan installation kit: 2430).



View of the open power section of the GFR1500 with attachable fan. Photo: Heißwolf Modellbahnzubehör

www.modellbahn.heisswolf.net

+++ HOS Modellbahntechnik +++

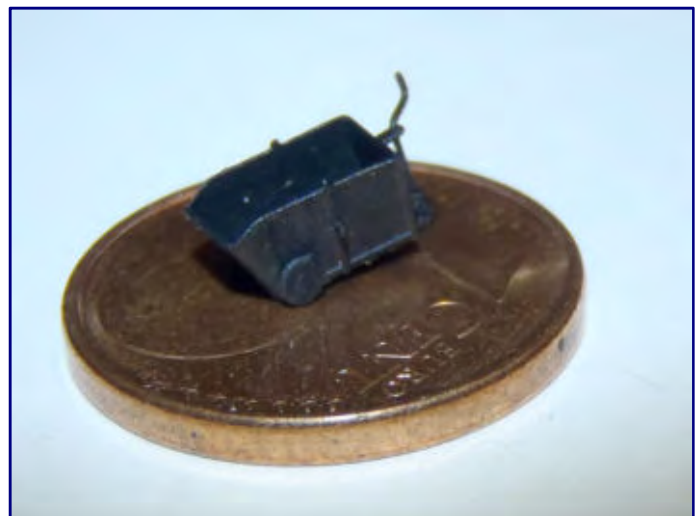
Heinz O. Schramm is changing more and more from a filigree model maker with a valid world record for Z gauge to an important supplier of accessories for the small scale.

Its has a broad range and offers finely detailed items that Zetties need sooner or later when building their systems.

The previously announced overhead catenary line range was completed a few weeks ago. The contact wires are etched from 0.3 mm thick nickel silver plate and solder very well without soldering flux.

They are available in five different lengths, so they can be used for most curve radii without having to bend them around the curve in a way that does not appear in prototype.

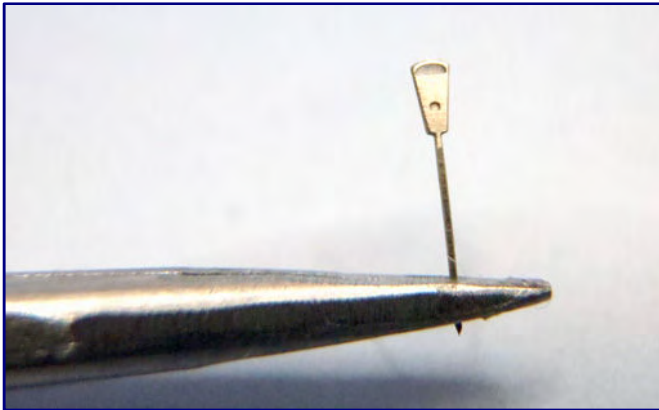
For this there are tower masts with tensioning devices, as well as LED units; also the necessary cross spans are available.



Kohlehunte (Art.-No. B0021; picture above right) and train end panels (B0034; picture below right). Photos: HOS Modellbahntechnik

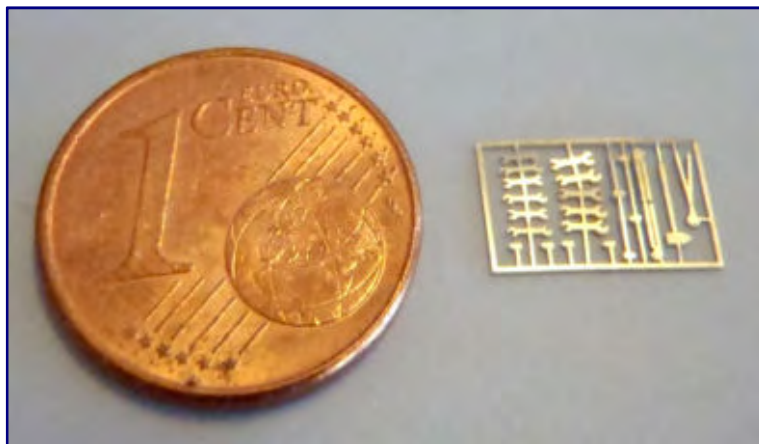
It should only be noted that the fine parts are not intended for an electrical or mechanical function. Pantographs should therefore be fixed just below the contact wire height, which is not even noticeable to the viewer.

And so we now begin the enumeration of the newly announced products for spring 2019: The three-wheeled coal shuttle (Art.-No. B0021) have their prototypes in the railway museum Bochum-Dahlhausen, but could be found in almost all depots in similar form. They did not require travelling on the rails and are made of nickel silver in the model.



The parking meter is still a typical large gravestone (B0040, image left), the tank container (B0045, image on the right) in 20-foot standard size today moves on both rail and road. Photos: HOS Modellbahntechnik

With the new window frames (B0048) the sides of the engine room of a series 221 (Märklin 8820 and others) can be completed or upgraded again. Without the train tail plates (B0034) no freight train should run on the layout, but also many passenger cars without their own tail lights were equipped with them.

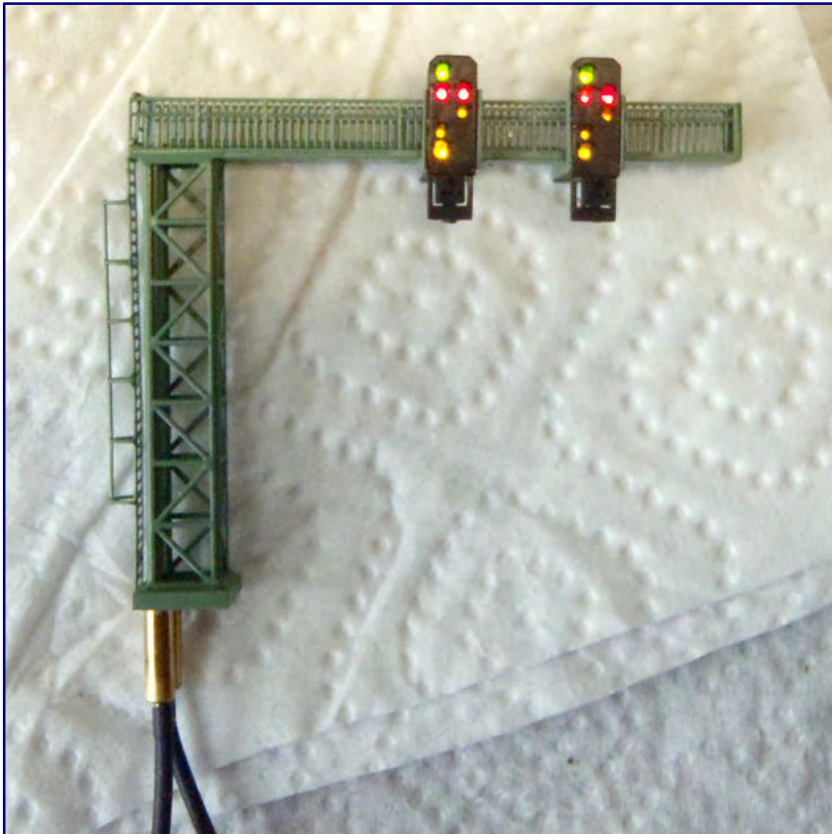


The tool set (B0047) is tiny, as the coin comparison makes clear. Photo: HOS Modellbahntechnik

The conveyor belt for all kinds of bulk goods (B0033) was often used for emergency charcoal removal at the Bw, but still does its job today for plan steam or museum train journeys. The tool set (B0047) of hand tools for the workbench is also an eye-catcher in the depot or in the breakdown service.

HOS Modellbahntechnik manufactures signal bridges (B0021) with lighting technology according to individual customer requirements only. Delivery times are planned and agreed accordingly. The 20-foot tank container (B0045) is available in various versions. Manufacturing and painting are carried out according to customer requirements.

The large cable drums (B0029) are frequently used when laying underground cables and glass fibre pipes. The cover plates made of nickel silver for Märklin's coupling shafts (B0038 / B0039) are practical for model railways, as they are unfortunately often lost or damaged during conversions.



Signal bridges (B0021) are only manufactured to customer order. Photo: HOS Modellbahntechnik

The new parking meters (B0040) made of new silver, which are long gone in the age of parking ticket machines and yet are still well remembered by many as great graves, also close the gap.

The sunshades (B0028) with a size of 16 x 16 mm, which can also be set up in the beer garden, also ensure a festive mood and the joy of eating on the barbecue trolley from our own programme. They are painted according to the customer's wishes.

A little more unusual are the bistro tables, aptly referred to as "Type Dieringhausen", because the prototype can be found there in the Railway Museum and has an old approach signal disc as a table top. The guests take a seat on two sleeve buffers, just like in the original.

The fine street lamps of various designs are not shortened models of size N, but are made to scale and fit precisely.

The range also includes a building luminaire for wall mounting. Depending on the type of lamp, SMD LEDs of types 0201, 0401 or 0601 are used; the light colour can be specified by the customer. The standard light colour is warm white.

www.hos-modellbahntechnik.de

+++ Kastenbahner +++

Surprisingly, this specialist of size N last year agreed to reduce and adapt a DB entry signal to a scale of 1:220. The parts are produced using 3D printing and are supplied as a kit.

Now it goes on, because it is to be inferred from the producer's side that further light signals of the DB construction type from 1969 and also the ÖBB are to follow, beginning from 1980. Detailed information can be found at <https://www.kastenbahner.com/shop/z-ecke-1-220/>.

www.kastenbahner.com

+++ KoMi-Miniaturen (Zpur®) +++

The two filigree specialists from the old land took a creative break. First, already planned and not yet assembled projects are to be processed, before newly announced items are made. What might appear during the course of the year will therefore also depend on customer enquiries received.

www.komi-miniaturen.de

+++ Kreativ3.de +++

New also for gauge Z are building boards "Bienenwabe Schiefer N/Z" from their own production. The individual panels of this honeycomb slate have original dimensions of 25 x 22 cm, so they are quite large for façade designs, while the more frequently used, simple "honeycomb covering" is smaller.

In the model, the slate slabs are slightly aged and therefore do not need to be reworked. Their parallel covering reflects the classically sophisticated cladding of facades, but is not so common for roofing.

www.modellkreationen.de

+++ Krüger Modellbau +++

Still available components of the covered freight wagon type G 10 are now to be offered as a kit (Art. No. Z63030.02). The kit includes a cast brass chassis, the plastic body and a sheet brass roof.



Application example for the building boards "Honeycomb Slate N/Z". Photo: Kreativ3.de

The model of a high track for coaling plants and industrial plants (Z99002.01) comes from the Osnabruck coaling plant. It consists of concrete columns with steel profile edges as well as sheet metal girder bridges, which can be single or double tracked due to the modular construction principle. Each girder is 52 mm long.

www.krueger-modellbau.de

+++ LDT Littfinski Datentechnik +++

After 47 successful years, the owner has sold his company to Buhler Electronic (<http://buehler-electronic.de>), who will continue the digital provider's program.

www.ldt-infocenter.com

+++ Markenburg +++

The Dutch supplier of through-dyed hardboard buildings has four new products in its luggage. The white house from Heistraat 75 with mansard roof (art. no. 02HES07) has been prepared for interior lighting. The same applies to the brick house from Oranjelaan 24 (02ORL01) with attached garage and terrace. The special feature is that different colours can be selected for the entrance door and the garage door when ordering.



The houses "Oranjelaan 24" (Art.-No. 02RL01, left) and "Oranjelaan 43" (02RL07; right) are two of four new products of the Dutch kit specialist. Photos: Markenburg

The building in Oranjelaan 43 (02ORL07), also built in sand yellow bricks, dates from the sixties and is very close by. It also has a garage covered with asphalt, a scullery and a fireplace built in dark stone on one side of the gable.

The blue, mobile toilet cabins (02DBP02) with brown doors are no strangers on the construction site, at events or concerts. This is why Markenburg is now moving into 1:220 scale. With these new products, the range hss now grown to 18 Z-gauge products.

www.markenburg.nl

+++ MBZ Modellbahnzubehör +++

Thomas Oswald has completely redesigned his website, which is very helpful for clarity and orientation. Also the new products can be found on the homepage. The Bw Bebra is currently one of the main topics here.

Here you can buy the boiler house with chimney (Art.-No. 16564) as well as both kit elements separately (16562 / 16563). The "bridge, arch with abutment" (16554) is also available for Z gauge and fits perfectly with the matching extension and supplementary parts in the range.

The self-propelled crane (86290) and the fixed crane with loading gear (86289) had previously appeared in the Conrad range. The Syke pump house (16537), built around 1900 as a brick building with its white windows rounded at the top, is typically North German.

The Almhütte (16552) and the Haus Fontanella (16533) are more at home in Austria. The quadratic splice cabin of the SBB (16541) can also be built without stairs, but is certainly not overlooked, despite its tiny size beside the track.

The current new products are completed by the Rotenfels freight shed (16539) from 1935, and the Baywa Fladungen freight shed (16538) around 1955.

www.mbz-modellbahnzubehoer.de

+++ Micro-Trains (MTL) +++

At MTL, the delivery of the EMD SD40-2 will be continued with two models of the Southern Pacific (Art. No. 970 01 251 / -252). A typical feature of their dark grey colour schemes is the “bloody nose”.



According to manufacturer information, the four-part Auto Train car package (994 01 230), consisting of three viewing carriages and one baggage car in the white-red colour scheme, is already available.

A new car series with the motto “Farm to table” will be launched by this supplier with an older 40-foot refrigerated car. The wooden side walls are painted grey and decorated with a “Monarch Finer Foods” motif (518 00 710). The model was on the road like this in the thirties.



The current Heinz series is continued by the sixth refrigerator car (518 00680), which is painted yellow on the side walls and has a different design on both sides. It advertises baked beans and spaghetti on the opposite side.

The refrigerated car “Monarch Finer Foods” (Art.-No. 518 00 710; picture above) is the foundation of a new series of standard cars of the New York Central (500 00 056; picture below) which adds colour to the system. Photos: Micro-Trains

In the usual brown the two 40-foot-cars of the covered standard box car design are shown, on the way for the Union Pacific (503 00 191 / -192). In addition to the large owner's lettering, they stand out above all with the three-coloured railway logo and the slogan “We can handle it”. The

coaches built in October 1957 were used in this design up to the late seventies after removal of the roof walkways.

For the New York Central, the two cars of the same design (500 00 056 / -57) were equipped with roof walkways whose black paintwork on the side walls was broken open by green surfaces. These are emblazoned with the large New York Central System logos. The prototypes were built in 1956 and were thus among the last of their kind before the railways switched to larger-volume designs.

www.micro-trains.com

+++ Modellbahn-Decals Andreas Nothaft +++

Andreas Nothaft has picked out some of the 300 new sets of addresses and highlighted them in particular because they are particularly topical at the moment. In principle, they can also be scaled and ordered for Z gauge. A suitable basic model is, of course, a prerequisite.

At this point we would like to draw your attention to the Doornkaat advertisement (Art.-No. 5936) for trams, which used to be seen on many buses and therefore also has a very special appeal on a scale of 1:220. Two further versions are also available (5937 / 5938). A suitable bus model could be the standard bus, as it can be found as a Mercedes-Benz bus from NoBa models.

The provider also has over 200 christened names for ICE multiple units, such as "Mainz" (21037) here with city coats of arms for epochs V and VI, in its range. These should include ones suitable for Märklin's models of the 406 series.

www.modellbahndecals.de

+++ Modellbau Laffont +++

Since Stefan Laffont is also active as a contract manufacturer, there is little time for his own innovations. However, the year should not go completely without them. It is planned to transfer the outbuilding known from 1 gauge in brick (art. no. Z1601) and plaster (Z1501) to scale 1:220. There, it can be used as an office, workshop or gatehouse.

The Zetties can also expect car garages (Z1901) from the fifties and sixties, when the doors were still opened to the side. Here even the small residents of the complex can well protect their dearest child from the weather.

www.modellbau-laffont.de



The brick toilet building (art.-no. Z1601; picture above) appears in parallel also as a plaster execution, the garages (Z1901; picture below) have the earlier usual side opening doors. Photos: Modellbau Laffont



Maßstabsgetreuer Coca-Cola-Automat für den Bahnhof (Art.-Nr. 2018-5). Foto: Modellland

+++ Modellland +++

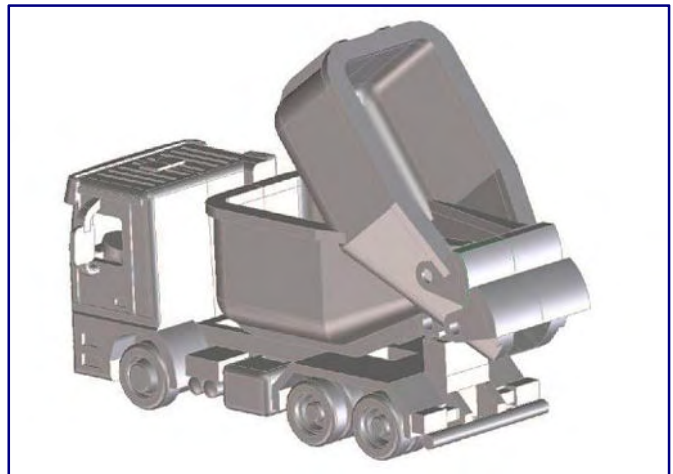
In addition to the vending machines already on offer for tickets and beverages as well as lockers, a classic Coca-Cola vending machine from Era III (Art. No. 2018-5) has now been added. Its role model will certainly be familiar and known to many model railroaders.

www.modellland.de

+++ NoBa-Modelle +++

A first 3D print housing for Rokuhan's shorty chassis is dedicated to the Austrian Rh 2095 diesel locomotive, which is available either as a colour-printed blank (5001) or hand-painted (8100).

Their site now also feature other locomotive housings, including one for the DB V 188 double locomotive for mounting on Rokuhan shorts and one for the powerful V 320, which was introduced by Henschel in 1962.



Shorty housing modelled on the Austrian Rh 2095 diesel locomotive (art. no. 5001; left photo) and CAD representation of the asphalt container on an Actros 6 x 4 (6335; right photo). Photo / Illustration: NoBa-Modelle

The Mercedes-Benz O 305 short-distance bus now stands at the bus stop (6324) with the doors open and can also be ordered in four printing colours. The blue Unimog U 1300 L (6219) with flatbed and tarpaulin makes the start for THW vehicles.



The local bus (6324) is now also available for use at the bus stop with the doors open. Photo: NoBa-Modelle

The asphalt container on an Actros 6 x 4 (6335) or Scania 8 x 4 (6336), the asphalt paver (6507) and the yellow road roller (6506) are intended for the design of construction sites. The roller is also available in other colour combinations. Water sports are not neglected with the two surfers on boards (8053 / 10475R).

www.noba-modelle.de

+++ Otto-Scrap-Design +++

How fine and fascinating even ordinary scrap can look is proven again and again by

this supplier, who sells his creations individually and quasi calls for the creation of something individual from his own mixtures.

New in the program are plate pieces in the format of about 2 x 6 mm (Art.-No. 076), iron rods (078) with 0.1 mm material thickness and 15 - 20 mm length, as well as 0.5 mm diameter (079) at 65 mm length. They are also much shorter with a length of only 18 to 30 mm (080). Bent (081) they look particularly good.

The manufacturer offers 1.0 mm thick iron bars (083) with a length of 30 - 35 mm, while the pieces (084) only have a length of 5 - 10 mm. Bundled (086) they are usable up to 35 mm in length, also in the small scale. Another bundle of about the same length (088) looks much less rusty.

The very fine iron rods (089) seem to be perfect for the Z gauge, as they have a diameter of only 0.1 mm and remain very "handy" with a length of 5 - 10 mm. The bundled tubes have lengths of 50 (054A) and 30 mm (054B). The only 40 mm short but very thick iron pipes (064B), whose bundles make them 7 mm wide, look like thick steel pipes on a scale of 1:220.

www.otto-scrap-design.de

+++ Passmann +++

Due to an irreparable database defect, electronic sales have to be completely rebuilt. This process has not yet been completed, and the provider is appealing for understanding that existing customers will unfortunately also have to register and re-enter their data in this context.

www.passmann.com

+++ Permo Perleberger Modellbauartikel +++

On the pages of Permo we have found a wooden horse-drawn sleigh for Z gauge, which was previously unknown to us. Unfortunately, we have not yet received any further data from him, as we had requested.

www.permo-miniaturen.de

+++ RATIMO Rainer-Tielke-Modellbau +++

On request Rainer Tielke always offers new projects, but new products from the milling machine for sale to everyone are currently not planned. New, however, is an extension of the offer to 3D printing, which includes an own design as well as the output on an own device. 4 The manufactured models are made of resin.

As first original construction a forklift with the dimensions 29.92 x 6.33 x 11.76 (L x W x H) was recently developed. The photos show it still fresh from the printer; the black printed parts have already received only a little colour. The fork is supplied separately and can be glued at any height by the customer. This model can be ordered beginning in May 2019.



Provisionally coloured, the first samples of the new forklift trucks with forks that can be glued on at any height are shown here. Photo: Ratimo Rainer-Tielke-Modellbau

The construction of an attachment for platform trucks is also complete, which allow the vehicle to mutate into a lift truck. Further designs are also in progress, but will only be announced here after completion.

www.rainer-tielke-modellbau.com

+++ Schrax +++

Winter still has the layout fully under control with the 3D-printed snowmen from Schrax. The miniatures for Z gauge are, as usual, produced in 3D printing and are the equivalent of 1.50 metres tall.



With the snowmen (left), winter returns to the layout. The tractor tyres (right) help to harvest the grain on the farm - they must not be too big. Photos: Schrax

Also new are tractor tyres in various sizes. Those with the designations S and XS are also suitable for harvesters, large tractors and ordinary tractors with Z gauge.

They sometimes lie on the fields to mark obstacles such as hydrants and prevent damage to agricultural machinery. But it is not uncommon for a tire to be used as a flower pot or a sandbox.

Track ballast and snow granules are now also available in larger containers and are then delivered in a practical PET wide-neck bottle.

www.schrax.com

+++ SMZ Sondermodelle Z +++

After the one-piece Regio-Shuttle RS 1 from Stadler apparently found great approval with the Zetties, SMZ announced further variants of this diesel railcar, which is widely used in Germany.

The specialists are now planning the versions for the Hohenzollerische Landesbahn (HzL), the Bodensee-Oberschwaben-Bahn (BOB), the Erfurter Bahn and the Schönbuchbahn with the same equipment.

www.sondermodellez.at



Regio-Shuttle RS 1 in the versions of the HzL (photo above) and the Erfurter Bahn (photo below). Photos: Sondermodelle Z

+++ Spur Z Ladegut Josephine Küpper +++

The most recent innovations in the Aachen programme are lignite loads for Märklin's Fads-Wagons (8630 and others) and the ore lld wagons (82800 and others).

www.spurzladegut.de

+++ Stillertec +++

Christian Stiller has just entered the Z gauge market and produces individual trees in 1:220 scale, which can be ordered in quantities of 5 or more. Since they are drilled or soldered from different wires, each tree is unique.



The photo shows a selection of the trees described in the text of the new supplier of handmade accessories. Photo: Stillertec

The blanks are primed and then planted with different methods in four to seven operations. Finally, they are preserved with a fixative.

The following are currently available for track Z: willow (40 - 55 mm), birch (50 - 70 mm), winter birch (60 - 70 mm), weeping willow (40 - 55 mm), pine (60 - 70 mm), spruce (60 - 80 mm; 110 mm), fir (60 - 80 mm; 110 mm) and larch (60 - 80 mm). Other new products include poplars in the preparation stage.

www.stillertec.com

+++ Tams Elektronik +++

The new Helios shuttle train control system, named after the Greek sun god, can control up to four different trains on an analogue shuttle train line with both single, or double-track terminal stations, and an (optional) intermediate station as a version for analogue DC systems.

It also ensures that each train travels at its individual maximum speed, that it starts and brakes with acceleration or deceleration appropriate for its locomotive, and that it stops at the terminal and intermediate stations for a predetermined period.

The KSM-4 reversing loop module for digital DC systems not only anticipates the correct polarity within a reversing loop, but also takes care of the correct setting of the turnout. If you like to do it yourself, you can do it with the help of buttons that are connected to the module. Alternatively, this can be transmitted to the address of the integrated turnout decoder via DCC accessory decoder commands.

Between the inside of the reversing loop and the turnout, short transition areas have to be set up in order to be able to use the functions and to work without short circuits. However, this module is not suitable for turntables for which a separate product is offered.



The 24th version of the light control module listens to the designation LC-24 and switches five outputs with light emitting diodes, small lamps or successive circuits. However, it can also be cascaded without restriction and thus offers the possibility of switching lighting for entire model railways or parts thereof on and off with a time delay using just one switch.

For this purpose, further LC modules can be connected to one or more outputs, which are to be switched with a delay.

www.tams-online.de



Barbecuing can become a sweaty affair (photo above), as this figure new item proves. The tow truck (photo below) is only an example of an older model. Photos: Trafofuchs

+++ Trafofuchs +++

Among the new features of the Trafofuchs are a greenhouse and a bus shelter. The kiosk, previously made of resin, was also replaced by a milled part for model railway layouts.

When it gets warmer again outside, the barbecue season starts, and then the hour rings for the figure pack "Am Grill". Sausages and meat on the small kettle grill are not ready yet, as the hungry observer unfortunately has to notice quickly.

The grill master first has to wipe the sweat off his forehead, but his wife is already beside him with the plate.

In the car model segment, an older tow truck appears, as can still be found at some scrap dealers today. New are freshly constructed platform benches, for which we are not yet able to supply photos.

www.trafofuchs.de

+++ Velmo +++

Shortly before the editorial deadline the decoder LDS170821 for Märklin's Rh 2048 of the ÖBB (V 100 at the DB) had been finished. It supplies the new models that have already been converted to the bell-shaped armature engine. The locomotive mentioned is the first, and so far the only one, with this feature.

It offers the same functions as its predecessor, the LDS216286 for the V-100 models with 5-pole motor.



Thanks to Claudius Veit, the Rh 2048 / V 100 converted to a bell-shaped armature motor is now also digitally on its way in Z-gauge.
Photo:

A must in the Velmo range is a decoder exchange board for the new E 41 delivered by Märklin. After first inspection there is enough space for the digital components, which is why Claudius Veit immediately started its development. We shall report on how this is progressing, and further advise our readers.

www.velmo.de

+++ WDW Full Throttle +++

William D. Wright has notified us of two new wagon packs. For the Great Northern, the cylindrical bulk wagons (Art. No. FT-1056) have a shiny metallic appearance, with the well-known GN round logo.

The modernised, 34-foot-long refrigerated wagons with wooden side walls also have a white aluminium-coloured exterior and a black roof (FT/B-9013). On the way is this wagon for the Pennsylvania. As this is an exclusive series, this model is only available in the United States at Z Scale Monster Trains.

www.wdwfullthrottle.com

+++ Werkzeuge Peter Post +++

In addition to fine brushes, the range also includes a ten-part pigment powder composition (light beige, ochre, rust orange, rust red, rust brown, grey, green, medium brown, dark brown and black) for ageing vehicles and buildings. The solvent-free acrylic rust paint in light or dark tones, on the other hand, is intended for painting rail sides.

Among the tools, the classic range from Duderstadt, the precision knife with knurled screw for a tool-free blade change is to be mentioned above all. The handle of the knife is rubberized, so it fits very well in the hand and is fatigue-free. Replacement blades are available in packs of ten.

The new digital calliper gauge has a tolerance of 0.01 mm or 0.005". Incorporated together in the shearing pliers are a cable cutter and pliers in a single tool. Their cutting performance reaches up to 1.6 mm thick copper.

www.peter-post-werkzeuge.de

+++ Westmodel +++

At Scholz Modelleisenbahnen (Heidenheim), preparations are underway for a re-issue of the tender steam locomotive class 93⁰⁻⁴ of Prussian origin (former T 14). It will only be offered as a finished model (Art.-No. 30048 FM). Their housings have already been cast.



The tender steam locomotive class 93⁰⁻⁴ / Prussian T 14 is to be re-released by Westmodel, but the delivery date is still unknown.

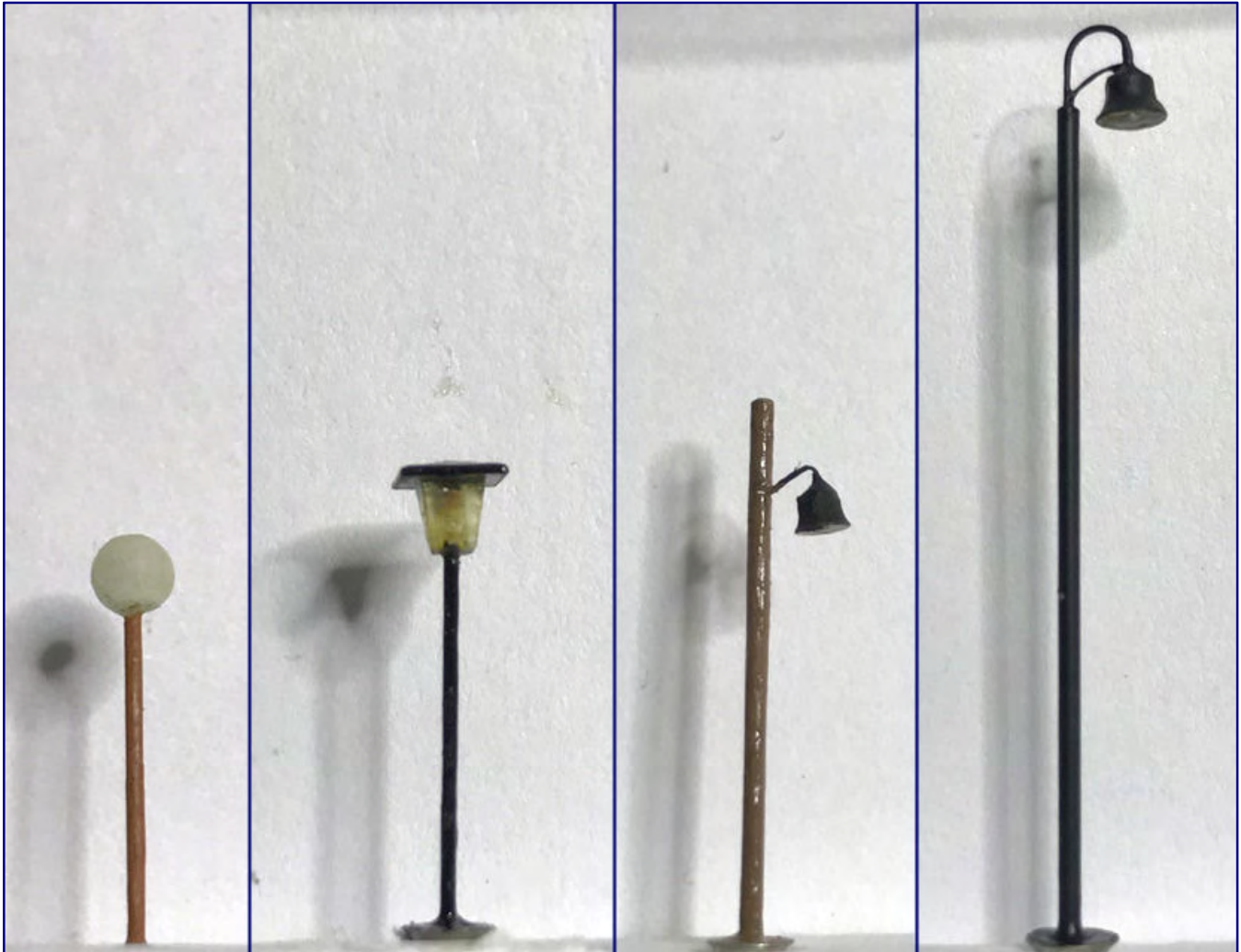
A delivery date is not yet foreseeable or even planned, because the ordered chassis has not yet been delivered by Märklin. Due to their conversion to a bell-shaped armature motor, it is not yet possible to estimate whether and which modifications to the locomotive housing may still be required.

Available again are the brass conversion kits for the 185 (40034) class high-speed steam locomotive, as was introduced in the December issue. This set of parts (40034) consists of a cast brass cab, which is glued on after the Märklin pointed cab has been cut off, and a coal box elevation, as it was once mounted on the tender by the Reichsbahn.

www.modscholz.de/Z-Serien/z-serien.html

+++ Z Norm (Stefan Schmid) +++

Stefan Schmid has also come up with something new for this year and presents four lanterns. The highest of them, 34 mm, is a nostalgic street lamp that captivates with its elegance. In the surroundings of rural railway stations, lanterns with wooden masts were frequently found, as he reproduced them 21 mm high.



The four lantern new products in size comparison (from left to right): Park lamp, sidewalk lamp, lantern with wooden mast and nostalgic street lamp. Photos: Z Norm

The sidewalk of the facilities can be illuminated by the sidewalk luminaire, whose glow at 16 mm illuminates the pedestrian area well. The park light, on the other hand, is at home in green cultivated areas and, with a height of 12 mm, is also intended for paths without motorised traffic.

All lights from Z Norm are maintenance-free, using light-emitting diodes, and are produced by hand.

[z-norm\(at\)web.de](mailto:z-norm(at)web.de)

+++ Z-Panzer (Andrew Hart) +++

The train with the railway gun K 5 is now complete. The accompanying coaches are from the Märklin range and were given camouflage paint as well as some new exterior walls or superstructures. Andrew Hart used the WR 360 C 14, better known as the later V 36, as the locomotive for the train.

Z-Panzer recognized a suitable version in the DR variant (Märklin art. no. 88771) with the eye-catching explosion protection box on the hood. It was also colour matched to the gun train.

www.z-panzer.com



Switch bushing for positioning wire (Art.-No. noba - 2001). Photo: Z-Railways

+++ Z-Railways +++

The new switch bushing for positioning wire (Art.-No. noba - 2001) forms a bushing with a slot for the passage of a positioning wire with a maximum diameter of 0.5 mm. Through the slot of the part, there is a clean guide from the drive under the layout to the adjusting threshold of the turnout. Even cleaning is easier, as the mechanics are already partially protected from sticky impurities. Travel is allowed up to 5 mm.

www.z-railways.de

+++ 1zu220-Shop +++

In addition to the three Archistories new products (see there), which are produced exclusively for the 1zu220 shop, two other items are in development or pre-planning. The Magirus-Deutz 230 D 22 AK 6 x 6 dump truck is already available as a handmade sample. The angular hood from Ulm (built in 1967) could once be seen on almost all construction sites or in gravel works.

The 1zu220 shop plans the striking and once most powerful vehicle of its series in attractive cobalt blue with red chassis. It will be distributed exclusively and free of charge only to customers who order the Märklin Insider annual model there.

Still without a fixed time plan, a fire engine house with a fire engine is being developed.

The latter has already been produced and many Zetties may have seen it last year in Altenbeken. Individually and optionally available will be a siren electronic component, which can be purchased from Noch for the fire engine house kit.



As a supplement to the Insider Club model, the 1zu220 shop would like to offer its new product in the form of the Magirus-Deutz 230 D 22 AK 6 x 6 angular hood (bonnet). Photo: 1zu220-Shop

www.1zu220-shop.de

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Practical benefits donated:

I look forward to the latest issue of Trainini every month. The issues help me to solve problems again and again or give me suggestions like the "Digital Model Railway Control" in the Trainini issues 2018.

The article series by Andreas Hagendorf came at the right time for me. I own a collection of Märklin 1 Gauge Maximi models, as well as in H0 scale. It is a mixture of digital (delta) locomotives.

I partly started to convert some H0 locomotives to digital drive. In addition to an already existing Märklin Mobile Station 2 another one was added by a 1 Gauge offer. Being able to control some models a little more comfortably moved me to buy a Central Station 3.

So far I hadn't thought about digitizing my Z-gauge locomotives. But the appetite came while reading. Because if you pay attention to the subtleties, e.g. suitable power supply (switching power supply 66111 from 67013) for Mobile Station 2, nothing stands in the way of digitization or a partial digitization. I am looking forward to installing a decoder in one or two locomotives in the near future.

Thank you very much for your great and voluntary work in the service of the Z gauge.

Hand Helbach, Bonn

Obituary for a deserved and committed model railway woman:

Siglinde Dinkelacker, born on 6 February 1957, died two days before her birthday on 4 February 2019 during a voyage aboard the MS Amadea. For more than 12 years, she was Moba's regional representative for Baden-Württemberg; since 2006 she had shared this office with Roland Scheller in the greater Stuttgart area.

Her activities at the Modellbahnverband in Deutschland e.V. consisted of looking after the information desk at the stand, together with her husband Siegfried. She was always critical, but always just as constructive and very committed, when it came to looking after members and exhibitors at trade fairs.

Also the "Forum" (association magazine) carried with her a clearly recognizable handwriting as author and proofreader as well as with the organization. This is no wonder, as she was also active from the very beginning at IG Spur Z, from which the Z-Dialog soon emerged, a magazine that she produced with just as much passion together with her husband Siegfried over many years.

In the years before, Sigi, as she was called, actively participated in trade fairs to present her modular system. Together with her husband, she was also happy to visit railway museums. With her, Z gauge loses a personality that was as committed as she was impressive.



Siglinde Dinkelacker †. Foto: Frank Zarges

With recognition and gratitude we now say goodbye to her. We mourn for an extremely deserved ambassador of the model railway, whose memory we would like to keep and honour.

Jan Tappenbeck reports from the exhibition in Bad Schwartau:

On 19 and 20 January 2019 it was time again and numerous exhibitors and dealers from all over Germany accepted the invitation of the Eisenbahnfreunde Bad Schwartau e.V. (Bad Schwartau Railway Friends) (EfS) to the model railway exhibition in the North of Germany.

The Krummlandhalle was again dedicated to the Z gauge, while in the other two halls model railroaders of gauges 0, H0, H0e and N showed their results of long handicraft evenings. Also here there were a lot worth seeing, but at this point we concentrate on Z gauge.



Rainer Tielke presented his first module under construction with the Hamburg-Dammtor station. The first trucks have already driven on it; trains and trams are still to do so. Photo: Torsten Schubert

The Krummlandhalle was of course the first point of contact for the friends of Z gauge. Here the 2nd northern Z Convention took place during the model railway exhibition. There everything that has rank and name in this community met, from the exhibitors to the visitors. Guests from München and Göppingen had the longest commutes.

Manfred Forst travelled from Hesse with his “messes,” and Thomas Hviid Thorn from Denmark showed his suitcase system. The concept of 2017, which also includes the renunciation of repetitions, has proven its worth.



Trafofuchs' Birgit Foken-Brock demonstrated her skills at the exhibition and offered figures from her wide range. Photo: Torsten Schubert

The facilities with a regional reference met with particular approval: Ronald Schulz from Mecklenburg-Western Pomerania was represented here by the "Klütz" facility, which focussed on Era I.

The more than 2,046 visitors (not including children under six years of age) on the two days of the event enjoyed everything that had been faithfully created in many hours and it was basically irrelevant to them whether this had been realised in the large 0 scale or Z scale. For the small guests the numerous facilities were interesting where one could drive very precisely or one could for once be a locomotive driver on longer routes.

As at the 2017 event there were two special wagons in Z scale at this exhibition, as well. Harald Freudenreich again presented a wagon with the logo of the Schwartauer Werke, this time as a container wagon. Our club colleague Otti made contact with the traditional Lübeck company LMG and had a Märklin sliding wall wagon printed.

For all those who could not attend personally, the photos shown here may give a brief impression. We hope to see you in January 2021 for a warm "Moin Moin" near Waterkant.



Ronald Schulz had taken over the Klütz plant from Hubert Halbey and continued developing it. He converted or completely rebuilt vehicles to match it. The results could be seen in Bad Schwartau. Two photos from the company should provide an example of his impressive work. Photos: Thomas Wycislo

Märklin deliveries at the beginning of the year:

Deliveries by Märklin shortly after the Christmas business are manageable. But the rule of thumb is: class instead of quantity! The first verdict of the Insider Club members on the steel blue standard locomotive E 41 012 (Art.-No. 88353) was unanimously positive.



The new E 41 series from Märklin (art. no. 88353) shows many details that were first implemented in this form and fineness on a Z scale model. We will show this in detail in the next issue. Photo: Jörg Erkel

The now delivered new model shows excellent driving characteristics, captivated by white-red light changes at the ends and has many fine details that are achieved with separately attached parts, so the unanimous positive judgement. We will take a closer look at this model and report on it in March.

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German Magazine for Z Gauge



The electric locomotive of the series E 41

Mechanical signal box
Church in neo-Gothic style

Introduction

Dear Readers,

Spring has finally arrived and with it also the time for spring exhibitions and fairs. In this edition we therefore briefly report on the Lahnsteiner Model Railway Days, where scale Z has long since taken on a fixed role.

Regular visitors to the exhibition rave about the high standard of the exhibits on display and repeatedly confirm to the organising association that it holds one of the best events of its kind in Germany every year.

In the next two editions we will follow our reports about the Faszination Modellbahn (Model Railways Fascination Exhibition) and the upcoming Intermodellbau exhibition, because at both fairs scale Z plays an important role again.

But the focus in March belongs to another topic: Märklin has recently delivered its E 41 012 for Insider Club members. Many more versions for the other customers will surely follow soon. This is reason enough to dedicate ourselves in detail to the history of the great icon and to measure, test and examine the miniature in detail.

Märklin did a lot different with this model than we are used to, and so the question of course also arises: Was this a good decision and will it become the standard in the future? In this context, we try to answer many of the questions that customers were asking themselves when they held their model in their hands for the first time. There are some noticeable features on the outside, but those who take off the shell are even more amazed.

We have also found a suitable book tip for you if you would like to dig even deeper into development, technical features and operating history. However, the second book presentation is also very interesting, because we would like to place it even as an insider tip.

Because model railroading does not only take place on the tracks, we also dedicate ourselves to the accessories. The church of Archistories has just been delivered and is now exclusively distributed by the 1zu220-Shop. We looked at this kit also. The sacral building has much more to offer than we had initially assumed.

But what would our hobby be without some handicraft work? It's not too warm yet to put the work tools aside! Our reader Wilfried Pflugbeil was creative and closed an existing program gap in his own way.

The roll supports for the mechanical signal boxes that he manufactured can be copied by anyone. That's why he wants to pass on his ideas to all other readers today. After all, all too often it's just a question of know-how, isn't it?

Our news in March are marked by the first deliveries of new products and supplementary information on the announcements we have received in the meantime. Enough written: My editorial colleagues and I hope you enjoy reading this issue.

Sin-Z-erly,

Holger Späing



Holger Späing
Editor-in-chief

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We thank the Petkelis family, Eisenbahnstiftung and Roland Hertwig for their photo support.

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Cover photo:

At the beginning of the sixties E 41 012 still wears her chic blue coloured finish. With an express train on the coupler, it crosses a steel girder bridge on the single-track main line, from which our view falls to the road in the valley.

Archistories' neo-Gothic church

For whom the hour strikes...

As an extension of their "Kallental" and "Dörpede" series of laser-cut kits, Archistories, together with the 1zu220-Shop, have jointly launched the neo-Gothic church "St. John Paul II", a sacral building in a typical 19th century style, but with a patron saint of the late 20th century. Our editor Dirk Kuhlmann immediately thought about its many potential modelling uses, once he came across this kit.

By Dirk Kuhlmann. Believe me, I too was amongst those who were quite impressed by the first photos of the Archistories Church, when we got a glimpse of them at our editorial office in the early autumn of last year.



Noble and full of pride, Archistories presents the neo-Gothic church "St. John Paul II" (Art. no. 404181) on their own promotional images. We agree that they have every reason to be proud of this product. Photo: Archistories

Of course our attention was directly drawn to the colourful windows and their intricate tracery. By the way, tracery is an architectural term and describes the delicate stonemasonry of windows and balustrades which were representative of the high skills of the masons at that time.

Shortly afterwards, I received a parcel with a finished specimen of the church. A close inspection of the model only confirmed its quality and coherence conveyed earlier through photos. Moreover, the structure is very sturdy and likely to survive a fall from a certain height, although I did not try my luck at testing this.



The church will cut a fine figure on any layout and can be easily integrated into either a rural or urban setting.

The model is kept in the neo-Gothic style, a historical art and architecture style of the 19th century. The neo-Gothic style is an undercurrent of historicism and draws from typical Gothic features such as lancet windows, intricate tracery and buttresses for purely decorative purposes and with the aim to create a sacral effect that corresponded to the aesthetics of the period.



The original medieval Gothic style tried to dissolve wall surfaces through the creation of huge windows. This required the use of buttresses in order to support the load of the structure. Examples of the Gothic style include the cathedrals in Reims and Cologne.

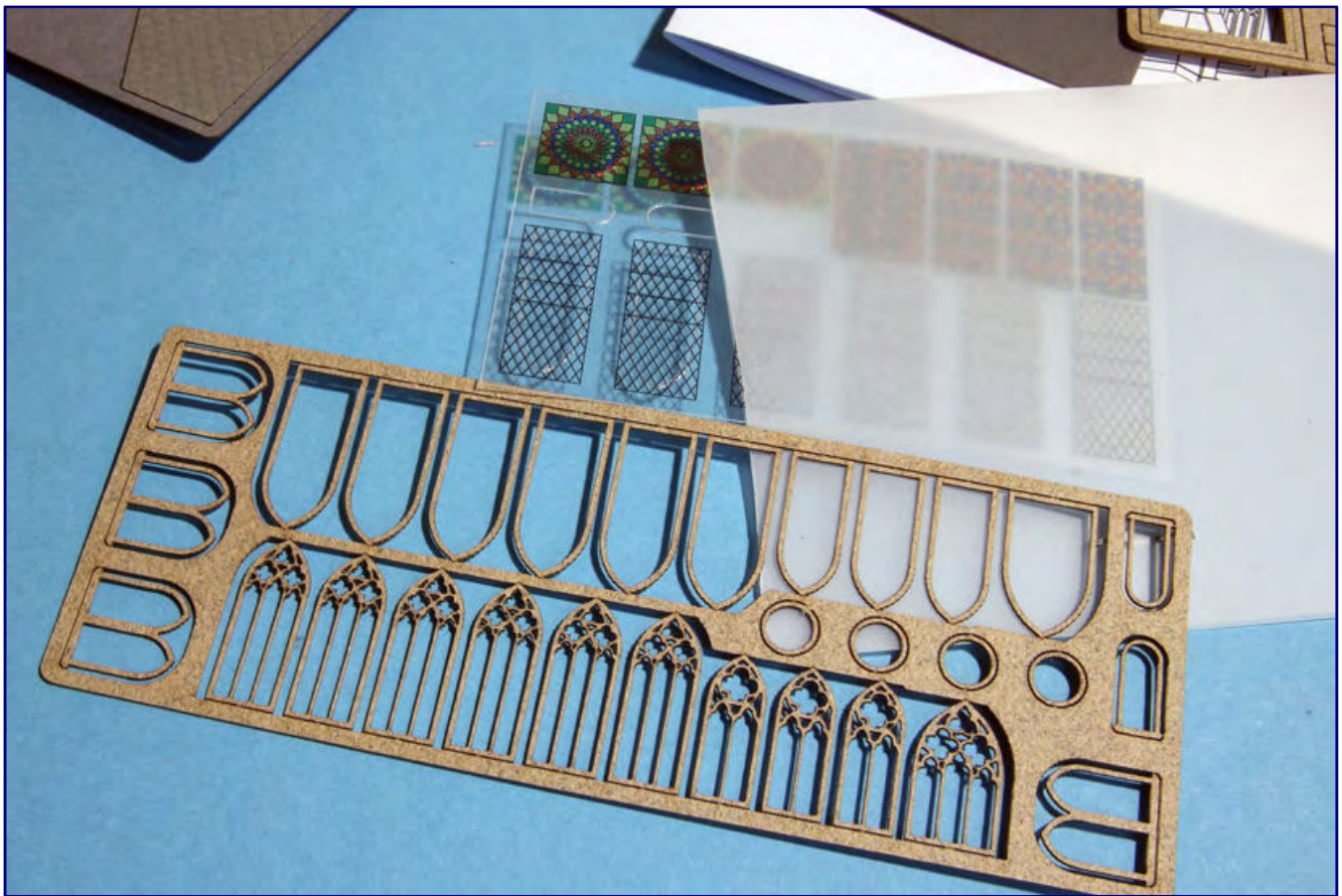
This neo-Gothic model, on the other hand, comes with massive looking walls in combination with gothic window forms. It represents thus a typical line of the neo-Gothic revival period and its frequent use of characteristic set pieces.

Left:
The kit includes all pre-cut pieces from high quality coloured rigid cardboard and printed window sheets.

These types of churches are quite frequent and can be used without problem for modelling both urban and rural areas. You will need to reserve, however, enough open space around this model if you want to bring its characteristics to full effect on your layout. As always, I do recommend to prepare for your project by first having a look at prototype situations.

The wide range of layout and diorama possibilities of the "St. John Paul II" kit (Art. no. 404181) are illustrated by the first photos of the finished prototype model, which also led to the picture on the packaging of the kit (see page 5). In any case, the model fits seamlessly into the "Kallental" and "Dörpede" series of Archistories kits. That was not enough for us, however, but more about this later.

Some time after having had the chance to inspect the finished version of the model, I also received the first kit. Knowing Frank Drees from Archistories, I was already sure that all parts would fit easily and be of high quality. A first visual inspection on my photography table confirmed this assumption.

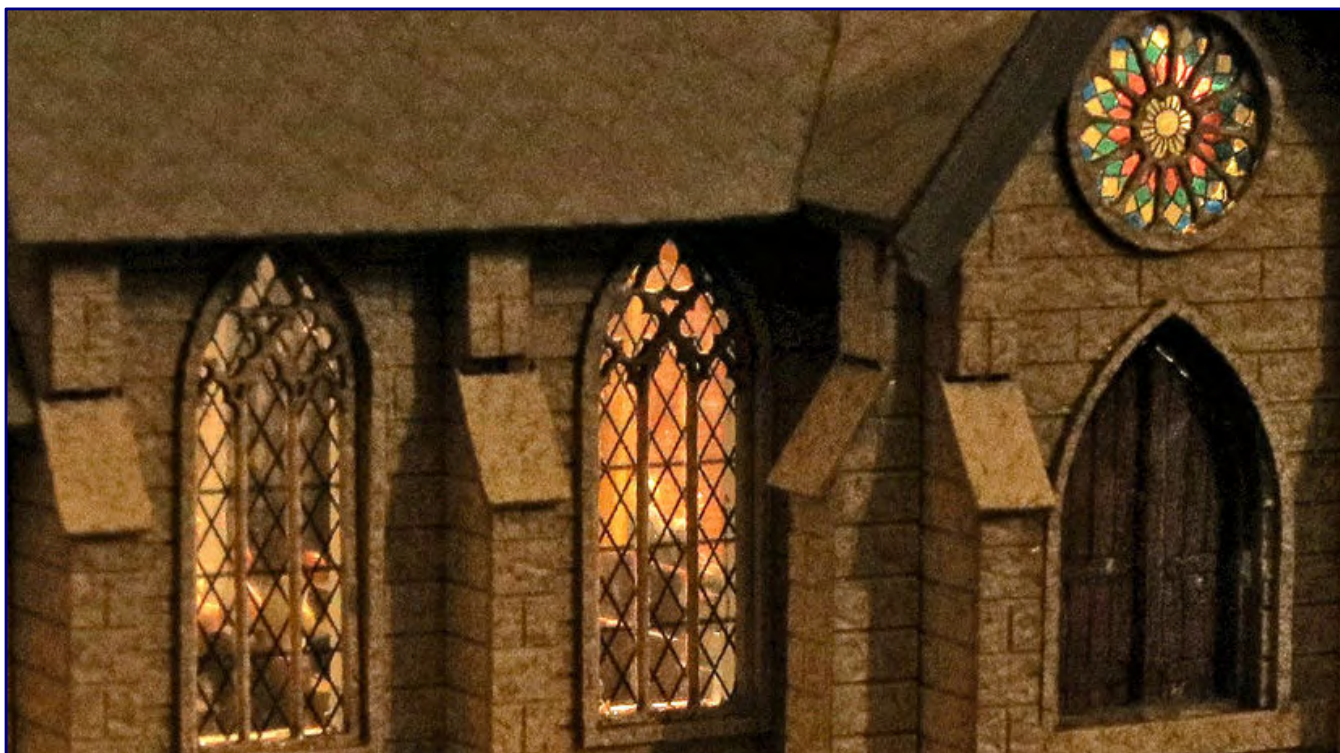


The intricate tracery and printed colour window panes are amongst the highlights of this new model.

The quality of the laser cut details is outstanding, as can be seen from the sheet with the very finely and precisely cut tracery windows. The instructions are kept in the usual way: self-explanatory and clear. A little tip for the built: I always tear out the page with the parts inventory and place it next to the page with the current stage of assembly in the instructions.

My initial intention was to show you, as usual, the construction of the church in pictures, but we have done so multiple times with similar kits in previous editions of **Trainini®** in the past. Just to say that the built is straightforward, but with lots of small parts which will give you plenty of modelling fun.

As was to be expected, I could not resist the temptation of adding some scratch built interiors to the model. Benches, the altar and the cross were made from Evergreen polystyrene profiles and painted accordingly.



A suitable interior adds value to the model (top; photo: Daniel Bittner). A few churchgoers sitting on the benches add to the realistic effect (bottom).

Some model figures and a slightly yellowish light source complete the scenery in the interior. For those of you who do not want to embark on this rather time consuming work, I can recommend the dedicated 3D-printed interiors from Noba-Modelle (Art. no. 10810).

Finally, I had the finished church standing in front of me. And as expected: everything fit without problems and no missing parts. That's how I like it! The only tiny detail which I did not use was the coat of arms with the slogan "Totus Tuus" referring to the church's patron saint Pope John Paul II.

My reason for omitting this detail was that this model be located in very different regions. Giving your church a different name with some personal meaning to you will add a unique touch to your layout.



Archistories' new church fits well with the style of their "Dörpede" and "Kallental" series of German regional buildings. Its potential uses go far beyond that, however, and also include other European countries and even the east coast of the United States. The potential for use in multiple settings by adding just a few minor tweaks to the model is illustrated by the early 20th century postcard imitation (page 9).

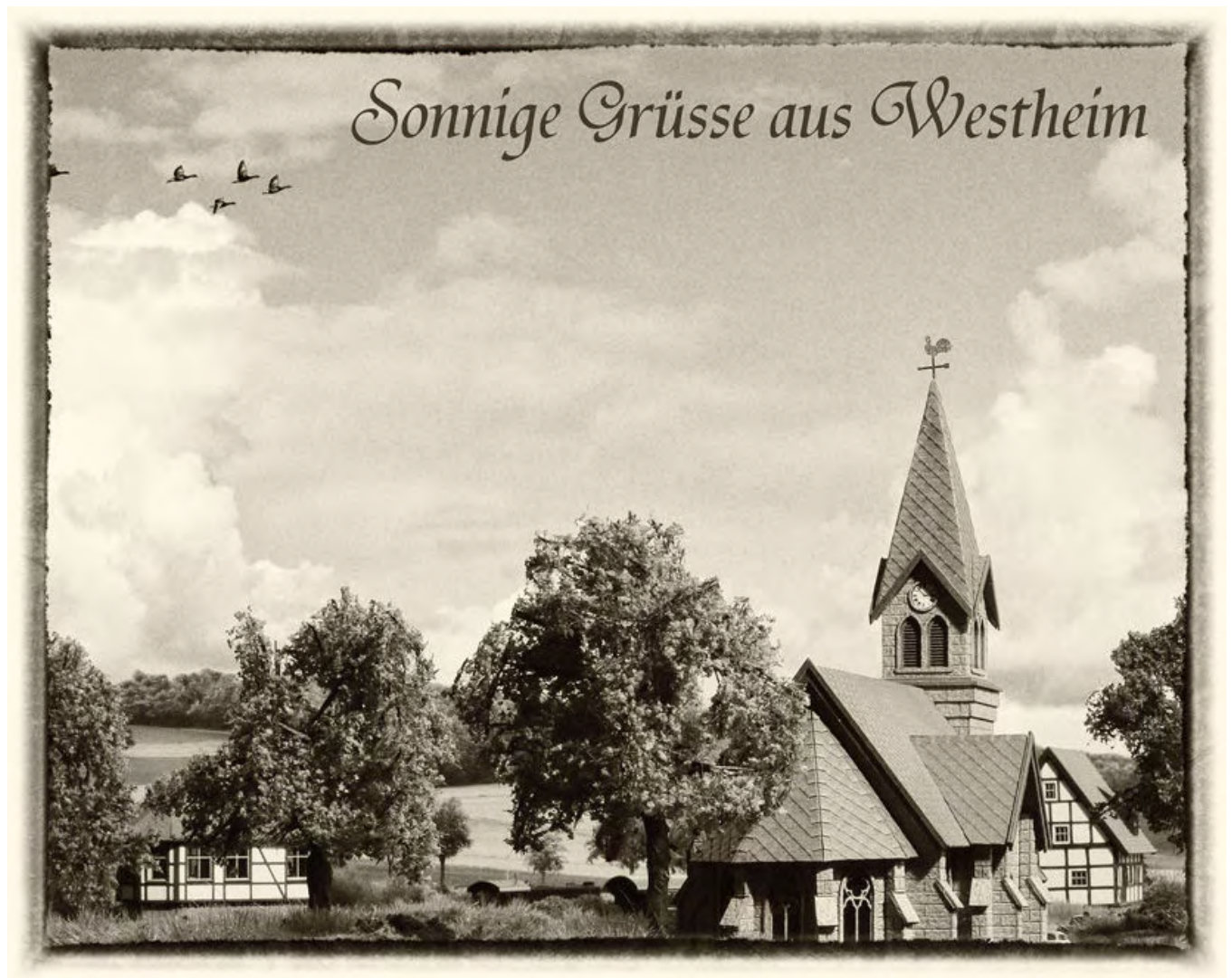
I always call this the "added value" of a kit. Whilst the prototype of the "St. John Paul II" church stems from the Sauerland region in Germany, model railway enthusiasts with a vision and a penchant for a fine, well-designed layout, will take the additional step of adapting their kits to the story they want to tell.

Of course, the Internet helped me with my research and I was amazed at the number of places where similar churches can still be found. Unfortunately, for copyright reasons, we are not allowed to include photos here, but I can tell you that I found examples in many European countries and even some east coast states of the US. Anyone with a certain degree of modelling skills will be able to modify this kit and adjust it to their concrete needs.

There exist also some neo-Gothic churches without a bell tower, something which could be easily kitbashed with a sharp craft knife - another advantage of laser-cut cardboard kits. And since we are at it and do have, as Z-scale modellers, a certain reputation for being wacky extreme hobbyists, the design of our own coat of arms should not be too difficult and would make for the cherry on the cake. Some might even be tempted to discuss this with a goldsmith, but let's not get overboard.

Honour to whom honour is due
We nominate Archistories' church kit "St. John Paul II" (Art. no. 404181) for our 2019 awards for best new products in the accessories category..

My church will be surrounded by old trees and stand next to a small creek. A cemetery with old tombstones, completes the village or small-town setting. And if you want to further add to the scenery, you will find a wide variety of figures and accessories at Trafofuchs and Klingenhöfer.



Still not enough? Well, add some sound to your church. There are now very small but effective loudspeakers which will fit into the bell tower and which can be controlled with a suitable MP3 sound module placed underneath your layout. An original recording of your favourite church will give you that extra bang, oh, pardon me, chime.

Manufacturer pages:
<https://www.archistories.com>

Exclusive distribution of this kit:
<https://www.1zu220-shop.de>

Church interiors (3D-Print):
<https://www.noba-modelle.de>



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Series E 41 from Märklin In complete new ways

If we think back fifteen years, then the E 41 stood next to the “crease” and a V 200 at the top of the wish lists of the Zetties. The last of these great wish models and symbols of the economic miracle on rail recently appeared to be the firecracker, to which, at first, only the members of the Insider club had access. Was the long wait worth it and does Z gauge continue to further progress with this model?

For the first time, an annual insider model has overtaken its predecessor in terms of deliveries. The V 80 has fallen behind a lot after a tool defect occurred that Märklin communicated to its customers. Instead, we are looking forward to the E 41, which just arrived at the dealers.



In its first years of service, the E 41 012, which is based at Munich Central Station (Bw München Hbf), is under way here with a passenger train made of four-axle conversion cars still painted bottle green.

Already during unpacking it becomes clear that this model should be something special. Hopefully, we think, this is not just the often quoted exclusivity for customer club members. With this locomotive, the thermoforming insert in which it rests also has a thermoforming lid.

It protects the tiny one under the now complete standard locomotive family from shocks and falls when (carelessly) opening the outer packaging box. However, this does not make it any easier to remove the box, because now the entire insert has to be removed before the lid can be removed.

But this is a question of habits and no reason for criticism. We are curious about what awaits us with this model. Now it is in our hands and looks quite familiar, its weight doesn't seem too light or too heavy to us, and so the locomotive should be in the familiar frame of similarly large bogie models. Will there be any surprises?

No, we'll find out soon enough. There is a lot different about this E 41 than we know it from its (only in the model) older sisters in the locomotive family! So now we start the test cycle full of excitement.



For the time being, the E 41 will be reserved for members of the Insider Club. As article number 88353 it is now available in steel blue as E 41 012 in its original version.

Exterior Design

The good, first impression is confirmed by taking the most important measurements and comparing them with the original template: All measurements have either been taken exactly or show deviations in the range from 0.1 to max. 0.6 mm, which is, therefore, more attributable to the usual measuring tolerances, than to conversion errors. We will discuss the two outliers herein.

All engravings and shapes that give the E 41 its unmistakable face are well on mark, and are sharply executed. Special features of the E 41 012 (Art. No. 88353), as delivered, are a circumferential rain gutter, five Schweiger fans with horizontal blades on each long side and the bare handrails on the driver's cab access steps.

For the first time on a Märklin Z gauge model, they are designed as separately attached metal parts. This also applies to the hand bar painted in the locomotive's colour, running horizontally under the driver's cab front windows. Both are particularly pleasant to look at from different angles. The side-mounted handrails are the cause of a dimensional deviation from the model in which the locomotive body formed the lateral boundary.

In the black bordered front windows with wiper simulations we can also see through the transparent panes mounted on the inside of the model, what are resting on the glass surfaces and protect them from fogging up.

The couplings and brake hoses are engraved only as hints, between the correctly sized buffers, so that they do not obstruct the system coupling, and it can swing unhindered through track curves.

The connecting cables on the outside of the buffers for the electrical heating of the carriages as well as the reversing train control are free-standing and individually attached.

Even their different connector shapes can be easily seen in the model.

Thus the “Knallfröschlein” (firecracker) surpasses every previous electric locomotive of this scale in the Märklin range and almost reaches the level of the Kittel steam railcar.

In contrast to it, this model even has an implied driver's cab replica, which can only be seen from the right angle.

In the light grey interior we could even see the horizontal hand wheel of the speed switch and the “boxes” of individually set-off indicator lights.

Märklin chose for first time for the original design of the headlight equipment with the headlight and taillight combined at the bottom in a lantern, while above we also find the larger original design centre head lamp of the first deliveries. A train radio antenna is not sprayed on.

Dimensions and data of the electric locomotive series E 41

	Prototype	1:220	Modell
Length over buffers	15.660 mm	71,2 mm	71,2 mm
Maximum width *	3.060 mm	13,9 mm	13,8 mm**
Height above SO*	4.422 mm	20,1 mm	20,7 mm
Overall wheelbase	10.500 mm	47,7 mm	47,8 mm
Bogie wheelbase	3.200 mm	14,5 mm	15,0 mm
Bogie centre distance	7.300 mm	33,2 mm	33,2 mm
Traction wheel diameter (new)	1.250 mm	5,7 mm	5,7 mm
Service weight	67 t	---	28 g
V _{max}	120 km/h		
Power	2.400 kW		
Design	Bo' Bo'		
Years of construction	1956 - 1971		
Manufacturer (mechanical)	Henschel, Krauss-Maffei, Krupp		
(electrical)	AEG, BBC, SSW		
Number of units	451 units		

* perimeter of the locomotive body (vehicle width) and the pressure air switch on the roof (vehicle width)

** with acceptance of the original dimensions; width over handrails 14,5 mm



Even the interior of the driver's cab is partially reproduced, but this is barely recognizable through the transparent panes visible in the window surfaces. Nonetheless, we managed to make the relief of the display devices and indicator lights visible in the photo.

The bogie panels, which show the sandboxes, track clearers, Indusi magnets and suspension elements, are also sharply defined and very three-dimensional.

Only the sand pipes have not been reproduced here. The steps for the ascent to the driver's cab, which protrude downwards over the edge of the locomotive box, were mounted on the bogie panels, a sensible solution in order not to impair the model's negotiable turning radius.

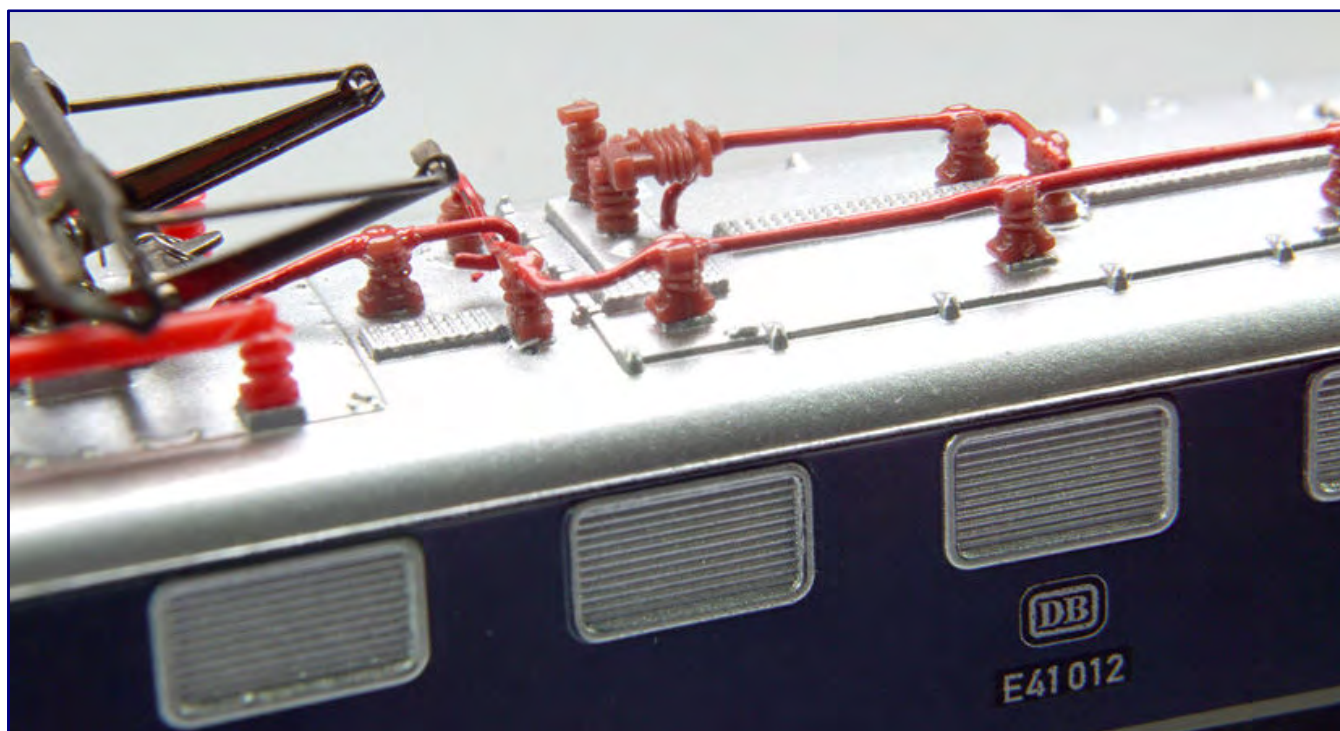
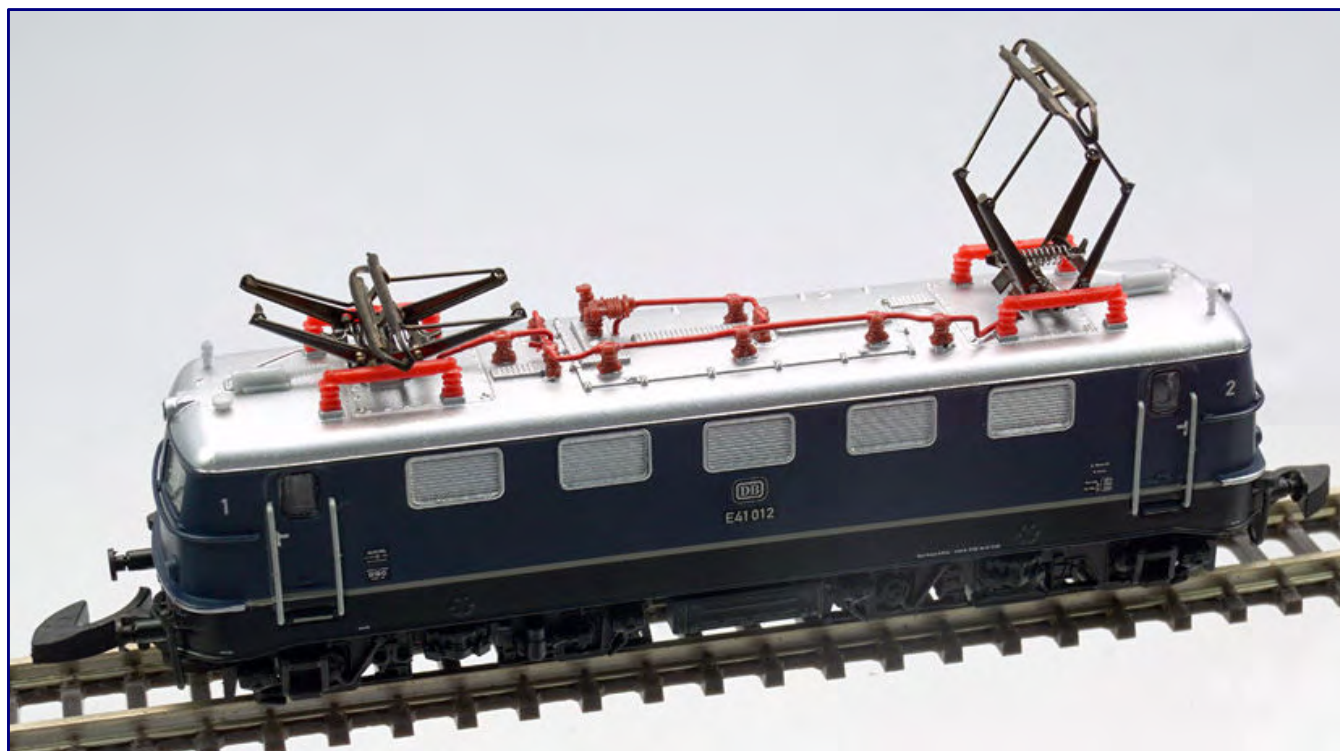
Between the bogies there are the air tanks of the brake system, the battery box and the radiator for the transformer oil.

Previously, these were included in the cast ingot in relief. Here, however, they are designed as three (black) injection-moulded parts for the first time, and only the radiator is silver-coloured.



The bogies are also finely engraved and have a good depth effect. The steps below the frame are moulded onto them (picture above). The locomotive whistle, the striking curl and the pantograph drive on the roof are also attached individually. For the first time in plastic injection moulding, the aggregates between the bogies (picture below) are also mounted: The oil cooler (above) and the battery box (below) are individually attached to the middle part with the air tanks.

The first glimpse usually falls on the roof of a model. Here, too, we find well-done details. The engravings for the removable centre of the roof to remove the transformer are as sharp as those for the roof sections, the contours on the bushing insulator or the supply lines to the current collector drives.



In addition to many attached parts, the roof is also characterized by sharp and convincing engravings of all structures, so that only the aged and not flat pantograph replicas attract attention (picture above). The roof cables reach all the way to the pantographs and have been reproduced for the first time with wires inserted into the insulators.

These no longer sit flat on the roof here, but the drives rise up like prototypes and lead up to the two pantographs. The other equipment is also complete with carrier insulation, insulators and compressed air quick-action switches made of injection moulded plastic parts.



At the front of the model, at the edge of the buffer plank, the separately attached heating cable (right-hand side of the locomotive) and the cable for the reversing train control are pleasing to the eye.

New and unusual for Märklin, however, is the installation of the roof cables made of bent and fire-red painted wire parts. We are familiar with this from Rokuhan, from small series manufacturers and from larger scales. Since the cables now also lead very close to the current collectors, we regard this as a great learning effect, which we would like to see as a new standard for the future.

The only drop of melancholy that has to lead to point extraction is the two pantographs themselves. These are the parts we have been used to since 1974. They are supposed to emulate the DBS 54 type, but are not exactly scaled and do not have diagonal braces.

Since they cannot be moved, lie flat on the ironed surface and instead protrude diagonally upwards at the ends, they are completely out of time with this fine model. The fact that Märklin has recognized this itself and has long since planned in a different direction is demonstrated by a special observation: Although, not explicitly stated in the product description, the pantographs have not been electrically connected.

The E 41 also has no adjusting screw on the circuit board. Overhead catenary line operation is therefore no longer planned, although these parts would allow it mechanically. Unfortunately, the Göppingen company did not use a fiery red paint. This only makes sense for us, if it is to be a temporary compromise.

Therefore we assume that in the course of the years the old pantograph will probably disappear and be exchanged against a more contemporary component. However, if this component is no longer designed for an electrical function, completely new perspectives for scale will open up.

Rokuhan had demonstrated this in the 181² class, which was well regarded. This has recently been proven by several conversions of Märklin models with catch-up pantographs.

But until then, the next step will be to take a liking to the nostalgic parts and wait and see. If desired, they can also be painted red by the buyer himself.

As always, the varnishing and printing are impeccable. The locomotive body is correctly painted in RAL 5011 steel blue, which corresponds to the colour scheme valid on delivery of the first machines.

The frame is finished in RAL 7021 black-grey, whereby a decorative line in RAL 9006 white aluminium marks the



Aus dieser Perspektive kommen die einzeln angesetzten Handläufe am Führerstands-aufstieg und vor den Stirnfenstern besonders gut zur Geltung.



Located in the Munich main railway station, locomotive weight 67 t and brake weights of 70 (position P) and 58 t (position G): All important operating labelling is designed to be flawlessly legible.

The test data "MF 1.7.57" refers to the acceptance test carried out by AW München-Freimann during commissioning.

colour separating edge, which is also executed exactly, precisely and sharply in the Modell. The roof above the rain gutter, the ventilators and the door handles are also in the colour of the decorative line.

Three colour shades were used for the lettering: all locomotive signs are under laid with black and printed in a silver colour.

Data that appears directly on the locomotive body was printed in white. All fonts are flawless and legible, although this reaches its limits in metallic printing due to pigment size.

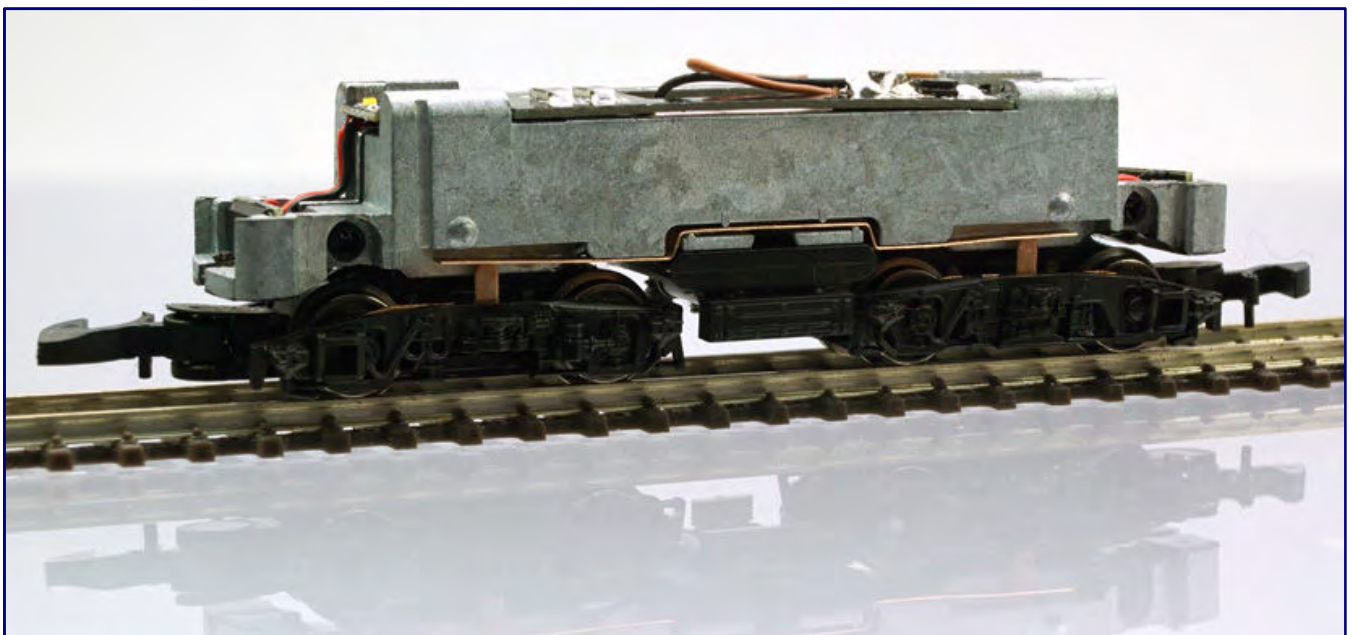
So we read that E 41 012 was built by Henschel and BBC and is located in Bw München Hbf of BD München.

Thus the operating condition corresponds to the beginning of the very first maintenance section of the locomotive with correct and historically verifiable data.

But does the locomotive also technically live up to its visual promise? Will we also find innovations under the hat? We now want to clarify this in the following section.

Technology and construction

If we remove the housing from the locomotive, we get the impression that this is not a Märklin model in front of us. And yet, the E 41 is a so-called “butter-and-bread locomotive” and by no means a special niche product.



The construction of the undercarriage and the type of current collection resemble that of AZL and Rokuhan. That probably means a departure from a 47-year-old design practice by Märklin.

But their construction method completely deviates from what was standard in Göppingen for well over forty years. What we have to describe, we know to a large extent from H0 scale, and also from the models of the companies AZL and Rokuhan. And we don't just mean the bell-shaped armature motor with two shafts and small flywheels!

Märklin has long since divided the engine block into two halves, which are bolted together, so that when assembled, they hold the bogies in place and guide them, and they can no longer be released by removing an axle.

What has remained is the power transmission from the motor via worms to gears in the bogies, but these are no longer centred on the axle. The current from all eight wheels is not taken from the wheel flanges on the inside, but rather from the tip bearings on the axles. From there, the voltage is passed on to thick strips of bronze spring plate, which can be found on the right and left of the lower edge of the cast block parts. So this model really represents a quantum leap, because its operational safety is also convincing.

Conventional and well-known is the circuit board on the block, which is responsible for LED control. As already mentioned, Märklin has not integrated a switching screw and has not electrically connected the pantographs. At the front and at the back there is a warm white LED, which illuminates the upper headlights.

Two small lighting boards can be found further down at the ends of the bogies. Electrically connected to the main board, they are also integrated into the directional light change. They fulfil this task with one warm white and one red LED each. These lights can be clearly seen early on when the locomotive begins moving.



Small lighting boards are installed at the front ends of the chassis block. They each carry a warm white (yellow in the picture) and red SMD-LED (sitting above) off-centre. The warm white light-emitting diode for the upper top light is located in the middle at the end of the main board.

Due to the lack of breakaway torque, it starts to move very slowly at 0.3 volts, with the equivalent of slightly less than 1 km/h. The engine is now in the starting position. Due to the engine technology, this is also the slowest continuous driving speed. Safe and uninterrupted on turnouts, it travels at 1.6 volts with the equivalent of 19 km/h speed.

It easily achieves its maximum model speed of 120 km/h and although it is not a racehorse compared to older designs, it clearly surpasses its prototype at the full driving voltage of 10 volts and could even cover for an Intercity.

The smooth and quiet running at all speed levels is definitely appealing. If it accelerates gently, this is no comparison to the prototype, which made real leaps and bounds due to its banging up gears.

The tractive power of the small locomotive, which at 28 grams is roughly in the weight class of Märklin's class 218, is also completely sufficient. Their tractive power is also comparable. Typical commuter trains with five or six Silberlingen or middle boarding coaches, therefore, pose no problem on level track.

The current consumption also remains within the usual limits of a bell-shaped armature motor. With transformer position 100 we measured an average of 21 mA, with 150 this value increases slightly to 28.5 mA. This speaks for a good drive system, as well as a smooth-running gearbox.

Prototypical use

Märklin closes a program gap with the E 41 and completes its family of first generation standard electric locomotives.

This also makes the existing gaps in the range of wagons all the clearer: the typical old building wagons, with which the model was often used as a fast train, are missing here, as are the hare box driving cars for the Silberlingen or middle boarding coaches.



E 41 012 pushes a turning train past the photographer in the streak of the last rays of sunlight. This is probably the best known and most popular use of this series for model railroaders. Unfortunately, Märklin does not have the matching control car in the form of a rabbit box.

The UIC-X passenger coaches in the Märklin range were not built until the early sixties. This means that Era III reversible trains cannot be built, but rather the E 41 will be used in express or even express train traffic, where at that time n-coaches (Silberlinge) and yl-coaches (middle entry coaches) were still widely used because they were considered to be of high quality.

The four-axle conversion cars are also always suitable, although in the meantime we have noticed a certain interest in the range of cars of Era III, which unfortunately leads to some monotony on the layout. They can also pull light freight trains in which fast running, modern cars may be used. With 120 km/h the E 41 had to throw something into the balance at that time.

Summary and outlook

Since a first digital exchange board from Velmo has already been shown, Märklin's new product will probably spread quickly on both analogue and digital systems. Further variants are already eagerly awaited, of which the model has a very large number to offer, as our model report shows.

Märklin can also operate many of these from existing moulds with suitable trolleys. The question remains to what extent Märklin will be able to vary its new shape in terms of fan grilles or lamp arrangement.



Middle boarding cars were once also widely used in commuter and express trains. Therefore they also belong to the typical passenger cars on the hook of a locomotive of the class E 41, as E 41 012 of Bw München Hbf proves here.

Both visually and technically, the E 41 is a milestone whose design is fully up to date. And it did not suffer any weaknesses in the driving test either. It sets new standards and is finally a model for the scale 1:220, which is suitable without hesitation for pushed trains.

That's why we also honour Märklin's choice of role models, design, implementation and spirit with a nomination for the best new releases of 2019 in the locomotive category.

Manufacturer pages:
<https://www.maerklin.de>

Decoder solution (in development):
<https://velmo.de>

First standard locomotive series E 41

A firecracker at the DB

The E 41 was delivered in 1956 as the first unit locomotive of the new DB type range and was a nose length ahead of the E 10 at least in this discipline. For about forty years it did its job faithfully, which made all its mockers liars. Because of its typical start-up noises, it has always been remembered as a “firecracker” by many train drivers, but especially by her admirers.

After the Deutsche Reichsbahn had already put a very innovative and ground breaking vehicle into service in the 1930s, the E 44 bogie locomotive without a drive shaft, it later became quiet about further development. The state preparing for the war of aggression had other priorities.

And so especially the Second World War and the first post-war years brought stagnation in German electric locomotive construction. Especially in Switzerland it looked different at that time: In 1944 the BLS put the Ae 4/4 developed by SLM and BBC into operation.



The Ae 4/4 of the BLS was an electric locomotive without a drive shaft, on which the DB could orient itself. In 1944/45 two units were purchased, followed by six more by 1954. Here the Ae 4/4 255 entered Kandersteg in June 1956 with express train 133. Photo: Kurt Eckert, Collection of Eisenbahnstiftung

It had a weight of only 80 tons, but 4,000 hp power, i.e. 1,000 hp per wheel set. With a top speed of 125 km/h, it fully met the requirements placed on it. Between 1946 and 1948 it was followed by a lighter counterpart at SBB called Re 4/4 with 2,480 hp and a weight of 56 tonnes. But the development of electric traction had also progressed in France.



Also the Re 4/4^I of the SBB, which like its successor Re 4/4^{II} even came with TEE honours, is in the line of ancestors of the German standard locomotive family. Before the TEE "Rheingold," the Re 4/4 10039 of the second series (built in 1950) left the Basel SBB station on 22 May 1969 to transport the train to Geneva. Photo: Joachim Claus, Collection of Eisenbahnstiftung

When the young Bundesbahn was about to set up a type program for electric locomotives, it was able to build on the project of an E 46 of the Reichsbahn, further developed from the E 44 series, but had to try to learn from the experiences and achievements of foreign countries: it was a matter of catching up on ten years of development from an international point of view.

The first plans still envisaged that a universal, four-axle E 46 would take over the light services in passenger, express and freight train traffic and that a heavy, six-axle freight locomotive E 50 would become the long-term successor of the German crocodiles.

For the E 46 a maximum weight of 82 tons for a locomotive without drive shafts had been given to the developing manufacturers.

The maximum speed was to be 120 km/h, because the 90 km/h of the E 44 did not seem to be sufficient in the long run even during the ongoing reconstruction.



E 41 026 is here in its original state at the exit from Farchant. On the hook it has a longer train of modern middle entry cars, only the old luggage car behind the locomotive is out of place. Photo: Archive Petkelis

Even during the planning phase, the DB increased the speed to 130 km/h and changed the project designation to E 10, a striking series from the number range of express trains.

A total of four different versions (E 10 001 - 004) were ordered from six companies for testing, a fifth (E 10 005) followed shortly afterwards and was identical in construction to the E 10 004. The DB drew its conclusions for the equipment of the planned series machines from the test runs and trials and set the standard for its standard locomotive range. This included the newly developed rubber ring spring drive.

From standard locomotive to the firecracker

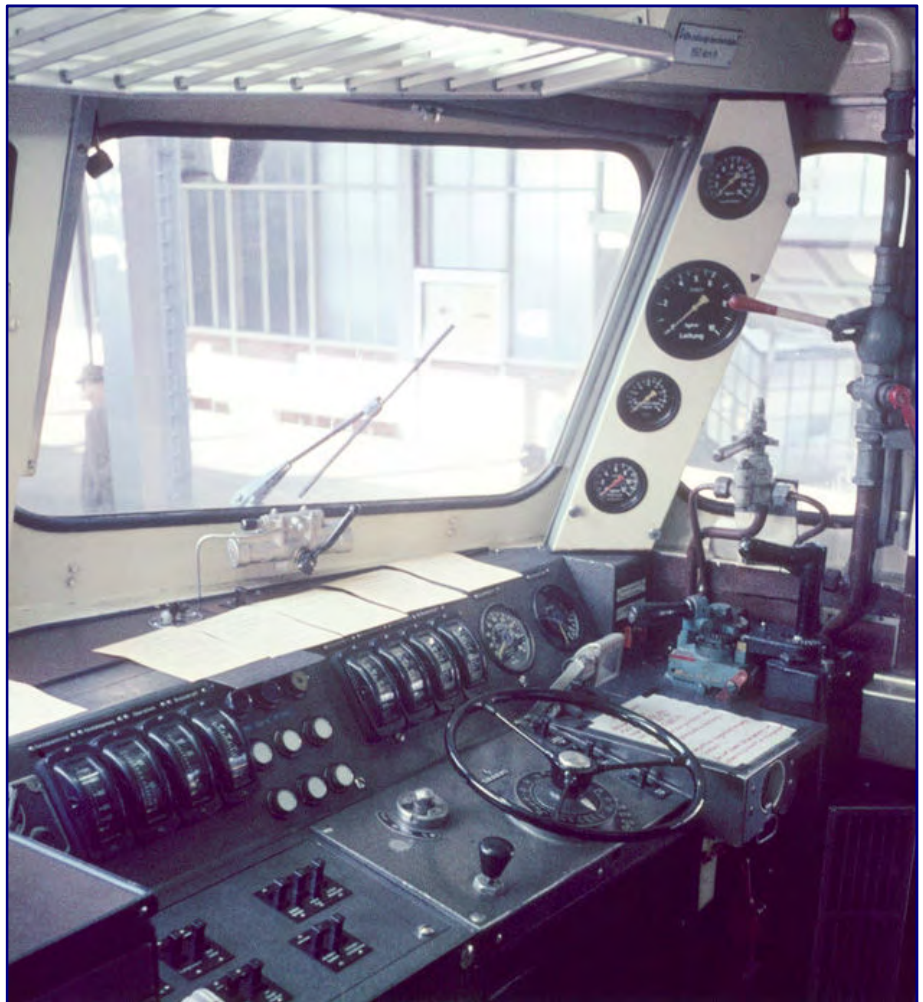
But it also became clear that not a single, four-axis series could economically meet all requirements. For some tasks, the E 10 was simply too complex and therefore too expensive. Thus the five pre-series locomotives were turned into an E 10 series, which was now clearly intended for express service (up to 150 km/h).

The other tasks were to be taken over by the E 40 for medium-heavy freight train service and the E 41 for light service on secondary and main lines, which deviated only in the transmission and the missing resistance brake.

A maximum axle load of 15 tons and a maximum speed of 120 km/h had been specified for this series.

The smallest and lightest type of the electric type program was to carry passenger trains with a weight of 500 t and a gradient of 3 per thousand at maximum speed, freight trains with a weight of 1,000 t and a gradient of 5 per thousand still had to travel at 65 km/h.

Because the trains were used in congested urban areas, a task that was becoming increasingly important due to the ongoing electrification, a reversing train control system was also planned, which all 451 machines in this series were equipped with.



An important achievement of the standard locomotives was their sitting operation, which is why the hand wheel of the speed step switch was positioned horizontally. The driver's cab shown here as an "iron on" of the E 10 series is essentially identical to that of the E 41. Photo: Archive Petkelis

The low axle pressure also upgraded the E 41 for some electrified branch lines in southern Germany, where there had already been an electrically operated network before the war. Here it became the successor of the E 44, compared to which it was faster, more powerful and at the same time even lighter.

BBC (electrical part) and Henschel (mechanical part) were commissioned with their development. It was the only series locomotive in the newbuilding program to be fitted with a low-voltage switchgear, as was used in the E 10 003. In principle, this had already proven its worth in the E 16 and E 32 series.



When the photographer photographed the E 41 035 in Basel Bad Bf in 1957, the still almost brand-new locomotive showed itself without dirt on the steel-blue paint and its bright roof. Good to see are the large, black edged lamps and the transparent panes, which we also find on the Märklin model. Also the signs on the side correspond to the final arrangement, as it was also reproduced on the model. Photo: Archive Petkelis

This was the reason for the typical banging of the rear derailer when the speed steps were switched on, which impressed many commuters and pupils on their daily journeys as well as the jerky starting: It soon became known as the “shooting gallery” or “springbok”.

However, the most memorable and widespread thing over the long period of use was the nickname “firecracker” for the majority of the green-painted machines. When it was dressed in ocean blue ivory, it was also called a champagne cork.

It is noteworthy that the Bundesbahn dispensed with pre-series locomotives and immediately started series production. Its experience with the five prototypes of the E 10 was enough for it, only individual components were tested in the E 44 087G and the ET 30. The latter, a multiple-unit train with then unknown high acceleration, was the origin of the further developed traction motors.

Compared to the E 10 class express locomotives and the E 40 class freight locomotive, the light E 41 had simplified technology. However, the fact that it was often referred to as a cheap locomotive treats the locomotives unfairly.

The reason for its development was that an E 10 was overpowered for this task and simply too expensive. The Bundesbahn, which was always keen on economy, had deliberately opted for a tailor-made machine. If its purchase price is broken down to one kilowatt of installed power each, it does not have to shy away from comparison with its stronger sister.

The first standard locomotive

The E 41 001 was delivered to the DB on 27 June 1956, making this series its first standard locomotive. By February 1970, 450 more locomotives followed, the last of which were designated class 141 as soon as they were put into service. A service life of more than 40 years and an average age close to this speaks for this good and proven design.



When 141 004-2 showed herself to the photographer in Munich on 15 September 1979, she had to endure several changes. She had lost her engine room windows early on, including her steel-blue paint. The upper top light was replaced by a smaller standard lamp and the continuous rain gutter was replaced by short rubber strips. Photo: Eckerle, Archiv Petkelis

increased yet again. They were available as parts fitted into the opening (Krapf & Lex type), or later as mounted Klatte fans with frames.

The new locomotives quickly conquered the fast local traffic in the various conurbations, and were also used for light freight trains and, especially in the early years, helped out quite frequently in express service.

For this reason, the E 41 001 to 071 numbers were painted in the steel blue of the F trains, before the DB ordered in 1958 that this paint should be reserved for fast electric locomotives with a top speed of more than 120 km/h.

The F trains were also painted in steel blue. All other locomotives up to the 141 451-5 were therefore already designed in bottle or chrome oxide green ex-works.

Another special feature concerns (according to our research) only the E 41 001 to E 41 004: When they were put into service they had one engine room window on one side and two on the other.

This provided more light inside, but not enough air flow to cool the units. Therefore, these three windows were exchanged for additional fans after only a few months.

As with the other electric unit locomotives, these were initially silent fans with horizontal slats. Later, double jet fans with vertical slats were used, through which the air throughput could be significantly

In the course of their service, old Schweiger fans were replaced with more powerful Klatte fans on the locomotives. However, the DB did not remain faithful to this project and later also mounted individual Schweiger fans on the sides of some of the locomotives, which were then quite colourfully equipped.

Five instead of three lanterns?

The reason for turning away from the large, lower headlights and switching to the separate DB standard lamps for red and white signals was damage caused by bird strikes, which usually caused both signalling systems to fail.

With the separate lanterns, the engineer was often able to restore a correct signal pattern by changing the lenses.

This was similar to the lamps of the E 41. As with the E 10 and the E 40, large single lamps were part of the original equipment of the older machines. The upper headlight was soon replaced by a smaller one, while the lower lanterns from E 41 121 onwards were replaced by separate headlights and taillights (DB standard lamps).

Older machines also received a conversion from three to five headlights in the repair shops. Often this was done as part of accident repairs, which is why locomotives sometimes left the works with different equipment at either end.



141 027-3 is still largely in its original condition on 10 November 1974 when manoeuvring in the Starnberg wing station (Munich main station), with a circumferential rain gutter, step grating and holding bar as well as the original lamps and silent fans. Only the signage was adapted to the computer age. The sounds of the rear derailer and the chrome-oxide green colour earned the series the nickname "firecracker." Photo: Eckerle, Archive Petkelis

Other striking differences in the course of the long service life were the omission of the horizontal handle bar at the front and the circumferential rain gutter, which were replaced with short rubber strips due to their susceptibility to corrosion. The last five machines (141 447-3 to 141 451-5) also received an electric regenerative brake, which was recognisable from other roof structures and made them about three tons heavier.

Further developments

The first unit locomotives of the class E 41 came to Bw München Hbf, where they would not play a role for a long time. As electrification progressed, Frankfurt (Main), Mainz, Cologne and Dortmund became the first home for many new deliveries. Later, Saarbrücken Hbf, Hannover Hbf or Hamburg-Eidelstedt were also added.



141 220-4 behind a pushed local train of five Silberlings: This was the typical view of how we often encountered the class 141 in everyday life, almost nationwide. It was also on its way in the pre-run operation of the S-Bahn Rhein-Ruhr, but then marked with an S-Bahn sign at the front. Photo: Archive Petkelis

In the Ruhr area, it was used in the S-Bahn pre-run operation (with the S-Bahn symbol on one end), 141 248-5 was finally coupled in 1978 with the Karlsruher Zug, a test train for the S-Bahn Rhein-Ruhr based on converted silver pieces. In return, the locomotive and the car were painted in ocean blue ivory, but the two colours were changed in comparison to the standard paintwork.

Another special feature was that the blue ribbon was pulled over the front and both long sides of the train, while the opposite front had a blue stripe at the level of the lower headlights. The roof of the locomotive was also painted ocean blue. The 141 378-0 in the regular ocean blue ivory also had the latter special feature.

The class 141 was to later make a major career in the S-Bahn service in Nuremberg. When such a network was also opened there in 1987, DB opted for locomotive hauled trains due to good experience in the Ruhr area. Like there, x-cars were used, but this time with covering by adapted machines of the 141 series.

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Two unusual variations of the series 141 can be seen here. 141 248-5 was painted according to the S-Bahn colour scheme, with the colours ocean blue and ivory reversed from the standard colours. On the upper picture it is in April 1981 with the Karlsruher Zug in Hattingen on its way. In contrast to her sisters, 141 378-0 (picture below) also received an ocean blue painted roof, which looked good on its face. Photos: Archive Petkelis

For this purpose, the Bundesbahn equipped the seven copies 141 436-6 to 141 442-4 with time-division multiplex reversing train control (TCC) and target film boxes, while adapting the exterior to the then new S-Bahn colour scheme with belly band in RAL 2012 salmon-orange and pastel-yellow settling strip in accordance with RAL 1034.



141 141-6 with time-division multiplex reversing train control was one of the seven machines destined for the new S-Bahn in Nuremberg. Here the locomotive with such a train from x-wagons waits as S 1 to Lauf (on the left the Pegnitz) on 1 October 1988 in Nürnberg Hbf for the departure. Photo: Archive Petkelis

But these were by no means all the special paint jobs that were carried out on this electric locomotive. In addition to the class E 10, the Bundesbahn also used the E 41 for smaller colour experiments. Three chrome oxide green locomotives were given a different frame colour for experimental purposes: dark grey (E 41 291), light grey (E 41 373) and ochre yellow (E 41 374).

In addition, there are the many standard paint jobs that were valid during the long service period of the E 41 / 141. Their representatives had all worn them: steel blue from 1956, bottle green from 1958, chrome oxide green from 1959, ocean blue ivory from 1975, oriental red with white bib from 1987 and finally the traffic red valid from 1996.

In all colours they proved themselves in nearly the entire Bundesrepublik (Federal Republic). Thus, on the Ammergau Railway, it created the basis for the retirement of the class 169, at that time already by far the oldest DB aircraft. It was also used in the tests with the central buffer coupling planned for introduction throughout Europe.

But when, in the seventies, the S-Bahn trains of the 420/421 series and, shortly afterwards, the 111 also started their service, the time of the 141 series seemed to run out slowly. But the dead lived longer, because that was far from being the case!

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On the Ammergau railway, the 141 series created the basis for the retirement of the 169 series. 141 006-7 (top photo) was taken near Altenau on 20 August 1977. Photo: Eckerle, Archive Petkelis

141 005-9 (photo below) the photographer caught in Nuremberg main station at track 2. The locomotive was located in Nuremberg until 1983. Photo: Archive Petkelis

Although there was an increase in relocations driven by under-utilisation or the concentration of vehicles on a few depots, stocks remained stable for a long time. It was not until 1987 that the first defective vehicles were taken out of service.

They even found a further field of work in intercity service. After the state capital Wiesbaden was disconnected from the IC network, the German Federal Railways (Bundesbahn) was covered with protests and complaints and reacted with a short-distance service called Wiesbaden-City as a replacement.



The two sisters 141 015-8 and 141 382-2 allow a direct comparison of the two faces of this series in Nürnberg main station. 141 015-8 has single lamps, while 141 382-2 shows the smaller DB standard lamps with separate headlights and taillights. However, both have already undergone extensive modernisation: This is evidenced by the rubber rain bars and the omission of step grids and the surrounding holding bar. They are replaced only by a short tread plate and the vertical UIC handle. The UIC socket next to it was also not part of the delivery condition. Photo: Archive Petkelis

The 141 series was used with specially prepared LHB local transport coaches, which had originally been purchased as test coaches for S-Bahn operation, but then did not go into series production and could hardly be used sensibly in the meantime.

When the City Railway concept introduced and tested on the Köln - Gummersbach route proved successful, further lines followed, for which less costly conversions were carried out and whose wagons also operated in the planned new standard paintwork. Here, too, the 141 series between Hamburg and Stade was the first choice, again.

It was not until the Deutsche Bahn AG was founded on 1 January 1994 that the local locomotive passed its zenith. Now it was increasingly displaced by the young class 143 and the 112 jointly procured by DB and DR. Likewise, the 110 series also increasingly migrated into local transport and received reversing train controls from the 141 that had been taken out of service. And as is generally known, the future was to include high-performance multiple-unit trains.

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141 183-4 shows us at station 210 in Rheine the oriental red colouring introduced in 1987. It is underway with its regional express on the Emsland route towards Emden. Photo: Roland Hertwig

The Braunschweig 141 336-8, parked here in Münster (Westphalia) on 9 February 2001, represents the last operational state of its series with Deutsche Bahn AG. In December 2006 this era finally came to an definitive end. Photo: Archive Petkelis

Nevertheless, the last “firecrackers”, meanwhile had been painted traffic red, and lasted until 2006. But on 10 December 2006, the last three DB Regio locomotives also came to an end, and that chapter in the locomotive's history was finally over.



The end: A long series of class 141 locomotives is waiting to be scrapped after being taken out of service in Opladen (top photo). The sister locomotive 141 006-7 (photo below) was better off. In the Dieringhausen Railway Museum in 2007, the traces of an external reconditioning could clearly be seen. In this photo it also had had new engine room windows installed.

However, several machines have been preserved for posterity, some of which we would like to list: E 41 001 was largely restored to the condition of its first years of service and can be seen, painted steel blue, in Koblenz-Lützel since 2001.



141 228-7 was until its last days in RAL 6020 chrome oxide green underway and was preserved in the railway museum Darmstadt-Kranichstein. It was on display in Korbach on 13 May 2006 at the Kurhessen Railway Festival.

141 006-7 is waiting for better times in the Railway Museum Dieringhausen, while 141 083-6 in its last painting (RAL 3020 traffic red) welcomes the visitors. The 141 228-7 on loan in Darmstadt-Kranichstein represents her sisters in chrome oxide green colouring.

Technical information on the series 141:

<http://www.ig-einheitsloks.de/?s=br141>

Cumulative locomotive nicknames:

<http://www.bahnstatistik.de/Spitznamen.htm>

<http://www.db-loks.de/tfz/spitznamen.html>

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Mechanical signal box Home made pulley holders

Many years ago the company D. Beier BMT offered pulley holders of sizes I, II, III and IV for Z gauge. But that was a long time ago and since this program was discontinued, there has been a big gap in the Z-gauge market. But our reader Wilfried Pflugbeil from Chemnitz is patient and knew how to help himself. And building yourself isn't nicer than buying it anyway?

By Wilfried Pflugbeil. The time of the mechanical signal boxes with their outdoor installations is also very nice to look at on model railways. Clamping mechanisms, deflection boxes and sheet metal channels are also available in Z scale.

What we rarely see, however, are rope pull guides over pulley holders (and pressure pulleys), as they were used on open track. Today I would like to describe how pulley holders can be produced simply.

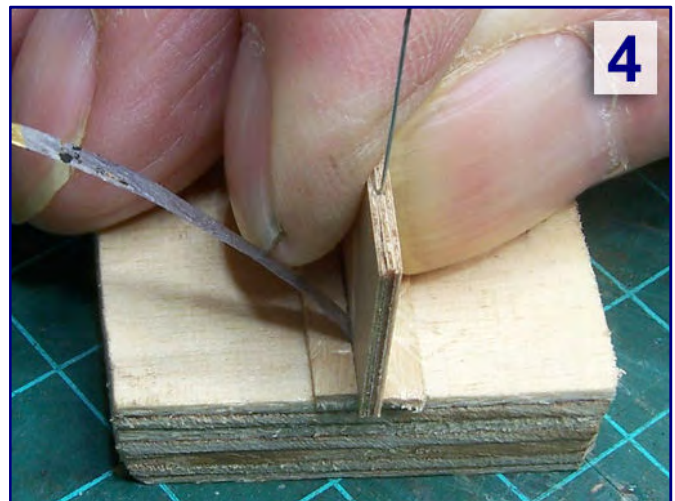
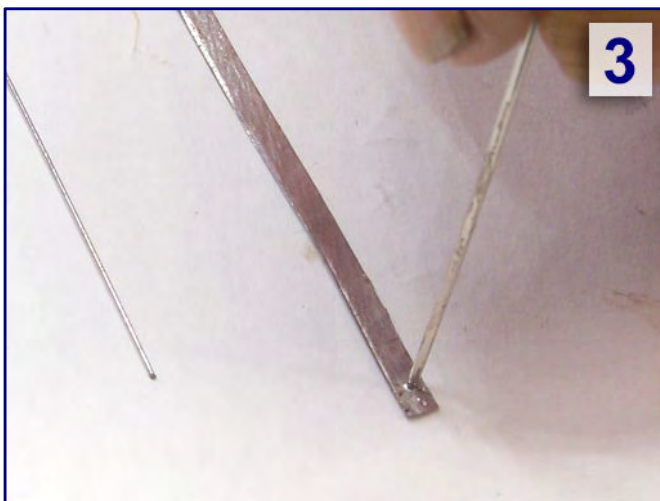
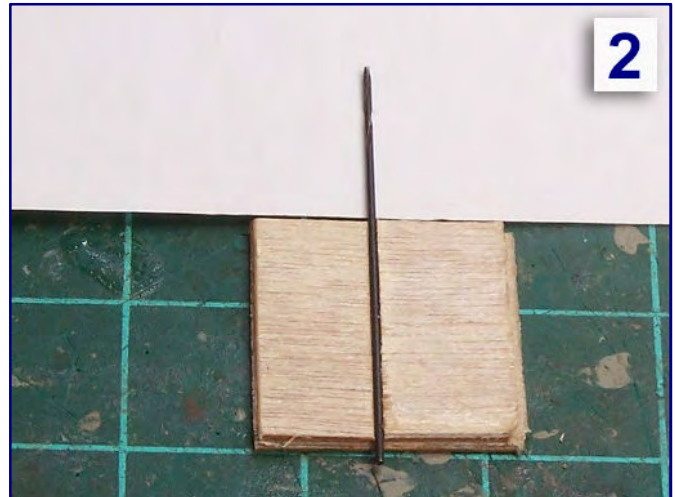
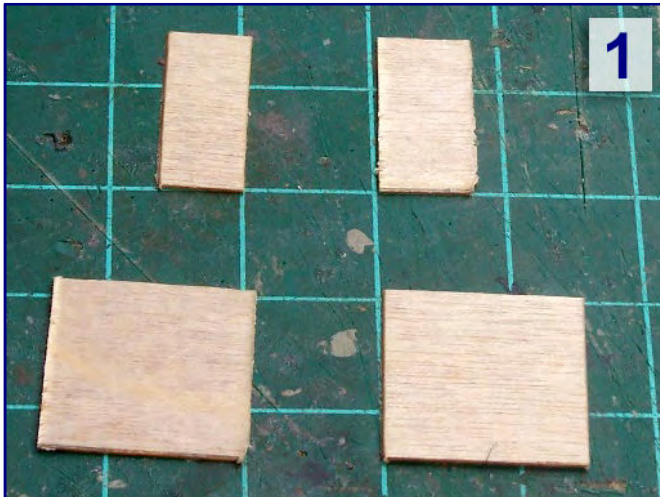


Parts of the mechanical switch tower can rarely be seen on Z gauge layouts, such as here the cable tensioning systems, deflection boxes and cable ducts near the switch tower on the layout of Wilfried Pflugbeil.

First we need a stamp to bend the covers. This should be 1.2 mm thick and have a hole of 0.4 mm diameter in the middle. For this, I glued the stamp together using 0.4 mm plywood, and with the help of a 0.4 mm drill bit as a spacer for the individual wooden parts.

For the cover, I cut a 2 mm wide strip from 0.05 mm thick brass sheet. A 0.3 mm thick steel wire is used for the mast. Both parts are now tinned and a hole is pressed into the middle of the sheet metal with the help of a pin.

Now we push the mast through the stamp and the sheet metal and form the sheet metal with our fingers to a U.



Procedure for building your own in sub-steps:

First, a die for pressing the defined shape is built. This consists of four plywood parts (Fig. 1) 0.4 mm thick. This is important so that the drill bit of the same diameter can be inserted seamlessly as a spacer (Fig. 2). The fourth plywood part is then glued to the top and closes the mould.

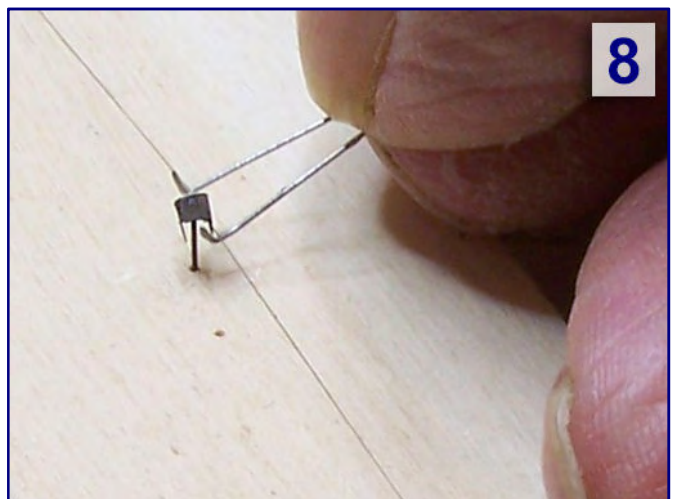
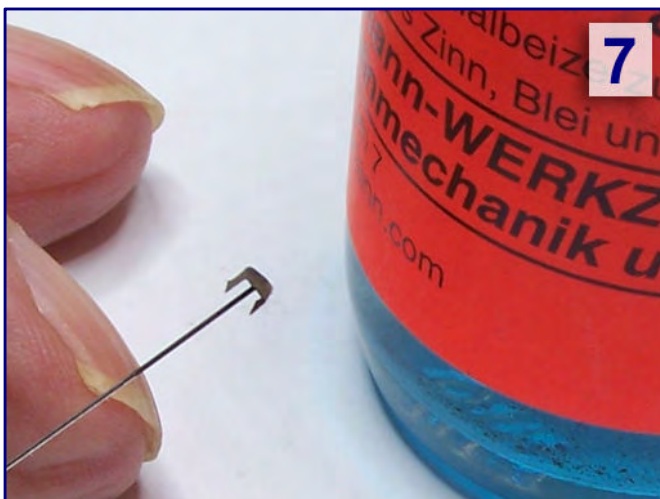
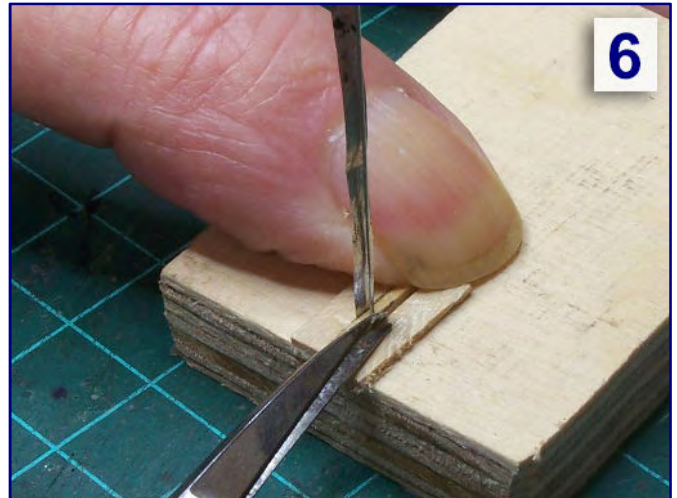
The cover is made of 0.05 mm brass sheet, cut into a 2 mm narrow strip. A pin is used to press a hole into the sheet, into which a 0.3 mm steel wire is inserted as a mast. Both parts are then tinned (Fig. 3). Now, the wire is led through the stamp and the sheet metal (Fig. 4). The later covering of the pulley holder can now be U-shaped with the fingers in the prepared form, which serves as a template.

Continuation of the series on page 39

In a prepared piece of plywood we make a hole with a diameter of 0.4 mm. Now our mast is put into this hole, before we glue 1 x 2 mm measuring sticks to the right and left as templates.

Now the mast is put through the stamp, the prepared sheet metal and the hole in the plywood. The sheet metal is aligned and then pressed down with the stamp. With a little solder we solder the mast flush with the cover. The excess solder is then filed off.

We now insert the component (without stamp) into the template and cut the sheet flush with the upper edge of the template. A small piece of wood measuring 1 x 1 mm and with a hole measuring 0.4 mm serves as a hold-down device.



Continuation of the step-by-step photos:

With a little solder, the mast is soldered flush with the cover and the excess tin is then filed off (Fig. 5). The almost finished component is now inserted into the mould again without the punch and cut off flush above it (Fig. 6). A small piece of wood measuring 1 x 1 mm with a hole measuring 0.4 mm serves as a hold-down device.

A bath in "tin and lead black" produces the desired black colour in the form of a burnishing (Fig. 7). The mast can then be cut to size and installed on the system.

The thread from dark tights or stockings reproduces the tensioning ropes. It is laid on both sides of the cable guides and fastened at its beginning and end. With a hook, it can then be pulled slightly outwards at the height of the rope pull guides (Fig. 8) and glued under the rope pull guides with a drop of adhesive.

A bath in Fohrmann's "tin and lead black" now brings us the black colour of the model without having to apply paint.

Now the mast can also be cut to size and installed. The thread from dark lady tights (stockings) should represent the tension ropes. It is laid on both sides of the cable guides and fastened at the beginning and end.

With a hook, the thread can be pulled outwards at the height of the rope pull guides, a drop of glue can be applied and glued under the rope pull guides. The finished result can be seen in the final picture. I find it very convincing. If you share my opinion, I would be happy if you would like to reproduce it.



As a small detail with great effect, the pulley holders have long since surfaced on the main line. And the attention-grabbing observer will very quickly find the rope pulls that pass through them.

All photos: Wilfried Pflugbeil

.....
Construction reports and projects from Wilfried Pflugbeil:
<https://f.z-freunde-international.de>
.....
Current source of supply for tin and lead black:
<http://www.glas-per-klick.de/glaser-chemie/patina-schwarz.html>
.....

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Dokumentation einer Baureihe **Der Lebenslauf der E 41**

Exakt fünfzig Jahre waren Maschinen der Baureihe E 41 oder 141, wie sie ab 1968 hieß, im Betriebsdienst der Bundesbahn und Deutschen Bahn vertreten. Sie war allgegenwärtig und bekannt, doch sie stand nur selten im Fokus der Eisenbahnfreunde. Sie war einfach zu unauffällig und doch eine wichtige Stütze des Nahverkehrs. Der EK-Verlag hat ihr schon vor zehn Jahren ein literarisches Denkmal gesetzt, das nach wie vor erhältlich ist.

Roland Hertwig / Werner Streil
Die Baureihe E 41
Entstehung, Technik und Einsatzgeschichte

EK-Verlag GmbH
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Als das vorliegende Buch im EK-Verlag erschien, hatte sich die Baureihe E 41 bereits aus dem aktiven Dienst verabschiedet. Exakt 50 Jahre lang hatte sie zuvor dem Nahverkehr auf elektrifizierten Strecken Westdeutschlands ein Gesicht gegeben.

Verschleiß und das Ablösen durch andere Elektrolokomotiven sowie neu beschaffte Triebzüge führten zu größeren Ausmusterungswellen, bis der „Knallfrosch“ 2006 ganz aus dem Betriebsbestand verschwunden war. Ein literarisches Denkmal hat sich diese einst allgegenwärtige Lok damit sicher verdient, was Verlag und Autoren dazu bewogen haben mag, ihr dieses Portrait zu widmen.

Mit den Autoren Werner Streil (für den technischen Teil) und Roland Hertwig (für Chronologie und Betriebsgeschichte) haben sich zwei äußerst fachkundige Personen ans Werk gemacht. Von ihrem Wissen profitieren folglich auch die Käufer dieses Titels, der umfangreiche Informationen und Eindrücke auch zum vorbildnahen Einsatz der Modelle vermittelt.

Das Buch ist klar strukturiert und beginnt mit einer Einleitung zur Geschichte der elektrischen Drehgestell-Lok bis zur Mehrzweck-Baureihe E 41. Die Technik dieser Konstruktion mit allen wichtigen Merkmalen wird ausführlich auf den folgenden, rund achtzig Seiten beschrieben.



Sehr deutlich wird bis hierher das enorme Fachwissen, mit dem der frühere Bundesbahner Werner Streil zu Werke geht. Leider verliert er nur, wie auch bei einem früher an dieser Stelle vorgestellten Buch, dabei den Leser bisweilen aus den Augen.

Dies äußert sich auch hier wieder durch ein zu weites Ausholen in zeitlichen Kontexten, der allzu deutlichen Fokussierung auf persönlichen Ansichten, wenn sie nicht im Einklang mit denen der früheren Entscheidungsträger stehen, und einem Festbeißen in einzelne Aspekte. Hier läuft er Gefahr, den Leser zu überfordern und abzuhängen.

Als Beispiel für diese Ansicht möchten wir das Ansetzen bei der Vorgeschichte zur E 41 anführen: Der Autor beginnt sehr ausführlich im Jahre 1907, als über die bayerische EG 4x1/1 entschieden wurde – also fast exakt fünfzig Jahre vor Anlieferung der ersten E 41. Dass die E 44 einen wichtigen Meilenstein darstellt, ist richtig und verständlich, doch auch hier möchten wir den Umfang der Abhandlungen in Frage stellen.

Deutlich besser wird der Lesefluss in den Abschnitten, die Roland Hertwig erarbeitet hat. Er beginnt mit einer Chronologie der Baureihe, die er eng in den Zusammenhang mit den laufenden Elektrifizierungen der Bundesbahn stellt.

Das ist insofern optimal, als dass diese Lok tatsächlich genau davon profitierte, denn längst wurden nicht mehr nur die großen Magistralen mit Fahrdrabt versehen. Der Bedarf an einer E 41 entstand genau daraus und auch die anfangs nicht mal zu erahnende, große Stückzahl war deren Folge.

Ausführlich behandelt werden anschließend die letzten Monate der Baureihe 141, die sich stellenweise wie ein „Tod auf Raten“ lesen. Bevor es dann mit den Heimat-Betriebswerken zu einem weiteren Hauptteil des Buches kommt, werden betriebliche Besonderheiten wie die Ammergau-Bahn, die S-Bahnen Rhein-Ruhr und Nürnberg behandelt.

Einen inhaltlichen Ausklang des Titels bieten Blicke in Betriebsbücher, Umlaufpläne, ein Lokführer-Bericht und Lebensläufe aller 451 Maschinen. Eng verbunden mit dem Vorbild ist auch deren Ausbesserung nach festen Instandhaltungsstufen. Deshalb haben sie auch noch Eingang in den Titel gefunden, was ein wenig wie ein (nicht erforderlicher) Lückenfüller wirkt, weil das ja baureihenübergreifend relevant ist.

Ein im Vergleich zur Gesamtseitenzahl kleiner, aber völlig ausreichender Farbbildteil rundet das insgesamt gut gelungene Werk ab. Beziehen wir auch das Buchdeckelfoto mit ein, so sind nahezu alle Zeitabschnitte der Einsatzgeschichte und Betriebszustände der Baureihe E 41 darin abgebildet, inklusive aller Farbgebungen und Sondergestaltungen. Ein Teil davon fällt noch in eine Zeit, als kaum ein Fotograf Farbfilme verwendet hat.

Auch das macht dieses Buch für den Vorbildfreund und Modellbahner so wertvoll. Vergleichbaren fachlichen Tiefgang bieten andere Verlage zudem selten bis gar nicht, weshalb auch an diesem Band wieder kein Weg vorbeiführt.

Erheblichen Anteil am guten Gesamteindruck haben auch die vielen Zeichnungen, Abbildungen und schwarz-weißen Fotos, mit denen die Textaussagen vollständig, aussagekräftig und perfekt dokumentiert werden. Auch sie sind folglich gut ausgewählt und wieder einmal bestens wiedergegeben worden. Und so möchten wir in Summe keine Seite und keine Zeile missen!

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Photo memories of the steam locomotive

A long forgotten epoch

True steam friends think they've seen almost all the shots made by their favourite machines and ever released. But what did the Deutsche Bundesbahn look like from the point of view of railway enthusiasts from other European countries? The Transpress publishing house is currently demonstrating this to us, as we would like to point it out here. And we can already reveal so much: You certainly don't know the photos shown there yet!

Andrew Fox
Dampfloks im Wirtschaftswunderland
Die 50er, 60er und 70er Jahre

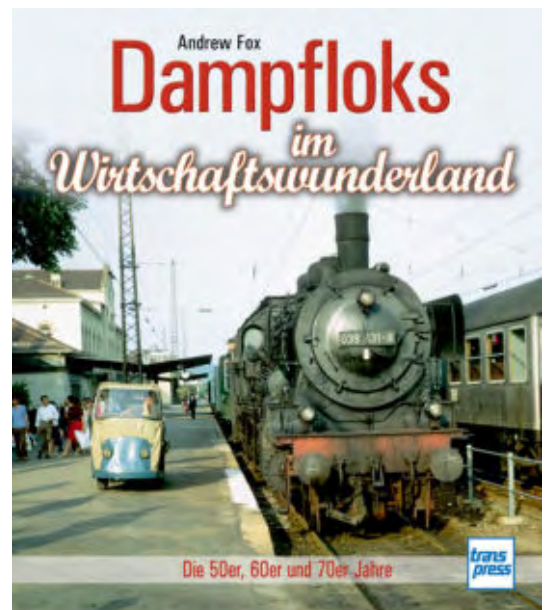
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or in the specialist book stores

This book is one of the great discoveries of the past year. More than forty years after the end of the steam locomotive era at DB, pictures still appear that German readers had never seen before - and all in colour!



Long gone are the times when the elegant class 01 had found its last refuge in Upper Franconia, but coal and steel trains in the Ruhr area were still transported with steam as a matter of course. In the Weserbergland the sound of the class 44 through the valleys was still heard daily and the Emsland route was still dominated by “king steam.”

But in 1977 all this was irrevocably over, in the Federal Republic an epoch had finally come to an end. Before the time came, many steam enthusiasts from other European countries also made the pilgrimage to the last places of use. This illustrated book by Andrew Fox summarizes many of these impressions and pays tribute to the last decades of steam traction in West Germany.

This is also a special feature of this book, because the few years after 1970 are by no means to be found again in it, in which the variety and number of the series used had already shrunk considerably. At first, the subtitle, which focuses on the fifties to the seventies, still had a very exaggerated effect on us.

It took a long time before we came across a picture while leafing through it that dated before the mid-sixties. Then we thought the cover of the book had promised too much. But far from it, because there was still a lot to come that pleased us in this respect.

And so we found in the sum a very balanced volume, which captured moments from three decades and was able to reproduce them consistently by regions and topic headings. Such themes are the development phases of German steam locomotives according to the Länderbahn, Einheitsbauart and Neubaullok (Länderbahn, standard design construction and newly built locomotive) of the DB or the use of normal and narrow-gauge steam locomotives for private railways.

The result is an abundance that is admirable and makes this book so unique, because hardly anyone seemed to be using up their film material to the left and right of the DB at the time, even less so with colour films which were still enormously expensive at the time. A little melancholy arises when the last chapter shows discarded machines that have to follow the path of all old iron. But that was to be guessed at the latest, when we met 10 002 as a heating locomotive in Ludwigshafen.

Without exception, the photographs were taken by English railway enthusiasts who had submitted their entire photographic collection to the Online Transport Archive, so that it could be preserved there for posterity, comparable to the German Railway Foundation.

In this respect, it may come as no surprise that there is also an English original of this book, which was published by Unique Books in the same year under the title "West German Steam in Colour 1955 – 1975" (ISBN 978-0-9957493-3-7).

Is there anything to criticize? The photos show their age almost throughout, which is of course due to the celluloid material. This includes the fact that the colour films, depending on the manufacturer, had a typical colour cast. This could have been reworked in the image processing, but here we are of the opinion that this would have damaged the authenticity of the images.

However, it would have been worthwhile to edit a few pictures with only a few exposures, in which no details can be seen in the chassis area of the steam locomotives photographed. Sometimes this also affects the immediate surroundings.

A slight brightening would have significantly improved the image reproduction here and made the motifs concerned much more appealing. This is clearly demonstrated by the reading sample provided by the publisher on the Internet. All other photos, on the other hand, fit well under this rating point.

The readers are also always given some information about the pictures as well as the routes that are currently being treated in a sequence of pictures. This is to be emphasized particularly, because straight the private railways and also many branch lines of the Federal Railroad are today long ago history, and to a large extent have been forgotten.

This title is not only an ordinary documentation, but also a testimony of contemporary history. For Zetties especially valuable are surely the many photographs, which show railway vehicles, which were or are offered to them also as models.

Particularly current is the recently delivered 064 136-5 from Märklin (see news in this issue), which was photographed in Lauda on August 5, 1970. The blue Ford freight car, which they were presented with in 2009 as an Insider-Jahreswagen (Insider's Club annual car), was a curiosity that is still strange for many today.

The fact that there is a model for this and what it looked like exactly can be seen in two coaches that were captured for posterity, driving behind a class 44 freight locomotive on the also well-known double-decker bridge at Bullay.

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Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details see imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative image is our goal. Likewise, here we note any events or meetings with a significance to Z gauge reference, if we are informed in time.

Important addition to an FR new product from Trainini® 2/2019:

For additional information and consideration: The Freudenreich ribbed wagons presented here Z-Stammtisch-Wagen Wien (Z Club Vienna wagon). The wagons are of a new construction, for the first time with a drop window and therefore fit most appropriately into epoch III. They complement our half luggage wagons of last year. They will appear with two company numbers and as smoking or non-smoking wagons.

Orders via the Stammtisch Wien are already possible (by e-mail to [Spantenwagen\[at\]gmail.com](mailto:Spantenwagen[at]gmail.com)). Delivery is scheduled for the 2nd quarter of 2019. I would like to thank you for taking this information into consideration

Helmut Engelbrecht (Z-Stammtisch Wien), per E-Mail

Enthusiastic reactions to the pink bullet train:

It does not happen very often that our readers get as excited about an article as they did about our report on the Hello Kitty Shinkansen 500 which entered service in Japan nine months ago. Even though we did





It is an instant hit, wherever it appears: The Shinkansen 500 in Hello Kitty design is simply one of a kind. We can now also show you one of the regional motifs on the sides of the carriages, here the example of Fukuoka (bottom right). Photos: Raffaele Picollo

not receive any letters from readers, we had animated conversations, personal feedback and enthusiastic contributions in forums on the topic, also fuelled by genuine surprise amongst some readers that there actually is a real-life prototype behind the Rokuhan model.

West Japan Railways, the train's operator, also sent us an explicit message of their enthusiasm for this article through a European representation.

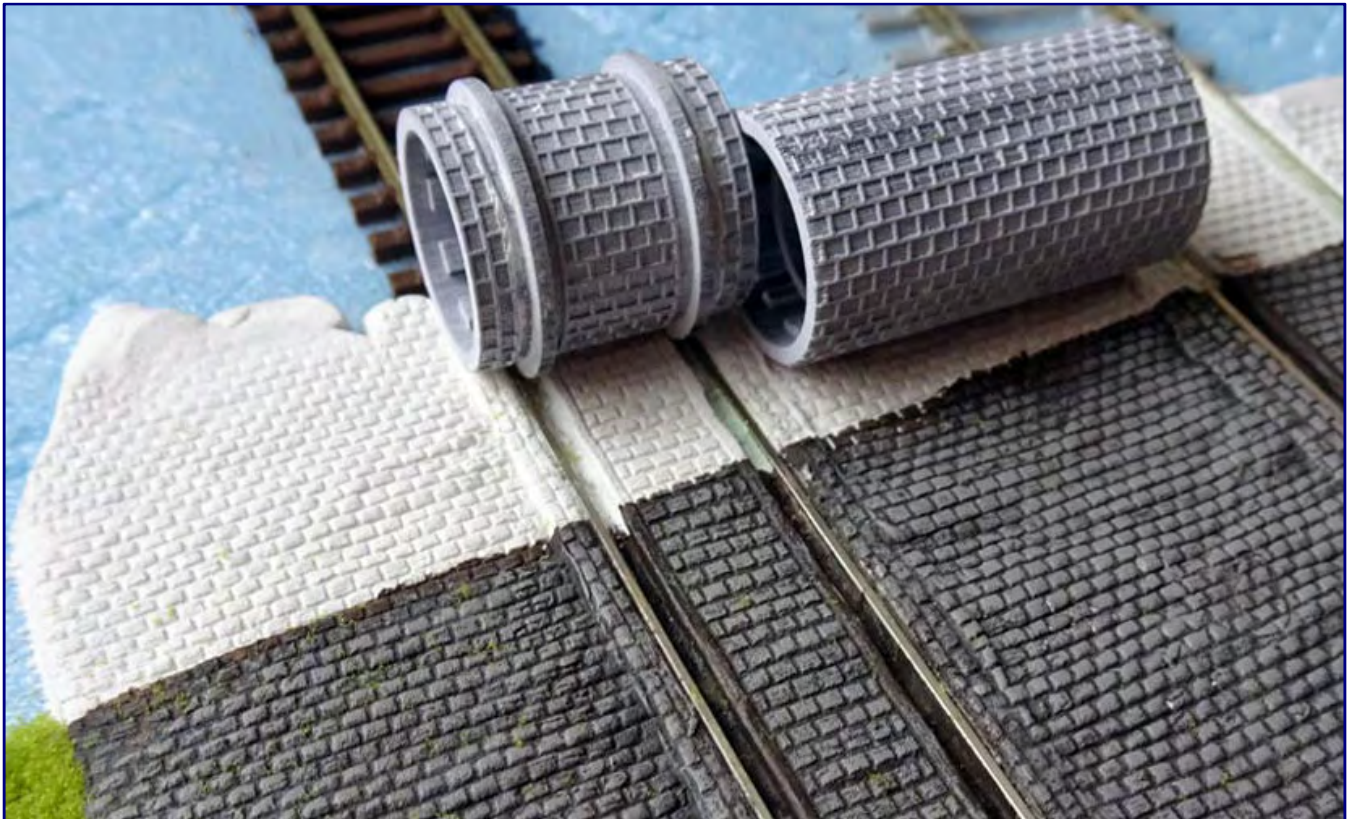
Similarly impressed was our reader Raffaele Picollo from Genoa, who sent us additional images of this high-speed train.



Innovative tool from Modellbahnunion:

Modellbahnunion (<http://www.modellbahnunion.com>) presented a clever innovation shortly after the International Toy Fair. The road roller for various sizes comes from Rainscale, but the tool is manufactured by DM-Toys in Germany.

For the Z and H0f tracks, a combination of a roller for 28 mm wide roads and another for 22 mm with an embedded track of 6.5 mm gauge (Art. No. H9535) is intended, for example, to simulate a tram line or an industrial track.



With the road rollers from Rainscale (Art.-No. H9535; picture above) you can also create attractive Z gauge roads with paving. Recommended as a working material is the modelling clay from DAS (photo below left) in white or terracotta. Photos: Modellbahnunion

Individual stone pavement streets can be easily created with this tool by embossing the structure into a rolled out modelling clay by rolling it. Modellbahnunion recommends the air-drying DAS modelling compound, which is also available in white (150 / 500 / 1,000 g) or terracotta (500 / 1,000 g).

Since there is nothing connecting the tracks H0f and Z apart from the track gauge, the size of the paving stones embossed on the roller is also important:

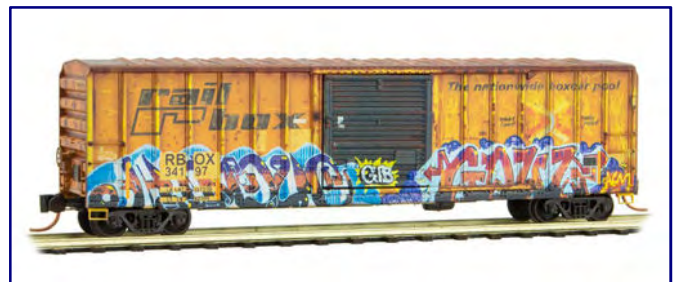
with a size of approx. 0.9 x 1.2 mm, it is optically well suited to the scale 1:220.

The following MTL models are currently arriving:

Micro-Trains is already delivering the second wagon of the Farm-to-table series. The brown 40-foot refrigerated wagon with wooden walls (Art.-No. 518 00 720) has yellow side walls and advertises the products of Joan of Arc.

The two covered cars with sliding doors of the CSX are much more modern in construction (510 00 431 / -432) and 50 feet long in the model. The externally visible box struts are striking. Without catwalks on the roof, they were in use around 2010 in this blue paintwork and with yellow lettering.

The yellow 70-foot carrying wagons of the TTX are in the range with two different company numbers (540 00 013 / -014) and previously ran under the name Trailer Train before the company renamed itself in 1991. A version of this car type is also available without painting and lettering (540 00 000) for your own designs.



The covered wagon from Railbox (Art. No. 510 44 017) is designed differently on both sides. While one side was sprayed with a portrait of the writer Edgar Allan Poe, the other side shows traces of operation and various graffiti. Photos: Micro-Trains

Also in the series of specimens delivered with patina there is a very attractively designed specimen in the most recent deliveries: The covered 50-foot "Edgar Allan Poe" by Railbox (510 44 017) is technically identical to the CSX car already mentioned, but has a different design on both sides.

One side shows the regular, but operationally drawn, rail paint, smeared with various graffiti, while the roof shows only rusty traces of operation. The other long side has been sprayed all over and shows the portrait of the famous writer between two crows.

Micro-Trains products can be purchased in Germany from Case-Hobbies, among others (<http://www.case-hobbies.de>).

New product already available:

Bernd Heißwolf (<http://www.modellbahn.heisswolf.net>) previously announced the availability of the new GFR1500 speed controller for analogue operation of bell-type armature motors. We presented it only last month as part of our trade fair report.

Maximum output voltage and the behaviour of short-circuit monitoring can be set by configuration, while over-temperature protection and the fan control ensure safe operation.

The speed controller consists of a handy control unit and a power unit, which is available with or without housings. A control unit holder, extension cables and a safety transformer are also available as accessories.



Association award for Noch:

The "Idee + Spiel" toy association awarded the Wangen-based accessories manufacturer a prize at the International Toy Fair on 2 February 2019.

The awards honoured the trade-friendly Internet ordering platform, the sponsorship for "25 years of Eurotrain," the active holding of model building seminars and the very good sales development in 2018.

Sebastian Topp (Noch Managing Director), Jürgen Leiter (Noch Sales Director) and Andreas Schäfer (Managing Director Idee+Spiel). Photo: Noch



Photos of the single wagons from the artillery train for the railway gun K 5. Photo: Z-Panzer, Andrew Hart

Photos of the latest products from Z-Panzer:

Unfortunately the photos of Andrew Hart (Z-Panzer) came too late to show the complete artillery train from his production in the magazine. In the meantime the photos are available (see page 49) and so we would like to introduce the camouflaged units around the railway gun K 5, led by the German Army diesel locomotive WR 360 C 14, at this later point..

Lahnstein Model Railway Days 2019:

As soon as all the carnival revellers had packed their costumes, the Oberlahnsteiner civic hall opened its doors to present a model railway exhibition for the 17th time, which is now very well known throughout Europe.

The international mix of exhibits confirmed this repeatedly and revealed a look at alternative possibilities for the design of a model railway layout. Currently, layouts from publications of various specialised magazines were announced again and could now be viewed on site.

Furthermore the MEC (Model Railway Club) Lahnstein under Gerhard Lehmkuhler organises every year the biggest exchange market for model railways and accessories in Rhineland-Palatinate. The always high numbers of visitors confirm the concept.



The maritime diorama layout "Marienfeld" by Dietmar Allekotte was one of the highlights of the Lahnsteiner Model Railway Days 2019.

Of course, one of the displayed layouts stood out. The "Rimkov" locomotive test stand by Rik Martens on a scale of 1:32 really attracted visitors with light and sound. This video is recommended for those who want to know more about the model roller test bench:

<https://www.youtube.com/watch?v=210xRoIUz6k>

Other excellent layouts, like the "Saufbähnchen" ("Guzzle train") with the station Mühlheim in H0 by Rainer Frank and the 1:87 coastal layout "De Kempen" of the Modelspoorgroep Valkenswaard from the Netherlands impressed us.



Meanwhile more and more editors from different model railway magazines are on the exhibitions as “simple” model railroaders. This year, Martin Knaden was quite a familiar face, especially since he holds the position of editor-in-chief of Miba.



The locomotive test bench “Rimkov” (page 51) by Rik Martens and the “playpen for the Tssd” (picture above) by Martin Knaden represented the large scales. Dirk Kuhlmann had among other things moved to the track H0e and showed the “Durlesbacher Weg” (Durlesbach trail, picture below).

Of course, the scale 1:220 was also well represented. On the ground floor, Birgit Foken-Brock from Trafofuchs stood with her layout and had brought a large selection of model figures for sale. There were also products in the scales H0 and 0. Often the booth was overpopulated, we are happy about that!

In the foyer of the theatre hall our editor Dirk Kuhlmann stood with two diorama layouts ("Contra Costa" & "Le viaduc sur la vallée") in scale Z and the "Durlsbacher Weg" in H0e. His meanwhile quite rare appearances are now already "small events", as the "crazy dog" presents again and again an innovation or other ways in landscaping for our gauge.



This FREMO-arrangement by Patrick Bopp after US-American motives in scale HO could also be seen in Lahnstein.

In many conversations with the audience he explained the basic "modern" landscaping and the general design. At times he was supported by the well-known Miba author Bruno Kaiser.

Dietmar Allekotte celebrated his premiere with his maritime "Marienfeld" layout. The small peep-box, of course brought to a good viewing height, thrilled the visitors and a flurry of flashing lights chased the next one. He is on the right track to represent our scale worthily in Europe.

The scenery is absolutely harmonious and invites you to take a mental stroll in the harbour area. We will soon be reporting in more detail about the "Marienfeld" in **Trainini®**, as this operating diorama layout will also be shown at the Intermodellbau exhibition in Dortmund.

News from American Z Line:

The light passenger coaches presented as part of our trade fair report follow in March in a further variant. Now the Southern Pacific is being considered, which had unpainted and therefore metallic shiny cars with a striking red stripe with a white company logo above the window ribbon.

This wagon type is offered as 4-4-2- (Art.-No. 73004-1 to -3), 6-6-4 sleeping cars (73104-1 / -2), dining cars (73504-1 / -2), luggage cars (73604-1 / -2), seating cars (73704-1 / -2) and observation cars (73804-1 / -2), each with different operating numbers.

The covered 40-foot AAR freight cars are on the road this month for the Nickel Plate Road and are available individually (904310-1), in double (904380-1) and quad packs (914310-1).

You can find more manufacturer photos of the current deliveries at <http://www.americanzline.com>.

Current Märklin deliveries:

The freight wagons of the package “DRG-Flüssigkeitstransport (liquid transport)” (Art.-No. 82318) have arrived at the dealers. Included in this Era II compilation are two acid pot wagons and another two-axle tank wagon of the replacement type. All three models have a brakeman's cab.

The tender steam locomotive of class 064 of the Deutsche Bundesbahn (88742) with bell-shaped armature engine has also been delivered. Like its predecessors, the first model of the universally usable locomotive for secondary lines of era IV has welded water boxes and no Indusi equipment. The running characteristics have increased enormously with the change to the new drive.



Märklin has now delivered the Epoch IV version of the Bubikopf as class 064 of the Deutsche Bundesbahn (Art.-No. 88742) with bell-shaped armature motor, but unchanged without window inserts in the driver's cab side windows.

It is a pity that the side windows are not equipped with pane inserts ex works. Fortunately, this can be remedied with Ratimo spare parts. The smoke chamber support was sprayed red for the first time, because many model machines in their years of service below the smoke chamber were very generously provided with red paint.

If you want to have it exactly, you should paint the steps of the lateral ascents and the central one to the smoke chamber black, as well as the running surfaces of the short circulation black, until only a red side edge remains.

The museum steam locomotive 80 030 (88001) has a heavy brass housing. Its model comes from the Bochum-Dahlhausen Railway Museum, where for many years it carried a photographic coat of paint with Reichsbahn addresses, which Märklin converted into the model.



Looks like epoch II, but it is not: 80 030 (88001) is part of the museum locomotive series and externally reflects the last operating condition at the RAG. It was painted by the DGEG as operator of the railway museum Bochum-Dahlhausen but in photographic paint.

Technical equipment of the locomotive reflects the last period of operation at Ruhrkohle AG (RAG), to which the welded water boxes and the alternator to the left of the chimney belong. This also corresponds to the museum model. Optically, it belongs to the Epoch II with the colouring and the two-light peak signal.

Instructional film of the accessory company Noch:

On 22 February another instruction video was put online that had 31,000 hits within a short period of time. In just under 35 minutes, the accessory manufacturer presents the construction of a model railway layout with Noch products and other new products.

The demonstration object is the current trade fair layout, which will be open to visitors on many occasions this year. It was shown for the first time at the International Toy Fair 2019 in Nürnberg. The film can be viewed free of charge at <https://youtu.be/LzJ75hW8OCQ>. If you don't want to miss any of the future films, you can also subscribe to the Noch channel.

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German Magazine for Z Gauge



Small layout with format

Move to Mannheim
US Railroading Meeting

Introduction

Dear Readers,

I guess winter's over for good. Look outside and see how trees and bushes bloom, grow new leaves and bees buzz again.

The short time of the cockchafer (May bug) is now upon us. For the model railway, this means a temporary turning away for many, because instead of modelling in the basement, the warmer weather pulls them outside.



Dirk Kuhlmann
Editor

Holger Späing
Editor-in-chief

Enjoy this time, breathe in the spring air and think about what a spring layout might look like? Far too seldom this beautiful season finds its way into our creations. But what could be more impressive and fascinating than a flowering horse chestnut with its white, candle-like inflorescences?

As editors, we have resolved to spoil you again this year with the ideas and works from our readers. Axel Pieke introduces us to his small layout Torstburg, which he himself quite simply calls "Test track on a square metre (Mark II)".

It is a successful proof of starting small and drawing new motivation from the result. Such a manageable project is also ideal for trying out new techniques or products as it is for gathering and deepening one's own experiences.

If you do it like our author and venture to one's own works, we will gladly report about it! In the further course of the year, we will also contribute a number of suggestions for layouts with which we would like to stimulate and inspire your imagination.

By the way, temporarily putting the model railway aside is not a good idea: Märklin has just delivered a striking dream model with its class 236 diesel locomotive with roof cockpit in Nuremberg design, which is equipped with matching and still very recent freight cars. We took a close look at these in the latest issue.

The season of spring fairs and exhibitions is not quite over yet. After the Faszination Modellbahn moved to Mannheim, the new products from the International Toy Fair were on show, and at the beginning of April they were also on show in Dortmund. Today we report on the premiere of the Faszination Modellbahn at the new location and next month on Intermodellbau.

Also worth a visit are the many club exhibitions, one of which is sure to be near you. Markus Gaa and Jörg Hausmann have summarised how a trade fair was transformed into the Z scale meeting of US railways.

The large modular system around the segments, most of which were designed by Peter Simon, was both an attraction and a meeting point. That's why it deserves its pages in this issue. Have fun reading!

Sin-Z-erely,

Dirk Kuhlmann and Holger Späing

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We thank Markus Gaa, Jörg Hausmann und Axel Pieke for their reports und the Eisenbahnstiftung for photos

Date of publication of the German language version of this issue: 24 April 2019

Cover photo:

V 200 020 arrives in Torstburg with its passenger train. The station's reception building seems familiar in its structures and yet somehow strange. Axel Pieke has consciously redesigned well-known kits for his layout and is now proud to present his results.

Shape variant of the series 236

From the roof cockpit down

The V 36 series brought a wide variety of variants to the tracks. Märklin has benefited from this and so do its customers, who are now allowed to put the fourth version on the rails. The latest version with the striking roof cockpit certainly arouses the appetites of many a Zettie. We have taken a look at the locomotive and cars from the newly delivered compilation and report herein our impressions.

Märklin seems to be fully exploiting the wealth of variants of the diesel-hydraulic diesel locomotive of the V 36 series from earlier Wehrmacht stock. With the unique "Light Freight Train" (Art. No. 81772) for the MHI, the fourth version was recently launched on the market.

Since the cars also still have an almost "fresh" construction, we take this as an opportunity for a closer look at the train set mentioned above. We have already extensively presented and tested the basic construction of Märklin's V 36 in **Trainini®** 9/2016. The strengths, weaknesses and mistakes of this model also apply in principle to this latest implementation.



236 123-6, the template for Märklin's latest V-36 model, helps with the electrification work at Eutingen station on 4 September 1973. Photo: Wolfgang Bügel, Eisenbahnstiftung

This means, for example, that the locomotive as a whole has a cohesive look, but its wheels are still too large in diameter and therefore seem out of place. The engine is driven unchanged by a bell-shaped armature motor with a small flywheel, which gives the model good slow running characteristics. After all, the model with a maximum speed of 60 km/h, and later only 55 km/h, was not a racing machine.



236 123-6 also stands out clearly as a model from the first Epoch IV model in this series. It is accompanied by three low sided wagons of the Klm 441 type, to which suitable agricultural cargos are also supplied.

As with all previous editions, the direction-dependent lighting, with warm white and red light-emitting diodes, is also convincing this time. The main lighting illuminates three lanterns in an exemplary manner, while the tail light illuminates just as correctly only the two lower lamps.

As with the first edition for Insider Club members, the focus this time is also on Era IV.

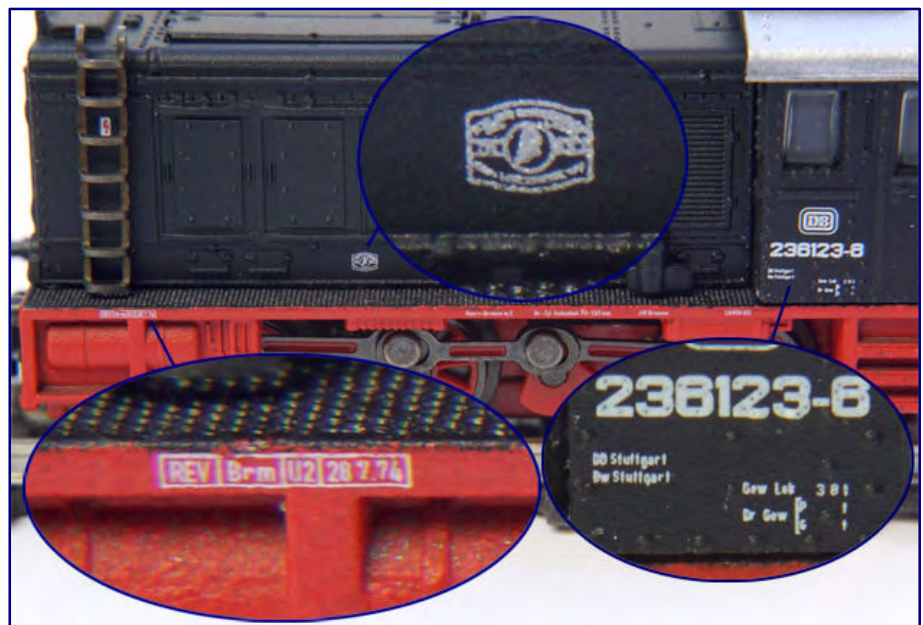
Nevertheless, there were still enough possibilities to distinguish the model from both halves of the "doppeltes Lottchen". On these deviations we want to focus in large part our discussion today.

But let's start with the acid varnishing in silk matt appearance and correct colours.

The white operating labelling and the (still existing) factory plate in silver colour are applied to it by pad printing.

The latter could no longer be printed flawlessly, but it is good to be able to recognize as such the Berliner Maschinenbau-Aktiengesellschaft (BMAG, formerly L. Schwartzkopf). Its model was built there in 1940 with the factory number 11382.

The imprinted last inspection date shows an investigation U2 in the AW Bremen, which was completed on 28.7.74. The inspection was completed on 28.7.74. The read off home at the BD Stuttgart fits to this



This time, too, the flawless printing is beyond any doubt. Only the BMAG factory nameplate (detail enlargement above) is no longer flawless due to the larger metallic pigments.

last section of operation of 236 123-6: It was put back in the Bw Stuttgart on 14 December 1977 for repair and finally taken out of service on 30 March 1978.

It is also worth mentioning that this time the high voltage electrical warning signs were printed on both sides of the engine hood and even in two colours with a red high-voltage sign on a white background. Whether it should be on the right side without a ladder remains open. The selected font of the locomotive number appears unusual, which already has caused some customers to doubt its correctness.



The high voltage electrical warning sign on the right long side at the front end of the bonnet makes no sense, because there is no possibility for personnel to climb up and therefore no danger of electric shock under contact wire (see also model photo on page 4).

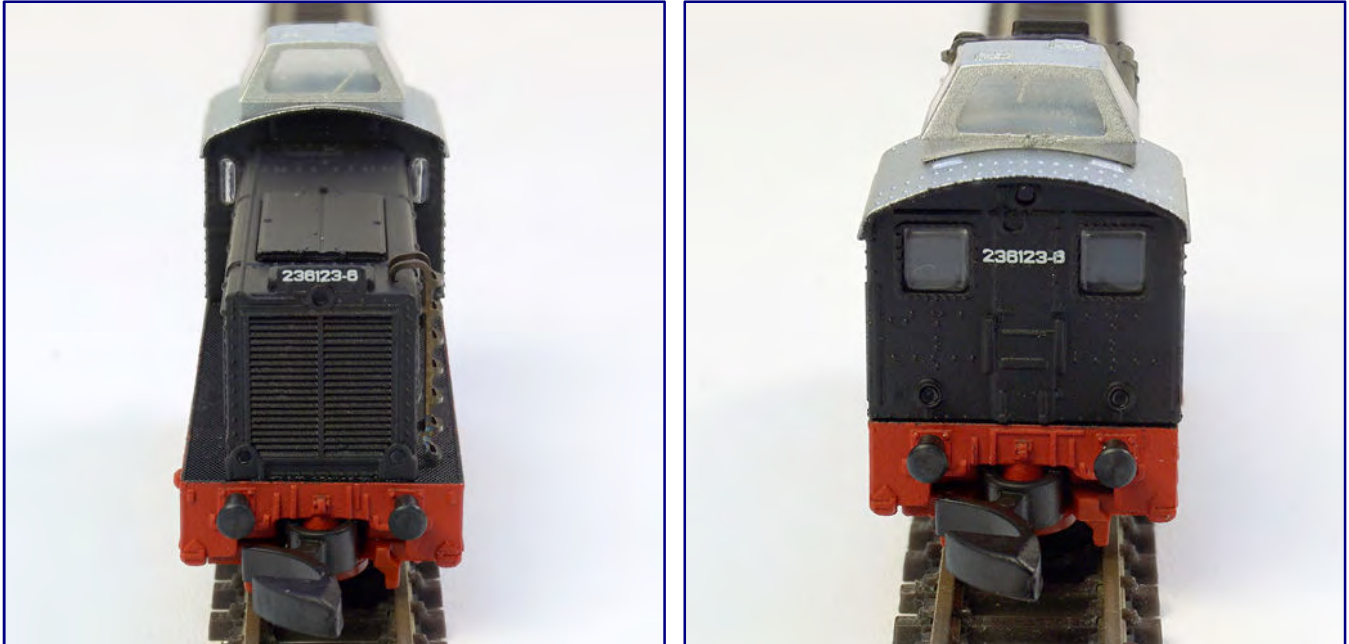
The default standard font was DIN 1451 "Mittelschrift DB", but it was precisely this font that was frequently not used in the V 36. This even applied to this template, which even received its computer number in wide font. Märklin correctly took this into account on all four sides. The missing gap between the series number and the company number, which is also documented, also deviates from the standard.

By the way, with this model Märklin has for the first time converted a locomotive of the V 361 subclass series from DB stock. Compared to the V 362 subclass, however, there are no external distinguishing features, as the number range was once assigned according to the type of engine used.

A striking external feature, however, is the design of the cooling water expansion tank. In order not to restrict the locomotive driver's view, 236 123-6 had a flat design in front of which the locomotive number plate was emblazoned.

In our opinion, Märklin made a mistake with the ladder on the left side of the vehicle: The DB retrofitted their machines with flaps, which covered the rungs and prevented a careless ascent under contact wires. The high voltage electrical warning arrow sign was always on the flaps.

The flap is unfortunately missing from the Märklin locomotive, although there is a suitable stamped part from the previous versions. The warning symbol is printed on the hood under the ladder instead. However, this error can certainly be overcome, otherwise it could be remedied by retrofitting parts. After all, this is a special prototype version, that should be in high demand.



The windscreen wipers have also been printed on the roof cockpit. Correctly, they are guided from below on the front side and from above on the rear side. However, Märklin forgot the horn of the prototype directly in front of the driver's cab, which was placed at a right angle to the direction of travel (photo left: cf. photo on page 4).

After all, 236 123-6 has a special feature that cannot be overlooked from afar: Its model was converted to one-man operation and was therefore from 26 February 1954 operated with a roof cockpit of the type Nuremberg. Since then, the locomotive has been at home throughout Bavaria at various depots before coming to Stuttgart in 1972.

The prototype for the new model is, by the way, the only one of 21 prototype locomotives with a roof cockpit that survived. Today, it belongs to the Dampfbahn Fränkische Schweiz (DFS) and is used there for museum trips. Thus it fits, we see beyond the operating labelling of the seventies, and also it could be used as a locomotive in front of the passenger cars from the starting pack "Museum Passenger Train" (81871).

Märklin has skilfully implemented this cockpit. It is a colourless, transparent injection-moulded plastic part that is cleverly printed or stencil-painted to reproduce the roof and window frame correctly. Even the windscreen wiper was inscribed on both front sides of the cockpit. At the front the wiper is guided from below, and at the back it has its pivot bearing at the top.

The windows are quite matt in order to prevent a view into the cockpit, which is placed on the roof by snap tabs. The reason for this is probably that the cab ceiling has no opening in the area of the cockpit and the clear view could otherwise rob the illusion of an elevated control station.

However, it seems attractive to us that the light from the locomotive lighting does shine through into the cockpit. If the model moves backwards, it looks like driver's cab lighting. If, however, the model is moving forward, the red light shimmering into the roof cockpit certainly raises questions.

Another question also arises as to how the locomotive driver should acoustically draw attention to the approach of his train? With the V 36, there was a true variety of acoustic signalling devices: Compressed air whistles, later horns on the cab roof and mostly a bell on the long hood.

The latter was mandatory for approaching unsecured level crossings on secondary lines. Since the class 236 had hardly been used in line service in the last ten years of operation and had long since disappeared from local passenger traffic on secondary lines, this made the bell superfluous. 236 123-6 does not properly have this horn any more.



The overall impression of the model is very good, but it is not free of minor mistakes. One such is the missing protective flap on the ladder, where the electrical warning sign would have appeared.

But what about a pipe or a horn? Without one of them, it would not be approved by the railway authorities in Germany. In order to resolve this, you should note that: The prototype had a modern horn lying across the hood directly in front of the driver's cab. Unfortunately, in the model it is missing. So there is also a fine detailing potential for the MHI one-time edition at this point.

Three low sided cars

The delivered set also includes three low sided cars, which deserve a brief look. These are further versions of the Rmms 33 stanchion cars, which were redesigned last year.

Three examples are included in the train package as later representatives of the genus Klm 441 and are used here for agricultural transport. All three models appear here without a handbrake platform and aptly reproduce the later version with steel sideboards, which were probably the only ones still on the road in the seventies.



The low sided cars in the train pack “light freight train” (Art.-No. 81772) belong to the genus Klm 441 and were until 1978 only without stanchions (photo above). But the grey painting of the loading area was considered. In addition to a hay bale load insert, the package also includes a new, very convincing tractor model with matching trailer (photo below).

Contrary to the product illustration in the catalogue and on the Märklin pages, however, the three new models have firmly glued forehead stanchions again. That's no big deal, but it doesn't fit in with the design, which is now only available as a low sided car. It would therefore be desirable if they could be removed, because it is precisely in this form that the excellently designed models fill an important gap.

For the sake of completeness, it should also be mentioned that matching side stanchions are also included in the package. They are inserted as complete mouldings for self-separation under the deep insert of the carton.

Märklin has also thought of suitable loads. We already know the load of hay bales from the first edition, and it proves to be a good idea here as well, because it is refreshingly different from the usual transport loads.



On the low sided cars it is worthwhile to highlight the finer details (photo above): perfect engravings and also flawless printing on the wagon frame. The finely designed tractor also deserves a look from the front (photo below). The eyelet instead of the perhaps expected eye on the trailer drawbar is by the way no mistake, and allows coupling to the tractor.

Also worth noting is the tractor with a flat trailer. Painted red and equipped with profiled tyres, the rolling cast models also look good off the track. The tractor also has a high, black exhaust.

Märklin has put a lot of effort into this successful addition with the colour differentiation of the radiator inlet and a beige decorative strip on the engine hood. It's almost too bad to load the tractor and trailer onto the other two Klm 441s. The only thing that speaks for itself in this respect is their brand new appearance due to the absence of any dirt.



There is no need for a separate train formation recommendation, because Märklin has already thought of everything. In the seventies, the class 236 diesel locomotives seldom went on main lines and, if so, then safely with short freight trains like this one.

So a busy farmer on a scale of 1:220 can look forward to his new agricultural equipment. However, the complete set of the "light freight train" will certainly continue to run for a few rounds on the layouts of its buyers.

Manufacturer of the models:

<https://www.maerklin.de>

Operator of the operational examples:

<https://www.dampfbahn.net>

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Axel Pieke's small layout

Test track on a square metre (Mark II)

Axel Pieke has been meddling in Z gauge for a long time. But a true model railway enthusiast never stops learning. And so he began working on a small-sized layout to implement new ideas and to hone his skills. In fact, the project presented here is already part 2 of a test series. Today, the native of Bremen, shares with us the aims and specifications of his project.

By Axel Pieke. How did this project's working title "Test track on a square metre (Mark II)" come about? Well, this simple, matter-of-fact name just reflects the fact that this layout serves the purpose of testing new techniques, kits and ideas which I had not tried or used before, for example, kit bashing and working with resin kits.

The version name "Mark II" simply means that there was a predecessor to this layout. And just like this latest layout now, the previous one also adorned the shop window of my brother's business "Optik Pieke" in Bremen-Vegesack.



View of the small town of Torstburg across the station roof: We are presented here with a harmonious panorama of structures and landscape whose construction is described in the text.

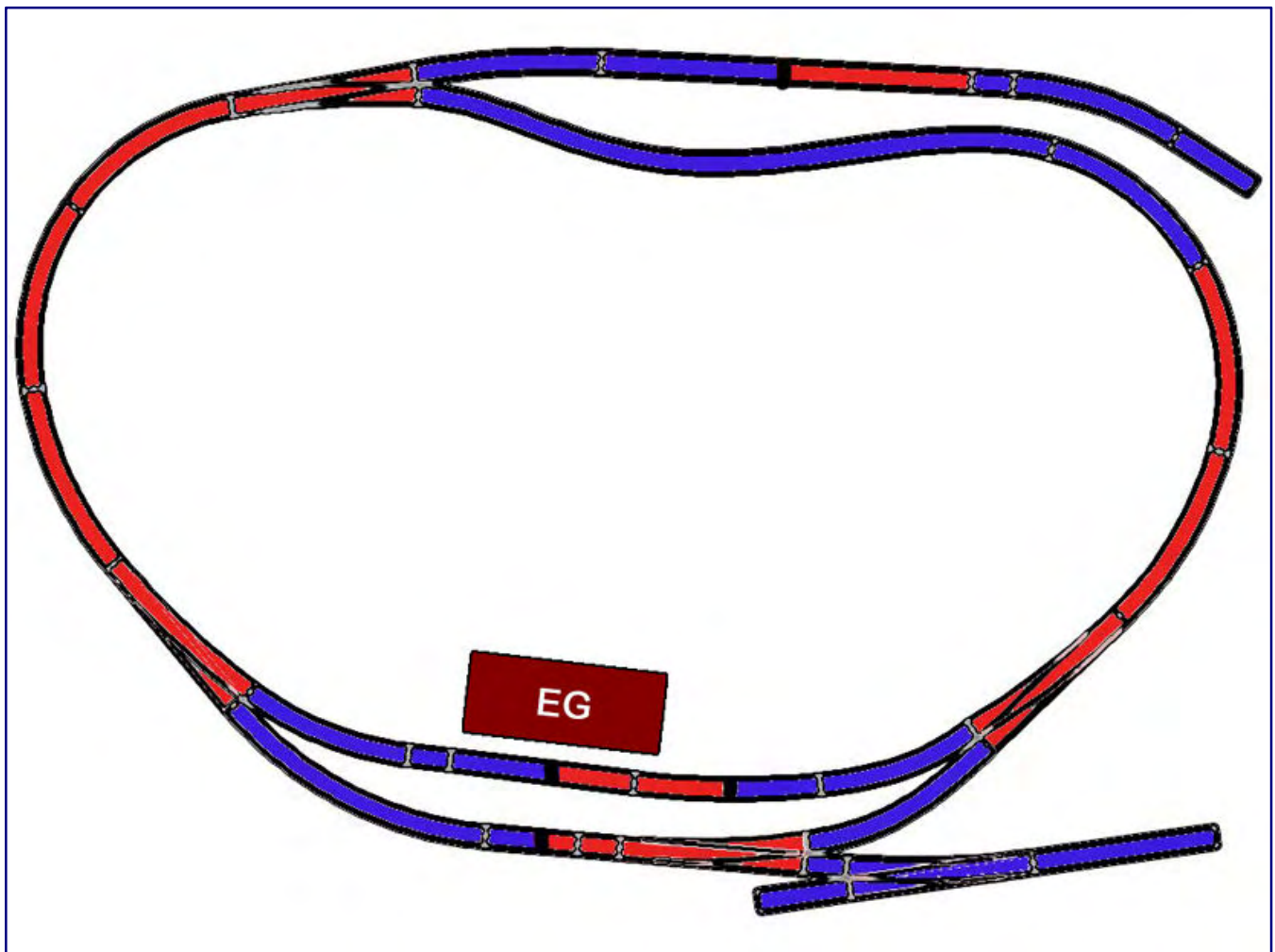
However, that first layout had some teething troubles, which I wanted to avoid with the new project. The main problem was the gradients with its figure-8 track plan. Given the limited space in the shop window, they were quite steep. For the new layout I decided, therefore, to stick to a track plan on a flat base.

In addition, I wanted to include the following elements with the layout:

- a castle,
- a small town with a church,
- a station with a small siding,
- a small farm, and
- a tunnel with a small staging yard, more like a siding.

Thematically, I envisaged a single-track main line or branch line with diesel traction. The system was to be controlled and operated fully automatically and digitally using the Rocrail programme. The FCC central control unit and function decoders are from MTTM, i.e., Selectrix is used for switching.

In the end, I came up with this track plan:



The design of the track diagram also takes into account the block division (highlighted in blue). Illustration: Axel Pieke

Baseboard and track

With the track plan decided upon, I could finally start with the construction of the layout. The frame of the baseboard was assembled from square pieces of timber. A plywood track bed was then fixed upon the frame and covered with a layer of 2 mm cork for noise reduction and in order to facilitate track laying.



The midday passenger train arrives on track 1 at the Torstburg railway station. Some passengers will use it as an important link in the long-distance connection from the next larger city to reach more distant destinations.

I then converted the points to under-table drive by first removing the black solenoid mechanisms and then drilling the necessary holes into the baseboard. Following that, I used pins to temporarily attach the tracks to the track bed, always making sure that the joints were perfectly aligned.

The tracks were then ballasted and permanently fixed with a mixture consisting of wood glue, water and liquid detergent. The pins were removed once the glue had dried.

The ballasting of the points was left for a later stage, given that this work requires extra care. For point operation, I installed servo motors below the track bed.

In order to be able to carry out a fully automatic digital train operation, each track block had to be connected to an occupancy detector.

Once this was done, I had reached my first milestone: operating trains, albeit still with provisional wiring.

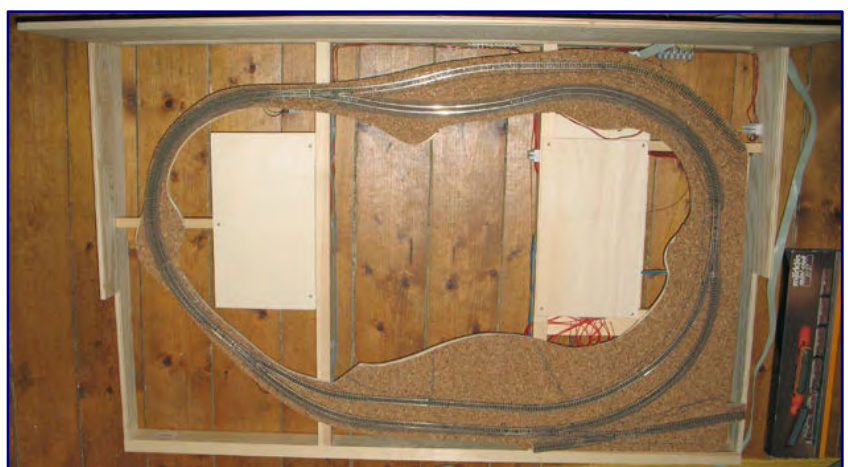


Image showing the baseboard frame with the track bed and laid tracks. Photo: Axel Pieke

At this stage, it was still possible to test extensively tracks and points and to eliminate errors before some sections of the track were to disappear under the terrain. From this point on, the Rocrail control software was also set up and tested for various automatic operating routines. This was the moment when the fun of playing with trains already started...

Terrain modelling

The terrain of the layout was to consist of three levels. The trains are running on the lowest level and stop at the fictitious "Torstburg" station. From the station, a connecting road leads up to the small town of "Torstburg", located on a plateau on the left side of the layout.



Along the road are a small farm and two residential buildings. A hiking trail on the far end of the layout connects the town with the ruins of its namesake, Torstburg castle.

A small gorge between the castle and the town opens up views of a short stretch of track, thereby adding movement and extra visual interest to the back of the layout.

While working on the middle level of the terrain, I realized that I had planned for too much space for the hiking trail. This was corrected in the next steps.

As for constructing the terrain structure I went for the classical approach: the supports for streets and buildings were cut from plywood and bent into shape with water and an iron, in order to avoid tension in the plywood panels once I would fix them onto the layout structure.



In the right rear corner of the layout I also prepared a level area for the castle ruins, before constructing the rest of the landscape with sheets of paper and wire mesh.

The terrain surfaces were now formed with plaster. I applied coats which were sufficiently thick as to avoid piercing the underlying structure when modelling the dried plaster with a small chisel.

In the meantime, the second level for the small-town settlement has been inserted onto the frame (photo above). The rough terrain contours are then created with paper covered wire mesh (picture below). Photos: Axel Pieke

And if one should nevertheless break the surface, it can be easily repaired using the same materials.



The final forms of the terrain were modelled with plaster (picture above). With the colouring of the rock structures, the landscape increasingly acquires a "face" (photo below). Photos: Axel Pieke

When designing the terrain, I always follow a rough plan: I have an idea where to place the buildings, roads, tracks and tunnels. The approximate structure of the terrain is created when the wire mesh is formed and attached to the wooden supporting structure. The exact topography of the terrain, on the other hand, only takes shape once the plaster is applied and has been worked with a chisel.

In the next steps, I coloured the background and the rock structures. For the backdrop, I opted for a cloudless blue sky. As I wanted to use spray colours, I first covered and sealed the layout with tape.



One can only guess how much work went into this layout: Here is a view of the elegant and quite effectively designed S-shaped road leading from the small town down to the train station.

I then applied a coat of primer to cover up the wood structure of the backdrop board, followed by an even base coat of blue colour. Afterwards, I sprayed white colour onto the blue, starting with plenty of white cover on the horizon and increasingly less white the higher the sky, with just a faint of white mist in the uppermost part of the backdrop.

Realistic looking roads

The road is divided into two parts. The first part is the one running through the small town and required sidewalks, kerbs and manhole covers. The second part, the connecting road to the railway station, is more like a country road.



Modelling sidewalks and streets: 3 x 3 mm squared timber with notches imitating joints was used to create the kerbsides. A placeholder was installed to reserve some space towards the back of the layout in order to later insert a low relief row of houses (top left). The next step (top right) consisted in removing the placeholder and levelling the tarmac with a spatula. This was followed by painting the road (bottom photo) and fixing the final position of buildings and the cemetery with the help of their plastic bases. Photos: Axel Pieke

For constructing the town street, I took a 3 x 3 mm square timber and, with a saw, cut notches every 4 mm. Afterwards, the road was modelled with plaster. Since I planned for adding a relief with a row of houses along the backdrop, I made sure to leave some extra space for that when modelling the street and sidewalk.

Reserving a bit of extra space was necessary, because this section of the urban backdrop scene was to be glued onto a plywood panel in order to create some illusion of depth. This also was going to give me some room for installing lights behind individual windows. More detail on how I designed this background scene follow below.



After completion of the design work, the small town street shows itself with visible kerbstones, realistic colours, manhole covers and lanterns (photo above). The relief houses with their front gardens can only be perceived as such from close up. The connecting road to the railway station (photo below) does not need sidewalks and has only unpaved side strips. Photos: Axel Pieke

After completion of the design work I used their base plates as placeholders so that the houses would not appear to have been added later. The result was a flat surface with plaster pavement, where the pavement seems to be made of one piece. As soon as the plaster had dried through, the base plate could be removed and used for building the houses. The street, the sidewalks and the curbs were painted with acrylic paint.

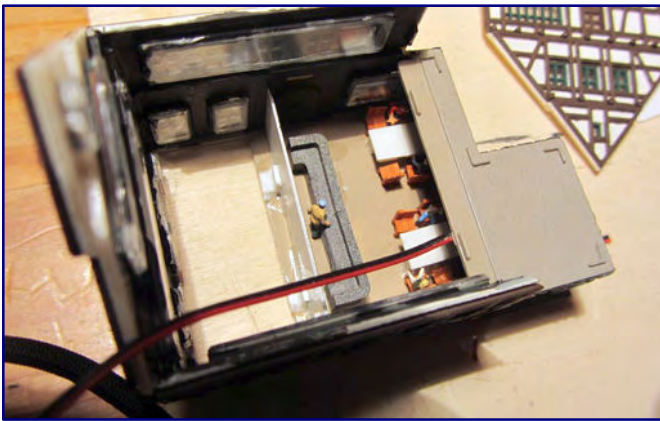
Street lights, fences, manhole and drain covers were sourced from "Rolf's Lädchen." For the installation of the manhole covers, I drilled small recesses of a suitable size into the street and placed the etched parts inside, making for a flat surface.

The rural connecting road did not get a sidewalk, but it was also constructed with plaster and coloured with acrylic paint. The unpaved sides of the road were modelled with grey gravel.

Modified buildings from standard kits

The residential and commercial buildings used for this layout are well-known kits from Kibri, Vollmer and Faller. Generally, I maintained the original forms of the buildings. I did add to most of them, however, small interior rooms in order to be able to illuminate only individual groups of windows instead of the entire building or floor.

Individual parts of the buildings were painted with acrylic paints before assembly, in order to prevent that unnatural plastic look. For additional variety, some buildings also got some details and colour schemes different from the ones proposed by the manufacturers. I also added interior details to some of the rooms. Unfortunately, these were not always visible once the buildings were placed on the layout.



A lot of effort went into the construction of the interiors of individual buildings (top left). Unfortunately, not much of that effort is visible later. The tower clock of the Faller church received a new clock face (top right). Vollmer buildings were arranged in such a way that they form a short promenade with shop windows (bottom). Photos: Axel Pieke

While constructing the small town, I also started my first attempts at kit bashing. For example, I built a small row of houses with shops from the Vollmer kits with the article numbers 9544, 9552 and 9553. These are kits for half-timbered buildings, and since this was the style I wanted to use for the town, I did make use of the timber-frame parts from the kits.



This image shows the lively small-town atmosphere of Torstburg with the short row of shops in the back which was kitbashed from Vollmer kits.

But first I had to think about the order of the houses in the row and how to achieve a harmonious looking ensemble. Could all shops be entered from the street side or were there any doors missing?

For example, the shop in the middle has a new entrance. Another example is the roofs: Here only the ones on the far sides of the row could remain as they were, all others had to be cut to size with my small milling machine.

During the assembly, I always had to plan several steps ahead and adjust components accordingly. If something had gone wrong, I could have bought another kit. But, for the modifications which I made, no additional parts were necessary beyond the ones already included in the kits.

Modelling the vegetation

All the vegetation on this layout was modelled according to established and conventional techniques. I used model trees with etched armatures from Modulator, a provider of architectural and artistic modelling supplies, and static grass and flowers from Mininatur (Silhouette).

Turf, in varying degrees of coarseness, and track ballast were obtained from Noch. Gravel in different colours and sizes for the roads and the landscape are from Minitec.

Any green areas were first given a base coat of brown and green colours. This was followed by a layer of different sizes of turf upon an adhesive coat of my home made wood glue/water/liquid detergent mixture. In doing so, I applied coarse turf only in some specific areas, and fine turf across all green areas.



The finished green areas consisting of different types of turf and static grass applied according to the procedures described in the text. Photo: Axel Pieke

The reason for this uneven application of different turf sizes was to create a more natural looking scrub like undergrowth. Once sprinkled upon the surface, I recommend lightly pressing the turf into the glue for better adhesion. Just make sure to work a limited surface area at a time, in order to prevent the glue from drying before the turf is applied.

Once dry, I removed any excess turf with a vacuum cleaner. Putting an old sock over the nozzle of the vacuum cleaner allows you to collect and reuse the loose turf. I found this modelling tip quite helpful and would like to pass it on here myself.

In case that too much turf should come off when vacuuming, or if the ground cover should not be dense enough, just repeat the procedures.

Once I had achieved a nice irregular looking ground cover of various shades of green, I applied another layer of glue and brought into action my static grass applicator. Unlike working with the turf, I applied the short grass fibres first, and the longer ones only at the end. I also mixed different colours and lengths in order to avoid the appearance of a closely cut English lawn.

The etched trees from the architecture and artists' supplier can, in principle, be used without further adjustments. I have, however, refined the tree trunks with a sawdust wood glue paste and brown colours.



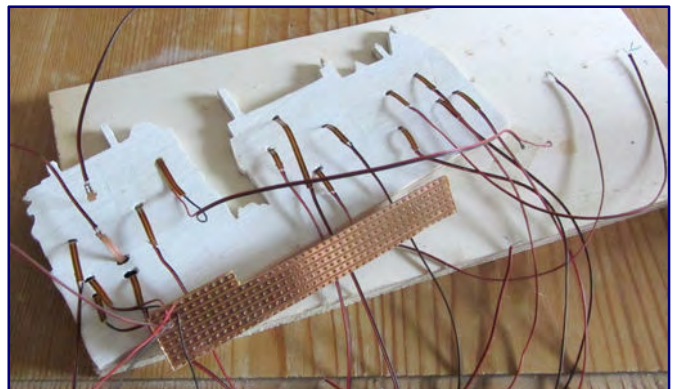
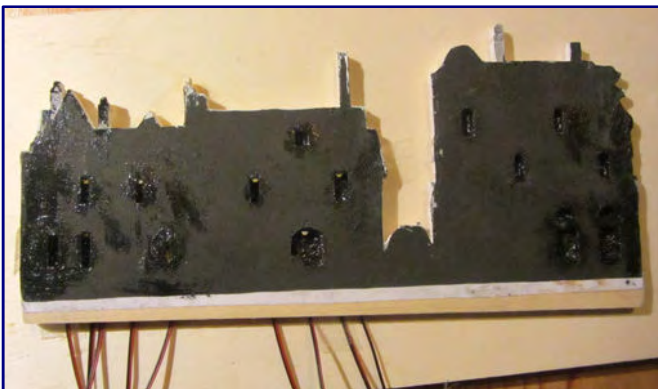
The trees are from a supplier of architectural modelling supplies and were slightly modified in order to attain more natural looking tree trunks.

The logs, which are smooth on delivery, have been given a rougher bark structure. The smaller trees, which had previously only been supplied with a semi-circular trunk, were given a real life like round trunk. The brown painting then took care of the rest.

Adding specific tweaks

In my introduction, I had already announced that trying out new ideas and kits was one of my main objectives with this project.

In addition to the blue sky, I also wanted to have a row of low relief houses at the back of the layout. The aim was to install LED lighting into the relief so that the houses would, during the “night,” visually match with the overall atmosphere of the illuminated layout and not appear as a black surface. Using different layers of plywood added to the three-dimensional effect.



The cut out relief with the milled window openings was pasted with black cardboard on the back side (photo left) and afterwards the LED was inserted and connected from behind (photo right).

I proceeded in the following way: First, I made a copy of scenic townscape backgrounds from MZZ, a railway modelling provider, and placed them onto 6 mm plywood. I then selected the position of the windows which I wanted to be illuminated and milled them out. In order to prevent any undesired light shining through on the sides of the windows, I made a stencil from black cardboard with holes for the lights and glued it onto the plywood.

I decided to use small SMD-LEDs as a light source, as they fit easily into the milled recesses. I soldered them at a distance of 2 cm onto an ultra-flexible circuit board.

One side of the circuit board was then cut in such a way that always one LED stood directly at the end of the board. In this way, a strip remained on the other side which was then pushed through a small opening which I had cut into the upper part of the window areas. The LED boards were then fixed with superglue and the overlay with the houses finally glued onto the plywood panel.



Photo above:
So the relief with the coloured background and illuminated windows looks at dusk.

Photo on the right:
The cemetery from Modellbau Luft is finished and awaits its installation on the layout.
Photos: Axel Pieke



I then arranged groups of four to five LEDs, which were to be controlled together by an MTTM decoder module, thereby adding some extra effect to the lighting.

I also added some additional structural effects to the relief buildings by applying some flock in those areas where the two dimensional print indicated the presence of vegetation.



The cemetery from the supplier Modellbau Luft was combined with Faller's village church to form a harmonious ensemble.

I continued with the construction of the cemetery. Like the "Drachenfels" castle ruins, the kit also comes from "Modellbau Luft." Tombstones, crosses and gates were included with the kit. The etched parts still had to be painted, whilst the small gypsum tombstones were ready to use.

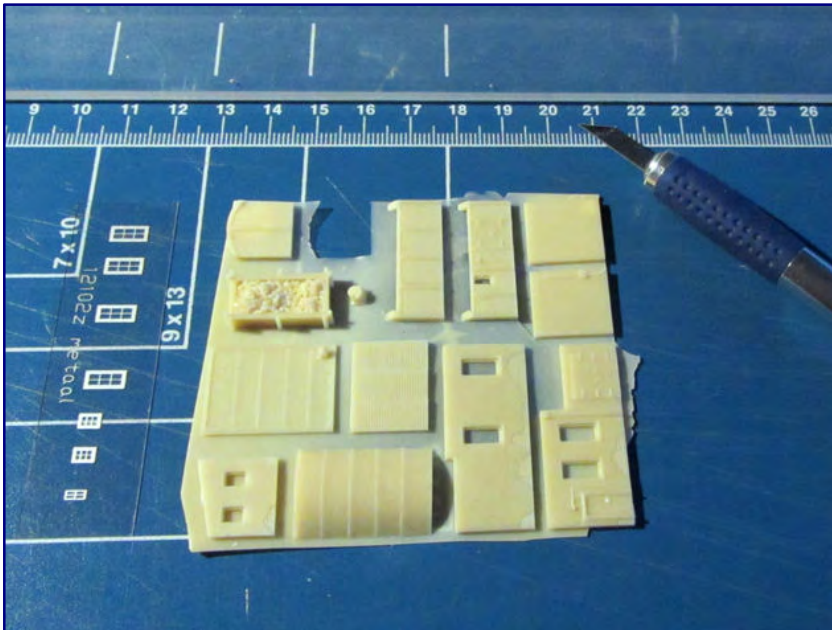
The pathways were modelled with fine gravel, because I did not like the aspect of a plaster surface. Material for blossoms came from Mininatur, which I used together with coarse turf to model flowers and bushes.

In the broadest sense, I made another small attempt at "rededicating" a kit by combining the cemetery kit, which normally includes the ruin of a church, with the village church from Faller. Altogether it was very easy to design the cemetery, because the good quality parts from Modellbau Luft. Combining them with the Faller church was a piece of cake.

Working with resin kits was a completely new experience for me at that time. In order to test this type of kit I decided to use Artitec's railway utility building.

A suitable place was also quickly found next to the siding at the station. Even though I had owned this kit for a long time, I had never really dared to put it together. As a first step, the individual parts had to be separated from their casting base.

In contrast to the other kits, I decided to assemble this one first and then colour it. At the end the windows were inserted and glued into place with superglue.



When painting the windows, I started with the large areas. For the plastered walls I applied the paint relatively thickly, in order to add structure and some flaking effect to the plaster on the walls.

I also carefully dry brushed the surface details in order to add additional depth.

The most delicate details were painted with a wooden toothpick.

Since the buildings were to be later illuminated from the inside, the walls had to be prepared to prevent light from shining through in the wrong places.

Initially, I tried to use cardboard templates. As this did not really work, I ended up applying a thick coat of black paint to the inside.

Only then did the light exit through the window openings as desired. Finally, the window sheets were glued into place.



The resin parts had to be cut from the base plate and reworked (top) before painting. The windows were attached only at the end. The finished result compensates for the effort (bottom). Photos: Axel Pieke

Finishing touches

Another beautiful kit is the one of a bus shelter, which, if my memory does not deceive me, was once produced by Roy Bergauer. I have actually used three of them for this layout. They are easy to assemble and paint, but these bus shelters do add an extra note to the overall atmosphere of the layout

The colouring was done in the same way as I did with the resin kit. The advantage of this etched metal kit, however, is that the material is not translucent. I used figures from Preiser and SMD LEDs for lighting.

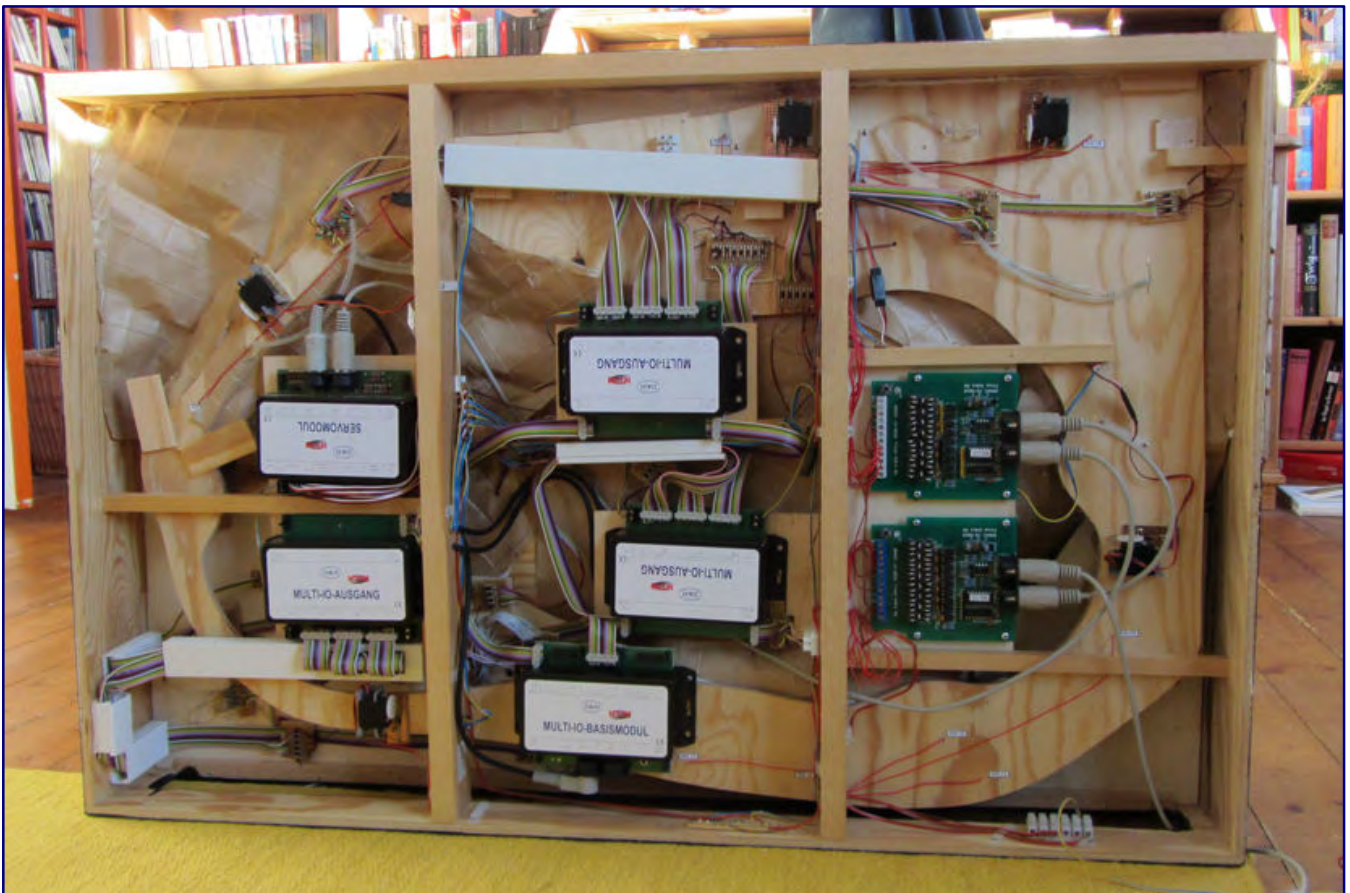
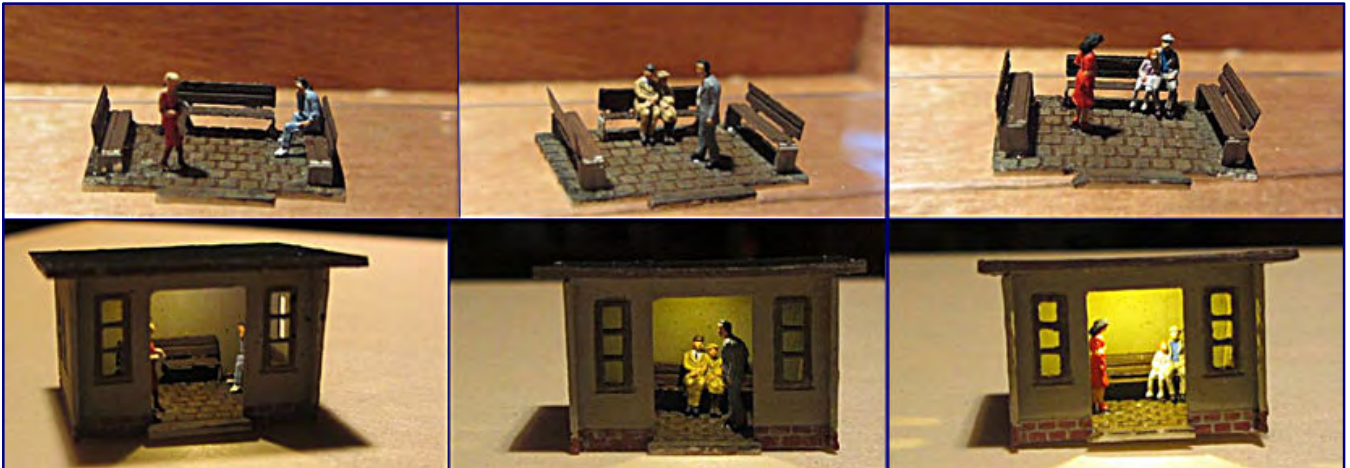


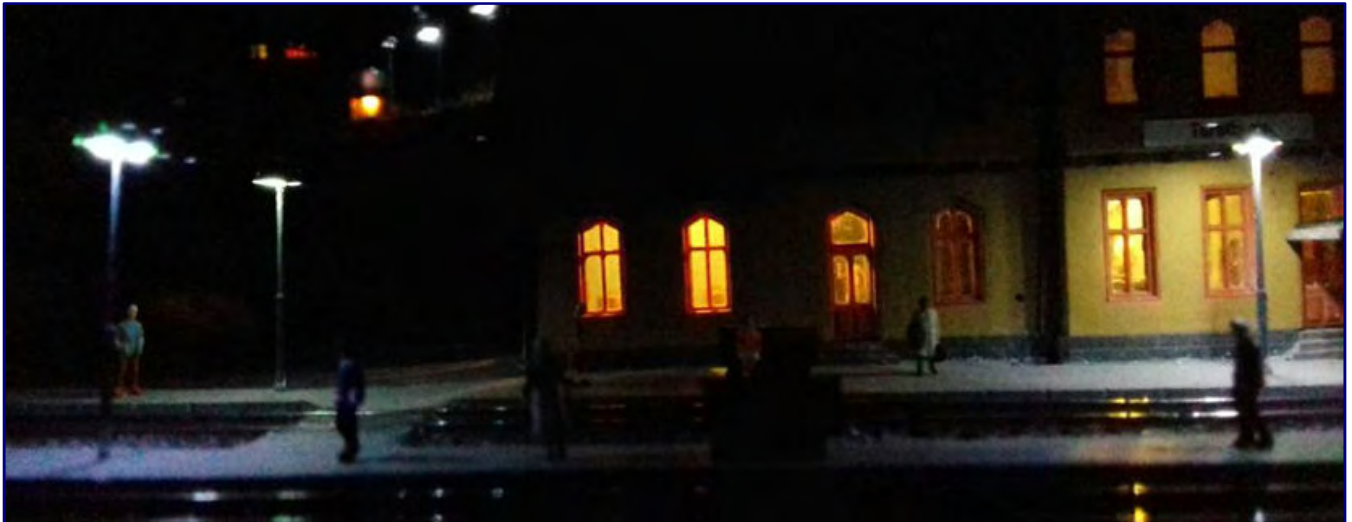
Photo above:
Three of Roy Bergauer's bus shelters have found a place on the Torstburg layout. Equipped with benches and occupied by different figures, each of them tells its own little story.

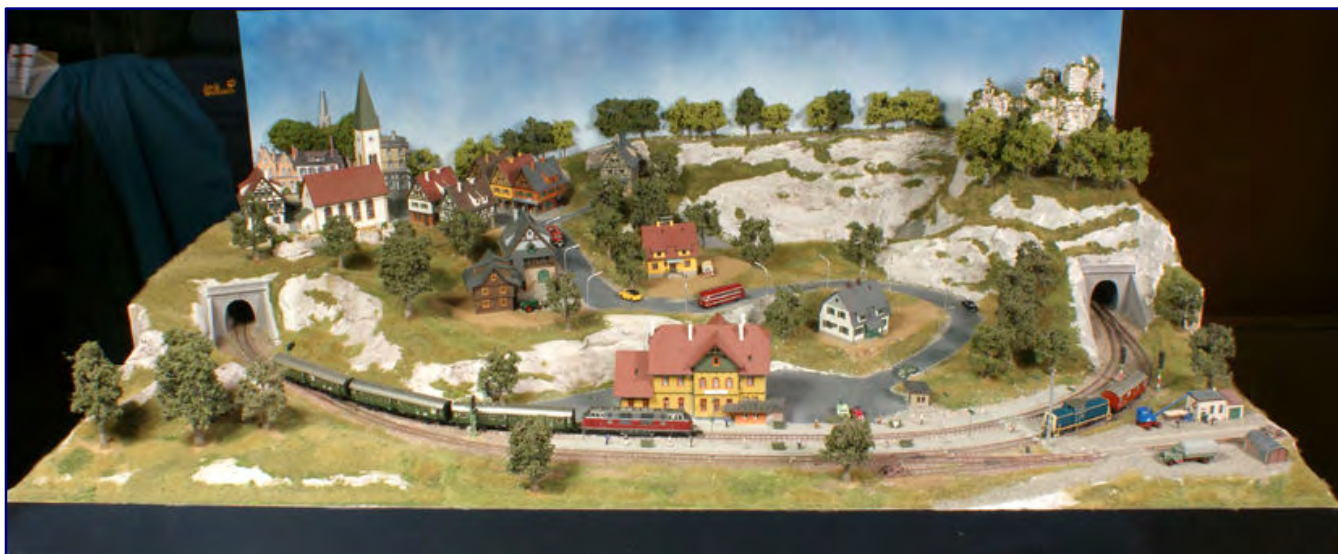
Photo below:
A lot of technology for a small layout - the purpose of each of the individual electronic components is described in the text. The right-left assignment of the descriptions in the text corresponds to the perspective of this image. Photos: Axel Pieke

Finally, let's have a look beneath the layout. The occupancy detectors from the producer Stärz are placed on the right side, the decoder module in the middle, and the light and signal controls on the lower left side.

The upper module in the left compartment controls the point motors. In the course of the construction process, the decoder modules were installed and the necessary wiring was added step-by-step. In the end, I was surprised myself how much electrical equipment one can fit beneath such a small layout!

With this, let me end my engineering report with a few atmospheric night time photos of the layout.





This overall view of the Torstburg layout will help our readers to identify the scenes described in the report.

Photos on page 29: Axel Pieke

QR-Codes to watch the video sequences:



Video sequences of this layout:

https://youtu.be/S7oQ_OYcZxQ
https://youtu.be/3Jw6_37Th6Q
<https://youtu.be/T97G7KKC-xk>
<https://youtu.be/dlNnohRVqvA>

Suppliers:

<https://www.aktionshaus-mzz.de>
<https://www.artitec.nl>
<https://www.faller.de>
<https://www.luetke-modellbahn.de/>
<https://www.mininatur.de>
<https://www.modellbauluft.de>
<https://www.modulor.de>
<https://www.mo-miniatur.com/>
<https://www.noch.de>
<https://www.preiserfiguren.de/>
<http://www.rolfs-laedchen.de>
<http://www.sceneryproducts.de>
<https://viessmann-modell.com>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Bahngeschichte des Sauerlands

Strecken mit Herausforderungen

Das Sauerland ist landschaftlich reizvoll, dünn besiedelt, aber keinesfalls frei von industrieller Geschichte. Erst spät und umständlich fand die Eisenbahn den Weg hierher, schuf aber eine wichtige Grundlage für die weitere Entwicklung. Der vorliegende Titel spürt der Geschichte nach und stellt dar, wie sich mit und dank der Bahn die Region veränderte.

Christoph Riedel
Eisenbahnen im Sauerland
Die wichtigsten Strecken und Fahrzeuge

Sutton Verlag GmbH
Erfurt 2019

Gebundenes Buch
Format 17,0 x 24,0 cm
106 Seiten mit ca. 150 überwiegend farbigen Abbildungen

ISBN 978-3-96303-003-1
Preis 24,99 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Das Sauerland gehört zu den landschaftlich reizvollsten Gegenden Nordrhein-Westfalens, doch genau das macht es topologisch auch äußerst anspruchsvoll. Zu Beginn der industriellen Revolution war es deshalb wirtschaftlich benachteiligt: Es zeigte sich verkehrsfeindlich und gleichzeitig wollte nahezu jeder Ort einen eigenen Bahnhof haben.

Trotz der geringen Bevölkerungsdichte gelang es schließlich, auch hier ein dichtes Verkehrsnetz zu ziehen. Es bestand sowohl aus Normalspur- als auch Kleinbahnen, wobei gerade einmal zwei Verbindungen den Namen Hauptbahn verdienen. Neben der Ruhr-Sieg-Verbindung ist das vor allem die obere Ruhrtalbahn.

Über die vielen Jahrzehnte von der zweiten Hälfte des 19. Jahrhundert bis zur Gegenwart verliefen die Entwicklungen spannend und abwechslungsreich. Dem schnellen Wachstum von Strecken und Stichbahnen folgte die Verstaatlichung, eine Konsolidierung und Rationalisierung des Betriebs, aber auch das Leben mit Kompromissen und Einschränkungen.

Während auf einigen Strecken der Verkehr zunahm, zeichnete sich für andere ebenso schnell das Ende ab, zumindest im Personenverkehr. Erst die Übergabe von Nahverkehrsbestellungen in die Hände der Länder scheint eine Wende zu bringen.

Autor und Bahnfreund Christoph Riedel aus Lüdenscheid war es eine Herzensaufgabe, das eisenbahngeschichtliche Erbe seiner Heimat und Region aufzuarbeiten und in diesem Buch zu verarbeiten. Das Ergebnis sind rund 160 Bilder der schönsten Strecken und wichtigsten Fahrzeuge aus immerhin vier Jahrzehnten.



Die Frühzeit der Eisenbahn konnte hingegen nicht bebildert werden, was dieses Buch sonst noch einiges wertvoller gemacht hätte. Was der Autor zusammengetragen hat, ist sehenswert und auch gut reproduziert worden. Schade ist es nur um einige lange Textpassagen, an denen das Bildmaterial aus der Pionierzeit der Fotografie schmerzvoll vermisst wird.

Die beeindruckenden Aufnahmen, auf die ein Zugriff bestand oder die vom Autor selbst stammen, dokumentieren hingegen den Wandel im Personen- und Güterverkehr eindrucksvoll. Nicht behandelt werden die Kleinbahnen des Sauerlands, wie etwa jener in Plettenberg mit ihren bekannten Kastendampflokomotiven.

Die Posse um die Dortmund-Märkische Eisenbahn, mit der die Volmetalbahn wieder zum Leben erwachte und die dann per Ausschreibung aus dem Verkehr gedrängt wurde und ein schnelles Ende fand, hätte ebenfalls etwas Raum in diesem Werk verdient gehabt. Immerhin zeigt dies auch die Schattenseite der Folgen einer Bahnreform. Sie wird aber nur kurz als Betreiber von Dieseltriebzügen erwähnt.

Nahezu lückenlos dokumentiert scheint aber der Einsatz von Lokomotiven und Triebfahrzeugen zu sein. Auch der Rezensent fand hier einiges für ihn Neues. Froh war er auch über einen Bildbeleg des nur kurz währenden Einsatzes der Baureihe 96, ehemalige bayerische Gt 2 x 4/4, im westfälischen Brügge.

Ein besonderes Kapitel ist gewiss der Neubau der Nebenbahn entlang der Biggetalsperre. Wird der Begriff eher mit Hochgeschwindigkeitsstrecken in Verbindung gebracht, hat es hier Mitte der Sechziger im laufenden Nebenbahnsterben eine eingleisige Strecke zur Neutrassierung gebracht. Hilfreich war dabei die touristische Bedeutung des Naherholungs-, Freizeit- und Trinkwasserspeichergebiets.

Der neu gebaute Abschnitt von Kraghammer zur Kreisstadt Olpe verläuft teilweise über eine in Betonbauweise errichtete Doppelstockbrücke, bei der aber, im Gegensatz zu Bullay, die Züge unten und die Autos oben fahren.

So ließ sich neben den schwierigen Finanzierungsverhandlungen auch gut auf übliche Einwände eingehen, die solche Bauvorhaben mit sich brachten und bringen – bisweilen auch derartige Projekte behindern oder ganz scheitern lassen. Die Sorgen richteten sich damals auf mögliche Rauchbelästigungen und Sichtbeeinträchtigungen der autofahrenden Menschen.

Gutachten der Bundesbahn, unter welchen Ausnahmbedingungen das auftreten könne, halfen nichts. Erst die eingeleitete Verdieselung des Bw Dieringhausen machte den Weg frei für die geplante Umsetzung des Vorhabens.

Zusammengefasst bedeutet dies ein ansprechendes Dokument zur regionalen Eisenbahngeschichte mit Anekdoten und Schilderungen, die auch mal einen Blick auf den vorherrschenden Zeitgeist zulassen. Hervorragend bebildert, erfreuen vor allem die gesammelten Aufnahmen aus der Vergangenheit das Herz der Nebenbahnfreunde.

So hilft das Buch sicher auch beim Umsetzen des Betriebs auf der heimischen Anlage, während es für Bahnfreunde und regional am Sauerland interessierte Leser sicher als ein Muss zu bezeichnen ist.

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Filmportrait der Baureihe 03¹⁰ **Nur für Anspruchsvolle**

Sie sollten, mit Stromlinienschale verkleidet, ein neues Zeitalter des schnellen Reisens einläuten, doch der Krieg verhinderte ihren Vormarsch. Nur 60 von 140 geplanten Lokomotiven der Baureihe 03¹⁰ wurden gebaut, 26 von ihnen kamen nach 1945 zur Bundesbahn. Ein Filmportrait von Rio Grande stellt dieser besondere Schnellzuglok vor, die mit neuem Kessel zur Vorlage für Märklins Insidermodell 2019 wurde.

JS-Filmproduktion
Folge 16: Die Baureihe 03¹⁰
aus der Reihe „Stars der Schiene“

VBG Verlagsgruppe Bahn GmbH
Fürstenfeldbruck 2006

DVD-Video
Bildformat 4:3
Tonformat Dolby-Digital 4.0
Sprache deutsch
Laufzeit ca. 44 Min.

ISBN 978-3-89580-658-2
Best.-Nr. 6316
Preis 16,95 EUR (Deutschland)

Erhältlich direkt ab Verlag
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Der Titel dieser Besprechung zielt nicht auf unsere Leser, sicher passt er aber zu den Käufern dieses Films auf DVD (oder zur bezahlpflichtigen Ansicht bei Leitungsverbindung zum Verlag). Doch gemeint war mit der vielleicht zunächst provozierend klingenden Überschrift die portraitierte Dampflok selbst.



Vor der Neubekesselung war sie eine tickende Zeitbombe, zugleich aber gleichzeitig auch ein willkommenes Zugpferd für die neuen F-Züge. Die Film Mannschaft um Joachim Schmidt hatte sie daher bereits 2006 für ein Portrait der Reihe „Stars der Schiene“ in den Diensten der Bundesbahn auserkoren.

Nachdem sie bei ihr den auch für die Baureihe 41 entwickelten Hochleistungskessel erhalten hatte, war das Risiko des nicht alterungsbeständigen Kesselstahls gebannt, dafür hatte sie nun aber ihre Tücken und Eigenarten, mit denen längst nicht jeder Lokführer zurechtkam.

Meist waren es die jungen Personale, die mit ihrem schwierig zu bedienenden Heißdampfregler zurechtkamen und ihre Leistungsfähigkeit zu schätzen wussten. Der Wasserstand im Kessel war stets extrem niedrig zu halten, sonst neigten die Maschinen zum Wasserüberreißen. Besonders bei der Fahrt in Steigungen führte das dazu, dass keine Wasserstände mehr angezeigt wurden. Hier war Erfahrung und gute Kenntnis der Baureihe gefragt, sonst drohte beim Nachspeisen ein Kesselzerknall.

Dass es zu einem solchen Unglück nie kam, beweist das Geschick und Können der Heizer und Lokführer vom Betriebswerk Hagen-Eckesey. Denn dorthin kamen die Schnellzuglokomotiven ab 1958 mit den neuen Verdampfern, wo sie bis 1966 blieben und sich durchaus bewährten.

Glauben wir selbst eingesammelten Augenzeugenberichten, dann verlangten die Hochleistungskessel der DB einfach nur eine völlig andere Feuerungsweise, um optimal zu arbeiten und nicht so stark zum

„Rotzen“ zu neigen. Das erklärt dann auch, warum ältere Bedienstete, die auf den DRG-Einheitslokomotiven erfahren waren, keine Freundschaft mit diesen Maschinen schlossen.

Auch die Kohlennachschubeinrichtung im mit Drehklappen versehenen Tender besaß ihre Eigenarten und war beim Fahrpersonal wenig beliebt. Daher fuhren die Lokomotiven meist mit geöffneten Klappen. Wohl ebenso an dieser Stelle waren Toleranz und Geduld gefragt, die nicht jedem gleichermaßen lagen.

Vieles von alledem ist auch dem zu besprechenden Rio-Grande-Filmwerk zu entnehmen, das erst durch die vielen Berichte und Ausführungen ehemaliger Bediensteter aus Hagen-Eckesey lebendig wird. Nicht vergessen werden darf, dass keine der 26 Vorbildmaschinen aus dem DB-Bestand erhalten blieb. Für ein Filmportrait ist das eine nicht zu unterschätzende Hürde.

Die JS-Filmproduktion hat dies jedoch meisterhaft gelöst und die elegante Schnellzuglok mit seltenem historischem Filmmaterial in Szene setzen können. Erst dadurch werden die Erlebnisse der alten Lokführer für den Betrachter nachvollziehbar.

Selbstverständlich ist den zeitgenössischen Film- und Tondokumenten das Alter anzuhören und anzusehen. Das reproduzierte Super-8-Material hat schließlich seine typischen Eigenschaften und Schwächen wie Staubpartikel auf dem zu belichtenden Film oder weniger gut belichtete Ecken.

Trotzdem erscheint es in der Gesamtbetrachtung gut, dass hier beim Aufbereiten nicht übertrieben wurde. Zum einen hätte dies auch Auswirkungen auf den Preis haben müssen, zum anderen hätte es aber auch der authentischen Wirkung geschadet.

So wie es gelöst wurde, weckt es nostalgische Gefühle und ruft auch im Zuschauer die eine oder andere Erinnerung wieder wach, die uns aufzeigt, warum wir der Eisenbahn (zumindest einen Teil) unsere(r) Liebe geschenkt haben.

1966 kam dann das unerwartete Aus für die Baureihe 03¹⁰. So ist es hier auch nach bereits rund 44 Minuten. In 03 1001, der Vorlage für Märklins Insidermodell 2019, erlosch als letztes das Feuer. Einige Jahre konnte der Dienststellenleiter sie noch für die Nachwelt hinterstellen und so erhalten, bevor auch sie dem Schneidbrenner zum Opfer fiel.

Was bleibt, sind Erinnerungen, Fotografien und eben dieser Film. Er verkürzt auch die Wartezeit für alle, die sehnsüchtig auf die Spur-Z-Umsetzung warten und mit ihrem Modell einem eleganten Dampfross ein eigenes Denkmal setzen wollen.

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Successful premiere in Mannheim When model railways fascinate

Moving a trade fair from one location to another is certainly not an easy task. The Faszination Modellbahn, as the first spring exhibition after the International Toy Fair, faced exactly this challenge. Our employee Stephan Fuchs was on site to capture pictures and impressions. So we can take a look back here for our readers and see what will await us at the new location in a well-known region in the future.



It is often claimed that the premiere will be a success if the dress rehearsal goes well and truly wrong. We do not know whether this also applies to the fascination of model railways.

The fact is that it moved from Sinsheim and took place for the first time in the Maimarkthalle Mannheim. In any case, our magazine representative Stephan Fuchs was enthusiastic and had the impression that the fair was better attended than in Sinsheim.

attended in rainy weather, which certainly favoured the event. 143 exhibitors from nine countries presented more than 20 model railways from Germany, Belgium, the Netherlands and Luxembourg.

This gives us the opportunity to take a closer look at the official figures and explanations: Between 15 and 17 March 2019, almost 18,000 visitors

With a spectrum ranging from sheet metal to digital high technology, they covered almost the entire spectrum of what our hobby has to offer. And Z gauge also played a special role, because it had the largest representation of all of the gauges (see the following report in this issue) and also impressed with trains up to six meters long, pulled by three locomotives. They exceeded even the longest train (in H0 gauge) in Hamburg's Miwula!

“The quality of the systems was extraordinarily high”, Sascha Bürkel, project manager of the trade fair, reflects, referring not only to the exceptionally good Z gauge participation. And this is exactly how he hits the nerve of the satisfied visitors.

Only the lighting in the hall was not satisfactory, which is why exhibitors with good additional lighting of their own were clearly at an advantage. Improvements have been planned here for the future, so let us be surprised.

“The relocation of our model railway fair to Mannheim was crowned with full success with its kick-off event”, summarised Andreas Wittur, who was born in Mannheim and has full power of attorney.



Project manager Sascha Bürkel was enthusiastic about the quality of the exhibited systems.

And he continued: "The urban infrastructure with a direct motorway connection, sufficient parking space and excellent rail connections also contributed to this".



Photo above:
Ulrich Günther (left) and Andreas Hagendorf (right) were in charge of the digital demonstration system, which also served as an explanation for last year's focus topic.

Photo below:
Peter Simon made by far the largest contribution to the US facility, which is impressive in its dimensions. When steering the passenger train powered by a Challenger, he was happy that visitors looked over his shoulder.



Claudius Veit (Velmo) proudly presented the first exchange decoder for the series presented in the last issue. E 41.

Jan Goschmann, Managing Director of Mannheimer-Hallenbetriebs GmbH, emphasised the diversity of the hobby and welcomed the fact that the trade fair was intended to remain in the region. It sounds as if two partners have found each other here. If this is the case and if this exhibition can be permanently established near France, the model railroaders will be happy!

Besides the ideal enthusiasts, also well-known manufacturers like Märklin, the big railway publishers, dealers, and for Z gauge the important small series manufacturers like Aspen-model, Luetke Modellbahn and MBZ took part in the exhibition. The participation and play offers of the manufacturers for children were gladly welcomed.



Gerhard Maurer operates his dream loop module system with the warm autumn colours (photo above). But it's also worth taking a look at the details here: the 050 class steam locomotive with its tender has come to a standstill with its train before it stops (photo below).

Claudius Veit (Velmo) was also present, and demonstrated his digitally controlled and further revised container terminal at the ZFI booth, and was able to show his first prototype of his decoder board for Märklin's brand new E 41.



Manfred Forst provided for busy traffic on small roads with dioramas like this one. Also, he brought along his small layout with an agricultural motive.

The fact that the guests at the stand of nominal size Z could not get bored was also due to Manfred Forst, Andreas Hagendorf, Gerhard Maurer, Ingo Sindermann and a play oval for children. The fascinating dream looping facility and the imaginative dwarf train for children's dreams could be admired here.

The fact that Axel Hempelmann cancelled due to a storm could be tolerated, since moving cars were also presented by Manfred Forst.

Two of his works were full of road traffic, but propulsion was not provided by the Z-Car system, but rather was provided by the Magnorail principle: the vehicles do not have their own drive and are pulled under the roads by a magnet.

Due to the location of the ZFI stand, fewer children than usual took advantage of the games offered, but despite this, the Z-Freunde International stand was never empty at any time: visitors who asked questions were sent here specifically. Often it was about technical advice or tips for perfect and gentle track cleaning.



The fire in a house on the Maasbuurtspoor grounds looked deceptively real.

The Zetties' always good mood was praised throughout. After all, they see themselves as a cosmopolitan and friendly community that looks forward to visitors, likes to exchange ideas and unites and integrates people of all preferences and track gauge choices. No matter how the exhibitors are thrown together, it's simply true! The impression of a large family has not disappointed the participants and visitors in Mannheim either.



Motivated by Lorient, the people of Oranienburg built the Hoppentedt power plant on their layout in TT gauge (top photo). Ulrich Günther was visibly impressed by the water design on some modules of the exhibited N gauge layout (photo below).

The expected highlight was, of course, the large US style layout. This was due to the imposing dimensions, the almost unmanageable length of the trains, and also to the very impressive design, which

perfectly conveyed the vastness and diversity of the “land of unlimited possibilities”. But the smallest gauge is as simple as it gets.

However, the premiere exhibition only became complete in interaction with the other gauges and “departments”: Here, real steam-driven models could be seen, as well as a burning house on the Maasbuurtspoor facility.

The fire here was really amazingly realistic and had nothing in common with the transparent flashing of light-emitting diodes.

Stephan Fuchs also found the idea of the Oranienburger company to build the Hoppenstedt, power plant funny, starting with Loriots Hoppenstedt family and their Christmas party.

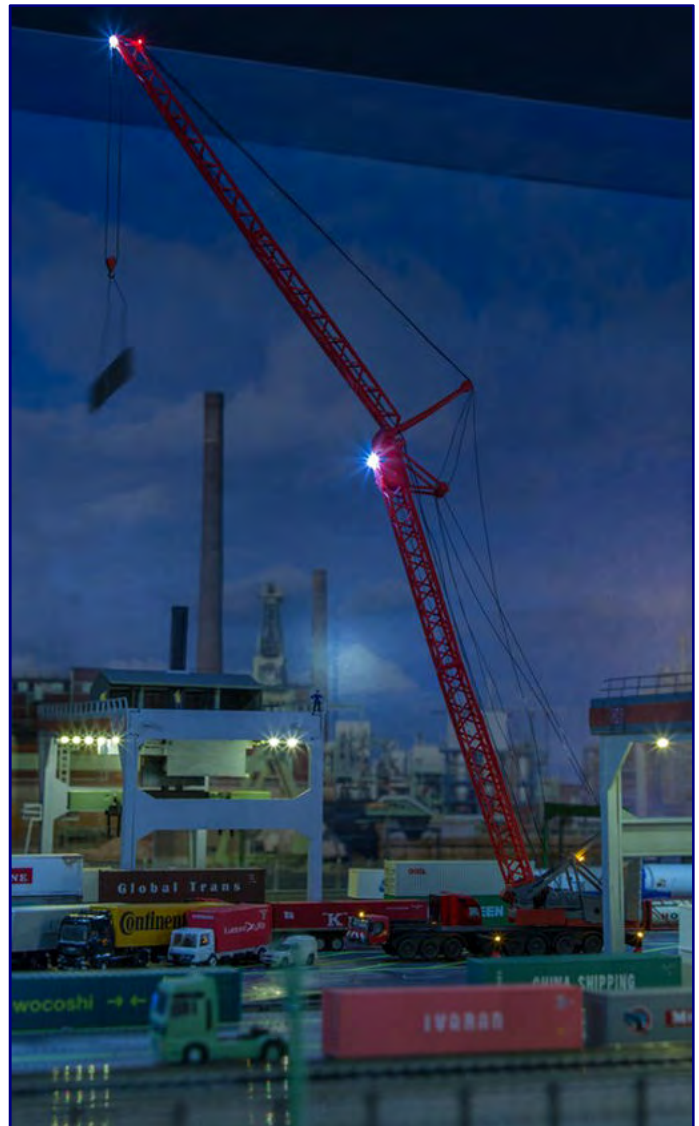
It was also nice to see that some of this plant was still under construction, so that it was also possible to obtain tips for one's own layout construction here.

The well-known Ulrich Günther was enthusiastic about the presentation of the water on some modules of the N gauge layout. Who knows what ideas germinate from it and will perhaps soon be admirable on a scale of 1:220.

And so the final conclusion is almost thoroughly positive: the trade fair was appealing, well attended and still has potential for the coming years.

The somewhat rustic charm of the Maimarkthalle will invite visitors again next year: Make a note of the 13th to 15th March!

All Photos: Stephan Fuchs



Finally, a night photograph of Claudius Veit's container terminal: the newly installed and unmistakable highlight on this digitally operated layout was the heavy truck-mounted crane with a high boom.

Web pages for this exhibition:
<https://www.faszination-modellbahn.com>

Eisenbahnmuseum Dieringhausen



Einzigartig in Europa

*Spur Z Ausstellung
im historischen Güterwagen
im Eisenbahnmuseum Dieringhausen*

Die Sonntags-Ausstellungstermine:

*08.06.2019, 09.06.2019, 16.06.2019, 07.07.2019,
28.07.2019, 11.08.2019, 22.09.2019, 13.10.2019,
27.10.2019*

jeweils von ca. 10:00 Uhr – ca. 17:00 Uhr

Infos unter: www.stammtisch-untereschbach.de

Through the vastness of America **A meeting of US Model Railroaders**

A layout consisting of 57 modules based on US prototype was the largest single layout at Faszination Modellbahn in Mannheim. Three regional divisions of the US Z Lines Europe group of Z gauge model railroaders, which call themselves US Z Lines Europe, had come together here in order to operate unusually long trains for the viewing of fair visitors. Markus Gaa and Jörg Hausmann report on this event.

By Markus Gaa and Jörg Hausmann. In March 2019, the US Z Lines Europe had met again in Mannheim in the context of the Faszination Modellbahn after about 5 years in order to assemble known as well as new modules to a large, digitally controlled module system. With a track length of over 150 metres, this was certainly one of the largest module systems in Europe.



The 22 x 6 meter module system of US Z Lines Europe guided visitors through the vastness of America on a scale of 1:220.

US railway enthusiasts who work with standardized module interconnections and digital control according to the DCC standard (usually with Digitrax products) gather under that name. They are easy to recognize by their well-designed modules.

Almost exclusively models of the two American manufacturers American Z Lines and Micro-Trains Line are driven. The locomotives are digitalized in order to realize long trains with several hitched up locomotives.

Switches and signals are usually also controlled digitally. However, there is no automatic control: Each train is personally accompanied and controlled by a "locomotive driver". Divided into different divisions,

the US Z Lines unites its own module standard and the freedom to implement its own ideas. There are no limits!



The landscape spectrum of the 57 modules shown ranged from rugged rocky landscapes (picture above) to green forests (picture below).



The 180° curve with the long freight train is almost smothered by the barren mountain massif (picture above). Here, too, the scenic diversity and vastness of America can be seen. In the industrial area (picture below), on the other hand, a small exchange of a letter turns a beer brewery into a bear brewery.

Further information on the ideas and work of US Z Lines Europe is best found on the websites mentioned at the end of this article. They can also be used to get in touch with us.

An exhibition or model railway fair - as is now the case in Mannheim - offers the opportunity to present a large number of modules and run unparalleled long trains. The 57 modules, coming from three divisions, were largely equipped with two tracks, i.e. they functioned as so-called "siding". continued on page 48



Rail traffic in the United States is colourful thanks to the many railway companies. The modular layout featured locomotives and trains of various classes, transported goods, tractions and rail companies, including the large Union Pacific (bottom picture)



In Orange Grove (picture above), oranges are packed into crates after harvesting and loaded onto trains such as the Pacific Fruit Express for onward transport. The "Rail Budd Car" is a counterpart to the German rail bus and served secondary lines, such as the strikingly designed representative of "Trinity Railway Express" (bottom picture).

However, there were also three single-track sections distributed over the plant. This increased our driving pleasure, as the locomotive operators or dispatchers had to coordinate with each other when trains came towards them or were to be overhauled

From the steam locomotives Mikado, Big Boy, Challenger and Pacific to ultra-modern diesel locomotives of the type EMD SD70Ace and even a pair of small shunting locomotives, everything was on the way. The fun of the game culminated in trains up to six metres long.

This year, our exhibition area of 22 x 6 metres was almost as large as the areas reserved for other tracks. In comparison, the modular system seemed even larger. This allowed Z-gauge to consciously get away from the impression of being just a minor railway.



The 22 x 6 meters of the exhibition complex were also filled with branches secured by signals such as this one. The operation was completely digital. Each "locomotive operator" can accompany the train with his hand-operated control unit and connect his controller to the control centre again and again at the plug-in points along the modules.

The audience was surprised that so much more is possible with this size than is generally thought. Children as well as adults watched the trains with excited glances over the complete module layout. The enthusiastic remark of a little boy to his father also matches: "Look, the locomotives are here in the front and the wagons are there in the back".

All in all it was a very successful performance of the Z gauge! Our personal thanks therefore go to the main organiser, Sylvester Schmidt, who once again organised the trade fair participation for Z-Freunde International e.V. in an excellent manner.

All photos: Stephan Fuchs

Information of this model railway group:
<http://uszlines.eu>

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Reminder for the announcement of the KlV 20 by Märklin:

It's hard to believe that this tiny model would appear to be powered: the KlV 20 (88025) from Märklin. This was described in the Trainini issue 163 of February 2019.

It had long since been forgotten that the Trainini reader Hilmar Weber was allowed to present "A little miracle on wheels" in the August 2015 issue 121. Among other things one could read there:

"Motorized KlV 20 by Hilmar Weber - and yet it runs. For Hilmar Weber from Frankfurt (Main), making tiny standing models operate is always a new challenge. Finally, the inventor ventured to the railway service vehicle KlV 20. The vehicle based on a VW Transporter T1 was considered before, but it was decided that it was not possible to install a motor in it. In this respect, with this model he quite rightly achieved the first place in our conversion competition, which we had started on the occasion of our 100th edition."

Bernd Petry, by E-Mail

Answer of the editors board: Thank you for reminding us of this extraordinary and, to our knowledge, unique self-built model! The fact that the technical masterstroke required by this tiny vehicle was very impressively demonstrated here, and makes Märklin's product decision even more impressive and admirable. After all, the assembly steps have to be carried out there hundreds of times a day.

A product recommendation from the readership:

I would like to draw attention to a HO innovation from Kibri that has recently been on the market and is also suitable for Z-gauge. A sound-proof wall is offered under article number 38623.

The wall can be assembled in three different heights from coloured and transparent segments.

For Z gauge, two segments on top of each other are perfect. For a passing passenger train you can still see the car roofs. As examples I, have added some photos.

Rainer Hecker, Düsseldorf



The sound insulation wall from Kibri can also be used for this purpose on a scale of 1:220. Photo: Rainer Hecker

Report from our reader Raffaele Picollo (Genoa) from Osaka Z Day:

“Osaka Z Day” is a biennial model railway exhibition in Osaka organized by the local group of Z-gauge enthusiasts “Baden-tani9” (<http://www.baden-tani9.com>). The name of this group is derived from the title of the first model Baden produced and the name of the nearby subway station.

It's an exhibition known throughout Japan, and many enthusiasts come to Osaka to share their passion for Z gauge.



Raffaele Picollo (left) is happy about the friendly reception and the hospitality in Japan. In the picture you can see his opened suitcase system with motives of a Japanese Manga series. Photo: Raffaele Picollo

The exhibited installations, inspired by both European and Japanese landscapes, are fitted with rail vehicles by Märklin, Rokuhan, Tenshodo and others. Japan has always been an optimal market for the small Z gauge.

The common feature of all the systems shown and their builders is the high precision in the details. And above all, there is a strong sense of sharing and shared fun; factors that unite friends everywhere across all events dedicated to the Z gauge.

The organisation was also flawless and everything went perfectly: from the preparation the day before, to the end of the event, including the usual farewell dinner.

In this last edition, which took place on 24 November 2018, I had the pleasure of participating with my suitcase system, which covered a total of around 22,000 km by practically all means of transport. The hospitality of the whole group was wonderful, for which I would like to take this opportunity to thank you.

continued on page 52



The organizers proudly presented their works based on German prototypes, which have everything from beautifully designed landscapes to exciting track plans to offer. On one of the layouts (photo below), the airplane moves through the sky (photo below).
Photos: Raffaele Picollo

Omiya-Saitama Railway Museum (Tokio) / <http://www.railway-museum.jp>

Tobu Museum (Tokio) / <http://www.tobu.co.jp>

SCMAGLEV and Railway Park (Nagoya) / <https://museum.jr-central.co.jp/en/>

Kyoto Railway Museum (Kyoto) / <http://www.kyotorailwaymuseum.jp/en/>

Tsuyama Railroad Educational Museum (Tsuyama) / <http://www.tsuyamakan.jp/en/index.html>

Kyushu Railway History Museum (Fukuoka) / <http://www.k-rhm.jp>

Spring comes to Trafofuchs:

Trafofuchs (<http://www.trafofuchs.de>) will soon be offering the Beckert bee transport wagon as a finished model. It naturally also includes a beekeeper in protective overalls with hat and net. Further new items should follow in time, and we will report in due course.

New products from Märklin:

This year's Insider special model of the elegant express steam locomotive 03 1001 (Art. No. 88850) of the Deutsche Bundesbahn can only be ordered from specialist dealers until 30 April 2019.

With the exception of the chassis of the locomotive and tender, this is a completely new design.

Our film tip in this issue makes you want to see this model, the model of which was the last representative of this series. The video also shows how the originals were used on the Bw Hagen-Eckesey and how they will be shown to their best advantage on the layout.



Den Bienenwagen der Modellmanufaktur Beckert gibt es bei Trafofuchs als Fertigmodell zusammen mit einer passenden Imkerfigur. Foto: Trafofuchs

But until then we still have to wait. This year also applies to the summer new products, because Intermodellbau Dortmund apparently occurred earlier this year, in order to already replenish here. Märklin is innovative and increases customer excitement.

If you want to be the first to know, you can experience the presentation the Märklinium or follow it simultaneously on the Internet from 18:00 CET at:
https://www.youtube.com/channel/UC_ulAke0RxHUEhvpWf49pRQ.

Until then there will be speculation and oracles, because the date of publication on 9 May 2019 does not seem to have been chosen by chance: It is the 175th anniversary of the merger of the transcontinental railways in the United States of America. This already has a well-known significance for LGB, but customers are already surpassing themselves with their assumptions and expectations.

The world's largest steam locomotive 4014 of the UP, better known as the "Big Boy," which has only just been rebuilt, is also considered a possible new product. Let's wait and see! Already in a few weeks we will know more...

(Model railway) trip into May:

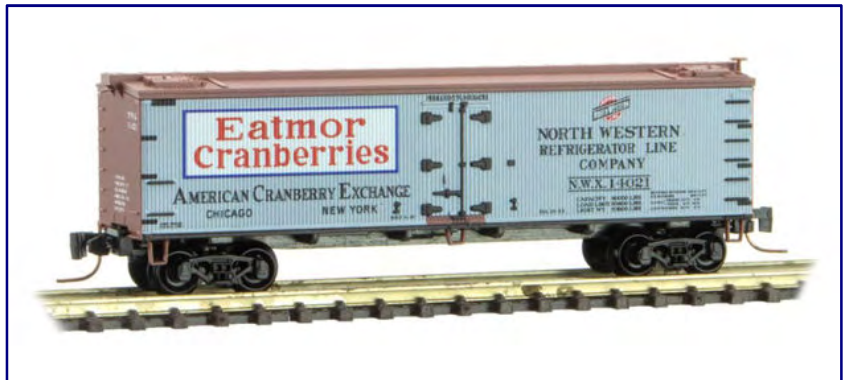
Our reader Siegfried Grundhöfer drew our attention to an event that is also worth visiting for Zetties. The Modelleisenbahner Soest 03 e.V. invite you to their May exhibition on Wednesday, May 1, 2019.

From 11:00 to 18:00 there will be a ride on all facilities (including gauge Z), a stock exchange with surplus club material, coffee and cake as well as advice and talks about the hobby of model railways.

The location of the meeting is the association area in Neuengeseke, whose address is to be found on the association sides (<http://www.mes03soest.de>) only with difficulty. This reads: Neuengeseker Heide 1, 59505 Bad Sassendorf.

The following MTL models are currently arriving:

Also, from Micro-Trains some new products are currently arriving at the dealers. Among them is the EMD SD40-2 diesel locomotive from CSX (Art.-No. 970 01 261 / 262), which is overdue one year, and will now be available soon. Also in mid-April, the four-pack of 40-foot flat wagons (994 00 108) of the UP should arrive, which has interesting cargoes with diesel engines and generators.



The farm-to-table series is currently being continued with wagon number 3: The 40 foot long, brown refrigerator car with wooden walls from 1927 carries the advertisement "Eatmor Cranberries" (518 00 730) on the light blue side walls and was rented from the North American Refrigerator Line.

In the model, the modern 40-foot wood transport wagons (538 00 180 / 190) often come from their own workshops. They are used to transport freshly cut tree trunks, which are also loaded onto the models, to the sawmill. The delivered models have normal-gauge models.



Refrigerator car "Eatmor Cranberries" (Art.-No. 518 00 730; photo above) and aged NATX tank car (530 44 440; photo below). Photos: Micro-Trains Line

50 feet long and equipped with double sliding doors are the templates for the covered freight cars of the UP (50600411 / 412), which are currently being delivered. On the side walls they carry the striking advertising set "Be Specific - Ship Union Pacific." The two 39-foot long tank wagons of the NATX (530 44 440), which were registered for delivery shortly before the editorial deadline, have been factory aged.

Micro-Trains products can be purchased in Germany from the following sources, among others Case-Hobbies (<http://www.case-hobbies.de>).

Deliveries from Freudenreich Feinwerktechnik:

FR Freudenreich Feinwerktechnik can now supply the new Xas73 ballast wagons from the BLS stock (Art.-No.: 41.345.32), limited to 24 packages. Two all-metal models each are included in the sets.



The Xas 73 new ballast wagons are currently being delivered as BLS rental vehicles. Photo: FR Freudenreich Feinwerktechnik

The models for these cars were leased by SBB to BLS between 2000 and 2005. For this reason, they retained their previous addresses, but were also given large logos of the Bern-Lötschberg-Simplon-Bahn.

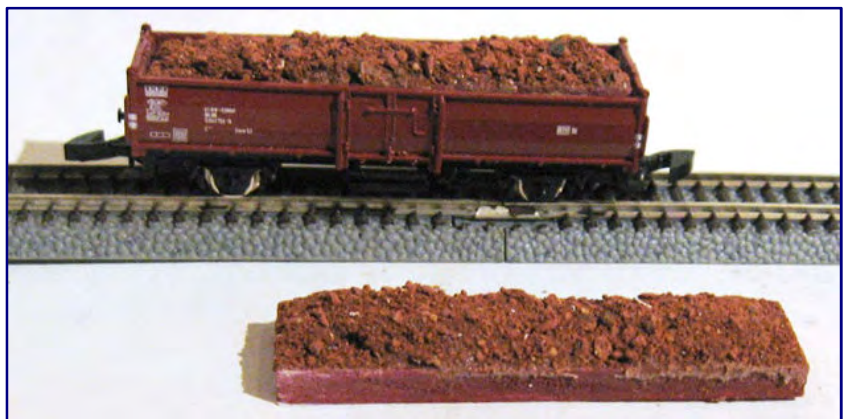
The models are available directly from the manufacturer: <https://fr-model.eshop.t-online.de>.

New products from Ladegut Küpper:

Spring also brings a breath of fresh air to the Z gauge cargo of Josephine Küpper. In addition to used goods and spare parts, the Aachen-based specialist is also offering new loads for two Märklin car types.

Broken bricks as construction site rubble to be removed is certainly one of the more unusual and rarer goods to be transported by rail, but it is precisely for this reason that it provides a welcome change and draws attention. Especially in epoch III it should fit well into the phase of reconstruction.

The new inserts "Ziegelbruch" are offered by Küpper for the (old) Märklin models of the open car Omm 52 (Art.-No. Z-1-1) and for the younger, four-axle brother Eanos (Eanos 9). They weigh about 2.5 and 5 grams.



For two different wagon types there is now broken bricks (here Art.-No. Z-1-1) as cargo. Photo: Spur Z Ladegut Josephine Küpper

As usual this supplier attaches importance to original material for a perfect effect also in the model. Therefore, no granulate, plastic or other foreign material is used, but real canal bricks.

As usual, the fine and quite inexpensive inserts can be purchased from the supplier himself: <https://spur-z-ladegut.de>.

New Products from American Z Line:

Our summary of deliveries also includes models that were delivered to dealers in March but were no longer covered by the editorial deadline.

For the NJ Transit there is the diesel locomotive GE P42 Genesis with two operating numbers (Art.-No. 63505-1 / -2) including a matching passenger train car of light-weight construction (73752-0). Other wagons of this type in various designs are offered as Phase III triple packs by Amtrak (72076-1 / -2) with a matching baggage car (73650-1).

The Commuter Railroad will also be equipped with a GE P42 Genesis in two variants (63506-1 / -2) and a matching passenger coach (73753-0). On the side walls the vehicles bear the inscription "Metro-North Railroad."

Light passenger coaches of the various types from the already well-known form stock in the brown-red paintwork of the Canadian Pacific (73041-0 to 73941-2) are among the new deliveries, a matching EMD E8A diesel locomotive with three different operating numbers has been in AZL's range for some time.



Diesel locomotive GE P42 Genesis of the NJ Transit (art.-no. 63505-1; photo above), matching passenger car (73752-0; photo middle), and observation car of the Canadian Pacific (73841-0; photo below). Photos: AZL / Ztrack

40-foot AAR cars of covered design with Chesapeake & Ohio logos are also currently being delivered. Available are a single wagon (904302-1), a double wagon (904372-1) and a quad wagon (914302-1).

You can find more manufacturer photos of the current deliveries under <http://www.americanzline.com>.

Interesting addition at 1zu220-Shop:

As already mentioned in our "Messeausgabe" (fair issue) 2/2019, the 1zu220-Shop (<https://www.1zu220-shop.de>) offers the customers of the Märklin insider model 2019 (express steam locomotive 03 1001 of the DB, art. no. 88850) a free addition in the form of a blue corner cap truck.

The Magirus-Deutz 230 D 22 AK 6 x 6 were manufactured exclusively by Wespe-Models and have since been delivered to the 1zu220 shop. Anyone whose interest has been aroused should hurry: The blue-red dump trucks are not available individually and will not be put on regular sale.

In the meantime, it has been decided that such supplements will continue to be used in the future. Vehicles in the same colour scheme are planned for the time being. The first samples of two vehicles from Krupp's former production facility are already available and will whet the appetite for further work.



This Magirus-Deutz 230 D 22 AK 6 x 6 corner hood dump truck from the 1zu220 shop has been exclusively designed by Wespe Models. It is only available as a free addition to an ordered Märklin Insider model 2019 in Z gauge.

However, they will only be presented to the public shortly with the Insider Club models in fall 2020 and 2021.

Sand and gravel from Wangen (Allgäu):

The fine natural products sand and gravel from Noch are ideal for creating realistic country lanes or sandy beaches by the sea or dredging lake. For Z-gauge, of course, this only applies to the much finer grained sand, while gravel is used for other purposes.

Noch supplies the sand in the two grain sizes “fine” (Art. No. 09234) and “medium” (09235). The gravel (09237) is much coarser, but matches the sand in colour.

Märklin deliveries in April:

In addition to the “Light Freight Train” (Art. No. 81772) for Era IV presented in detail in this issue, another compilation from the 2018 summer new products has made it onto the dealer shelves.

The ICE 3 MF of Deutsche Bahn AG, which belongs to the class 406 (88715), also required product maintenance. Now, it is presented after the revisions with air conditioning units and redesigned car transitions. The standard also includes a new current-conducting close coupling and the well-known bell-shaped armature motor for improved operating characteristics.

Depending on the direction of travel, the lighting of the end cars is provided by warm white and red light-emitting diodes. The interiors of the two end cars and the 2nd class middle car are also illuminated with maintenance-free warm white LEDs. The high-speed train requires a minimum radius of 195 mm (R2).



The multi-system train ICE 3 of the Märklin 406 series (art. no. 88715) now has short couplings, redesigned car transitions and air conditioning attachments that were later retrofitted to the prototype, in addition to other innovations.

The four-part basic package of the multi-system version in international ICE traffic consists of an end car 406.0 (1st class), the unlit dining car 406.3 with the drive, a middle car 406.8 (2nd class) and the end car 406.5 (2nd class). The four-part extension for a prototype long train has not yet been delivered.

New Herpa Wings aircraft for July/August 2019:

From the Herpa innovations announced for the months of July and August 2019, we have again selected interesting aircraft suitable for the usual system dimensions. The first of these is the new Douglas DC-4 Skymaster (Art.-No. 559799), which will be available for the first time in the civil version with the registration PH-TAR and the baptism "Rotterdam." It was flown by the Dutch KLM.

The Aeroflot Tupolev TU-154B-2 "Blue Tail Livery" (559812) was at home behind the iron curtain. The Boeing 737-200 of Malév Hungarian Airlines (559782), on the other hand, has its model in the time after the collapse of the Eastern bloc.

In the present the Flybe Bombardier Q400 in new colours (559829), the Airbus A320neo of the Lufthansa in the similar current paint scheme (559768) and the Airbus A220-100 of Swiss International Airlines (558471-001) reissued with small deviations belonging to Flybe.

In the broadest sense of a reissue is also the following new product, which has only minor differences in the so-called parabolic paint compared to an earlier model: This Lockheed L-1649A Super Star (559805) is the last aircraft of this type delivered to the German Lufthansa, as its identification D-ALOL reveals. The age of jet aircraft was also introduced here and outstripped its piston engines.

The announcements are supplemented by two military models: The Tupolev TU-160 "Backjack" / "White Swan" of the Russian Air Force (559867) and the Lockheed C-130H Hercules of the transport units of the Belgian Air Force (559843), which were painted to commemorate the 70th anniversary of the foundation of the 15th squadron and the 45th anniversary of this aircraft type.

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German Magazine for Z Gauge



Clear message to Märklin?

Heavy boys from Göppingen
What Intermodellbau was like

Introduction

Dear Readers,

May has come... and is going again. Anyhow this applies to the publication date of the current issue. But as you know, this magazine is the result of voluntary work of many involved, diligent helpers.

Month after month they research, write, take photos, edit, build, do handicraft work and translate in order to provide you with regular, sophisticated and entertaining articles. When private commitments take their toll, you show concern, but not impatience, as we have been able to gather from the questions to the editorial team. We thank you for that!

We are happy to be part of a big but still manageable Z-gauge family, in which community and enjoyment of the hobby count and one stands up for the other.

In the current month I often had to wait patiently to complete a procedure that is usual for me. A whole backpack filled with empty (but lockable) containers was waiting to be filled with various types of earth and sand. The supplies for a whole year of landscaping were to be available.

We try to reproduce nature as well as possible on our layouts, but what is better than the material from nature itself? Of course, all finds must be sieved and subsequently disinfected. But the result is always persuasive.

Since the big exhibitions are over and most of the model railroaders have the courage to leave the hobby room because of the coming warm season, it would be a reasonable activity. Here even the spouse could be engaged. A little tip: The root balls of last year's potted plants to be disposed of are also a wonderful resource!

During summer pause you can take a look at this edition, because with NoHAB we are looking at a very special locomotive from FR Freudenreich Feinwerktechnik. In addition to the extraordinary running smoothness, the model also excites in detail.

Meanwhile Märklin opens a new chapter of the model railway production, the technology of the metal-filled plastic has been explained to us and we now present you the implementation here in the **Trainini®**.

Of course we also report about the Intermodellbau in Dortmund, which took place this time from 4th to 7th of April 2019. In addition to the presentation of the Z-gauge layouts exhibited there, we would also like to take a closer look at the work of young people. It is never wrong to invest in the future!

The content of the late May version will be completed by two book recommendations, the first of which is a perfect match for the location of the model exhibition already mentioned. There has also been a great deal of news gathering with a view to the larger time window.

Sin-Z-erely,

Dirk Kuhlmann



Dirk Kuhlmann
Editor

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We thank the 1zu220-Shop for provided test models and Harald Möller for his photographic support.

Date of publication of the German language version of this issue: 30 May 2019

Cover photo:

Freight traffic is the daily bread of the MY 1131 in the services of the Erfurt railway service. We caught the diesel locomotive, which was designed in the late DSB painting and roaring strongly, leaving the harbour area after it had put together a short freight train from several groups of wagons.

New FR models under test Round noses from Sanitz

Märklin has had a replica of the NoHAB diesel locomotive in its range for many years, but real enthusiasts of these machines never really liked it. Harald Thom-Freudenreich has therefore ventured to design his own locomotive with a view to his clientele in Scandinavia. It is already in its first edition for a German railway company and will now have to compete with the Göppinger model.

The origin of the locomotives with the official type designation NoHAB AA16 goes back to the American E and F series of EMD (General Motors). Developed from the thirties onwards, the manufacturer also made an early effort to export its models.

For this purpose the locomotive body was designed somewhat lower and instead of the two-axle bogies three-axle bogies were used, in order to lower the axle load according to the regulations in the export markets. At the same time, the Clyde A16A, built in 1952, was equipped with two end driver's cabs. The first A was an indication of the streamlined shape of the ends, the number stands for the 16 cylinders of the two-stroke diesel engine.

This (still further developed) export model was the basis for the NoHAB AA16, which became successful in Europe as licensed production from 1954. The first units went to the Danish State Railways (DSB).



The first built NoHAB AA16, the MY 1101 of DSB, could be seen on 11 May 2013 in Tepestrup, Randers, in front of VP 226410. Photo: Morten Jensen (CC-BY-2.0)

This type of construction was briefly and simply called “NoHAB”, although this is actually only the manufacturer's name: in 1949 the company Nydqvist och Holm AB, or NoHAB for short, acquired the licence for construction in Europe in Trollhättan, Sweden.

Anglo-Franco-Belge (AFB) was responsible for the CFL (Luxembourg) and NMBS/SNCB (Belgium) specimens, also known as “potato beetles” because of their decorative stripes, as sub-licensees of NoHAB.

Like the similar US models, the NoHAB locomotives were equipped with the EMD 16-567 engine, driven by a generator either by four or six traction motors per bogie, i.e. in the axle sequence (A1A)' (A1A)' or Co' Co'. These are therefore diesel-electric machines, generally equipped with a steam boiler for train heating.



Many NoHAB locomotives found a new home after their retirement. The MY 1147 arrived at STRABAG Rail, for which it ran in GM demonstrator paint when it was picked up in the eastern parking area of Dortmund Hbf on 14 April 2012.

Depending upon the number of traction motors and the equipment with train heating, the weight of a locomotive was between 70 and 109 tons. To maintain the UIC clearance profile, further external adjustments were necessary: The roof had to be curved more in the direction of the lower side edge of the roof.

This also changed the position of the side windows. The outer ends of the driver's cab front windows were pulled downwards and, together with the more pronounced curvature of the roof edge, were the most striking feature that made the machines immediately recognisable.

The snout of the “round noses” was more inclined than the American and Australian models. This allowed them to better absorb the impact forces of the side buffers. In all other respects, the NoHAB revision corresponded to Australian Class B 60, which EMD had internally called AA16C.

The template for the FR conversions are the machines supplied to DSB as the MY series and their externally identical representatives, which were or are used for other railway administrations. The concrete model for the first edition is the MY 1131, which was delivered to DSB on 25 October 1957 with the serial number 2372.



The former MY 1135 of the DSB is long under way with the UIC number 90 86 00-21 135-7, which it was assigned on 5 July 2001 in Vojens, (Denmark). Her bleached out colour paint is the one that FR also realized in the special edition presented. Photo: Peter L. Svendsen (CC-BY-SA-2.0)

It served there for forty years until her retirement on 2 June 1997. In 2010, she joined EBS (Erfurter Bahnservice) via various companies, where she was soon returned to her black and red design from the later DSB period. It remained in service at EBS until 2018, before joining Altmark Rail.

The external impression

The announcement by FR Freudenreich Feinwerktechnik at the end of 2018 of a new design of the “round noses” had surprised the model railroaders of gauge Z: After all, the large series manufacturer Märklin had already launched several models based on the same model beginning in 2005: SNCB (Era III), CFL (III) and two versions of the DSB (V / III).

In view of the engine conversions, which in the meantime had also reached the six-axle chassis used, reissues or previously unoffered variants of the NoHAB were very likely and sooner or later probably foreseeable.



The DSB original version with the article number 88633 from the year 2011 (epoch III) is the last Märklin edition of a NoHAB diesel locomotive to date.

An important goal of FR Freudenreich Feinwerktechnik and the 1zu220 shop booking a special edition should therefore be to deliver their own models before the Nürnberger Spielwarenmesse 2019 (International Toy Fair) and to use the attention they received with their announcements.

Unfortunately nothing came of it, because only four months later the first vehicles were ready.

And so it came as it had to: At the toy fair Märklin actually announced an SNCB and an MAV version of this locomotive.

In addition to the drive converted to the bell-shaped armature motor, the new models are to be equipped with revised bogie covers, which will eliminate the biggest point of criticism.

Other weaknesses will remain, but at least in weight the upcoming editions will increase significantly and therefore develop more tractive power.



Freudenreich's new model of the MY 1131 from the EBS stock has to prove itself in the test. For the first time, the same locomotive is available for order in analogue and digital versions.

Consequently, the new FR design today has to be measured against the Göppingen model, because there are only reasons to buy if it avoids the known weak points, and can also gain such an advantage that Märklin cannot be expected to open the door at eye level with product updates.

The small locomotive from Sanitz, which is available as an exclusive model for the 1zu220 shop (art. no. 49.125.01) in analogue and digital versions (DCC and sound module: additionally art. no. 00.125.00), is given a critical look at its long side when it is removed from the package. And it can truly stand up to this: This is what we think a NoHAB must look like. Shapes and proportions generally appear to be appropriate.



“First impressions count,” is an old saying. And with the former MY 1131 of the DSB (Art. No. 49.225.01), which is recorded in the NVR as 92 80 1227 003-1 D-EBS, this is definitely true. This new product comes from FR Freudenreich Feinwerktechnik.

This impression is also confirmed all around, as the small series manufacturer has done a great job here. If you are a real fan of this type of locomotive, you won't be able to avoid this model.

But probably nobody can deliver a perfect model and therefore we still look at the details and measure. We really like the engravings, which show even the finest features, especially in the roof area. Only those who take a close look can recognise individual layers of print. This reveals that the master model for the vacuum casting (plastic housing) in a silicone form came from the 3D printer.

Very finely engraved are also the bogie panels, which correctly and absolutely convincingly reproduce the design typical for types with the axis sequence (A1A)' (A1A)'. For use in Germany, however, the model lacks the Indusi magnets, which is a small mistake in relation to the MY 1131 of the EBS.

This is certainly due to the fact that FR Freudenreich Feinwerktechnik's target group is focused on Scandinavia, because the manufacturer has invested a great deal of time and effort in injection moulds that do not allow every tiny detail to be individually adapted to the individual variants. This is an economic compromise, which we also consider to be justifiable for small series manufacturers.

If we look at the model dimensions, we can immediately see the centre of gravity of the construction: As expected, Harald Freudenreich focused on absolutely harmonious dimensions and proportions. His model meets almost all model dimensions in tenths of a millimetre.



The bogie panels are also very finely engraved and correctly represent the version with a central running axle. However, the Indusi magnets between the first and second axle on the right side, which are required for a locomotive running in Germany, are missing.

Only in the bogie axle position do we measure a small, imperceptible difference, which is still within the range of measurement errors. However, the overall wheelbase seems to confirm our measurement, as it resulted in an interference of 1 mm. Märklin's predecessor model, which was built around an existing casting block, definitely surpasses the small series production.

As examples for the claim that was worked with here, we would like to mention the rail scrapers made of etched parts. They reproduce exactly the shapes of the EBS machine, the version "Railcare" (see below) produced for their own account in the FR in-house range was given a different design.

The fine coupling hook is also well thought out, which does not disturb the overall impression and safely interferes with the Märklin system couplings. After all, you don't have to be shunted with a NoHAB.

The strengths of the miniature also include separately attached handrails on the driver's cab access steps, which, however, do not quite correspond to the original model, as an image comparison of various machines reveals.

The shape, size and design of the side windows in the engine room and, above all, the driver's stands do not repeat Märklin's mistakes. They contribute enormously to the good overall impression.

Nevertheless, there are also small weaknesses in this area. The four portholes on each side of the machine room are fitted with transparent inserts, which look very accurate.

Dimensions and data of the diesel-electric locomotive MY 1131

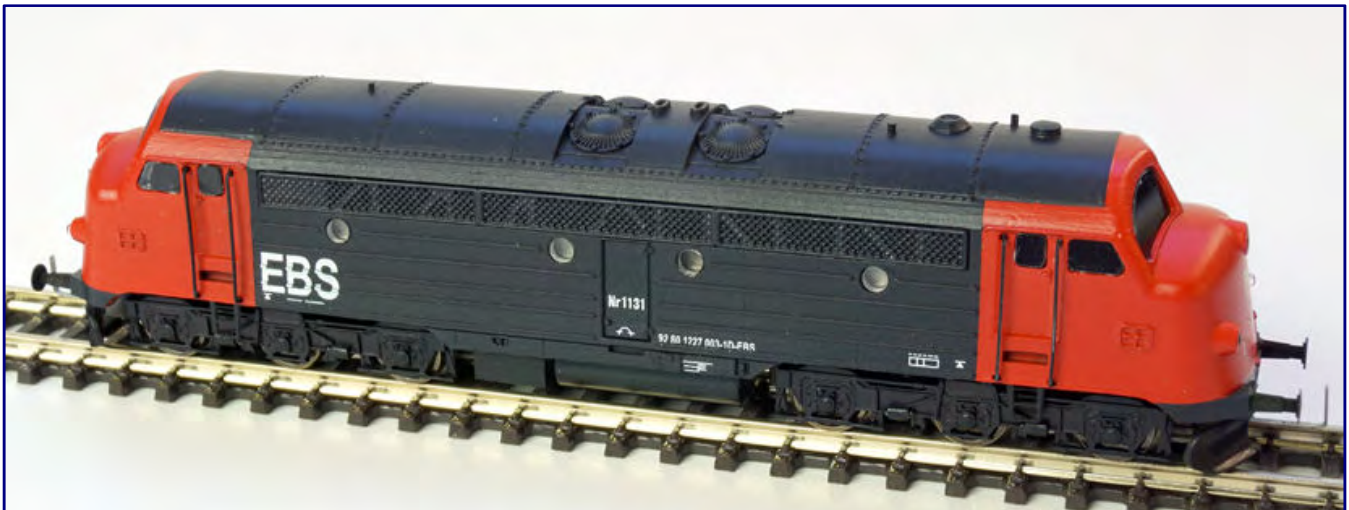
	Prototype	1:220	Model
Length over buffers	18.900 mm	85,9 mm	86,2 mm
Widest point	3.090 mm	14,0 mm	14,3 mm
Height over rail head	4.295 mm	19,5 mm	19,5 mm
Total length	14.300 mm	65,0 mm	64,0 mm
Wheelbase	4.000 mm	18,2 mm	17,8 mm
Wheel diameter (new)	1.040 mm	4,7 mm	5,0 mm
Tare weight	89 t	---	48 g*
V _{max} (Maximum Speed)	133 km/h		
Power	1.250 kW / 1.700 PS		
Design	(A1A)' (A1A)'		
Years of construction	1954 - 1965**		
Manufacturer (Locomotive)	NoHAB		
(Diesel motor)	General Motors EMD		
Quantity built	159 units***		

* Digital model 50 g

** Delivery period for all three series for the DSB (59 units)

*** Total of all production models NoHAB-AA16-Locomotives

The windows of the driver's cab can't quite match this. They have been produced with the help of shiny black polycarbonate films, which have been precisely cut and inserted. So far there would be no cause for complaint. However, if we put the analogue locomotive on the track and let it run, disturbing light emissions are soon noticeable, which appear along the window edges.



The paintwork of the model is sharp and clean, even with the (model-like) rather sparse lettering flawlessly executed (picture above left). Compared to the original, the front windows of the driver's cab look a bit too high on us (photo above left). One of the features that characterize the good overall impression is the accurate roof design with sharp engravings and fine rivet reproductions (photo below).

This means that the driver's cab seems to remain dark as a whole, as we know it from the model on the road, but the bright ring of light around the windows does not fit into this image at all. We will get to the bottom of the cause in the technical evaluation.

But also the shape of the front windows does not completely convince us: Compared to Märklin they are clearly in front, because the inventor of the Z-gauge didn't even attain the typical shape correctly. But while they are clearly too narrow in Märklin's case, in Freudenreich's they seem a bit too high for the viewer.

Unfortunately, this shows that it is not always the miniaturization to the nearest tenth of a millimetre that decides, but that some details need to be over- or under-proportioned in order to appear subjectively correct to the viewer. Well-known examples of this are rows of rivets, which would actually not be visible on a scale of 1:220. In the case of the front windows, a slightly reduced accentuation would have given us a better impression.

We would like to close our visual evaluation with a glance at the paintwork and printing: FR chose the black-red DSB paintwork with which the model for EBS was on its way. A completely correct part of this is a deep black roof area, which almost eliminates traces of ageing.

This model lacquering has been applied correctly, precisely and cleanly in its colour distribution. EBS's rather sparse company labelling, which are based on those of the Danish State Railways, were applied by pad printing. This was done with white ink in the correct size and with respect to the fonts chosen by the operator.



The turning out of the fine bogies is limited by the ladders of the driver's cab, but the manufacturer does not specify any radius limitation. Another glance is taken at the fine snow ploughs, which differ in shape from the Railcare version.

As an interim conclusion, we can state that although Harald Freudenreich did not create a perfect model, he clearly leaves the deficiencies of the large-scale production model behind him. In this test discipline, the small series model is clearly the first choice, despite a few weaknesses.

A variant from FR

FR Freudenreich Feinwerktechnik emphasizes the advantage of small-series models right alongside the Railcare model (46.125.01), which appeared in parallel and can also be optionally selected in digital version (DCC and sound module, 00.125.00 in addition). The TMY 1150 of this Swedish private railway company is also a former DSB machine.

Nevertheless, there are some differences, which have also been taken into account in the model: It has a different housing which correctly shows a door between the two lower headlights. We even got the explicit hint that there will be a third housing variant in the future, where the right driver's cab door is missing.



The TMY 1150 (46.125.01; photo above) from Railcare is not a pure colour variant, as can be clearly seen. It carries a front door below the top light (photo below left) and a different snow plough. The buffer ascents with railings also attract the attention of the observer (photo below right). Photos: Harald Möller

We had already pointed out the typical snow plough shape of the EBS locomotive. Consequently, the Railcare version has a different shape, which is also strictly based on the original. Further distinguishing feature are the shunting platforms mounted above the buffers, which were not fitted to the prototype locomotive until its late service period.

Technical evaluation

Before we look at the other aspects of this discipline, we would like to get to the bottom of the reason for the previously criticized light emissions on the driver's cab windows of the analogue version. In the digital version, there was no disturbing light at the front and hardly noticeable at the side windows.

This either speaks for a qualitative diffusion in the series or must have a constructive reason. So only "hats off" and a lookup will help. On both models, the light in each of the three top lanterns comes from three warm white SMD LEDs mounted on a small, vertically mounted circuit board that is firmly connected to the horizontally mounted main circuit board on the engine block. Only the digital model has two red LEDs at the bottom for the rear lights.

Short light guides are located in the housing openings of the front lights, which do not reach seamlessly up to the light sources. Instead, there is a gap between them, in which the light emitted by the led can spread almost freely through reflection.



The light emitted from the edges of the window is very disturbing with the analogue model (left photo). We have found the cause to be a lack of light shielding on the lighting boards and inside the housing (photo right).

So it is not only captured by the clear light guides, which are supposed to transport its glow to the outside. All the gaps in the model therefore participate in the emission. The rails of both models are illuminated to a very small extent, but it is the edges of the windscreens that make the light so unpleasant and contrary to the original.

The digital version is not (visibly) affected by this because it has a horizontally glued shielding part between the upper and lower light emitting diodes. From this we concluded that mainly the lower row is responsible for the phenomenon.



The digital version is not (visibly) affected by this because it has a horizontally glued shielding part between the upper and lower light emitting diodes. From this we concluded that mainly the lower row is responsible for the phenomenon.

The weak point of the analogue locomotive could have been avoided if it had been equipped in the same way, or, alternatively, the windows had been covered from the inside with additional opaque material. According to our ideas, this could have been done with a black adhesive tape, for example, which the buyer could possibly do himself.

In their basic forms the printed circuit boards are identical, although of course differently equipped: We have already mentioned the main board with the light boards plugged in at the ends.



...the five LEDs on each side of the digital version are shielded by an insert between the upper centre mounted light and the lower lights. This small part has a significant effect, because unwanted light emissions can barely be detected with this model.

In addition, to the additional tail lights of the digital version, the essential and immediately recognisable differences are the front loudspeaker from the mobile phone area, which radiates upwards into the housing, and the actual decoder, which sits on its own circuit board at the rear under the main plate.

This solution was developed by Velmo on the basis of the D&H SD18A decoder with NEXT18 interface from Doehler & Haass especially tailored for the FR novelty. Therefore, the purchase of a DCC noise module includes the manufacturer's installation in the locomotive purchased at the same time. The decoder operates on the DCC protocol, which is widely used internationally in two-wire digital operation.

Later retrofitting would be technically possible in principle due to the modular design: According to Harald Freudenreich, two screws and six solder joints on the system boards require changes.

The acquisition of a digital model is definitely worthwhile from our point of view. Once you've gotten to know its advantages, you'll quickly become a digital enthusiast: In addition to comfortable driving, the digital extension offers 21 noise and light functions, of which only 19 are listed in the enclosed brief instructions.



The side view allows an idea of how the three PCB elements are connected to each other. The analogue locomotive looks very tidy due to its low proportion of electronics inside.

During test operation on Märklin's Mobile Station 2 with 10 Volt track voltage, we were only able to test the first 16 functions, which convinced us all round. To call up all functions, a suitable central control unit with a larger operating scope is required. The function key assignment for the NoHAB looks as follows ex-works:

Light functions

- F 0 (Light) direction-dependent headlights and taillights
- F 1 Dimming / Fading of the peak signal
- F 2 Switch off top signal / taillight to driver's cab 1
- F 3 Switch off top signal / taillight to driver's cab 2
- F 4 Shunting gear and shunting lighting (top signal on both sides)

Sound functions

- F 5 Horn
- F 6 Ballasting
- F 7 Compressor
- F 8 Starting / Operating sound / Stopping
- F 9 Fan sound
- F 10 Coupling / uncoupling
- F 11 German Railway Station
Announcement (not mentioned in
the product sheet)
- F 12 Danish station announcement
(not mentioned in the product sheet)
- F 13 Dynamic brakes
- F 14 Compressed air release
- F 15 Braking sounds



Only in the digital version does the locomotive also have (switchable) red taillights.



The direction-dependent lighting is activated via function F 0. The digital locomotive then shows the faded-up peak light at the front (left picture). When the F 1 function is activated, it is dimmed by the decoder (right picture) - an effect that is hardly visible on the photo, but is much better in reality.

Special functions

F 16	Operating sounds on- / off (Tunnel operations)
F 17	Volume gradually lowered
F 18	Volume gradually louder
F 19	Switch off squeaking brakes
F 20	Idle running

In the delivery state, the decoder for DCC is preconfigured with the short address 3 and 28 / 128 speed steps. The preconfigured volume of all operating noises appears appropriate and not disturbing. The sound spectrum is also really impressive, especially in view of the tiny size of the model and speaker.

The NoHAB design has also been well thought out in other respects: As we traditionally know it from Märklin models, the plastic housing snaps onto the solid chassis block and can be pulled off by spreading. In our test, the structure of the digital test candidate was much tighter than that of the analogue one.

This is important to know because both models do not come with a manufacturer's care and maintenance manual. The removal of the "little hat" is therefore not explained anywhere. It is driven by a bell-shaped armature motor of the Maxon RE8 type via a worm and plastic gears on all six axles.

The bogies are guided via inserted pins in the grey painted chassis block which extend through the centring point of the large gears - this is also a Märklin standard for many years. However, the current is not transmitted to the motor via sliding contacts, but via strands. This means that bogies and frames are firmly connected to each other during operation.

The curves are limited by the separately attached etched parts for the ladders to the driver's cabs. In the test cycle our candidates were driven through a radius of 220 mm, the manufacturer does not specify the minimum curve on which the locomotive can be operated.



In addition to the lighting and special functions, the digital version also has operating sounds and two platform announcements available. Here, we just start the operating sounds via function F 8, where the locomotive first starts the starter, and, after that the diesel engine and generator can be heard.

However, a test track on the industrial circle of 145 mm did not cause any problems surprisingly, but the locomotive does not look good at such radii, because the coupling hook and snow plough swivel far out to the sides with such small radii. It also became apparent that we had reached the limits of the curve running capability here at the latest. Therefore, we decided not to go for further tests at such radii to protect the fine ladders.

A large brass flywheel mass mounted on the drive shaft ensures good running smoothness, which not only ensures safe driving over currentless sections and points at creep speed: in addition to the milled brass main frame, it also contributes to the high weight, which is responsible for good traction. The models do not have traction tires, but this favors the safe power consumption of all six axles by means of wheel grinders.

While Märklin is now saying goodbye to the drive of the centre axle on three-axle bogies, FR has consciously decided to include them in this design. It is therefore all the more surprising that there are no pendulum axles on the model that would guarantee constant track contact for all wheels. The centre wheels also do not have an upwardly offset position, as was once the case with the models from Göppingen.

From our point of view only the open gearbox of the NoHAB deserves real criticism. All gears are completely open at the bottom, and covers are completely missing. During layout operations, the gearbox could therefore also collect dust and dirt and distribute such on the gears. Lubricants favour its adhesion and shorten the maintenance intervals.

We consider this to be relevant insofar as the small-series model proves to be a real driving miracle and is certainly too good for the display case. This brings us now to the values of the technical evaluation to be measured.

Even at a track voltage of 0.3 volts, the small locomotive hardly moves visibly. With the digital model, this happens in speed stage 2. The measured starting speed, converted to the model, is only 0.4 km/h and thus the lowest value that we have ever measured. In analogue mode there is no light from the warm white LEDs that is perceptible.



The D&H decoder shows an amazingly good sound spectrum with the loudspeaker on the left upper side of the circuit board (picture above), which originates from mobile phone technology. Claudius Veit mounted it on the right side below the main PCB. We are not convinced by the open gearbox of the NoHAB (picture below), because here the door and gate are opened to dirt.

This changes when we increase the track voltage to 1.6 volts. The tiny muscleman, which with ten attached bogie (4 axle) tank wagons does not reach the limits of his tractive power even when cornering, now sneaks safely over every switch at walking speed (5.2 km/h).

The locomotive can be comfortably and finely controlled in all speed ranges. It also travels much slower than the elderly NoHAB interpretations from Göppingen, which is good for prototype-like operations.

But at the equivalent of 275 km/h, it can also compete with any ICE at full track voltage of 10 V. However, the operator will not adjust it so high, because transmission noises will then also be quite noticeable.

This also applies to the digital version at the highest speeds, where the operating sounds are additionally superimposed. Slow driving in particular, however, is an optical and digital pleasure, as well as an acoustic one. Seldom has the operation of a Z gauge locomotive provided so much pleasure.

The measured current consumption of the model is unobtrusive, as the motor and light source are not power guzzlers. With transformer position 100 it is 41 mA, with position 150 it rises to 65 mA. This also suggests a smooth-running gearbox, as the other values have already suggested.

Use of the model

According to the range of services offered by the Erfurt Railway Service, the main focus of its activities is on hiring out machines, operating special trips, long-distance services and operating sections of track that are difficult to negotiate shortly before a train reaches its destination. The transport of dangerous goods is explicitly highlighted.

The main focus is on rail classics from various series, which are used in historical colours. The application possibilities of the FR model also depend on this.



Model photos show EBS locomotive 1131 in front of tank wagon trains, among other places. Märklin's articulated tank cars as the most modern representatives would certainly be even better suited, but machines from Erfurt's railway service can also be parked in front of museum passenger trains.

First of all, historical passenger train sets of any kind are conceivable, as they are used for special trips. An arbitrary example would be TEE car sets such as the trains marketed as "Rheingold." Many other combinations do not appear to be far-fetched either.

Freight trains, too, are occasionally historically compiled and operated for photographic purposes. However, we find any shorter sets of modern wagons to be more commonplace here. However, we found an interesting model of the converted locomotive in front of modern tank cars.

This train can be reproduced, for example, with Märklin's articulated tank wagons (82530 train set and single wagons; individually also offered aged). In addition to the dark grey VTG specimens, the other versions can also be used.

For fans of Scandinavian models, FR Freudenreich Feinwerktechnik itself offers a larger selection of suitable models, which can be assembled, as desired. There are certainly plenty of models to be found in the worldwide network. Märklin can optionally add the car package "Green Cargo" (82532) to the list.

Our overall impression

In Märklin's NoHAB model, the bar was not really high, which probably challenged Harald Freudenreich. It seems courageous to consciously compete against a large-series model and to enter into a double development.

With their distribution network, the Göppingen company has completely different and much more possibilities to sell their models. FR Freudenreich Feinwerktechnik, on the other hand, has built a regular clientele in Scandinavia in particular, whose demands Märklin's model probably did not satisfy: What the V 200 series is for German model railroaders, has a counterpart there in the NoHAB AA16.



For her circle of fans the NoHAB AA16 is not just any locomotive: It enjoys a high reputation and is popular as for example the V 200 of the DB in Germany. That's why FR Freudenreich Feinwerktechnik has deliberately thought of its customers in Scandinavia with a harmonious model in which the lifeblood of the construction cannot be overlooked.

With this we have described the motives for the new model aptly. The task was solved with bravura, even if not completely error-free. There is still, even if little, room for further improvement.

Clearly the bar has now been raised considerably and it is already foreseeable that Märklin will not be able to beat them in the optical evaluation. Whether they will catch up technically remains to be seen.

This lets customers decide whether they want to give priority to the price or the precision with which the model features were implemented. If you want a NoHAB that is coherent and shines with strong driving performance, you can't get past the FR. Many a Zettie will now seriously consider switching to digital operation at the earliest possibility, we believe.



We nominate the basic model (49.125.01) and the optional digital upgrade (00.125.00) for the best new releases of 2019 in the categories locomotives and technology.

And that is why we nominate the MY 1131 of EBS (Art. No. 49.125.01), representing this model series, as a candidate for the best new releases of the year 2019 in the category locomotives. Specifically included is the optional digital version (00.125.00) with operating sounds.

This is a premiere in that never before has a model been represented in two categories at the same time.

- **Manufacturer of the model:**
• <https://www.fr-model.de>
- **Exclusive cover of the tested EBS version:**
• <https://www.1zu220-shop.de>
- **Digital components used:**
• <https://doehler-haass.de/cms/>
• <https://velmo.de>

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Märklin on new paths

Increasing instead of losing weight

A manufacturer from Japan has shown the way, unnoticed by many, and now Märklin is following suit: With metal filled plastic, the Göppingen market leader opens a new chapter in model railroad production and pursues completely different goals than the earlier pioneer. We had the technology explained to us and tried to shed light on its possibilities and limitations. A comparison of the various production processes provides further information.

Plastics and metals have only a few properties in common. They usually behave extremely differently and are used, therefore, for different purposes: Plastics are very light compared to metals and have a higher degree of flexibility.

Plastics started to be widely used after WWII. New and ever more types of plastics were developed to meet very different and increasingly special requirements. With the rise of plastics, the chemical industry also strengthened and grew to its present importance. In retrospect, it should come as no surprise to us that plastics also led to a fundamental transformation of the model railway sector.



In 2019, Märklin launches a new edition of an SNCB “potato beetle” in SNCB livery. This will be the first time that Märklin will use its new metal filled plastic process with an already existing model. As a result, the loco will have a much higher weight on the tracks.

At that time, locomotives and wagons were mostly made of thin sheet metal, and Märklin was an early adopter of heavy die-cast zinc alloy shells. Model buildings, initially made of cardboard and wood, soon



Minitrix product manager Claus Ballsieper illustrates the idea of the new technology for Märklin: “The housing with tungsten alloy (right) is considerably heavier than that made of ordinary plastic (left).”

became mass produced articles, once accessory manufacturers such as Faller turned to plastic injection moulding.

In recent years, manufacturers have increasingly been confronted with the desire for more individual models. At the same time, the willingness to compromise on the quality of rolling stock has diminished, whilst the overall customer base has, unfortunately, also shrunk.

Fewer customers demanding higher quality products requires producers to rethink in many areas, with answers being found in rediscovering materials such as hardboard and the use of new techniques such as laser cutting or 3D printing.

In the meantime, Märklin has pursued for many years a further approach and is now, in 2019, finally in a position to integrate it into its series production.

The aim is not to adapt to lower quantities of a model, but to produce any number of models as economically as possible and with the highest possible weight.

Traditionally, the market leader has relied on zinc alloy die casting, because the resulting heavy metal body shells are meant to convey to customers a sense value for money for the quality products from Göppingen.

At the same time, however, plastic body shells, frequently made from ABS (acrylonitrile butadiene styrene), can be injection moulded at lower costs because of the lower required pressures and demands on tooling, compared to metal die-casting.

So why not combine the advantages of both processes? This thought is precisely at the heart of a long development and learning process at Märklin, which ultimately led to success with the introduction of metal filled plastic.

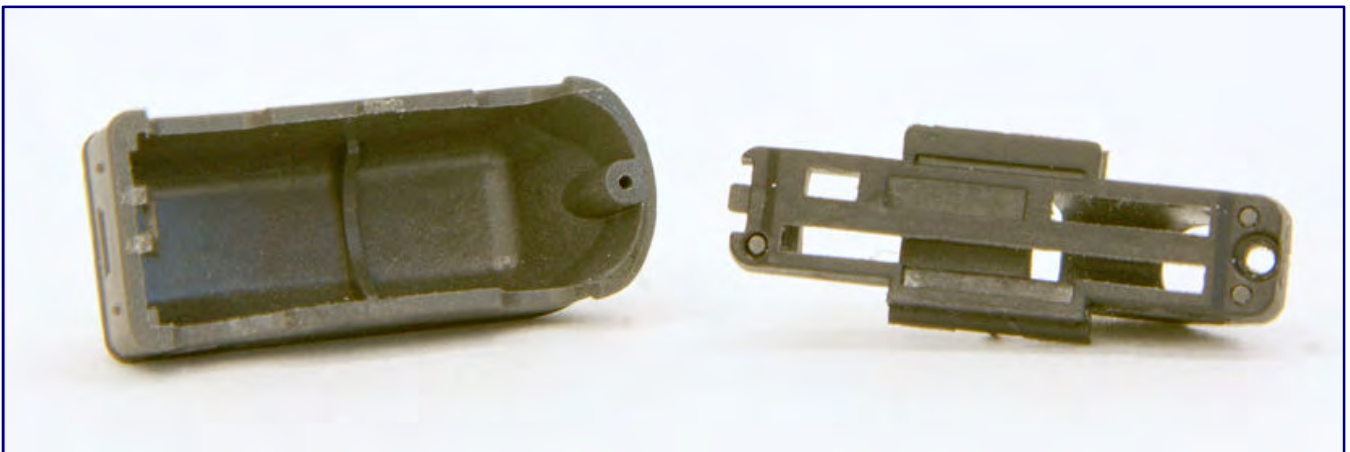


The addition of metal particles changes the colour of the material: The ABS plastic body shell is blue, whilst the metal filled plastic shell of the class 103 locomotive takes on the grey colour of the tungsten metal.

Metal filled plastic

However, Märklin is by no means a pioneer in this field, although it is the only European manufacturer that currently uses this production method. The first to introduce it in the model railway sector was the Japanese manufacturer Kato at the Nürnberger Spielwarenmesse 2006 (2006 Nuremberg Toy Fair).

At the time, Kato presented an American container freight car, where the addition of ferrous metal to the plastic served two purposes: improved running abilities through higher weight and the quite elegant solution to stacking and fixing containers through magnetic force, as each container was equipped with a metal plate and a magnet.



The Z gauge version of the class Kl_v 20 rail van is the first and currently only newly designed model that will be produced with metal filled plastic. As shown here, both the chassis base and the body shell are made of the new material.

Märklin, however, is pursuing a different path, which does not involve the use of a ferrous metal. To date, three motorised models have been presented: two versions of the NoHAB diesel locomotives in Hungarian and Belgian livery and the small DB class Kl_v 20 rail van.



As for H0 scale, the possible uses for metal filled plastic are significantly less than for the smaller scales, but the Era I "Glaskasten" (glass box) lightweight tank locomotive presented at the Nürnberger Spielwarenmesse certainly will benefit from the new material.

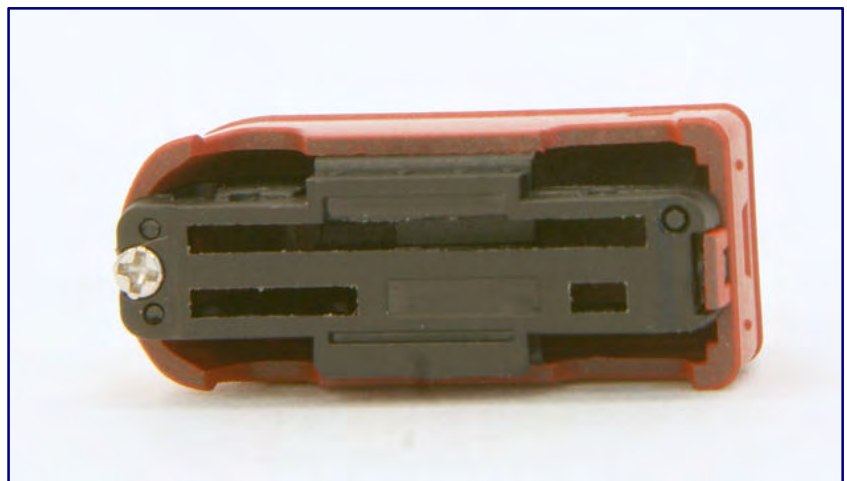
Märklin's ongoing switch to new types of motors and a corresponding loss in weight in the die cast chassis frames created a need for adding weight in other places. This is most evident with the class KlV 20 rail van whose tiny dimensions limit its attainable weight in a critical way. After all, there is a direct relationship between weight and tractive force, as well as, continuous current collection from the track.

Traction tires offer no solution, because they do not solve the basic problem of ensuring a reliable contact with the tracks. On the contrary, they would limit the number and total area of contact points with the track and would thus have an additional negative effect on the power supply to the motor.

Using magnetic force to increase the contact pressure on the track, such as with Busch's 1:87 scale narrow gauge light railway on 6.5 mm tracks, would not be a solution either, because of the Z gauge non-magnetic nickel silver rail profiles.

Lead weights are also taboo, as their toxicity does allow for their usage in toy products.

The perfect solution would therefore be to combine the light and robust ABS plastic shells with the heaviest possible metal, without creating health risks for customers



A clip and screw mechanism connects the class KlV 20 rail van chassis with its body shell. Producing a motorised version of this tiny model is quite a venture and a great challenge for Märklin.

This is exactly the combination which Märklin and its external partners have been developing for years, until they managed to successfully test and integrate metal filled plastics in their regular production injection moulding process.

This year, Märklin plans to produce several models with metal filled plastic shells: one model in H0 (Class PtL 2/2 steam loco ("Glaskasten")), and two each in N gauge ("Crocodile" and a DB class 103) and Z gauge (two versions of the NoHAB and the KlV 20 rail van). With the exception of the KlV 20, all models will be new editions of existing models and will thus allow for a direct comparison with earlier versions.

All of these models will benefit from the combination of extra weight (possible until now only with zinc alloy casting) and the fine detailing possible with plastic injection moulding. The impact will be mostly felt at the Z and N gauge level, but also with the smaller and lighter H0 models.

A significant increase in weight is possible because 70 % of metal (in terms of weight) is added to the plastic granulate. In theory, many types of metals could be used for this, but the choice was made for tungsten because of its high density and mass. The melting point of tungsten is at 3,422° C, meaning that its solid particles remain preserved in the significantly cooler, liquefied plastic material. They are injected together with the molten plastic into the moulds and firmly bond with the plastic during solidification. Technically, the result is a so-called composite material, as was already the case with the Kato process.



This class 144 body shell was part of Märklin's trial runs to master the process of working with metal filled plastic. A close look at the unpainted shell reveals that some fine details have been more clearly rendered than is the case with an earlier production model made of conventional plastic (Art.-Nr. 8811).

It is somewhat less flexible than a pure plastic and also differs from it with respect to tensile and compressive strengths. Nevertheless, it is still very distinct from a pure metal, which is particularly evident from the absence of electrical conductivity. And the fact that it does remain a plastic is a prerequisite for producing the same locomotive shells which were previously moulded from pure ABS plastic.

A key thing Märklin had to learn to master was controlling the shrinkage of the new material so that all parts would continue to fit and, at the same time, remain true to scale. The limits of the process became apparent during the development and testing phase, and Märklin had to realize that it cannot continue using all of its existing injection mould tools under the new process.

For example, the moulds needed to be adapted with respect to heating and cooling during injection process: Instead of hot runners, cold runners were now required. Injection parameters such as material temperature and injection pressures also had to be adjusted, and even the colour treatment of the ejected shells sometimes required some modifications.

Märklin also performed climate change tests in order to prove that the new material met all of the physical requirements necessary for model railways and in order to avoid any unwanted surprises. And the tests also showed that the new material makes for an often better representation of the finest details, compared to conventional materials.

During the many years of developing the new process and getting it ready for the market, Märklin sometimes had to learn the hard way. But the considerable investment seems to be justified as it secures Märklin a unique selling point and puts it clearly ahead of its competitors across all model railway scales.

Effects of the new manufacturing technology

Cost savings, as claimed by many observers, were not a consideration to introduce the new material, especially as the metal filled plastic granulate costs ten times more per kilogram than conventional ABS plastic. However, raw material costs account for only a small part anyway of the total cost of the entire production process.

However, one cost driver that does need to be taken into account is the higher rate of mechanical abrasion in the moulds, caused by the grinding force of the fine metal particles. This decreases the overall service life of the moulds, but is acceptable and can be mostly neglected due to the fact that the output in numbers for model railways is rather small compared to other larger scale industrial processes.



Especially this tiny Z gauge model will benefit from the weight gain caused by metal filled plastic. Although the Kliv 20 is more on a scale of 1:200, it is hardly larger than the tip of a pencil.

Our photos also show a class E 44 body shell made of metal filled plastic. It can be directly compared with the pure plastic shell of the same model, which was once produced under article number 8811



The smaller a model, the more effective the metal alloy plastic becomes. Mini-Club product manager Jürgen Faulhaber knows this very well and is therefore firmly committed to this technology.

(picture on page 27). We show this model because we did not have any comparable NoHAB parts at the time of writing.

This is one of many attempts to master the new process and to gain sufficient experience to venture onto the market.

In addition to other material properties already described, there was also the question of how the new material would perform in terms of flexibility.

The body shells should hold securely on to the chassis and at the same time are spread for detachment without breaking.

Let us now take a final look at the weight gains that can be achieved with metal filled plastic.

Not surprisingly, we find that the new material shows its full potential in the Z gauge segment of the market.

We can explain this by taking the already mentioned shell of the class E 44: whilst its previous ABS body only weighed 3 grams, its metal filled plastic equivalent comes out at a rather hefty 14 grams, almost five times as much as before. Using a zinc alloy die-casting process would have yielded a weight of about 16 to 17 grams, which is only slightly more than with metal filled plastic.

The "Potato Beetle" (NoHAB) shell has increased from 4 grams (plastic injection moulding) to 19 grams for the metal filled plastic. This mass is also very close to what could be achieved mathematically with zinc alloy die casting (approx. 22 grams). This is, of course, a purely mathematical exercise, because the injection mould cannot be filled with zinc and may also require different material thicknesses.

Especially, the class KlV 20 small rail van is true to its official name. With a shell weight of 0.4 grams, a normal plastic body would have contributed nothing to the traction, with the likely result that the model would have hardly moved or would have stopped for lack of contact with the track.

With the alternative technology, however, the body shell arrives at a weight of 2 grams, heavy enough together with the chassis and motor to make for a usable model. Using the more expensive zinc alloy die-casting moulds would not have made a significant difference in this case.

We are now looking forward to the first deliveries of and experiences with the new models. In the case of the NoHAB diesel locomotives, we are sure that we will also hear from some of our readers about their first impressions and how the new models measure up to previous editions. At least the new bogie frames will make for a more authentic and interesting look for this locomotive.

Manufacturer pages for the topic:
<https://www.maerklin.de>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Industrialisierung und Verkehrsentwicklung **Blick in Dortmunds Eisenbahngeschichte**

Viele Leser sind vermutlich Anfang April mit der Bahn zur Intermodellbau angereist. Da lohnt sich angesichts des thematischen Schwerpunkts dieser Ausgabe ein Blick auf die regionale Geschichte dieses wichtigen Verkehrsträgers. Die Deutsche Gesellschaft für Eisenbahngeschichte tritt auch als Verlag auf und hat der größten Stadt Westfalens einen neuen Band in der Reihe ihrer Monografien gewidmet.

Rolf Swoboda, Norbert Tempel und Wolfgang Fiegenbaum
Die Eisenbahn in Dortmund

DGEG Medien GmbH
Hövelhof 2018

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Bereits fünfzehn Jahre ist die erste Auflage eines Buches unter diesem Titel schon wieder her! Vergangen ist eine eisenbahngeschichtlich lange Zeit, in der viel geschehen ist und die es nun fortzuschreiben und zu aktualisieren galt.

Der Basisteil des vorliegenden Werks hat seinen Umfang allein dadurch bereits verdoppelt. Damit sollte der vorliegende Titel auch für alle diejenigen interessant sein, die bereits das ältere Werk ihr eigen nennen und unverändert Wissensbedarf an der regionalen Bahngeschichte zeigen.

Doch warum widmen die Autoren der Eisenbahngeschichte Dortmunds überhaupt ein eigenes Buch? Die Antwort ist einfach: Die größte Stadt Westfalens und des Ruhrgebiets war und ist ein Eisenbahnknotenpunkt herausragender Bedeutung. Dortmund gehörte zu Bundesbahnzeiten zu den größten Zugbildungsbahnhöfen der Bundesrepublik Deutschland.

Der Start für den Aufschwung der Eisenbahn in der einst blühenden Hansestadt, die nach dem dreißigjährigen Krieg einer Bedeutungslosigkeit verfiel, in der Folge klein und längst auch unbekannt war, gab die Köln-Mindener Eisenbahn. Sie erreichte Dortmund als erste und baute hier auch große Anlagen, die dem Unterhalten ihrer Fahrzeuge dienten.

Aber in der zweiten Hälfte des 19. Jahrhunderts trafen hier dann auch die übrigen großen Eisenbahngesellschaften Preußens aufeinander und legten den Grundstein für einen raschen



industriellen Aufschwung: Das Land brauchte wie die Dampflok Kohle, und nur die Eisenbahn war in der Lage, sie in Massen über größere Strecken zu transportieren.

Das vorliegende Werk betrachtet diese Entwicklung ganzheitlich. Behandelte Themen sind die geschichtlichen Abrisse von der Privatbahnzeit über die preußische Staats- und Deutsche Reichsbahn, die Bundesbahn bis hin zur Deutschen Bahn AG.

Revue passieren lassen die Schreiber den zunehmenden Schienenverkehr, der bald wichtige Straßenwege am großen Bahnübergang nahe dem Hauptbahnhof lahmlegte und der als Konsequenz folgenden Verlagerung und Höherlegung von Station und Trassen. Zäsuren bedeuteten die Ruhrbesetzung durch Frankreich und Belgien sowie der Zweite Weltkrieg mit einer völligen Zerstörung der Innenstadt und des imposanten Hauptbahnhofsgebäudes.

Einbezogen in die Betrachtungen sind aber auch alle übrigen Personen- und Rangierbahnhöfe, die Betriebswerke und betrieblichen Brennpunkte, Strecken- und Technikgeschichte. Hier erwähnen wir kurz den früheren Rangierbahnhof Dortmunderfeld, den viele als Heimatbahnhof der Bierwagen Dortmunder Brauereien kennen.

Nicht vergessen wurden die ungewöhnlichen Brückenkonstruktionen, ohne die das Höherlegen der Gleise ohne niveaugleiche Kreuzungen nicht möglich gewesen wären. Dortmunder Brückenbaufirmen wie Klönne und Jucho waren weltweit bekannt und durften bei den Betrachtungen daher auch nicht fehlen.

Eine ebenso bedeutende Rolle spielt die aus der Hafen- und Kleinbahn hervorgegangene Dortmunder Eisenbahn GmbH (DE), die bis heute existiert und deshalb auch umfassend portraitiert wurde. Nur eine kurze Nebenrolle spielen hingegen die früheren Werksbahnen der Stahlbetriebe.

Mit dem Erreichen Dortmunds durch den Fahrdracht 1957 und dem Aufbau eines S-Bahn-Netzes bis in die achtziger Jahre gelangen wir langsam schon in Richtung Gegenwart. Etwas fremd im Buch, wenn auch nicht minder interessant, ist die von Siemens entwickelte, führerlos fahrende H-Bahn, die den Universitätscampus, den S-Bahn-Halt und einen von den Studenten bevorzugten Wohnort miteinander verbindet.

Das alles wird auch durch gut ausgewählte, aussagekräftige Fotos und Abbildungen unterlegt. Verständlicherweise ist allerdings die Pionierzeit der Eisenbahn nur recht dürftig bebildert, für die Zeit nach der vorletzten Jahrhundertwende konnte auf deutlich mehr Aufnahmen zurückgegriffen werden.

Einen kleinen Kritikpunkt gibt es allerdings auch: Als den Lesefluss störend haben wir die fast durchgängig falsch, das heißt nicht im bahnamtlichen Sinne, benutzten Baureihenbezeichnungen von elektrischen sowie dieselbetriebenen DB-Fahrzeugen empfunden. Für die Zeit nach dem 1. Januar 1968 lesen wir hier nämlich beispielsweise von „V 260“, „V 290“, „E 111“ oder „E 141“.

Der Ersatz der Buchstaben V und E zum 1. Januar 1968 durch die Ziffern 2 und 1 an erster Stelle der neuen Baureihenbezeichnung dürfte allerdings auch den Autoren bestens bekannt sein. Daran haben wir angesichts der deutlich erkennbaren Fachkenntnis keinen Zweifel.

Deren fundierte und gewissenhafte Arbeit ist es dann auch, die diesen Band so empfehlenswert macht. Und das gilt für alle, die an regionaler Eisenbahngeschichte interessiert sind, mit Dortmund und dem Ruhrgebiet irgendwie verwachsen sind, oder einfach authentische Eindrücke und Wissen zu Themen für ihre Anlage nach Motiven der Montanindustrie suchen.

Auch wer einfach erleben möchte, wie wandlungsfähig eine Region, eine Stadt und ihre Infrastruktur sind, wird von diesem Buch nicht enttäuscht.

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Ein Überblick über die Welt der Modellbahn Viel Wissenswertes und Kurioses

Was muss ein Modellbahner wissen und was nicht? Darüber ließe sich trefflich streiten, doch der Geramond-Verlag gibt eine aus seiner Sicht eindeutige und klare Antwort dazu. Wir haben uns das jüngste Taschenbuch aus einer unterhaltsamen und informativen Reihe angeschaut, um festzustellen, wen dieser Band ansprechen wird und wer ihn braucht.

Martin Menke / Peter Wieland
101 Dinge, die ein Modellbahner wissen muss

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Die Autoren Martin Menke und Peter Wieland zeichnen seit vielen Jahren für die Modellbahnredaktion des Eisenbahnmagazins verantwortlich. In dieser Funktion sind die beiden mit ihren Arbeiten langjährigen Lesern auch schon aus den Zeiten vor Übernahme der Alba Publikation durch Geramond bekannt.

In der bekannten 101-Dinge-Reihe des Verlags, die weit über den Bereich des Modellbaus hinausgeht, schien es nun an der Zeit, auch zum Thema Modelleisenbahn nützliches Wissen sowie interessante oder amüsante Fakten einzufangen.

Immerhin begeistern bereits seit dem 19. Jahrhundert verkleinerte Modelle alte und jungen Menschen. Sogar Johann Wolfgang Goethe soll drei Jahre vor seinem Tod, also 1829, eine Miniatur der englischen „Rocket“ für seine Enkelin geschenkt bekommen haben.

Ob derartige Dinge tatsächlich zum Pflichtwissen eines Modellbauers gehören, bleibt der Sichtweise und Meinung jedes einzelnen Menschen überlassen. Fakt ist aber, dass es einen sinnvollen Rahmen gibt, der beim Orientieren im weiten Feld von unterschiedlichen Maßstäben, Spurweiten, Betriebsarten, Epochen usw. helfen wird.

Und eben den haben die beiden Autoren zu sortieren, zu strukturieren und in diesem Band wiederzugeben versucht. So soll auch dieses Werk helfen, persönlichen Zugang zum Titelthema zu finden und den Leser dafür zu begeistern, was ruhig als Versuch einer Nachwuchsakquise verstanden werden darf.

Ob das gelingen kann, vermögen wir hier nicht zu bewerten. Dafür spricht aber, dass der Preis gering ist und sich das Taschenbuch auch als Mitbringsel oder kleines Präsent eignet, zumal es selbst in größeren Bahnhofsbuchhandlungen zu finden ist. Beleuchten wir aber den Nutzen für gestandene Modellbahner, die auf der Suche nach einer etwas unterhaltsameren und eben mal weniger tiefgreifenden Lektüre sind.



Ausdrücklich ausnehmen von unserer Wertung möchten wir das Kapitel 26 (Seite 60) zur Spurweite Z, denn wie bereits im Vorwort zu lesen handelt es sich hier in Wort und Bild um eine konkrete Zulieferung des Rezensenten, der natürlich versucht hat, die vielen Facetten des Maßstabs 1:220 kompakt zusammenzufassen.

Dem genannten Vorwort entnehmen wir aber auch viele weitere Namen, die als bekannte Experten beispielsweise für die Bereiche historischer Modelle, der Deutschen Reichsbahn in der DDR, der Modell- oder Digitaltechnik samt deren Geschichte gelten. Das verspricht in Summe auf jeden Fall ein gelungenes und vielseitiges Buch.

So erstreckt es sich über ein sehr weites Feld, das mit Anlagenformen, deren Unterbau und Verstaumöglichkeiten beginnt und sich vielen, teilweise auch sehr speziellen Facetten zuwendet. Dazu gehören beispielsweise Industriethemen, Kopfbahnhöfe, Zahnrad-, Schweb- und Straßenbahn oder auch Bahndienstfahrzeuge.

Selbst der Modelleisenbahn auf Briefmarken und Film haben die Autoren je einen Abschnitt gewidmet, ebenso haben sie die Gleisreinigung, den 3D-Druck, Modellbahnbörsen und Auktionen nicht vergessen. Wissenswert ist sicher auch das Benennen prominenter Modellbahner des In- und Auslands. So kamen allerlei, teils auch kuriose, Fakten zusammen.

Eingang gefunden haben aber selbstverständlich auch klassische Themen wie Bahnübergänge, das Gestalten von Wasserflächen, Begrünen der Landschaft oder Lokschuppen. Damit wird das Taschenbuch zu einem abwechslungsreichen Ratgeber mit vielen Grundsatzinformationen zum Themeneinstieg. Tabellarische Übersichten runden dies ab.

Nicht ideal, aber mit der gewählten Gliederung leider nicht anders zu lösen, ist der Umstand, dass viele Informationen zu einem Themenfeld auf viele Kapitel verteilt vorzufinden sind. Gefundene Schwächen zeigen sich vor allem im Mittelteil des Titels, was vor allem die Textpassagen betrifft.

Hier zeigt sich das aus unserer Sicht deutlich unterschiedliche Können der beiden Autoren. Wortwiederholungen, eine bevorzugte Ausdrucksweise in der „Leide-Form“ und mit regelmäßiger Hilfe des Pronomens „man“ belegen kommunikative Defizite und wirken persönlich distanziert.

Dies findet seinen Gipfel in holperigen und umständlichen Formulierungen, die an den Schriftverkehr eines Untertans mit kaiserlichen Behörden erinnern und daher seit über einhundert Jahren als überholt gelten dürfen. Hier hätte es ganz klar redigierender Eingriffe bedurft!

Doch auch die farblich und ebenso im übrigen Druckbild gute Wiedergabe der ausgewählten Aufnahmen findet an einigen Stellen ihre Grenze in handwerklichen Fehlern des Fotografen. Dies betrifft dann Ausleuchtung und Schattenwurf oder auch nur allzu sichtbare Unschärfen über weite Strecken eines Bildmotivs. Besonders ärgerlich aus Sicht des Käufers ist das etwa beim Foto von Stuttgart Hbf aus der Anlage von Wolfgang Frey, welches im Buchdeckel abgedruckt wurde.

Hier wurden ganz klar Chancen vertan, die aber auf keinen Fall zu einem Abraten vom Kauf dieses Werkes führen. Beachtenswert ist und bleibt die erstaunlich breite Themenstreuung über ganze 101 Themenpunkte, die nicht leicht zu bedienen waren. Und so ist das kleine, aber feine Taschenbuch wohl auch jeden Cent des verlangten Preises wert.

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Intermodellbau Dortmund Visiting the construction site

The biggest spring fair for the general public is always Intermodellbau in Dortmund. The most recent edition from April 4 to April 7, 2019, also fits in seamlessly here and was also very well attended in terms of the Z gauge. We report on what could be seen there across all scales and divisions and, of course, also take a targeted look at the important junior product ranges.



Some key data for Dortmunder Intermodellbau from 4 to 7 April 2019 quickly illustrate the importance of this trade fair not only for model railways as a traditional driving force: 87% of the trade fair visitors have already visited the exhibition in recent years, almost all the federal states are represented in the Westphalia metropolis and about 10% of the guests come from abroad.

With 77,000 visitors, this important trade fair, which has also played an important role for Z gauge for many years, has shown itself to be stable in its latest edition. The approximately 500 exhibitors came from a total of 18 countries, all exhibition areas were occupied with Halls 1 to 8, but the numbering had changed within these halls.

One change that visitors immediately noticed on arrival was the continuing construction site due to the redesign of the exhibition grounds, including entrances and the large lettering "Messe Dortmund" on the roof of the entrance area.

He referred to a reorganization carried out shortly before Intermodellbau: With immediate effect, the rich events, congresses and trade fairs will be organized and responded to by separate subsidiary companies. Intermodellbau will therefore be organised by Messe Dortmund GmbH, with immediate effect.



The offers for children and young people are becoming more and more important. These included the "Track 5" area in Hall 4, where this little model railway enthusiast can pursue his play instinct to his heart's content.

The former and well-known Hall 1 for concerts, television recordings, sports and evening events has been removed from the number concept. It is now again what it actually was for the Dortmund-based company earlier: the Westfalenhalle.

This resulted in the already mentioned new number structure, which now starts at 1 instead of 2 and ends unchanged at 8. The reason for this is that the letters A and B were previously added. However, this had no effect on railway model construction, as it was to be found unchanged in Halls 4 and 7.



Sea and coast are themes that also attract children. The modular layout of the coastal N-Bahn in Hall 7 was therefore very popular with our young reporters. Only the appearance of the "Tobler One," of which they had raved so much, was not captured in the photo. Photo: **Trainini®** / Hendrik Späing

However, a visit to Hall 8 was also worthwhile, as topics related to railways were also found there: Steam model making, fairs and the circus dioramas by Rolf Böhm (Recklinghausen), who had also thought of the scale 1:220.

At this point we would like to record the satisfied conclusion of the organizer, because the visitors rated the fair in a survey with the average school grade "good," an impression to which we have nothing to add.

However, in view of the great challenges facing young talent, the view of children is always particularly important to us. Does the fair also meet their expectations? How do exhibits and offers look from their perspective? Do they feel invited and addressed?



Where there is movement and excitement, children are not far away. They will be attracted by demonstrations of remote-controlled truck and construction machine models, which will also have to cross level crossings as crossings with steam railways at this large-scale facility in Hall. Photo: **Trainini®** / Hendrik Späing

In the run-up to the event, we made our concern clear to the organiser, as we had done at other trade fairs before. He showed himself willing to accept the two children of the editor-in-chief as official representatives of the press, and to have them researched and photographed on site and provide with press credentials.

The results to be evaluated would be a four-page, very detailed and conscientious report of the daughter and a large yield of pictorial material produced by the son.

There was no way around these two, many a young observer was asked by them about his impressions and compared them with his own notes as well as the appropriate pictorial material produced.

That's why we don't want to withhold it from our readers, because after all it can help everyone to think over their own presentations and to adapt them so that much larger target groups feel addressed by them.

If we think, for example, of the viewing height of children, it corresponds roughly to that of wheelchair users. They don't want to be forgotten any more than the majority of adults walking on their own feet.



Always well attended and popular with young and old was Michael Rehorik's bizarre ride, which grew like a roller coaster like no other.



Michael Rehorik stands here next to his layout after motives of the British cartoonist Rowland Emett.

A magnetic attraction (not only) to children is a showpiece that has life and movement to offer. Besides trains, road and water vehicles, figures or even bizarre implementations, also are implemented here.

A prime example of this was the 1:160 scale module layout of the coastal N Bahn.

To the children's delight, a bascule bridge moved along the main railway line here.

Also the moor typical for Northern Germany on the module of Harald Käfferlein had it in itself, because here the tower of a submarine appeared in short distances.

The written name "Tobler One" showed it to be of Swiss origin, the

lieutenant captain entering the conning tower, and soon declared that he had gotten lost and had not appeared in Switzerland. With this humorous interlude, the community had already provided enthusiasm at several other exhibitions.

Michael Rehorik also had children on his side: In Hall 7, he showed a layout on which the trains were running on rails of size H0, but whose vehicles were more on a scale of 1-gauge or Hm. It was the only one that was built consistently in height and exceeded a total dimension of 2 meters.



The British railways keep reporting new transport volume records: in the cartoon, this is expressed in highly stacked boxes and huge coal mountains already trickling away in open freight wagons.

The youngest were amazed by the imaginative, almost magical design of the trains, the almost roller coaster-like track layout and an airship above the event, which was moved loudly in circular orbits with propeller motors.

Memories of Pinocchio came back to us: it too was once created for adults and met the hearts of children. A deeper meaning was also hidden behind this bizarre railway: it went back to the British comic-strip artist Rowland Emett.

Based on his models, also reproduced here in the model, a brightly coloured and richly distorted park railway was once created. Together with some overdrawn figures from society and the railway environment, it looks like a cartoon.

And that's exactly what it is, because Emett was also a cartoonist who liked to and often made a mockery of the English railways. Layout constructor Michael Rehorik was so enthusiastic about the work of the draughtsman that he wanted to create three-dimensionally the richly decorated and mostly already



Rowland Emmett's legacy also includes some recurring characters, which can also be found in Michael Rehorik. These include the locomotive driver on locomotive number 6 with the name "Hattie," the bald gentleman, the fine lady in the background or the stationmaster waving his flag at the left edge of the picture. This masterpiece stands out not only for its typical facial features but also for its size.

surreal works. It is and remains a completely new, almost artistic approach to model railways, which did not fail to have an effect.

We are returning once again to the promotion of young talent, because the focus this time was once again on the numerous opportunities for participation. Probably every model building division has now discovered this as the key to success. And while some are using tried and tested methods, others are making an effort to offer a regularly changing programme.

As regular visitors to the fair, our test persons therefore omitted the well-known model truck driving licence and the Junior College Europe of the BDEF in order to look for further offers. They were enthusiastic about the ship's driving licence, which was probably the result of demonstrations of interesting models in the show basin. Submarines, discharge ships and rescue cruisers were the clear favourites here.

The flight demonstrations among the remote-controlled models were also well attended. And there it does not necessarily have to be aerobatics with particularly fast models. Comparatively leisurely floating helicopters of large scales radiate a great fascination for young and old. This also makes it attractive to build a small balsa-wood glider at the association's stand and take dreams home with you.

This year the handicraft stand of the Youth Welfare Office of the City of Dortmund was missing. In the previous year's editions, plastic model kits were always allowed to be assembled there, accompanied by experienced young people. This was not only fun for the children, but also built up valuable experience, which is also helpful in railway model making.

The still very young robot show fights are enjoying an ever increasing popularity. It was therefore clear that our young reporters would also discover this point in the programme. The machines, which compete against each other in different weight classes, try to make each other unable to move.



Although the robot show fights have developed into a new magnet for visitors, the flight demonstrations also enjoy the favour of the spectators. The helicopter shows are particularly popular. Photo: Silvia Kriens / Westfalenhallen

They have armour to protect themselves, sophisticated forms to be agile, to push the opponent out of the arena or to knock him over, as well as effective weapons and tools to take the opponent out of combat. They meet under loud machine drumming or banging, protected from Plexiglas panes to the outside. For a while they work against each other, the victory takes place by knockout or by points.

What sounds brutal and martial here, however, is most comparable in the sports world with the Olympic discipline of wrestling: No robot suffered a total loss here. Perhaps this is the reason for the clearly perceptible influx of visitors?

Traditionally, the most important venue at the fair for young and old is the model railway. That's why it occupies two of the halls (4 and 7) and takes up the largest exhibition space. For the younger visitors there were several opportunities to participate and do handicrafts. Roco and Märklin invited visitors to play directly at their stands, as did the toy car brand Siku, which can be found in Hall 7 because of the Wiking range for model railways.

Not far away is always the stand of the Bundesverband Deutscher Eisenbahn-Freunde e.V. (Federal Association of German Railway Fans) (BDEF), where the young model railroaders were able to design small dioramas under expert guidance and supervision in the Junior College Europe. However, the children found a larger play and craft area under the name "Gleis 5" in Hall 4.

Here, many hours passed in which parents had to wait patiently while their youngest children played extensively with garden railway material. For the older ones there were kits to build, where the offer always differs from fair to fair.

From the point of view of our young journalists, however, the participation for token fee charged there is somewhat unfortunate for children who visit the exhibition without parental accompaniment or at least move around there without accompaniment. The question is always how much pocket money the children are allowed to have with them, and how they divide it up at the site.



The association Furka-Bergstrecke (section NRW) annually demonstrates the construction and dismantling of the bascule bridge on the Furka-Museumsbahnstrecke. When they see children enthusiastically following the process, they also like to invite the children to control the train and operate the level crossing. Photo: Trainini® / Hendrik Späing

Karl-Friedrich Ebe, who organises the model railway area for the Modellbahnverband in Deutschland e.V., was also enthusiastic about the overall good response: "The newly designed action area in Hall 4 was also very well received by the public. We are very much looking forward to the coming year in which we will celebrate our 25th anniversary."

Button actions also act children like a magnet, because they can influence what is happening on a system themselves. The stand personnel from the Furka-Bergstrecke association (NRW section) often go one step further: when they see that children are enthusiastic about their demonstrations and watch them repeatedly, they are gladly invited to help with operations. This is, of course, a great experience, as our two young journalists also found.

As far as the influx of visitors is concerned, we have the impression of strongly changed habits resulting from shortening an exhibition day (Wednesday): Thursday and Saturday have now been identified as the days with the most visitors.

At the start of the trade fair, all trade visitors who appeared spread over the first two days will now come. They often have specific questions and need advice, but they don't always get through the stands because of the crowds. This was also confirmed by exhibitors who, from their point of view, could otherwise have made even more sales.

In the meantime, Saturday has become the classic family day, which is certainly also reflected to a considerable extent by the weather. The fact that Sunday tends to be used much less in principle, however, is probably also due to the fact that the event time is one hour shorter.



The flow of visitors has changed noticeably on the individual days: Thursday and Saturday are the busiest. While families only have the weekend to spend, shoppers and those seeking advice use the first day of the fair.

Also annoying from a visitor's point of view is the continuing fact that many ideal and increasingly also commercial exhibitors start to pack up from 4 p.m., and, sometimes, even completely dismantle their stands, from our point of view, this shows disrespect to paying visitors, which the organiser should not tolerate.

So the exhibition leaves a "plundered" impression at first sight, and the guests are met in the corridors by exhibitors with exhibits and only partial stands. Where the stands are still erected, nothing moves anymore or at most a single train; showcases are generally empty and open.

We would like to remind you that the one hour reduction on Sundays was once a courtesy on the part of the organisers, because the dismantling and departure operations began at 17:00 hours. Instead of providing the entrance payers with an appropriate experience without such disturbances through a compromise, their problem has become even worse. As a result, Intermodellbau, like unfortunately many other trade fairs, is no longer worth the full admission price on Sunday (or the last day of the event).

Let us now devote ourselves more to what is shown in Z gauge and close scales: There was much more to see in Hall 5 with the ship and cardboard model building than many had initially expected. This is primarily due to paper and cardboard model making, which is often also available in small scales.

We are thinking of some other dioramas that we have already shown in earlier reports, for example Bernhard P. Schulte from Hagen, who announced a new showpiece for next year.

Contrary to expectations, there was also an interesting find in the field of RC model building: Björn Esser from SMC Hamburg took care of the Revell kit of the sea rescue cruiser "Hermann Marwede", the largest ship of the DGzRS, on a scale of 1:200 and built it as a remote-controlled model.



Björn Esser's SRK Hermann Marwede of the DGzRS, built as a remote-controlled ship from a Revell kit, was a masterpiece in 1:200 scale, which is close to Z-scale. Hermann Marwede was also an exhibitor at the trade fair.



Thomas Oswald inspired with a small suitcase system for his large MBZ building programme.

He opened the window openings and glazed them, exchanged the railing, the railing on the helicopter deck including bridge with self-designed etched parts to make the model look more realistic, classic fine model making, as we know it.

Shafts, rudders and propellers were also exchanged in order to become operable. Now, motors and gears from small linear servos take over these functions.

We would like to briefly describe the numerous manufacturers and dealers of Z gauge represented in Halls 4 and 7, as long as this is relevant, in retrospect.

Besides the big publishing houses and accessory suppliers with their own trade fair offerings, Aspenmodel / Bahls model railways and MBZ

appeared in Dortmund. Michael Bahls proved that Märklin's Series 10 can include not only fully movable detail controls, but also space for functional engine lighting.



The Märklin special car for the fair (Art.-No. 80729 was modelled on an FaL of the Dortmunder Eisenbahn (picture above). An exhibit steeped in history was the "Marx-Engels-Platz" N-gauge layout (picture below) by Bill Roberts.

Thomas Oswald (MBZ) had many construction kits from his program in large boxes sorted by gauge, which is why the stand was always surrounded by a dense crowd. A suitcase system in Z scale was an excellent illustration of what can be created from these kits. Also, Bahnreisen Sutter promoted his offerings with a previously shown suitcase system.

Manfred Lütke (Lütke Modellbahn) had arrived without any new items, but the available kits, made of milled polystyrene and plywood, are very popular again. Märklin also refrained from presenting summer new products due to the early date of the fair and made up for this at the beginning of May, as you can read in the reports of this issue.

However, this year's Gattung Fals exhibition car with the company number 3645 of the Dortmunder Eisenbahn (Art. No. 80729) in green paint with predominantly yellow lettering attracted a large number of visitors. In contrast to one of its predecessors, however, it was delivered without a load.



Dietmar Allekotte's showcase "Marienfeld" with a harbour motif on the sea had its German premiere in Dortmund and was rightly well received by the trade press.

Anyone who is interested in the possibilities of spray painting was really impressed in Hall 4 at Harder & Steenbeck. Artist, model maker and author Mathias Faber, as well as the stand personnel, provided interested parties with expert tips. Almost all of the manufacturer's injection styli filled with ink were allowed to be tried and tested under instruction.

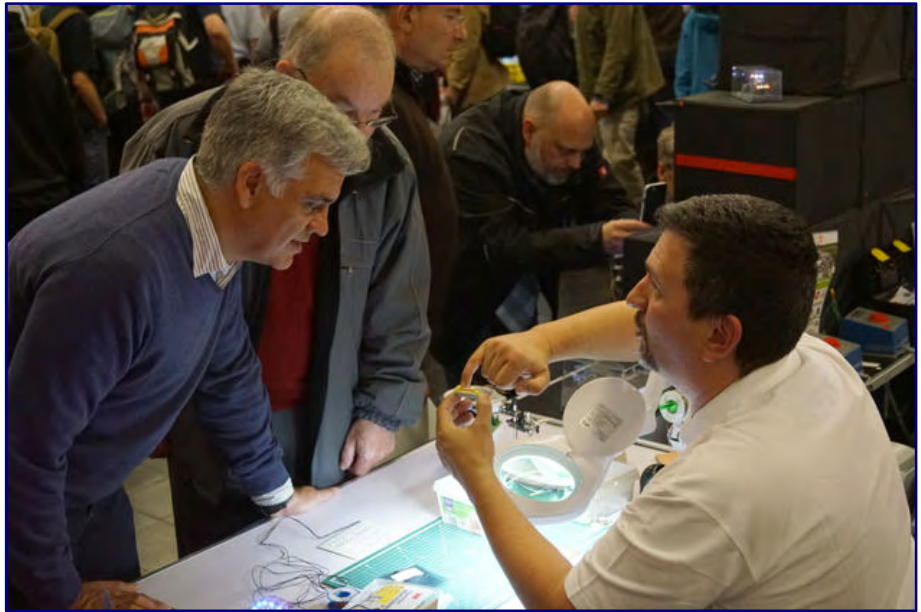
We would like to address at this point with a view over the edge of the plate also still another N-gauge plant with historical requirement, which had arrived from England. Bill Roberts had replicated the Berlin S-Bahn stop Marx-Engels-Platz on a scale of 1:160, as it looked shortly after the reunification. Today it is renamed Hackescher Markt.

The most important contact point for gauge Z was the joint stand of Dietmar Allekotte, Rainer Tielke and Torsten Schubert. Dietmar presented in a German premiere his small and much admired showcase Marienfeld, which shows a small harbour with pier on the German coast.

Torsten Schubert never got tired of giving soldering demonstrations and explaining to visitors, in detail, how they themselves can achieve excellent results.

In his exhibition wall with black showcases, he displayed many lighting effect exhibits this time as well. The latest of them was the demonstration of a light exchange at a street lamp.

On a scale of 1:220, two adjacent examples were used to show how this would have to look like following accident prevention regulations, and how this would have to look most adventurous, albeit entertaining in the model, and with great risk.



Torsten Schubert explains to the visitors in announced soldering demonstrations the correct procedures to achieve convincing results themselves.



Replacing a light source on a switched off street lamp by means of a lifting platform (left) or in a more adventurous way on a live specimen? The visitors acknowledged the juxtaposition with a smile

Rainer Tielke impressed the guests once again with his double-decker bridge Bullay. Here, thanks to a lot of digital sound support, it was sometimes quite noisy, which was mainly due to the heavy block train with two class 44 steam locomotives at the front and a class 50 following.

The children enjoyed the mouse train, for the outgoing fair team of Eisenbahn-Romantik, the Borussia Dortmund train presented in **Trainini®** 7/2017 was temporarily on track. Unfamiliar attendees were astonished by the cars moving along the lower level of the bridge.



The heavy coal train that crossed Rainer Tieleke's double-decker bridge from Bullay captivated the eyes and ears: under the pre-stress of class 50, a 44-oil engine struggled loudly at the front (photo above) and its exhaust blows had hardly ebbed away, followed by the equally loud thrust locomotive of class 44 coal (photo below). Meanwhile, there was a small rear-end collision on the road on the lower level.

They searched in vain for chain-guided magnets under the girders and the floor of a side-mounted turning plate, which, according to their ideas, pull the small trucks across the streets. They were surprised by the Z-Car system from KK Produkcja in Poland, which has already been further developed, allows self-propelled cars even for the smallest model railway scale.



Photo above:
Surprised, many visitors looked for the trick to move trucks and buses on Rainer Tielke's road. They had not expected the self-propelled cars based on the Z-Car system from KK Produkcja.

Photo below:
The pink and white Hello Kitty Shinkansen had been announced as one of the main attractions by the regional press and attracted many children and families to the Z-scale stand. But also the Bernd-Knauf-Lok and many other valuables were not to be missed in the showcase.

But it was also worth taking a look at the showcase at the stand. Jörg Erkel (1zu220-Shop) had actively supported Torsten Schubert to show some extraordinary gems in the nominal size Z. In addition to Torsten's locomotive and car models with special paintwork, a large selection of Archistories buildings was also on display, one of which illustrates their construction.

But the white and bright pink painted Hello Kitty Shinkansen of the 500 series from Rokuhan, which was shown here in its full length, also received true interest from the audience. Representatives of the daily press and the local radio station had pointed it out as one of the main attractions several times and thus lured mothers and daughters into the halls.

And so we conclude the report with a satisfied conclusion: If we want to bring model railways and model making back into people's minds and bring this hobby back into the children's rooms, we need the sustained and concerted efforts of all those involved. Intermodellbau 2019 left a very good impression with us, and it was also well received in the media.

Exhibition website:

<https://www.intermodellbau.de>

Exhibitor selection for model railway in scale 1:220:

<http://www.artitec.nl>

<https://aspenmodel.com>

<http://www.bahls-modelleisenbahnen.de>

<https://www.bdef.de>

<http://das-kantoor.de>

<https://www.faller.de>

<https://www.harder-airbrush.de>

<http://www.hecklkleinserien.de>

<http://www.luetke-modellbahn.de>

<https://www.maerklin.de>

<http://www.mbz-modellbahnzubehoer.de>

<http://www.mkb-modelle.de>

<http://www.moba-deutschland.de>

<https://modellplan.de>

<https://www.noch.de>

<https://www.revell.de>

<http://www.saller-modelle.de>

<http://www.viessmann-modell.com>

Ideal exhibitors of track gauge Z:

<http://www.rainer-tielke-modellbau.com>

<http://www.z-lights.de>

RC ships in scale 1:200:

<https://besser-modellbau.de>

<https://www.smc-hamburg.de/smc-flotte/special-1-200/>

Eisenbahnmuseum Dieringhausen



Einzigartig in Europa

*Spur Z Ausstellung
im historischen Güterwagen
im Eisenbahnmuseum Dieringhausen*

Die Sonntags-Ausstellungstermine:

*08.06.2019, 09.06.2019, 16.06.2019, 07.07.2019,
28.07.2019, 11.08.2019, 22.09.2019, 13.10.2019,
27.10.2019*

jeweils von ca. 10:00 Uhr – ca. 17:00 Uhr

Infos unter: www.stammtisch-untereschbach.de

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Reader's note on the missing catenary function of the E 41 (Märklin):

For your information, the answer of Märklin to the missing catenary function of the E 41. Their statement to generally want to do without the catenary function in the future is certainly worth publishing.

Frank Wenzel, Kaarst

Editorial response: We would like to reproduce Märklin's answer to our readers in a more precise form following a question. In the requested feedback to our editorial staff it says: "The switch to overhead line operation is only used by a small group of customers, who are also becoming smaller and smaller due to the advancing digitalisation. However, it will not be the case that this function will be omitted in all vehicles, but it will no longer be taken into account in new designs. Customers who need/request this function in their models should refer to the corresponding pictogram in the catalogue presentation/product description. This indicates whether the function is available in the model or not."

Spring impressions on the model railway:

In the foreword to the current issue of Trainini, you editors have suggested the construction of a spring facility. Well, my layout (which has been under construction for more than 17 years) is now receiving spring landscaping, even if, in late spring.



Why always use summer as a model? A delicate spring green characterizes this system, along with newly built track magnets for the Indusi and 30-year-old light signals and telephone boxes. Photo: Jochen Brüggemann

The aim is to recreate a landscape as it was usually experienced in the late May/early June 1960s to early 1980s in southern Niedersachsen and northern Hesse. So: based on significantly more precipitation and lower average temperatures in spring.

At the moment I am working on the basic landscaping (grasses, herbaceous plants, shrubs etc.) in the rear area of the layout. Trees and bushes follow later. For illustration, I have added three pictures, which I take from time to time, to document the construction progress on my layout.



Even the work on landscaping grass covered areas can be convincing. By the way, last year our reader also presented how it works in **Trainini®** Photo Jochen Brüggemann

It is impossible to overlook the large construction site itself (...). The 4 signals and the signal telephones (...) by the way still come from my old layout and are in the meantime well over 30 years old, make themselves, however, quite appropriate in the background. The Indusi magnets, on the other hand, I made only last year; they decorate since then (covered with "rust dust") all main and approach signals (in direction of travel right).

Jochen Brüggemann, by E-Mail

Letter to the editor from the United States:

Hello, my name is Eric Schellenberger and I am new to model trains and choosing Z scale. I live in California, USA.

Thank you so much for the English translation of your fine magazine! If only I could read the issues published before 2018. Hint! As a newcomer to this great hobby, I have learned so much by reading the Trainini magazines available in English. You have a wonderful and informative magazine. Just looking at the pictures is an education but the articles are also written really well.

I particularly learned from the articles on DCC electronics and Märklin motor development. Also, engine maintenance articles, and how to articles on landscaping, and building the Archistories models. Please take pride in how much your translations mean to us outside of Germany. My foundation in train modeling is being taken directly from Trainini and Märklin Insider literature. Thank you!

Eric Schellenberger, per E-Mail

Answer from the editors: Dear Mr Schellenberger, we will not be able to translate (have) all volumes of our magazine; retroactively. Also the free time of our three translators is, unfortunately, limited. However, you can easily copy texts from our magazine into translation aids in order to use electronic tools.

The Märklin deliveries of the last weeks:

In the last few weeks Märklin has delivered the dust silo car package (Art. No. 86665) for Era IV. It contains two cars of the type Ucs 908 from the DB stock with different service numbers.

They are painted pebble grey in accordance with RAL 7032, as prescribed by the standard paint scheme from 1977 for this type of freight wagon for body and underframe. In contrast to the similar wagons of earlier production (8666), this colouring now also includes attachments (here: suspension springs and buffers), which was expressly permitted if this was more favourable for production.



For comparison, we have placed the two Ucs 908s from the recently released car package (Art. No. 86665) in front of one of their predecessors (8666, rear left), which is shiny and prone to yellowing, and the Museum Car 2019 (rear right).

What remains incomprehensible is why the ladders and railings are not also included in the pebble-grey paintwork. This would be conclusive according to model photographs and is also shown in the product illustration. Märklin has already proven that this is technically possible with the 2018 Museum Car of the same design, which has pursued a similar painting concept.

The extension packs for the revised ICE 3 of the 406 series (art. no. 87715 / 87716) are also currently arriving at the dealers. Like the basic pack, they featured air conditioning attachments and improved car transitions. The five-piece vehicle package (89023) from the 2019 summer new products, which is described below, is also available for delivery.

Rokuhan compatible products at **Ztrack**:

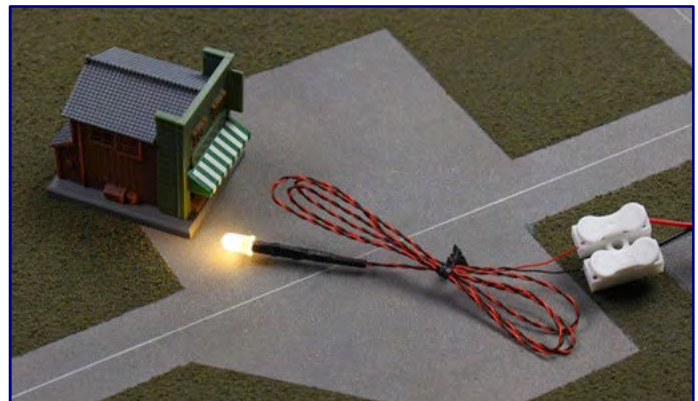
New in the distribution of our correspondence partner **Ztrack** (<http://www.ztrackcenter.com>) in the United States are the following accessories from Ztrains for a lighting system using Rokuhan controllers. The accessory power distributor (art. no. ZTR-251) serves up to twelve light emitting diodes or the LED lighting units of the same manufacturer (ZTR-260).

Six connections (plug connections) are available on each side, 5 mm light-emitting diodes were selected for the in-house lighting, which distribute the emitted light widely and thus ensure a good impression when operating in a house.

Additionally, the range has been extended by one- (ZTR-270; 3 versions) and two-arm street lamps (ZTR-271; 2 versions), as well as individual light emitting diodes in warm white (ZTR-262) and cool white (ZTR-263).

Anyone who operates many such lighting units and does not want to rely on the power supply provided by the Rokuhan speed controller can only use the independent LED power supply unit with Rokuhan compatible cable (ZTR-250) in America. It is designed for the usual North American mains voltage of 110 volts, and therefore also has the usual (US) plug.

Company anniversaries and viaduct festival in Ostwestfalen:
The 1zu220-Shop (<https://www.1zu220-shop.de>), well known to our readers, is (only) a business field of the Marsberger company HRT Informationstechnik GmbH. This year, this information technology specialist celebrates its 30th company anniversary.



Power distributor (art.-no. ZTR-251; photo above) and LED lighting unit (ZTR-260; photo below) in use. Photos: **Ztrack** / **Ztrains**



On 25 May 2019, the special event was duly celebrated with employees, their partners, the landlord and neighbours in a small group. Relocation helpers were also invited, because the new seat in the estate administration building of the Counts of Stolberg's Brewery Westheim was occupied exactly one year ago and provided a further celebration occasion.

Thanks also to the growth in the model railway segment; the number of employees has now risen to nine full and part-time employees.

The next event for the company is the Vivat Viadukt Festival from 5 to 7 July 2019 in Altenbeken (<https://www.vivat-viadukt.de>). There, the model of the famous railway bridge will be shown on a scale of 1:220 and a Märklin special car in the HO and Z lanes, exclusively available on these days, will also be offered with an edition of 100 pieces each.



For "Vivat Viadukt" the 1zu220-Shop sells a special car in the tracks HO and Z, limited to 100 cars. The mini club model (item no. 8600.172) is shown here. Illustration: Märklin

By the way, a visit to this festival is worthwhile not only because of this and with a view to the model exhibition and partly steam-guided special trains, but also because of its further Z gauge participation, which is ensured by the Stammtisch Blomberg with its extremely appealing exhibits.

New models at WDW Full Throttle:

Colourful is the first of two new models from WDW Full Throttle. The closed, cylindrical bulk freight wagons have a red background with the striking Zagnut logo (Art.-No. FT-1064), which almost looks like graffiti.



Bulk goods wagon "Zagnut" (Art.-No. FT-1064). Photo: WDW Full Throttle

Also new are the Great Northern's open 33-foot bulk wagons with high dropsides (FT-3408), which appear considerably more common with a brown paint finish. They are factory fitted with wood chip loading inserts.

In Germany, full throttle models are available from Case Hobbies (<http://www.case-hobbies.de>), among others.

Märklin summer new releases 2019 presented:

This year Märklin chose a different and completely new way for the announcement of the summer new products 2019. For the first time, the brochures were not distributed at the Intermodellbau in Dortmund, because this fair apparently took place too early to be followed up.

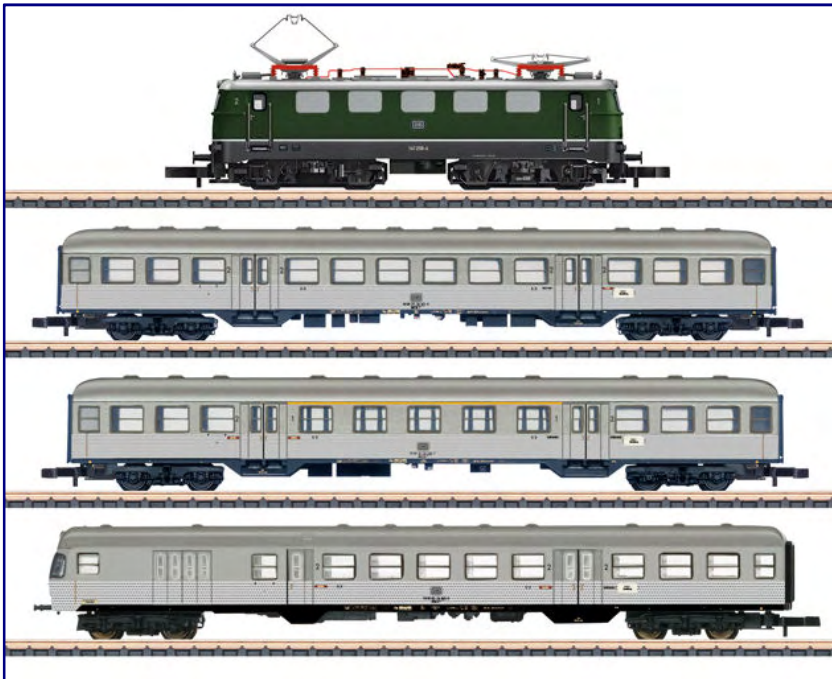
Not all customers had previously seen the spring new products announced in Nuremberg. In addition, Märklin has been actively looking for alternatives for some time in order to draw attention to its products using contemporary media.

This year, the choice fell on a moderated real-time transmission on the YouTube channel. In constant succession, a presenter displayed the innovations of the various brands and gauges, accompanied by studio discussions and film recordings.

For Z gauge, however, the result looked quite sobering, because here it was only enough to fade into the brochure pages. In our opinion, this did not create any greater incentives for purchase. So we remain confident that Märklin will evaluate its attempt carefully, and also find a better approach for our small scale.



The following new products were announced for the Mini-Club: The traditional Märklin Magazine Annual Car 2019 (Art. No. 80829) opens the series. It belongs to the type E 037, which is better known under the old designation Omm 52. It bears the blue of the magazine and is supposed to be based on a fictitious operating condition around 1990.



The five-piece vehicle package (89023) contains four trucks and a metal forklift. They are all already known in identical or similar form from other assemblies or earlier museum carriages, which is why this is probably a sale of stock goods.

The NoHAB diesel locomotive with a housing made of metal filled plastic and a bell-shaped armature motor is equipped with a third version, which is at home in epoch V. The NoHAB diesel locomotive is equipped with a metal filled plastic housing and a bell-shaped armature motor.

The TMY 1100 of the Swedish Tågab (Art.-No. 88636; picture above) wears a strange colour dress, which she had originally been given for filming. The highlight of the summer new products is the chrome oxide green 141 258-4 with commuter train (81356; picture below), which unfortunately will appear with a not reworked control car. Illustrations: Märklin

The TMY locomotive (88636) belongs to the Swedish private railway Tågab and was formerly classified as MY 1100 by the DSB.

It wears the livery of the American Great Northern, which suits it well.

But the highlight among the new products is a train package "Nahverkehr" (commuter train) (81356) for MHI: It contains a chrome-oxide green ellok (electric locomotive) of the class 141, which was colloquially known as the "firecracker." It's the first freely available version of the previous Insider locomotive.

In contrast to this, it differs from the other colouring and classification in epoch IV, also by the double jet fan with vertical blades and the 5-lamp design on the front of the locomotive.

The three accompanying silver cars of the types Bnrzb 725 (2nd class), ABnrzb 704 (1st / 2nd class) and the control car, which is not described in more detail with regard to its type, known forms and have no interior fittings.

They will wear black lower edges and again the Ege cookie in negative execution, which we consider at least at the (with the prototype converted) control car as not correct or at least very unusual.

The front and rear lighting of this BDn(rz)f 735 or 740, which is to be provided unchanged with outdated incandescent lamps, will not be revised.

Märklin chose the early design of the Karlsruhe heads without the orange warning bars on the ends - a version that did not exist before in Z gauge, and which we certainly welcome.

New vehicle kits from EtchIT:

EtchIT has published some very interesting vehicle models as finely converted kits using 3D printing. They can all be described as classics, only the modern, two-axle semitrailer tractor (Art.-No. XD016_Z) is a current model.

A MAN already in the range is now also the basic vehicle for the Haller body of a garbage truck. This is the long-awaited MAN F8 refuse collection vehicle in a two-axle version (XD015_Z).

The small, almost forgotten delivery van Tempo Matador 50 in its older version as "crumple face" (face of a boxer's dog) enters the program: It is available as panel van (ET031box_Z), minibus (ET031bus_Z) and so-called combination van (ET031kbi_Z).

Like all kits from this manufacturer, the individual parts have been designed and divided in such a way that they can be painted easily and prototypically. The resolution of the print surfaces can only be described as excellent. Their production method can barely be seen. That speaks for the experience of many years of this manufacturer, whose products you can find on <http://www.easy01.de/etchIT-store/>.

Newly Delivered MTL-Modelle:

Finally, the delivery of the revised and reissued US diesel locomotive SD40-2 from EMD progresses, as it is the most important successful model among North America's role models. This month we will continue with two operation numbers for the Canadian National (Art.-Nr. 970 01 151 / -152).



Modern semitrailer tractor (Art.-No. XD016_Z; photo top), refuse vehicle MAN F8 (XD015_Z; photo centre) and Tempo Matador 50 as panel van (ET031box_Z; photo bottom). Photos: EtchIT

Model number 4 moves up from the Farms-to-table series: This is a wooden refrigerator car with a brown primer and light blue side walls (518 00 740). It bears the company number SBIX 12911 and advertises "Stokely's Finest Foods".



The brown, covered freight cars of the Southern Pacific (505 00 391 / 392) with their yellow advertising "Hydra-cushion for fragile freight" look comparatively simple in comparison with their design. However, they have opening sliding doors and thus provide a clear view of any cargo.



The 40-foot long flat cars of Pennsylvania (525 00 171 / -172) will attract attention because they are equipped with a load that fits, but is not described in detail, which immediately catches the eye.

EMD SD40-2 of the Canadian National (art. no. 970 01 152; photo above) and flat wagon with load (525 00 172; photo below). Photos: Micro-Trains

Micro-Trains products can be purchased in Germany from Case-Hobbies, among others. (<http://www.case-hobbies.de>).

Sondermodelle Z puts on another "Red Arrow":

SMZ has announced a new version of the "Red Arrow": SBB's Re 2/4 201 is the original version of these multiple units and, among other things, was considerably shorter than the version previously offered. It was only on the rails between 1937 and 1948 with this unique designation, which did not correspond to today's SBB scheme. Then it was renamed to RCe 2/4 601.



The "Red Arrow" in its original version before modifications and marked as Re 2/4 201 of the SBB is the spring new item 2019 from SMZ. Photo: Sondermodelle Z.

As a model of the first series it has no buffers and couplers and is in its original state as delivered. The designation CLe 2/4-201 it carried only between 1935 and 1937 afterwards it was re-designated to the Re 2/4.

The prototype of the model was the first to be manufactured and was the only one to receive all the conversions that were carried out on these trains: Retrofitting of pulling and buffing equipment, reinforcements, and vehicle extensions. As one of only two originals, it is still in museum use today.

The model has the proven chassis with bell-rotor motor, which drives a bogie. The motherboard has a Next18s interface and can accommodate both analogue and digital circuit boards developed and manufactured by Velmo.

In digital operation, the miniature has a switchable light change (white/red) and interior lighting. In the premium version there is also complete operating sound equipment with SD18a sound decoder by Doehler-Haass and a loudspeaker. Another special feature is the use of true-to-scale SBB pantographs from Sommerfeldt.

You can see pictures and descriptions from Sondermodelle Z at the following web address: http://www.sondermodellez.at/content_d/M71_1.asp.



The shape of the front of the wagon looks quite accurate. It is still inscribed, as usual at that time, with 3rd class markings. Photo: Sondermodelle Z

New tool from Microrama:

Helpful tools for landscaping are the new Minigras Junior (Art.-No. 81 01 1001) and the smaller Microgras Pen (Art.-No. 81 01 1010) from Microrama. The French specialist has thus launched its first two electrostatically working grassing machines on the market.

They work with 9-volt block batteries similar to the Grasmaster from Noch. The Microgras Pen is tiny among such tools and generates a voltage of 15 kV with a current that is harmless to humans. According to the manufacturer, it is suitable for grass fibres from 1 to 12 mm in length.

Due to its small size, it can only hold a small amount of material, but delivers it very precisely. However, since the two poles can be brought much closer together than the fine feeder made in Wangen, much better results can be achieved with it. This certainly recommends it especially for demanding landscapers on a scale of 1:220.



The handy Micrograss Pen (Art.-No. 81 01 1010) works like a large electrostatic and with its small container is suitable for fine work on the layout.

The Minigras Plate (81011020) also deserves a short note at this point. It offers an electrostatically chargeable metal plate on which the company's leaves are scattered. If the opposite pole is connected with a tree from the Microrama program, the leaves jump to the tree blank that has been sprayed with spray glue.

Trees can thus have leaves added to any desired thickness, which is particularly helpful if, for example, the greenery only begins to sprout in spring; or many of the coloured leaves have already fallen off in autumn. We will test this tool and present the results in an upcoming issue.

The products are available at <https://eshop.microrama.eu/de/>.

Trainini®- Pages changed:

Since the publication of the last issue a lot has changed on our Internet pages. Even though we do not collect any personal data there, the **Trainini®** pages under the domains DE and EU have been converted to certificate-secured connections.

This can be recognized in a browser's command line by the "https" protocol at the beginning of the URL and usually also by a padlock displayed closed as a symbol next to it. This was possible after our EU site was transferred to the new service provider Netcup with a delay of a few days.

The previous provider Celeros was not willing or able to confirm our cancellation with effective date and to transmit the requested transfer key for the relocation of our EU domain in the last five weeks until the publication of this issue, although the receipt of this request had even been confirmed directly by the system.



New products from American Z Line:

The theme in May is the former name "Columbian" of the B&O, which is now being produced as light passenger carriages. A 6-6-4 sleeping car (item no. 73110-0), a viewing car (73410-1 / -2), a seating car (73710-0), a dining car (73510-1 / -2) and a pulpit car (73810-1 / -2) are in the program.



They are supplemented by a luggage van (73610-0) and a mail van (73910-0). The range already includes two matching F3 double units (62913-1 / -2).



The diesel-electric locomotive GE Genesis P42 now is available in the colours of the New Haven (63503-1 / -2). The covered AAR freight wagons, which are offered individually (904305-1), in pairs (904375-1) and quad packs (914305-1), have the Erie road name.

GE Genesis 42 of the New Haven (art.-no. 63503-1; photo above) as well as vantage (73410-1; photo center) and pulpit (73810-1; photo below) of the B&O. Photos: AZL / **Ztrack**

You will find further manufacturer photos of the current deliveries under <http://www.americanzline.com>.

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Trainini

German Magazine for Z Gauge



On the siding

Helpful for Zed Heads
Large fan meeting

Introduction

Dear Readers,

Also this month we are quite late with the current release, but we made up for lost time last month. I don't have to have a guilty conscience, because I was able to experience how you patiently wait for the new issue.

Some readers were worried in May, which impressed me. The fact that we took a little more time in June, contrary to the plan, is (again) due to my person: In my hometown Dortmund the German Protestant Church Congress took place until the last weekend.

Together with many other helpers from all over the Federal Republic of Germany, I took care of the emergency services for the hundreds of thousands of visitors. For more than twenty years I have voluntarily served a Christian order of knights in this function.

Such events are places of contact. Where people meet, pray or celebrate together and inform, many different points of view, groups and even cultures collide. I have enjoyed it and got to know people who live where my hobby has occasionally led me.

One such meeting place is Sindelfingen, where a large group of model railroaders meet every two years to exchange and present their own works. Stephan Fuchs reports today about the youngest of these regulars' tables.

We will also continue with our focus for the year, as Ralf Junius presents his competition diorama, with which he entered a competition last year. Dirk Kuhlmann supports this series with layout suggestions, for which you may read the first with basic information today.

But our model railway is also always a very up-to-date topic. Therefore not only the part of the letters and messages to the editor is very large, but we have also included two further reports with which we would like to introduce new and only recently available products.

Anyone who repairs and maintains his models himself will appreciate a locomotive maintenance pad. If you don't have one, you'll quickly grasp its benefits and think twice about accessing the new model. I am very sure of that.

At Microrama we also came across two new tools that sparked our curiosity. The trigger for this was already the trade fair in Cologne in November 2018, when we experienced a sample in action. Because we were deeply impressed, it was clear that we wanted to follow up on our reports on grassing and leafing.

And as always, there are also two literature suggestions this time, which will certainly serve different target groups as summer reading. Get through the summer well and stay loyal to us! And now enjoy reading this edition.

Sin-Z-erely,

Holger Späing



Holger Späing
Editor-in-chief

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We thank Ralf Junius for his report and the 1zu220-Shop for samples and photo support.

Date of publication of the German language version of this issue: 26 June 2019

Cover photo:

This work by RZS-Kies has been shut down for several years and is thus slowly beginning to decay. Due to a derailed bogie, a self-unloading wagon could not yet be salvaged, nor could a defective truck trailer. This story of Ralf Junius transformed into a fine diorama.

Landscape & Track (Part 1)

Somewhere in nowhere

Our annual theme for 2019 is “Layout diversity”. In addition to many successful works from our readership, we would also like to make our own suggestions here. They may serve as inspiration or a source of creativity for all those who are still undecided, or those who are simply looking for an inspiring idea. We will be happy to present your results in the magazine as well. Dirk Kuhlmann explains what he considers to be important and presents a first track plan.

I would like to welcome you to our new report series “Landscape and Track”. In **Trainini®**, we will present you with track plans as an inspiration or as a replica. It starts with a track diagram at Ellzee, a country station somewhere in nowhere. But I would like to say a few words to you in advance.

The species of draughtsmen and designers in the model railroading hobby is (unfortunately) extremely rare in nature, at least, in comparison to the high number, literally whole herds, of collectors and hunters of pure rolling stock. As a representative of this rather threatened genus, I would like to point out the important step before it after our annual theme in 2017 “Landscaping”.



If some basic rules are observed, planning and designing a convincing and realistic landscape is not so difficult. Here, the V 100 has found a perfect home.

In the last five years, I have been able to see again and again at exhibitions with my exhibits that the installed track systems met with considerable interest from the public. There was also a great deal of demand for published articles on the installations here in **Trainini®** or in the print press.

“Their track figures are so very different from the usual. Will you tell us the secret?” The same question was asked by an older (Dutch) couple in Utrecht on the occasion of the presentation of Kniephaven at OnTraXS 2017.

The only correct and true answer is that each model railroader has his own unique talents. Once they have been recognized, the way ahead is marked out.

As a young person I had the downright outrageous luck to be allowed to look over the shoulder of Rolf Knipper several times.

His drawings and plans are unrivalled to this day, and his track plan booklets also form a wonderful basis for many model railroaders today.

So my little plan was painted out according to the rules.

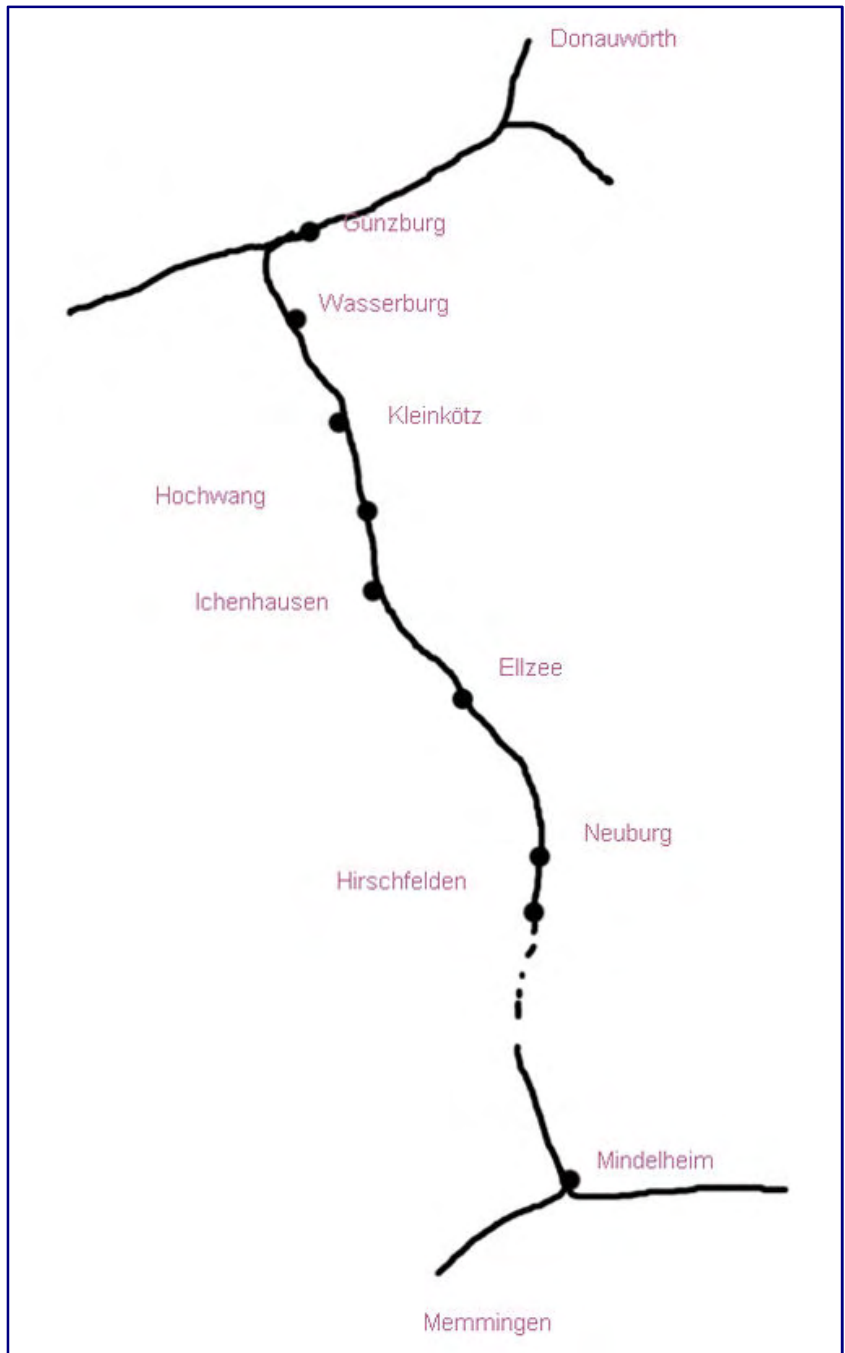
Free drawings are the origin of all things, followed by detail plans and already a coming layout, also independent of scale, could go into construction.

In this phase the so inclined model railroader should separate himself from the usual and well-worn out track figures. Let your imagination come into play instead!

Exceptions are, of course, track plans that are drawn up strictly according to a model: Here, the high art of “omission” and compromising is the order of the day.

Unfortunately, I only have two hands and could not put my quite large pool of track plans into practice in this life. Therefore, we will present you some of my drawings in the future, which, of course, will be supplemented with the appropriate “surroundings.”

By the way, you won't find a computer-generated track diagram with landscape here. What counts for me here is the personal feeling, expressed by hand, with a pencil on a sheet of paper.



Before we start with the first track diagram, I would like to give you some tips for your own consideration. Of course the tastes are different, but a layout should have some important points for an exhibition at the end of the second decade of the 21st century:

- Avoidance of rails running parallel to the edge of the system
- The landscape must have “depth and height”, this is also possible with an installation board
- Construction of tunnels, only as required
- A station doesn't always have to be the main point of attraction
- A finished layout should work for itself, i.e. above all, also without rolling stock
- Circuit camouflage
- Subsequent illumination of the layout should also be incorporated into the planning.

The fourth point in particular is the great mystery that the Dutch couple tried to fathom at the exhibition in Utrecht. And then, of course, there is the most important tip:

Observe in reality, from which the implementation into the model results, but again and again you also practice the courage to dispense with some or even many scenes and details.

What remains? For me, the answer is quite clear: an unparalleled harmony, because this is what often distinguishes an extraordinary model railway layout.



The track diagram

Here is now the first track plan, it is the small stop Ellzee at the line from Günzburg via Wasserburg to Mindelheim, to be found in the southwest of Bavaria. The railway office turns it into a formal railway station with its switches.

In the epoch III the traffic volume was still quite large, since on working days the trans-shipment of agricultural products from/for the surrounding farms took place. Thus the tracks for an otherwise so contemplative station were very extensive. Of course, the surrounding villages also profited from his cargo handling.



1

10395 bayer Agenturgebaude



2

10468, Klohaus



4

10412 Schrankenwarterhaus



6

10048 Schuppen



3

10122 Gemuseladen = Restauration



5

10308 kleines Siedlungshaus



10027 Schuppen

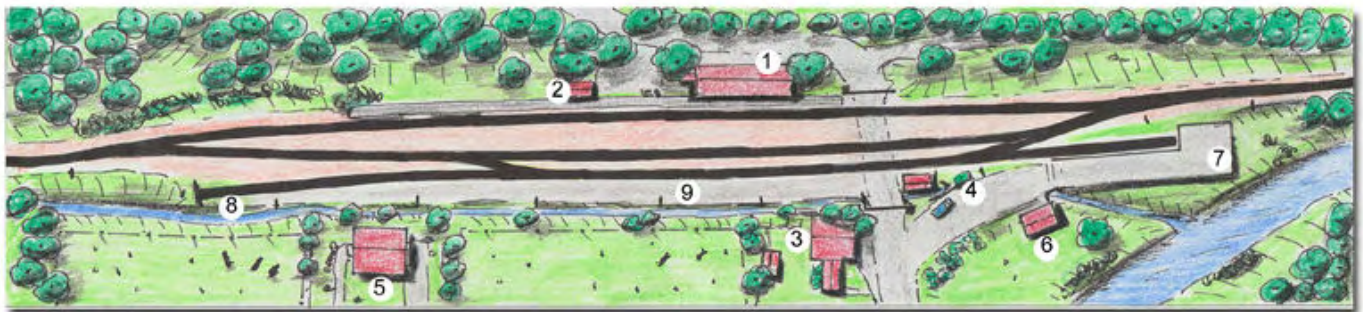
3 Anbau an 10122

Rail buses or battery railcars, as well as the V 100 with local freight trains, could often be seen here. In the steam locomotive era, lines like these were still in the hands of standard locomotives of the 64 and 86 series or of Bavarian local locomotives such as the 988 class.

Sometimes trains were also on the road as PmGs (passenger trains with freight transport). Then, the travel time became noticeably longer. Gerhard Zimmermann reported about this station in his wonderful article "Zeitreise" (Time Travel; Eisenbahn-Journal 9/2015).

We now take his track plan as a template for an individual implementation, but remain close to the prototype. Also, in this case a research of the regional vegetation is advisable. The houses must also have the typical character of the area, and this is where MBZ comes into play. Here, we found what we were looking for.

The models could be implemented directly without conversions or variations. Only the vegetable shop (MBZ Art.-No. 10122) was converted into a station restoration, as the landlord was also the agent for the agency building (MBZ Art.-No. 10395).



Selected buildings and their functions (illustrations with MBA article numbers and numbering in the plan see previous page):

1 Agency building	2 Toilets	3 Station restoration with shed	4 Gatekeeper's house
5 Residence	6 Sheds	7 Loading dock (ramp)	8 Siding
9 Open loading track			

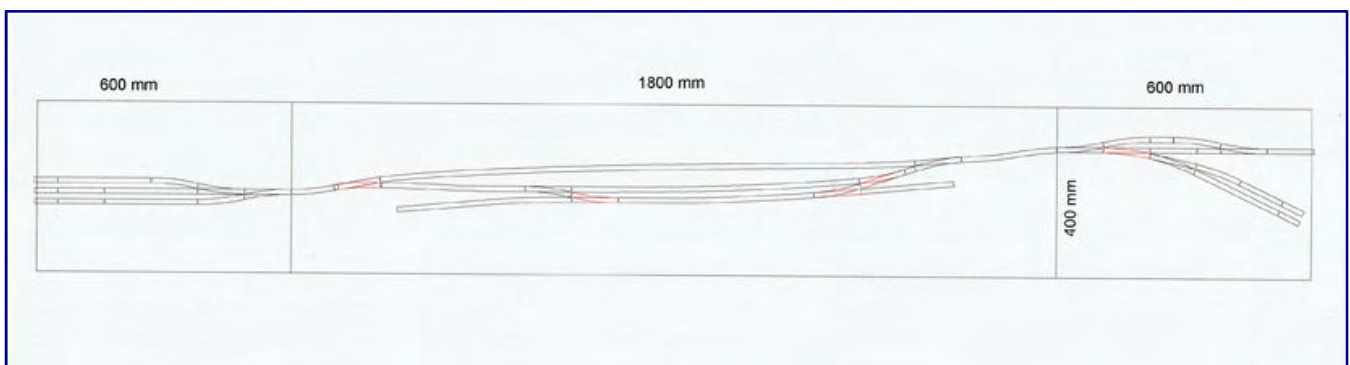
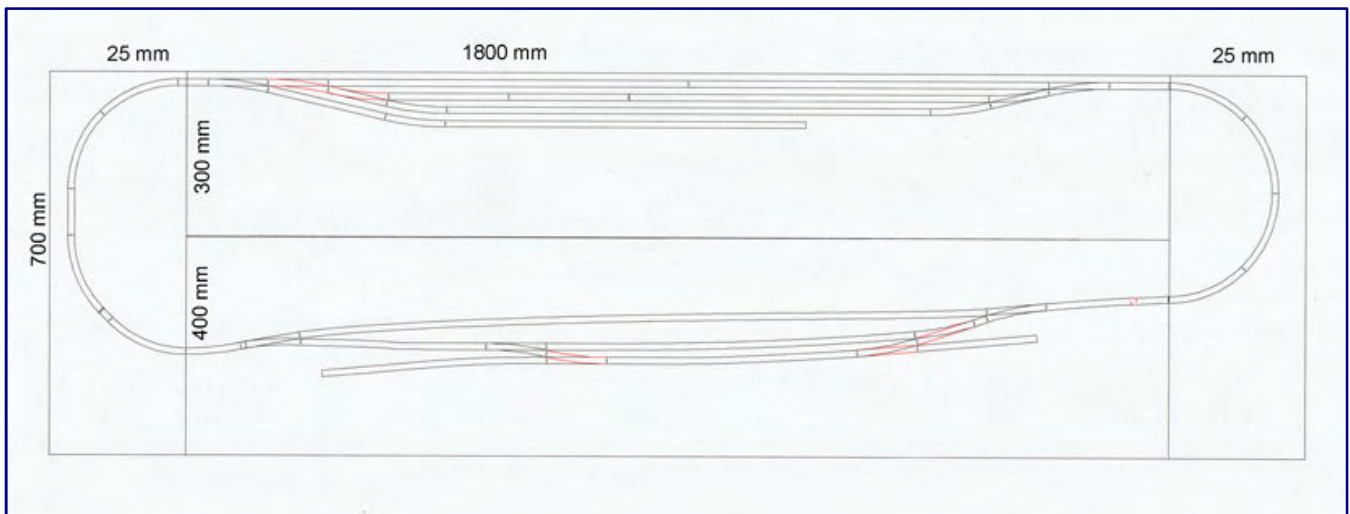
Contrary to prototype, the scenery should also have a small river, which we positioned in the lower right corner of the planned 180 cm x 40 cm large Z-gauge layout. This takes the sense of harmony into account: the landscape only appears completely balanced, as a result.

A continuous forest row, which should continue on the colour-coordinated background scenery, forms a finish to the rear and slightly rising rear edge of the layout.

Of course, we do not want to make any recommendations for a preferred material for landscape design at this point. Every hobbyist has his personal favourites. For the beginner or beginner, however, our annual theme from the year 2017 is again advisable.

Round-trip traffic or not?

For our exhibit we have two good possibilities to operate it: The pure observer of moving trains naturally prefers the round trip. The classic staging yard can be located behind the background scenery.



The track plans for circular trip traffic (picture above) and alternatively for an operation with staging yards at both ends (photo below).

The followers of shunting games prefer rather the open staging yards at both ends of the complex, which are called “fiddleyards” in English. Here, trains can be moved on plates with tracks and thus exchanged and also a locomotive can circumnavigate its train. Sometimes, turning circles are also used, for example to be able to send tender locomotives back onto the track after turning.

Give the designing part a “dwelling” with integrated lighting, generally referred to as a “peep-box”. Thus our exhibit serves in time without play, separate from the rest of the layout, as a pure diorama, perhaps even integrated into a wall shelf. The curve elements as well as the shadow or turning station, whichever design, should stand on independent boxes (frame construction) and could also serve their purpose in future installations.

Final Remarks

You will no doubt have noticed that our proposal is completely different from many presentations in the printed media. We do not want to guide the inclined hobbyist with the typical woodwork or electrical equipment. You will certainly get this presented frequently enough, in the worst case, even as pure repetition.

We also do not commit ourselves to a track system, the choice of which is up to you. Our focus is rather on the stage design of the exhibit, we “only” offer you the suggestion of a harmonious and certainly worth viewing system, this time somewhere in nowhere...

A selection of creations by our editor:
<http://helenensiel.com>

Eisenbahnmuseum Dieringhausen



Einzigartig in Europa

*Spur Z Ausstellung
im historischen Güterwagen
im Eisenbahnmuseum Dieringhausen*

Die Sonntags-Ausstellungstermine:

*08.06.2019, 09.06.2019, 16.06.2019, 07.07.2019,
28.07.2019, 11.08.2019, 22.09.2019, 13.10.2019,
27.10.2019*

jeweils von ca. 10:00 Uhr – ca. 17:00 Uhr

Infos unter: www.stammtisch-untereschbach.de

Competition work by Ralf Junius

Forgotten places in decay

Our reader Ralf Junius also took part in the 2018 diorama competition of the railway magazine after our appeal and the report in the ZFI Forum. Under the given topic "Along the rails", he devised a gravel works that is about to be shut down. The area of the RZS Gravel Works, some of which has already decayed, radiates the charm of the morbid as a model. Ralf Junius explains how this work was created today as part of our annual focus topic.

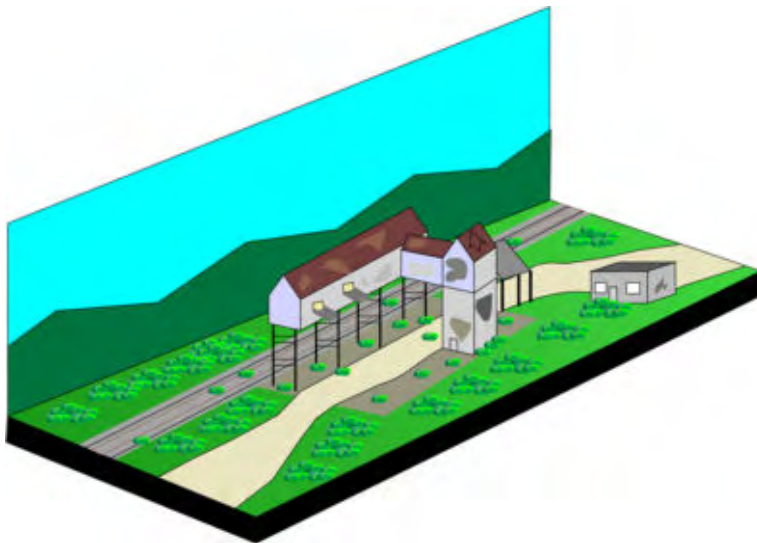
By Ralf Junius. In June 2018 there was an article in the Z-Freunde-International-Forum about a diorama competition of Eisenbahnmagazin. This article aroused my interest. That's why I first bought the corresponding issue to get more information.

There were no special requirements for the competition: The diorama should have the dimensions of 30 x 15 cm and deal with the topic "Along the rails".



A look at Ralf Junius's finished diorama including the background described in the text shows the enormous care with which our reader has worked here. Examples are the colour work on the buildings or the dried tire tracks of the gravel trucks.

I'm still quite inexperienced in the field of equipment, but I've already done some handicrafts at regulars' table boards. In the issues of **Trainini®** and in the forum of the ZFI there were many ideas and hints for building and designing landscapes. I wanted to try this out in practice on 450 cm². So I decided to participate in the competition.



In this article I show the history of my diorama. For the initial registration, I needed a drawing of what I planned to build.

Drawing has never been one of my strengths, so I designed a layout on the computer.

My decision fell on the subject of gravel and gravel works. As a basis I had a kit "Schotterwerk Steiner" from Kibri (former Art.-Nr. 6603) in my kit.

On my diorama the condition is shown shortly after its (the gravel work's) closing. So not everything is 100% in order anymore, but the buildings are not yet marked by decay.

After the confirmation of my registration the tinkering began: First the baseboard and another board for the background were prepared. A primer in the colour sandstone and a black border for the baseboard were my first changes.

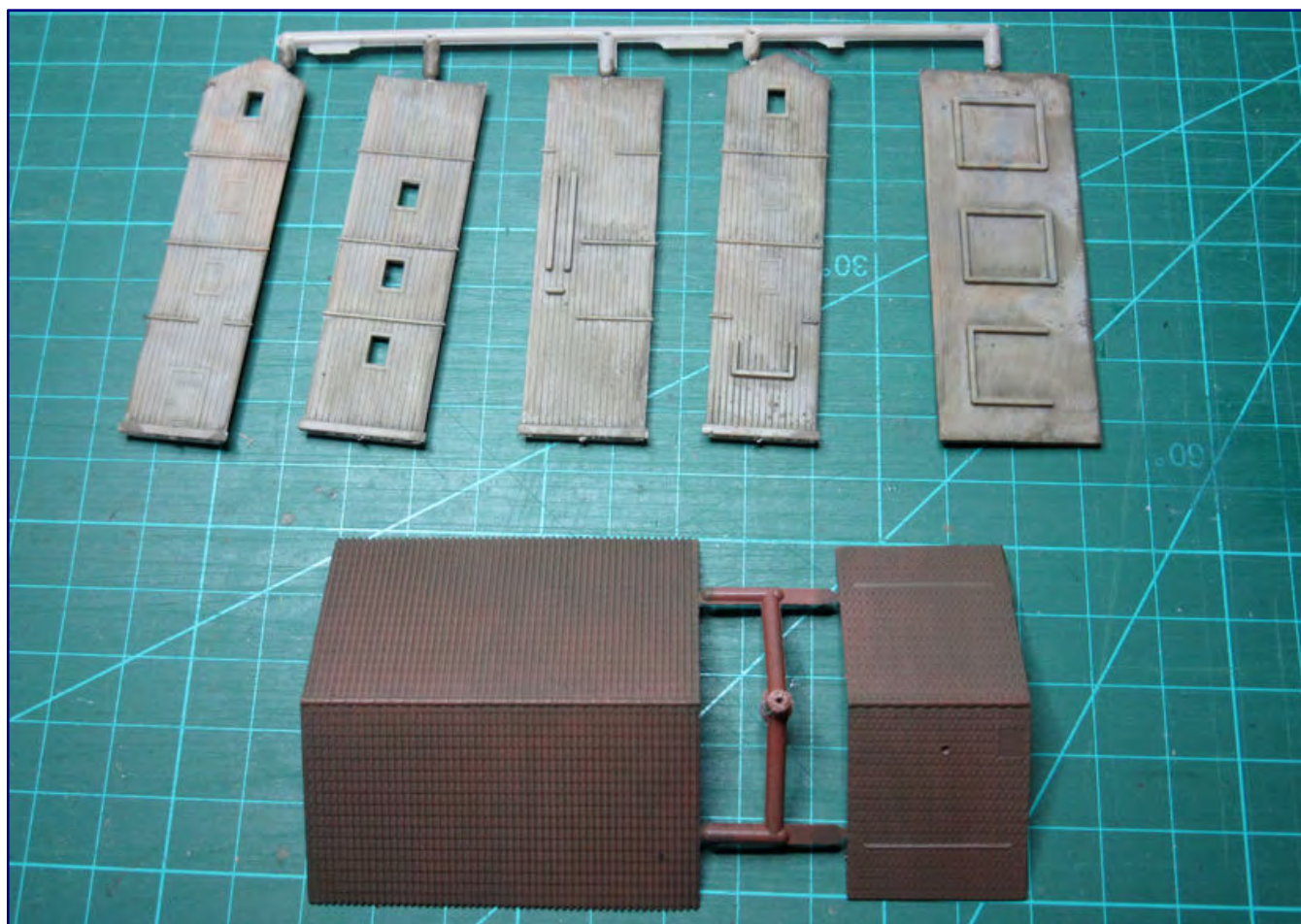
The rehearsal before the design already showed the first changes to the original drawing. The arrangement of the buildings, at least the storage hall, had to be adapted.



The first rehearsal leads to changes in the plan: The storage hall (middle base plate) clearly must be moved to the right.

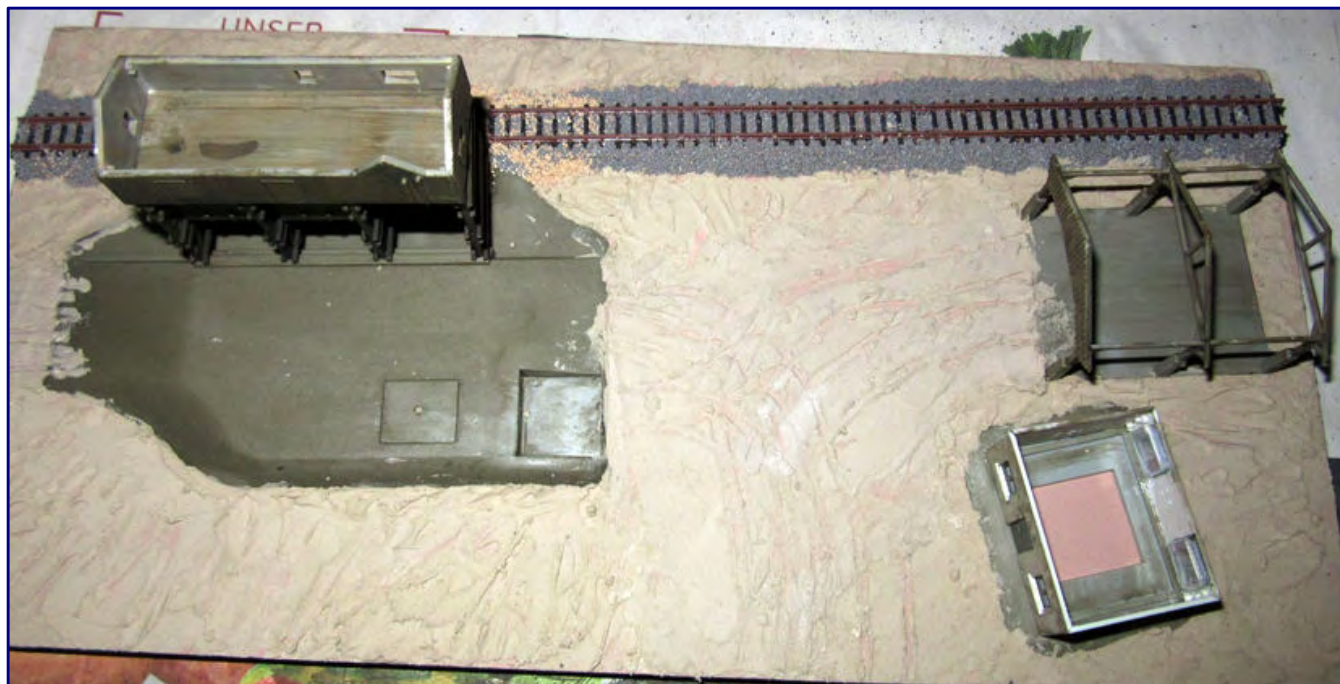
What I don't like about plastic kits is the sheen of the components. I tried to remove this with a glaze of black, brown and green. I once read that this was best done before assembly. Now the parts look pleasantly matt, and the assembly of loading station, storage hall and administration building can begin.

continues on page 14



Before they are assembled, all kit parts are given a glazed patina, which makes the plastic sheen disappear (top picture). The track rails are aged with Vallejo paint "Rust" (Art.-No. 71.080) and then gravelled (photo below).

Because of the theme “Along the rails” my diorama also needs track. That's at least how I understand the description. There are some old Märklin track sections lying around with almost all of us. And since there are no operations, the track doesn't have to be in an excellent condition. At a slight diagonal angle, the finally selected pieces lead past the loading station on the rear side of the diorama.



After the suggestions of our magazine, the base plate was modelled with a wooden spatula (photo above), in which the tire tracks of departing trucks were pressed. After spreading soil, we also landscaped with clumps of grass and grass fibres (photo below).

The tracks were still aged and gravelled. I made the mistake of gluing the track down first, and aging it afterwards. Aging beforehand would have been easier.

There are small cosmetics brushes for make-up. With these you can easily reach the depressions on the track. I then placed gravel on top of the ballast, which fell next to it during loading.

After the placement of the buildings I began with the landscaping. The idea for using wooden spatulas was taken from an issue of **Trainini®**. The entire open area was covered with a thin layer of putty. The still soft putty was then filled with truck tyre tracks that used to be on the site.

I then applied a brown paint to the dried wooden spatula. It should be earth colour; however, I did not make them so complete. But that's not so bad, because in the next step the whole surface will be covered with earth anyway.

For the background, I took a suitable photo on a Sunday walk. Some green should still be on the terrain, but not too much. So I placed some grass or bushes in random places.



A self-unloading wagon (left in the background) and the trailer of a gravel truck stopped due to defects in the decommissioned plant. The cause can be seen on the photo: a broken axle.

On the diorama there should also be a wagon. But how can I explain it in a decommissioned layout? My idea was this: There is still a loaded self-unloading wagon on the site, which derailed during transport and has not yet been salvaged.

Is that credible? Anyway, I'll give it a try. The freight car must not move during transport and at the trade fair in Köln, where the dioramas are shown as part of the competition. How do I fasten it appropriately for this?

My approach is to drill two holes through the wagon and the plate and then fix them with a sturdy wire. In the end, this wire has become a nylon thread, because it is hardly noticeable. In addition to the slight aging of the model, there is also a load of gravel on top. A large part of the loading space has been filled with foam beforehand, and only the uppermost layer consists of gravel.

Trucks and trailers are also part of the Kibri kit. Therefore, such a trailer is also part of the diorama. Due to a broken axle, this was not removed when the trailer was closed. And like the buildings from the kit, it was also still colour treated.



The diorama is complete on the desk of Ralf Junius: The final view of his masterpiece compensates for all efforts.

Especially the red wheels had stopped me from using it before. But with black wheels on silver rims, the model looks quite different. With the assembled vehicle then still an axle was not inserted correctly, in order to represent the effect of a broken axle. Loaded with some gravel, the trailer now stands in the storage hall.

Illustration and all photos: Ralf Junius

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: **Manufacturers' pages for the materials used:**

: <https://www.maerklin.de>

: <https://viessmann-modell.com/kibri/>

: **Source of supply for Vallejo colours (with readers' discount*):**

: <https://www.modellbaukompass.de>

: * Enter the keyword „Trainini“ in the free text of your order
:

Useful tools from Microrama

Specialists for precision work

The French specialist manufacturer for model railway scenery revolutionised the methods for working with static grass and modelling natural looking trees. For the 2019 International Toy Fair, Gwendal Theis then added lightweight rock designs to his portfolio. But the hard-working expert does not rest and is constantly looking for ways to take things to new levels of perfection. Today we present two new tools achieving extra-fine detailing.

It was with great enthusiasm when we first presented a revolution in working with static grass and subsequently also a slightly modified method for flocking trees in **Trainini®** 11/2018.

As for modelling trees, Microrama owner Gwendal Theis already gave a demonstration at the toy fair of how to apply foliage to his fantastic looking trees with the help of a new tool. In the meantime, the device can be purchased through his online shop.

It is available under the name “Minigras Platter“ (Art.-no. 81 01 1020) and looks, at first glance, completely inconspicuous. Neither the product name nor the simple, black plastic housing with the attached, brushed metal plate reveal much about its properties.



The Minigras Platter (Art.-no. 81 01 1020) is a useful tool and works according to same basic principles as a static grass applicator. The metal plate at the top of the device is statically charged with a 9V battery.

Only when we notice the copper wire with transparent insulation and the attached clamp, it becomes clear that this seems to be an electrostatic device. The battery compartment on the back of the box remains unobtrusive. Although not secured with a screw it does have a child lock.

Once a 9V battery is inserted and the protective film is removed from the metal plate the device is ready to be used. It is activated by a toggle switch located on the front panel which is illuminated in the “on” position. Once switched on, the device emits a noticeable buzzing sound.

It has a voltage of 15kV which is more or less equivalent to the voltage of a first generation Noch Gras-Master. Since the voltage is generated from the inserted battery, its current is so low that it does not pose any risk for the human organism.

However, we do recommend that people with pacemakers refrain from operating or getting too close to the device, as its voltage field might disturb the electronics of a pacemaker’s puls generator and electrodes. Better safe than sorry!



Tree foliage (Magileaf) is scattered onto the metal plate and the device is switched on. Applying spray adhesive onto the tree armature, connecting it to the grounding wire, and moving it slightly back and forth whilst approaching the device will make the leaf particles “jump” onto and attach themselves to the branches. This procedure makes for a very fine and natural looking tree canopy.

For all other groups of people, accidentally touching the switched on device can result in a noticeable electric shock, but one that is will not be detrimental or even dangerous to their health. And it happens faster than you think, as we found out.

The "Minigras Platter" is intended for the finely dosed application of foliage or fibres to the Magitree trees. We had already pointed out in our earlier article that the working method can also be applied to all other tree armatures, provided they are electrically conductive or have been treated with conductive materials, such as zinc spray paint, for example.

Once the grounding wire is connected to the tree armature, it acts as the opposite pole to the electric field generated by the metal plate below. Before activating the electric field, however, the selected leaves (Magileaf in various grain sizes and colours) must be scattered onto the metal surface.

Do not forget to spray the tree (which had been already prepared with fibres to imitate small branches) with the special glue (Magispray), and off you go!

Pick up the clamp by its insulated part and slowly move the attached tree upside down and back and forth over the metal plate. The foliage particles that have been already "dancing" on the metal plate after switching on the device will now jump up to the tree and attach themselves to those areas which have been covered by glue.



From our point of view, an important advantage of the Minigras Platter is that it is also suitable for working with tree armatures made from wire. It can also be used with leaf material from other manufacturers such as the autumn leaves from Noch shown here. Another area of application is the production of grass tufts which can then be attached to the layout.

This procedure makes it considerably easier to control the amount of leaves applied, compared to manually scattering them over the armature or using a sieve. And this is not only due to the quantity of material being used.

With a sieve, there is hardly any control over the quantity of leaves falling out, which is inevitably always higher at its lowest point than in the outer areas. The flat surface of the Minigras Platter, on the other hand, makes for a uniform distance to the object and does not employ the force of gravity.

By constantly moving and slightly turning the armature back and forth, the leaves will spread evenly and also comparatively slowly across the branches. Especially spring and autumn trees with a lighter canopy can be easily and very convincingly produced in this way.



Once the process is complete and the Magispray has dried, the trees are treated with Microrama's proprietary wetting agent and plasticizer as described in **Trainini®** 11/2018 and thus preserved. The "Oak grey" texture paste (18 010 107) from the Microrama range is ideal for modelling the bark of the trunks.

According to the manufacturer's recommendations, the typical bark structure can be refined by scribing the paste. Birch trees always pose a challenge, in particular with respect to attaining the right degree of white on the bark. The best strategy is to work slowly and constantly check which tones will produce the desired effect. Checking against photos of real trees always helps. Whites mixed with a little bit of black make for less brilliant and therefore more natural looking tones.

For reference, it should be mentioned that the Minigras Platter is not produced by Microrama. They discovered it as an ideal tool for their method of modelling realistic looking scenery, but in fact it is manufactured by War World Scenics (WWS), a British company, which had intended it for a completely different purpose.



Our trees created with the help of Microrama products look convincing and will cut a good figure on the layout.

Photos on page 20:

We were not really convinced by the colour of the "Oak grey" bark texture (18 010 107) but at least it does give the trunk a typical rough texture (top). Once dry, we gave the trunks an additional coat of Tamiya "Flat Brown" (XF-10). For the birch tree we also added "Flat White" (XF-2), mixed with a drop of matt black (bottom).

The device is also ideal for producing grass tufts to be later placed on a layout. We all know the mostly circular ready-made tufts from Heki, Mininatur and Noch, but they can be easily and more cheaply made at home with the help of a Minigras Platter (although this is not the only possible method).

For this purpose we apply individual drops of a transparent and, above all, flexible grass glue onto a flat surface, preferably a sheet of non-absorbing paper or film which we then attach to a level and non-bending support surface.

Afterwards, we scatter grass fibres of our choice onto the metal plate and connect the clamp off the grounding wire to the surface carrying the glue dots. After switching on the device, we turn the support board upside down and move it close to the electrically charged metal plate. This will make the grass fibres shot vertically into the glue dots.

Vertically standing grasses are the result. After drying, the tufts can also be coated with grass glue at the tips and sprinkled with fine turf. This technique allows for modelling new plants, which will certainly add further detail to the scenery. Modelling swaying reed in swampy areas would be one example that could be produced with this technique.

Specialist for intricate details

Let us take this opportunity to also briefly introduce the new Microgras Pen (81 01 1010) from Microrama. The device is also sourced from WWS and distributed by Gwendal Theis. We don't know if it is a departure from or an addition to the RTS-Greenkeeper. In any case, it fits in perfectly with the product and work philosophy of the French landscape modelling specialist.

Upon first inspection the device looks familiar to the previously presented Minigras Platter: black plastic casing, copper wire with transparent insulation and attached clamp, and a compartment for a 9V battery.

But instead of the metal plate on top, this device comes with a second wire and, attached to it, a unit that looks like a small microphone. But the end of the unit carries a sieve and on its inside one can detect a small cavity with a metal head at the bottom.

What we have in front of us is essentially a miniature version of a Grasmaster: Its battery powered electronics also create an electric field of 15kV, but due to the lack of space in the microphone shaped applicator unit, the electronics have been moved into the housing which also carries the on-off switch.

In terms of price, the device can be described as very affordable, as it does not even cost half the price of other static grass applicators in this performance class. The Microgras Pen is no alternative, however, to a bigger applicator, but that is also not its role.



The Microgras Pen (81 01 1010) is the smallest static grass applicator we know of. It is designed to also work in difficult-to-reach areas of a layout.

The tiny amount of fibres that can be loaded into the applicator unit is too restrictive to cover any significant areas on a layout. Large pastures or fields are not feasible with it in a reasonable amount of time. But that is also not the purpose it was designed for. Its strengths lie where other devices cannot be used due to their larger size.



The Microgras Pen is helpful in creating such an idyllic scene. For example, fibres can be added on top of already existing layers of static grass in a targeted manner.

The Microgras Pen can be used to apply fibres from 2 to 4 mm in length to small or hard to reach areas of a layout. Of course, it also works with shorter 1 mm fibres and can thus be used to create a small closely cut English lawn in the garden of a Z-scale house.

It is therefore perfect for working in built up areas of a layout and even on the tight space between two buildings or a narrow green strip between fields. At the same time, it seems also perfectly suited for applying a second layer of partial patches of longer grass onto a first (and already dried) layer of shorter grass.

The Microgras Pen is certainly not meant for covering large areas, but it does meet the highest demands of the perfectionist modeller. It is often precisely such precision work that adds life and extra visual interest to an otherwise monotonous surface.

Manufacturer of the tools:
<http://microrama.eu>
Electronic sales / source of supply:
<https://eshop.microrama.eu/de>

Maintenance & Care (Part 8)

Lying patients

“Please lie down”, will not work as a request to a locomotive model. And yet our little patients are easier to treat if they are not standing on their wheels and are also protected against falling over. In the ideal case, a professional foam train service tray serves as an aid, of which we would like to present a new product from the company Noch today.

There were and are many professional foam train service trays on the model railway market. What they have in common is that they should make work easier for model railroaders. We can never have enough hands, which is why we quickly reach our limits.

If we hold the screwdriver and immediately apply a screwdriver, we already lack one: After all, we are not working on a solid wall, but on a model that can fall over, slip away or, in the worst case, even follow gravity towards the ground. We have to securely hold the locomotive we are repairing or maintaining in order to work on them safely and efficiently.



With the professional foam train service tray N/Z (Art.-No. 99352) Noch has brought an interesting tool for the maintenance and repair of models onto the market.

The main task of professional foam train service tray is to replace this third hand, which we lack. In it a model should lie securely, be fixed and at the same time protected from damage. In the best case it offers protection (on up to three sides) and leaves the fourth freely accessible. This means that such a tool must not scratch the surfaces and must have sufficient flexibility to accommodate all vehicle models equally.

Today we would like to present a novelty that is now also included in the manufacturer-independent range. The accessory supplier Noch has delivered two different sized models at the same time. The larger one is intended for the gauges H0 / TT, the smaller model of the professional foam train service tray relevant for us was developed for the nominal sizes N / Z (Art. No. 99352).

The Wangen based supplier claims that its novelty has been “developed by professionals for professionals”, as can be seen from the many details of these foam train service trays. This is exactly what we have investigated and tried to evaluate.

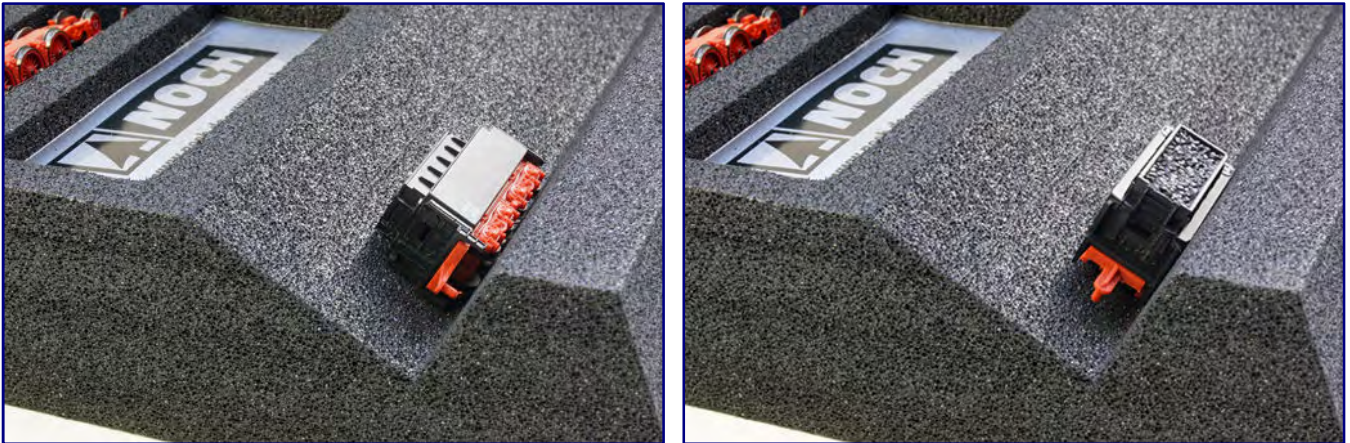


In order to be able to work on wheels and gears, the model is placed in the head position. Unfortunately the cheeks of the tray do not grip Z gauge locomotives, which is why they can tilt a bit to the side. With some foam, kitchen paper or fabric, it could be easily fixed in this position.

The professional foam train service tray from Noch has two slots for locomotives. In one of them the locomotive can be held upside down. This makes it easy to inspect the bottom of the locomotive.

This position is indispensable for lubricating the gearbox and axle bearings. Without any aids, we will always have to reach around again, which can cause the model to fall over and can also cause the lubricant to spread somewhere on the housing, possibly causing damage.

The foam chosen by the manufacturer is quite firm and grips every locomotive safely, as long as its external dimensions and surface structures fit the indentation. Exactly this is also a small weak point,



The model can be placed in two angular positions on the inclined shelf. Depending on the work carried out, its side surfaces are thus ideally accessible. If necessary, the foam train service tray can also be turned so that the tender is fully accessible even in the position shown on the right.

because both locomotive service trays are designed for two track gauges each. Models of the larger scale are very difficult to push in under pressure, because the width of the slot is very narrow.

Fortunately, the locomotive extends past the edge, and the narrower side can be easily grasped and pulled out a bit. The flexibility of the foam is sufficient enough to allow this. From our point of view, there is no risk of damage to the paint and lettering.

On attachments, especially handlebars or fittings on the steam locomotive boiler, however, great care must be taken. After all, plastic parts are more at risk than metal housings. However, there is no danger here for Z track gauge, as the smaller of the two sizes mentioned. This is a different problem, but one that can be mastered.

Z gauge models are naturally narrower, and therefore have no simultaneous contact with both outer sides of the clamping bay. Nevertheless, they cannot fall down, which is why it is quite comfortable to work in this way. In the case of more difficult assembly steps such as inserting compression springs in the coupling shafts, and the subsequent "locking" by the coupling hook, however, a tipping model is quite annoying.

In these cases a piece of foam or some kitchen paper, whose absorbency will be useful when spilling oil drops, can help as padding. And so we find the professional foam train service tray to be a very helpful tool.

However, we still have a lot more to think about and have also integrated an inclined shelf. The manufacturer is also not alone with this feature, some foam train service trays combine both storage options in a single shaft, but then at the expense of a secure fixation when the locomotive is set on its roof. In the inclined position mentioned above, locomotive or car models can also be processed laterally, by the way in two different angles.

Whether side walls have to be cleaned or decals have to be applied, there are various possible applications, in practice. With steam locomotives, this layer is the only way to reach the rods safely as long as the locomotive is not to be laid flat on the table.

If a dust fluff has to be removed with tweezers, or the wheel bolts have to be removed, only a locomotive bed equipped with an inclined support offers full utility. This is the case with the new product presented today.

The Noch foam train service tray is supplemented by various compartments and recesses. Brushes, oiler pencils or tools can be stored here in an accessible manner. Without having to move away from the work surface and take your other hand off the model, full access is available at all times.



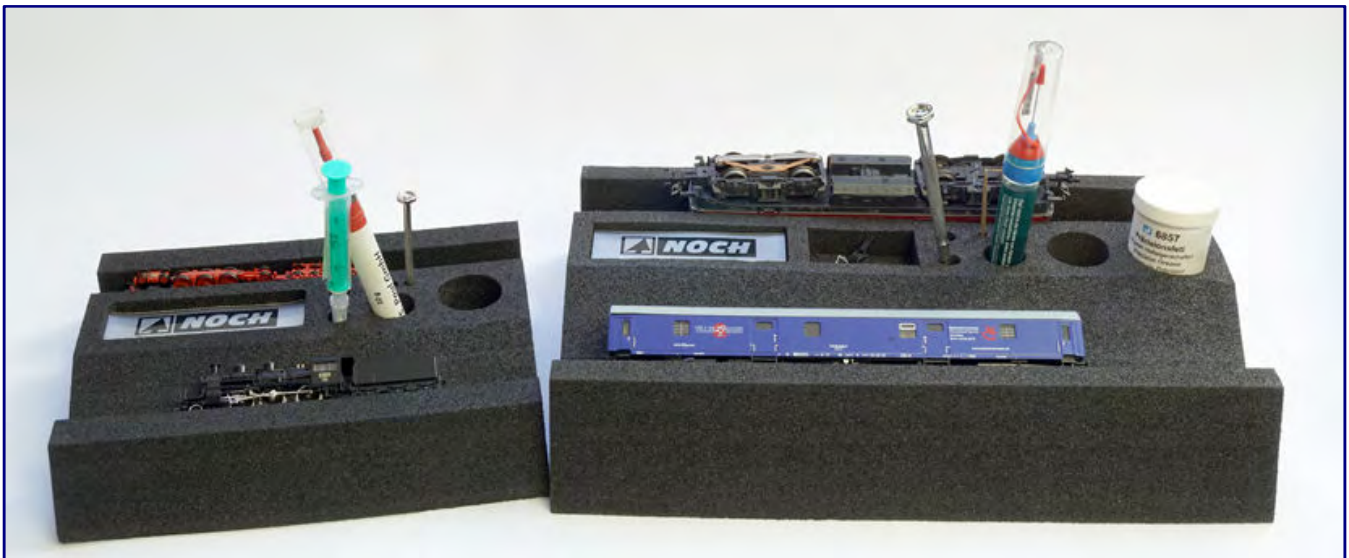
The Noch professional foam train service tray also has slots for screwdrivers, syringes with lubricants or oil dispensers (top picture). Also useful is the magnetic foil with Noch lettering (picture below), which protects against the loss of compression springs and small screws.

For the sake of completeness it should be mentioned that the smaller locomotive tray considered here has two compartments less than its counterpart for the nominal sizes H0/TT. However, we do not feel a lack of such compartments.

In the largest compartment, small parts such as brushes, clutch springs and hooks, lamps, light guides that have fallen out or screws can be stored while the model is being processed. More helpful, however, is usually the magnetic power of the incorporated foil (with Noch company logo) in a rectangular tray, which safely protects all ferrous metal parts from loss.

The overall impression

Foam train service trays are available from different suppliers and in different price ranges. There are choices of simple foam parts with shafts, which can be equipped with additional magnetic foil, foam parts with slots for parts, and screwdrivers, as well as fully equipped tools, which can also be obtained from Noch.



The Noch professional locomotive service trays (left N/Z, right H0/TT) are definitely among the more demanding tools of their kind. The larger model has two compartments more, as can be seen in the picture: The rectangular one for couplings or axles we would have liked for the smaller model as well.

It is still a luxury model that combines much usefulness into one unit. Thanks to a good dealer network and electronic distribution, it is broadly and easily available. If a direct purchase is made from Noch and the local retailer is indicated, the local dealer also receives a sales commission. The Wangen based specialist is thus thinking of those customers who would have to travel long distances to find a dealer without depriving the specialist trade of its livelihood.

The price of the novelty is certainly not low, but still worth its money. Those who already own a train service tray and are satisfied with the chosen model will have no need here. For all other model railroaders the presented model is a good choice, which should pay off in the long run in any case.

Manufacturer websites on the topic:
<https://www.noch.de>



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6. JULI 2019 IN DIETENHOFEN

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Die Reisezugwagen der DRG Für Historiker und Eigenbauer

Gut versorgt sind Modellbahnfreunde mit Fachliteratur für Güterwagen. Zu Reisezugwagen gab und gibt es hingegen sehr wenig Buchauswahl. Wer an einen Selbst- oder Umbau für seine Modellbahn denkt, wird den Mangel umso stärker spüren, weil ihm meist Zeichnungen und Informationen zu den vergebenen Reihen an Betriebsnummern fehlen. Der erste von drei Bänden im EK-Verlag schafft Abhilfe für von der DRG gebaute Personen-, Heiz- und Gepäckwagen.

Joachim Deppmeyer
Reisezugwagen der Deutschen Reichsbahn – 1
1921 bis 1931 - Regelspur

EK-Verlag GmbH
Freiburg im Breisgau 2018

Gebundenes Buch
Format 30,0 x 21,0 cm
240 Seiten mit 114 S/W-Abbildungen und 89
Skizzen

ISBN 978-3-8446-6414-0
Best.-Nr. 6414
Preis 45,00 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Wer immer seine Modellbahn sehr nahe am Vorbild ausrichtet oder gar Eigenbau betreibt, der hat naturgemäß auch großen Bedarf an hochwertiger Vorbildliteratur. Technische Beschreibungen, Daten und Fakten zur Einsatzgeschichte sowie Zeichnungen sind der Grundstock solcher Tätigkeiten.



Bereits 1982 hatte der Eisenbahn-Kurier dazu ein literarisches Werk vorgelegt, das sich mit den Reisezugwagen der Deutschen Reichsbahn-Gesellschaft auseinandersetzte. Der vorliegende Band ist Teil 1 einer dreiteiligen Überarbeitung, Neuauflage und auch Erweiterung.

Mit Joachim Deppmeyer hat sich wieder ein anerkannter und erfahrener Autor auf diesem Gebiet an die Arbeit gemacht. Was er seinen Lesern vorlegt, hat uns begeistert und weckt die Vorfreude auch auf die beiden noch folgenden Bücher. Teil 2 ist aktuell für den Herbst 2019 geplant.

Werfen wir zunächst einen Blick auf das, was dem Vorgängertitel noch gefehlt hatte: Heizwagen fanden jetzt ebenso Eingang wie das Erweitern durch eine Gegenüberstellung der Bauarten nach Vorcomputer- und Computernummernzeit. Dies erleichtert dem Vorbildfreund die Orientierung erheblich und hilft vorbildgetreuen Modellbahnern ebenso, ihre Modelle historisch korrekt einzusetzen.

Der neue Band profitiert aber auch von seinem neuen Format. Dies erlaubt, die Wagen deutlich großzügiger abzubilden. Die Wagenzeichnungen sind im Maßstab 1:100 gedruckt, was sich als anerkanntes und „spurweitenneutrales“ Maß etabliert hat.

Beschrieben und gezeigt werden hier nur die zwischen 1921 und 1931 gebauten Personenwagen der DRG. Von den Länderbahnverwaltungen übernommene Fahrzeuge spielen hier keine Rolle, jüngere Exemplare werden sich in den noch folgenden Büchern wiederfinden.

Sehr gut und hilfreich sind die ersten Kapitel zu verschiedenen Entwicklungsstufen der Bauarten wie auch die unterschiedlichen Entwicklungen der Baugruppen. Hier wird Grundsätzlich behandelt und chronologisch wie auch technikgeschichtlich wiedergegeben, was alle vorgestellten Vorbilder verbindet oder auch voneinander unterscheidet.

Die Wagenbauarten selbst werden mit einem kurzen Abriss, einem Schwarz-Weiß-Foto und tabellarischen Angaben sowie einer Zeichnung vorgestellt. Für jede (Unter-)Bauart ist hier stets eine Doppelseite vorgesehen.

Den Abschluss des Werkes bilden Einzelaufstellungen der Wagenbauverträge von 1921 bis 1931 sowie eine Übersicht über Beschaffungen von Personen- und Gepäckwagen nach Länderbahn-Zeichnungen.

Ein so hervorragendes Werk vorzulegen, war eine große Herausforderung für den Autor. Zum einen standen und stehen Wagen weniger im Fokus von Fotografen und Bahninteressierten als Lokomotiven, zum anderen sind aber auch viele Originalunterlagen verlorengegangen oder nicht mehr auffindbar.

Doch das mag der Autor selbst mehr bedauern als seine Leser, denn das zusammengetragene Wissen ist beeindruckend und Lücken sind für Dritte nahezu nicht zu erkennen. Auch die Qualität der ausgewählten Bilder und Zeichnungen ist gut, was auch deren Wiedergabe einschließt.

Bestand 1982 noch das Manko, nicht alle Bauarten mit Fotos aus ihrer Indienststellung zeigen zu können oder gar in Einzelfällen eine vollständige Lücke bestanden hatte, so wurde auch hier Hervorragendes geleistet. Das Buch ist in sich rund und ein großer Sprung gegenüber der Erstausgabe.

Eisenbahnhistoriker werden das Buch als Pflichtlektüre betrachten und den älteren Vorgänger sicher hierfür ausmustern. Modellbahner werden in die Lage versetzt, ihre Modelle korrekt zu beschriften und in geeigneten Zeitabschnitten einzusetzen, denn die dafür erforderlichen Angaben reichen sogar noch bis in die Zeit der DB.

••••• Publishing pages with reference possibility: •••••
••••• <https://www.eisenbahn-kurier.de> •••••
••••• <https://www.ekshop.de> •••••

Das Lokomotiverbe der DDR **Schienenfahrzeuge aus dem Osten**

Zu den in der DDR generalreparierten, rekonstruierten und neu gebauten Dampflokomotiven gab es bereits einen Typenkompass bei Transpress, der auch deren Bundesbahnschwestern einschloss. Abseits davon klaffte noch eine große Lücke, denn die Mehrheit der DR-Lokomotiven blieb noch offen. Der heute vorgestellte Band widmet sich daher ausgiebig der Dampf-, Diesel- und Elektrotraktion in der DDR und macht auch vor Triebwagen und Werksbahnen nicht Halt.

Klaus-Jürgen Kühne
Typenkompass Loks der DDR
1949 - 1990

Transpress Verlag
Stuttgart 2019

Taschenbuch mit Klebebindung
Format 14,0 x 20,5 cm
128 Seiten mit 62 Farb- und 53 S/W-Fotos

ISBN 978-3-613-71591-2
Titel.-Nr. 71591
Preis 12,00 EUR (Deutschland)

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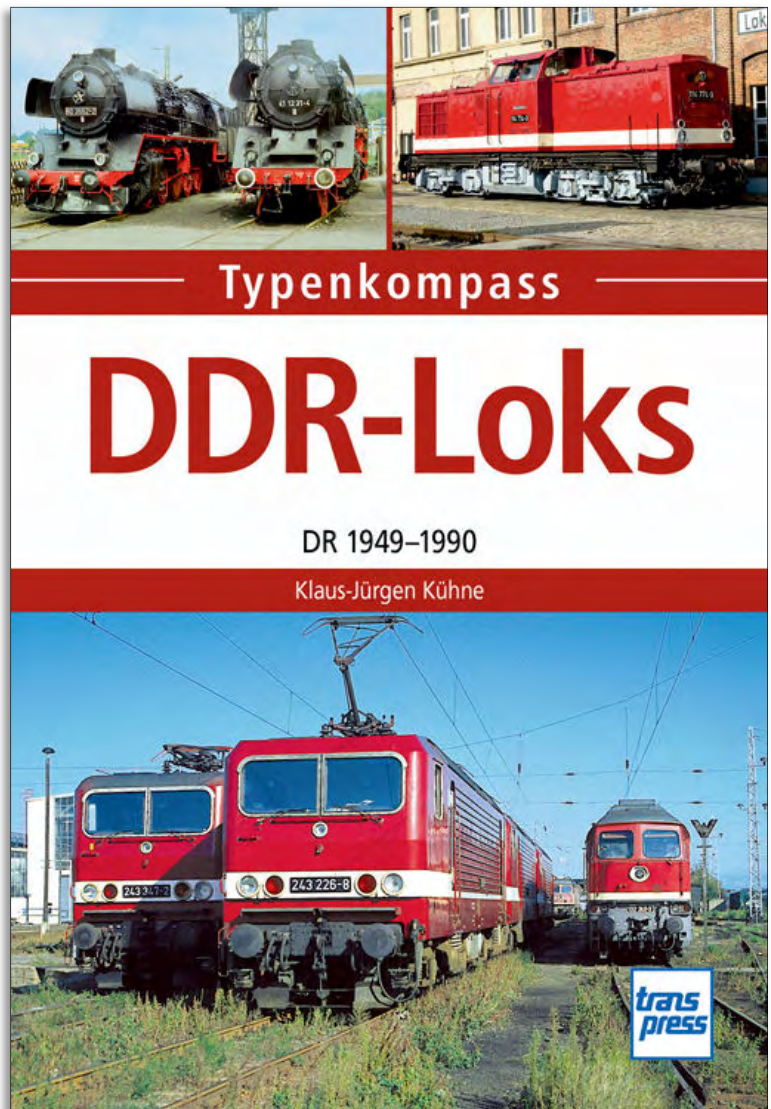
Fast dreißig Jahre liegt das Ende der DDR bereits zurück. Mittlerweile sind auch die letzten Lokomotiven aus dieser Ära, ebenso wie ihre Verwandten aus Bundesbahnzeiten, auf dem Rückzug.

Viele fanden bei privaten Eisenbahnverkehrsunternehmen eine neue Heimat, ebenso wird ihr Erbe in Museen gepflegt.

Der vorliegende Band aus der Typenkompass-Reihe von Transpress komplettiert diese nun (wieder) um ein weiteres Werk, das sich von der DR entwickelten, umgebauten oder auch importierten Lokomotiven aller drei Traktionsarten und Triebwagen widmet.

Nach den Titeln zu DR-Güterwagen und deutschen Bahnpostwagen war dieses Buch abzusehen und irgendwie fällig. Mit Klaus-Jürgen Kühne aus Halle (Saale) hat sich ein interessierter und auf diesem Gebiet kompetenter Autor daran gemacht, die Lücke zu schließen.

Das uns vorliegende Buch reiht sich nahtlos in die Portraitreihe von Transpress / Motorbuch-Verlag ein. Alle beschriebenen Fahrzeuge wurden kurz und kompakt auf ein bis zwei Seiten vorgestellt, beschrieben, um eine oder mehrere Aufnahmen ergänzt und mit tabellarischen Angaben versehen.



Schon das Durchblättern des Buches bereitet Freude: Die Auswahl und auch die Wiedergabequalität der Bilder sind nicht zu beanstanden, weshalb sie die Bildaussagen gut und sinnvoll zu unterstreichen vermögen.

Berücksichtigt wurden auch die Fahrzeuge der Berliner S-Bahn sowie regel- und schmalspurige Vertreterinnen von Werk-, Anschluss- und Feldbahnen. Der Autor selbst schränkt zwar sein Werk auf die wichtigsten Fahrzeuge zwischen 1945 und 1990 ein, doch wir haben keines vermisst.

Das bedeutet, dass die Auswahl entweder doch vollständig ist oder auch uns keines einfällt, das hier noch zu berücksichtigen gewesen wäre.

Selbst die Exotin der Baureihe 08, umgebaut aus einer französischen Schnellzugdampflok, findet sich in diesem kompakten Taschenbuch wieder. Bestens bekannte Lokomotiven wie etwa die Baureihen 01⁵, 106, 118, 130 oder 132, 211, 242 und 243 finden eine ausreichende inhaltliche Würdigung.

Das haben alle diese und weitere Fahrzeuge auch verdient, denn bei Bau, Beschaffung und Modernisierung von Eisenbahnfahrzeugen gingen beide deutsche Staatsbahnen ihre eigenen Wege.

Jener der DR verlief nicht immer zielstrebig und mit klarem Kurs. Ideologische Vorgaben und Mangelwirtschaft erschwerten die Arbeit der Reichsbahn, was auch im erst 1988 erfolgten Ende der Dampftraktion deutlich wird.

Wer sich eine kleine Vorbildbibliothek auf Basis der Typenkompass-Reihe angelegt hat, der wird, um sie vollständig zu halten und rundum gut informiert zu sein, sicher auch zu diesem Taschenbuch greifen. Alle wichtigen Informationen werden hier schließlich stets kompetent und komprimiert vermittelt.

Es sei der Vollständigkeit und Richtigkeit halber aber auch erwähnt, dass das Buch nicht völlig neu geschrieben wurde: Es handelt sich um eine überarbeitete mit neuem Bildmaterial angereicherte Auflage auf Basis eines früheren Verlagstitels.

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: Publishing pages with reference possibility: :
: <https://www.motorbuch.de> :
:.....

Joint meeting in Sindelfingen South German large fan meeting

Every two years regulars' table friends of the Z gauge from Bavaria and Baden-Württemberg meet in Sindelfingen in order to exchange technical information, to show their works and also to demonstrate new achievements of technology. A company hall is needed to accommodate all the guests of this no longer small meeting under one roof. Stephan Fuchs explains why it was worthwhile to be there on Mother's Day weekend.

By Stephan Fuchs. Every two years, the southern German Z gauge model railroaders meet at the company premises of Eberhard Krug in Sindelfingen. The whole thing is deliberately conceived as a interest group or fans meeting and not as a public event.

The common hobby and also the meeting, as well as the discussions with friends outside of the well-known fairs, are to be put into the foreground!

The rooms are very well chosen, the area is easily accessible, there is sufficient space and above all there are many, also bizarre and rare, exhibits stored on Eberhard's shelves. You will also find various postcards with mostly (un)fitting sayings, which you are welcome to take along with you.



A view with a fisheye lens of the company hall of Eberhard Krug, which was also the place for the meeting this year.



This showpiece inspired our author and photographer as hifi enthusiasts very much.

The fact that Eberhard is also a perfect host and that drinks and barbecues were available rounded off the great thing.

A small drop of bitterness: The floor swung even more than the hall floor in Altenbeken, which is unfavourable for photographs. However, the talks and the meeting of friends were in the foreground, so photos became an accessory.

And so on the weekend of 11 and 12 May 2019 many friends of the small track south of Stuttgart came together, showed and saw a cross-section of the possibilities that can arise with the Z gauge.

In addition, one or two small series manufacturers joined in to demonstrate innovations or their range of products.



Some small-series manufacturers such as NoBa-Modelle (not pictured), Railex and Zcustomizer also presented a cross-section of their range at the interest group meeting.

Eberhard Krug presented his America module layout, while Karl Sinn had a layout with a Swiss landscape. Both systems showed once again how much a harmonious background contributes to the good overall impression. This can also be seen very clearly on the pictures in this article. Both systems also offered many small details that contribute to a vivid image of the model.

From the Schorndorfer Stammtisch Volker Klein was represented with the Stammtisch modules. In the interior between the modules, he had built a landscape with a city. This resulted in a complete system that is very easy to set up and transport due to the modules. The dangers are analogous.

Jürgen Walther had reforested his Hirschsprung massively in the meantime. Who knows the route in the original, recognizes it here doubtlessly also in the miniature. Perhaps the biggest compliment for model railroaders is if the model was successful in the chosen period, in miniature.



The module "Hauptstrecke in Tuscon, Arizona" by Eberhard Krug (photo above; built by Spur-Z-Atelier) and the station Kesswil am Bodensee (Lake Konstanz) exhibited by Karl Sinn (photo below) clearly show what depth effect creates a suitable backdrop for photos.

With this layout it became also very clear that only in our scale with still manageable space requirement enormously realistic replicas of given motives are possible. Some of the new guests who happened to be present were perhaps not yet aware of this.

continues on page 39



The layout of Volker Klein (photo above) was composed of regular table modules. And on the Tucson module a new stagecoach took place during the meeting (middle photo). Impressions of Eberhard Krug's USA layout (photo below left) and Karl Sinn (photo below right) testify to the excellent quality of the modules shown in Sindelfingen.



Some layouts were still at an advanced stage of construction. It was noticeable that this was an incentive for many guests to also exchange experiences in landscaping. It is precisely this exchange of experiences that distinguishes regulars' meetings, because this is far less the case at trade fair appearances, e.g. of Z-Freunde International.



Picture above and page 38: Jürgen Walther has reforested his plant "Hirschsprung." And beyond that there were many new details to discover.

Klaus Moser showed for the first time his newly acquired module system, including minor teething troubles, which however did not bother anyone. On the contrary, some of them could only notice that they were not alone with one or the other problem.

Rolf-Dieter Wörz also tinkered with his unfinished module system. And so it was also very nice to see how a system grows step by step and gains shape.

Siegfried Dinkelacker had exhibited a tram system as well as a route for buses and trucks. NoBa-Modelle also showed self-propelled vehicles for the Z-Car system, with the vehicle housings coming from the 3D printer. Especially interesting was the VW transporter T1 with open platform.

The small, but very fine composition was rounded off by the countless exhibits in the many shelves and showcases. Therefore, a 2021 visit is worthwhile, in any case, in order to explore everything!

In summary, I would like to say that Saturday has become a great meeting with many visitors. Some of the visitors had come several hundred kilometres. It was nice for me to see new faces as well as many familiar ones.



Photo above:
Rolf-Dieter Wörz worked on his unfinished modular system, allowing visitors to participate step by step in the creation of the landscape.

Photo below left:
On the basis of the new chassis for the Z-Car system, the NoBA models now also taught VW Transporter T 1 with flatbed to run, or better yet, drive.

Photo below right:
Siegfried Dinkelacker uses the Shorty models from Rokuhan to simulate European tram operations.

The joint barbecue in front of the hall, Eberhard Krug's great organisation with his helpers, the exhibits on display, and the guests, simply everything contributed to an all-round successful event, just, that it's not supposed to be an event. Even conversations between not so benevolent colleagues should have taken place: The focus was on what they had in common!

All Photos: Stephan Fuchs

Webpages of the participants:

- <https://www.noba-modelle.de>
- <http://www.spur-z-atelier.de>
- <https://www.zcustomizer.de>

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Safety measures on the chimney:

A while ago I had already called in with a hazard light for the Kibri chimney. I decided to do it again with a new Kibri chimney and now it could be implemented as I had imagined. Attached is a picture of the chimney.

The new tower now has two hazard lights at the top and one at the front and one at the rear slightly above the centre of the tower.

I realized the lighting with a Viessmann LED, which is located in the base of the tower. The chimney contains four 0.5 mm diameter optical fibres which are held together by the inner part of a plug sleeve and bundled above the LED.

Above all, it's nice that you can't see anything of the modifications in daylight. Visitors get a surprise when the light comes on.

Rainer Hecker, Düsseldorf

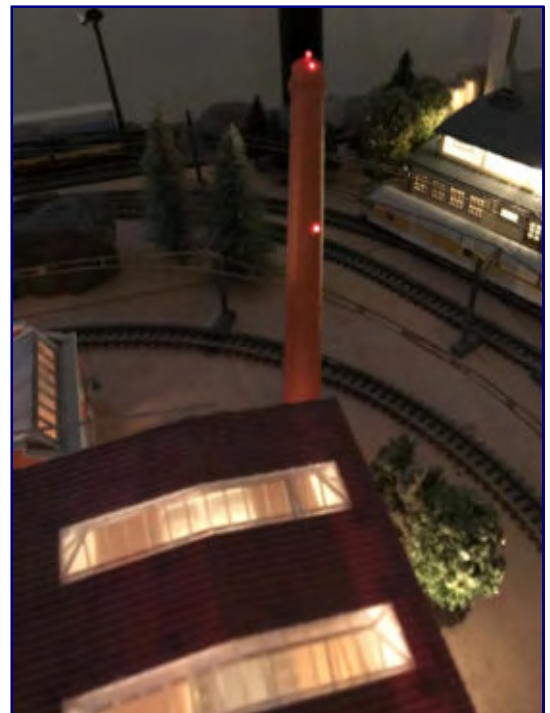


Photo: Rainer Hecker

To the Rowland Emmett layout - presented in the trade fair report of the May edition:

I do believe that model railroaders should break new ground - whether this is to be recorded under art or kitsch or under something completely different (e.g. affectionately: nonsense), that probably remains rather a subjective approach and perception.

I myself have built a tiny Z-layout (quarter circle) with tunnel openings into a Milka heart-shaped chocolate box, for example, so that a full circle can be put together at any time - rather kitsch, or even rudimentary art?

I also equipped a box layout with a Z train: winter landscape with (almost) all features, including an (imaginary) terminus station with a hidden track that goes through the station building and makes the system appear like a diorama, although there is a winding roundabout. Probably more art - or nonsense?

I'll just send you three pictures - have fun!

Markus Krell, Tiefenbach



Our reader sent us these three photographs to underline his intention. Photos: Rev. Markus Krell

Answer from the editors: We are delighted with the clear statements about new paths that Michael Rehorik took and presented at the Intermodellbau exhibition. Like many other readers who expressed themselves similarly in oral form, we are of the same opinion. Something refreshingly different was shown here, expressing passion, emotion and attention to details in its own way.

That it is also connected with caricatural statements and forms of expression, we see it by no means as kitsch, but all the more as an artistic form of expression that initially captivates, inspires and finally invites reflection and understanding. We are happy when people see the chance to convert the model railway into an art object.

Personal railway impressions from Japan:

Our translator Christoph Maier visited Japan with his family in April and was just as enthusiastic about this country as our editor-in-chief, Holger Späing, was during his visit three years ago.



Rokuhan's showroom in Tokyo's Asakusa district carries their full product range and has this large demonstration layout on display which was run for our translator. Photo: Christoph Maier



Photo above:
Rokuhan's only model so far of a European prototype: the first two versions of the class 181² displayed on the "Hp Grund" diorama, a present from our editorial board to Rokuhan during our visit in 2016. AZL products are, by the way, also sold at the showroom.

Photo below:
Visiting the Kyoto Railway Museum and its Shinkansen Series 0, which entered service for the 1964 Tokyo Summer Olympics as the world's first high speed train. Photos: Christoph Maier

A visit to the Rokuhan show room in Tokyo's Asakusa district was of course on the agenda for him. He was impressed by the full availability of the manufacturer's entire product range, and the large demonstration layout, which was also put into operation especially for him. Unfortunately, it was not possible to communicate with the shop attendant due to the language barrier.

Dirk Kuhlmann's diorama "Hp Grund" is also on exhibit at the Rokuhan showroom. It was a gift from the editor to Rokuhan during a visit in 2016. Rokuhan uses it to showcase its so far only model of an European locomotive, the DB class 181².



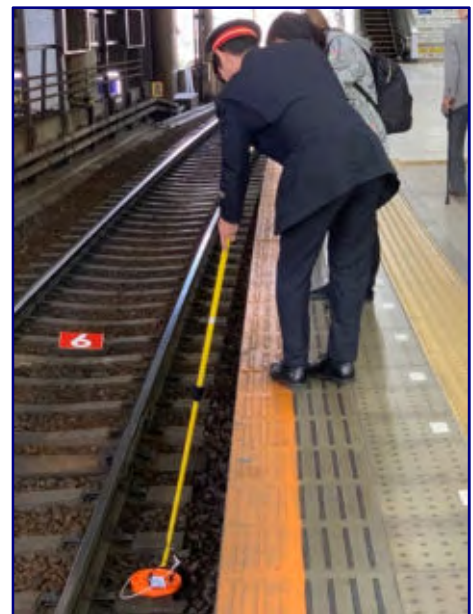
The visit to the Kyoto Railway Museum took place on the recommendation of our reader Raffaele Picollo from Genoa, Italy. In view of the interesting exhibits from Japan's rich railway history, it was definitely worth it. Photo: Christoph Maier

Having visited also three other model railway shops during his trip, Christoph had the impression that railway modellers are well taken care of in Japan, as long as N scale is your thing, where the choice of (Japanese) rolling stock, kits and vehicles is almost overwhelming. The range of Z gauge products, on the other hand, seems to be very limited in Japan as well, with some items such as road vehicles, not available at all.

On the recommendation of our reader Raffaele Picollo, Christoph Maier also visited the Kyoto Railway Museum and was very impressed by its exhibits, which wonderfully convey the great significance of railways as a means of transport in a country that embraces and celebrates technology.

Christoph was also enthusiastic about how easily and smoothly rail travel works in Japan, compared to (present day) Germany. Both the long distance Shinkansen lines as well as local trains are always clean, on time and operate at a high frequency.

Trains and platforms are well indicated, with signs almost always also in English, making it easy for tourists to find their way. Even



Rescuing a mobile phone from the tracks: A special emergency call switch for such cases is mounted on the platform. Photo: Christoph Maier



A ride with the Hello Kitty Shinkansen was, of course, a must for someone who works for our magazine (top). Our February issue, which covered the prototype and the Z-gauge model, was also there on April 16, 2019 (bottom). The sales staff in the train were enthusiastic about the magazine.

Meanwhile another Hello Kitty themed train, the Haruka limited express, has hit the tracks in Japan (photo on next page). All photos: Christoph Maier

public toilets are well maintained and freely accessible in every station, even the subways. And they are certainly several quality levels above the often dirty and foul smelling pay toilets of German large city railway stations.

As a little concluding anecdote, Christoph also sent us a photo of a mobile phone rescue. Would such a thing be conceivable in Europe?

Current deliveries Noch:

In addition to the foam train service tray presented in more detail in this issue, Noch also supplied patina markers for models (art. no. 61158) and for the landscape (art. no. 61159). Their colours are matched to each other and should deliver appealing results.

The markers are also very useful and have two different tips: a soft brush tip that adapts to the structure and a wide



one for the treatment of surfaces. With the “Blender pencil” of the second pack, it is also possible to subsequently lighten or wash out the paint application.

Further information on the new NOHAB from FR Freudenreich Feinwerktechnik:

Harald Thom-Freudenreich sent us some interesting information and explanations about our test report in the last issue. We would like to share them with our readers.



Due to the great success of the model presented by us, the NOHAB of FR Freudenreich Feinwerktechnik is revised. In future, these locomotives were to be fitted with injection-moulded inserts in the driver's cab windows. Photo: Jörg Erkel / 1zu220-Shop.

So we learned that the light emission we observed on the outside windows of the driver's cab only occurs on the first five analogue models. The reason for this is not the missing light shield, as it is built into the digital models. Apparently, this had also been recognized by the manufacturer, because from the sixth analogue locomotive there was a change: the window edges have been covered with undiluted black paint by means of a retouching brush.

This solution shows a good effect, as the tested digital version proves in this context. Nevertheless, it is only a temporary solution. Due to the good success of the NOHAB, plastic-injected window inserts they will replace the milled polycarbonate inserts.

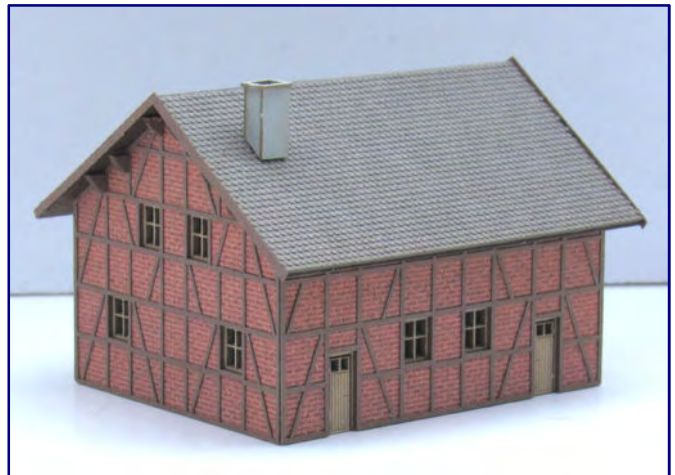
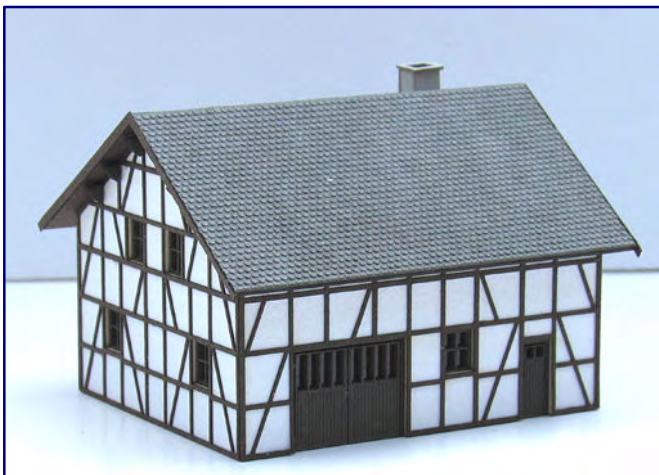
If one of the five purchasers of the first version is disturbed by the light emission, the matter can be dealt with within two minutes using the explanations given here. Otherwise, FR Freudenreich Feinwerktechnik will allow owners to send in their model and have it done by the manufacturer.

There was also an explanation for the tighter mounting of the case on the digital locomotive: The reason for this is the cuff of the sound capsule, which has to lie close to the round roof and is squeezed together for correct function.

With this topic, we are now touching on another concern of the **Trainini®** editorial team. Since it is very difficult to convey the lighting effects and the sound functions with an article, we are currently preparing a short video. As soon as this has been edited and cut, we will make it available for viewing via a link on the **Trainini®** website.

Three new kits at Modellbau Laffont:

"In between," as the manufacturer writes us, there are now three new products from Modellbau Laffont: The half-timbered farmhouse in plastered finish (Art.-No. Z501) as well as with brick facade (Z502) fits perfectly to the farms produced for the 1zu220-Shop by Archistories, and extends them meaningfully, but they also work just as well on their own.



Stephan Laffont offers the new farmhouse both with plastered half-timbered house (Art.-No. Z501; Photo left) and with brick lining (Z502; Photo right). Photo: Modellbau Laffont

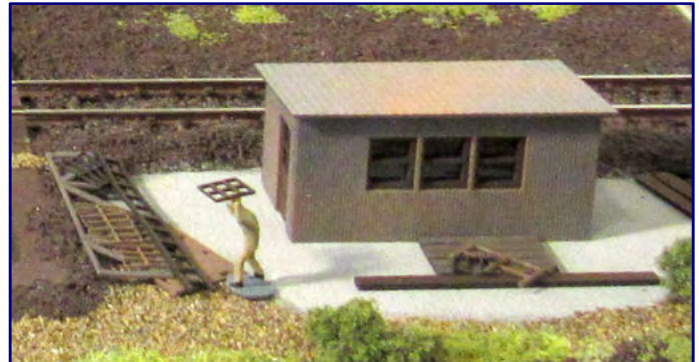
The building dimensions are 51 x 42 x 37 mm (L x W x H). The buildings, made of solid-coloured architectural hardboard, do not require any post-treatment and impress with the finest brick and, above all, roof tile engravings.

The trio is completed by a works hut with scrap yard parts (Z2101). Nine damaged wooden sleepers, one steel ladder and eight steel scrap parts are included in the design of the small building for the railway environment.

All kits are supplied in PE bags with seal; the lasercut kit parts sit in a stable component frame. These kits, as well as the well-known program, can be ordered directly from Stephan Laffont: <https://www.modellbau-laffont.de>.

New models at WDW Full Throttle:

Two deliveries have to be reported to this small series supplier from the United States. For Pennsylvania there are two double packs of the 70-ton self-unloading wagons with outer struts and three chutes (Art. No. FT-5009). Also new are the double packs of the cylindrical bulk wagons "Alberta Heritage Fund" in blue paint (FT-1032).



Intermediate track spaces and other railway areas can be filled with the depot hut including scrap yard parts (Z2101). Photo: Modellbau-Laffont

These models are offered in Germany by Case-Hobbies, among others (<http://www.case-hobbies.de>).

The Märklin deliveries of the last weeks:

June seems to become the Rheingold month. The train package "90 Jahre Rheingold" (Art. No. 81332), which is pulled by a class 184 steam locomotive, was delivered. Compared to the earlier 8133 edition, it differs above all in its bell-shaped armature drive and the missing smoke deflectors. Of course, a different company number was also chosen.

The five enclosed cars are essentially identical, but now have close couplers. This improves the closed appearance of the train.

The five-piece wagon pack "Rheingold" (87269) with the later TEE train is also on the market. It consists



The Rheingold for epoch II (Art.-No. 81332) now appears with close couplers. Locomotive 18 433 has no smoke deflector plates yet, but is equipped with bell-shaped armature motor, detail control, brake imitations and track clearers. Photo: Jörg Erkel / 1zu220-Shop

of two compartment cars Avümh 111, one large-capacity car Apümh 121, the hump-roofed dining car WRümh 131 and the viewing car ADümh 101.

The models now have interiors that look very successful, but seem to stand out from recently produced 3D-printed wagons for similar designs. There is no interior lighting, but this would have made the interior much more visible. The roof window print of the dining car with frame was successful; its DB logo differs in the representation from the earlier model.

On the other hand, we would like to criticize the not convincingly painted, purple-red ribbon of all five cars. It has not been painted around the corners of the car, which means an unprecedented step backwards for the "hump" and reflects a ten-year-old condition for the other four models.

The bright beige car heads, even without offset transition doors and folding panels, disturb the overall image of the train, especially in photographs; only the rubber bulges have been blackened ex-works. The fact that Märklin painted the train cleanly and even printed rubber door seals on the side doors proves the high art of painting and printing in both Göppingen and Győr.



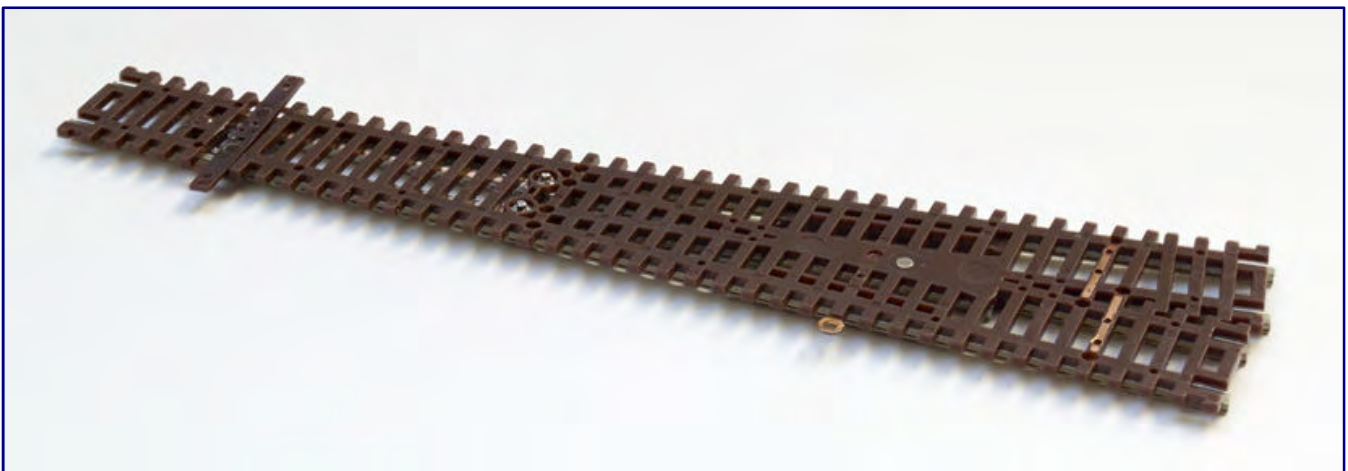
The well reproduced roof windows of the hump roofed dining car from the Rheingold car package (87269; picture above) as well as the visible interior design are convincing. It is only from this perspective that the front sides, which from our point of view were unsuccessful, do not attract attention. The Wascosa gas tank cars (photos below) are certainly an eye-catcher on the plant. They are delivered in two copies with sun protection and one without this feature. Photos: Jörg Erkel / 1zu220-Shop

Also available is the three-part gas tank car package "Wascosa" (82531) with the radiant blue cars of the present. Two of them have a sunshield; the third model with two visible seams on the pressure vessel is used without this protection. The OOtz 43 self-discharging wagon is now fully delivered. (82803).

Atlas switches available again:

The new switches from Atlas Model Railroad (<https://shop.atlasrr.com>) were already delivered in small quantities and complement their flex track in a useful way. Now they are available again, which is why we would like to introduce them here.

A left (Art.-No. 2811), a right (2812) and a crossing (2813) have been produced. All three are equipped with Code-55 profile track made of nickel silver and have the close tie spacing which is common in North America.



The right switch of Atlas Model Railroad (Art.-No. 2812) from top and bottom side. It is delivered without drive and has a polarized frog.

At this point we show pictures of the slim legal switch, which the 1zu220-Shop (<https://www.1zu220-shop.de>) has borrowed from us as a distribution partner for recording purposes. The Atlas turnouts are supplied without drives, but equipped with a control threshold, which has two intervention points, over which it can be operated with a self-selected and installed turnout motor.

So that the position does not change when a train passes over, an integrated wire spring holds the switch blades in their respective position. With 110 mm straight track, the turnout corresponds to the basic length of the Märklin geometry.

A special and for the first time implemented feature, which it also recommends for DCC digital use, is the polarizable core, the polarity of which can be determined by a power contact on the outer rail.

The switch also includes three push-fit rail connectors so that the points can be safely inserted into the track layout.

Reissues of popular cargo inserts:

At Spur Z Ladegut Josephine Küpper interesting loading operations have been relaunched. The modern bulk goods side dump cars (Märklin 82430 / 82431 / 82432) were equipped with grey gravel/ballast in both high (Art.-No. Z-153) and low fills (Z-154).

Also available again are inserts for the three containers of the BT 10 (Märklin 80320 and identical in construction), which further the door-to-door traffic with coal (Z-149). In this connection, we would like to remind you that this type of wagon with similar containers will also be announced in the 2019 new products.

Küpper products can be purchased at <https://spur-z-ladegut.de>.

New 3D printing kits at EtchIT:

Really impressive is the new long timber transporter with cargo based on the Magirus Merkur (Art.-No. XD179_Z). In addition to the recently introduced MAN-based garbage truck, this month the following refuse personnel will be available, consisting of five men with round garbage cans (XD015wrk_Z).



Impressive in appearance is the long timber transporter based on a tractor Magirus-Deutz Merkur (Art.-No. XD179_Z). Photo: EtchIT

All kits are manufactured again in high resolution using the 3D printing process. These new products can be found and purchased directly under <http://www.easy01.de/etchIThome/index.htm>.

Current deliveries of Micro-Trains:

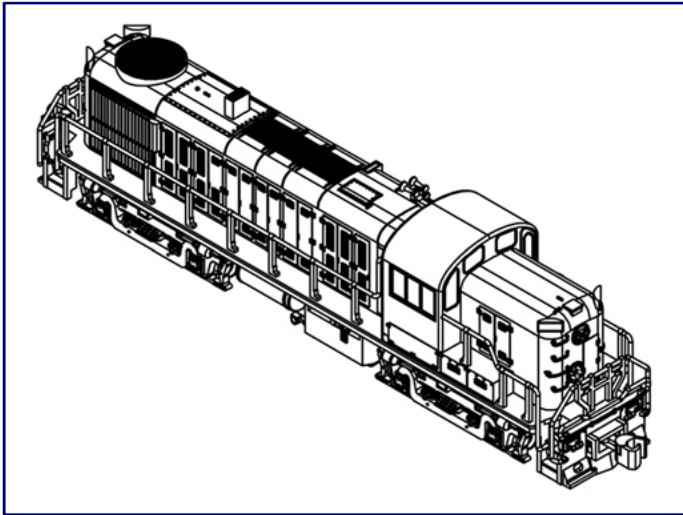
For Rock Island there are now two cattle wagons on the dealer shelves (Art. No. 520 00 251 / 252), which versions were in use by this railway company in the thirties of the last century with about 3,000 wagons.

The farm-to-table series is now served by wagon number 5. It is a covered 40-foot wagon with wooden, light blue lacquered side walls and advertising of the Wisconsin Cannery Assoc. (518 00 750).

Micro-Trains products can be purchased in Germany from, among others Case-Hobbies (<http://www.case-hobbies.de>).

News and updates from American Z Line:

AZL announced with the two diesel locomotives RS-3 and RSD-5 models after models of a design of ALCO, which was built between 1950 and 1956. While the RS-3 is the four-axle basic version of the typical US diesel locomotive for combined shunting and mainline service (RS = "Road Switcher"), the RSD-5 is the six-axle version of this model.



American Z Line announces the US diesel locomotives RS-3 (shown here) and RSD-5, as new models. Illustration: AZL

Their basic form with an almost centrally mounted driver's cab was able to assert itself for this purpose, as around 1,700 of these vehicles were built in the six years. The last of them remained in service until the eighties.

American Z Line would like to score with its announced models with many unique details. These, of course, are primarily aimed at the four-axle AAR Type B bogies and the three-axle Trimount models as the main distinguishing features.

The exhaust openings, turbochargers, headlights, number plates, air horns, snow ploughs and fuel tanks are all designed specifically for the selected railway companies.

Among the current new releases we find the light passenger coaches of seven different shapes for the former "Empire Builder" of the Great Northern Railroad, the model of which was used from 1929 between Chicago and Seattle / Portland (Oregon), particularly worth mentioning.

Among the cars transferred to the model there are also those provided by CB&Q and SP&S Railway in the same colours.

You can choose from the following cars: sleeping car 6-6-4 (art. no. 73115-1 to -4), viewing car (73415-1 to -3), dining car (73515-1 / -2), combined mail and luggage transport car (73615-1 / -2), seat car (73715-0 to -2), pulpit car (73815-1 / -2) and railway mail car (73915-1 / -2). Suitable A/B units of the EMD F3 diesel locomotive are already included in the AZL range.



Among the novelties now delivered are the cars for the "Empire Builder", here in the picture one of the observation cars (Art.-No. 73415-1). Photo: AZL / Ztrack

The 40-foot long, covered AAR cars are delivered in three combinations with different operating numbers for the Canadian National: single wagons (904303-1), two wagons (904373-1) and four wagons (914303-1).

DODX's 89-foot flat wagons will also be delivered again. They are all loaded with two M109 tanks and one M992 in sand colour from the production of Z tanks. The cars themselves are either brown (911023-1S / -2S) or yellow (911024-1S / -2S).

You will find further manufacturer photos of the current deliveries under <http://www.americanzline.com>.

Herpa new products for early autumn:

The following new products from the Herpa Wings and Snapfit series are already scheduled for delivery in the months of September and October 2019 on a scale close to Z gauge 1:200.

The mouse from WDR television can now also be seen on an Airbus A321 Lufthansa aircraft with the name "Flensburg" in the new company colours (Art. No. 559959; Snapfit 612432). The Airbus A320 "Sindelfingen" promotes the idea of the European Union with the English saying "Say Yes to Europe" (559997).

At the subsidiary Eurowings, on the other hand, the yellow-painted sister aircraft "Hertz 100 Years" (559904; Snapfit 612449) is on the road, promoting the car rental company's anniversary. The Airbus A220-300 "Fête des Vignerons" (559935) by Swiss, which also belongs to the Lufthansa Group, is artistically sophisticated.

Further models suitable for use in layouts are:

Yakovlev Yak-40 of the Czech Air Force (559898),
Air Berlin USA Boeing 707-320 (559911),
Air Lingus Avro RJ85 in the new colours of 2019 (559928),
Sabena Boeing 737-200 (559942) and the
Helvetic Fokker 100 (559966).



The Boeing 707-320 from Air Berlin USA (Art.-No. 559911) has a completely different look than we were used to from the later German airline. Photo: Herpa

The combat aircraft Panavia Tornado GR.4 appears in three special designs of the British Air Force (570503 / 570510 / 557527). Let's finish the new list of a further Snapfit model of the Airbus A320neo in the colours of Air Malta (612418).

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High summer on the field railway

Fish Belly Bridge Plettenberg
A new advertising locomotive is created

Introduction

Dear Readers,

outdoors the sun is shining, the temperatures are approaching 30° C again - right, it's finally summer. And it is well known that exactly this time of year should be used for leisure time. But is that also true for model railroading?

The model railroad in the cool basement would be an option in hot weather, but is handicrafts fun at all and do people really like to think of models and landscaping during their holidays?

We are absolutely sure of this; so sure that we are referring thematically in this edition especially to the summer.

We build a bridge between summer and model railway, so to speak, and on this occasion we take a look at Märklin's new fish-belly bridge. It's a great kit that doesn't even stop us from sunbathing that long and it's really fun.

Our diorama series "Four Seasons" has been waiting to be continued since its beginning in 2009. Finally it is time, it continues with the... Right guess! We have planned the summer.

Why it took so long and which details can be worked out, if we have the real summer in all its colours and special features "at hand", we would like to reveal today and encourage you to visit the hobby corner yourself.

We witnessed full of excitement how a newly painted locomotive of the 101 series becomes an advertising locomotive on a scale of 1:1. Unbelievable precision, but also improvisation and changes from the original plan during the foiling, all this is a sweat-inducing and very time-consuming work. Only we have pictures and our own impressions, which will certainly give a completely new face to the appreciation of such a work.

But summer time is also time for excursions. So why not visit a museum railway or even a mine railway? In our literature tips we can give you some suggestions. We particularly recommend a visit to the DB Museum in Nuremberg in the near future. It is certainly worth a visit all year round, but the special exhibition "Beer, Railway and Bananas" is only open until 31 October 2019.

Also the manufacturers do not take a summer break, we can announce a few news today again. We especially liked the fire brigades "Kallental" and "Dörpede". The matching fire engine Magirus-Deutz LF can actually be seen on our summer diorama.

Thus the circle closes, the sun shines and we model railroaders enjoy it. Whether you read **Trainini®** on the beach or on the porch or even put the suggestions and tips into action - we wish you lots of fun!

Sin-Z-erely,

Joachim Ritter



Joachim Ritter
Editor

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We thank Modellbau Laffont for their support in the run-up, DB Regio for the film permission as well as the Eisenbahnstiftung and Klaus Eckert for photos.

Date of publication of the German language version of this issue: 21 July 2019

Cover photo:

The dry and hot summer makes it necessary for the Dörpede volunteer fire brigade to provide fire guards at the forest edges of the western Sauerland. A field train of the Rhene-Diemeltbahn crosses the journey of the fire engine.

New Fish Belly Bridge from Märklin With a curve over the Lennesand

The penultimate turn of the century marked the beginning of the age of steel truss bridges. One of its variants was the Fischbauchbrücke (fish belly bridge with the arch pointing downwards). Märklin's new product, based on such a model, already radiated some local colour for us and demanded a test construction including for its appropriate use. Shortly before delivery we may now share our impressions with you.



601 017-7 is on its way on August 12, 1984 as special train Et 28190 on the Lenne bridge near Plettenberg-Ohle. The listed bridge over the river and the floodplains, which is Märklin's model for the kit to be discussed, was part of an arc of more than 180°, which was described by the railway line Plettenberg – Herscheid (closed in 1969) in the branch off from the Ruhr-Sieg line. Photo: Wolfgang Bügel, Eisenbahnstiftung

Soon the time has come: Märklin's Fish Belly Bridge (Art. No. 89758) will be delivered at the end of this month. Currently, it is in production for delivery to Göppingen. We were allowed to take a look at the kit in advance and test it.

We were once again enthusiastic about the precision fit of the hardboard architecture kit from Modellbau Laffont for Märklin. Anyone who has familiarized themselves with the technology and assembly of such kits would not want to do without these kits on their system.

In contrast to the polystyrene construction kits that have been available for decades and are no longer supplemented by new products from many suppliers, viewers cannot get enough of the museum-quality architectural cardboard kits.

This is due to the matt surfaces, which appear realistic and do not necessarily require colour treatment, and also to the fact that their production method permits smaller quantities. This leads to one-off and smaller runs, which are constantly being updated with follow-up innovations. So it is no question for us that model railway systems gain individuality and new expressions with this modern technology.

And so we approached the new Fish Belly Bridge from the spring novelties 2019 with expectation, joy and excitement. Its prototype comes from Plettenberg-Ohle in the Sauerland region and is less than an hour's drive from the editorial office. From 1914 to 1969, the Plettenberg - Herscheid route crossed the Lenne River on a total of eight steel truss segments at low heights and with large arches.

In the model, the bridge is of course straight so as not to restrict the buyer from the outset. Its pillars are also higher than those of the original, but here there are optional adaptations for the most fastidious modeller.

In contrast to many models, the supports of bridges in the course of the arch itself are not curved, but straight. The arch is created by stringing together several segments in the curve.



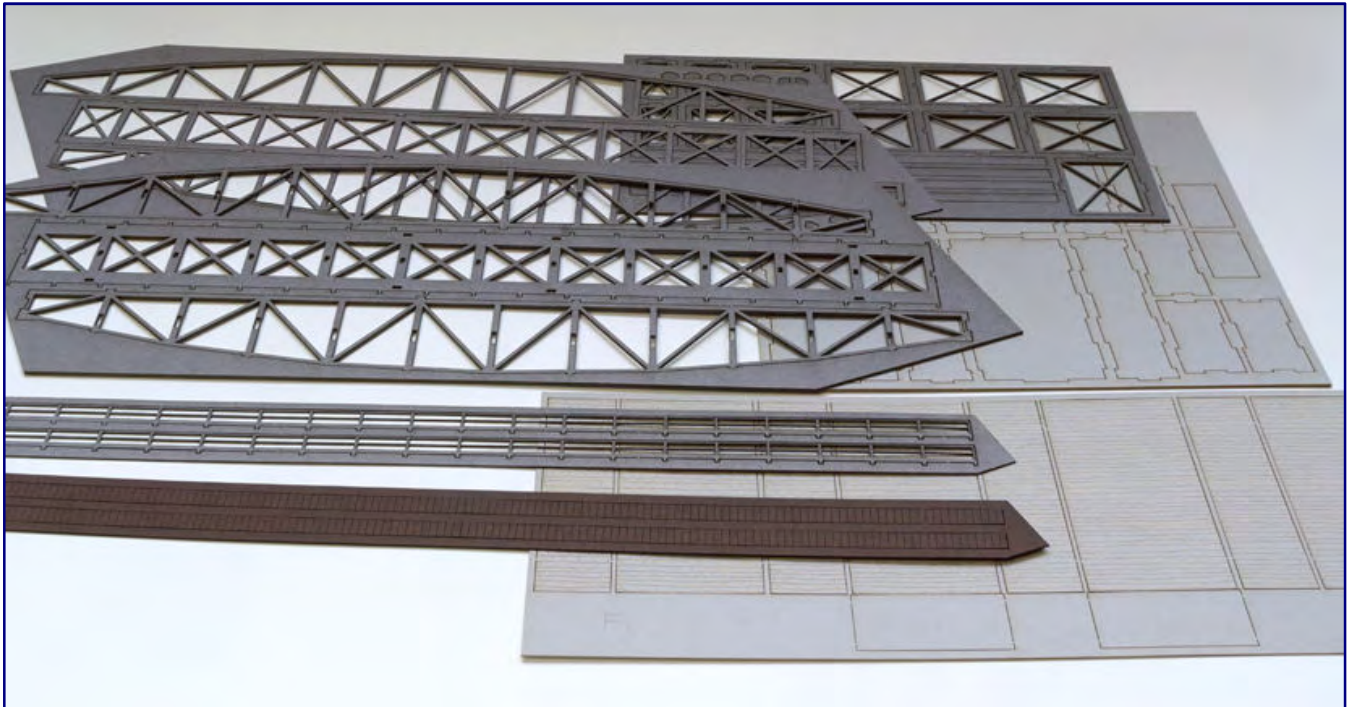
When 601 017-7 and 601 004-5 crossed the Lenne on October 7, 1984, it was easy to see how a steel truss bridge was built into the curve. The special train Et 28269 is on its way back from Hüinghausen to Plettenberg, a train station that Faller has exemplarily converted into a model and that fits the theme. Photo: Wolfgang Bügel, Eisenbahnstiftung

Either way, we would like to leave behind the advice not to purchase the beautiful bridge as an individual kit. It is exemplary and impressive, especially because of the stringing together of several kits.

There should be at least two, three is better. If you have enough space, you can of course build the original model exactly with eight bridge sections. It becomes an eye-catcher in any case when a short branch line train can cross the bridge in its full length and find enough length for the entire train on it.

Construction begins

With these preliminary considerations, we get straight to work. A brief review of the instructions to familiarise yourself with the kit is just as much a part of the preparation as the completeness check of the kit parts. 63 parts from seven sheets in three basic colours (light and medium grey, brown) need to be assembled.



Seven arches with a total of 63 parts await professional assembly. Since the degree of difficulty remains manageable, the Fish Belly Bridge (Art. No. 89758) is also suitable for beginners. A lot of fun is guaranteed.

To ensure that this works well and cleanly, we use the multi-purpose glue “Faller Expert Laser” for all fine and tiny gluing areas and the express wood glue Bindan-RS for all surfaces.

We prepare a fresh, sharp craft knife as well as tweezers. We classify the degree of difficulty of the kit as manageable.

So, even beginners can gain important experience here without having to fear a substantial financial loss.

Peace and patience during assembly should be a matter of course, because that is simply part of model building! Two to three hours be planned for the complete assembly.



Only a few tools and glues are required for assembly. What is still missing is a toothpick that serves well as a positioning aid.

The procedures correspond precisely with the instructions: The working direction is always from the inside to the outside, starting with the piers, the cores of which are the first in line. Also with the fish belly girder, it is the massive parts of the bridge support and outer arches with cross struts that are built first.



We build the bridge in the order and following to the instructions given in the enclosed instructions. We began with step A, which described the construction of the pillar body.

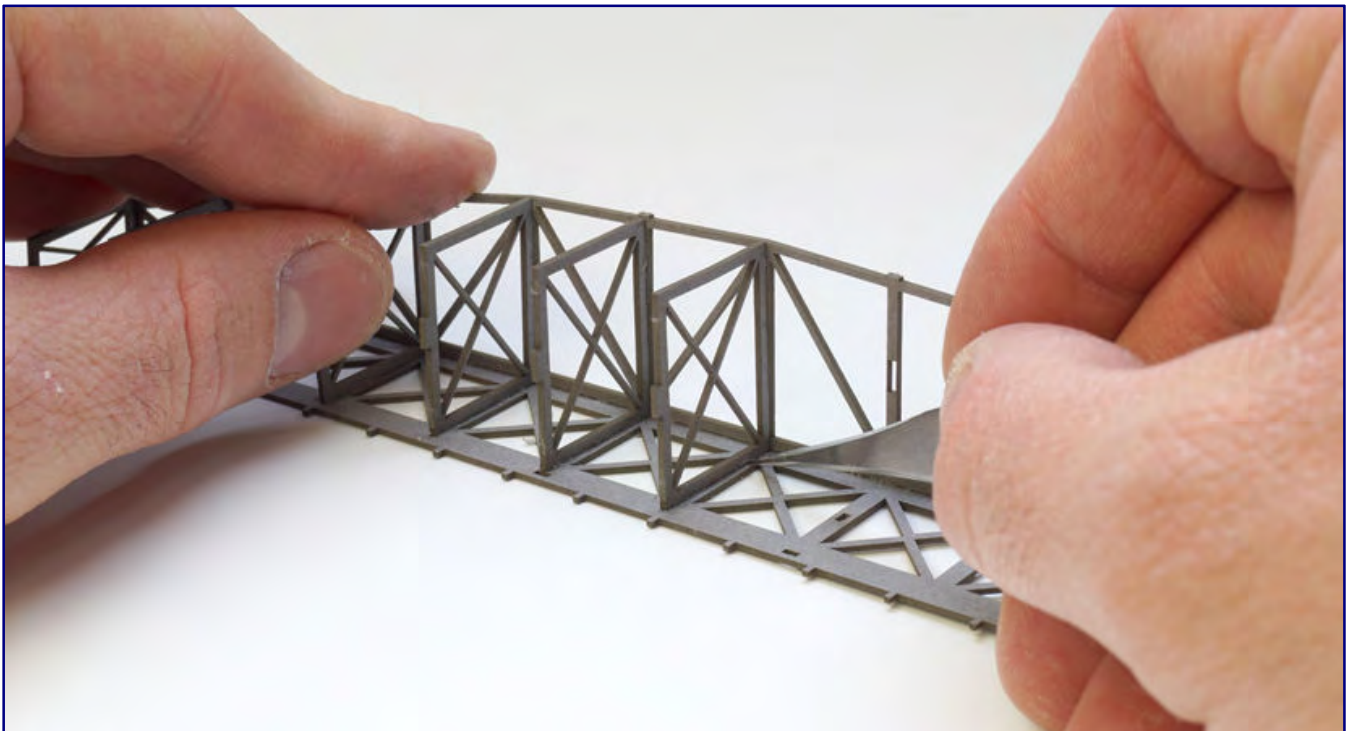
This step proves to be the most demanding. Here, no components may be interchanged, therefore we refer to a helpful Laffont principle: Identical elements always have the same part numbers, even if they are present several times in the kit.

If the designations differ from each other, the associated parts are also different, even if the differences are not immediately obvious. It is, therefore, mandatory to take a close look and identify the parts and their locations.

This is important in the specific case for the individual steel truss elements below the bridge deck. They are to be glued one after the other in accordance with the prescribed arrangement transverse to the direction of travel as soon as one of the two fish bellies is inserted and glued. The second is placed at the end of this section.

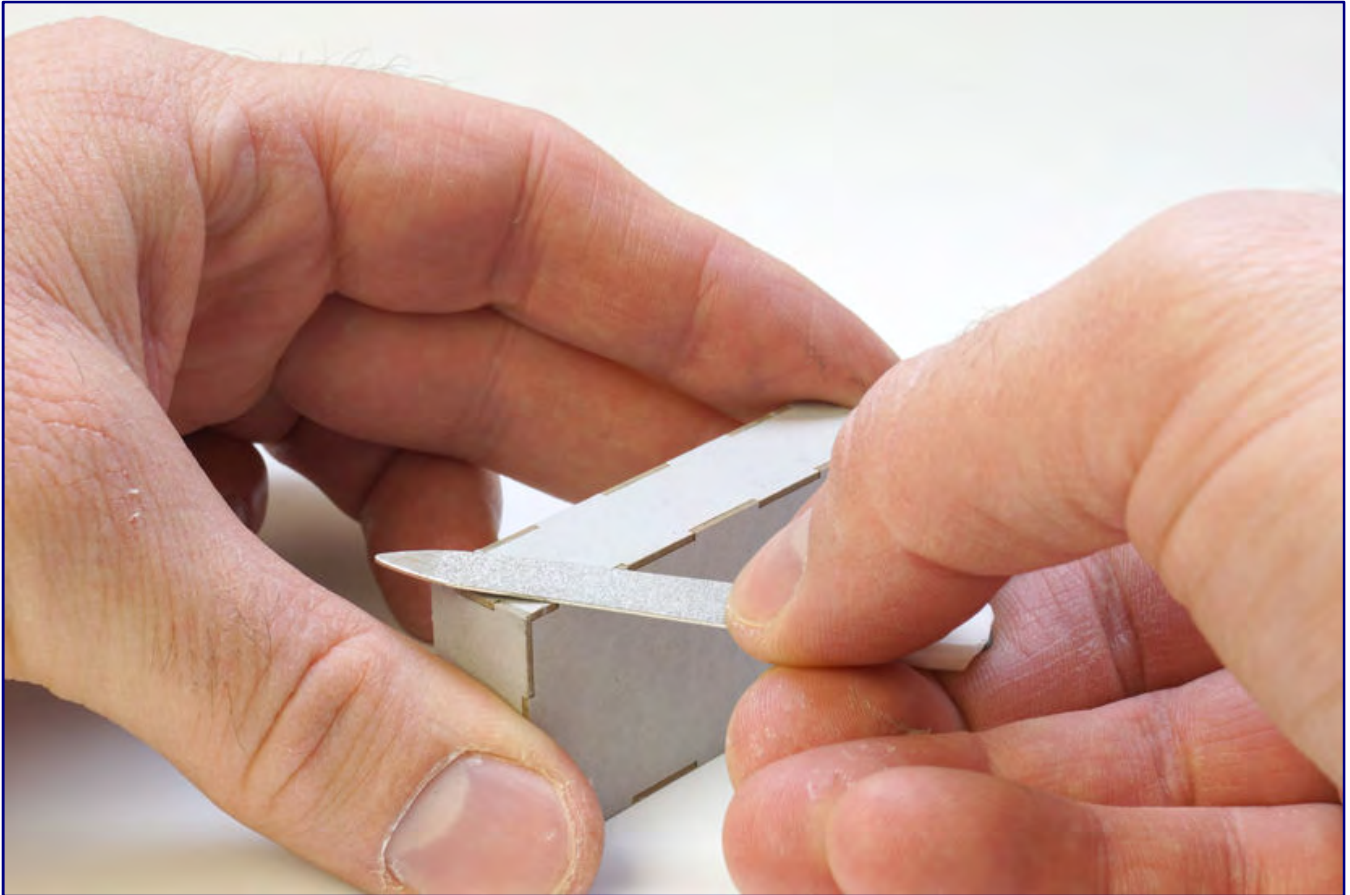
Clean working is just as important. If some glue swells out at the edges, it is carefully removed with a toothpick or a pointed tool, which was made from a defective pair of tweezers, as soon as the glue overflow appears. We even grind the edges of the pillar cores a little to ensure an absolutely flat surface.

continues on page 9



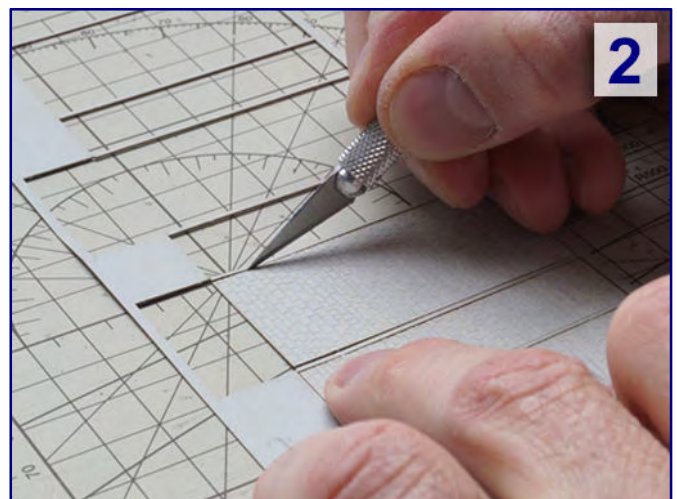
After gluing the first fish belly girder under the roadway, the cross beams follow. It is important that they are not inserted in a reversed position (photo above). If some glue emerges from gaps, we wait until it has tightened a little and then remove it with a pointed gluing tool from a broken pair of tweezers (photo below).

We continue with the masonry facings and abutments on the upper side. First the cover cardboard of the long sides is attached, and only then do the two front sides follow. The pier construction is completed with the thin cover plate on the upper side.



Before we apply the masonry veneers, we carefully grind off (smooth) all the teeth of the already glued parts. In this manner we prevent unevenness of material or glued areas, which could later affect appearance.

A part made of stronger cardboard is glued to it, which holds the bridge head in position as a bearing. A centring aid is not marked, so it makes sense to do a fit test without glue to familiarize you with the correct distances of all four sides before final positioning.



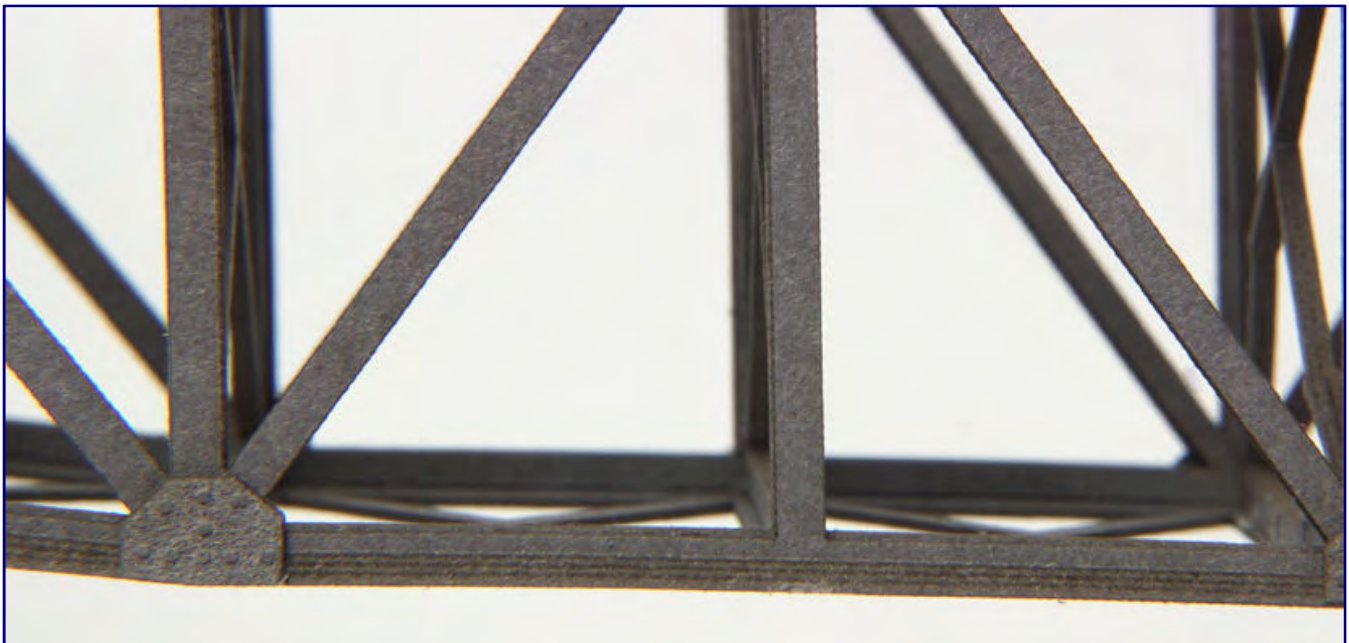


Covering the bridge abutments in four steps (page 9 bottom / page 10 top):

Do not apply glue to the surface of the cover carton. Small glue dots in the corners, at the edges and on parts of the surfaces are sufficient (Fig. 1). The masonry facings need to be carefully cut out of their thin arch (Fig. 2). The longitudinal sides are glued first and then the narrower ends (Fig. 3). At the end the base plates follow on the upper side. The supports for the bridgehead are first subjected to a dry test for correct alignment (Fig. 4).

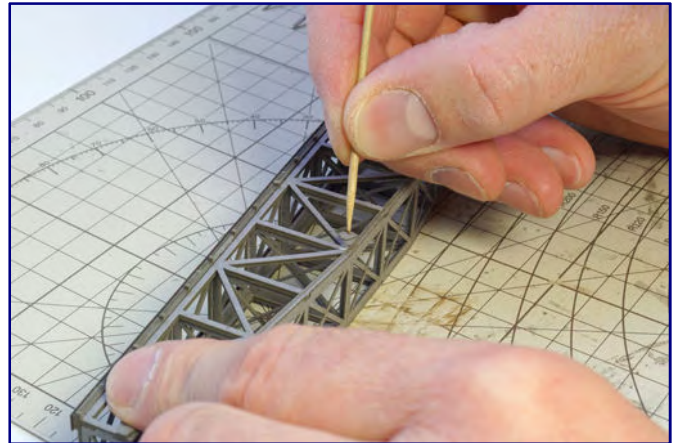
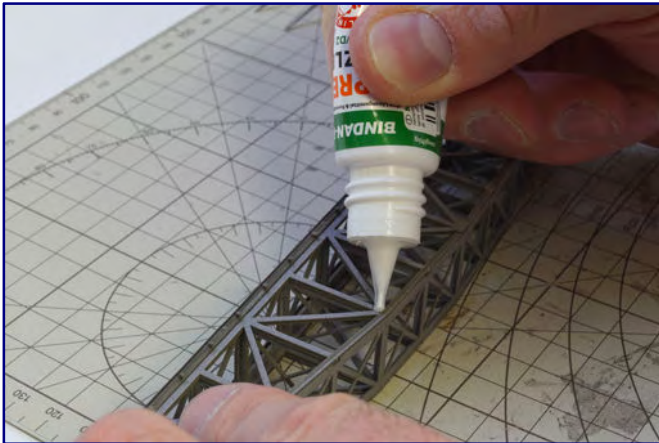
At the bridge itself, the “outer skin” of the lower portion in the curve now awaits. The gusset plates, which clearly mark this bridge as a riveted work from the turn of the century, are then glued in place.

It is important to pay close attention to the illustrations of the individual shapes and their part numbers so that each part finds its correct place and position. Thanks to detailed instructions and clear marking, this is not a problem as long as we remain attentive and do not act too hastily.



Besides clean cuts and engravings, the gusset plates, which must not be swapped during application, also play a major role in the overall appearance.

Of course, all components are separated with a hobby knife mentioned at the beginning and with practiced cuts. Precision and skilful cuts are important so that there are no tear outs.



Gluing on the gusset plates is a fine job: a tiny drop of glue is all that is needed, which can be dispensed precisely to size and point thanks to the dosage cannula (left photo). The toothpick helps to position the tiny cardboard part (right photo).

The tweezers prove to be helpful, because the small parts, in particular, can hardly be gripped and positioned precisely any other way.

A toothpick tip helps to set fine adhesive dots without staining, unless the Faller adhesive is used in such places. The toothpick is also indispensable as a positioning aid.

The construction of the bridge progresses quickly, because there are only five working steps to the finished model. The fact that they have been divided up in a meaningful way, and presented graphically makes it easier for us.

Finally, running boards and railings are on the agenda, after which the first bridge is already waiting for installation on the system. Repeating this work several times is not a problem and does not lead to boredom. The impact of the extended Fish Belly Bridge compensates for everything in the end, anyway.



After the railing has been put in place and the running boards have been attached, the Fish Belly Bridge is ready. It looks good, but its impact is increased considerably when several elements are added together.



The bridge looks so beautiful when it is set in scene in the light of the setting summer sun: the longer, the better. And with several construction kits, it can also be used to create outstanding paradise stretches.

The new bridge can be used from epoch I to the present day and offers a wide range of options. In addition to the impressions described above, this is another reason to nominate this model for the best new releases of 2019 in the category accessories.

.....
: **The website of the supplier:**
: <https://www.maerklin.de>
:
: **The website of the manufacturer:**
: <https://www.modellbau-laffont.de>
:.....

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Märklin's new advertising locomotive 101 064-4

Well dressed

Do you remember “150 Years of Märklin”? That was in 2009 and an elegant black class 120 ellok promoted that anniversary. Since the company had to file for bankruptcy a few days after the International Toy Fair, it also became known as the “Trauerlok” (funeral locomotive). That was ten years ago, Märklin is celebrating its next anniversary and is doing so much more colourfully with a new locomotive.

It's June 29, 2019, 6:00 in the morning. In the Dortmund plant of DB-Regio is now at the time of shift change. But it is no ordinary Saturday morning. In the halls, the workers who worked the early shift see a long-distance locomotive: 101 064-4 is standing here, washed and thoroughly cleaned and freshly painted. It wasn't until April, that it was inspected and repainted in Dessau.

Therefore it was chosen to put on a “fresh robe” and to be allowed to advertise Märklin's 160th company anniversary. So the youngest of all advertising locomotives currently in the Deutsche Bahn AG fleet is before us. It thus continues a tradition that began more than twenty years ago with a Christmas locomotive.



In the best summer weather, 101 064-4 with its advertising design “Märklin verbindet Generationen” (Märklin connects generations) rolls out of the engine shed early in the morning of 30 June 2019. Before she returns to regular service, the Märklin documentary will photograph her and record every detail for posterity and compare it with colour cards. Photo: Klaus Eckert

If the machine could think, it would probably be most curious about how it would look after the transformation. But at the moment only the two experts who brought plans and foils for their design, and are diligently arranging and studying them know this.



The plans for the two different sides of the new Märklin locomotive are available throughout the entire gluing process. They help if individual slides cannot be assigned or aligned at the first attempt. Detail on the edge: No company number has yet been noted on the graphic representations.

But soon the work begins. The first rolls are pulled out and sighted, measured and tested. A work that has been followed with anticipation takes its turn, because we witness how an ordinary red locomotive becomes a moving work of art step by step.



The first films are sifted and rolled out. The gluing will start soon.

But until then, there will be a few more hours in the country. At 14:00 hrs the DB Regio shift ends, the later appearance can already be guessed.

But 101 064-4 is by no means finished, the process of applying the film continues into the evening hours.

However, the end of the work is quite precisely specified: On Sunday, at 9:00 a.m., a Märklin Documentation employee will arrive.

His task will be to photograph the locomotive from all sides, record details and compare the colours used with the colour cards and fans he has brought with him.



Work on the side walls is progressing rapidly. The first long side with the metal construction kit motif is already finished, but it gets tricky when the sloping front and roof areas are reached.

This is the only way to ensure that everything is recorded for posterity and that a coherent model can be produced at any time. Even if no models have been announced yet, the big locomotive will certainly not remain alone for long. Why else would Märklin invest in a moving advertising space?

But this Saturday there is still nothing to see or suspect from all this. While the maintenance staff are busy maintaining or repairing locomotives and railcars, work on 101 064-4 is progressing quietly. Their metamorphosis is accompanied by the camera of our editor-in-chief and the camera of Märklin-TV.

Interested rail fans and Märklin model railroaders can also participate in this process. If they can enjoy the balanced picture of the finished advertising locomotive, this will certainly also be good for the sales of the models, which Märklin will soon release.

But we are starting to dream. Back on the ground, we have to follow their development closely and capture the most important steps. We are happy to receive any explanations on the individual work that we repeatedly receive.

What looks so simple at first glance is a long and demanding job that requires a lot of skill. Our two experts, whom we are allowed to observe, can be seen in their routine.

Nevertheless, there are situations in which things get tricky and not everything wants to fit properly right away. These situations require prudent thinking and an adjustment of the plans in order to track down the devil in the detail.

continues on page 18





Picture above and page 17:

As soon as the first foils are attached to the locomotive after fixed orientation points, all further foils will be applied step by step. They are aligned and only temporarily glued to individual points because critical points such as the gutters on the driver's cab side window still require reworking (top photo). Further to the right, the mounting of the access rail has already been cut out (photo page 17) and the foil is now spread out with the plastic spatula.



Here, too, it is necessary to cut the base free for one of the access rails.

In addition, the constantly rising temperatures make it increasingly difficult. When the thermometer has jumped well over the 30 degree mark outside, the heat is also noticeable in the throat.

Further challenges are posed by the locomotive itself: The areas to be covered reach almost four metres high up to the fasciae of the roof cladding.

Ladders and mobile working platforms with railings are required to reach them all. Despite this, it is sometimes not possible without stretching by the participants.

In the beginning, everything goes quite fast. From bottom to top, the first side surface is covered with foils in horizontal rows. We call it the metal construction kit side. The opposite side, which follows next, are more difficult to photograph. It shows historical motifs from the Märklin H0 range including its predecessor in the form of the E 03 series.

continues on Page 20

Auftrag: MAERKLIN_LOK_RECHTS_SEITE_TEIL_1B.PRINTING)



Cutting out the surfaces along the gutters and windows is more difficult than it looks (photo above). The problem areas are the slanted parts of the locomotive, which also have an arc to the front. The foils here have to be laid flat and aligned exactly, so combined forces are required (photo below).

Photo page 19:

All foil parts are clearly labelled and numbered. So that no seams appear later, they are printed overlapping, as can be seen here on the V of the diesel locomotive.

The smooth walls make it easy to apply and align the foils. If everything fits, the protective foils on the adhesive surfaces are removed and the motif prints are finally connected to the outer sheet metal of the



Accompanied by cameraman Andreas Stirl for episode 99 of Märklin-TV, the work now continues on the model railway page. The fronts prove to be the biggest time wasters, because nowhere else are so many fitting and cutting jobs necessary, as here.

locomotive. “Dürr cookies” (nickname of the actual DB logo) and company number disappear under the advertising. They are later replaced by separate stickers where necessary.

The process of film gluing that we are allowed to follow here differs from what we have seen and learned a few years before in the context of motor vehicle gluing. There, the films were often applied to a layer of soapy water in order to allow them to “float” and still move and align them.

This is then comparable with the application of wet decals in model making. However, the large areas of a locomotive obviously require a different procedure. Here, the protective foils are initially only removed at selected corners in order to allow them to adhere to the vehicle body. They are not completely removed until the drawing file is completely and correctly aligned.

Again, the smoothing to the edges with plastic spatulas is identical in order to obtain a seamless and bubble-free film surface. The image shown on the locomotive will end up with many small single images with little overlap. On white edges, text markings facilitate the assignment of all components to the planned overall image.

Surfaces such as windows are simply glued over in this phase and cut out again shortly afterwards along the seals. The cuts must be made immediately on the supports for the driver's cab access rails, as they protrude from the wall and would otherwise cause distortion.

Again and again small bubbles occur which cannot be completely removed despite all efforts and care. They are then punctured with a needle so that the air can escape - problem solved.

Slowly, our two experienced experts work their way up to find out that the locomotive is not equally accessible from all sides. The large, mobile working platform cannot be positioned to allow access to all points on the locomotive.

Since 101 064-4 stands under a hall section without contact wire, otherwise such work would not be possible due to electrical arcs, it cannot be moved by a locomotive driver. So now the men from DB-Regio have to come in.



Four strong men are enough to overcome the rolling resistance and move 84 tons of service weight on the rail. Our protagonist 101 064-4 has to be moved twice, because otherwise the roof areas on all four sides cannot be reached. After all, the mobile working platform still needs to have room.

One of them releases the spring-loaded brake in the engine room and four others gather at the buffer beam to manually move the 80-tonne colossus. This works for freight wagons and passenger coaches, but also for a locomotive? Yes, it works, because the machine slowly starts to move and is then braked again. The gluing continues immediately afterwards.

The corners, slopes and especially the curves become exciting later on. If a two-dimensional film is laid over this structure, reworking cannot be avoided. In order to avoid distortions in the advertising motif, the designers of the motifs do not create pictorial representations in these problem areas.

At the critical ends of the new advertising locomotive, the beige base of the motif with the striking lettering "Märklin verbindet Generationen" (Märklin connects generations) is simply screened out to the basic tone that deviates only slightly from traffic red.

This smooth transition is well balanced and appealing, but also poses challenges for the experts, because the many points must be precisely assigned and aligned according to size and distance. In addition, especially in the upper area of the driver's cab side, where both bevels and curves meet, a lot of reworking would be necessary.



The front sides take shape:

First, the foils of the sloping side surfaces overlap parts of the front. They still have to be cut off at the edges as soon as all gaps have been closed (Fig. 1). The working direction here is from top to bottom. You start with the Märklin lettering (Fig. 2). The area under the front windows makes a lot of work, because cuts for windshield washer nozzles, windscreen wipers, UIC socket and handles are required in this area (Fig. 3). Important operating instructions, such as those on the apron here (Fig. 4), must not be omitted when making the cuts. The final front can be seen on page 24.

It takes a few hours until everything is fitted, and the foils are cut with high precision at the problem areas in such a way that all parts fit together seamlessly. A further challenge is the two front sides, on which the DB emblems and Märklin lettering are finally emblazoned. The absence of the contrast bars next to the logo seems strange. The thin white frame, which normally sets it apart from the red background, has also been omitted.

continues on page 25

märklin

DB

101 064-4



The entire surfaces above the buffer planks are also covered with foils on the fronts. This may come as a surprise, as they were and remain red. But the client certainly doesn't want to risk any colour deviations from the basic colour. It is also possible, however, that water and dirt could infiltrate the edges pointing forward when driving fast in the rain.

Nowhere else on the locomotive are there so many places to cut in and out as in this area: handles, windscreen wiper attachments, washing water nozzles, sockets, driver's cab front windows and lanterns are to be exposed cleanly in order to maintain all functions.



The locomotive is completely foiled and the result can be seen. The design with historical catalogue motifs comes from Märklin's in-house advertising department.

Finally, fine work is carried out on the lower sloped area. There, a panel section extends over the end of the frame on all four sides. They are normally painted traffic red and are therefore also included in the advertising design. The tricky thing is that important operating instructions are also attached with stickers here.

They must therefore also be cut free, which must be done extremely carefully so that it later appears as if these instructions are on the foils of the advertising design. Almost twelve hours have already passed, and we understand the high demands of foiling such a large object as the 101 series.

Even removing film is not as easy as we thought it would be. It is crucial to know from which manufacturer the films used are obtained; here it is 3M, and which physical properties they possess. Their age, i.e. the time elapsed since they were applied, is also decisive for the type of procedure and the time to be estimated.

But we are far from there! At least one year, we hope, will see 101 064-4 in their anniversary design. One of the most beautiful advertising spaces, which ever rolled through Germany, first wants to be produced as a model in different scales, before we want to say goodbye to it again.

The pages of the client:

<https://www.maerklin.de>

Märklin-TV (episode 99) to the creation of this locomotive:

<https://www.youtube.com/watch?v=fxaYdKaI9GY&feature=youtu.be>

The four seasons of Z gauge (part 2)

A sequel with pitfalls

In the November 2009 issue, almost exactly ten years ago, we announced a diorama series “Four Seasons”. It all started with autumn, but the rest has been a long time coming. However, the continuation within the editorial staff was never forgotten, rather new products offered better possibilities, while other topics required a postponement of the series. For last year's EM diorama competition, Part 2 was finally completed, we present it here today, as well.

Summer 2018 was one of the longest and hottest summers ever recorded by meteorologists in Germany. But while some of them enjoyed more than ten weeks of open-air swimming pool weather with temperatures regularly reached via 30°C, others sweated in the brooding heat.

Nature has also given little pleasure this summer of the century: The potato harvest was modest, and many other crops also suffered from the drought. Pests have increased enormously, as we unfortunately had to find out this year. The groundwater level has not fully recovered to this day.



The hot and dry summer of 2018 brought the idea for the second diorama of the Four Seasons series. The theme was the continuing high risk of forest fires as a result of high temperatures and lack of precipitation.

Another permanent topic was the high and ever increasing risk of forest fires. Many people are certainly aware that not only carelessly discarded cigarette butts can cause widespread fires with great damage

to people and nature. Even a carelessly discarded bottle can act as a magnifying glass and trigger a forest fire.

Therefore we had the idea to deal with this topic in connection with the model railway and to focus on our sensitive environment. The theme of the second part of our Four Seasons series should therefore be called "Fireguard" and show a dry summer landscape at the edge of the forest.

The contrast of a red fire engine should draw attention to itself and clarify the imminent danger. The fire engine should be alone and make it clear that it is not on its way to an extinguishing mission. Rather, it is about another of the four tasks of the fire brigades: Protecting (the population from danger).

So far, the question of how the diorama could slip into this year's focus remains open? This is also answered quickly: Originally, a spring diorama was planned for the second part as a continuation of the seasonal series.

However, its construction was always delayed, because this too was to have unmistakable features that would distinguish it from early summer without a doubt and at first glance. The idea was to create a flowering horse chestnut that would be beautiful to look at and that would have all the characteristics mentioned above. A timely publication at the time of the year seemed to us to be an obligation, so that our readers could compare this in nature.



It was clear early on that the beautiful Saller light railway would finally be staged at the summer diorama. This is where the O&K diesel locomotive sets out to cross the level crossing on the country road with its short train of box and tipper wagons.

And so the spring became summer, with it should also change the theme. The course of summer 2018 then allowed the planned performances to mature further. But then the timetable fell by chance.

Dietmar Allekotte appeared with his coastal diorama, which he had built for the EM diorama competition, for the photo shoot. In this context he was allowed to inspect works that were in preparation for the magazine.

And so he found out that the summer diorama also follows the competition guidelines. It remained to be seen whether participation would also be admissible in view of the designed area, which was considerably smaller than allowed. As an answer to a question to the editors, a participant number promptly came back.

This made it clear that the planned report would have to take a back seat because the diorama would initially take part in the competition. After all, its design was no longer allowed to become publicly known in order not to influence the voting behaviour. This finally resulted in the new publication date in July 2019.

Choice of motifs shown

The planned landscape had to be clearly and unequivocally classifiable as summer. As a result, the green should not look too fresh and juicy, and even grasses should, at least, partly look dry.

It turned out to be interesting for our research that fly mushrooms, in contrast to many more inconspicuous representatives of this species, are not typical for autumn, but spread their caps in July. The fact that they are mainly to be seen on the ground under spruce trees also determined the desired vegetation.

As the topic of an extremely dry summer came more into focus, the conifers came further towards us, as they made it much easier to depict groups of trees that suggest a forest edge instead of solitary trees.

Another desired theme was to show something different, instead of a normal gauge railway. In general, the street should play a bigger role here anyway, which further limited the area for railway lines on the small diorama. After all, new car models also want to be shown in a suitable environment!

At no time was it a matter of explicitly aligning the diorama with the competition and giving it a central eye-catcher that would catch the eyes of every visitor. Rather, the idea was to create a photo backdrop that would later be used in daily editorial work.

There are therefore several small scenes that speak for themselves and want to be discovered for themselves. They can be captured and highlighted with the camera or the observer takes



A diorama stands or falls with whether it knows how to convey life. A wanderer scene with Trafofuchs figures (photo above) and the very successful fox of C-M-K (photo below), which invites to a search game, were selected for this.

the time (at exhibitions) to “go off” centimetre by centimetre with his eyes, and that's exactly what wouldn't happen in the competition.

The specifications described were best met by a light railway. For years we have been fascinated by the models offered by Saller, which disappeared from the focus of the Zetties for a long time, but still offer many products with great designs.

The ideal, but very freely implemented model was provided by reports by Jörg Erkel (1zu220-Shop), in whose vicinity the Rhene-Diemeltalbahn once operated and also crossed a country road at least once. Since this light railway has long since been dismantled, impressions could only be gathered from the specialist literature.

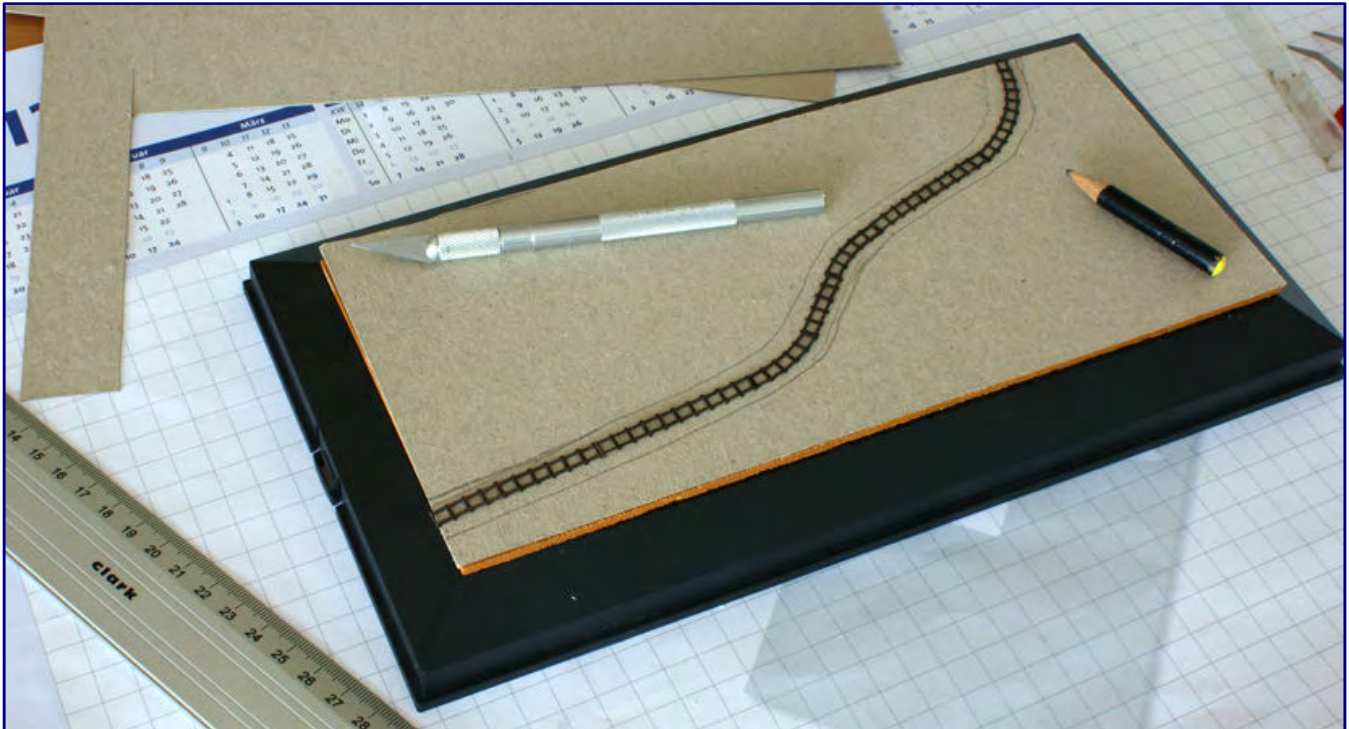
The construction of the summer diorama

The diorama is embedded in a universal collector's showcase made of acrylic glass which we found in the Conrad Electronic program (order no. 211389). Their dimensions of 30 x 15.5 x 11.5 cm had already been determined with the first diorama of the series for all objects of this series. The background of the diorama in it is a plywood board, which was glued to the plastic base with Uhu Max Repair Universal, a powerful power adhesive.



A lacquered plywood board forms the base for the construction of the diorama. It is glued to the plastic base with a special glue from Uhu and fixed there with screw clamps until it is completely dry.

In order to be able to plan the terrain structure better, the following construction of the scenery starts with the country road and the field railway line crossing it. To do this, we first lay a piece of drawing cardboard in the format of the total area shown and draw the planned road and route with a pencil.



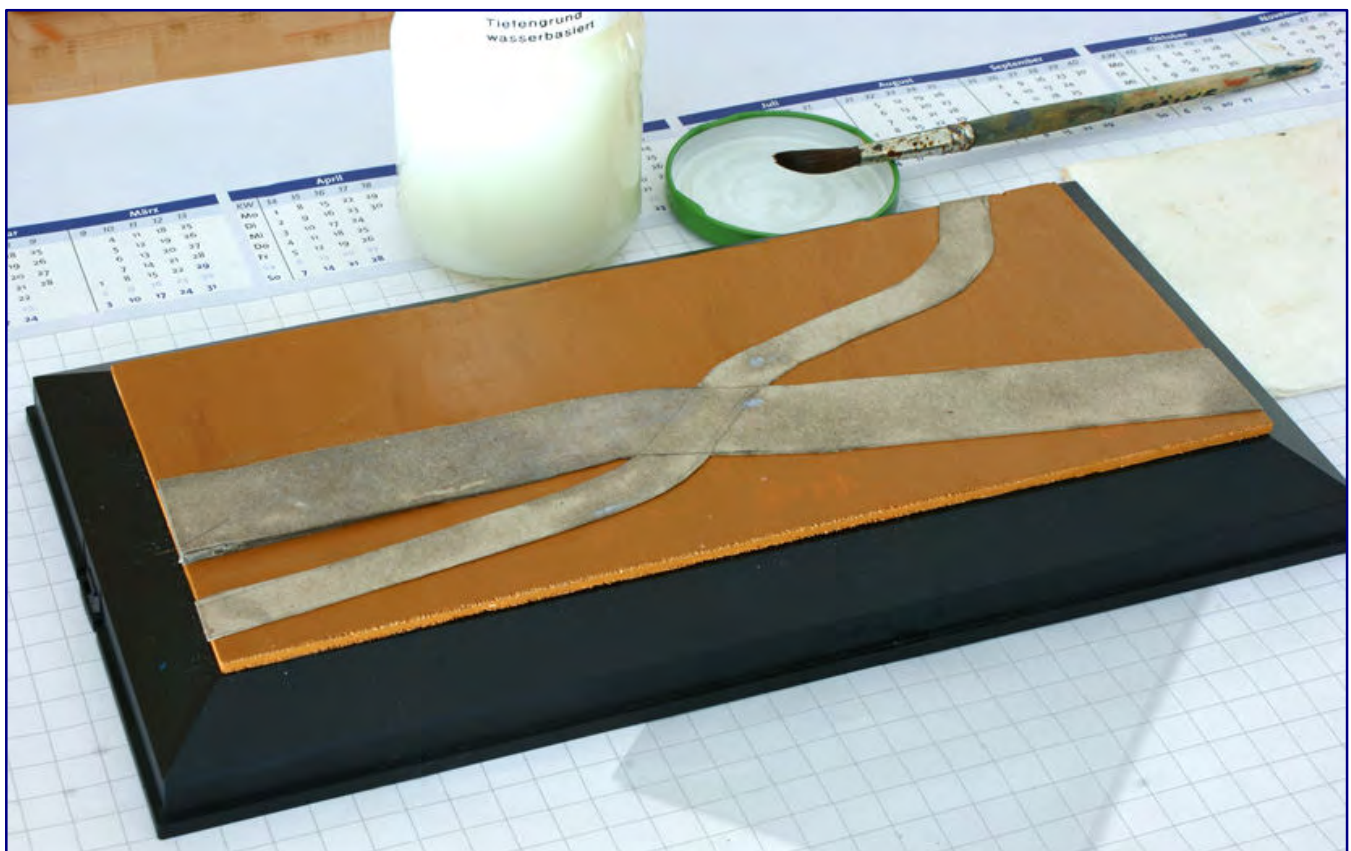
On a piece of drawing cardboard, which is the same size as the base plate, we first draw the line of the light railway (photo above), as soon as we are satisfied with its course. Then follows the route of the country road, with which we can cut out both traffic routes (photo below). It is important for a realistic effect that the routes are not laid parallel to the edges.

First of all, we use various Saller track pieces made of white metal to lay out different routes, and assess their impact on the observer. This requires a certain spatial imagination, because the planned landscape structure should be included, as far as possible.

In order to open the lines of sight for the camera and achieve a clear end to the rear, the scenery will rise slightly to the rear. Also the country road should not be completely level so as not to be boring, and will have a slight gradient to the left.

Therefore, the light railway has to lie there in front of the road in order to be able to photograph the trains unobstructed. The level crossing will be in the middle of the diorama, on the right the narrow-gauge railway can disappear to the back in a terrain incision. A harmonious and credible track layout is quickly found under these conditions and is drawn on the cardboard with the later track width.

We defined the road width as 6 m, which fits well into epoch III, when the vehicles were usually narrower than today, which is why even minor roads had a smaller road width. The dimension converted to our scale is therefore 28 mm. With this width we also record their course as soon as it is certain for us.



The street on the left, which rises slightly, is already lined and glued with further cardboard strips. Now it is time to treat the drawing cardboard with a deep base and make it largely insensitive to further work.

After cutting out the road and track, the rest of the cardboard can be glued to the base plate with Uhu Hart. To prevent it from warping from the water of glue or dispersion paint, due to its rather low quality, we treat it with (water-based) deep primer from the DIY store. The gradient of the roadway has already been created and glued with the underlying parts.

As soon as the primer has dried thoroughly, it was time for the rail parts to be glued on. Before this, however, they receive a paint treatment with the spray gun. The parts are not colour treated ex-works, and therefore still look too new.

We imitate the usual rust tone of outdoor iron and steel parts by fogging them several times with “Sepia Shade” from Vallejo (73200). This glaze still allows the basic colour of the metal used to shine through and is therefore particularly realistic.

After drying, we sand the rail heads free again with fine emery cloth, because here the wheels of the locomotive and the wagon ensure that no rust gets stuck on them. After all, our field railway is still in operation and has rail traffic several times a day.



According to the defined viewing axes, Modur foam pieces are stacked and fixed with contact adhesive and glued together (top photo). Then they are roughly cut to size and processed with emery cloth (photo below). And at the level crossing, the road surface is adjusted to the level of the rail heads. Painting with primer is also necessary here.

After drying, we determine the course of the landscape with various pieces of a foam slab (purées). This material is used in the construction industry and is sold in the usual sizes and quantities for model railway purposes. It can be modelled with knives, saws, files or sandpaper just as easily as Styrodur.



The roughly designed landscape and the ramps of the country road are given a smoothing coat of Molto Fine Spatula. After drying, the sand-like wood repair putty of the same brand provides a realistic surface structure.

Instead of electrostatically charged parts, fine crumbs are left behind, which are to be absorbed promptly before they spread throughout the room. An advantage of Modur over the alternative polystyrene material is its resistance to solvents in adhesives.

For example, we do not have to take into consideration the bonding of the individual parts to the base plate or to each other, nor do we have to take into account the subsequent bonding in holes, such as may be necessary for trees.

But we are not there yet. First we have to close the remaining gaps after levelling the structures. Here the Molto-Feinspachtel is used, which our editorial colleague Dirk Kuhlmann also swears by. It also serves well for smoothing the road area in the area of the asphalted level crossing.

We later create the rough landscape structure with the coarser Molto Wood Repair Filler, which sand structure is ideal for our landscape. Now the time has come for the first test installations of the HOS traffic signs and the new crash barriers.

In particular, the position of the St. Andrew's crosses (railroad crossings) needs to be carefully considered so that they stand sufficiently far from the clearance of the light railway, but can still be seen well and in adequate time by road traffic.

Landscaping

With the already completed steps we now reach the phase of greening. It begins with the application of suitable soil colours. The basic colour design is carried out by a glazed application of chocolate brown tinting colour from the DIY store.

It only dampens the inherent colour of the wood repair putty slightly in the direction of natural-looking tones. The matt acrylic paints in dark brown (61189) and light green (61194), on the other hand, create contrasts.



Now tinting paint and two Noch acrylic paints are used to give the surface an earth-like appearance. We paint the country road with Faller's street paint.

We imitate the road surface by applying the darker Faller road colour (180506), which is better suited to the road depicted here, than the light concrete colour, which is more suitable for contemporary high traffic streets.

For the ballast of the Saller models' field railway tracks, the professional ballast limestone N/Z "beige-brown" from Noch (09161) is used. It is processed in the classic floating way.



The Noch ballast is applied with a thin cardboard dosing aid and pushed to the sleepers with a bristle brush. It is glued wet with a mixture of white glue, water and a drop of detergent. But first it is wetted with the spray bottle so that the tiny stones do not float on contact with the ballast adhesive.

For this purpose, it is first spread dry on and next to the tracks with a dosage aid made from a folded piece of a separating sheet and then pushed straight with a fine bristle brush until it is evenly distributed and has neither gaps nor mountains. It is then wet with a spray bottle.

The water-wood glue mixture with a drop of dishwashing liquid dripped on with a small bottle afterwards can spread better and flow between the fine stones to fix them all equally. This process can be repeated several times if necessary. The only important thing is that the diorama is given sufficient time to dry afterwards.

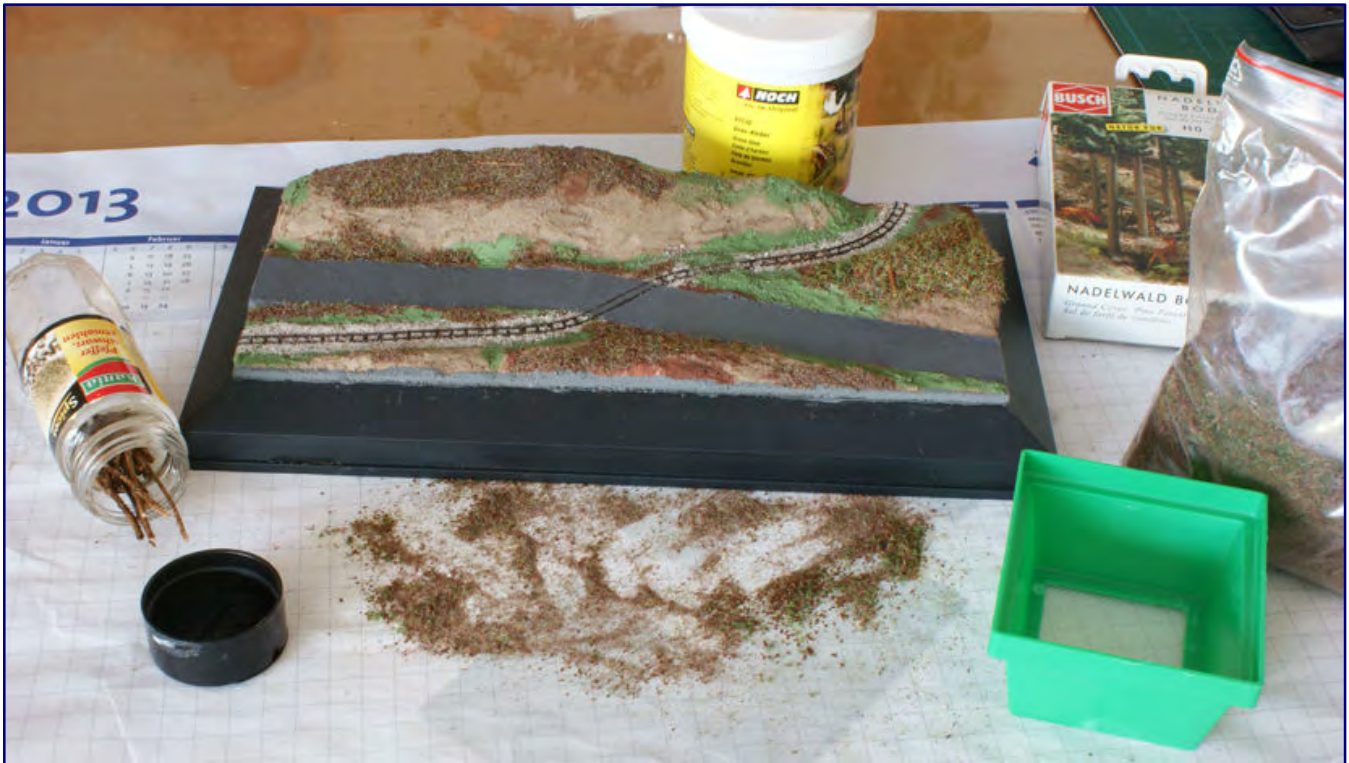
Next, we work on all surfaces on which the spruces are to stand later, as well as a smaller surface in the foreground, which should look particularly puffed by the heat. Here we apply the coniferous forest soil of Busch for the track H0 (7529).

So that it doesn't look too coarse for Z gauge, we sift it out using Minitec screens, which were available at least before the brand was sold to Unique, in the largest mesh size. We sprinkle the forest soil into the grass glue bed (Still 61130) applied with a paintbrush and throw in any short cut sticks.

They come from a willow in our own garden, just like a tree trunk glued to the road. This material was sterilized in the oven at 180° C for 30 minutes, as was the East Frisian arable land (collected and sifted out by the farmers themselves). Only when the high temperature penetrates the entire material for several minutes does it destroy the proteins of living organisms in all conceivable stages.

Meanwhile our diorama with forest soil and soil application looks like fallow land in all other places. We are now beginning to change that. Even during extreme drought, there are always plants that look well soaked. They are often flat groundcovers that live in the shade of larger plants or thrive on a vein of water. We show them selectively with the medium green flockage of Noch (07242), which is scattered on the grass glue from the same firm.

Included in the wild growth, we also have individual parts of the field railway track bed, including a somewhat larger, continuous area near the level crossing. A few stalks should also sprout here. In order to keep the overgrown track gaps credible in relation to the low rail height, we use only 0.6 mm short grass fibres "Green 7" from Linea Secondaria (2A-7) from Italy.



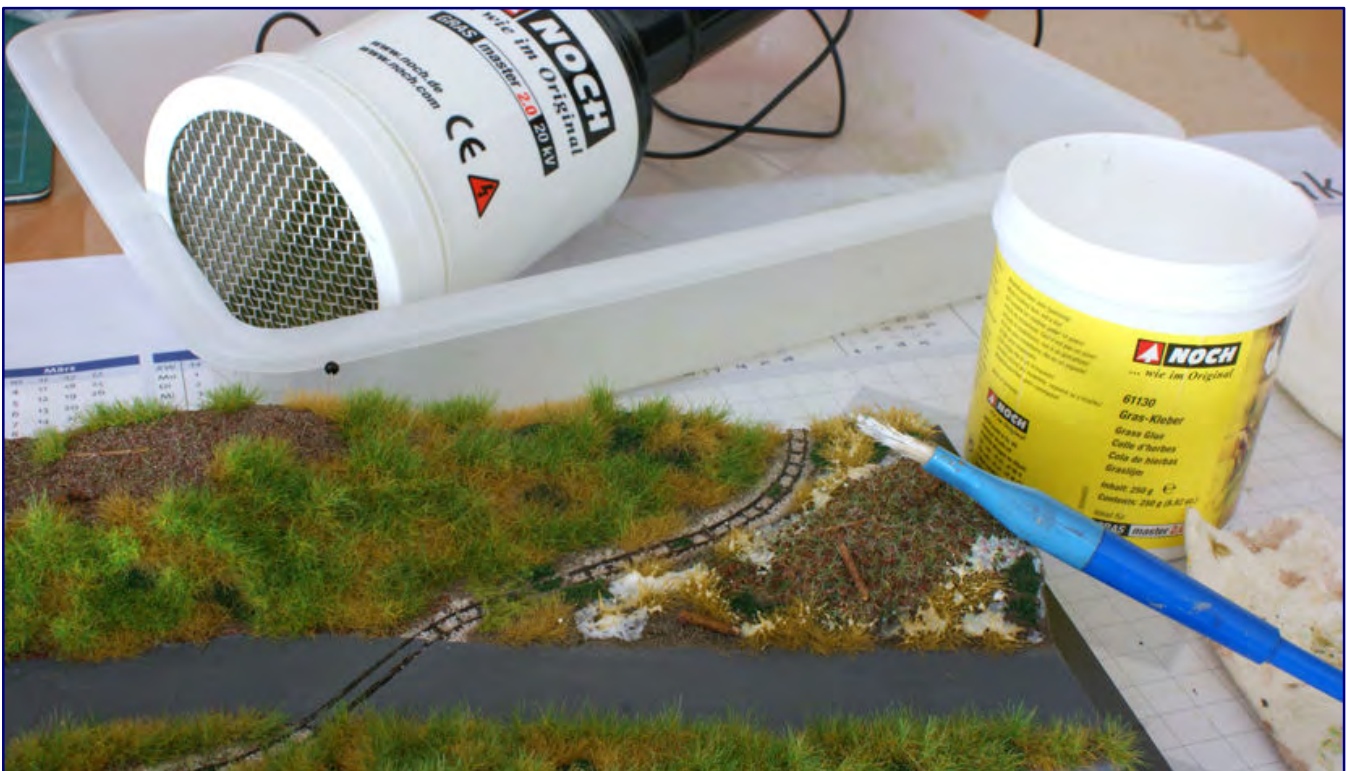
The places where spruces are to be planted later are sprinkled with coniferous forest soil from Busch. Since it is intended for nominal size H0, we sift it with stackable inserts from Minitec. We randomly throw a few withered and dried pieces of willow branches onto the glue cover of the forest floor.

The subsequent landscaping of all other surfaces is carried out according to the "Kuhlmann method": the grass adhesive is not applied to the surface, but only dabbing, which in places always leaves a bare surface behind and is the only way to ensure that the meadow surfaces do not appear monotonous in terms of density and height.

The further procedure corresponds to the double grassing already described several times in this magazine, i.e., fibres of different lengths and colours are applied in separate passages and with dry pauses.

The first pass is made with 5 mm long, very dry field grass fibres in ochre brown from Noch (07086). The intermediate result, which you can see in our pictures, should not frighten you, because we are far from the end of the greening phase. The steppe landscape will only be of short duration.

Once it has dried, it continues with a summery green mixture. We quickly said goodbye to the much fresher and more realistic spring meadow grass (Noch 08300), because it did not harmonize with the field grass and the desired dry impression.



The grass fibres are shot in two steps with the Noch-Grasmaster 2.0. We start with the dry field grass (photo above), after drying the summer grass mixture follows (photo below). The grass glue is applied in accordance with the “Kuhlmann method”, not over a large area, but dabbing.

Noch's master grass mixture Sommerwiese (summer meadow) (07072) with a fibre length of 2.5 to 6 mm, which was shot into the areas already partially covered with grass, but not limited to small areas, fit much better here.

Otherwise, we tend to reject such colour mixtures because they contain too much yellow for our taste, which we prefer to apply separately and self-dosed, and above all red stalks which have nothing in common with reality.

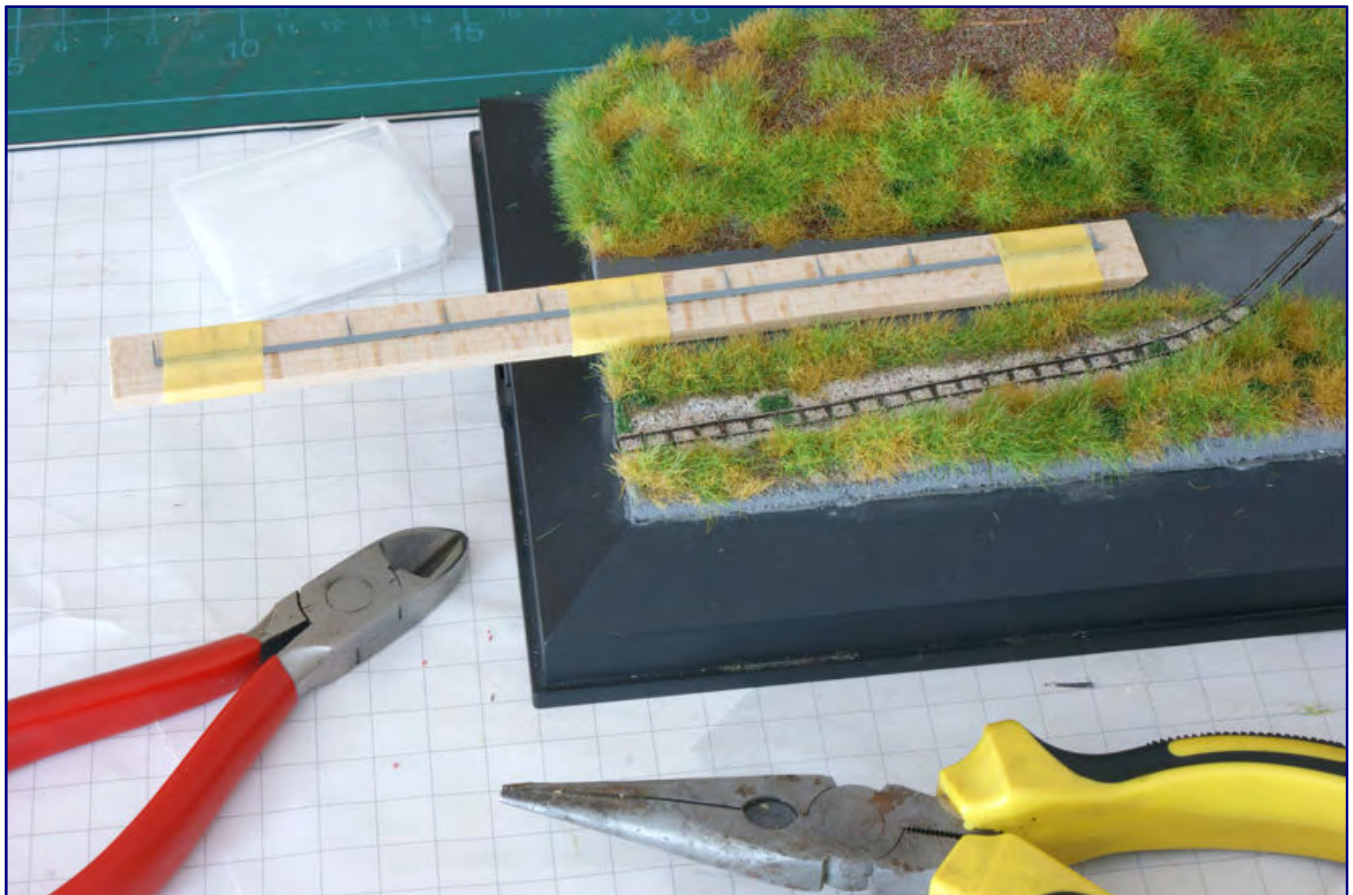
Strangely enough, it is precisely the grass mixtures that are most frequently bought by model railroaders. Perhaps we can help to change this behaviour for more realism?

Final fine sanding

Landscape structure and low vegetation are in place, with which we can now go to the finishing touches. We start by fitting the guardrail, which has to be bent towards the ground at one end before we can drill the holes for the brackets.

Also the traffic signs with St. Andrew's crosses, speed limits and lifting of road bans are now in the line. Heinz O. Schramm really did a great job here and created clearly legible traffic signs that give our diorama the final touch.

While the crash barrier has already been installed and protects cars from slipping onto the track bed, we are still waiting a bit with the signs glued into their holes. Before that we want to make the road look a bit used. Here soot-black powder paint of Kremer pigments helps, which we brush in cracks and small gaps of the tar layer and bind with the adhesion and fixing spray of Noch (61152).



Now crash barriers and traffic signs from HOS-Modellbahntechnik are used. The crash barrier to secure the difference in height between the road and the field railway line must be shortened and angled at the right end.

For the spruce trees we have decided for different models of Microrama, whose needling we have already presented in detail last year. So only the fly mushrooms are missing, which led to the choice of the conifers. They should be shown as true to scale, as possible.

This is only possible with track nails of gauge Z. These are hammered into a wooden batten, which can sometimes be crooked or crooked. Their heads are dabbed with a swab of epoxy resin glue (Uhu), which slightly rounds off this surface. After drying, a white spray primer to simulate the white stems is applied.



A matt and not too uniform appearance of the road can be achieved by finishing with powder paint. Using fine bristle brushes, it is worked into the cracks and joints that the coating shows. The pigments are permanently fixed with Noch's adhesive and fixing spray.

The caps can easily be set off with red paint and a small hairbrush, but it gets tricky with the typical dots. We gratefully reach for the pointed gluing tools that we once found in the model compass (Heinz Wagner), perfect, but too tiny!

A Zettie will certainly notice these almost scale jewels, but a trade fair visitor with a supposedly rather gross motor disposition and a limited focus on H0 gauge will find them very interesting. Without further ado, we decided to also produce some oversized specimens from N-gauge track nails according to the same scheme and added them.

Let them laugh, because maybe they will recognize the tiny ones right next to them. And then it's safe to swallow or even snap! After the fair, we decide, the big mushrooms should experience again soft, smaller replacements and perhaps times on a diorama of the children to move in, which are still on the way in larger scales.

Our crowning finale is to bring some life to the showpiece. As a commissioned work Trafofuchs produced a pair of seated hikers, which we place permanently on the lying tree trunk next to the road with instant binding glue in gel form.



So our diorama finally took part in the EM competition. But the question arises: Where is the fox? A little tip: Look for it in front of the conifer tree on the far left. Another highlight are the almost scale fly mushrooms (photo bottom left).

But the true hammer for us is a small forest animal, which despite its reddish coat colour is hardly recognizable in the undergrowth and proves how well it is actually camouflaged in the surroundings of a forest. The inventors of Creativ-Modellbau Klingenhöfer have perfectly implemented the dog-like predator (TF01) and provided us with an eye-catcher that also finds a permanent place here.

And so our diorama now also has a second name in the form of a challenging slogan: "Be a fox and find him". Well then, have fun searching! Fortunately, we are fine-motorically blessed mini model railroaders also equipped with a good sense of intuition...



Manufacturers of Materials used:

- <https://asoa.de>
- <https://www.bindulin.de>
- <https://www.busch-model.info>
- <https://www.conrad.de>
- <http://www.hos-modellbahntechnik.de>
- <https://www.klinghoefer.com>
- <http://www.lineasecondaria.it>
- <https://eshop.microrama.eu/de>
- <https://www.molto.de>
- <https://www.noch.de>
- <https://www.puren.com>
- <https://www.saller-modelle.de>
- <http://www.sceneryproducts.de>
- <http://www.trafofuchs.de>
- <http://www.molto.de>
- <https://www.uhu.de/de>

Vallejo-Farben with Trainini®-reader rebate:

- <https://www.modellbaukompass.de>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Schmalspur im Diemeltal **Vergessene Grubenbahn**

Heute ist es kaum noch vorstellbar, dass in Deutschland einst Eisenerz gefördert und nicht nur verhüttet wurde. Neben der Region um das westfälische Siegen war auch der Martenberg in Nordhessen, nicht weit von Brilon Wald (Nordrhein-Westfalen) und Waldeck (Kurhessen) entfernt, eine Quelle für den begehrten Eisenstein. Mit ihr verbunden war eine von Deutschlands unbekanntesten Eisenbahnen, die eine durchaus wechselvolle Geschichte erlebte.

Rolf Löttgers
Die Rhene-Diemeltalbahn Bredelar - Martenberg

Verlag im Biebortal, Rainer Haus
Biebortal 1990

Taschenbuch mit Fadenbindung
Format 17,0 x 23,0 cm
64 Seiten mit 78 S/W-Bildern und -Zeichnungen

ISBN 978-3-98014471-1
Preis ab ca. 30,00 EUR (Deutschland)

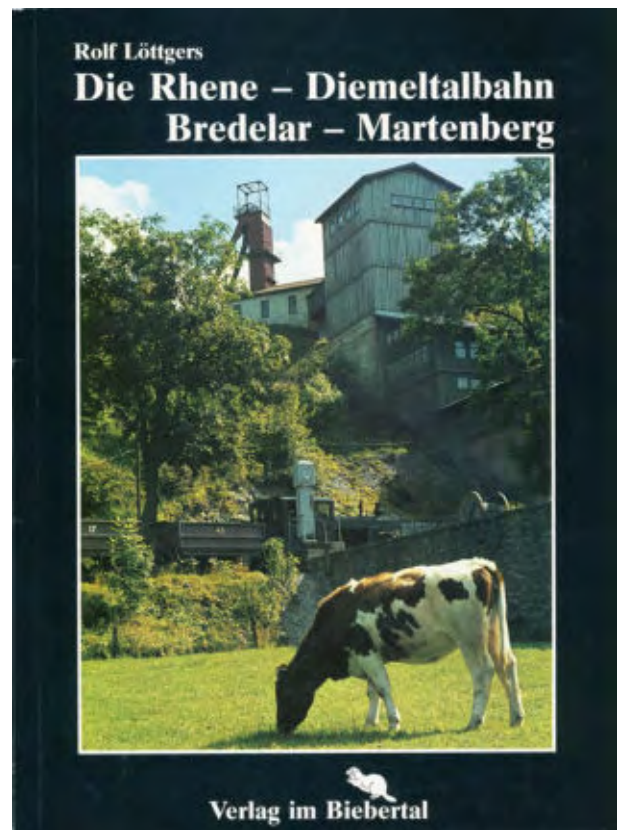
Erhältlich nur noch antiquarisch

„Die Rhene-Diemeltal-Eisenbahn gehört zu den unbekanntesten Privatbahnen, die in Deutschland überhaupt existiert haben. (...) Das hat weiter seinen Grund darin, dass die Bahn nur Güter-, aber niemals auch Personenverkehr gekannt hat, in den Kursbüchern der Staatsbahn also nie verzeichnet war.“

So beginnt der Siegener Autor seine Beschreibung der meterspurigen Bahn, die als Grubenbahn für den Erzverkehr in der Region nahe Waldeck (Kurhessen) geplant wurde und in einem strukturell schwach entwickelten Gebiet einst für Arbeit sorgte. Trotz der geringen Bedeutung der Erzvorkommen, deren Ausbeutung nur für sehr kurze Zeit wirtschaftlich interessant war, hat die kleine Bahn eine interessante Geschichte.

Sie hatte ihren Ursprung im Jahre 1872, konnte aber erst nach einigen Wechseln und Bereinigungen unter den Eigentümern der beteiligten Gruben gebaut werden. Eine bedeutende Rolle spielten dabei vor allem die Aktiengesellschaft Union (Dortmund) und die Aplerbecker Hütte (heute Stadtteil von Dortmund), die Abnehmer des Eisenerzes waren.

Die Betriebsführung war aufwändig, weil es damals noch kein preußisches Kleinbahngesetz gab und so die regulären Eisenbahnvorschriften galten. Außergewöhnlich für eine Schmalspurbahn mit 1.000 mm Spurweite war, dass die Fahrzeuge mit Regel- statt Mittelpuffern ausgestattet waren.



Bereits 1917 schien der kleinen Bahn mit dem Schließen der letzten Grube die Existenzgrundlage endgültig entzogen. Das geförderte Gestein wies zuletzt einen Eisengehalt von nur noch 20 % auf, was mit Blick auf die Kosten importierten Materials nicht mehr abbauwürdig erschien.

Mit neuer Spur von 900 mm auf politischen Druck durch Mannesmann wiederaufgebaut, erlebte sie ab 1937 eine Renaissance, die sich auch nach dem Krieg bis ins Frühjahr 1963 fortsetzte. Genutzt wurde die alte Trasse, die Anlagen wurden aber so einfach wie möglich gehalten. Grund war die gewünschte Selbstversorgung Deutschlands mit dem geförderten Rohstoff.

Rolf Löttgers weiß die Geschichte von Bahn und Eisenerzabbau trotz bescheidener Archivunterlagen gut zu erzählen. Untermauert hat er dies mit aufschlussreichen, historischen Aufnahmen. So wird aus dem dünnen Taschenbuch ein Werk, das nicht nur lokal Geschichtsinteressierte ansprechen kann.

Sein Werk kann auch beim Gestalten der Modellbahnanlage helfen: Wenn eine Schmalspur- oder Feldbahnstrecke integriert werden soll, ist guter Rat teuer. Da es solche Bahnen heute kaum noch gibt, ist es schwierig, glaubhaft wirkende Bahnanlagen nachzubilden. Besonders bei der Anlage minimaler, aber funktioneller Gleisbilder zeigen sich Probleme.

Aber auch die Streckenführung entlang von Straßen oder Flüssen, samt Überquerungen auf Brücken oder niveaugleicher Bahnübergänge, erfordert Fachwissen, weil hier durchaus wahrnehmbare Unterschiede zur Regelspur bestanden. Und gerade da liegt der Reiz, dergleichen in die Modellbahn zu integrieren.

Dabei kann dieses Buch eine wertvolle Hilfe bilden, besonders im äußerst interessanten Umfeld einer Grube – es muss ja nicht zwingend eine Großzeche aus dem Ruhrgebiet oder dem Saarland sein.

Zu einem Hingucker aufwerten lassen sich da Erbstollen oder ganz besonders Umladeeinrichtungen von der Schmalspur und Feldbahn auf LKW oder Regelspurgüterwagen. Passende Modellumsetzungen finden sich beispielsweise auf der Anlage „Sägethal“ von Götz Guddas oder an der „Schwerspatgrube Clarashall“ von Gerhard Maurer.

Viele Modelleisenbahner lieben doch das Besondere, denn gerne erleben wir Abwechslung beim Betrachten der Anlagen auf Ausstellungen. Schnöder Kreisverkehr um Vollmer-Häuschen lädt wohl niemanden ein, länger an einem Arrangement zu verweilen und Details zu studieren.

Dieser antiquarische Band gehört zu denjenigen, die erforderliche Anregungen liefern und Gespür für eine korrekte und vorbildnahe Umsetzung liefern können. Es lohnt sich, sich mit der Materie abseits der vollspurigen Eisenbahn auseinanderzusetzen.

Museumsbahnen Nordrhein-Westfalens Zur statt mit der Bahn in den Urlaub

Der Sommer lockt mit warmen Temperaturen und Sonnenschein zu Ausflügen. Eisenbahnfreunde möchte aber auch im Urlaub nicht auf ihr Steckenpferd verzichten. Bei der VG Bahn ist kürzlich ein Buch erschienen, dass sich als perfekter Reiseführer für Ausflüge zu nostalgischen Eisenbahnen im bevölkerungsreichsten Bundesland eignet. Warum also nicht mal woanders und auf andere Weise den Urlaub genießen?

Korbinian Fleischer
Reiseführer Nostalgiebahnen
Nordrhein-Westfalen

VGB Verlagsgruppe Bahn GmbH / Klartext Verlagsgesellschaft mbH
Fürstenfeldbruck / Essen 2019

Taschenbuch mit Klebebindung
Format 14,8 x 21,0 cm
160 Seiten mit ca. 120 Farbfotos

ISBN 978-3-8375-1646-3
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Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Sechs Jahre nach Erscheinen eines ersten Titels dieser Reihe zu den bahntechnischen Sehenswürdigkeiten Baden-Württembergs folgt nun rechtzeitig zur Sommersaison 2019 ein Band für Reiseziele in Nordrhein-Westfalen.

Auch im bevölkerungsreichsten Bundesland lassen sich heute immer noch technische Zeitzeugen längst vergangener Zeiten auf den von Museumsbahnen befahrenen Strecken in Aktion erleben.

Als Besonderheit im Sauerland, im früheren Aachener Kohlrevier und ebenso im Ruhrgebiet vermitteln diese Bahnen ein ganz besonders Bewusstsein: Ohne die Eisenbahn wäre der industrielle Aufschwung vieler Regionen gar nicht möglich gewesen.

So sind sie Bestandteil einer ganz besonderen Industriekultur, die es verdient hat, in einem Buch zusammengefasst und katalogisiert zu werden. Die Idee von Autor und Verlag passt umso besser, als das sich Nordrhein-Westfalen und besonders das Ruhrgebiet seit Jahren an steigendem Tourismus, ablesbar an den Übernachtungszahlen, erfreuen.

Das ganz besondere Flair, wie technische Denkmäler und Industriekultur hier Eingang ins tägliche Leben und die Stadt- wie auch Landschaftsbilder gefunden haben, ist wohl einmalig. Da ist es sicher kein Zufall, dass der zweite Band einer sicher noch weiter fortgesetzten Reihe diesem Bundesland gewidmet worden ist.

Eingerahmt von einem Grußwort des VDMT-Präsidenten zu Beginn und abschließenden Erläuterungen zur „standesgemäßen“ Anreise mit der Bahn, Blicken hinter die Kulissen der Museumsbahnen



(Aufarbeitung und Instandhaltung von Strecken und Fahrzeugen) und einer Übersichtskarte der Bahnstrecken finden sich Portraits und Vorstellungen folgender Museumsbetriebe:

- Museumsbahn Rahden – Uchte
- Museumsbahn Minden – Hille und Bohmte – Preußisch Oldendorf
- Landeseisenbahn Lippe (Extertal-Bösingfeld – Barntrup – Dörentrup)
- Eisenbahn-Tradition (Lengerich und Osnabrück – Mettingen)
- Dampf-Kleinbahn Mühlenstroth
- Museumseisenbahn Hamm-RLG – Lippborg-Heintrop
- Sauerländer Kleinbahn (Hüinghausen – Köbbinghauser Hammer)
- Bergische Museumsbahn (Kohlfurth Brücke – Möschenborn)
- Hespertalbahn (Essen-Kupferdreh – Haus Scheppen)
- Ruhrtalbahn (Hagen – Hattingen und Hagen – Ennepetal-Altenvoerde)
- Hist. Schienenverkehr Wesel (Rheinpromenade Wesel – Hohe Mark)
- Krefelder Schluff (St. Tönis – Hülser Berg)
- Selfkantbahn (Gillrath – Schierwaldenrath)
- Millionenlinie (Kerkrade-Centrum – Simpelveld – Valkenburg – Vetschau)
- Eifel-Bördebahn (Düren – Zülpich – Euskirchen)
- Oleftalbahn (Kall – Hellenthal)
- Pützchensmarkt-Express (Bonn-Beuel – Pützchensmarkt – Hangelar)
- Wiehltalbahn „Bergischer Löwe“ (Dieringhausen – Wiehl – Waldbröl)
- Rheinisches Industriebahnmuseum Köln

Jeder der bundesweit bekannteren oder auch nur regional verankerten Bahnen sind mehrere Seiten im Buch gewidmet, die über Anfahrt, Programm, Geschichte von Bahn und Museumsbetrieb sowie die eingesetzten Fahrzeuge informieren.

Als kleine Besonderheit haben wir die Sauerländer Kleinbahn herausgepickt, die mit dem Titelfoto bedacht wurde. Sie ging nicht direkt aus einem Regelbetrieb hervor, sondern wurde nach Stilllegen einer Bundesbahnstrecke als Museumsbetrieb gegründet und baute auf den früheren Trassen einer Meterspurbahn auf. Ihr Ziel ist es, das Erbe der vielen Kleinbahnen im Sauerland zu bewahren.

Der vorliegende Reiseführer zu den Nostalgie- und Museumsbahnen Nordrhein-Westfalens bietet in Summe eine wohl vollständige Übersicht für alle Bahnfreunde, die sich auf die Spuren früherer Dampfeisenbahnen begeben wollen.

Wer den Sommer oder auch die kalten Tage mit prächtiger Dampfentwicklung für Eisenbahnurlaube oder Ausflüge nutzen möchte, findet hier den passenden Begleiter. Hinweise auf Fahrtage samt Streckenkarten/-übersichten sowie Empfehlungen für die ganze Familie runden dieses Buch ab.

Publishing pages with reference possibility:
<https://shop.vgbahn.info>

Eisenbahnmuseum Dieringhausen



Einzigartig in Europa

*Spur Z Ausstellung
im historischen Güterwagen
im Eisenbahnmuseum Dieringhausen*

Die Sonntags-Ausstellungstermine:

*08.06.2019, 09.06.2019, 16.06.2019, 07.07.2019,
28.07.2019, 11.08.2019, 22.09.2019, 13.10.2019,
27.10.2019*

jeweils von ca. 10:00 Uhr – ca. 17:00 Uhr

Infos unter: www.stammtisch-untereschbach.de

Excursion to the DB Museum Nuremberg

Beer, bananas and more

The world's oldest railway museum is located in Nuremberg and has been welcoming visitors almost every day during the past 137 years. Its collection is diverse and its conceptual approach to exhibitions is changing. If you like railways, this is the place to visit. Until 31 October 2019, there will also be a special exhibition. Let us take you on a tour of the DB Museum Nuremberg.

Founded in 1882, the Nuremberg Transport Museum, renamed DB Museum after the privatisation of the German Federal Railway, is the oldest railway museum in the world.

The museum welcomes more than 200,000 visitors every year, and offers a first hand experience from various angles of the history of the German railway. In addition to its main facility in Nuremberg, the museum has two additional locations in Koblenz and Halle (Saale), which focus on special topics and regularly organise their own events.



The DB Museum, which emerged from the Transport Museum for Railway and Post, is the oldest railway museum in the world. Photo: DB Museum / Mike Beims

For some years now, the museum has been operating under new management and under the umbrella of the Deutsche Bahn Foundation, which has obviously helped to increase its attraction. In comparison to previous visits, some sections appeared visibly fresher and more modern, and thus more appealing, especially to younger visitors.

The core collection of the Nuremberg Museum includes epoch-making items such as the saloon cars of King Ludwig II and Chancellor of the Empire Bismarck, a replica of the "Adler" (Eagle) and the original of the class 05 001, as well as models of the Royal Bavarian State Railway in 1:10 scale.



A classic among the Nuremberg exhibits is the famous saloon car of Ludwig II, King of Bavaria.

We owe it in particular to this large collection of scale models that we preserve the knowledge of the livery and colours used in this specific 19th century railway administration.

All these scale models are originals from that period and their age alone already deserves them a place in the exhibition.

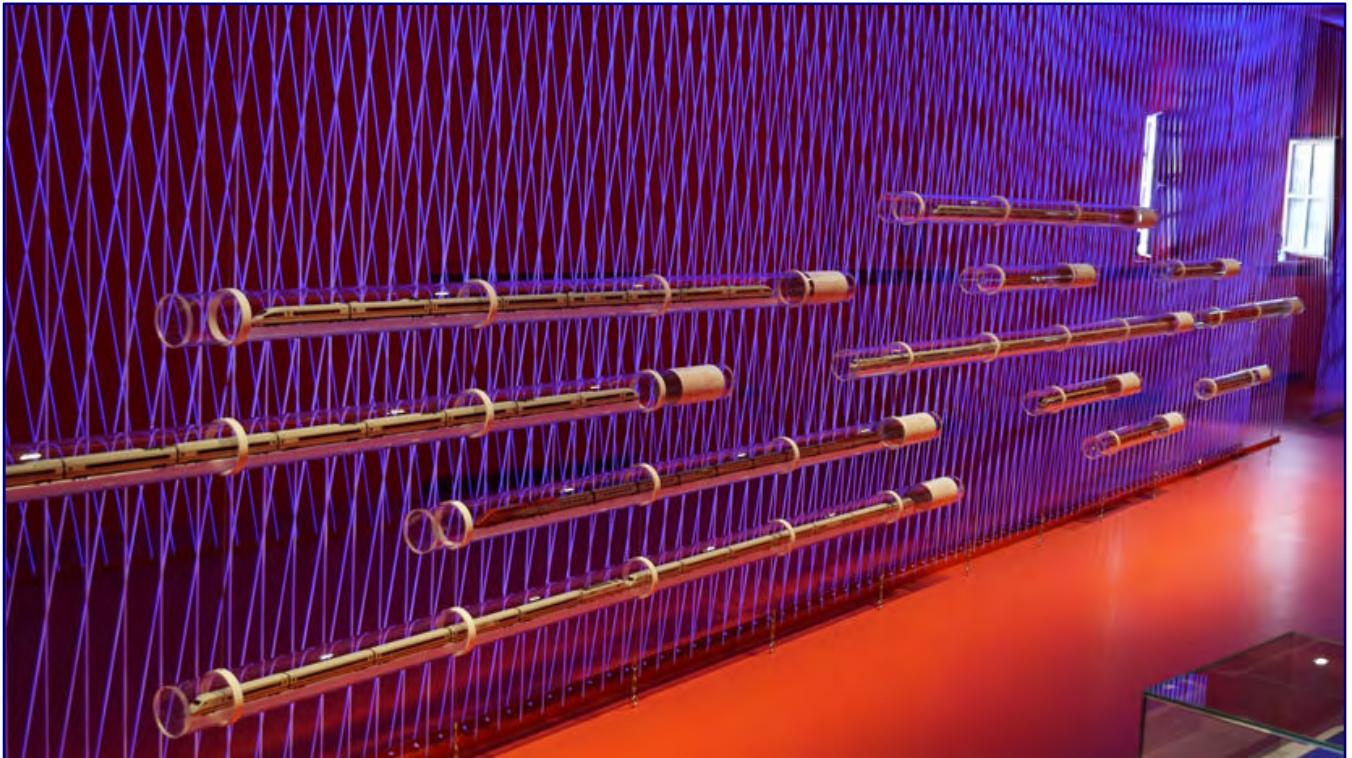
All in all, the exhibits keep alive German railway history throughout all of its periods. This also includes dark times of history such as the participation of the Reichsbahn in deportations and war.



The "Flying Hamburger" in the shape of a SVT 877. Unfortunately, only one section of this diesel rail car remains, albeit in excellent condition.

The museum's honourable halls present visitors with an accessible account of railway history from its beginnings around 1800, including two original cars from the 19th century, to the present day.

A particularly exciting part of the museum is its new permanent exhibition "On separate tracks: the railway in divided Germany from 1945 to 1989", which opened on 8 June 2019 and shows the differences, but also parallels between the two German railway systems during the four decades after the Second World War. The TEE routes in Western Europe served by the West German class VT 11⁵ and the East German DR Class VT 18.16 railcar make for a quite an illustrious and interesting example in this respect.



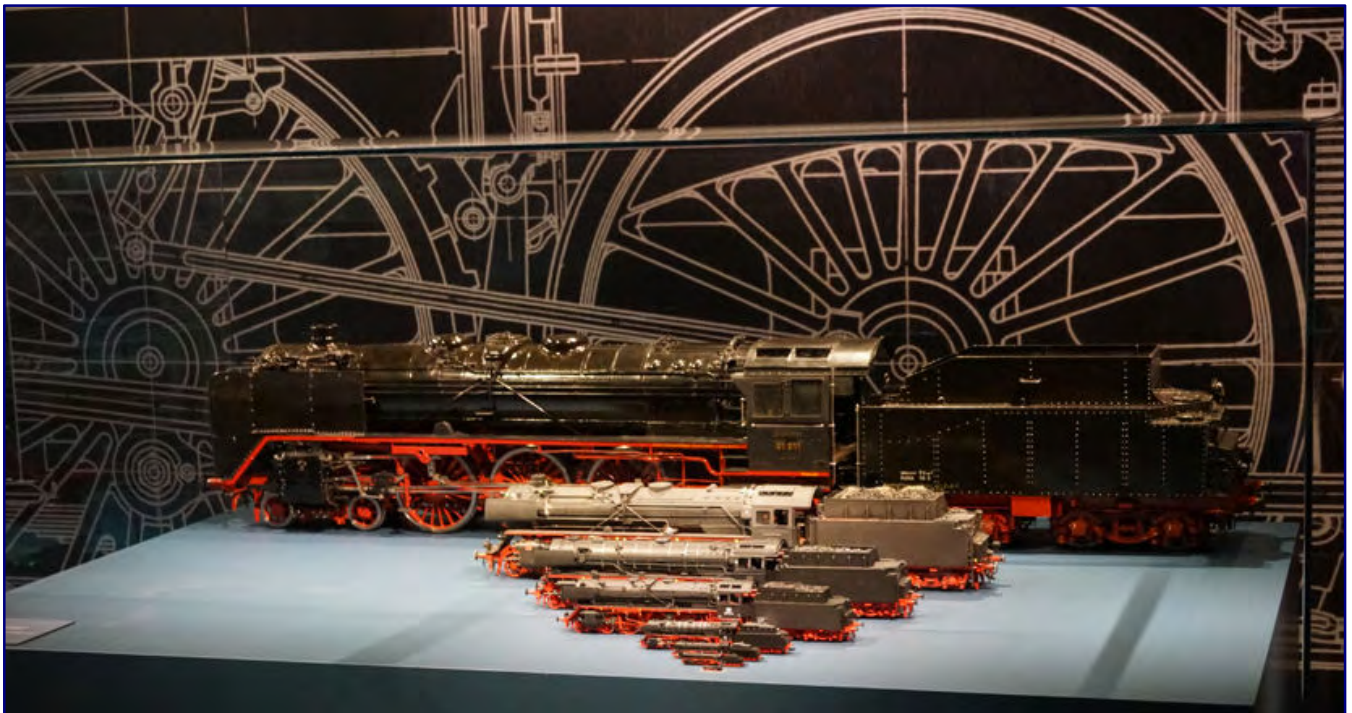
The exhibition on the future of railways impresses with clever lighting and visual effects. Part of this presentation are also models of the high-speed trains that are in service on the German railway network.

Younger children also get their money's worth in Nuremberg at the "Kibala" (Children's Railway Land) devoted entirely to them on the 2nd floor of the museum. Here they can experience railway history in a playful way in the truest sense of the word and even dress up as typical railway employees or take a trip across the entire floor on a large 5-inch gauge train.

Kids will receive a free ticket at the box office, and parents will find a place for taking a break and recharging their batteries.

The people in charge of the museum obviously also wanted to set an example with covering the future of train travel. Focusing on digitisation, networks and high-speed train technology, the "Future" section of the museum makes use of illuminated displays in otherwise darkened rooms, clever visual presentations and advanced lighting techniques.

Similar exhibition techniques add to the attraction of the model railway displays in the "Modellarium" section on the 1st floor of the museum. Cool but bright lights highlight the display cases and their contents of various modelling scales against the surrounding relative darkness.



Right in the entrance area of the railway models exhibition section we find the class 01 high-speed steam locomotive in all conceivable model railway scales (top). A scale 1:10 replica of the V 80 001 is also on display in this part of the museum (bottom).

Amongst the models on display are more recent scale 1:10 ones from DB times, a large steam locomotive 23 001 as well as many other exhibits from various model railway scales down to a T gauge ICE model.

One large display case is exclusively dedicated to N gauge models. As for Z scale, we were particularly struck by the direct comparison of scales and track gauges on the basis of a class 01 steam locomotive. This seems to be the only locomotive that has been produced across all modelling scales.



105
1705

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E

B

A modelling classic is the museum's large Fleischmann H0 layout which is operated several times a day. Whilst the layout looks somewhat dated, compared to today's expectations and possibilities, it is nevertheless an item not to be missed. It impresses with its sheer size, realistic track layouts and a prototypical operating system. Once cutting edge technology, it does represent a milestone in model railway engineering.

The main reason for our most recent visit, however, was the special exhibition "Beer, Railway and Bananas" on the outdoor grounds, which has been running since 12 April. Amongst the two permanent signal boxes (allowing kids to operate different types of semaphore signals), the DB Museum has brought together nine freight cars and one passenger car as well as two freight locomotives (E 91 99 and 45 010).



Since 12 April and until 31 October 2019, the DB Museum has been running a special outdoor exhibition entitled "Beer, Rail and Bananas". This event deals with the colourful facets of freight transport - starting with the transport of beer kegs on the Adler train in 1836.

They give visitors the opportunity to see and understand the many facets of freight transport by rail and to understand its development, starting with what began on 11 June 1836 - and not, as often assumed on 7 December 1835 - by transporting two barrels of beer brewed by Georg Lederer from Nuremberg's Plärrer railway station to the innkeeper at Fürth's station.

This marked the beginning of rail freight transport in Germany. In the following decades railways became the most important means of transport and a driver of Germany's industrialisation already at the end of the 19th century.

Some of the cars on display have been opened and equipped with partitions. They show the development from bulk cargo to container transport, the transport of foodstuffs such as bananas or beer in covered tank wagons or even curiosities such as the transport of animals, illustrated with a photo of a circus elephant. By selection of cars presents an arc from 1835 to the more recent past.

Continued on page 53



The refrigerated and covered freight cars at the loading ramp are accessible (top) and display images of some curious rail transports, such as that of circus elephants (bottom).



An exhibit in the goods shed, showing historical tools which were used to handle bulk materials at railway stations throughout the long history of railways.

Further exhibits are on display in the adjacent goods shed. They mainly deal with the problem of how to handle and load bulk materials into covered freight cars.

From the historical wheelbarrow, the "Ant" for moving pallets to other types of tools for moving and lifting cargo, the main items and their development over time of this particular freight handling challenge are shown here.

If this report has whet your appetite for visiting the Transport Museum, please know that it is suitable not only for rainy days, as most of the special exhibition is outdoors.

A thorough visit of all the sections of the museum will take at least four hours, so it is best to come with some peace and quiet. But time is pressing, at least if you want to see the special exhibition "Beer, Railway and Bananas" which runs only until 31 October 2019.



Part of the special exhibition on the history of bulk cargo transport is also 45 010, the last remaining specimen of Germany's heaviest freight steam locomotive.

Opening hours and prices:

https://www.dbmuseum.de/museum_en

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

This reader is pleased about the June issue:

This issue was a very special highlight for me. The report "Landscapes and Tracks" was for me, as a searcher for a possible plant form, stimulating, as well as unique.

So I'm very excited about the sequel! Also, tool tests are helpful again and again, so the presentation of the locomotive service tray from Noch.

It's great what this little editorial office conjures up month after month! I've wanted to tell you that for some time now. I wish you much success and enthusiasm in continuing this little gem Trainini!

Dietrich Frank, Düsseldorf

Märklineum stays open later:

The Südwest-Presse (SWP) announced on 3 July 2019 that the new Märklineum will not be fully opened until later than planned. The new factory outlet has been open since September 2018, but the showroom and company history are still a long way off. Most recently, the Märklin pages spoke roughly of the first half of 2020.

Now it was confirmed what had been suspected for some time, because the date is slipping far behind. The newspaper referred to press spokesman Eric-Michael Peschel and named May 2020 as the new date. The museum will not be ready for the Märklin Days in September 2019, but visitors should, at least, be able to get a glimpse of how the exhibition will be set up.

The reason given for the delays was that an intermediate ceiling had to be removed for the exhibition. Märklin doesn't want to break a knee there.

Retrofitting kits from Z-Railways:

With the V 36 Märklin has created a beautifully constructed locomotive, which is characterized by excellent running characteristics and attractive appearance. But in all versions it has not remained without compromises, as our test had already shown.

This also means that it offers potential for upgrading and fine detailing. After the green DR version (Item No. 4017-19093071) had already been considered, Z-Railways (<https://www.shop.z-railways.de>) now also offers an upgrade kit for the Bundesbahn version with raised cockpit (4017-19103111).



It was developed and produced by eXact-Modellbau in Bad Doberan, but is exclusively available from the small series distributor. It consists of two cast brass typhoons, with which the missing part on the front wall of the cab can be retrofitted, a foldable ladder with climbing protection and the missing shunter handrails on the front ladders of all Märklin models.

Also the coupling rods can be exchanged against finer and one-piece etched parts, which belong likewise to the scope of the delivery. We would like to supplement at this point also that also further kits of eXact-Modellbau are in the delivery program: finely etched stage railings for Märklins Donnerbüchsen, conversion kits for thunder rifle command wagons and the hay wagons developed already some years ago (two copies per kit).

Exclusive innovations at Archistories:

Archistories was busy again and announced several innovations. The first two are produced exclusively for Ztrack-Center. It is an expandable warehouse complex in the typical North American style.



Already the basic construction kit of the warehouse "Railway Express Agency" (Art.-No. 103191), produced exclusively for **Ztrack**, results in an impressive building. Photo: Archistories

The REA-Transfer-Warehouse (Art. 103191) is a warehouse of the Railway Express Agency and, together with the extension kit (104191), makes it possible to display elongated freight and loading areas along the tracks. The possible dimensions of such a building are impressive, as the corresponding photo shows.

The kit is at least theoretically infinite, in practice in any case arbitrarily expandable. Its strengths lie in its mature and warp-free construction as well as the many small details.



With the extension sets (104191) the complex can be extended at will, theoretically infinitely. Photo: Archistories

These include a filigree external staircase, the optional building lettering, elaborately constructed windows and even movable gates. The dimensions of the basic kit are 80 x 155 x 80 mm (L x W x H), those of the extension 80 x 91 x 72 mm.

These two kits can only be purchased via <https://ztrackcenter.com>

The 1zu220 shop also exclusively produces two other new products which are expected to be available from the end of August 2019. They continue the popular series of buildings in local architectural styles, most recently supplemented with a village church.



Here you can see the backside of the fire station Dörpede (402191) with the attached crate. Photo: Archistories

The fire station "Kallental" (401191) shows a massively bricked sandstone base zone with a plastered framework in the upper floors.

This combination gives the functional building an almost romantic touch. The inserted hose tower underlines the use of the building with village character.

On the back side, a flat storage crate with a fine wood engraving enlivens the appearance. The two vehicle garages with large windows can be effectively illuminated and offer plenty of space for the fire engines to be stored here.

With the fire station "Dörpede" (402191) a counterpart is offered again, with which the framework is bricked up. The warm shade of the brick ensures a harmonious overall effect. In the other features, it follows the kit described above.



The dimensions of both village fire stations are 83 x 56 x 92 mm (L x W X H). Like all Archistories kits, they are made of high-quality, solid-coloured hard cardboard and do not require any further colour treatment.

In addition to the fire stations, the 1zu220-Shop (<https://www.1zu220-shop.de>) also offers a sound module "Siren" (NO-SOUND-SIRENE) for operation on a commercially available model railway transformer, with which the volunteers can be called in if necessary.

So that this can also be perfectly represented in the model, only the buyers of one of the two fire brigade kits can also purchase a Magirus-Deutz LF (WM-FEUERWEHR) fire fighting vehicle, also exclusively manufactured by Wespe Models. This model can also be seen on the title page and in our contribution to the summer diorama.

News about Märklin deliveries:

According to Märklin's schedule, some new products were about to be delivered at the editorial deadline. The following models were already available in time to discuss them here.

The series starts with the passenger steam locomotive class 24 with cross-country tender (art. no. 88031). Its model 24 061 was equipped with the tender 2'2' T 26 of the 50 909 and the Krauss-Helmholtz steering frame of the 64 512 in 1948 for the test office Göttingen (later Minden). On the one hand, this should increase the range and improve the running characteristics, especially when reversing.

Its miniature is equipped with a stationary bell-shaped armature motor with flywheel mass and shows a direction-dependent LED tip lighting in warm white at the front. The locomotive has no steam bell and benefits from a modified tender coupling that brings it much closer to the driver's cab. This is very good for the model's appearance.

It is astounding that after a slight reworking of the shape after a minor accident, the missing step under the smoke chamber has now been repaired, as has the standing boiler bulge removed during the same process (mould was a H0 derivative). This suggests the assumption that there must be at least two die casting moulds for the 24 series produced in large numbers.

Coal traffic on the Z-gauge plant can now also begin. The four-part freight wagon pack (86307) with three OOtz 50 wagons and one OOtz 43 wagon including loads was also delivered.

In addition, the self-unloading wagon OOtz 50 (86308) also rolls to the dealers. It also bears the side address "Erz III d" and also has a coal load insert. A second extension is the OOtz 43 (82803) self-discharging wagon in a comparable design and construction.

The car transport wagon DDm (87093) is in far blue paint and with modified Ege cookie (old DB logo) to the transition from epoch IV to V assign, suitable passenger coaches are not offered at the same time however. The double-decker wagon is also delivered unloaded.

Many Zetties have been waiting eagerly for the tender steam locomotive of class 86 (88962). After it had already undergone a model upgrade and since then has appeared with fully functional rods, replicas of the brakes and lamp brackets, only the engine technology had to be revised.

The newly delivered version as 86 234 of the Deutsche Bundesbahn (German Federal Railroad) with a written out owner's name also features the bell-shaped armature motor. Apparently, also the current decreasing wheel grinders were improved, which was a known weak point of the models.



Märklin's Bundesbahnlok 86 234 (Art.-No. 88962) with detail control and now also bell-shaped armature motor can be seen here together with the beer advertising wagon "Westheimer Weizen" (98165) from the 1zu220-Shop, also produced by Märklin.

Window inserts would have been desirable, but were not obstructed. Top lighting is provided by warm white LEDs. The colour scheme and printing have been successful: the cables below the driver's cab are individually set off in fire red, the lettering also includes lightning warning signs. However, the edges of the smoke chamber were not red.

Beer wagon series continued:

The 1zu220 shop (<https://www.1zu220-shop.de>) has continued its Westheimer beer car series with a sixth model. Available since 25 June 2019, the latest model of a thermally insulated freight car G 10 with the special print "Westheimer Weizen" (Art. No. 98165) is now available in a unique edition of 100 copies.

The basic colour of the model is RAL 9002 grey-white and thus follows the DB painting regulations for refrigerated wagons of model era IV. The roof colour of this extremely attractive design is RAL 5025 Perlenzian. The unprinted car bottom is unprinted and sprayed in RAL 9005 deep black (see photo on page 58).

The new products from AZL in July 2019:

Main topic of the beginning high summer is the passenger train "The Eagle" of the Missouri Pacific.

AZL serves it with appealing grey-blue painted light passenger coaches (Empire-Builder colour scheme) with yellow set-off stripes. A matching train locomotive in the form of the EMD E8A (Art. No. 62610-1 to -4) has been in the range for some time.

We search in vain for a suitably designed dining car in the delivery list, but at least the following car types have been considered: sleeping cars 6-6-4 (73114-1 / -2), viewing cars (73414-1 / -2), post-pack cars (73614-1), seat cars (73714-0), pulpit cars (73814-1) and rail mail cars (73914-1 / -2).

The 40-foot long, covered AAR freight wagons from 1937 now bear striking logo lettering "New York, New Haven & Hartford" on their brown lacquer and are available individually (904308-1) as well as in sets of two (904378-1) and four (914308-1).

You will find further manufacturer photos of the current deliveries under <http://www.americanzline.com>.

And here is what's new at Micro-Trains:

The second US large-scale series manufacturer is planning to deliver the EMD SD40-2 diesel-electric locomotives in the colours of the merged BNSF (Art. No. 970 01 291 / -292) this month. So far, however, only graphic representations of the model design have been shown.

Wagon number 6 (518 00 760) continues the series "Farm to table." The basic colour is brown, the side walls of the wooden 40-foot refrigerated wagon are set off in yellow and bear advertising logos for "Mrs. Tucker's Shortening."



Observation wagon (Art.-Nr. 73814-1) and the EMD diesel locomotive E8A of the Missouri Pacific (62610) which fits to the "The Eagle." Photos: AZL / Ztrack

40-foot standard cars with one-piece sliding door are the covered copies of Norfolk & Western (500 00 066 / -67) with black paintwork and white lettering. In addition there are the covered bulk wagons with two outlet openings of the Cotton Belt (531 00 291 / -292). (<http://www.case-hobbies.de>).

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German Magazine for Z Gauge



Holiday mood in model

Scaffolding work on the V 36
Model car history

Introduction

Dear Readers,

summer seems almost over to me as I write these lines. But it can't have been all yet, can it?

That's right, many of our readers are still on vacation. In some German federal states there are still summer holidays. And so we also want to see a summer edition for August, in which holidays and wanderlust echo.



Holger Späing
Editor-in-Chief

But why wander into the far distance when the good is so close? Right, and so we found the perfect solution not far from the editorial office, with which we could reconcile our ideas. Our reader Dietmar Allekotte lives less than 50 kilometres from the editorial office, and his diorama "Küstenland" (coastland) had everything we imagined by theme.

I am all the more pleased to be able to present it to you today. After all, the works of our readers are what we would like to focus on in our reports this year. I therefore think that you will feel the same excitement when you read them as we do.

It was midsummer and hot when we visited Herpa in Diethofen not quite two months ago. After all, we don't want to miss a round anniversary. We offered there our popular hobby activity for children, in which they can give a beer mat a small landscape and experience so first contact with the model building.

Noch and Uhu have generously supported us as sponsors again, for which I would like to thank you at this point. We were also able to capture many personal impressions and most of all pictures. Therefore you will find a report in this edition.

A further topic is to be the V 36. As a locomotive with roof cupola it was almost a small sensation in spring, but a few things are still missing for an original-oriented model railroader.

So we started with a model construction kit from Exact and equipped a model with it. We would very much like to show in this edition's tinkering article that the parts produced in small series and carefully varnished and mounted considerably enhance the Märklin model.

But August is also full of news and novelties. This time there are many pages in the section where we would like to stay in dialogue with you. Our two book reviews are not quite as recent, but they really pack a punch. If we had to decide for one of them, we would probably have a hard time.

This brings me to the last missing topic: With the VW Transporter we are turning to a special car that has shaped and continues to shape everyday life on the road as well as on model train layouts. A short history and model overview should therefore show the entire spectrum of how we encounter the "Bulli" again and again. Have fun reading!

Sin-Z-erely,

Holger Späing

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We thank Dietmar Allekotte, B. and N. Heller, Raffaele Picollo, Jens Wimmel and Sascha Braun for their photos.

Date of publication of the German language version of this issue: 26 August 2019

Cover photo:

In places it is close to this coastal section and the railway line borders directly on the beach promenade. Swimmers cross the line over the pedestrian bridge in the background, while VT 98 9667 roars loudly past them.

Upgrading work on Märklin's V 36 Optimization potential identified

Without a doubt Märklin's V 36 is a successful model. But the missing handrails on the front turret and the ladders, which can certainly be better modelled, still offer potential for all those who take it very seriously or want an almost perfect locomotive. The latest edition as 236 123-6 also lacked a horn. With the help of the Exact-Modellbau modification kit, we have now eliminated these small mistakes, as well.

With the V 36, Märklin has delivered a good design and put another desired model on the rails, which has already been released in four different versions. But even this locomotive still offers some ideas for our modifications. Whoever would like to transform, for example, from the 236 123-6 with roof cockpit (Märklin 81772) into a top model, can do so with a limited expenditure.

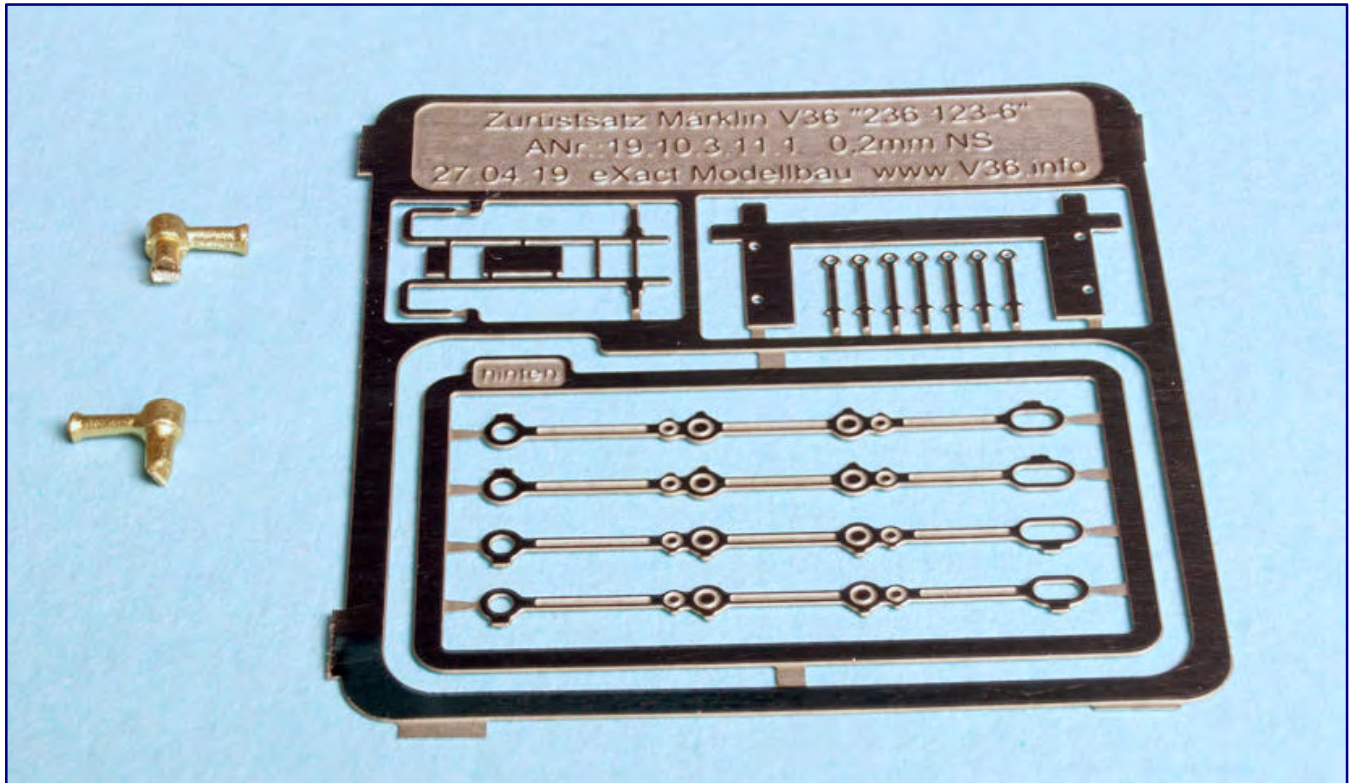
Exact-Modellbau from Bad Doberan has developed a special modification kit (Art.-No. 19.10.3.11.1) for the DR-version of the diesel locomotive, which contains missing or improved parts for the Märklin-model. It is distributed exclusively by Z-Railways.



With the kit of Exact-Modellbau (Art.-No. 19.10.3.11.1) you can get a lot out of Märklin's Kanzeldiesellok (roof cockpit diesel locomotive) 236 123-6.

With this kit I have upgraded the locomotive so that it can be introduced once again. First, I scan the supplied nickel silver plate, and obtain an overview of the scope and effort: I find a folding ladder, two pairs of one-piece coupling rods, seven shunting handrails and a drilling template.

Also included are two horns as brass castings. A set of coupling rods, three rods for the hand rails and one of the horns are therefore intended as a replacement or surplus. The modeller will be thankful if something should go wrong, which may also encourage beginners.



The parts set contains two horns made of brass investment casting (left) and a nickel silver etched sheet metal bow with drilling template (top right), and all necessary parts. If only one shunter hand rod was included, a second model could be completely upgraded without replacing the ladder.

Soldering work is obviously not required. The modelling work is limited to separating, folding, painting and attaching parts. If you already have experience with etched parts, you should not encounter any problems here.

And that's where it all starts: Painting is best done while the parts are still attached securely to the sheet. That's why I only cut out the drilling template, which is supposed to help with the correct positioning of the shunter handrails. It will be set aside for the later work steps.

Basic painting

All parts still left on the sheet, as well as the parts made of brass investment casting, require painting. Due to the smooth surface, which offers little adhesion, not all paints and varnishes are suitable for metal.

This requires a good choice and pre-treatment of the surfaces. So there must be neither dirt nor grease on them. While the former is not an issue, grease from our hands plays an even greater role: if we take the sheet out of the bag without gloves, we could leave invisible traces which can impair or even prevent the adhesion of lacquers.

Ideally suited for degreasing is acetone, which should be used carefully because of its volatility and toxicity. With good ventilation, we wipe the sheet from both sides with a bristle brush, the tip of which is

first moistened with the solvent. Tools, such as tweezers or pliers, should be used to avoid contact with the skin.

Touching with the fingers is now taboo. If everything is gone, I fix the coupling rods with Tamiya masking tape, so that they remain immediately free of paint mist. Ladders, shunting rods and a horn are now primed black on all sides with an airbrush, while they are held by a "third hand".

A (unfortunately no longer available) nitro-based Modelmates primer is used which, like Badger's black PU primer, shows no colour deviation from the deep black base of the Märklin model.



All parts to be mounted later on the black superstructure have been spray painted on both sides in RAL 9002 deep black, the coupling rods, on the other hand, are painted fire red with a brush. After drying, they are removed together with the black rod bearings with a piece of beer mat which has previously been moistened with nitro thinner.

As soon as the spray application is surface dry, the masking tape is removed. When you are certain that the freshly painted parts cannot accidentally touch, work with the coupling rods can continue. However, these can only be painted with a fine brush.

I paint the parts milled in the model as exactly as possible in RAL 3000 fire red, the axle bearings I emphasize with RAL 9002 deep black. The black varnish may be mixed a little more liquid, because full coverage is not required. I see the water-based products from Oesling Modellbau as the means of choice here.

Of course, no matter how small the brush tip, it is not possible to reach all parts of the poles so precisely that the parts could be used immediately after drying. So I put something into the trick box to put the surfaces that should appear blank back into this state.

Traditionally, the finest sandpaper with a grain size of at least 1,000 was always used here. But since this also leaves tiny grooves on the surface, I received a helpful tip from Ronald "Larry" Schulz (Exact-Modellbau): Use an absorbent piece of drawing cardboard that has been carefully sprinkled with nitro thinner.

It must not be wet or even dripping; a slight surface moisture is completely sufficient. So I cut a piece out of a beer deckel (mat), which is supposed to be even more absorbent, and carefully wipe the sheet with it. It lies flat and safe on the work surface of the table.

In no time at all, the excess paint disappears from the raised surfaces, leaving behind red depths and black bearing rings -- perfect! Later, a tarnish protection of silk matt clear lacquer, applied with the airbrush, will permanently seal this movement.



In addition to the Xuron tweezers, the Tamiya bending pliers are particularly useful for the necessary bending work, as shown here with the drilling template.

Next I turn to the ladder. Märklin's punched part on the left side of the ladder can't satisfy my expectations at close inspection: It lacks the climbing protection that was provided by the "Doppelten Lottchen." In addition, the basic shape can also be represented much better with an etched part.

So now the ladder is separated from the arch and bent. A pair of Xuron tweezers and Tamiya bending pliers help me. The sides are quickly angled precisely, the top step is only brought into shape later, as soon as the assembly takes place.

It's also satisfying that the primer shows a good hold and doesn't come loose at any point. Only at the outer bending points will smaller repairs be necessary, because the application now shows less coverage because of the stretching.

Meanwhile I can think about the assembly of the parts on the model. I am still undecided whether I should actually mount the new coupling rods? Although the short parts from axle to axle give a good and straight

picture to the reference model, the original part looks better, because it shows more details like the rod bearings and the lubrication tanks.

But my insecurity is fed by the fact that Märklin's chassis is dark nickel plated, but the new rod is bare and will therefore stand out clearly. Will that look appealing? But there's still some time until then, because more work has to be done now.

Partial disassembly of the Märklin locomotive

In order to be able to do the necessary preliminary work on the model as well, the locomotive must be partially dismantled. The housing must be removed, otherwise, the ladder cannot be removed.

To do this, loosen the retaining screw with which the hood (bonnet) is fixed to the chassis block from above.

However, it can only be accessed if the plastic cover fitted over it is removed. It reproduces the longitudinal flaps through which the engine was removed when the prototype was opened.

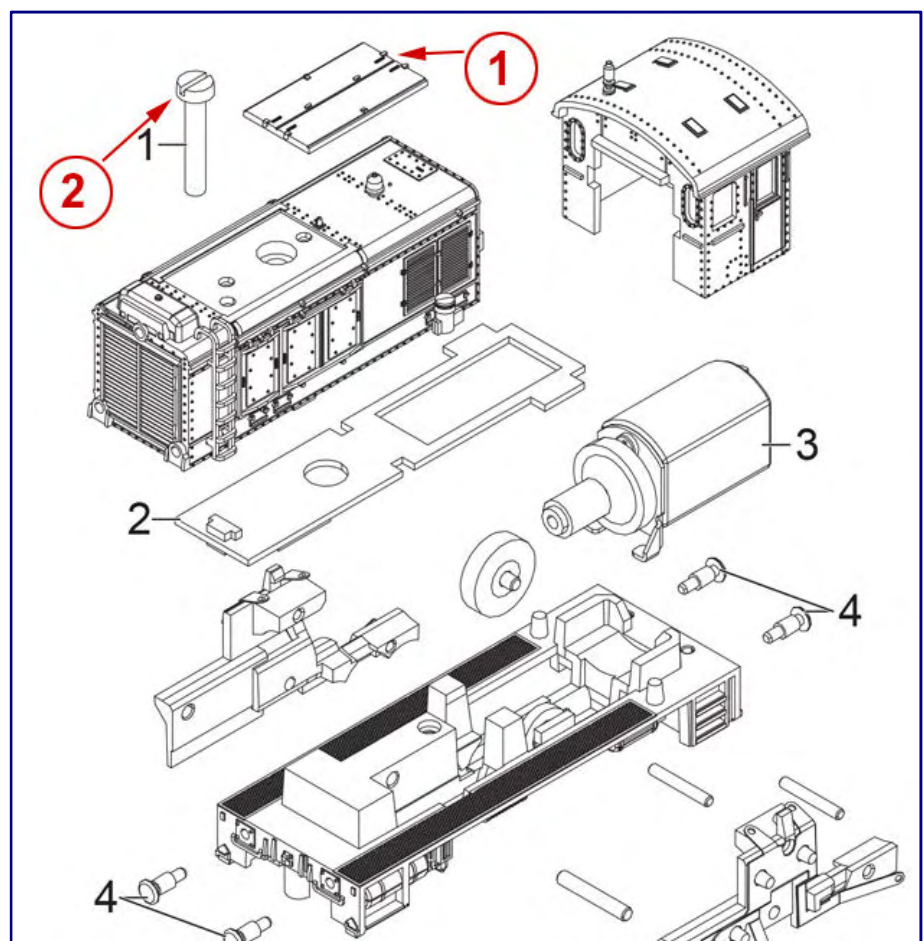
Unfortunately, it sits very tightly and can only be removed with a lot of sensitivity and patience.

Tools are out of the question, because they would have to be very flat to get under the edge and would leave scratches on the paint, or even grooves if they slipped.

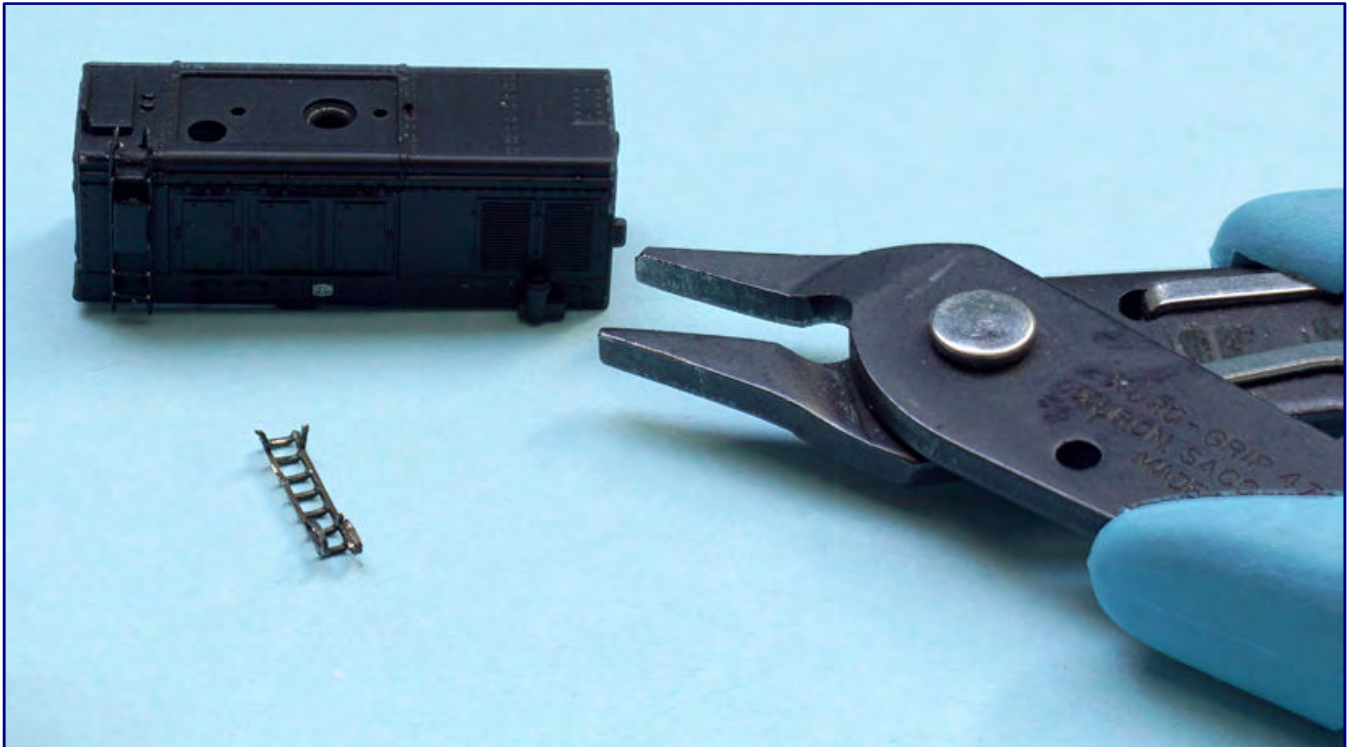
Therefore please do not use blades or screwdriver tips! With a fingernail, it takes a little longer, but the necessary fingertip feeling is maintained in the truest sense of the word.

If one end only lifts a little, you can carefully pry it until the cover inserted into two openings can be pulled upwards. The screw is then accessible and can also be loosened.

Inside, I discovered another disturbing factor with the LED board, and found that it is tightly glued to the case together with some foam and the lower light guides. But this construction can be easily removed and later fixed again by lightly pressing on it.



The extract from the exploded view illustrates the assembly principle and thus the individual steps of disassembly: In the first step, the flap replica of the hood must be lifted off in order to reach the housing screw. This can then be loosened in a second step to unhook and remove the complete engine hood from the driver's cab. Illustration: Märklin



The disassembly of the stamped ladder (picture above; lying in front of the locomotive housing), with which the model was delivered, is difficult because the inside bent ends are difficult to prise open. Once this has been done, a dry fit test is carried out, which can show any necessary readjustment or re-drilling of the receptacles. Once the new ladder is mounted (photo below), its ends are also bent to the inside, and additionally fixed with a drop of glue.

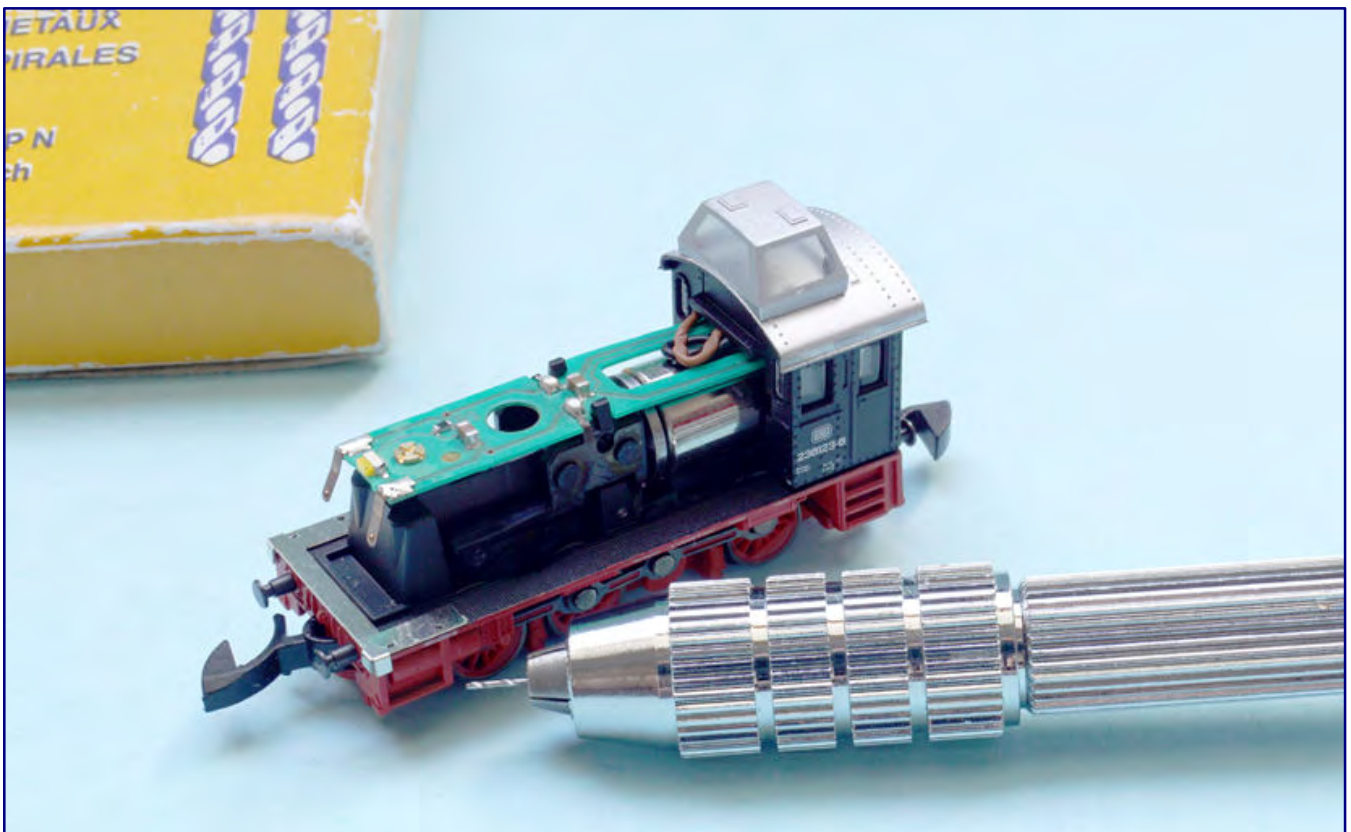
Somewhat annoying is the fact that the four brackets of the ladder, which are bent inside, cannot be prised open. They are too tight to reach underneath. I have no choice but to pry from the outside to put some pressure on them, and at least bend them a little.

With regard to the sensitive varnish, only a toothpick or a comparable piece of wood remains as a tool. Better sacrifice a few toothpicks than ruin the expensive model! With patience this step is finally successful and you can go on with the "reconstruction".

Face-shaping assembly work

The assembly activities take place parallel at the chassis, on which otherwise only the driver's cab sits, and at the bonnet. I would like to start with the installation of the manoeuvring rods. Ronald Schulz has drawn a drilling template and integrated it into the etching sheet so that they can be placed exactly at the same height on both sides of the vehicle.

This auxiliary tool has a positioning angle at the front end. Four flaps of the sheet metal are bent around 90° with the bending pliers, in each case two of the short strips then together form a corner. We place the tool on the front circuit board of the locomotive, and hold it there precisely in the correct position.



The drilling template can be placed precisely thanks to the angled corners. When drilling the holders for the holding rods, however, it should also be fixed with light finger pressure.

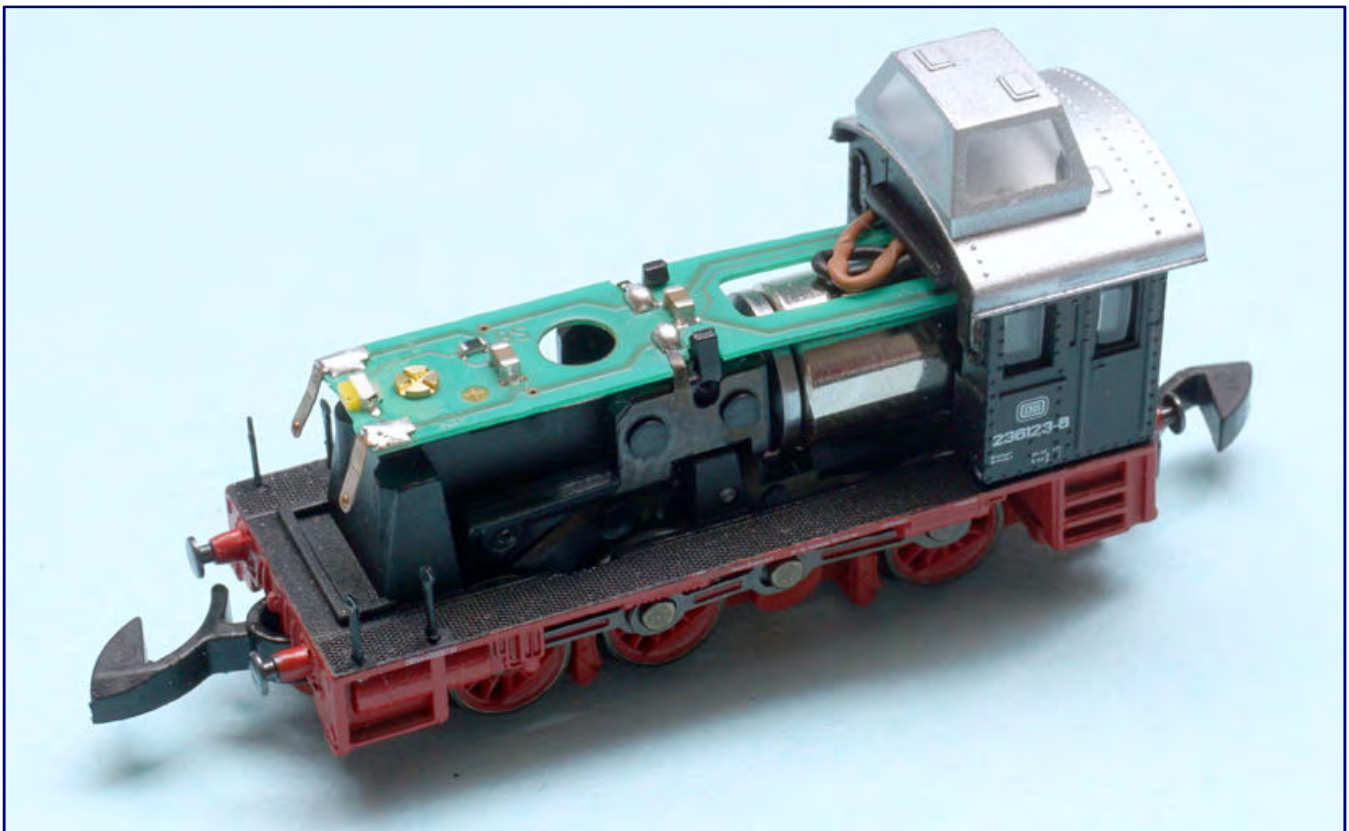
However, it still doesn't work completely without pressing with one finger, but somehow the model and drilling template have to be held with one hand anyway, while the other one guides the pencil vise with a 0.4 mm HSS twist drill.

Four holes are to be drilled in this way. I have already removed a holding bar from the etching arc with the nail scissors in order to check the drilling depth repeatedly. After all, the circulation has only a small thickness. If the short shaft disappears up to the widened foot in the drill hole, the suitable depth is found.

Nevertheless, what should be avoided here happens during a single pass: The tiny drill goes through the hole and causes a scratch on the battery box underneath. Fortunately, this spot can be easily repaired later with a drop of RAL 3000 fire red and is then only visible to those who know of this mishap. The correction then takes place when the sleeves of the buffers are reworked in the same colour.

First of all, nothing stands in the way of gluing the shunter poles with a tiny drop of Uhu glue Super Strong & Safe. The selected adhesive is very resilient when dried and allows minor corrections for a short time. But it is also chosen because I consider it to be a more durable adhesive than pure cyanoacrylate (superglue).

During the drying process, a fit test of the already painted and folded ladder is carried out on the hood. While the factory openings on the left side show no problems, it hooks on the top side. Therefore I drill these two holes also with the still bordered drill. 0.3 mm diameter would have done it already, however, as I recognize later.



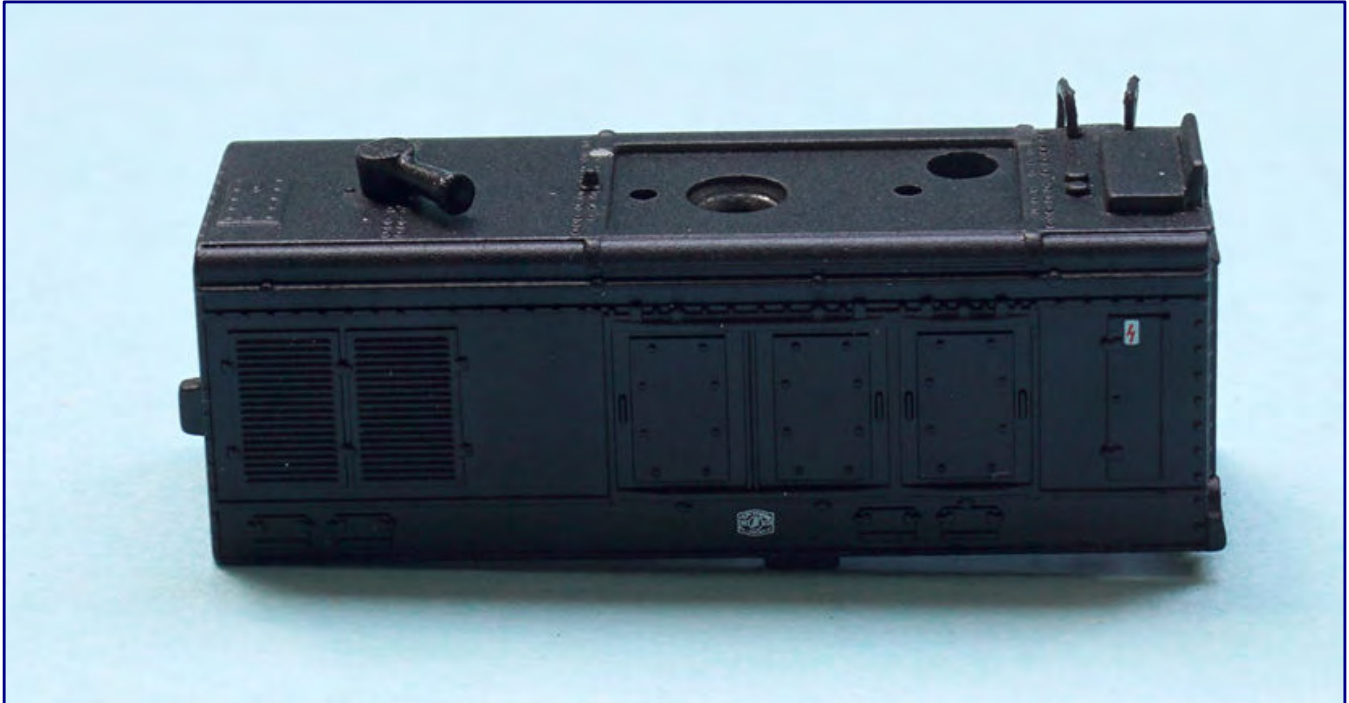
The four poles on the right and left stairs are inserted and glued. If necessary, their alignment can be readjusted as soon as the housing has been replaced.

Now the ladder is attached, whose inner ends are then bent over and glued with a drop of the already mentioned superglue, also from the inside. After the glue has dried, I will cover it with black paint on the inside of the openings, because Märklin has also done something similar ex-works, presumably to prevent unwanted light leakage.

Research requires the horn, but its exact positioning is still unclear. Photos show that it sits on the hood and points diagonally forward to the driver's side. However, the exact mounting position must be determined.

Unfortunately, there are no drawings to be found, so all other photos that can be found on the prototype locomotive and similarly equipped sister locomotives must be used. These photos are unfortunately as varied as the operating numbers of the locomotives in question: At least three different positions can be determined for V 36 119, 120 and 123, at first go.

Finally, a photograph from which the concrete model locomotive was photographed from an elevated position helps to achieve success. In the comparison to a side view, it shows clearly that the compressed air horn is mounted at the point where a bell should otherwise be, or was previously located.



Some photo searches require the correct positioning of the horn on the hood. Finally, it is correctly located in a 1.0 mm diameter hole just in front of the driver's cab. There it points diagonally right and forwards.

This is also obvious in so far as a compressed air line has probably already been laid there. The economical Bundesbahn will certainly have spared further rebuilding requirements and continued to use them. On the Märklin model, this location is easily recognizable by a row of rivets (interrupted in the middle). The angle of the inclination can now also be determined more accurately with the help of the side view: it should be exactly 45°.

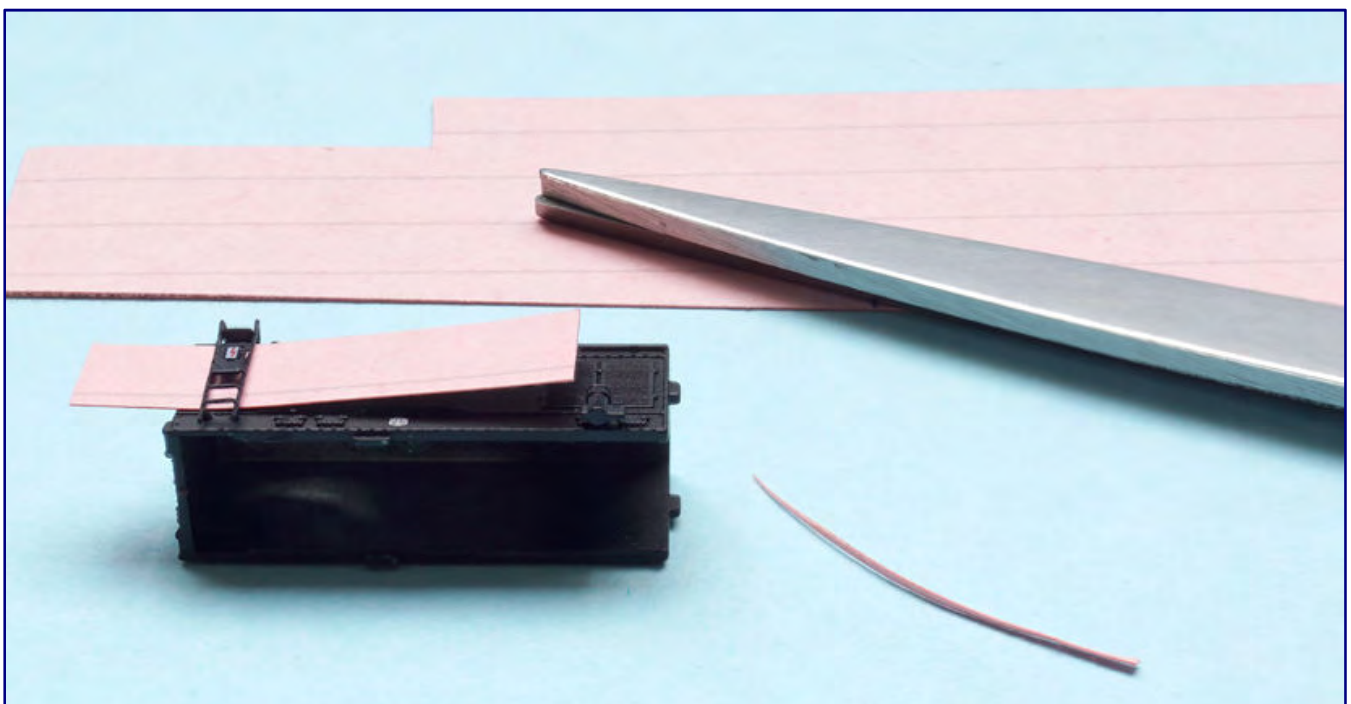
Next, I pre-drill the spot determined in this way with a diameter of 0.4 mm. This ensures that the drill does not run out and that the hole can be placed exactly in the middle. Then I widen it to 1.0 mm. Since the brass base does not yet fit properly and the horn cannot be inserted deep enough, I drill a second time with 1.2 mm diameter. The zinc die-cast part is of course worked on with the pencil vise again for the highest possible sensitivity.

After a successful fit test, I apply the Uhu glue Super Strong & Safe on the inside and insert the horn. Before the glue attracts and sets, I properly align it to the driver's side. The conversion in the area of the hood is also finished, and only minor touch up painting in this area may still be necessary.

The completed work

When the conversion work is already in full swing, Ronald Schulz surprises with a set of slides "8 Blitzpfeil-Schilder" (19.10.4.12.1), which he has commissioned not only for the parts set used here.

The company Hartmann-Originale (HaO) is responsible for these decals. Resolution and print sharpness inspire me, because they surpass the decals which I have used so far: The sign set shows the red high-voltage arrow on a white rectangle with just 1 mm edge length on the longer side, so that it can easily withstand the view through a magnifying glass with 10x magnification.

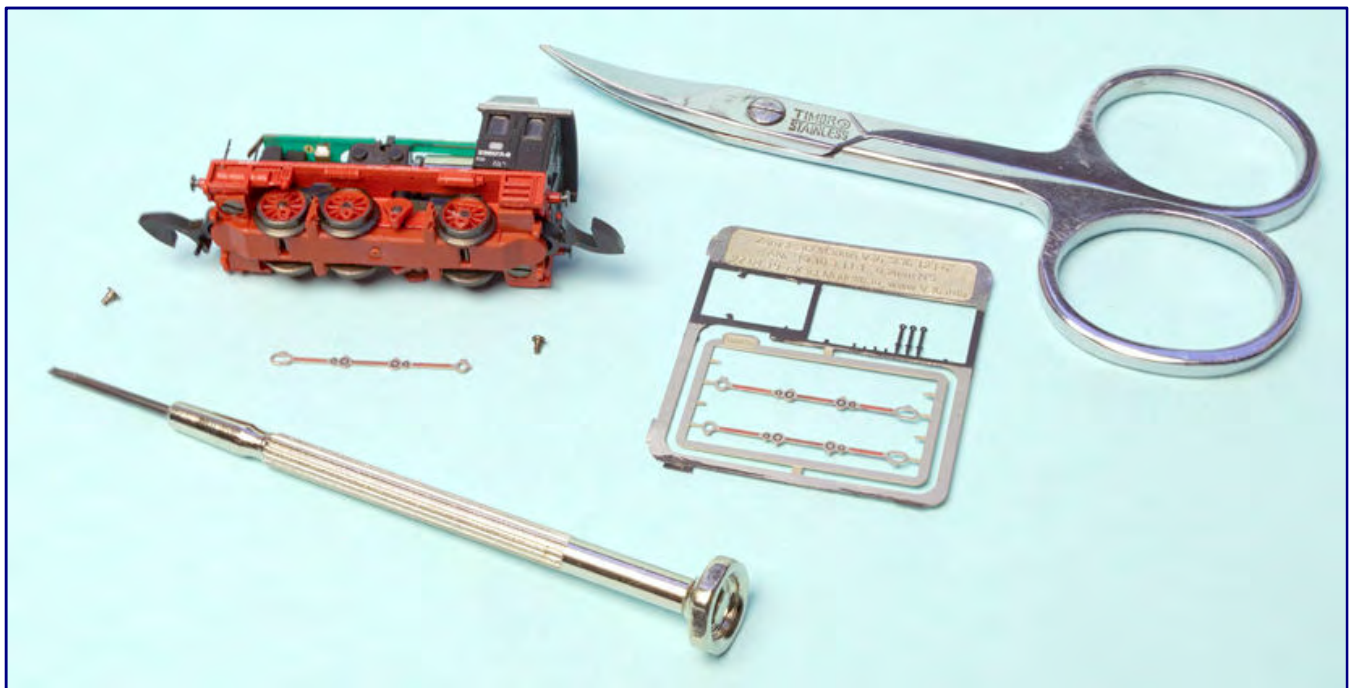


According to the original, a lightning arrow warning sign from Hartmann originals follows the protective flap of the access ladder (photo above). In order to protect it from damage and to seal it permanently, clear varnish is used, and the area to be worked on is masked. Below the ladder, a paper strip cut to size covers the surface of the hood (photo below).

It has also been thought through that the corners to be trimmed are marked with black printed angles at a small distance from the motif. White areas are otherwise very difficult to see on the light blue backing paper.

One of the eight arrows quickly finds its position on the ladder's climbing protection plate after the substrate has been prepared with adhesive fixer and the decal has been treated with plasticizer.

In a single operation with the coupling rods that have been reset so far, I plan to fix the dry decal permanently with silk matt clear lacquer. But I still have to come up with something to avoid fogging the entire case and possibly creating different degrees of gloss on the model.



The new, one-piece rods from the parts set are fastened to the wheels with only two instead of four rivets. They are not only finer than the standard four-part coupling rods, but their design also guarantees a straight seat at all times.

The solution is a narrowly cut strip of thicker paper that I can slide under the ladder. It is held in place by simply gluing it to the Tamiya masking tape that protects the outer areas.

This step is also done quickly and the factory parts can disappear from the chassis. Carefully the holding rivets are prised off, and only the outer ones are used again. The rest is put together with the now loose rod parts in a bag and stored in the model box.

Now, cut a rod for the right and left side from the etching sheet (the rear end is marked on the sheet) and assemble it. Before carefully reinserting the rivets, I check the correct position: Are all the rod bearings in the right position (coupling pins on the wheel)? Are the lubricating vessel replicas pointing upwards?

With the completion of this step, the V 36 has gained a much improved appearance. The degree of difficulty is kept within manageable limits, which is why even model railroaders with little experience can dare to try this fine detailing.

Sudden questions, such as the location of the horn, can always occur. This obstacle was also mastered without any major loss of time. In the end, the result always compensates for all the trembling that can occur.

The only tricky part was painting the air horn, which was difficult to fix in the clamp of the third hand because of its short foot. If it popped out, a long search would be required.



The appearance of the model has been noticeably improved once again with the horn, the new climbing ladder including climbing protection, the handrails on the ascents and one-piece coupling rods.

For similar components, it would therefore be worth considering providing this casting chamber for a slightly longer period in the future in order to be able to more easily access all of areas of the component and paint it completely with an airbrush. It is easier to pinch off the part later, than to clamp the short end. But if you work with a brush, you will hardly be able to understand this suggestion, because the painting will perhaps only be done after the parts are mounted on the model.

But be that as it may, the thoughts are already circling on a very high level here. I was dealing here with a kit that was reasonably priced and well thought out in terms of construction, and that did not miss its mark! Therefore, it is only logical to nominate it for the best new releases of the year 2019 in the category technology.



Fully equipped, 236 123-6 returns to layout operation and now cuts a particularly good figure in front of switching trains.

Basic model and reference of the finishing set:

<https://www.maerklin.de>
<https://www.shop.z-railways.de>

Pages of the developer:

<http://v36.info>

Aids and tools used:

<https://hartmann-original.homepage.t-online.de>
<https://www.oesling-modellbau.com>
<https://www.tamiya.de>
<https://www.uhu.de/de>

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The legendary “VW Bulli” Retired transporter

Like the Beetle, the Volkswagen Transporter, referred to internally by VW as Type 2, dominated the West German roads during the early years of the Federal Republic. These vehicles have long since reached retirement age, but their descendants are still omnipresent today. Inevitably, this legendary car became a common sight on model railway layouts as well - clearly noticeable also in the Z scale world. Let's have a closer look at the prototypes and their Z scale reincarnations.

The first prototype for the Volkswagen Transporter was built in 1948, and the new model went into production in March 1950. Regardless of which year is considered its official date of birth: This vehicle has long since reached retirement age.

For many years, VW referred to it internally as Type 2, because it was Volkswagen's second model after the VW Beetle (Type 1), with both designs sharing many features. The nomenclature is a bit confusing since everyone outside VW started the Transporter generation count with T1, T2 and so on. It was only from the 4th generation onwards, i.e. the T4, that VW itself also changed its internal nomenclature.



Its unique front design and two-colour paint scheme contributed to the characteristic look of the first generation VW Transporter. For a long time, the hinged double side doors were also part of the package, whilst the high bumpers were reserved for exported models.



The VW minibus came in many shapes and forms and not all of them were standard features. Many of the first and also second generation vans were particularly colourful as they became the vehicle of the hippie movement. The bumper slogan on this VW T1a is symbolic for the peculiar way-of- life of that generation: "Flashbacks happen". Photo: Marshall Astor (CC-BY-SA-2.0)

Right from the start, this small utility vehicle was a resounding success. Its close relationship to the Beetle, with whom it was sharing the same 25 HP Boxer engine, helped to keep costs as low as possible. With sales taking off, VW soon had to increase capacities and relocated its Transporter production from Wolfsburg to a new factory in Hanover on 8 March 1956.

The original idea for this bestseller is attributed to Ben Pon Sr., a Dutch VW importer. He is said to have proposed a light utility vehicle based on VW's Type 1 (i.e. the Beetle). The rest is history, with the Transporter soon dominating German roads in a similar way as its little brother. Its heydays were the fifties, sixties and seventies, but even today the current models are a very common sight.

The VW Transporter, in the German speaking world also known as VW Bus, Caravelle and "Bulli", has long since become a classic. Be it railway or postal services, craftsmen or hippies: they all drove it. Its most frequently used colloquial German name is "Bulli", which probably originates from a combination of bus and delivery van (in German "Bus" and "Lieferwagen"), and which at one stage was even on the shortlist for being its official product name.

In the English-speaking world, however, the nickname "Bully", never caught on, given the possible negative connotation of "bullying". Instead, it was often called "Splittie", alluding to its split windscreen.

One of the reasons why Volkswagen apparently did not officially go with the name "Bulli" was the fear of disputes with Heinrich Lanz AG, the manufacturer of the Bulldog brand for tractors. Moreover, the company Kässbohrer Geländefahrzeug AG also owned rights to the names "Bulli" and "Bully" for its snow groomers, for which it is a world market leader.



The minibus pictured here at a classic car meeting in 2009 in Zandvoort (Netherlands) is a VW T2b. These later versions of the second generation are recognizable by the short bumpers, which do not reach around the edges anymore, and turn signals which are positioned further up on the front side compared to previous versions. Photo: Alf van Beem (CC-Zero)

But branding issues did not stop the success of the Type 2, both on its home market and rapidly also on export markets. One of the main reasons for its success was versatility brought about by a range of varieties: minibus, panel van, emergency vehicle, camping version or as pickup with either single or double cab.

The VW Transporter's original design concept of an air-cooled and rear mounted engine survived up to the third generation and as such clearly outlasted the Beetle production in Germany. The T1 generation (1950 - 1967) still had hinged double side cargo doors, and sliding windows in the driver's and passenger's doors the side. Characteristic features were the decorative strips converging to form the V at the front with the large company emblem and the split windscreen.

With the T2 (1967 - 1979) the standard design changed to a sliding side door (previously available for the T1 from 1963 as an optional extra), windows that could be cranked down and one large windshield instead of a split screen.

The T2 also came with a much more comfortable cab, an improved brake system and more powerful engines, although we mention these features only in passing as our focus in a modelling context is more on externally visible features.

The switch to the third generation occurred in 1979. Produced until 1992, the T3 looked strikingly different from its predecessors.

Sporting a distinctly boxy shape, it was also bigger, more spacious and technically more advanced.

However, VW still held on to the rear mounted engine with rear transmission. Between 1981 and 1982, however, they were at least converted to water cooling.



The VW T3 with its boxy design was the last generation with rear-wheel drive, which was technically outdated but this did not diminish the great popularity of this model on the German market. Photo: Sven Storbeck (GPL)



The design of the VW bus has changed considerably over the decades - here a late T5 and to the right an early T4 – but it has always been popular with emergency services, here in the livery of the Deutsche Lebens-Rettungs-Gesellschaft (German Lifeguards).

The T4 generation (1990 – 2003) finally brought the Transporter into modern times. Having its engine moved to the front meant an end to its flat-faced design which had dominated the previous generations. The T4 now had a front-wheel-drive, but similar to the T3 from 1984 onwards, optional all-wheel-drive version was also available.

One of the big differences with the T4 Transporter was that it was available with two wheelbases and that it had - compared to its predecessors and thanks to its modified transmission concept - a cargo area which was of the same height throughout the vehicle, and which was significantly lower with the pickup version, thus offering a much better use of space.

The T5 (from 2003) and T6 (from 2015) generations merged seamlessly from one to another. On the outside, the Transporter has changed very little; the regular updates (2009 and 2015) then led to the new generational nomenclature at Volkswagen. The names for the different models are now Caravelle (minibus), Multivan (large-capacity sedan) and California (camper).

In Volkswagen's commercial vehicle segment, the van had and still has relatives against whom it has been and still is clearly distinguishable: above it the VW LT (1975 - 2006), known as the VW Crafter since 2006 and offering more space and a higher payload; and one size below the one-of-a-kind VW Fridolin (Type 147, 1964 - 1974), which had been designed for the German postal service.

A generational question

In view of its strong presence (not only) on Germany's roads and its high popularity, it is hardly surprising that the VW Transporter also plays a major role in the world of Z scale.

After all, the Transporter was and still is a vehicle frequently used by postal services and police, making it a part of everyday life and as such an almost indispensable accessory for model railway layouts. In the early decades of its long history it also had hardly any competition and was therefore also the typical vehicle of choice for craftsmen and service providers.



The VW Transporter arguably had its most glorious days during a time when the customer was still king. It was the ubiquitous vehicle of choice for craftsmen and customer services in German cities during the time of Germany's post-war economic miracle (and has been also playing a major role on many model railway layouts).

The honour of being the first ones to produce scale 1:220 VW Transporter models belongs to the former Swiss limited-edition manufacturer MZZ and Noch based in Wangen, Germany. Early on, both companies began providing modellers with very different varieties of VW Busses from two different generations. Over the years, other manufacturers followed suit, making it almost impossible to come up with a complete inventory of Z scale VW Transporters having been produced in the past or still in production today.

Due to the fact that car models in our scale tend to be much less catalogued and documented than rolling stock, we will structure our account of Z scale VW Transporters according to prototype generations instead of when a model was first released.

Since many model railroaders still focus on modelling Era III layouts (i.e. 1949 – 1970), it is not surprising that the first generation of the transporter is the one receiving the most attention from model producers and customers. There is probably no body type which has not been available at one time or another.



MZZ was the first manufacturer to produce a Z scale version of a Volkswagen Transporter. It was a panel van version of a VW T2a, and came as part of the package with the article number 3102 (left). Märklin, on the other hand, has two body styles of the VW T1 in its inventory of model moulds (centre and right).

Märklin has long had a T1 panel van version amongst its stock of metal casting moulds and has been using it many times as an accessory to its limited museum editions or its regular production runs of rolling stock. For example, a pickup version with tarpaulin was part of the “Sylt” auto transport train pack (81428).



An unpainted panel van from Märklin (left) displaying its fine engravings such as the VW emblem or the hinged double cargo doors. For comparison, two models with different body styles - “pickup (with tarpaulin)” and “high roof” from MWB’s VW T1 range.

It is worth to note here that the motorized rail car Klv 20 (model year 1955) announced by Märklin in Spring 2019, is based on the first VW Bus generation. An unmotorized version of this vehicle (based on a MWB model) was also available from Rail exclusive (Detlev Stahl, Art. No. BS-017).

During its many years of manufacturing, the first Transporter generation went through some external design changes that are not all immediately recognizable on the models: The first prototype vehicles did not have a rear bumper yet, but no 1:220 models of these types seem to exist.

The mechanical turn signals of the early models were replaced by turn lights due to changes in regulation and placed above the headlights. The roof was pulled over the front windows in 1955. Subsequently, the vehicles were also fitted with larger and oval tail lights. In August 1963, a wider tailgate with a larger rear window followed – all bringing the 1964 T1 model optically close to its successor.

Wolfgang Baumann (MWB) from Wesel probably produced the largest range of models of the first generation VW Transporter. Known models include a fir-green police van, a large-capacity box van (with high roof) and pickup varieties (including double cab) without and with tarpaulin. The models were mostly available in a fully painted “Exclusive” version and an unpainted basic version.



Wolfgang Baumann was the producer of the largest variety of Z scale T1 models. Here a selection of seven different body styles (from left to right): two door regular cab pickup, pickup with extended flatbed, double cab pickup, police van, pickup with warning light, covered pickup and high roof version.



The US manufacturer ZThek also repeatedly had car models in its range, amongst them a panel van version with two side windows in the front area of the cargo bay. The model we are aware of came in fire brigade livery.

What we could not find out is if this particular model was based on a real life prototype or a product of the manufacturer’s imagination. During that period there were many body shops which adapted standard vehicles to their customer’s needs, some close to the original body shapes and others completely remodelled.



Top left:
This unfortunately very small picture shows the fire brigade panel van variant from ZtheK with two side windows in the area of the cargo bay and pump trailer from Memotec. Photo: ZettZeit, Jens Wimmel

Bottom left:
The hinged side doors and the double-colour paint scheme suggest that this Limited model is supposed to resemble a VW T1, but its rather boxy and flat shape do not really correspond to the shape of the prototype.

Limized also came out with its own interpretation of a T1. Unfortunately, this two-coloured minibus appeared to be very boxy and lacked the typical curved front section. A special model is a pickup version presented by NoBa models at a railway modellers meeting in Sindelfingen. It is a scale 1:200 model, but allows for the installation of a motorised chassis for the Z-Car system.

The current Trafofuchs “Volkswagen Transporter” model (KL01), on the other hand, is a fine rendition of the prototype version. It clearly and without doubt represents a variety from the T1a series. What we could not establish during our research into the history of VW Transporter Z scale models was if there ever has been a model of the double cab version produced from 1959.

Rhythm in the blood

One of the most popular VW bus types ever was the “Samba“, or officially the “VW Bus Special Edition”. The most recent Z scale versions of the Samba are from Zcustomizer and from Raffaele Picollo, Genoa (Italy). Both models come with transparent windows, quite a unique feature amongst Z scale car models.



The Samba buses without rear curved windows from Zcustomizer (picture left) and Raffaele Picollo (picture right) are among the most recent additions to the portfolio of VW buses in scale 1:220. Both have opted for the most common prototypical paint schemes. Raffaele Picollo’s model comes also with decorative chrome strips, exterior rear view mirrors and illuminated headlights. Photo right: Raffaele Picollo

Most other 3D-printed or resin cast models are solid and therefore have painted windows. Before we continue with our report on other models, let us briefly provide some information about the prototype of the Samba.

The Samba bus was first released in 1951 and offered a number of comfortable extras and a larger number of windows for a better view, compared to the standard bus. Each side had four panoramic windows on edge of the roof as well as curved windows at the rear end. Both window types had plastic panes. This type was reproduced in the MWB range of VW bus models.

From August 1963, the rear corner windows finally gave way to a modified tailgate with a larger rear window. This design is generally less well known, probably due to the smaller number of models produced.

In the sixties and seventies, and even more than the Beetle, the Samba bus became a symbol of the hippie movement. But also many ordinary T1 and T2 vans were converted into richly coloured campers and served to travel the “Hippie Trails”, meaning east and south in Europe and direction West Coast in the United States.

Volkswagen was actually not pleased with this development. Its marketing managers were concerned about the possible negative impact on sales for their most important vehicles and their hitherto positive public image.

Turning back to the history of Z scale Transporter models, we now arrive at the Dutch 3D printing producer A2 Models, which offered or still offers T1 versions of a minibuss, a panel van, a pickup with a loaded ladder and a Samba bus in green, grey and yellow.



The hippies' desire to travel is also a frequent scenic theme for model railways, especially as the hippie times corresponds to the popular Era III model railway period. Raffaele Picollo gave two of his buses a hippie design (top left and right). Rolf's Laedchen also sold two different designs: a natural green VW T2b version based on a model from MWB (bottom left), here with matching figures from Trafofuchs, and a repainted Samba bus from A2 Models (bottom right). Photos top left and right: Raffaele Picollo

Although marked as a 1950 model year, these are more recent versions of the prototype. This is particularly evident in the "Special Bus" with its panoramic windows. This Samba bus has the correct number of windows, but the rear corner windows are already missing. It therefore corresponds clearly to the model years starting from 1963 (1964 model).

As shown on their website, all A2-Models interpretations of the Samba come with a roof that protrudes over the windshield and with turn signals over the headlights. Z scale accessories specialist "Rolf's Laedchen" has also jumped on the Hippie bandwagon.

Based on a MWB-model it offered T1 panel vans under the name “VW Bully sunny yellow” (10300.Z) and “natural green” (10301.Z). Although probably not suitable for travelling the colours of these vans were rather nice. Our images show the natural green version using the example of the successor T2.

For those wanting to their own paint job: give it a try, painting is not that difficult. Suitable inspiration is easily found on the internet. As soon as you have decided on a model, all you need is a steady hand, a particularly fine hairbrush and some paint. We based our own Hippie-Sambabus on a model from A2-Models.



A Samba bus, officially simply called “VW Bus Special Edition”, from A2 Models (left) meets a specimen of its successor generation, a VW T2b police version from of MWB (right).

A car grows up

Compared to its predecessor, the second generation VW buses were much more mature. Their Z scale incarnations are also among the most attractive, but also the most demanding models to work with, mainly due to the challenges of achieving a faithful rendition of its slightly curved front section.



VW transporters in the livery of the West German postal service: They were once a very regular sight on German roads and no period specific railway layout would be complete without at least one of these vehicles. In particular the high roof version of the T2b (far right) will still be remembered by many modellers. All four models shown here are based on MWB Basic models.

By the way, MZZ produced the oldest buses of this VW generation. They were initially sold as a package package together with either a Ford Transit (3101) or a Unimog (3102), in the first case as a pickup with tarpaulin, in the second as a panel van. Later, they were also sold individually at trade fairs without having been assigned a new article number.

Limized also came out with a T2a version of a body type different from what MZZ had in its range. As was already the case with the Limized T1 model, some of their T2 models also had front sections which were too flat compared to the original.

This case illustrates quite clearly that a fully precise downscaling does not always produce a successful and good looking model. Sometimes, it is better to under- or overemphasize certain characteristic features in a model in order to achieve a credible and prototypical rendition.

To some extent this issue also affected the "VW Camping-Bus" in silver (10330.Z) and blue metallic (10330a.Z) taken from the stock of the "Ideen-Magazin" and redistributed by Rolfs Laedchen.

These models also represent the earliest T2 body type, with turn signals placed at the bottom, a bumper which still was bent around the corners and steps for entering the driver's compartment.



As with the T1 models, Modellbau Wolfgang Baumann (MWB) was also the most prolific producer of second generation VW Bus Z scale models. All models shown here are T2b varieties: Panel van and transporter versions at the top and below a municipal utilities version, a Westfalia camper and a police version.

It was again Wolfgang Baumann (MWB) who produced the largest range of T2 models. For example, his panel van was temporarily available in the earliest design version as well as in the later version with shorter bumpers and indicators located at the top. The younger design was also available in a high roof version, once a typical feature of the fleet of buses of the West German postal service.



Encounter at Westerende station between a rail car and holidaymakers in a Westfalia camper bus (MWB model) on their way to the East Frisian coast. Soon they will have reached their destination and be able to cool down by taking a swim in the North Sea.

Police and fire brigade versions with signal lights on the roof were also available, together with versions painted in orange livery of municipal utility vehicles.

A unique model was the Westfalia camper van with its rather eye-catching design including a front mounted covered spare wheel. All these models were always available in basic (unpainted) and "Exclusive" (fully painted) versions.

Last generation with a rear engine

Compared to the T1 and T2 generations, the choice of available models of the T3 has been much more limited. Actually, we are aware of only two manufacturers who have come up with Z scale versions of the T3.

One is the model railway accessories manufacturer Noch which produced versions with transparent windows.

Right:
A rental car in front of the cemetery? Let's hope it's just a relative of a deceased laying flowers on the grave.





Noch produced several Z scale versions of the boxy Volkswagen T3. All of these, with the possible exception of the model to the far left, would not meet today's quality expectations.

The paintwork on the NOCH was rather crude by today's standards - sometimes quite thick and mostly too glossy – and rather poor imitation of lights.

The models came as VW transporters in a double pack (4732), in neutral yellow for possible labelling as postal service vehicles (4771) and as part of a two-part rental car collection "Interrent" (4772).

An optically more satisfying model of the T3 was produced by Limited. They made a high roof ambulance version, which had four signal lights. They also made a THW emergency vehicle with double cab, but that was a model of a VW LT 45 - the larger brother of the VW Transporter.



This version of a high roof T3 ambulance counts definitely amongst the better Z scale implementations of a VW bus. Unfortunately, this model is no longer available today.

Arriving in the present times

With the T4 generation, we are now entering the more recent past as well as present times. Whilst the older VW Transporter generations already fall into the classic car category, the T4 is still very much present on our streets.

The T4 was the first VW Bus which came with the option for different wheelbases, a feature which was taken up by MWB when designing its Z scale versions of the T4. The MWB product range also included various body types from the Minibus and Transporter



varieties. Some of the panel vans also had different roof heights and were available with either a tailgate or laterally hinged doors.



Different MWB models of the T4 generation with different rear doors and flaps as well as different wheelbases. The much more recent 3D print model from Zcustomizer now even has aluminium alloys and exterior mirrors (page 30 bottom right; photo: Zcustomizer, Sascha Braun).

Currently, Zcustomizer offers a panel van with well-designed aluminium rims. To the best of our knowledge, there are no further suppliers of scale 1:220 models of the T4 (1990 and 2003).

The situation is better with the current T5 and T6 generation, which are optically and technically very close and hardly distinguishable from each other at a scale of 1:220. NoBa-Modelle is one manufacturer and is run by a couple from southern Germany who are 3D-printing enthusiasts and who have produced models of the T5 as a Multivan (6207 / 6207R / 6208 / 6209R), pickup (6203 A / 6205) and camper motorhome (6204) with a roof box.

According to their catalogue, there is also an ambulance version available (6201), which does seem to be, however, a different Volkswagen Type (KTV).



VW T5 busses from NoBa-Modelle, available as ambulances (left) and multivans (right). Photos: NoBa-Modelle

For a layman, both may (seemingly) be identical, but emergency rescue vehicles typically have a larger and heavier equipment and the patient must be accessible from three sides. The VW Transporter is actually too small for these purposes and does not have the sufficient payload.

But some degree of artistic freedom and creativity is fine and enriches the hobby. Trafofuchs, for example, offers a camper mobile (KL20) with an angled folding roof which looks very similar to a Volkswagen T6.

Top-class model cars have been coming from Portugal for some years now. Álvaro Cortes produces with great passion and skills highly detailed models with many intricate features. New this year are replicas of the T5 and T6 as emergency vehicles for Germany, Austria and Switzerland (police and emergency doctor).



Álvaro Cortes launched a variety of T5 and T6 models only this year. Here several T6 varieties with individually designed wheels. Photo: Álvaro Cortes.



Currently, only few Z scale models of the VW bus are commercially available (here a T4 panel van from MWB). This raises the question about their future availability to modelling enthusiasts.

He also offers the VW T6 in different civilian versions (metallic paintwork and black), including a yellow version as a delivery van with a windowless cargo bay. Also available is a van version with a window in the sliding door and a panel van with a long wheelbase.



Remembering the good old and mostly quiet times with a VW T1 panel bus parked outside the signal maintenance shop.

Summing up, we have learned that there have been Z scale models for all generations of the VW Transporter. But the picture is heterogeneous and some types and generations have been better represented than others. Some models have not stood the test of time, and some manufacturers are not active anymore. We therefore conclude that "There is still a lot to do".

Pages of still active providers:

- <https://www.maerklin.de>
- <https://www.noba-modelle.de>
- <http://www.rolfs-laedchen.de>
- <http://www.trafofuchs.de>
- <https://www.zcustomizer.de>

Journeyman's piece for a masterpiece

On the North Sea coast...

...on the Platt (low) German beach. A diorama invites you to implement the finest details, which can only rarely be realized in such a way on facilities. They also offer an excellent surface for finger exercises and the rehearsal of new techniques. Dietmar Allekotte used the EM diorama competition to prepare an even larger work.

When our editor Dirk Kuhlmann pointed out a diorama competition of the railway magazine in the forum of the Z-Freunde International and we also called for participation in this magazine, Dietmar Allekotte from Mülheim (Ruhr) immediately knew that he wanted to participate.

For him it was a matter of trying out new techniques and creating a sample piece for his project "Marienfeld", which was to be tackled as a peep-box system. Where else can you try out, sharpen or rehearse your skills and working methods?



Dietmar Allekotte's diorama "Küstenland" shows a stretch of beach whose access is cut off by the railway line. Swimmers access the sandy beach via a pedestrian bridge.

Thanks to the North Sea motifs for "Marienfeld", the theme for the diorama Dietmar wanted to submit to the competition was also set: a stretch of coast with a beach. In contrast to the landscape shown, the title was simply "coastal land".

There was also no consideration to the basic dimensions of the showpiece, because these were binding on the part of the competition conditions. So the viewer had to be content with a base area of 15 x 30 cm - small for larger scales, but for Z gauge it was an area on which to let off steam.



The V 36 was still quite common in northern Germany at the beginning of the 1960's, and was always used in local transport before conversion cars, when the capacity of a rail bus was not sufficient during busy periods.

The start of construction

Now we had to come up with a plan before we could get started! After a short consultation with Dirk Kuhlmann he was determined and it went fast. Now our reader could prepare everything for the construction in no time at all.



Cork plate parts, Faller Styrofoam walls and a piece of woodchip wallpaper form the essential foundations of the later landscape. Photo: Dietmar Allekotte

Even in the run-up to the event, he had intensively discussed the works and working methods of his mastermind and idealistic pioneer Dirk Kuhlmann.

Dietmar tried not only to see the designed surfaces, but also to question and understand their emergence.

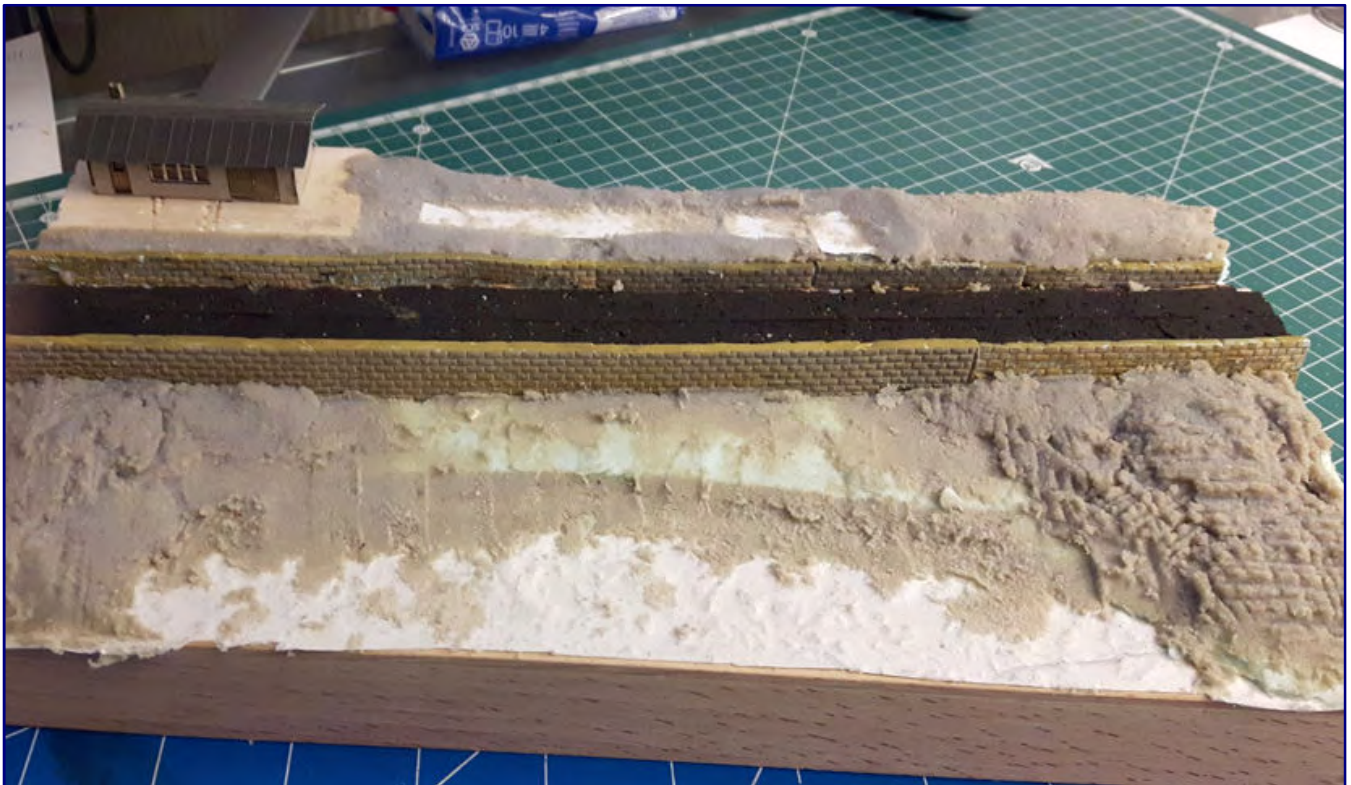
If such powers of observation are combined with one's own handwork, then this offers the best basis for outstanding results. And this has certainly been achieved in this case, as the accompanying photos also show.

However, the first to be sought now was a suitable board as a base. As chance would have it, Boesner found another painting base in exactly the right size. So we could start right away. He roughly drew the ground plans onto the board with a pencil and divided it into sections.

Dietmar wanted to proceed exactly according to the method he had read and meticulously followed in **Trainini®** construction reports. So he first laid out a piece of cork and in the area of the later water a piece of woodchip wallpaper.

This was followed by a test of the plan with the track, which was bordered by a strip of Styrofoam wall (Faller) once it was cut to size. Dietmar Allekotte thought this was the way it should work, and set out to give the landscape permanent structures.

As soon as everything was glued into place, he roughly preformed the terrain and covered it with the Molto-Reparaturspachtel (Repair Filler), which seems to have become a new model building standard material. The client spent the necessary dry season combing his sources in search of a suitable bridge. After all, there was a need for a safe crossing possibility for bathers seeking beach access.



After the rough landscaping, a coat of Molto Reparaturspachtel (Repair Filler) was applied, whose sandy structure was well suited to the theme of this diorama. Photo: Dietmar Allekotte

Didn't he have something from his friend Peter Rogel somewhere? But it didn't help. His find was too rough for the intended purpose and didn't really fit the motif. His ambition and his expectations, which had risen in the meantime, required more filigree parts.

And so he finally ended up with HOS-Modellbahntechnik, one of the recognized specialists, when it has to work really well and be especially fine. A short phone call and a few photos later, Heinz O. Schramm set to work and designed exactly the bridge that was on our reader's wish list.



For the pedestrian bridge, which some recreation seekers have now discovered for flying kites, Dietmar Allekotte finally resorted to a custom-made HOS model railway technology.

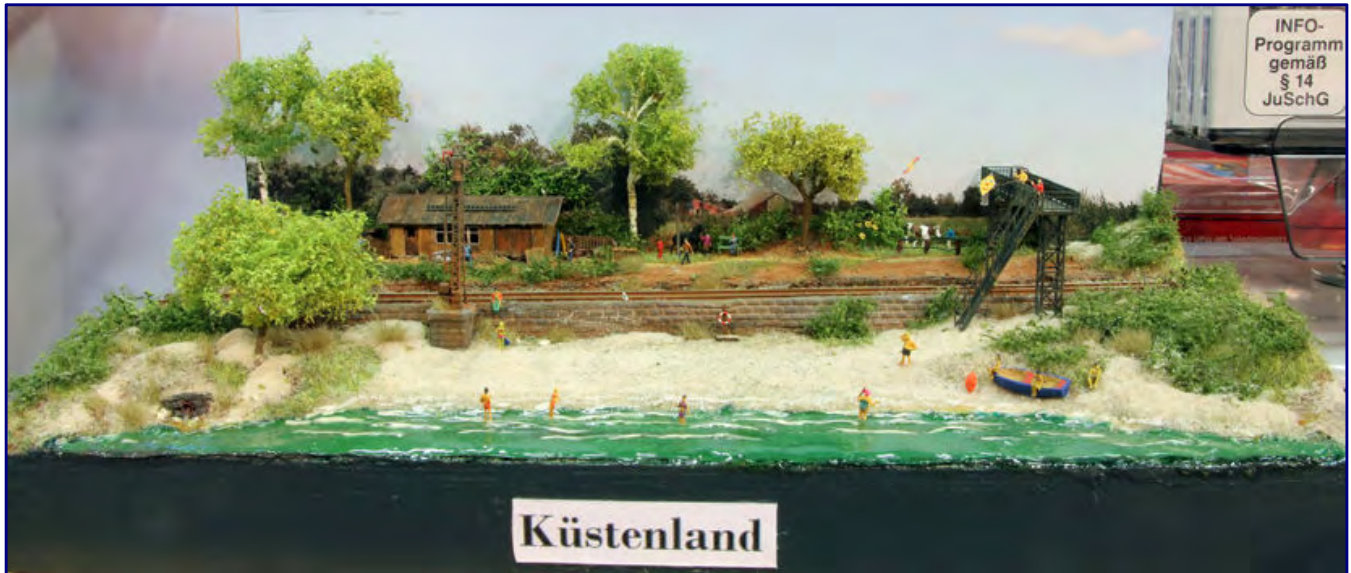
As a kit for the small locksmith's shop behind the railway line, however, he used a hut from MBZ, which was already in the kit. Once the first colours of the terrain had been applied, the necessary positioning tests could take place to bring the plan into line with the actual effect.

It all took shape, but somehow the designer didn't like it yet. Without further ado, the seawater area was extended to the outer edges. Another point was the number of details to be found here so that it would not be boring for the viewer. After all, the viewer has to be captured and captivated when panning over many dioramas in order to motivate the fair visitors to vote.



The first sand and paint were applied, then a one armed signal followed, mounted on a pedestal. Photo: Dietmar Allekotte

So Dietmar came up with the idea to position a one armed signal near the centre, for which even a defective model would be sufficient. After all, its electrical function was not required. Remnants that could be used to build a base were also found in the bargain bin. Another feature with a function questionable to the viewer was the drain pipe near the beach: what is discharged here near the bathers in the sea?



At general view, also drain-pipe (far left of picture) near beach is to see: What is discharged into the sea there?

Construction progresses

In the meantime, the terrain structure had been completed to such an extent that the first green bushes and trees and the sandy beach could already be added. After everything was in the right colour and the tracks were painted in the proper rust colour, which was indispensable for a credible effect, it was time to design the water.

Also here our attentive reader worked exactly according to the procedure described by Dirk Kuhlmann, which goes back to the former design expert Rolf Knipper. After the application of the paint for the sloping bank, which had been washed over by the tides, two layers of high gloss boat paint followed.

In the meantime, the bridge had already been delivered and required to be installed at the intended location on the right edge of the diorama. Meanwhile, the landscaping progressed with grass from Noch, which had been applied with a Grasmaster.

Thus the diorama "Küstenland" was already largely finished, but suitable figures were still missing. After all, it is these figures that make life noticeable. The order went to Trafofuchs and was solved by Birgit Foken-Brock to the full satisfaction of the contestant.

And so Dietmar Allekotte draws a satisfied conclusion despite missing an overall victory. We let him speak again personally: "This diorama was exactly the right project for me to test important things for my next plant "Marienfeld". For me, the construction reports in the **Trainini®** were like a gift. I remembered and tried a lot for myself."

Display

14. Modellbahn-Ausstellung Zeller Adventsmarkt



ACHTUNG!
Zeller Altstadt gesperrt -
Anfahrt nur über Bulley möglich!

ACHTUNG!

Zell an der Mosel
Zeller-Schwarze-Katz-Halle

Sonntag, 08. Dezember 2019, von 11 bis 17 Uhr
Veranstalter: Z-Freunde International e.V., 56856 Zell (Mosel)

Well then, for us it is also a gift to be able to share the creation and the finished diorama including experiences with all our readers.

Feel invited to share your ideas and work with us at any time. Tens of thousands of people all over the world are looking forward to working with you!

Motivation was provided by the following websites:

<https://eisenbahnmagazin.de>
<http://helenensiel.com>

Websites of the suppliers mentioned in the text:

<http://www.hos-modellbahntechnik.de>
<https://www.mbz-modellbahnzubehoer.de>
<https://www.noch.de>
<http://www.trafofuchs.de>



The small locksmith's shop behind the railway line comes from an MBZ kit. Embedded in the landscape, it is surrounded by lush greenery.

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Der VW Transporter im Wandel der Zeit **Auto-Legende – legendäres Auto**

Wenn ein Auto Geschichten und Geschichte schreibt, dann muss es etwas ganz Besonderes sein. Freunde des „Bullis“ werden dagegen nicht ihr Wort erheben, denn für sie ist der Transporter genau das. Mit ihm verbinden wir noch heute Wirtschaftswunder, Blumenkinder, Vereinsfahrten und Kegelausflüge, aber er war (und ist) so viel mehr. Wenn ein Auto zum Alltag gehört, dann kommen wir auch als Modellbahner nicht an ihm vorbei, dachten wir, und lasen dieses Buch.

Alexander F. Storz
Die Bulli-Legende
Als Bullis noch Samba tanzten

Motorbuch Verlag
Stuttgart 2016

Gebundenes Buch
Format 23,0 x 26,5 cm
192 Seiten mit 122 S/W- & 186 Farbbildern

ISBN 978-3-613-03883-7
Titel-Nr. 03883
Preis 24,90 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Der Autor des hier besprochenen Buches, Alexander Franc Storz, arbeitet als Journalist im Bereich der Automobilhistorie und ist für verschiedene Fachmagazine tätig. Die Liste der von ihm veröffentlichten Bücher erscheint fast endlos lang. Viele berühmte Automodelle waren darunter, hier ist es nun mit dem Transporter von Volkswagen ein weiterer Klassiker.



Mittlerweile erscheint dieser gemäß der Zählweise von VW schon in der sechsten Generation. Seine Liebhaber, die der Wolfsburger Konzern schon mit einem Microbus in Retrogestaltung zu einem Umsatz verleiten wollte, nennen ihn bis heute liebevoll Bulli. Das gilt freilich nur für den deutschen Sprachraum, wie wir in unserem vorausgegangenen Artikel erläutert haben.

50 Jahre Zeitgeschichte sind über den „Typ 2“, dessen Produktion wegen seines schnellen und großen Erfolgs rasch vom Stamm- ins neue Werk Hannover verlegt werden musste, gegangen. Geblieben ist sein Ruhm: In den Fünfigern symbolisierte er neben dem Käfer das Wirtschaftswunder, in den Sechzigern zeigte er dann eine wahre Modellvielfalt.

Die Siebziger erlebte er als Lieblingsgefährte der „Blumenkinder“, die sich für Liebe und Frieden aufopferten und dabei gern dem Drogenkonsum hingaben, in den Achtzigern schien er dann erwachsen geworden zu sein. Bis heute ist er ein akzeptierter und beliebter Transporter geblieben.

Der Autor nimmt seine Leser mit auf diese erlebnisreiche Zeitreise durch immerhin vier Jahrzehnte. Er erzählt vom Ursprung und der Idee zum Erfolgsmodell, das von einem niederländischen VW-Importeur vorgeschlagen wurde. Mit „Geschichte und Geschichten“ lässt es sich wohl am besten umschreiben, was Alexander Storz da im größten Kapitel des Buches ausbreitet.

Bis einschließlich zur Seite 121 des 192-seitigen Bands befasst er sich nur mit der ersten Generation des Bullis, der offiziell nie so heißen durfte. Er lässt den Leser erleben, wie dieser Klassiker auch modernere Konzepte von Konkurrenten wie Gutbrod, Goliath, DKW oder Ford abhängt und neben dem „Typ 1“ zum Symbol des deutschen Wirtschaftswunders wird.

Unterstützt wird all das von hervorragenden, zeitgenössischen Fotos, die anders als Werks- und Werbeaufnahmen den Alltag der Deutschen jener Zeit zeigen. Und da gehörte der VW-Kleinbus immer dazu: Sei es im Kundendienst, beim gewöhnlichen Handwerker, als Lieferwagen, Sambabus für Kleingruppenreisen, Lotsenfahrzeug auf dem Flughafen, bei Polizei, Post oder Bundeswehr, dieses Auto gehörte immer dazu.

Natürlich kommt auch dieser Band nicht ohne die genannten Werks- und Werbeaufnahmen aus, aber hier bringen sie vergleichend zum Ausdruck, wo und wie Anspruch und Wirklichkeit auseinanderlagen. Privataufnahmen zeigen das wahre Leben, das so nur deutlicher wirken kann.

Jahrzehntelange Normalität und deutscher Alltag sind es, die authentische Anregungen für die heimische Modellbahnanlage liefern und es ermöglichen, in den Geist jener Zeit einzutauchen, ohne sie als zu schön zu verklären.

Wer guten Modellbau wünscht und umsetzen möchte, der kommt an zeitgenössischen und unverfälschten Eindrücken nicht vorbei. Und so hat auch uns ein Buch beflügelt, an das wir uns sicher noch lange erinnern werden, weil es stets gute Dienste leisten kann.

Schön ist, dass es sich nicht am Transporter erster Generation erschöpft. Bis zum letzten Hecktriebler in Form des T3 reicht es und wirft dann kurz einen Blick in die Zukunft, die wir nach dem Ende der Lektüre auch längst schon wieder als Vergangenheit erkennen müssen.

Selbst das Ende vieler Fahrzeuge wird in Einzelschicksalen gezeigt, sei es auf dem Schrottplatz, nach Unfällen oder gar als Schafstall. Auch das ist wahres Leben. Kein Hehl wird auch daraus gemacht, dass die Begeisterung auf den Exportmärkten Ende der siebziger Jahre wegen des überholten Antriebskonzepts abebbte.

Volkswagen hielt dennoch auch in dritter Generation daran fest und wusste zumindest im heimischen Markt noch lange damit zu begeistern. Aber das macht Legenden aus: Sie sterben nicht, sie verblassen höchstens.

So lautet immerhin das Schlussfazit des Autors, dem wir uns gerne anschließen. Es spielt eben keine Rolle, ob wir den „Bulli“ mögen oder nicht. Wir sind alle in ihm oder mit ihm gefahren, haben Erinnerungen an ihn und kommen auch im Hobby nicht an ihm vorbei.

Deshalb lohnt es sich auch, ihn zu würdigen und sich mit ihm zu beschäftigen. Wer seine Anlagenbetrachter einfangen und fesseln möchte, der muss schließlich auch ihre Erinnerungen wecken und ihnen Bilder in den Kopf setzen.

Womit ginge das besser als mit diesem Auto, das (bis zum zeitlichen Schnitt im Buch) vierzig Jahre Geschichte begleitet und geprägt hat. Und wo wird das besser aufbereitet und dokumentiert als in diesem Buch? Wohl nirgendwo, denn es ist kein klassischer Technikband, der nur Puristen und Sachverständige begeistert, sondern ein lebendiges Fotoalbum.

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<https://www.motorbuch.de>
.....

Fahre lieber mit der Bundesbahn **Jugend auf großer Reise**

Was tun, wenn man Ende der Sechziger gerade in der Ausbildung ist, kein eigenes Auto besitzt und die Bundesbahn werbewirksam das Ende der Dampflok ankündigt? Da bleibt doch nur eines: Tasche packen, Urlaub nehmen, Route zu den letzten Fotostätten planen, Fahrkarten kaufen und mit der Bahn auf große Reise gehen. Genau das haben die beiden Autoren des an dieser Stelle besprochenen Buches getan. Und ihr Druckwerk ist nun so einmalig wie ihre wohl größte Reise.

Rainer Schnell / Helmut Philipp
Bundesbahn-Tour '68
Als die DB ihren Loks das Rauchen abgewöhnte

VGB Verlagsgruppe Bahn GmbH / Klartext Verlagsgesellschaft
Fürstenfeldbruck / Essen 2018

Gebundenes Buch
Format 22,3 x 29,7 cm
192 Seiten mit über 260 S/W- und Farbbildern

ISBN 978-3-8375-1996-9
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„Wenn jemand eine Reise tut, dann kann er was erzählen!“, weiß der Volksmund. Was in diesem Sprichwort an Wahrheit steckt, wissen die Autoren Rainer Schnell (Jahrgang 1949) und Helmut Philipp (Jahrgang 1948) zu berichten.

Gemeinsam mit ihrem bereits verstorbenen Freund Ulf Schleef haben sie im Juli 1968 eine elftägige Fotoreise per Bahn unternommen, um den letzten Dampftrössern in ihren noch verbliebenen Hochburgen aufzuspüren und sie auf Zelluloid zu bannen und für die Nachwelt zu bewahren.

Akribisch und mit Leidenschaft bis ins kleinste Detail geplant, wollten sie möglichst alle noch im Dienst befindlichen Baureihen – mit Ausnahme der noch an ihrem Heimatort Hamburg alltäglichen Maschinen – auf den Bahnhöfen und in den Betriebswerken dokumentieren.

Dass eine solche Reise quer durch die damalige Bundesrepublik angesichts der langen Reisewege und vielen Unterwegshalte bei gleichzeitig eher kurzer Reisedauer mehr eine Hatz als ein Erholungsurlaub ist, wird sich jeder wohl problemlos vorstellen zu können.

Glücklicherweise ist das den mitgebrachten und bewahrten Aufnahmen nicht anzusehen. Alle sind sie handwerklich korrekt, schön hinsichtlich Perspektive, Umfeld und Motiv sowie obendrein auch hervorragend im Druck umgesetzt worden.

Vielen ist durch die Körnung - den Farbbildern bisweilen auch durch das den Filmen typische Farbspektrum - das Alter von inzwischen fünfzig Jahren anzusehen. Aber das ist hier kein Mangel, sondern ein hervorragender Beleg für die Authentizität solch historischer Dokumente. Wäre dies nicht der Fall, würde der Leser bei der Lektüre glatt Raum und Zeit vergessen.



So packend ist das Buch, wenn es erst einmal in die Hand genommen wurde. Die Autoren berichten voller Enthusiasmus von ihren Begegnungen mit technischen Dinosauriern, der Willkommenskultur in den damaligen Betriebswerken, sintflutartigem Dauerregen, einer wegen Nachtruhe verschlossenen Jugendherberge und einem Dienststellenleiter, der überhaupt nicht in die ansonsten so gut gelungene Reise passen wollte.

Die drei Berufsanfänger hatten nichts dem Zufall überlassen wollen und sich in allen Beheimatungsorten der Lokomotiven zuvor schriftlich angemeldet. Meist erhielten sie nicht nur einladende Antworten, sondern auch Aufstellungen der Fahrzeuge, die sie dort erwarten durften.

Dass sie auch den Maschinen des BZA Minden sowie dem AW Schwerte (Ruhr) einen Besuch abstatten wollten, erwies sich als Glücksfall, denn besonders im Ausbesserungswerk stießen sie auf eine leider nicht erhalten gebliebene Sammlung längst ausgemusterter Relikte, darunter die Baureihen 17 und 62.

Und in Minden erfuhren sie wenigstens noch, dass sie 45 010 nicht abschreiben mussten, weil sie nicht ausgemustert, sondern zusammen mit ihrer letzten Schwesterlok kurz zuvor nach Mühlendorf umbeheimatet worden war. Eben das führte dann noch zu einer kurzfristigen Planänderung. Wo der Regen in die Quere kam (Köln), konnte der Bildbestand immerhin später noch bei einem zweiten und dritten Besuch aufgefüllt werden.

Doch was macht dieses Buch so besonders und hebt es von den vielen ähnlichen Werken über diese Zeit ab? Zum einen ist es die Zeit der Revolution, denn die dem nach dem Reisejahr benannte Bewegung, sorgte gerade für besondere Unruhe und Nervosität in den Amtsstuben und Polizeidienststellen. Eine Revolution in verkehrlicher Hinsicht stellte auch das dar, was sich parallel auf den Schienen abspielte.

Während die Bundesbahn ihren Lokomotiven gerade werbewirksam „das Rauchen abgewöhnte“, kamen die Elektrifizierungsarbeiten langsam zu ihrem Ende und die V-160-Familie setzte mit Versuchslokomotiven zur elektrischen Zugheizung zum anstehenden Höhenflug an. Zugleich war es das Jahr der Umnummerierung: Alte Baureihenbezeichnungen und Computernummern waren für kurze Zeit nebeneinander zu sehen und zu erleben.

Das Glück des Lesers ist, dass die drei jungen Fotografen all das in großer Voraussicht miteingefangen haben. So richteten sie ihre Kameras nicht nur auf die geliebten Dampflokomotiven, die schon bald verschwinden sollten. Vielmehr dokumentierten sie alt neben neu und opferten ihr teures Filmmaterial auch für Stangenlokomotiven der elektrischen Generation, Prototypen der E 10⁰, Dieseltriebwagen, Einzelgängerinnen und Raritäten oder sogar der hochmodernen E 03. Auch die V 36 ist mehrfach dabei.

An elf Tagen im Juli 1968 haben sie auf 3.000 Kilometern Reisestrecke ein fast repräsentatives Bild dessen eingefangen, was die Bundesbahn ihren Kunden in einer Zeit des Umbruchs anbot. Dagegen erscheinen andere Bücher leider häufig wie Zerr- oder Wunschbilder.

Insofern fasst der Rezensent seine Eindrücke und unsere Begeisterung wie folgt zusammen: Einmal zu lesen begonnen, mag ich dieses Buch nicht mehr aus der Hand legen! Die vorliegende Bilddokumentation lässt die Hektik der Reise glatt vergessen. Kurze Erläuterungen zu ausgewählten Themen oder Baureihen krönen das gelungene Werk.

Vielseitig wird das Buch durch die vielen Gegensätze zwischen moderner und Dampftraktion, alten und Computernummern, aktiven Maschinen und solchen auf dem Denkmalssockel. Und genau das hebt es auch von vielen anderen Veröffentlichungen zu vergleichbaren Themen ab und macht den Unterschied!

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<https://shop.vgbahn.info/vgbahn>

The Herpa Summer Festival 2019

The sun smiles for the anniversary

The 20th summer festival of Herpa was under a good star, because the weather could not have been better. And so a record number of visitors arrived to literally hike through an equally record-breaking exhibition. There was also a lot on offer for children, making it an event for the whole family. And, of course, Trainini® was also there.

Every year the accessories manufacturer Herpa invites you to its summer party in Diethofen. On a Saturday at the beginning of July, the small community in the Ansbach district near Nuremberg then becomes a pilgrimage site for friends of model cars, small aircraft and above all large trucks in a special design.



The calm before the great storm: In Diethofen, preparations are being made and set up at the Herpa headquarters for the 20th summer festival. The next day, 11,000 visitors celebrated Herpa's 70th anniversary here in the best weather possible.

This year the festival took place on 6 July and had a special occasion: it was the 20th edition and the Herpa brand celebrates its 70th anniversary in 2019. So the open house was allowed to be much larger than usual.

An organiser must always be concerned about the weather at meetings of this kind, which take place predominantly outdoors. But that also played along and the sun laughed all day long about Diethofen. It was almost too hot to stay outside for a long time.

But that didn't scare the guests: Herpa was able to welcome around 11,000 visitors, almost twice as many as at many a previous summer open houses. And a colourful and exciting programme awaited the guests.

This ranged from manufacturer presentations, musical performances on a stage, painting demonstrations with an airbrush, to culinary delights for the well-deserved breaks.

The cake buffet with a large selection was once again donated by company employees. The proceeds from the sale are traditionally donated to charitable causes.

An integral part of the summer festival is always the Herpa Museum, in which the company's history is presented and where long-term cross sections of the programme are shown.

Those who went through the individual rooms here attentively, and then also took part in a production tour were able to answer most of the questions in the competition.



That's what winners look like! Three winners of the Herpa Rally were each presented with a shopping voucher for 100.00 Euro.



Painting demonstrations were also part of the event programme. Decorative clothing with colourful motifs could also be purchased afterwards.

At this summer festival rally there were three vouchers for 100 Euro to be won, which could be redeemed at the museum's factory outlet. In addition to products from the current program, there were also rare pieces to buy: On a rummage table, samples of earlier models, which Herpa wanted to part with, were again waiting for buyers.

With a single SE-210 Caravelle in LTU paint, it included a 1:200 scale aircraft model from 2006, which would certainly be an eye-catcher on any layout or diorama because of its striking colour. For friends of the H0 gauge, a bargain sale at the edge of the Herpa administration building was recommended, which included current as well as older miniatures.



The Sud Aviation SE-210 Caravelle 10B1R from LTU (Art.-No. 551496) is a 1:200 scale Wings model from the first half of 2006 and was one of the rummage table finds in the Herpa Museum at this year's summer party.



Numerically and optically, however, the templates for many Herpa models of the scale 1:87 dominated the summer festival.

The organisers have provided around 100 showcases in elaborate paintwork, ranging from a simple high advertising effect to sprayed-on surface works of art.

They were placed on surfaces of the work and at the roadside. There they occupied several hundred metres to the border of the neighbouring town of Leonrod, where a large meadow area had to be included as well.

Photo left: Renault trucks are in the minority among viewers. Therefore, the "Green Mamba" stood out even more among the exhibited vehicles.



Photo above:
The Heide-Logistik from Lower Saxony has a number of show trucks in its fleet and is one of the regular guests at the Herpa summer festival. And so the "Hanse pirates" were just one motif from a very appealing series.

Photo below:
A well-known Zettie, Torsten Schubert, is responsible for the effective implementation on the basis of Herpa models. This motif is called "Ghost Rider".

There had never been anything like this before in Diethenhofen and was certainly the biggest visitor magnet for the company's 70th anniversary. For children it was all the more attractive as some trucks and the company's own historic VW Beetle also provided them with an opportunity to take a ride along the closed Leonrodstraße.



With the support of Conrad Electronic, Modellbahn-Union, Noch and Uhu, **Trainini®** had set up its first stand at the summer party: Here, children were able to design a beer mat with a small landscape according to their wishes with expert help: The choice included scatter grass, groundcovers, small plants, trees and figures. Those who wanted could also purchase a Herpa miniature car and equip its lid as a street.

If they and their parents had had enough of the big vehicles, they could also admire some of them as miniatures. As in previous years, Torsten Schubert was also present and showed his elaborately illuminated models including some finely designed Z-scale dioramas on which he also used Herpa products.

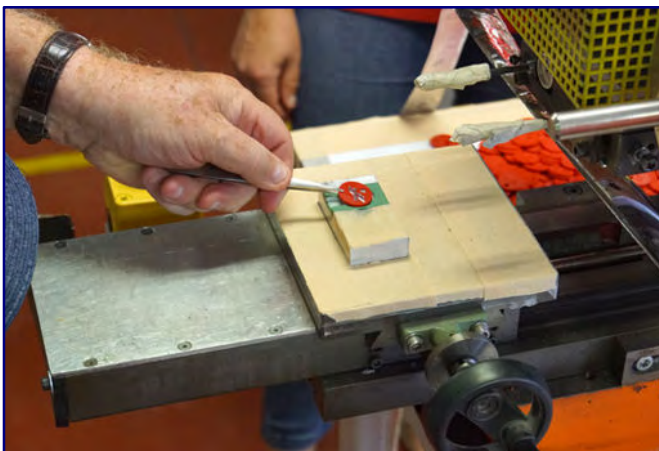
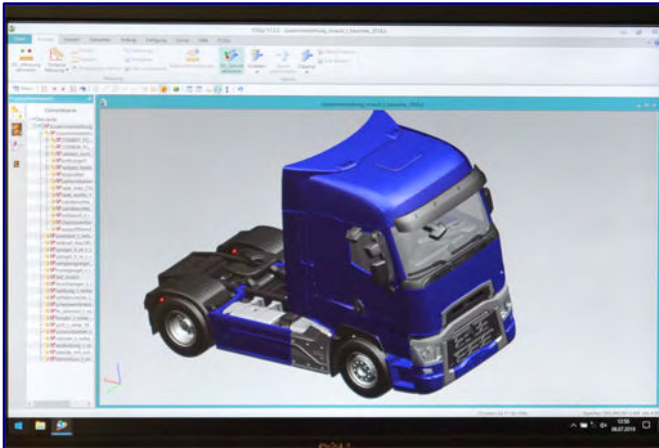
But the youngest were also awaited by many opportunities to participate and craft tables. They could, for example, assemble houses with Faller and Auhagen kits, assemble a Herpa car or, with **Trainini®** design a beer deckle (mat) with these articles. These proven actions enjoyed again large Zuspruch.

The diorama "Auf der schwäb'schen Eisenbahn" (On the Swabian Railway) from **Trainini®**'s younger generation served as proof that model making is child's play. It should have a motivating effect, challenge the creativity of the children and allow them to create their own designs.

On the other hand, the dioramas shown by other groups with vehicles in H0 size or the airport dioramas presented by Frank Meisen in 1:500 and 1:200 scales, which demonstrated equally great skill, were more aimed at adults. What comes out of it, even if one thinks a little more unconventionally, was shown by various bags made from the life jackets of airplanes.



There was also a little model railway to see: At the **Trainini®** booth the children's diorama "Auf der schwäb'schen Eisenbahn" was shown, which is presented in the current Märklin magazine (photo above). Frank Meisen showed excellent aeroplane dioramas in 1:500 and 1:200 scale (photo below) according to civil and military models.



A brief look at Herpa's production:

We skip the injection moulding and the assembly, because we had already shown them using Märklin as an example a few years ago, and look at the construction screens (top right photo). Every model today has its origin on the computer that replaced the drawing board many years ago. Shiny chrome parts and window frames don't get their colour from varnishing or printing, but from foil embossing. There are a number of colour ribbons of different shades to choose from (top right photo).

Foil embossing on the injection moulded blank takes place at a temperature of 250°C. The finished part is then removed with tweezers (bottom left photo). Finally, we show some injection moulded raw parts, embossed with chrome foil and a finished car model that has completed all production steps (bottom right photo).

And it is precisely these "weird" approaches that keep us model builders and railroaders moving forward and discovering new ways! If you want to be there next year, you should, at least, mark 11 July 2020 on your calendar.

Webpages of the organizer:

<https://www.herpa.de>

Exhibitors for Z gauge:

<http://www.airport-diorama.de>

<http://www.z-lights.de>

Bags made of life jackets:

<https://www.bag-to-life.com>

Eisenbahnmuseum Dieringhausen



Einzigartig in Europa

*Spur Z Ausstellung
im historischen Güterwagen
im Eisenbahnmuseum Dieringhausen*

Die Sonntags-Ausstellungstermine:

*08.06.2019, 09.06.2019, 16.06.2019, 07.07.2019,
28.07.2019, 11.08.2019, 22.09.2019, 13.10.2019,
27.10.2019*

jeweils von ca. 10:00 Uhr – ca. 17:00 Uhr

Infos unter: www.stammtisch-untereschbach.de

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

To the article "Specialists for fine work" (Trainini® 6/2019):

I do not wish to withhold Mr. Theis's answer to my question (...)!

With the same performance of Microgras Pen (81 01 1010) and Pro Gras Micro Applicator (81 01 1001), (...) the Pro Gras Micro Applicator (81 01 1001) with the integrated power supply seems more advantageous for me because working under difficult conditions on a system would be easier and the risk of damage would be reduced:

- no disturbing additional part of the power supply, which would have to be placed on or directly beside the layout
- one cable less above or around the construction site
- easier working possibilities near buildings or in already partially completed landscapes.

Now all I have to do is watch how I manage to get this device with reasonable effort ...

Jochen Brüggemann, per E-Mail

Editor's note: In order not to rip our reader's input out of context and make it more difficult to understand, we also give the decisive feedback from Microrama in the exact wording: "The Pro Gras Micro Applicator (81 01 1001) has a 9-volt block battery as its energy source. The two grassing devices (81 01 1010 and 81 01 1001) have the same power. The battery is housed in the applicator arm".

We received the following reader's request:

First of all many thanks for the always very interesting free magazine, which is much anticipated by me every month.

Right now I'm hanging something in the air. The speed controller from Jörger that I used earlier is no longer produced and the FR-1 from Passmann is also no longer available.

So how can I let the new bell armature motors run nice and slowly? In my opinion, there is currently only the hot grinder SFR-1500 on the market for this purpose. But I don't want to buy it before the electronic controller kit from Noch (88163) is on the market. According to Noch, it should be available from September.

(...)



I'm sure you can imagine my request now. I would be very happy about a detailed test in one of the next issues of Trainini. A comparison with the Heisswolf SFR-1500 would also be nice.

Bernd Severloh, Flensburg

Editorial response: We have already directly replied to our reader, and, hopefully, been able to help him. It's true that both controllers mentioned are no longer available from System Jörger and Passmann. However, we will not be able to carry out the desired comparative test, as there is no access to at least one of the devices or to a suitable measuring tool to display and evaluate the transmitted current pulses.

Since our reader's expectations are based on the operation of bell armature motors that do not require pulse width modulation (PWM) and could even suffer damage (shortened service life), we have recommended the Heisswolf products, which allow locomotive-specific and storable settings. As far as quality is concerned, we rely on Bernd Heisswolf's acknowledged good reputation and third-party test reports.

Error correction on one's own account:

On page 57 of the July issue of Archistories' Kallental fire station (for the 1zu220 shop) the copyright notice for the product photo was missing. As many readers may have noticed, this photo was also taken by the manufacturer himself, which we would like to pass on for your information.

In the report about the DB-Museum Nürnberg we made a mistake which could be corrected in the English edition (International Edition): On page 51 we have written in connection with the role of the railway in the industrialisation of the late 20th century. Of course, it must read correctly 19th century!

An autumn novelty blabbers away?

In the current issue of "Züge" (issue 4/2019) you can read that the Märklin 101 064-4 advertising locomotive, which we presented in the prototype, will be available in H0 and Z gauges for the 160th anniversary of the company. While it has already been announced on a larger scale, the Zetties were waiting for the appropriate information.



Since Klaus Eckert, who is present at the application of the films in Dortmund, is responsible for "Märklin-TV" as well as "Züge", and is, therefore, close to the Göppingen manufacturer, this announcement should be valid, as long as it was not a printing error.

Since such an anniversary model should sell well, especially for the core brand, and since the 101 series has only recently received a product update, we do not want to assume that it is an error.

Autumn new products for Nothaft:

Andreas Nothaft (<https://www.modellbahndecals.de>) and his drawing team were also busy again and created many new lettering sets over the summer. Two of them, which should also be interesting for the scale 1:220 and will be scaled accordingly on request, are presented here.

First of all, there are several new locomotive numbers and coats of arms for SBB Ae 6/6 locomotives, among them the 11469 "Thalwil" (Art.-No. 11469). The EHG (6600) marking sets for pig iron cars are available in six different versions for 12-axle and 16-axle cars. This should also allow Märklin's torpedo ladle cars to be customised on request.

Fine accessories from Artitec:

The Dutch manufacturer Artitec from Amsterdam has developed into an important supplier of accessories in gauge Z as well. The Zetties are now eagerly awaiting announcements and new deliveries every year.

Shortly after the editorial deadline of the last issue the time had come and the first models for 2019 could be delivered. This is first of all the finished model of an AEG transformer (Art.-No. 322.021) as load for heavy trucks.

The fine model shows many details and sharp engravings, which make it clearly recognizable even from the usual viewing distance. However, special regulations of the prototype were also observed: the three insulators are correctly dismantled and fixed separately on the loading frame for vertical (standing) transport.



The AEG transformer (Art.-No. 322.021; photo above) and the horse with plough (322.023; photo below) are current Artitec new products, which are more than just successful and set standards in this design.

The cleanly painted and very realistic looking model is again made of resin. This also applies to the second new product, which almost took our breath away at the International Toy Fair: Although the horse with plough (322.023) cannot do without small metal attachments, it is so tiny and filigree that the observer instinctively holds his breath, so as not to inadvertently disturb this figure.

Artitec (<https://www.artitec.nl>) has created a unique selling proposition for itself here after selection and implementation quality, which the Zetties hopefully reward with good encouragement. Therefore, we would like to remind you that horse-drawn ploughs were by no means typical only in epochs I and II.

In the context of an article on agricultural machinery on a scale of 1:220, we had explained how the motorisation of agriculture slowly took off after 1945. Until well into the fifties, the cultivation of fields was still characterized by hard work for humans and animals. Due to a lack of sufficient financial resources, small farms in particular switched very late to tractors.

Cross-Scale Schrax New Products:

Schrax, the 3D printing accessories supplier, has designed a piping system that can be plugged in. Depending on the construction scale and application, it can be equipped with a variety of tasks: District heating pipe, waste water system, chemical plant or also as loaded goods. Therefore the parts with a pipe diameter of 9 mm are not permanently assigned to a single track gauge.



The pipeline system is to be further expanded and expanded. The new pipe sections are loaded here as examples on an H0 model. Photo: Schrax

The offer initially includes straight pipe pieces (68 mm length) and bends with branch angles from 30°, 45° and 90°. An extension of the assortment, among other things around branches and further diameters is planned. The assortment is to be found at <http://www.schrax.com> over following the path: Start page "gauge H0 - 1:87," "small parts," "building site accessories" piping system.

Digital-Infoday in the 1zu220-Shop:

The 1zu220-Shop (<https://www.1zu220-shop.de>) organizes a digital information day at its headquarters in 34431 Marsberg-Westheim, Kasseler Straße 7, on September 3, 2019.

From 10:00 to 18:00 a Märklin digital trainer will demonstrate the Märklin H0 demonstration system and answer all questions about digital topics. A seminar room on the 2nd floor of the brewery building will be used.

Also for Zetties in attendance is worthwhile itself, because Jörg Erkel and its coworkers will inform also about the digital enterprise in this scale of modelling. They will also show a Z gauge demonstration system with Märklin components (CS3plus control centre, busy detector and switching decoder). Also on display are the "Kallental" and "Schmiedehagen" systems known from several exhibitions, as well as the viaduct diorama.

Confirmed world records:

The Hamburg Miniatur Wunderland is proud to have confirmed or recognized two world records. With 315 new track meters, they increase their own world record as the largest model railway in the world. For



We continue with the Miwula on the other side of the canal photo: Miniature Wonderland.

the first time, the show layout was explicitly recognised as the largest performance to scale. After all, 1367.21 km converted real length can now be admired in the Speicherstadt (warehouse city, Hamburg).

The congratulations came just in time for the 18th birthday of the "Miwula". We would like to think that it has grown up in the meantime.

But what was told to the press sounds almost crazy and dreamy, but it is true: The show will be extended and will soon also be on the opposite bank with a bridge over the canal.

Since 25 % of them come from abroad, and seem largely the same for many European landscapes and cities, England was dropped as a new model and replaced by South America with Rio de Janeiro, Patagonia and the rainforest.

3,000 m² of new space is to be developed here. It is planned to invest around 10 million euros there and to open up a "new world" for visitors.

Central America, the Caribbean and Asia are to follow by the end of 2026. The first part is already under construction and is scheduled to open at the end of 2021. It is to cover 200 to 220 m² and will be built on site, as the work on it is already being carried out together with a model railway enthusiast family in Argentina.

News about Märklin deliveries:

Now Märklin's current edition of the Swiss crocodile Ce 6/8III (Art.-No. 88564) seems to have been delivered completely. The green "reptile" for the 100th anniversary of this locomotive type was reproduced as an operational museum machine with the company number 14305.

In keeping with the times, it rolls forward with an inwardly moved switching screw, bell-shaped armature motor, fine printing and lacquering as well as LED lighting, thanks to which the Swiss light change has now also been implemented. Worthy of the occasion is the genuine wooden case used as packaging, including a metal replica of the manufacturer's board and a small brochure on the history of the machine.



Era VI is currently being served by a two-piece wagon pack (Art. No. 82425) to extend the DBG track construction train.

And then, as this summer draws to a close, it's time again for the Märklin Magazine's annual car (80829), for which in 2019 the still young form of the open freight car Omm 52 will have to be delivered in the correct length. The model is painted in the usual magazine blue. Already in the last issue we had with our report the forthcoming delivery of the very successful Fischbauchbrücke (fish belly bridge; 89758), which has now been completed.

For the friends of Era VI there is a supply of the DBG (82425). With the yellow flat and self-discharging wagon, the track construction train for Deutsche Bahn AG, which was launched a few years ago, can be extended even further.

We had to wait a long time for the second car display (87408) with three passenger coaches each from the DB (Bpmz 291), SBB (Eurofima couchette car Bcm) and SNCF (type B9u).

The SNCF models are declassified, former 1st class Eurofima cars. Therefore they still carry the yellow stripe above the windows, but are correctly labeled for the 2nd class. A further distinguishing feature to the cars from the previous pack 87409 is the newer logo of the railway administration.

All models are excellently varnished, finely inscribed, individually packed and without train plates, which allows a generous use on the model railway without style break. Delivery of the revised BR 798 / 998 (88167) rail bus has only just begun. We will hold a short meeting as soon as larger quantities are available and we can evaluate it ourselves.

For the outstanding model of the Kittel steam railcar (88145), Märklin informed the dealers in an information letter that technical problems had been solved, but that the supply of open orders could probably only be continued in the 1st quarter of 2020.



The Uerdingen rail bus, which has been significantly upgraded in the context of product upgrades, is now also available as a two-part unit of the 798 / 998 series for Era IV (88167).

Also the Insider model 2017, a V 80 of the DB (88803), is even longer in coming than last communicated. Since there have been rumours recently that this model does not come from Märklin and the construction, tools or even the complete model have only been purchased, we publish here the complete answer of Märklin to our readers in exact wording:

“Unfortunately, we have to inform you that (the articles) 88145 and 88803 are subject to longer delays in delivery. 88803 had a serious tool defect, the elimination of which has a massive impact on our original production planning.

At the present time, taking into account a completely new planning of 88803, it can be assumed that the article will not be available until the third quarter of 2020. We very much regret this new, massive postponement. Unfortunately, this cannot be avoided.

But we assure all our customers that we are working at full speed on the processing of customer orders and will also fulfil them. Also, it is definitely not the case that at the V 80 there is any kind of cooperation with a company that announced this model several years ago.”

There is no summer break at C-M-K:

Also the couple Klingenhöfer (<https://www.klingenhoefer.com>) did not give themselves a summer break and has informed us about some new products. And they have it in them again.

Why only stage weddings? For the design of church scenes Creativ-Modellbau Klingenhöfer now also offers a priest at the baptism (Art.-No. ET02). The scope of delivery includes the person baptized, his mother and the clergyman. As premium figures they carry enormously fine paintings, the child was even coloured under the microscope.



Carpenters (art.-no. Z01; photo above left), priests at the baptism (ET02; photo right) and the raccoon on the tree trunk (TW14; photo below left) are only three of the current new products of Klingenhöfer. Photos: C-M-K

A few years later it starts for the father and the child with the handcart (VK01). This duo of figures will certainly attract many spectators. This also applies to the paramedic with a rescue dog (TH01), whose jacket and dog vest are an eye-catcher due to their warning effect.

Indispensable on building sites are the four figures of the carpenters (Z01), who struggle with heavy roof beams in their typical and very well reproduced working costumes. But nobody wants to get acquainted with the ram (TS12). This applies explicitly to the mighty horns of the ram, which also becomes impressive as a tiny figure.

More dainty is the little raccoon on the tree trunk (TW14), which is easily recognizable thanks to its black and white face mask. Its earliest European examples came from America, and have spread rapidly in

some areas, even displacing native species. They also like to plunder garbage cans in search of food. This also allows their use on plants according to European examples.

News and current position demonstrations of AZL:

At the National Train Show, the most important model railroad exhibition in the USA, in Salt Lake City, American Z Line announced new models and showed samples of well-known novelties. At first, there was an eleven-piece "California Zephyr" train car package made up of various 1952 passenger coaches from the CB&Q, D&RGW and WP railway companies. In addition, there is a supplementary single train car of the PRR. They should be available in September or October 2019.

They will be complemented by EMD F3 diesel locomotive sets, which will be available separately with C&BQ, D&RGW and WP markings. We had already reported on the announcement of the EMD shunting locomotives SW1000 and SW1500. However, the first samples of the Alco RS3 and RSD-5 locomotives were shown and delivery is scheduled to begin at the end of 2019.



This official image shows the California Zephyr train with matching locomotive in the format of an A and B unit of the EMD F3. Photo: AZL / Ztrack

At about the same time, the depressed centre flat wagon units "Maxi-1 5-unit Well Car" will also be released, which were also shown as the first pre-series samples and were also demonstrated. And so we turn to the current AZL deliveries, which anyone interested can test on their own layout.



This time the focus is on the name "North Coast Limited". Those who have green as their favourite colour will certainly get their money's worth here, because a two-colour finish in that tone characterises the light passenger coaches.



Most of them come from the Northern Pacific, but the railway companies CB&Q and SP&S RY provided selected examples, as is indicated on the respective coach.



There are 4-4-2 sleeping cars (item numbers 73033-1 to -3), viewing cars (73433-1 to -4), dining cars (73533-1 / -2), luggage cars (73633-1 / -2), seating cars (73733-0 to -2) and pulpit cars (73833-1 / -2) available. The matching double units EMD F3 have the article number 62918-1 and -2.

4-4-2 Sleeping Coaches (picture above), Dome Cars (picture center) and Observation Cars (picture below) from the current deliveries. Photo: AZL / Ztrack

The 89-foot flat cars to extend military trains were given new registration numbers. The brown (911020-3S) and yellow units (911010-6S) are each loaded with two M1 tanks from the production of Z-Panzer.

The 40-foot long, covered AAR freight cars now run for the Kansas City Southern and are again available individually (904306-1), as twin (904376-1) and quad combinations (914306-1).

The limited edition AZL range is supplemented by a double pack of M1126 "Stryker" (AZL-ZP-M1126O) light wheeled tanks, which are used by the US Army as infantry transport vehicles. The models developed by Z-Panzer are made of resin and painted olive green. They are offered as limited edition.

You can find more manufacturer photos of the current deliveries under <http://www.americanzline.com>.

Anniversary of the Faszination Modellbau:

When Faszination Modellbau once again invites visitors to Friedrichshafen from 1 to 3 November 2019, there will be an anniversary to celebrate: this trade fair will be 25 years old in 2019. The venue (Messe Friedrichshafen) will be open daily from 9:00 a.m. onwards. The exhibition closes at 18:00, one hour earlier on Sunday.

Many manufacturers of large and small series will be represented in the South German border triangle, but Z-Freunde International e.V. also wants to raise the flag for our small gauge again. Also the former railway romantic presenter Hagen von Ortloff will participate again as an exhibitor.

If you want to experience the many facets of model making outside of the railway, this year you even have the chance to win tickets. On the occasion of the anniversary, the organiser is launching a photo competition, for which you can find further information at <https://www.faszination-modellbau.de>.

You can always find something out of the ordinary at Küpper:

If you are looking for more unusual loads, you will find them at the Küpper couple from Aachen (<https://spur-z-ladegut.de>). The specialists are currently pointing out turbine axles in the colours red (art. no. Z-135), blue (Z-136) and yellow (Z-138).

They are already mounted and tied down on a carrier floor as a transport frame on delivery, as required by the rail regulations for goods that could otherwise slip.

This also applies to the sewer pipe elements, which are transported in a standing position. Here, two joint positions are offered in which either five (Z-160) or four segments (Z-160-2) are loaded.

But also used models and spare parts can be found at Spur Z Ladegut Josephine Küpper at any time to make the best possible use of the one-time postage.



Blue turbine axles (art. no. Z-136; photo above) and sewer pipe elements (Z-160; photo below) are among the currently available cargoes from Aachen. Photo: Spur Z Ladegut J. Küpper.

New chemical tank car for Velmo:

The 1zu220-Shop (<https://www.1zu220-shop.de>) offers a new articulated tank car (Art.-No. 98171), which was ordered by Claudius Veit. He is (rightly) very proud to be the first customer to have realised this wagon with Märklin as an advertising wagon.

Similar to the container wagon with Velmo container around four years ago, as the customer he made sure that his model could ride in the train without only being perceived as an object for company advertising.



The new Velmo advertising van (art. no. 98171), produced by Märklin, has no concrete role model, but is nevertheless designed close to the factory. According to the hazardous substance labelling, the chemical tank car transports ferric chloride for etching printed circuit boards. Photo: Velmo

For this reason, warning pictograms can be found on the model, as well as the operating and adjustment instructions for the plant or the UN number 2582 (for iron(III) chloride) including the hazard number (Kemler number) 80:

- 8 – caustic effect
- 0 – no further dangers (placeholder).

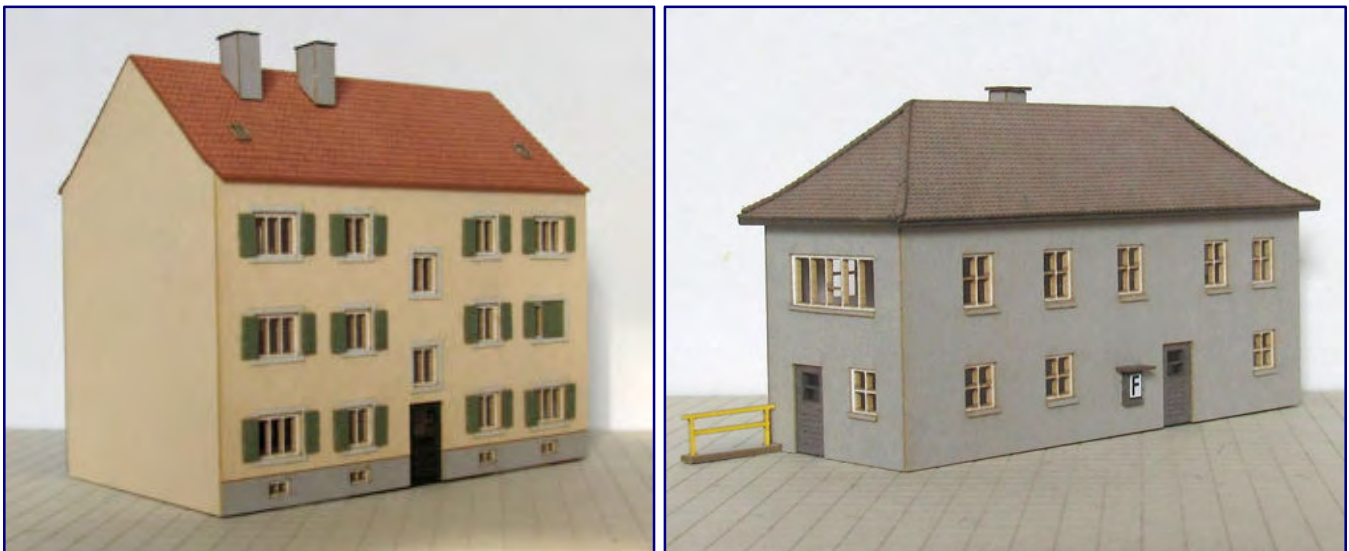
It is therefore a chemical tank car for acids, which is filled here with an acid necessary for etching printed circuit boards. Velmo uses this as advertising for its decoder boards. Consequently, the white belly bandage of Märklin's own editions is also missing, which points to forced ventilation - not recommended here because of poison fumes escaping.

As far as printing has permitted, the area of the boiler below the filler neck is also black rubberized in the model to protect it from damage in the event of spilled acid. The frame is also finely printed, the UIC rope hooks are even set off in yellow: Rarely before has an advertising car been designed with such comparable effort.

New building kits for Modellbau Laffont:

Two new architectural kits are expected to be available at the end of September 2019, and Modellbau Laffont is now announcing that they will be available. The apartment building (art. no. Z2201) based on the southern German model can also be used in almost all other regions of Germany without any substantive changes. Even the omission of shutters and window surrounds gives it a certain uniform character.

Further shape and colour variants are already planned by the manufacturer. The kit of the three-storey town house has fine roof tile engravings and can be used from epoch II onwards. The dimensions are 67 x 44 x 60 mm (L x B x H).



The new multi-family house (art. no. Z2201; picture left) is versatile, while the locomotive line building (Z2301; picture right) will probably only be found in depots. Photos: Modellbau Laffont

This is complemented by a locomotive line building (Z2301) in the form of a grey plastered, railway-typical functional building with a stepped hipped roof. Like the model from the Heilbronn railway station, it has a yellow safety railing right next to the entrance door to prevent accidental access to the access track to the turntable.

With this railway building for the stay and division of the locomotive personnel, the circular engine shed, which was included in the scope of supply of the last edition of the Märklin turntable, can be supplemented according to the prototype.

This kit also shows fine engravings on the roof tiles and can be used as a prototype in epochs II to V. The new model is also available in the Märklin turntable. The basic dimensions in this case are 76 x 28 x 44 mm.

The full manufacturer's program of this manufacturer is at <https://www.modellbau-laffont.de>.

No business operations with Full Throttle:

At the US small series manufacturer WDW Full Throttle by William Dean Wright, business operations will be suspended until further notice. The reason is a serious illness that requires a break of an unforeseeable length.

Military equipment from Andrew Hart:

Z-Panzer (<http://z-panzer.com>) has focused strongly in recent years on developing military vehicles exclusively for AZL. After a few years, the M548 transport and ammunition tank from the United States is now a new model under AZL's own name.

The original was already developed and put into service in the sixties and was based on the M113 light tank, which was also part of the Z tank range. In Europe, the prototype of the new model was mainly used by the Swiss Army.

The monochrome olive-green miniature is supplied in the usual high level of detail in a limited edition as a resin model.



Transport and ammunition tank M548 by Z-Panzer. Foto: Ztrack

And this is new at Micro-Trains:

MTL plans to deliver the EMD SD40-2 with Union-Pacific labels in a camouflage finish (Art.-No. 970 01 300) and with the company number 3593 this month.



Refrigerator car "Dingfelder & Balish" (Art.-No. 518 00 770). Photo: Micro-Trains

From the Farm-to-table series the model number 7 appears with a wooden 40-foot refrigerator car "Ding-felder & Balish" (518 00 770). It is painted brown on the sides and has yellow sidewalls with the advertising banners.

Two black tank cars each appear in parallel for the Union Pacific (530 00 391 / -392) and the Baltimore & Ohio (530 00 061 / -062).

In Germany Micro-Trains products can be purchased at Case-Hobbies (<http://www.case-hobbies.de>).

Herpa new products for the Christmas business:

The Dietenhofen model manufacturer has announced its innovations, which are to be available during the Christmas business in November and December 2019. With a few size-related exceptions, they are suitable for illustrations based on European models:

Wideroe De Havilland Canada DHC-7 (Item No. 570565) and Classic Antonov / LTS Air Taxi Service Antonov AN-2 "Anushka" (570602).

Especially worth mentioning is the Douglas DC-3 of Swissair in the so-called neutral colour scheme (570558) with wide, white and red stripes on fuselage and wings. It was used in the first half of the forties. The aim of this coating was to prevent an accidental launch by the warring parties.

Among the military models the current Lockheed Martin C-130J-30 "Super Hercules" of the US Air Force of the 62nd Airlift Squadron, 314th Airlift Wings of the Little Rock Air Base in design "D-Day Heritage Flight" (570541) plays a special role. 75 years after the Allied air landing, two of these transporters took part in the anniversary celebrations on the French coast.

The model comes with non-functioning airport floodlight masts "360" (570626), two of which are supplied in the package. They show a luminous wreath, the model of which emits light all around. From our point of view, such luminaires are also suitable for larger industrial areas on model railways.

The new products are rounded off by the simplified plastic model of a "Eurowings Airbus A319" (612487) from the Snapfit series in flight position.

Eisenbahnfreunde Marsberg celebrates 40 Jahre:

Die Eisenbahnfreunde Marsberg n Westheim/Westphalia celebrates their 40th anniversary on 21 and 22 September 2019. We cordially invite you to do so and would like to receive the following information.

"The Eisenbahnfreunde Marsberg e.V. was founded on 9 June 1979. We are a registered association and since April 2005 we are recognized by the tax office Brilon as a non-profit organization.



In the last years the signal tower got a new beautiful exhibits also in the scale 1:220 are safe for the visitors, if the Eisenbahnfreunde Marsberg e.V. invite to an exhibition day.

in front of it. With the help of our young people, ballast was shovelled from the 3rd track to the former track bed in front of the signal tower.

Our clubhouse is the signal tower Westheim (Westf.) in 34431 Marsberg-Westheim. This signal tower is the last remaining railway building in the Marsberg area. The association has leased the building from DB and is responsible for the maintenance of the building.

In the last years the signal tower had a new roof and new windows installed. On the upper floor there is an exhibition with exhibits and photos showing the history of the railway line. The signal tower is a member of the Hochsauerlandkreis museum landscape.

On 3 September 2002, a railway wagon of the type B3yg was erected

The required rails were provided by a siding from the Messinghausen quarry. The journey across the road from Osnabrück was made possible by the company Freitag. Jörg Erkel's father was allowed to erect the wagon with a small Hartinger crane. Our wagon is now used as a meeting room.

The module layout with the theme "Im Diemeltal", started in 1993, was extended in 2007 by the section "Westheim" on the occasion of the 925th anniversary of the village of Westheim. The theme of the modular railway is the Ruhrtalbahn from Bredelar to Westheim.

Meanwhile, the system has been digitised and given a new automatic staging yard. It is located in the former beverage market in Westheim on Waldecker Straße. Visitors are welcome here on Wednesdays from 6 p.m. The same applies to our two anniversary operating days, which will take place there on 21 and 22 September 2019.

The 33 members of the Eisenbahnfreunde Marsberg are on their way in all gauges, three of them in Z-gauge, by the way."

Short film about the digital NOHAB by Freudenreich:

In the May 2019 issue we published our NOHAB test report by Freudenreich Feinwerktechnik and nominated the model for the best new releases of 2019. Since the digital sound functions, in particular, cannot be reproduced in the magazine, we had announced a short film about it.

This was published on our website on August 16, 2019. In the almost 13-minute video sequence, all 21 digital functions, including 16 on sounds, are presented in detail to the solution developed by Velmo, so that our readers can also form their own opinion.

To view the film, please go to our home page (<https://www.trainini.de>) and click on the image representing a film strip. You will then be automatically redirected to the **Trainini® TV** page where you can start the film. Make sure that you have switched on the loudspeaker on your computer.



The Märklintage in Göppingen:

The 12th edition of the Märklintage will take place in conjunction with the 36th International Model Railway Exhibition from 13 to 15 September 2019 in Göppingen. The venues for the event are the railway station, the Märklin factory with the Märklineum, the EWS Arena and the Leonhard-Weiss-Areal.

Contrary to what was assumed at the last exhibition two years ago, Stauferpark can also be included. Here, in the shipyard hall and tents, there will be display installations and manufacturer stands. The ZFI will compete for Z gauge, and can be found in an adjoining room of the Leonard-Weiss-Areal. However,

we are expecting further participants, because a Track Z Convention for Southern Germany has been announced again.

But even less than a month before, the organizer has not yet provided any detailed information on the installations and exhibits, official documents unfortunately read like copies of earlier editions without any concrete information content. This makes planning particularly difficult for those who have to make long journeys and therefore want to carefully plan their visits.



Is she coming or isn't she? The Bavarian S 3/6 3673 from the railway museum Nördlingen belongs to the regular guests of the Märklintage. Whether the steam locomotive, which last operated for the Bundesbahn as 18 478, will also be there this time, remained unknown until the editorial deadline. Unfortunately, the same lack of information also affects other models and exhibited installations.

Likewise, it is almost unknown which model locomotives will be arriving and will be seen on site. What is certain, however, is that visitors will be able to take a closer look at the progress of the Märklineum exhibition. If you are fond of a visit, you can keep up to date from the official pages: <https://www.maerklin.de/de/erlebnis/maerklineum/maerklin-tage-2019/>.

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Trainini

German Magazine for Z Gauge



Small steep ramp in the Alps

Museum steam in Z-gauge
Ghost wagon with Rokuhan

Introduction

Dear Readers,

the Eisenbahnfreunde Marsberg e.V. (model train friends Marsberg, registered association) have become forty years this year. I was proud to participate in their anniversary event and to see many esteemed Zetties again.

It was quieter than at many other events despite good attendance and so there was time for a chat or two. That made me very happy, especially as I get so many suggestions and ideas or even find out what to expect at the next bigger meetings.

Therefore I would like to use this introduction today to remember and promote the events in Zell (Mosel) during Advent and in Altenbeken at the beginning of spring 2020. Both are “family gatherings” in the best literal sense.

We are all looking forward to it and enjoy what only Z gauge can offer: No matter from which corner of the world we come, somehow we all know each other and if not, we are curious to get to know each other and admire other works. Be part of it! Show us your most beautiful creations, be amazed and enjoy the great atmosphere, which is so characteristic for Z gauge!

We would like to motivate you with our article about the construction of a ghost car. Thomas Heß has been an enthusiastic reader for years and found it time to give something back to our unique community and to present his idea.

Keven Horat also dares to go public for the first time in this edition: he continues our annual focus on “Layout diversity” and presents his Gotthard diorama. That it is his first work is by no means apparent from this expressive display.

To match the topic my editorial colleague Dirk Kuhlmann literally wants to swing aboard a train by dealing with the track centre distance. And so he examines track geometry, MOROP standards and the best possible appearance. Some future works will certainly also benefit from his ideas.

Last but not least, we risk a small leap in time: We start in the middle of the DB (German Federal Railroad) steam locomotive era and look at the second career of selected tender steam locomotives. In 1985 (when steam train operation was permitted again on tracks of the DB) they experienced a renaissance that lasted for years and has unfortunately noticeably abated in recent years. Nevertheless, special steam trips offer a great opportunity to use the relics of bygone times in epochs V and VI as well.

To tune in and round off this special topic, we have also selected our supplementary literature proposals. On the one hand, the EK publishing house dedicates a portrait to a popular role model with the Bubikopf (bob hairstyle, German nickname of class 64 locomotives), on the other hand, the class 10 experiences its operational highlight once again in a DVD film from the VG Bahn.

If this now appears varied and balanced, then I am happy and wish fun reading.

Sin-Z-erely,

Holger Späing



Holger Späing
Editor-in-chief

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We thank Keven Horat and Thomas Heß for their reports and the Eisenbahnstiftung for photo support.

Date of publication of the German language version of this issue: 26 September 2019

Cover photo:

This old Ce 6/8^{III} 13302 is hauling a freight train on the Gotthard north ramp in beautiful weather. It is hard to believe that this impressive scene is the first work of one of our readers. Photo: Keven Horat

Museum steam on a small scale Character heads wanted

Do modern railways necessarily mean monotony? Do steam and the Taurus simply not fit together? We would like to deny both. Anyone who knows how to install and use his preferences in a targeted manner can also have old steam engines run alongside the most modern electric locomotives. We make a plea for special steam trips and museum railways in the epochs V and VI.

The present epoch is rather underrepresented on the Z gauge model railways. The focus of the majority of the modelling is mainly on epochs III and IV. The modern railway also has a lot to offer and is now more colourful than ever before.

The main reason for this are the many private railway companies that are now active in freight and local transport, and have their own colour schemes, to give their brand a lasting impression, instead of a product. Only long-distance transport remains firmly in the hands of Deutsche Bahn AG. Only Flixtrain shakes the role of the top dog there.



With regard to the predominant forms of modern locomotives, for a long time the railway had little variety to offer. The Taurus and the Vectron were the first to break the monotony, and second-hand locomotives do the rest. Here, you can see 185 CL 006 from Rail4Chem with an articulated tank car train on 7 May 2011 on the route Dortmund - Hamm (Westphalia), shortly before Dortmund-Kurl.

But why do so few Zetties turn to epochs V or VI? Some people think it's due to the lack of offers, because Märklin hasn't redesigned any modern vehicles in recent years and has instead left it at the colour variants of existing models.

The last notable new design to appear was the modern articulated tank wagons in a composition (Art. No. 82530) of various options intended for single wagon sales. Later, models with the same operating numbers were sold individually in factory aged form.

The supporters of modern rail transport have waited in vain to this day for the Siemens Vectron, assigned to the 193 series. With the ICE 3, however, at least, recently, an important model has been reworked, considerably.

In the area of diesel railcars, which are typical for modern mass transit, the Desiro (class 642) from Hagemodell in Hungary and the class 628 are at least worth mentioning. The latter vehicle is a still young construction of special models Z (SMZ) from Austria, which was not only produced an excellent technical conversion, but, to our knowledge, has produced considerable numbers for a small series producer.



Märklin has also provided its customers with articulated tank cars, but many suitable locomotive models have not appeared until recently. The NoHAB from FR Freudenreich Feinwerktechnik, therefore, offers variety and an excellent alternative locomotive. Steam powered special trains can bring even more options to modern systems.

If we were to seriously investigate the causes, then it would probably be too short-sighted to cite only the supply side as the reason for the low level of representation. After all, Märklin as a commercial enterprise can assume that they are serving a lucrative market, even if it is here.

We will not find any satisfactory answers at this point, because this would require representative surveys of as many market participants, as possible, in order to rule out statistical errors, as far as possible.

However, we can assume that the traction types limited to diesel and electric vehicles could also be jointly responsible. After all, it is often stated that the great variety of vehicles of the end of the steam age has a decisive influence on the choice of epoch of model railroaders.

Thus the sixties and early seventies of the last century did not offer the enormous colour varieties of today, but locomotives still appeared as true “character heads”.

They produced variety through the archaic appearance of a steam locomotive, the last representatives of the pole-mounted locomotives, the round forms of the economic miracle period (V 200 and V 160 “Lollo”), and the rather cubic era, which was already apparent with the standard locomotives and became the standard with the V 320.



When 64 289 entered the Feldberg-Bärental railway station with their special train on 8 May 1975, exactly 30 years after the end of the Second World War, their career as operational museum locomotives had only just begun. Photo: Burkhard Wollny, Sammlung Eisenbahnstiftung

As an indication for our thesis, we also state that many railway enthusiasts have complained about a very monotonous appearance of modern machines. Beginning with the 101 class, continuing with the 189 freight locomotive through to three TRAXX generations, they offered indeed little visual variety.

Only a few locomotives built in very small numbers deviated from this very uniform design: The most prominent example are the Taurus locomotives of ÖBB (Austrian Federal Railways), which were later also manufactured for many other railway administrations without this brand name.

Progress by Tradition

Nevertheless, a modern model railway system does not have to be monotonous and boring in terms of its typical design style. The different train types already promise variety. Although container trains may dominate, goods are still transported in other types of cars, even today.

The usual trains common today even offer an advantage from the model railroader's point of view: with the same number of cars, a train of the same type of cars looks longer to the observer than a train of mixed types. This is a popular way of concealing the compromise that is unavoidable on layouts, as long as it is only used consciously.



The anniversary year 1985 brings the steam locomotive back on Bundesbahn tracks! 86 457 is on its way with a shuttle train from Hersbruck to Nuremberg near Neunkirchen am Sand on 25 May 1985. Photo: Peter Schiffer, Sammlung Eisenbahnstiftung

But that's not the point today either. Instead, we were concerned with the question of how modern rail can be enriched with what the majority of railway enthusiasts still don't want to miss: the steam locomotive.

It offers a unique opportunity to counter modernity with a counterpoint that creates interest in the observer and thus increases attention. Equipped with suitable historical carriages, the contrasts could not be any greater.

But there is also a need for a suitable argument for historical representatives. If several of them were to travel on the main line at the same time, this would perhaps be a planned steam event. If only individual trains are involved, the branch line connected to the station can be justified as a museum railway, which also allows the integration of a complete depot with round shed and turntable.

Today, we have put our thoughts back to the time immediately after the 150th anniversary of the German railways: In 1985, the Deutsche Bundesbahn lifted the ban on steam travel on individual lines, and finally dropped it altogether.

Special trips with the recently reactivated vehicles are now enjoying great popularity again, and more and more clubs and museums are therefore having their treasures restored to working order in order to participate in this success.

Over a period of almost twenty years, the steam locomotive is finally back in business, before it settles down again, and the number of operational locomotives drops again.



When 64 289 was photographed on 30 May 1977 on the Zollernbahn in Hechingen, it shows a different appearance than our model. The smoke chamber supports and ladders have been painted red, and the buffers are decorated with a warning coat of paint. Photo: Wolfgang Bügel

But not only this time window offers many possibilities to put old next to new, to join crowds of people with and without cameras to the platform or to glue passengers into passenger coaches leaning out of lowered windows. The invigorating factor emanating from the figures will do the rest.

Operational mode!

Märklin models, in particular, offer many options for recreating museum steam or special steam rides on a scale of 1:220. If you're not so precise, you can use new, out of the box steam locomotives for this purpose.

Those who want to orient themselves strictly to prototype, or who want to upgrade their models, further, will then choose suitable operating numbers of popular museum vehicles, which will most certainly be quickly noticed by enthusiasts.

Since branch lines are most often reproduced on model railway layouts, we have specifically selected two steam locomotives which had appropriate applications there. It should be noted that we deliberately avoided the large tenders.

The 64 289, which is today the oldest locomotive in the stock of the Eisenbahnfreunde Zollernbahn (EFZ), provided the first model. It was built by Krupp under factory number 1298, delivered to the Deutsche Reichsbahn-Gesellschaft on 31 January 1934, and accepted one day later.



064 289-2 (picture above) stood from 1 February 1934 to 5 March 1974 in Staatsbahndiensten. After being taken out of service at the Crailsheim Railway Station, it was initially taken over by Eisenbahn-Kurier in Hildesheim, but one year later was passed on to Eisenbahnfreunde Zollernbahn. Photo: Will A. Reed, Sammlung Eisenbahnstiftung

On 28 March 1969, 64 289 (photo below) still carried their old signage and waited in the Tübingen train station for its next mission. It shows here exactly the condition, which was also reproduced in the model. Photo: Wolfgang Bügel, Sammlung Eisenbahnstiftung

In the EFZ inventory, it is in the state of the late federal railway age, which is why we would like to take a brief look at it this time. Its home in Schwerte (Ruhr) from 3 October 1954 to 4 December 1955 was only a short episode, but this also allows some local variety in Ruhr area themes.

From 2 May 1957, however, the model was at home in southern Germany until the end of his service. With its old number, which it also carries as a museum locomotive, it was at home as follows:

2.5.1957	-	30.6.1959	Tübingen
1.7.1959	-	26.9.1966	Rottweil
27.9.1966	-	8.3.1971	Tübingen

Later it arrived in Heilbronn and Crailsheim, where it was put back from the repair on 21 December 1973 (z 21.12.1973). Between 22 July 1974 and 14 March 1975, it was in service with the Eisenbahn-Kurier.



When 64 289 was travelling with a special train on the Leine bridge near Göttingen-Weende on 6 October 1974, it was in the service of the Eisenbahn-Kurier and, with the exception of the buffer plate warning paint, was still in the condition of the model. Photo: Prof. Dr. Willi Hager, Sammlung Eisenbahnstiftung

The EFZ took them over the following day and continued to use them for special trains. After the steam driving ban was issued, this was only possible on lines that did not belong to the DB. In September 1985, in front of a Donnerbüchsen passenger train, the locomotive took part in "150 Years of German Railways" parades in Nuremberg-Langwasser.

It was not until 1991, when the ban on steam travel was lifted, it was able to operate on DB tracks again. In 1996, for the DB nostalgia programme, it finally operated regularly on the Gäubahn from Stuttgart to Freudenstadt, for which it was rented by Deutsche Bahn AG.

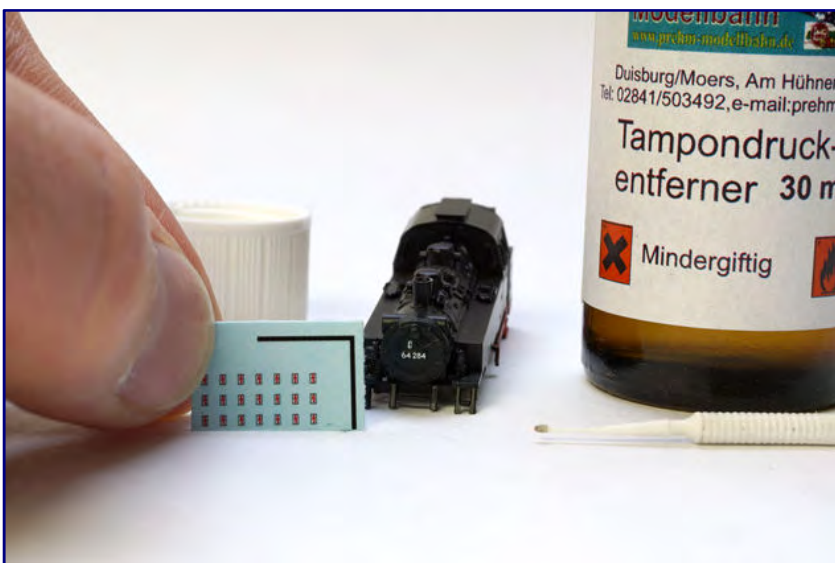
On 28 April 2001, the fire in the boiler of 64 289 finally extinguished after its boiler and running gear periods had failed. It was transferred to the Süddeutsche Eisenbahnmuseum (SEH) in Heilbronn in autumn 2005 and has been waiting to be re-conditioned ever since. After a long period of outdoor storage,

their condition today can only be described as moderate at best. At least its further preservation seems to be assured, and it is operable, as well as unmodified.



The Märklin model required only a few adjustments to make it the 64 289: new signage, window inserts, minor colour corrections on the coloured edges, and moving the lightning warning arrow shield on the smoke chamber door.

The choice of model was supported by her long career as a museum steam locomotive as well as the fact that she was one of the last representatives of its class in DB stock.



The printed lightning warning sign could be removed and wiped off with tampon printing remover without damaging the varnish. A new replica with the correct red arrow was then used as a transfer decal.

A reproduction in the last operating condition for the Bundesbahn also corresponds to its first years as a museum locomotive, which makes it very versatile for the epochs IV and V.

These features, which have been implemented on the model (base: Märklin 88740), include a red perimeter edge with black ladders, spoked wheels in the front and rear, screwed DB emblems on the side walls of the driver's cab and signage with riveted numbers in DIN 1451 middle typeface.



Photo above:
64 289 makes powerful steam when it leaves Rottweil on 8 May 1975 with its special train. Photo: Burkhard Wollny, Sammlung Eisenbahnstiftung

Photo below:
With a comparable train also our model is on the way. The fact that it can't be a regular train pulled by the class 64 can be seen from the ocean blue lower edge of the Silberling (passenger car), which was only introduced in 1975. But, by this time, the DB had already discarded the Bubikopf.

The number plate on the smoke chamber door is located at the level of the former central locking device, the lightning warning arrow plate has been placed to the right of it (in the direction of travel). Before it

could be attached to the model, however, the higher pressure, which the basic model carried ex-works, had to be removed. The tampon printing remover from Prehm Modellbau did a good job here.



Between 1977 and 1985 museum steam locomotives could only run on private tracks. This is to be expressed in the special train by setting a heat protection car of the Öchsle-Bahn (photo above). When 64 289 were rented later for the DB nostalgia program, it was probably to be seen however with historical vehicle park also under contact wire (photo below).

The model was accordingly upgraded with window inserts on the driver's cab (Ratimo), glued in with Contacta Clear (Revell), real carbon lining in the tender (Jeweha Modelbouw), etched signs (Kuswa) and decals (Nothaft), as well as colour corrections.

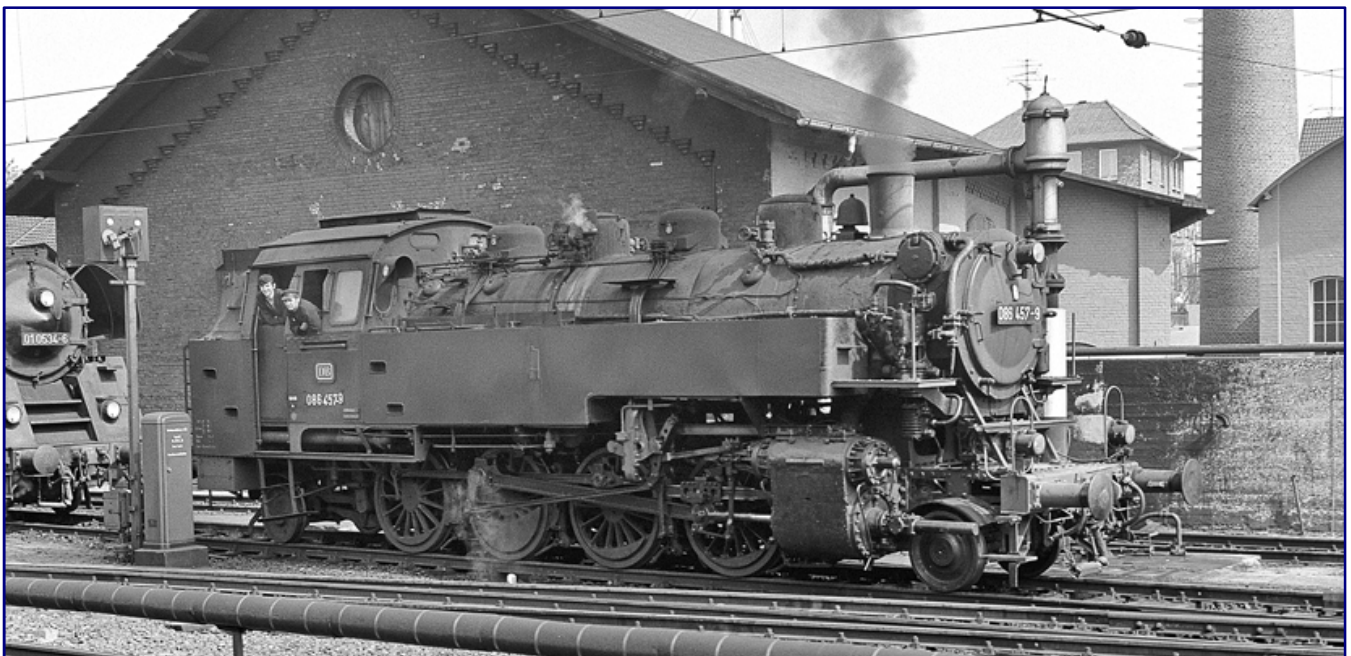
In Transport Museum service

The second locomotive that we selected, the 86 457 unit tender locomotive, is closely related to the “Bubikopf” and the class 24. All three belonged to the 15-ton axle pressure series for use on branch lines.

The 86 series was intended for freight service, but, it was also used for passenger service from the outset. Series delivery began in 1928 and by 1938 they had reached a total of 300 units.

After the Anschluss (annexation) of Austria, they were also manufactured in Vienna; after the beginning of the Second World War, simplified versions were produced until 1943, as transitional war locomotives (ÜK locomotives). A total of 776 locomotives of this series were built.

Differences in design concerned the design of the driver's cab (standard design and ÜK), “refinements” and disc wheels in the front and rear runners of ÜK versions (from 1942), riveted or welded water boxes, and the non-consistent use of Bissel axles, or the Krauss-Helmholtz steering frame.



When 086 457-9 passed through Bebra on 9 April 1971, it was probably on its way to the AW Braunschweig. In any case, at the end of its service with the DB with disc leading and trailing wheels, it appears completely different from its second career from 1985 on. Photo: Joachim Claus, Sammlung Eisenbahnstiftung

86 457 is one of the machines equipped with a Krauss-Helmholtz frame, whose maximum speed was therefore 80 km/h instead of only 70 km/h. The maximum speed of the Krauss-Helmholtz frame is therefore 80 km/h instead of 70 km/h. It was built in 1942 with the factory number 442 by the Deutsche Waffen- und Munitionsfabrik Posen. Delivered on 29 November 1942, the locomotive was able to start its service in the RBD Danzig on 11 December of the same year.

The locomotive selected by us was at home in the inscribed condition shown after delivery to the Bw Kaiserslautern on 13 May 1951 for about eighteen years in the BD Mainz of the Deutsche Bundesbahn, where it operated on main and branch lines throughout Rheinland Pfalz.

However, it is also interesting to note that it was used in the duty rosters of the sister series 64, according to photo documents when it belonged to its last office, the Bw Nuremberg Hbf (Hbf = central station) from 13 August 1971.

This also allows common journeys with 64 289 on a model railway layout. Another attraction for the time of their planned deployments is the focus on beet transport, which was carried out by DB in campaigns. Such agricultural themes are also ideal for branch line motifs on a scale of 1:220.

In the DB, the 86 series was once scattered over large areas and was therefore in use in many regions, but line closures and the traction change that was initiated soon pushed it noticeably back.

In 1965, the phase-out began on a massive scale when sufficient V 100 class locomotives were in service. This development only came to an end when the last three locomotives were parked at the end of 1974.

86 457, however, this fate already befell at the beginning of 1972 (z 8.3.1972, + 15.8.1972). It outlasted the time on the monument pedestal in the AW Trier, before it was selected in 1984 for a fully working order restoration for the 150th anniversary celebration.



This motif was once often photographed: The DB museum locomotives 50 622, 23 105 and 86 457 are standing in front of the shed of the Bw Nuremberg 1 on 1 August 1985. Anyone who carefully compares such photos from different years will also notice how the appearance of 86 457 has changed in some details. Here, for example, it has a completely red painted smoke chamber support. Photo: Volker Fröhmer, Sammlung Eisenbahnstiftung

In contrast to the 64 289, it is thus one of the late museum locomotives, which, as an operational machine of the Nuremberg Transport Museum, was once again housed for its last service at Bw Nuremberg Hbf.

After its first special trip on 24 February 1985, it was used for many years from there on numerous routes. So it remained until the night of the fire on 17-18 October 2005 in Nuremberg-Gostenhof, when this well-known representative of the DB Nostalgia Programme also suffered serious damage.

At that time, around one fifth of the historic vehicles remaining with Deutsche Bahn were saved from the flames (see report in **Trainini®** 11/2005, issue 4). At the beginning of 2006, 86 457 together with 23 105 were borrowed from SEH Heilbronn, towed there, and subsequently cosmetically restored.



On 21 February 1988, 86 457 with special train E 19691 and wonderful steam development is on its way from Neumarkt/Oberpf. to Beilngries on the long since shut down line. In contrast to our model, it also carries signage with the full name of its railway administration at that time. Photo: Wolfgang Bügel, Sammlung Eisenbahnstiftung

Since then it has been seen there as a non-operational exhibit. A correct museum use of our locomotive model is therefore limited for the time being to the years from 1985 to 2005, a period of twenty years, which includes both epochs IV and V.

For a long time, 86 457 instead of DB biscuits bore the older Bundesbahn lettering on the driver's cab sides. In the last phase of its regular service it then had disc wheels. If we include both features in our considerations, then the model is quite easy to identify as a museum locomotive, because only a rather narrow time window opens up for planned services.

We have therefore decided on the following features on the miniature based on an old Märklin model (Art. No. 8896): red revolving edges and smoke chamber support corners in this area, Ege biscuits on the driver's cab as well as spoked wheels in the leading and trailing wheels, which did not require any change compared to the standard state of the original model.



The chassis with fully movable detail controls, increased counterweights on the driving axle and brake replicas has been super detailed by Michael Bahls. New buffers, brake hoses, towing hooks, track scrapers and a front coupling hook as a replacement for the missing system coupling are also on his account (picture above). The body with lamp brackets, colour corrections, etched signage from Kuswa, Ratio window inserts, real carbon in the tender box (picture below), removed smoke chamber central shutter and replacement by of lightning warning sign was worked on by himself.

This resulted in an upgrade of the model with various parts and measures. First, our 86 457 was fitted with etched nickel silver plates from Kuswa, and the font DIN 1451 Mittelschrift DB was correctly used.

However, a compromise was required when the locomotive number was affixed to the smoke chamber door. The simulated central lock, which is not typical for a DB machine, could easily be sanded off and painted deep black in RAL 9002 after priming the now blank area.

However, the cast-on number plate underneath was problematic. This could not be removed in such a way that the curved shape of the smoke chamber door would have remained undamaged with all its details. Therefore, the decision was made to lower the new number plate accordingly, and not to place it at the same level as the earlier locking ring.

Above the front number plate, there is an lightning warning sign for the prototype and model, for which a shift image by Andreas Nothaft was used. The completion with two fixed side windows on the driver's cab could also take place here thanks to Ratimo. The crystal clear inserts (16009) were glued in again with Contacta Clear from Revell, which does not obscure the transparent plastic.



Since the lighting had to be kept functional, the lanterns on the buffer plank that were too tight were not removed and replaced. Instead, they were fitted with lamp brackets at the correct height. Under the housing, an LED light base by Hans-Jürgen Zimmermann now provides a highly visible, warm white top light at the front of the locomotive.

As with the Bubikopf, a real carbon layer followed in the tender box (Jeweha Modellbau) and colour corrections at the front edge (Oesling Modellbau). Technically, an upgrade was carried out with the help of an LED exchange insert (1001 wws) by Hans-Jürgen Zimmermann, which replaced the front bulb base. At the rear, the locomotive has to get by with its light bulb.

The work was completed by a Bahls chassis superung. A fully moveable detail control in front of wheels, with a black background and painted wheel tyres, now ensures maximum enjoyment, especially during slow manoeuvring.

Also, brake replicas, buffers of correct size with flat and curved plates, buffer plank ascents, as well as a buffer plank fully equipped with towing hooks and brake hoses were part of it. For this, the front system coupling was replaced with a graceful but fully practical coupling hook.



The use of 86 457 in front of contemporary train sets, which like the locomotive on a modern layout stand out clearly from the rest of the rolling stock, is appropriate (top photo). But even before photo freight trains (photo below), steam locomotives that are ready for operation can be seen again and again, to this day.

A cautious outlook

Certainly there are many more models available for similar projects. We think of the 74 1192, which was on the road for many years for the DGEg Railway Museum in Bochum-Dahlhausen with DRG signage.

Also within the series 64 and 86 further sister machines can be found, which were on the road in East and West in museum service. But when we look at other series, we have to make more compromises or carry out more complex conversions.



Steam trains are a good place for enthusiastic passengers behind open windows who let the smoke blow around their noses. When, when entering a railway station on the way, there are other passengers on the platform who want to travel, who move to the doors or just marvel at the machine, then the scenery is perfect for a special steam ride.

The boiler equipment of the Märklin model 8806 does not fit for the 78 468 which is operational today and which was already in service on the Teutoburger Wald-Eisenbahn. The 94⁵⁻¹⁷ series (88943) looks similar when we think of the position of the feedwater heater.

If this could be changed, nothing would stand in the way of a replica of 94 1192 (Deutsche Reichsbahn of the GDR) or 94 1538 (Deutsche Bundesbahn). Both are today at home at the Rennsteig, after damage to both machines, at least one is operational again today.

If we look even further in the direction of the forthcoming Advent meeting, then an ELNA locomotive will inevitably come to mind. Not only on the Moselbahn was such a representative on the way. The identical 146 BLE is now in Bochum-Dahlhausen and was once operational for many years.

On a scale of 1:220, it would attract attention because it stands out pleasantly from the appearance of the standard Reichsbahn series. However, those who want to stay with the Staatsbahn are welcome to wait for further conversions of the class 80 at Märklin: As 89 039, it could work for Museumseisenbahn Hamm (MEH), where it once came to after being retired from service at RAG.

This “rocking horse” would certainly please many Zetties, since the basic model is one of the latest and best running designs from Märklin. Due to its high axle pressure, however, the model could only be used to a limited extent on secondary tracks.

Basic model, refinements and materials:

<http://www.bahls-modelleisenbahnen.de>
<http://www.jeweha-modelbouw.be/info%20duitsland.html>
<https://www.kuswa.de>

<https://www.maerklin.de>
<https://www.modellbahndecals.de>
<https://www.oesling-modellbau.com>

<http://www.prehm-modellbahn.de/>
<https://www.rainer-tielke-modellbau.com>
<https://www.revell.de>

Contact with H.-J. Zimmermann (LED-Sockets):
possible via the editorial office

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14. Modellbahn-Ausstellung Zeller Adventsmarkt

**Zell an der Mosel
Zeller-Schwarze-Katz-Halle**

Sonntag, 08. Dezember 2019, von 11 bis 17 Uhr
Veranstalter: Z-Freunde International e.V., 56856 Zell (Mosel)

Swiss mountain panorama

A journey over the Gotthard Pass

Overcoming one's own prejudices and emotions is not an easy thing to do, says our reader Keven Horat. After years of admiring the works of others, he takes the plunge and puts into practice the many ideas that he has collected over time. Having designed his first diorama for a competition, he is enjoying the positive reaction he has been receiving for his first effort at railway modelling. Today, he tells us how everything began and what finally became of it.

From Keven Horat. Having thought about it long and hard, I decided to participate in last year's diorama competition organized by "Eisenbahnmagazin", a leading German railway modelling magazine. I have always had a great enthusiasm and passion for model railways and especially for landscape design.

However, not having built or designed anything since my youth, I was at first quite hesitant to register for this event. Everything appeared clear and logical in my imagination, and I could think of the most beautiful landscapes and track plans, but putting it into practice seemed a different matter altogether.



A Sersa-Köf shunting engine from Zcustomizer pulling a track construction train on Keven Horat's first railway model built. It serves as an alibi for the missing catenary masts on the left track that were broken off when the train was returned to the builder after the diorama competition.



This aerial view of the little masterpiece shows the different scenes and elements described in the text. Who would have thought that this attractive diorama occupies a footprint of just 30 x 15 cm?

After all, this competition was open to all regular model railway gauges and I was keen on showing what is also possible in Z-scale. Yet, at the same time I was wondering if I was really up for the challenge.

Finally, in a brief moment of madness, I submitted my registration for the competition. I had a rough plan of the overall design and the official deadline was also known. But now I was forced to turn my ideas and thoughts into reality. After many years of theorising and admiring the works of others, it was all of a sudden up to me and my own skills.

What if I would not be able to live up to my own expectations? And what if the mountain turned out looking like a big rock or the mountain stream like a water feature in a theme park?

But by now it was too late to second guess my decision, and so I started, like everyone else, with the detailed planning. My theme was going to be the Gotthard route -- what could be more natural for a Swiss than dedicating himself to this world cultural heritage with its countless bridges and tunnels?

The competition rules stipulated that all dioramas had to have a footprint of 30 x 15 cm. It was clear to me, therefore, that I would not be able to fit the entire Gotthard North ramp into such a small space. But a double-track section complete with bridge, tunnel and gallery should work: Thanks to the small Z-gauge!

Another reason for taking up this topic was that I had been sitting in my drawer for some time a plan for a real model railway layout. Of course, this should also be Swiss themed, making this little "newbie" diorama project the ideal test bed for my future layout "Thanwil", which is now under construction.

But back to the diorama of this report: Its key feature is a stone arch bridge, which crosses a gorge. On the right side the diorama ends with a tunnel and on the left side with a gallery. Since I had never modelled a body of water before, I absolutely wanted to try this out on this diorama. So this was also on the list of specifications for this built.



The view through the right-hand tunnel portal down the track with the Swiss signal bridge as a focal point. Hay is being made up on the pasture to the right above.

The diorama also needed a bit of depth and structure. In keeping with the competition theme “Along the track”, and with the bridge as the centre piece, I decided on a rock face with a waterfall and a small embankment, a motif that is quite common in Switzerland. In my eyes this was going to make for a harmonious backdrop and would allow me to also practice rock design.

And here we go

The base board consists of stacked XPS boards, which I have framed with a wooden frame at the bottom, so that everything is built on a solid foundation. The basic shape of the landscape was then carved into the foam, resulting in a first impression of the overall proportions.

Transitions and finer landscape features were modelled with putty. Next, I had to build the bridge before doing any further work on the landscape. I had it clear from the beginning that the bridge could only be built from scratch. This was done by cutting pieces wood to their appropriate size, gluing them together and covering it all with printed stone wall sheets from my collection of modelling materials.

The tunnel portal and the gallery were also made of wood. But in contrast to the bridge, I carved all the stones of the portal by hand. Let me just say that I will return to the topic of “carving stones” later on.

Sitting on my balcony some time ago, my eyes wandered off to my potted palm tree and I had a sudden inspiration: the texture of the pine bark used for planting the tree actually looked like rock and boulders!

Immediately I went to the pot, took a closer look at these pieces of bark and thought: “I’ll have to try that one day...” Having started on with my diorama, the idea came back. I picked out the most beautiful and finest pieces of bark from the pot and glued them onto the diorama where I planned to have the rock face. I filled the gaps with putty, aiming for a natural looking transition between the bark pieces.



The rocks that free climbers now conquer were created with the help of pine bark. The brown cattle on the pasture don't mind all this.

After everything was dry, I primed the rock with a grey tinting paint from the DIY store. This was followed by a coat of strongly diluted black wash which added depth to the texture. Finally, when everything was completely dry, I dry brushed the rocks with white paint in order to add highlights, and the rocks were ready.

Afterwards, I swiftly moved on to the landscaping. Here, too, the first step consisted in priming the ground with a coat of brown paint. As for the grass, I had it clear from the beginning that the fibres should be shot straight into the surface instead of being just scattered flat across the meadow. Grass, after all, grows vertically.

However, buying a static grass applicator to cover the few square centimetres on my diorama did seem a bit over board. Looking for an alternative on internet websites, I came across the the static grass puffer bottle from Noch (Art.-No. 081001), which I also ended up buying.

“There is practically no cheaper method and if it doesn't work, I just have a new salt shaker for the kitchen”, I thought to myself.

After delivery, the puffer bottle was immediately put to a test and I was very satisfied with the result. Of course the grass fibres were never as vertical as with a battery-operated Gras-Master or a similar electrostatic grassing device.

But for my cow pasture and the individual pieces of meadow on the rocks, it was quite sufficient. Also from Noch, I bought some finished tufts of grass (07700), which I distributed selectively on the diorama. These serve also wonderfully as basis for smaller bushes, which can be covered with appropriate turf.

For larger scrubs, I chose lichen as a base, which was then treated and upgraded with different of types of turf and flock. Thus, little by little, the vegetation in my small and happy world took shape.



Noch's static grass puffer bottle, which also does the job on smaller projects.



Sersa's track construction team is working hard to make the line passable again on both tracks.

Next came the design of the waterfall and the stream. The stream was also modelled with putty and coloured afterwards. This method left me with different options for modelling the water surface.

One method would have been to apply several coats of clear varnish. Or I could use the product “Water Effects” (60872), also from Noch, which I had already used for the waterfall.

I ended up going with the Noch product. Why buy clear varnish when the alternative is already at home?

But even the Water Effects method took several fine layers until the water looked as it should, and I was satisfied with the result. On such a small

surface, this method is adequate, but with larger areas, I would probably proceed differently.

A source of irritation: the Bridge

After the landscape was growing and flourishing, I was going to tackle the installation of the bridge. But the more the detailing on the rest of the diorama progressed, the more I was disturbed by the two-dimensional appearance of the printed wall sheet which I had applied to the body of the bridge. It also looked unconvincing in photos, and did not fit well into the surrounding landscape. Yet, building the bridge again from scratch also did not make any sense, as the rest of the diorama was already designed to fit the dimensions of the bridge.

Finally, I ended up covering the bridge with a fine layer of putty and carved the stones by hand after the putty had dried. After a little paint and the subsequent installation, the model suddenly fit much better with its surroundings and no longer looked out of place. So in the end my sixth sense prevailed, and the redesign of the bridge turned out well.



The bridge with a more convincing look after it has been reworked and the stones are now carved by hand. An SBB Intercity is passing over it.

Once the bridge was in place, I could start with laying the track work. Let me just mention a few parameters which were important to me, and which I intend to follow in any of my future projects:

1. The spacing of the double tracks in the visible areas was reduced from 25 mm (Märklin standard) to approximately 20 mm;
2. Curved sections in any visible area should have a generous radius;
3. The grain size and colour of the ballast must be to scale and have a natural look.



The view from above of the embankment and the brick bridge also makes it easier to see the self-made SBB masts and the signal bridge. The track centre distance has been sensibly reduced compared to the Märklin geometry.

4. The catenary masts must follow the original SBB design, i.e., no standard mass produced catenaries.

The tracks were glued to the previously laid cork bed (2 mm) with superglue and painted with a rusty looking brown paint. After drying I scattered the ballast along and between the tracks and fixed it with the well-known mixture of white glue and water.

Once everything was dry, I applied some grey and brown paint to give the track its final appearance. In doing so, I was careful to dab the ballast only lightly with a dry brush in order to avoid having too much paint smear the fine structure of the ballast. It takes a bit longer to do it this way, but the final result speaks for itself, and is definitely worth the additional effort.

I also think it is important to use for Z scale projects ballast with a grain size which is as small as possible. For my new layout, I have chosen a grain size which is even one degree finer than the one used for the diorama. Looking at it with the eye, the difference does not appear to be much, but it does become noticeable in high-resolution photos where the finer grain delivers a more balanced overall look to the ballast, track, and rail vehicles.

The same story applied to the catenary masts for this project, which I also wanted to be as fitting and true to scale, as possible. The four masts for this diorama were scratch built from 1 mm plastic H profiles and 0.5 mm round profiles.

Visually, they fit perfectly into the overall scene. However, two of them did not survive the return transport by mail from the competition and are missing now. Fortunately, they both broke off on the same side, so I can always pretend that they are being replaced and that the route is on single track service during construction.

I don't want to say much about the other small details and scenes, which were created on the diorama. I will rather leave it to the observer to tour around the diorama and let their imagination flow around the "Braunvieh" cattle typical of Switzerland, the two farmers making hay above the embankment and reservoir, the free climbers on the rock face, or the railway maintenance team from Sersa Company – just make up your own story!



The viewers of this diorama are invited to come up with their own story about the various detailed scenes. The alpine mountain world is full of charms and idylls, which should certainly inspire one's own imagination.

Finally, I would like to thank all readers for their inspiration and ideas and for sharing them with the rest of the modelling community.

Be it in the form of pictures and tips in the social media, with forum contributions of all kinds, with video contributions or the like: Building my diorama was a lot of fun and I could learn a lot for my future projects. I would never have thought that a report on my first work would appear in **Trainini®** magazine, and I feel extremely honoured by this - thank you very much!

From my personal point of view, I can now answer the question I asked at the beginning whether I can do this with a "yes". Let's go on - let's do it...



The final view of the bridge scene in front of the embankment shows the waterfall, which was modelled after several trials with "Water Effects" from Noch. And what could make this wonderful scene look better than a Swiss class Ce 6/8^{III} "Crocodile"?

All photos (with the exception of page 26 top): Keven Horat

The motivation was provided by the following site:

<https://eisenbahnmagazin.de>

Pages of the suppliers mentioned in the article:

<https://www.maerklin.de>

<https://www.noch.de>

<https://www.zcustomizer.de>

Perfect track spacing **The golden mean**

The Märklin track geometry, with which the Z gauge was launched in 1972, has been followed to this day by almost all manufacturers who entered the market on a scale of 1:220. This brings advantages, especially if an existing system is to be converted to a different track system. At the same time, its weak points are taken over and solidified. One of these is the large parallel track spacing, which is usually not necessary here.

By Dirk Kuhlmann. Märklin's Z-gauge rail material has been on the market since 1972. Over the years the range has been expanded with additional products and remains unchanged. Other manufacturers such as Micro Trains or Rokuhan have also adopted its geometry.



The perfectly working parallel track spacing on the "Kallental" layout is measured according to the NEM standards, and not the Märklin track geometry, which almost all manufacturers have adopted.

According to the standards of European model railroaders (NEM), a code 60 profile with 1.5 mm track height applies to Märklin. Further data can be taken from the NEM 120. In this report, we concentrate only on a part of the track geometry: the track centre distance, also called parallel track spacing distance.

For Märklin and other competitors, this is quite generous at 25 mm. If the observer looks closely, the large distances between two trains on a double-track main line are quite noticeable.

According to NEM 112, the track centre spacing distance should be at least 19 mm on a main line and at least 21 mm in the station area. The upward deviation chosen by the manufacturers follows the standard in this respect, but impairs the effect on the observer of a system.

The problem with too tight radii

With the straight track material, the hobbyist can at any time create a track centre spacing distance on his layout that corresponds to the model, but the 19 mm distance with the commercially available narrow radii (195 mm & 220 mm) cannot be maintained. In this case, only, at least, a partial installation of flexible tracks can help.

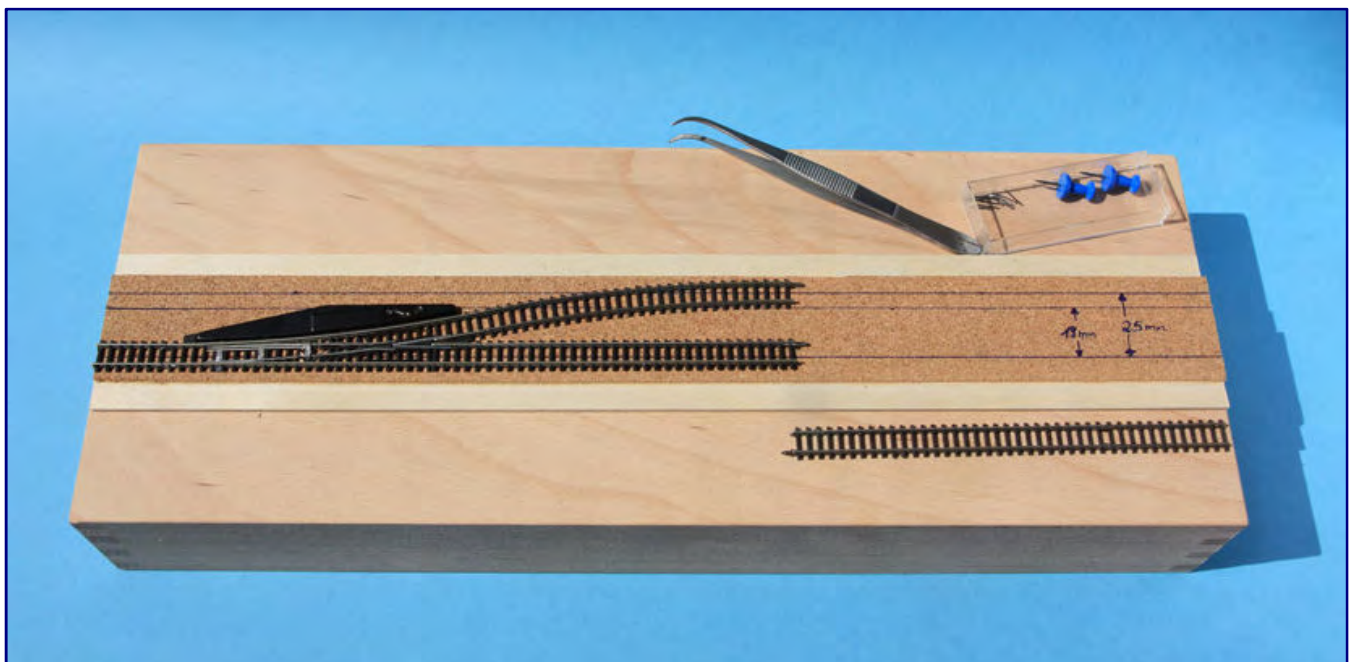
So that two trains do not strike each other when cornering, it is advisable to make a prior attempt with long, wide swinging-out rolling stock. In connection with the installation of flexible track, this even appears to be even more necessary, in order to reliably determine the resulting larger minimum distance in the radii.

It should also be mentioned that in highly modern model railways, the narrow radii that appear in the standard range are often no longer visible to the viewer. When I built the “Kallental” and “Bad Rothenzell” dioramas, I proceeded exactly in this manner.

The track curves leading to the staging yard are no longer visible there due to the superstructure of the turnstiles with a cover (“black box”). Thus my double track parade line with 19 mm track centre distance could unfold in the best possible manner.

Only to the scenery passage the distance of both rail lines spreads here again on the usual 25 mm. Here the model landscape helps with an effective camouflage. However, if at least one “normal” turnout comes into play in the visible area of the layout, the tinkering really starts. With it, the usual track geometry has to be changed.

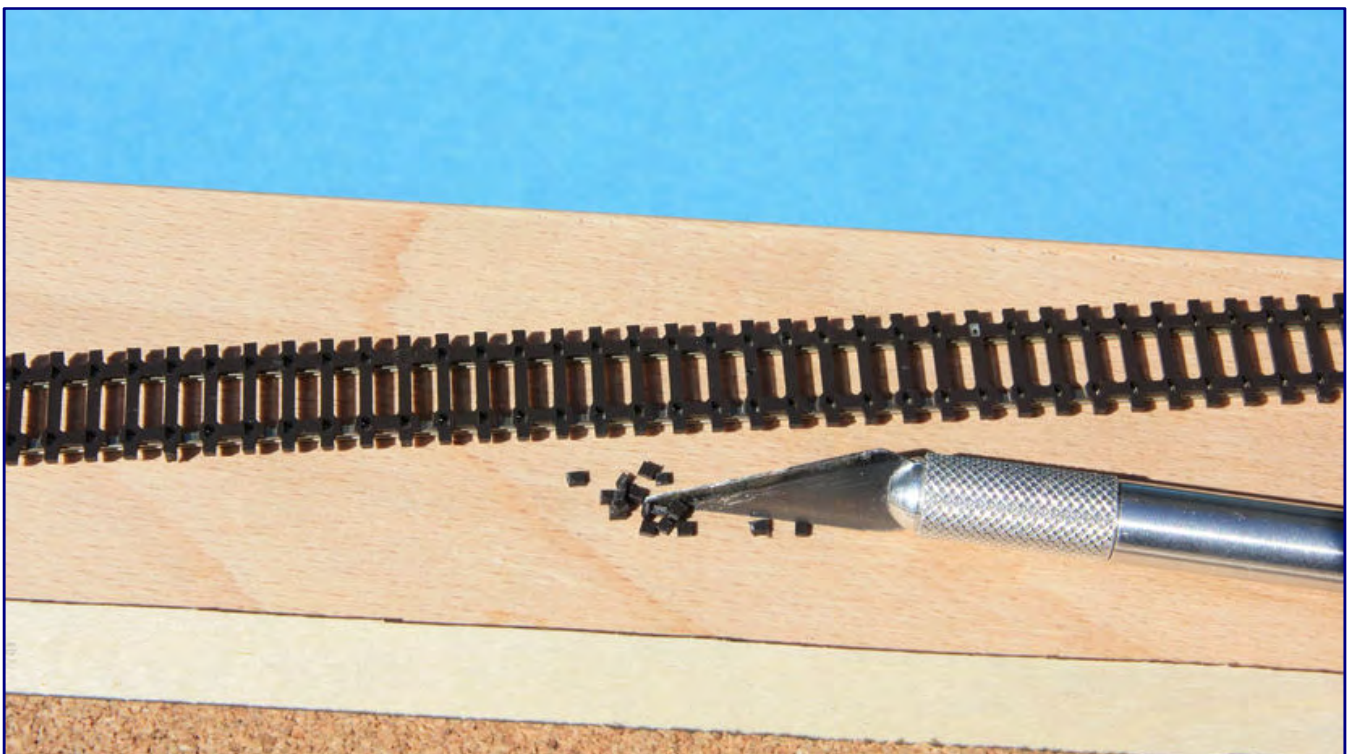
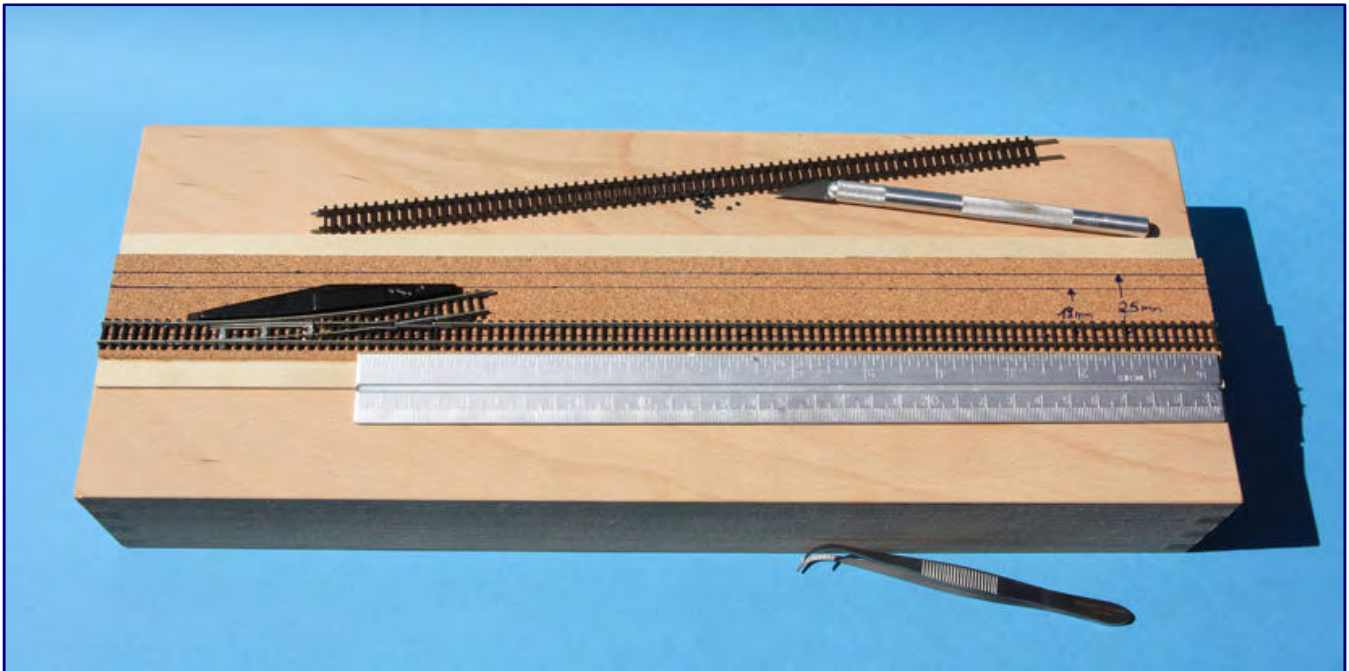
The following is a variant for a satisfactory, “slim” track figure:



The test setup with an 8591 counterbend from the Märklin track range illustrates the track centre distance of 25 mm (distance between lower and upper line) provided by the system. Our goal is a distance of 19 mm (distance to the lower marked line).

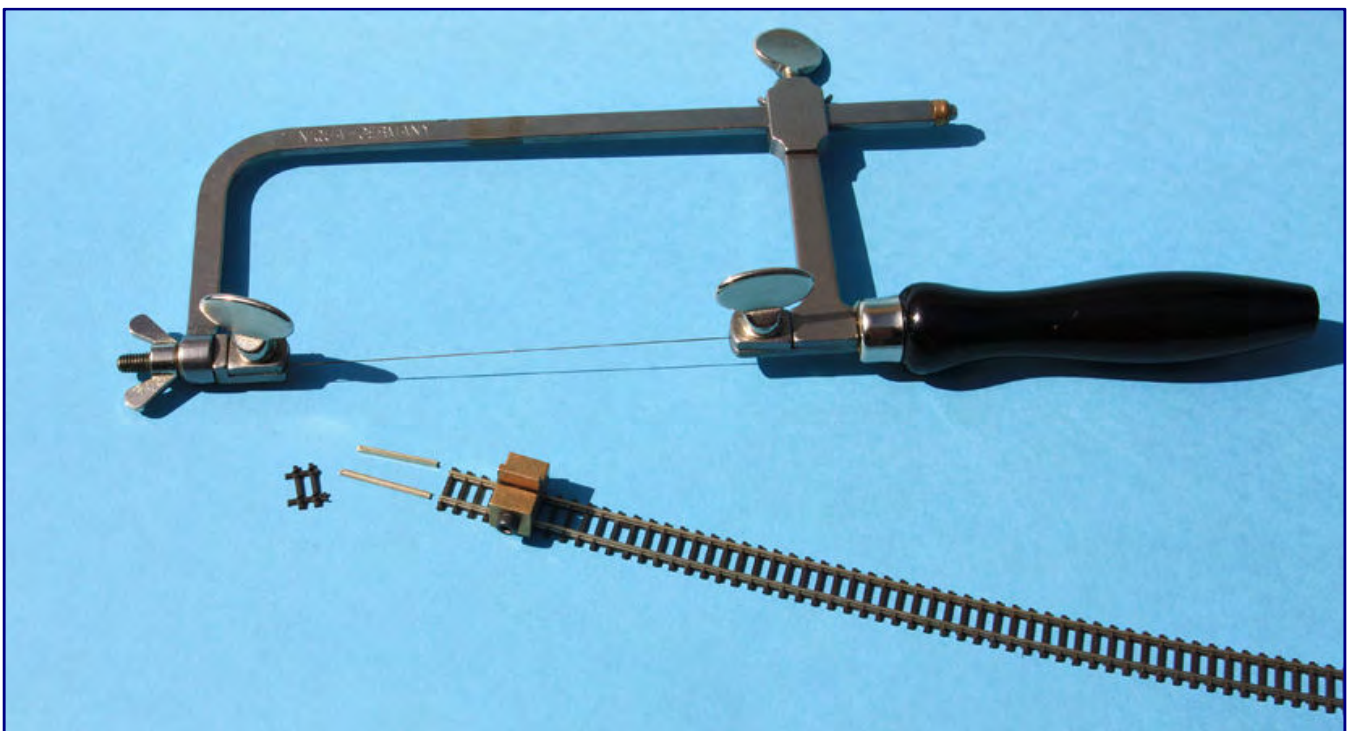
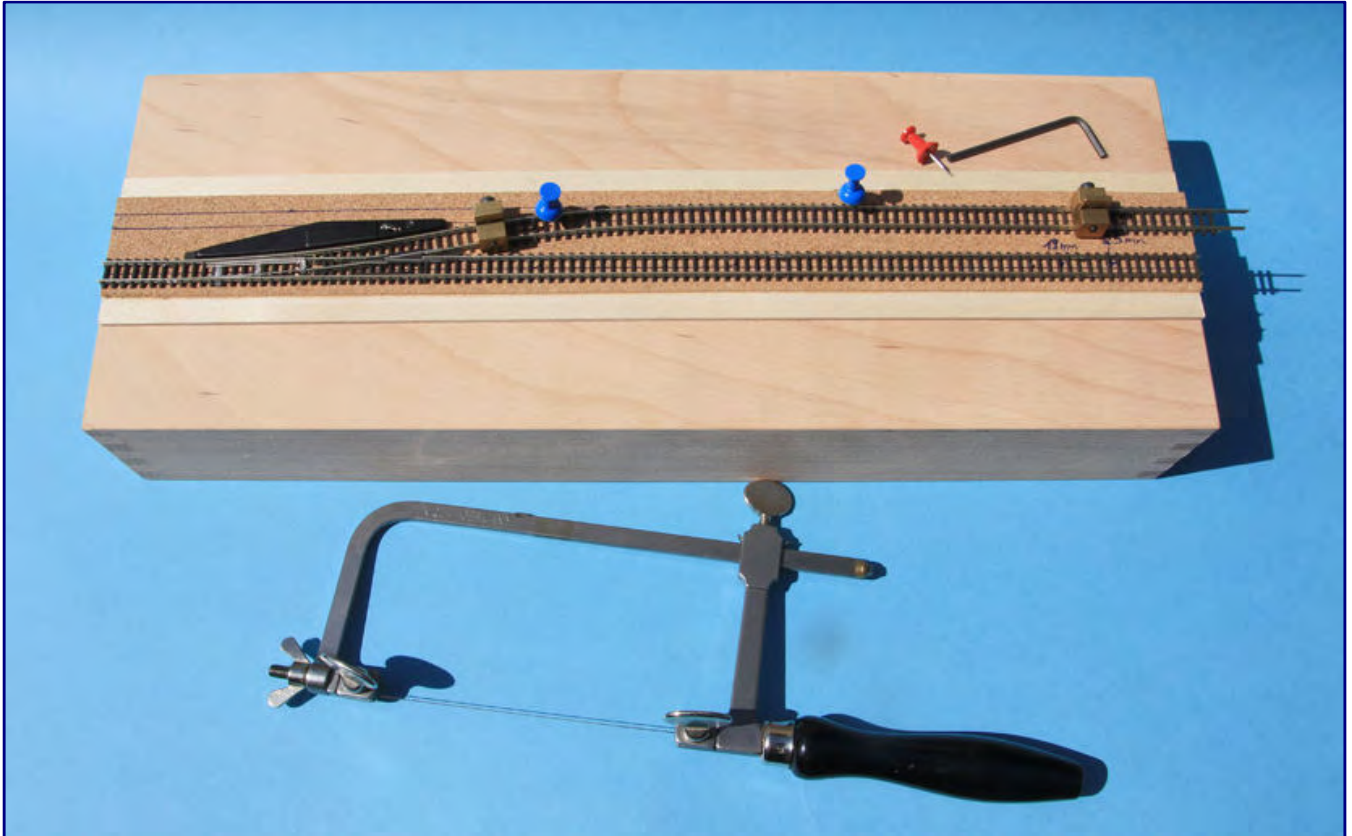
The test setup shows how the track centre distance of 25 mm is achieved with a turnout (Art. No. 8562) and the opposite bend (Art. No. 8591) from the Märklin range. The branching angle here is 13 degrees with a radius of 490 mm.

However, we would like to achieve the scale and optically finer distance of 19 mm. This is to take place with few and careful treatments of the proven track system.



For our project we cut the sleeper tape of a straight track section on the outside of the planned curve (photo above and below).

Therefore, the sleeper band of a straight track (e.g. Art. No. 8594) is first cut with a sharp scalpel only on the outside of the curve. If the connecting webs are cut out between each pair of sleepers, the result is a return curve with a high degree of freedom of movement.



The flex track cut into the sleeper belt is brought into the curve by Krause track clamps and fixed with pin board needles (photo above). The overlapping tracks are finally cut off with a jewellery saw (photo below).

The main track has already been fixed with nails, and an aluminium ruler is used for a completely straight alignment. Switches should not be fixed, as they have a small movement when operated. But the later ballasting does not stand in the way of this.

The next step is to bring the flex track rail with the so-called Krause clamps into the desired curve to the 19 mm line and fix it with pinboard needles. The overlapping tracks (right) are marked.



If ballasted as a trial, the optical effect of the narrower track centre distance can be assessed even further. This makes it ideal for a layout.

Cutting is done with a jewellery saw, which is fitted with a metal blade with a cutting width of 0.16 mm. Then the new return curve is also fixed with nails, and the track clamps can be removed.

On our Casani wooden base (40 x 15 cm), a ballasting provides an idea of how a track centre distance of 19 mm can be achieved and will appear on a diorama or a layout.

Now a small block place placed in the middle of pastures and old trees, finished would be our showpiece somewhere on a side road. If the black drives of the Mini-Club switches were now separated from the sleeper grate as well, an almost perfect picture would result - well noticed, in the scale 1:220.

Tool requirements:

Jewellers curved saw (curve depth ca. 70 mm) Fohrmann-Art.-Nr. 14121

Fretsaw blades (32 Teeth / 0,16 x 0,32 mm) Fohrmann-Art.-Nr. G14480

Krause Track clamps (2 or 4 units) Fohrmann-Art.-Nr. 01424 or 01425

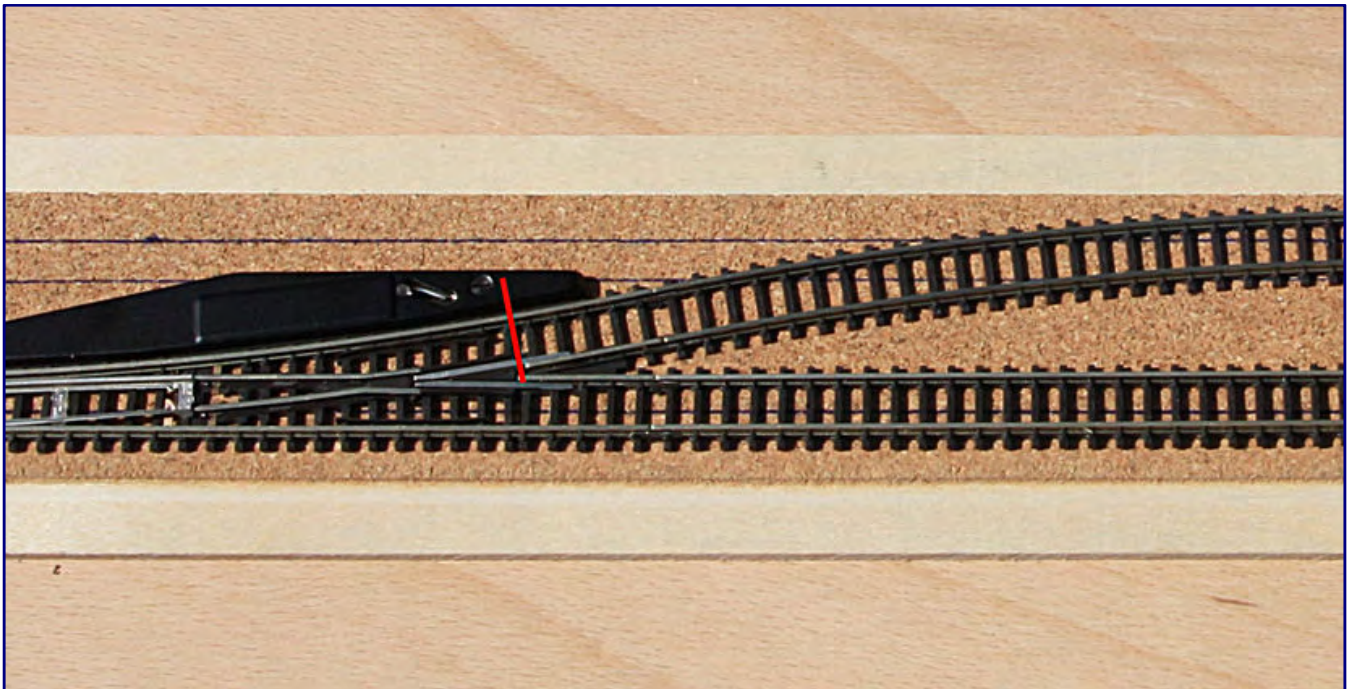


Photo above:

For comparison with the previous picture we show another perspective. It illustrates even more clearly the prototype-like effect of the distance between the parallel tracks.

Photo below

For experienced model railroaders there is an alternative to laying flex track by shortening the switch at the red marking. Then the return curve track 8591 could also be attached directly to it and reached to the same centre separation distance.

The experienced model railroader might shorten the turnout (red line in the photo above) and use the counter curve (Art.-No. 8591). In the area of the frog, the copper contacts would have to be loosened and reattached. This would not be a problem with special tools, but due to the high possibility / danger of a functionally impaired switch, it would only be advisable to a limited extent.

Is there a better solution for the industrial material?



This recording of “Bad Rothenzell” shows us how what is described in this article works in plant practice: The parade track on the right side has been laid with 19 mm track centre distance, station and depot on the left received the distance of 25 mm specified by the large series geometry.

In 2022, the Z gauge will be fifty years old. In addition to the anniversary articles that can certainly be expected, the manufacturers could (only by the way) delight us with slimmer standard switches or a refreshed or reworked track system. It would be an appropriate time, after half a century.

Most fortunate are those who own the tracks and switches of Eckhard König. For example, there is a EW 49 - 190 1:9 turnout with a radius of 864 mm and a branch angle of 6.34 degrees. I simply call this “simple elegance”.

- **Source of supply for the tools:**
• <https://www.fohrmann.com>
- **List of the standards mentioned (MOROP):**
• <https://www.morop.eu/index.php/de/nem-normen.html>
- **Manufacturer of the König track (source of supply):**
• <http://weichenlaterne.de/index.html>

Grenzenloser Modellbahnspaß in 1:220

**8. Internationales
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28. und 29. März 2020

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Sonntag: 11.00 - 17.00 Uhr

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Self-built project on shorty chassis

As if driven by the hand of spirits

Thomas Heß has been reading our magazine Trainini® for many years. He has used a lot of advice and ideas from the contributions of our readers, and the specialist authors working for us and the editorial staff. Now it seemed to him the time to give something back to this community. In the following he describes the construction of a ghost (powered) car using a Rokuhan shorty chassis.

By Thomas Hess. It was not until the year before last that I bought myself a small "Ticino" still-fabricated plant, and set it up with the Märklin Mobile Station 2, which I had from the H0 carpet track, as well as digital switches and lighting switches from JSS. This is how my system Winzlingen (Schwaben (Swabia)) came into being.

By Thomas Hess. It was not until the year before last that, I started to rebuild selected locomotives with decoders from Velmo and Doehler and Haass. In the last handicraft season I dared now to a change of rolling stock. My specifications said: It should be a ghost car with appealing driving characteristics; it must also be possible to control it digitally with DCC. The tractive power should be higher than normal locomotive models, because the grade on the Noch pre-formed layout Ticino is quite steep!



In Winzlingen (Schwaben (Swabia)) station, the battery-powered small locomotive (Ka) shunts a short freight train. The fact that the drive in this case is in the first freight wagon, however, should only be noticed by connoisseurs. Photo: Thomas Heß

And it must be possible to carry out the conversion at a reasonable cost - in terms of time, technology and money. In fact, I had already purchased a ghost car quite some time ago - a conversion based on the 2-axle Märklin rail bus with a housing from the sliding wall car.

However, I am not really satisfied with the performance and the contact reliability. So it was out of the question. On the contrary it came to me that the manufacturer Rokuhan delivered a steep pattern with the motorized, four-axle "Shortys". There should be something that can be done.

It was worth a try, especially since the basic model is available from Jörg Erkel (1zu220-Shop) for about 18 Euro. Its contact reliability and driving behaviour should also be correct: With the models of the series 181² of Rokuhan I was allowed to make finally very positive experiences.



A motorized shorty chassis from Rokuhan and an ordinary self-discharging wagon 8630 from Märklin are sufficient for the conversion presented today. Using a D+H decoder, the ghost car was even digitized.

Their positive properties include smooth running due to the bell-shaped armature motors, the high tractive force due to traction tires and their basic characteristics as bogie locomotives. The Rokuhan Shorty also has all this, but only the front bogie is driven.

And a suitable housing for my ghost car should certainly be found in the Märklin freight car range? I would have to cut the floor out of the Märklin car so that it can be attached to this chassis.

And a Doehler and Haass Micro-Lokdecoder (DH05-C) was supposed to be accommodated as well... The decision was made quickly, and the specifications were already in place. The conversion should be done by hand: "Trying makes you smarter", and that increases the fun of it.

When the Rokuhan shorty arrived a few days later, I set to work. First I looked at the dimensions of the Shorties.

I had not found anything, in advance, that would have shown me its exact dimensions. And so there was a surprise: The Shorty is relatively short after all!

So it had to be a quite short housing for a four-axle car as well. In my freight wagon collection I found different models: The telescopic hood wagon was too long, and the inside was too narrow.

Also, the Erz-Ild wagon is too long and too low. I would have gotten the locomotive decoder in, but then the housing would have "floated" over the bogies.

The best compromise for me is the housing of the self-discharging car Fads 176 (Märklin 8630): wide and high enough to accommodate the undercarriage completely. The locomotive decoder also fits, and the conversion can be concealed by a load insert. However, this car is also too long!

The basic idea was to move the pin for the rear, non-driven bogie on the Shorty by about 5 mm towards the end of the wagon. This increases the distance between the bogies. Then, this is again reasonably correct.

- Required tools:**
- magnifier with light
 - precision mechanic screwdriver
 - sharp craft knife
 - side cutters, small flat nose pliers
 - key file, sandpaper medium and fine grain
 - soldering irons
 - "Third Hand"
 - multimeter
 - tweezers
 - fine brushes
 - craft kit
 - tray for storing the individual parts



The chassis of the Shortys is, nevertheless, very short, which made the selection of a suitable base model for the ghost car difficult, at first.

All other alternatives seemed to be too elaborate or not feasible: Extending the chassis or shortening the Märklin car's housing were ruled out as options, if I were to equip the cars with Märklin compatible couplings.

For the realistic impression of the axle bearing covers, it was first necessary to be convincing, because it was a quite complex solution to implement with the help of glued on covers of the genus Fads 176 (Märklin 8630).

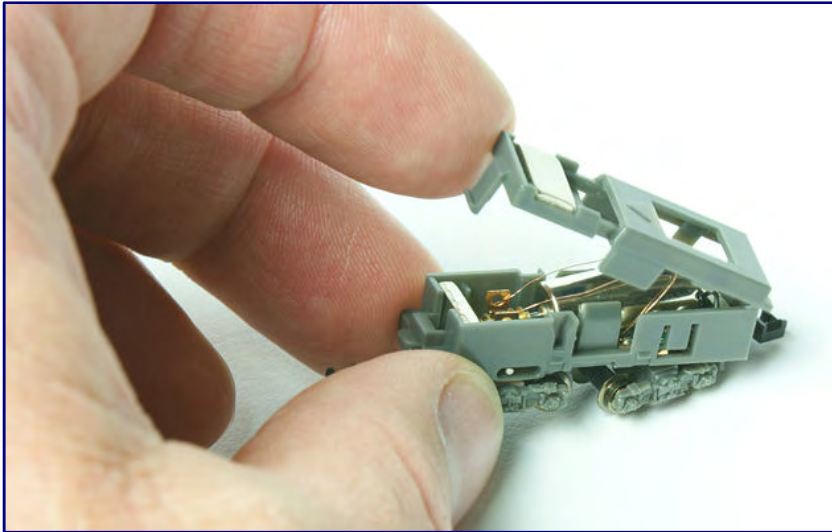
However, this step was most worthwhile, as it contributes decisively to the positive overall impression of the car. I

would like to thank Mr. Holger Späing of **Trainini®**, without whose persuasive assistance, I would not have been able to make this decision, as I had feared that the project would fail.

Extend bogie spacing

First of all, the shorty has to be dismantled. The underframe and the gearbox are made of plastic. I prefer cutting plastic rather than sawing or filing metal.

I started by removing the front driven bogie. As with Märklin models, this is done by pushing out the metal quick-release axle. However, be careful, because the gearwheel on the axle is very small, and might get lost, if you don't put it in a safe place right away.



To remove the upper latch, the latching lugs can be prised open with a jeweller's screwdriver. The upper part can then be removed carefully. The driven bogie (left) falls out, as with Märklin, as soon as the pin, which also holds the central gear wheel, is pulled out..

I certainly overlooked that. And since it is made of plastic and makes little noise on impact, I only missed it during assembly, and only found it with a lot of searching efforts, and under heavy swearing I found it on the floor between two boxes.

Then I took down the cover of the Shorty, it is secured at both front ends with snap hooks in corresponding eyelets in the frame, there are no screws.

With a jeweller's screwdriver the snap-in hooks can be carefully prised open, and the upper portion can then be carefully removed.

should be removed (motor connection and power connection to the front bogie). The motor is snapped into the side cheeks of the locomotive frame. It is also best to unsolder the wires on the circuit board immediately, so that the subsequent work is not hindered by the stripping.

Then the bell-shaped armature motor together with the small circuit board

Rokuhan has mounted small metal plates in shafts on the front side of the locomotive frame to weigh down the locomotive. These like to slip out. So they too should be secured against loss and stored away. To do this, unscrew the rear bogie, cut off the pin on the underside of the locomotive frame flush with the base, using a sharp hobby knife, and glue it back in place.

I have shifted the position of the pivot by about 5 mm in the direction of the rear end of the undercarriage. To find the exact location, you can use the selected housing as a guide.

I did the gluing by careful use of liquid superglue (manufacturer Bondo in a bottle with dosing tip or pin for application). When closing the bottle, make sure that the pin is seated correctly and that it is not off-centre or crooked.



In order to create the correct bogie spacing of the basic model, important for the uniform effect in the train, the free-running bogie was removed after loosening the retaining screw, and the guide pin was moved outwards. The contact surfaces for power transmission to the running gear had to be enlarged as well. Photo: Thomas Heß

Extension of cut-outs and contact surfaces

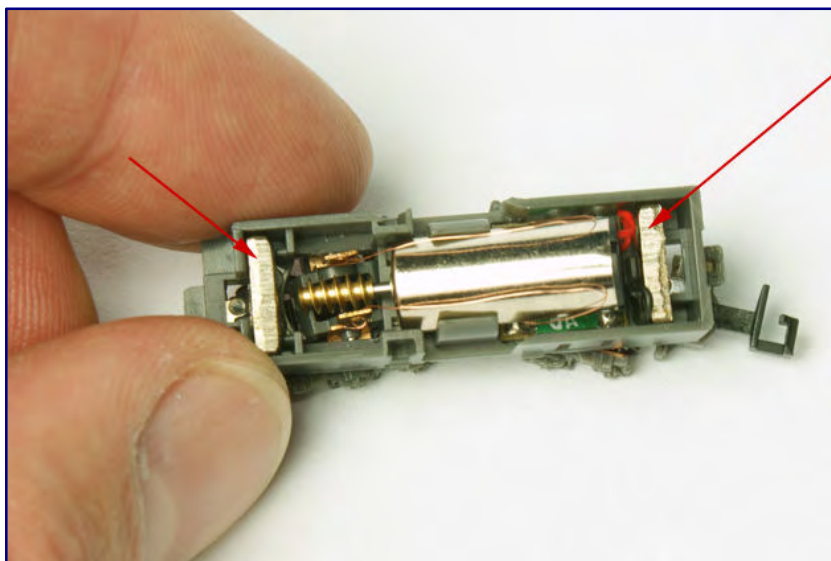
So that the rear bogie can be mounted again after moving the pivot and can also transmit the current, I have extended both of the cut-outs in the car floor and the contact surfaces.

Now the cut-outs in the car floor can be extended towards the ends of the car. With the sharp craft knife I tried to reproduce the curve. This is necessary because the contact flags in the bogie swivel around the vertical axis of the frame. If a straight cut is made, the swivel range could be restricted if the contact lugs touch the cut-out.

However, it is also possible that on the later mounted housing of the Märklin 8630, the bogie axle bearing covers on the downward moving side walls will stop the car, so that the rounded cut-out, would not be an absolute necessity.

Construction ex-works:
Rokuhan realized the current transmission for the bogies with the help of metal flags standing vertically in the bogie. The ones at the rear press on contact surfaces on the underside of a small printed circuit board.
This is snapped into the interior of the car floor. The motor connections are soldered onto it, and the connections of the front driven bogie are also brought here by means of very thin, flexible enamelled copper wire.

In order to extend the contact surfaces for the transfer of current between the rear bogie and the small circuit board with the motor connections, I used thin copper sheet (thickness 0.3 mm). This is placed under the board. Therefore I cut two 2 mm wide strips and glued them in the shorty's bottom with superglue.



The photo shows the sensitive enamelled copper wires that conduct current from the driven bogie to the motor. Our reader decided to exchange them for more flexible strands. Two of the three inserted small weight plates are shown marked with arrows to increase the traction.

The correct length is obtained by simple trial and error. However, it is important to ensure that the glue does not interrupt the current flow to the circuit board later.

It is just as important to protect the inserted plates against short circuits, both in relation to each other, and to the motor (especially when using a locomotive decoder).

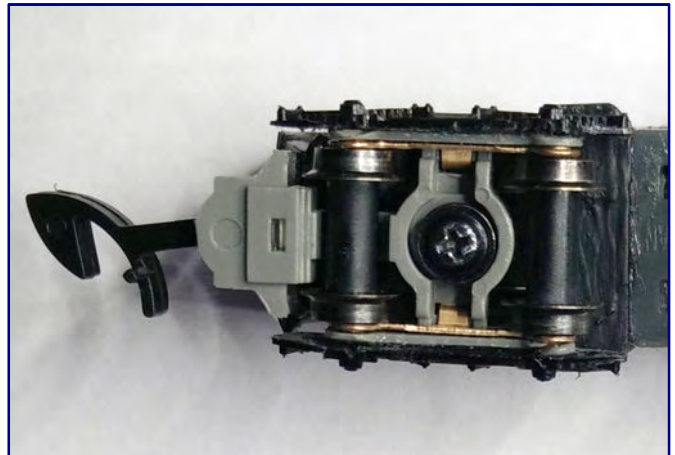
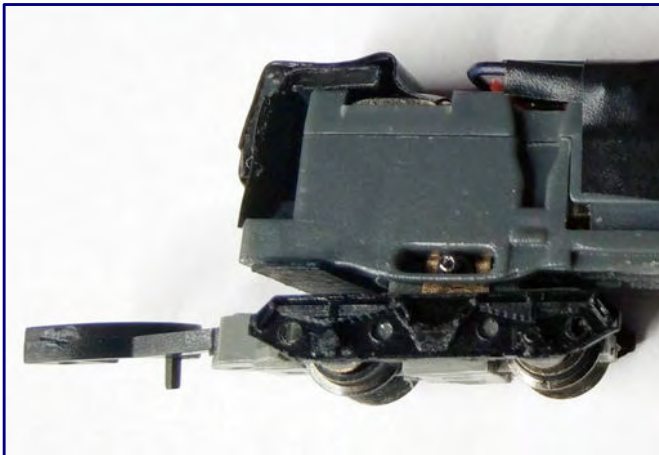
After the adhesive has dried, the rear bogie and the printed circuit board can now be mounted for testing.

A multimeter can be used to check the current flow between the wheelsets and the circuit board. The left and right wheels of an axle are also insulated against each other by Rokuhan.

Realistic design of the bogie panels

The Fads 176 from Märklin (8630) served as a parts donor. Its bogie covers were glued to the bogies of the Shortys after modification.

In the first step, I cut off the front panels of the Märklin wagon flush and sanded them flat on the back, so that they still had a material thickness of approx. 0.45 mm. Previously, the thickness above the point where the covers represent the axle bearing was approx. 1.1 mm.



According to the description in the article, the shorty bogie frames were removed in order to be able to glue the Märklin model's panels on with superglue (left picture). However, these also had to put up with a "rejuvenation cure" beforehand (right picture). Both photos: Thomas Heß

The grinding process was carried out with the help of 240 grit sandpaper, which I had fixed to the worktop. I removed the thickness by placing and moving the screen over the sandpaper.

Handymen like me, who are blessed with relatively thick fingers, will quickly notice what happens if the fingers rub permanently on the rough surface because the small grinding part is difficult to grasp. Repeated thickness measurement is necessary during the grinding process.



After adjusting the bezels, the ghost wagon (right) is barely recognizable as a ghost wagon. Only the light grey coupling shaft can reveal it, if we disregard the exchangeable, bright nickel-plated wheel sets of the left series model. Photo: Thomas Heß

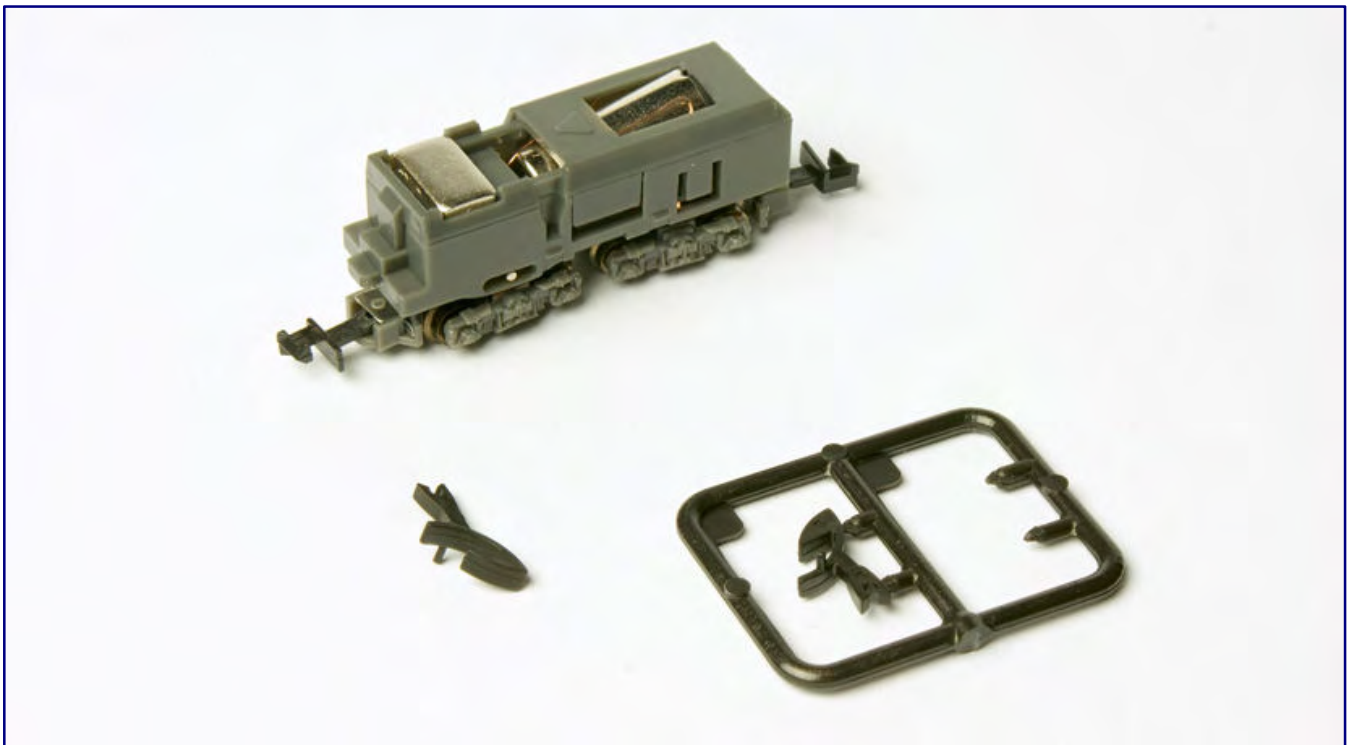
On the Shorty I cut the removed frame parts flat with a scalpel and then grinded them down, as well. The material thickness after the grinding process was approx. 0.6 mm.

So that the bogies were not damaged during the grinding process, I had not disassembled them before. I had to disassemble and clean the bogies of the Shortys anyway before gluing on the covers. Nevertheless, extreme care had to be taken, and, most importantly, the journal bearings for the axles could not be damaged.

Then the parts were glued together with superglue. Care had to be taken to ensure that the bogie covers were seated correctly. In particular, the bogies of the Fads 176 still had to be able to swivel perfectly, so that later in switches and in curves, there would be no derailments.

Coupling hook and assembly

Instead of the original Shortys couplings, I replaced them with Märklin compatible Rokuhan couplings in the long version. These couplings are available in specialist shops. The replacement is done by pulling the old couplings out of the front of the receiving shaft and plugging in the new ones.



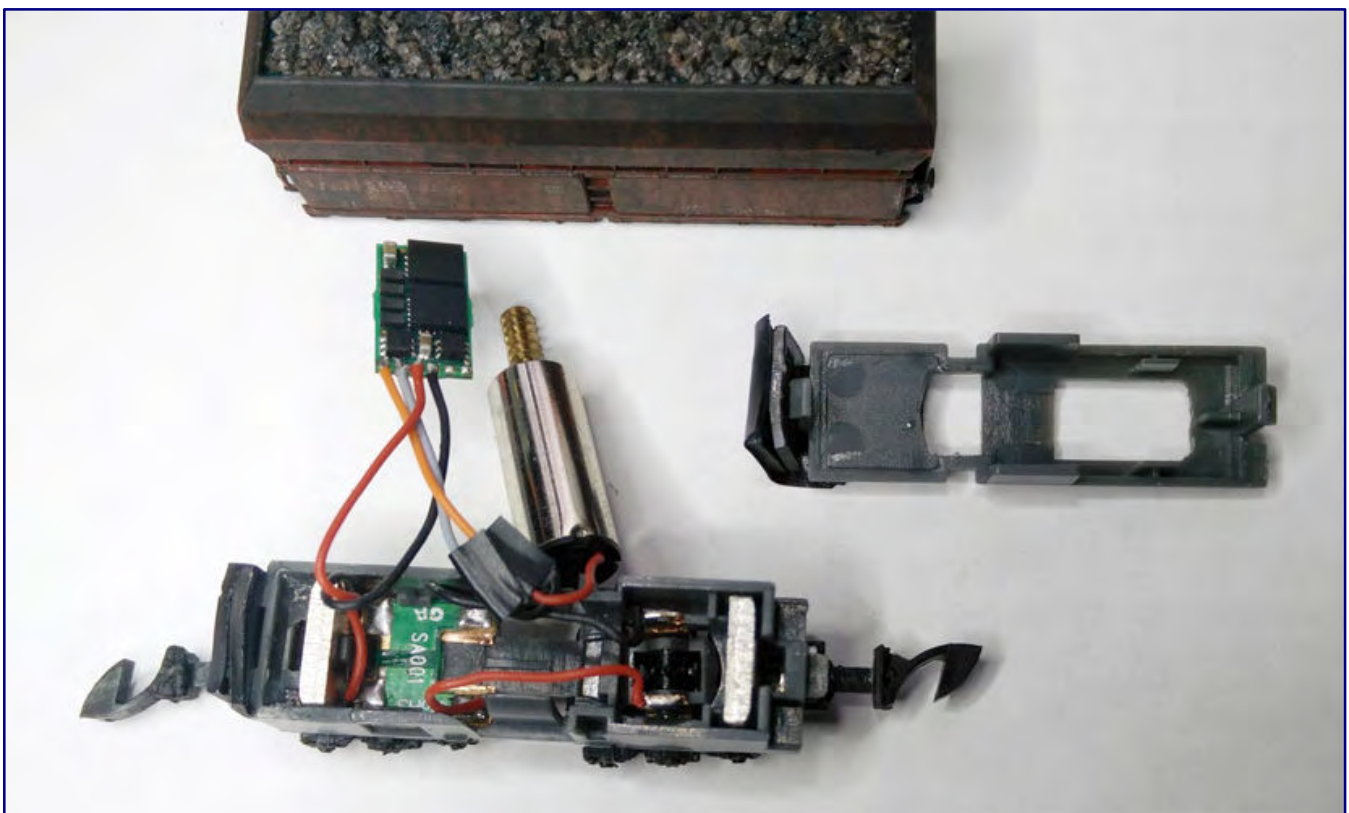
The couplings on the Shorty that are not Märklin compatible can be easily removed and replaced. Suitable replacements with Märklin compatible couplings on long or short shafts are available in the Rokuhan accessory range.

Readers who want to convert another model into a ghost car may want to consider using the Rokuhan couplings in the short version. Alternatively, the Rokuhan couplings can be cut off at the base of the claws and glued to the claws of the Märklin couplings instead. Thus it would be possible to realize length compensation, if the total length of the body does not fit.

The assembly is then carried out in reverse order. At this point, however, I would like to mention a few special features:

- I changed the live connection between the front bogie and the circuit board in the car floor. The reason is simply that the very thin wire used in the factory broke several times. I used some flexible decoder strands and soldered them to the contact lugs of the front bogie and to the circuit board.

These strands are laid over the motor during assembly and then laterally past it. It is best to mount the front bogie, then insert the circuit board and guide the strands in an arc. For reasons of operational safety, I have enlarged the existing front (smaller) cutout in the cover of the shortys: I have cut in a slight curve.



The fine enamelled copper wires were replaced by red and black strands after several breaks, which were fitted, as described in the article. Photo: Thomas Heß

The strands I use can be laid in the area of the front bogie in such a way that they have more room to move and can “work” by means of a loop. This way the swivel of the bogie is not hindered by stiff strands.

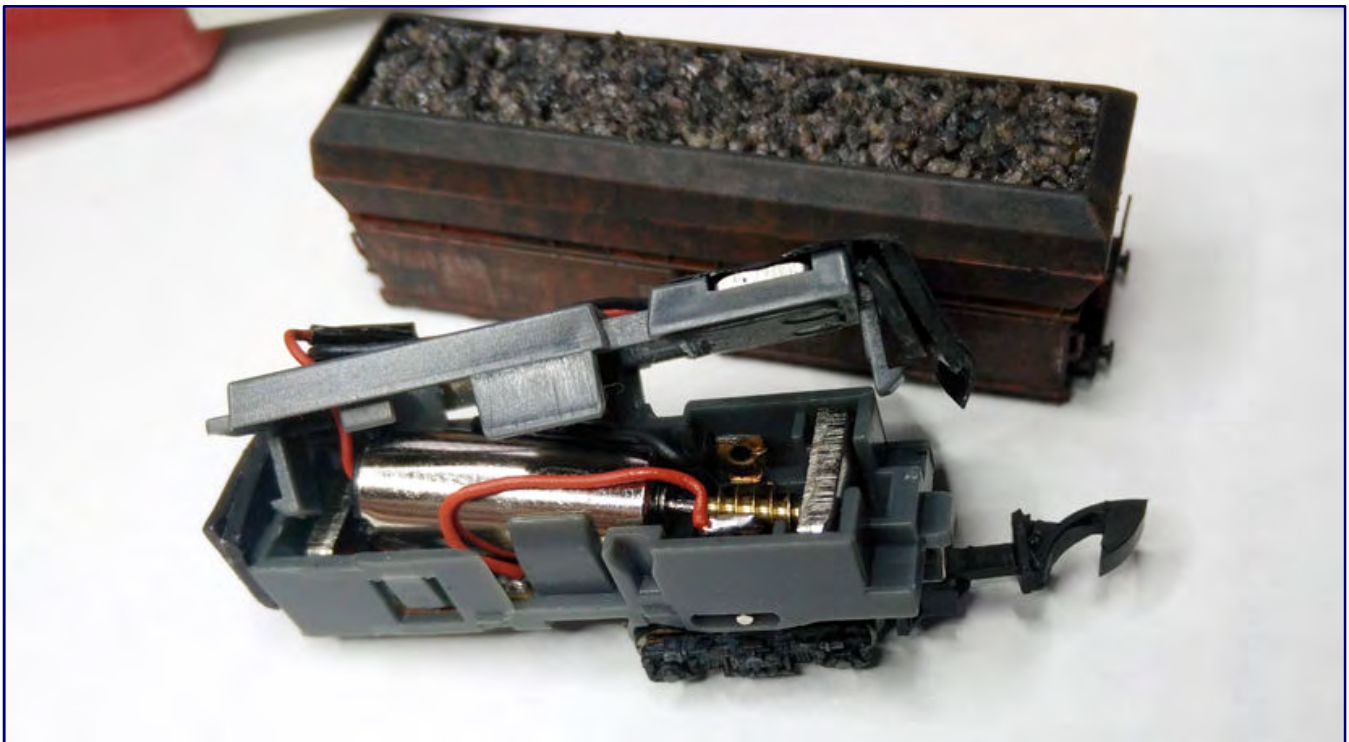
- When mounting the motor, make sure it is seated correctly. The motor is located at an angle in the locomotive housing. In the area of the circuit board underneath, it must find its seat on a small shoulder. It is snapped laterally into the frame.

The gear worm must engage cleanly with the plastic gearwheel on the drive axle at the front, if the bogie is already mounted. When fitting the cover, always make sure that the connecting wire strands are not crushed.

- For operational safety, I also used small pieces of cut rolled lead in addition to the existing plates in order to increase the weight. These were glued on the front and on the lid of the Shortys.
- The DH05-C locomotive decoder from Doehler and Haass is suitable for digital upgrades. This is placed on the lid of the Shortys. The current-carrying connection is made with thin, flexible strands. The rear cutout has been extended to the rear, the dimension is about one strand thickness.

Thus the connecting strands (4 pieces: 2 x track and motor each) can be laid better and the decoder itself can be threaded better. The engine seat is not disturbed, because it is secured by lateral cheeks.

If you don't want to use a decoder, you don't have to do this step, the wiring remains completely inside the Shortys. For use with bell-shaped armature motors, D&H gives advice on programming the CV values on its pages (in my case, for the DCC format).



For the conversion of our reader the choice fell on a decoder of the type DH05-C of Doehler and Haass. With the described adjustment work the tiny one could be accommodated in the ghost car. When reassembling the Shortys, make sure that no wires are crushed. Photo: Thomas Heß

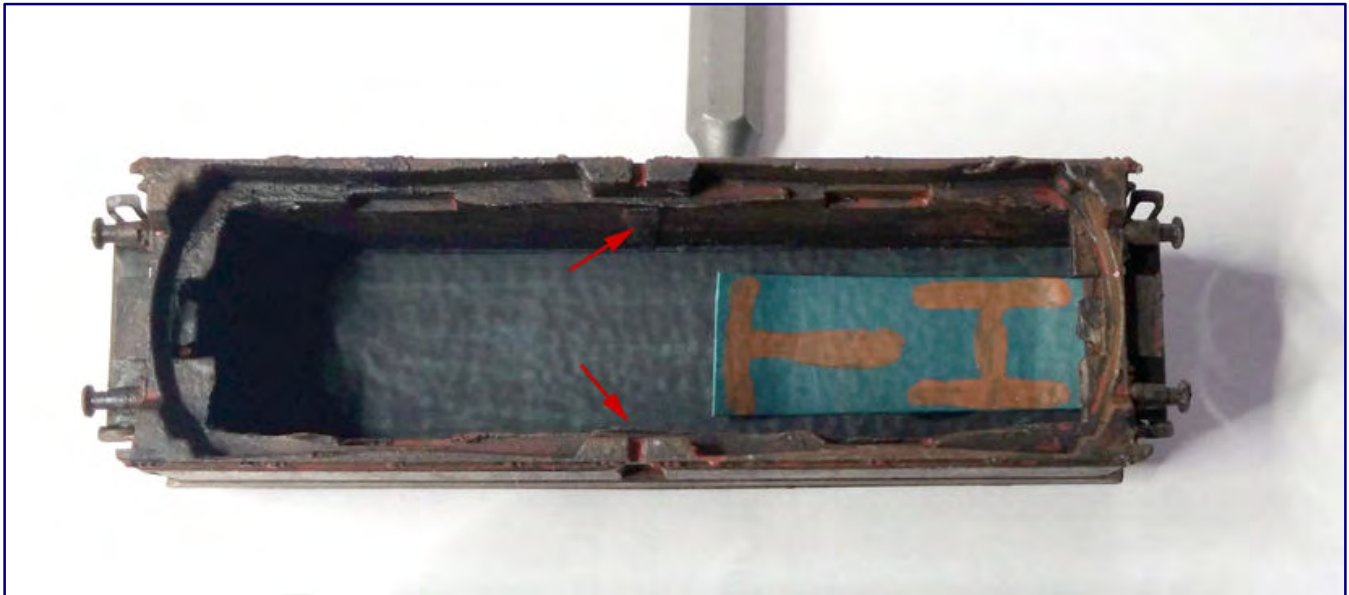
Customizing the Base Car Body

In my case the case of the Märklin self-discharging car Fads 176 (Art.-No. 8630) is used. First its bogies are removed. They can be carefully prised out with a jeweller's screwdriver. The same procedure is used for the metal pouring surfaces.

Since the floor of this car is not removable, the floor must be removed with a sharp handcraft knife. I have removed it almost along its entire length. But you have to make sure that the cut-out is not too wide or asymmetrical to the longitudinal axis of the car.

On the one hand, the width should be just wide enough so that the car is still firmly clamped into the chassis without losing its seating, but, on the other hand, not wide enough so that the side walls of the housing are spread out. Therefore, it is advisable to constantly check and adjust the seat during the work.

In the middle of the vehicle there is an upwards pointing bridge at right angles to the direction of travel, which must also be removed and requires special care. For its removal, I used a small side cutter for the first two cuts. If the remains of the bridge remain, this can also have a positive effect on the housing seat. Here, too, the rule is to remove only as much as is necessary.



In the middle of the car body, a crosspiece had to be removed so that it could be placed as a “little hat” on the machined Shorty chassis. At the top the model closes a self-made load insert, the bottom of which can be seen here together with the marking for the suitable side of the chassis. Photo: Thomas Heß

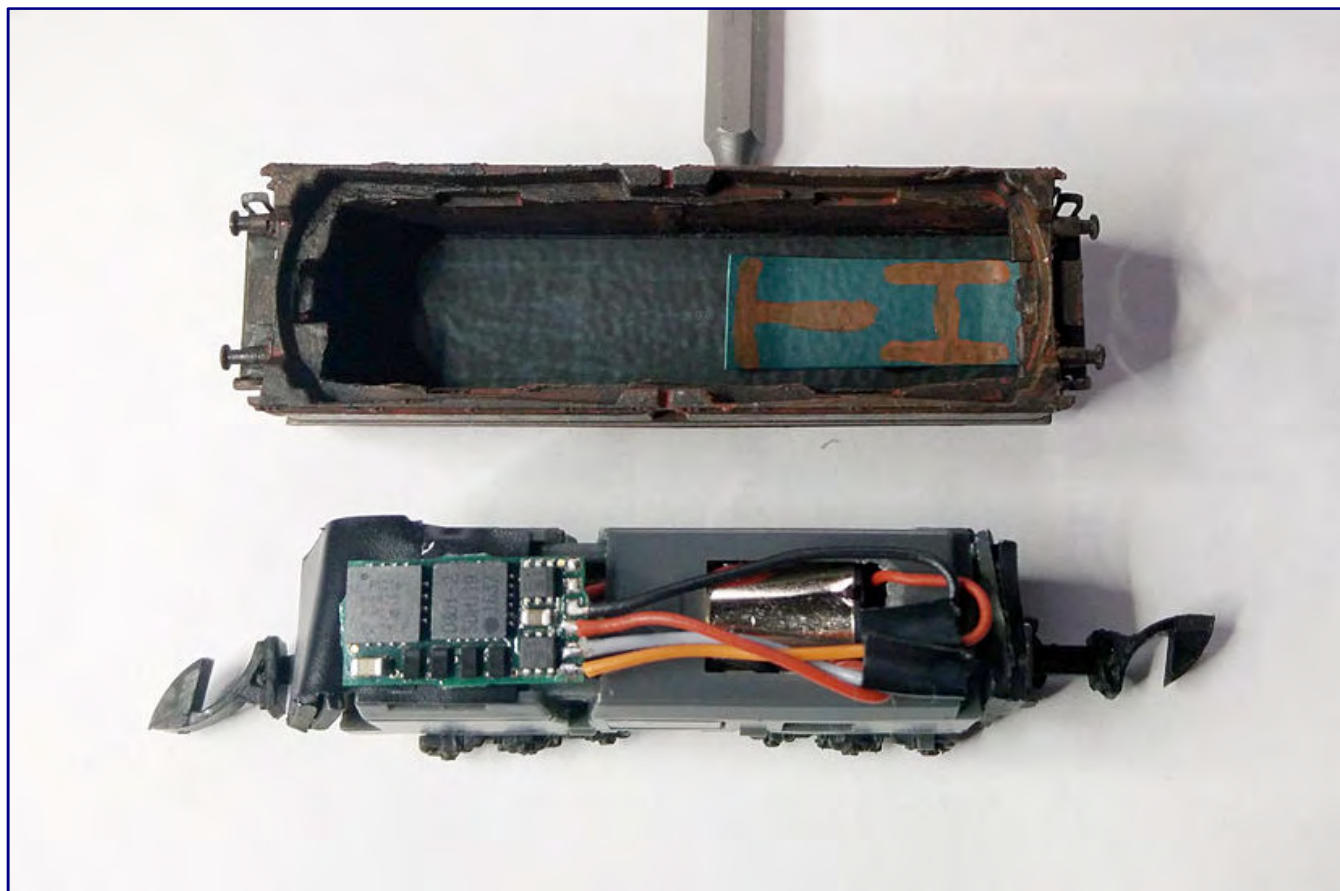
I cut the insert for the load from the back of a plastic folder, inserted it from below and then glued it together. Then I applied gravel and coloured it discreetly with well diluted acrylic paint. This insert is also used to adjust the height of the housing on the locomotive frame.

The height fits on one side at the front because the load insert rests on the decoder. At the rear, however, the resulting misalignment must be corrected: For this, I glued a second piece of plastic on one side. Its other end can be bent and set up, as required. By a spring effect now a height compensation takes place and the inclination is corrected.

To hide the plastic shine of the original case and the Rokuhan bogies, I used brown, black and anthracite acrylic lacquers and, mixed differently, applied them with a fine brush. Their application was strongly diluted and in several layers. So far, no clear coat has been applied, because I don't know which coat is suitable.

The conversion was a lot of fun for me. I think the result is quite respectable in relation to the effort involved. The tractive force has been increased to such an extent that nine wagons with loads are no longer a problem, even on the “Winzlinger Steige (incline)” with 4 per mille.

In pushing mode, however, the ghost wagon weakens with so many wagons - probably because only the front of the two bogies is driven. The overall running smoothness is, however, very good, and the contact reliability is impeccable.



All work has been completed and the car body can be placed on its new chassis. Equipped with the digital decoder placed on top, it will teach the locomotive models without drives to run in the future. Photo: Thomas Heß

I am also very satisfied with the cost side, as I have used a basic type that is easy and inexpensive to procure. Shorty chassis, decoder, adhesives and other working materials included, this project only costs about 70 Euro.

Manufacturer pages to the article:
<https://doehler-haass.de>
<https://www.maerklin.de>
<https://www.rokuhan.de>

European source of supply :
<http://www.1zu220-shop.de>

Suppliers outside of Europe:
<http://www.rokuhan.com>
<http://www.ztrackcenter.com>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Die Baureihe 64 im EK-Portrait **Geschichte der Bubiköpfe**

Die besten Baureihenbände erscheinen für viele Vorbildfreunde zweifelstfrei im EK-Verlag. Das neue Werk zur beliebten Baureihe 64 scheint nahtlos daran anzuknüpfen. Zu beachten ist allerdings, dass es sich hier um die überarbeitete Auflage eines früheren Buches handelt. Deshalb stellte sich uns besonders die Frage, für wen sich der neue Titel wohl lohnen mag.

Peter Melcher
Die Baureihe 64
Die erfolgreiche deutsche 1' C 1'-Einheitstenderlok für Nebenbahnen

EK-Verlag GmbH
Freiburg 2019

Gebundenes Buch
Format 21,0 x 29,7 cm
344 Seiten mit 537 S/W- und 47 Farbbildern

ISBN 978-3-8446-6032-6
Best.-Nr. 6032
Preis 49,90 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Die Einheitsdampflok der Baureihe 64 war eine erfolgreiche und häufig gebaute Tenderlok der Deutschen Reichsbahn, die in ganz Deutschland über immerhin vier Jahrzehnte bis in die siebziger Jahre hinein zuverlässig ihren Dienst versah. Zu sehen war sie vor allem vor Personenzügen auf Nebenbahnen, aber auch Übergaben im Güterverkehr oblagen ihr dort nicht selten.

Vor über 30 Jahren erschien im EK-Verlag erstmals ein Baureihenportrait des beliebten Bubikopfes. Und auch wenn ihre Betriebsgeschichte längst abgeschlossen scheint, lohnte sich hier auf jeden Fall eine Neuauflage des Buches.

Das lange Warten hat sich da auf jeden Fall gelohnt, denn herausgekommen ist in diesem Fall sogar ein völlig neues Buch: Im Umfang deutlich gewachsen zeigt es sich mit vielen neuen Fotos und erheblich besserer Bildwiedergabe als im früheren Werk aus dem Jahre 1987. So geben sowohl die Auswahl der Aufnahmen wie auch deren Darstellungsqualität keinen Anlass zu Klagen.

Profitieren werden seine Käufer zudem auch noch von vielen Erkenntnissen, die zwischenzeitlich gewonnen werden konnten. Geschlossen werden beispielsweise viele Wissenslücken bei Maschinen, die nach dem Zweiten Weltkrieg in Ostblockstaaten verblieben sind. Trotzdem sind viele Dinge hierzu nicht dokumentiert worden oder Informationen unzugänglich geblieben, weshalb auch heute noch Lücken bleiben müssen.

Das ist dem Buch leider auch bei einigen Bildlücken und Aufstellungen zur Stationierung anzumerken. Doch es schadet dem insgesamt hervorragenden Werk nicht, denn eine derart umfassende



Zusammenfassung von Entwicklung, Konstruktion, Einsatz, Bauartänderungen, wie auch Beheimatungen und Verbleib der Lokomotiven ist bislang einmalig.

Die herausragende Qualität der EK-Baureihenbände zeigt sich folglich auch bei diesem Buch ein Mal mehr. Zu verdanken ist dies der Akribie des Autors und begleitender Unterstützung durch EK-Chefredakteur Jörg Sauter.

So wird auch die enge technische Verwandtschaft zu den Baureihen 24 und 86 bestens herausgearbeitet. Die umfasst sowohl die beabsichtigten Vorteile wie auch daraus resultierende Kompromisse. Im Bereich der Einsatzgeschichte umfassen die Ausführungen sowohl die deutschen Bahnverwaltungen der Vor- und Nachkriegszeit als auch andere Länder, in die es einzelne Maschinen oder eine größere Zahl von ihnen verschlagen hatte. Nicht vergessen wurden erhaltene Museumsexemplare.

Der vorliegende Titel empfiehlt sich deshalb für jeden Freund der kleinen, aber erfolgreichen Tenderlok und verweist gleichzeitig seinen Vorgängerband deutlich aufs Abstellgleis. Wer ihn bereits sein Eigen nennt, kann ihn bedenkenlos durch die neue Auflage ersetzen oder ergänzen. Letzteres macht insofern ebenfalls Sinn, weil viele Bilder ja gleich ausgetauscht wurden.

Publishing pages with reference possibility:
<https://www.ekshop.de>

Schwarzer Schwan im Filmportrait Das Beste zum Schluss?

Die Baureihe 10 sollte alles in den Schatten stellen, was je auf Deutschlands Gleisen herumgedampft ist. Sie war zur Nachfolge der Baureihe 01¹⁰ bestimmt und sollte den deutschen Dampflokbau krönen. Doch mit den Ansprüchen wuchsen auch die Ideen rund um ihre Konstruktion in den Himmel. Als sie fertig war, wurde sie gar nicht mehr gebraucht. Ein Filmportrait erinnert an die beiden Vorserienmaschinen ohne Zukunft.

JS-Filmproduktion
Die Baureihe 10
Die größte Schnellzugdampflokomotive der Deutschen Bundesbahn (Krupp 1957)
aus der Reihe „Stars der Schiene“ (Folge 19)

VGB Verlagsgruppe Bahn GmbH
Fürstenfeldbruck 2010

DVD-Video
Bildformat 4:3
Tonformat Dolby-Digital 4.0
Sprache deutsch
Laufzeit ca. 55 Min.

ISBN 978-3-89580-660-5
Best.-Nr. 6319
Preis 16,95 EUR (Deutschland)

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Gerade einmal zwei Exemplare wurden von der legendären „Zehner“ gebaut. Ihre lange Vorgeschichte hatte zur Folge, dass die schwere Pazifik am Ende zu spät auf den Gleisen erschien. Zu weit war der Traktionswandel bereits fortgeschritten und die V 200 hatte einen großen Teil ihres Einsatzgebiets besetzt.

Große Fortschritte machte auch die Elektrifizierung der großen Magistralen, wo mit der E 10 eine Ellok dominierte, die jeder Dampflokbau weit überlegen war. 1950 war das noch anders. Da dachte die junge Bundesbahn noch, auf lange Sicht weiter auf die Dampflokbau setzen zu müssen.

Deshalb entwarf sie ein Neubauprogramm, dessen Höhepunkt eben die dreizylindrige Schnellzuglok der Baureihe 10 war. Teilweise verkleidet, sollte sie durch Schnelligkeit, Leistungsfähigkeit und Eleganz Aufsehen erregen.

Krupp setzte 1957 mit zwei Exemplaren einen Schlussstrich unter dieses Neubauprogramm: Zwar wurden bis 1959 noch Maschinen der Baureihe 23 abgeliefert, aber die Entwicklung von Dampflokbau war nun endgültig vorbei.

Mit nur knapp über zehn Jahren Betriebsgeschichte, nie vollständig abgestellten „Kinderkrankheiten“ und einer schwierigen Ersatzteilversorgung aufgrund ihres Status als Splittergattung sind Aufnahmen von dieser besonderen Reihe eher selten. Zudem schieden sich die Geister, ob dieser Typ nun gelungen und angenehm zu fahren sei oder lieber gleich aufs Abstellgleis wandern sollte.



Die Filmmannschaft um Joachim Schmidt, auch Gründer der Eisenbahnstiftung, hat es dennoch geschafft, ein spannendes Portrait zu drehen. Es ist zwar schon fast zehn Jahre alt, was besonders am Filmformat 4:3 zu erkennen ist.

Dennoch ist es unverändert aktuell und passt hervorragend in das Thema dieses Monats über Museumsbahnen. Für das filmische Denkmal musste aber auch in die Trickkiste gegriffen werden, denn nur eine der beiden Maschinen blieb mit 10 001 in Neuenmarkt-Wirsberg erhalten und ist nicht betriebsfähig.

Trotzdem ist es gelungen, die betagten „Schwarzen Schwan“ wieder zum Leben zu erwecken. Sie pusteten ihm Dampf um die Zylinder und sorgten auch dafür, dass es aus den beiden Schloten wieder zu rauchen scheint.

So vermitteln die neu gedrehten Farbaufnahmen einen guten Eindruck, was diese schwere Schnellzuglok ausgemacht haben muss. Natürlich dürfen dabei auch historische Aufnahmen nicht fehlen, die ein solches Portrait erst abrunden und auch den großen Anteil an der Gesamtlaufzeit haben. Doch diese sehen halt auch alt aus, weil sie eben das typische Flimmern und die dunklen Ränder der damaligen Super-8-Filmstreifen aufweisen.

Einmalig wird diese JS-Filmproduktion allerdings durch seltene, wohl einmalige 35-mm-Aufnahmen aus den Krupp-Hallen, die den Bau dieser besonderen Krönung der Dampftraktion zeigen und einen ebenfalls beachtlichen Anteil am Gesamtportrait einnehmen.

Klug entschieden war es, alle diese Beiträge nicht nur mit neuem Material einzurahmen, sondern mit Aussagen und Erläuterungen der Experten Manfred van Kampen und Gerhard Modell sowie des früheren Lokführers Rudolf Henning moderierend zu verbinden.

So ist in Summe ein unterhaltsames, höchst informatives und spannendes Werk entstanden, das unter der Bestellnummer 6319-e inzwischen auch als Videostream (für 9,99 EUR) zu erwerben ist.

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: **Publishing pages with reference possibility:**

: <https://shop.vgbahn.info/vgbahn>

: <https://www.riogrande.de>

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www.Echtdampf-Hallentreffen.de



Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

To the letter from Mr. Severloh (Trainini® 8/2019) we received the following answer:

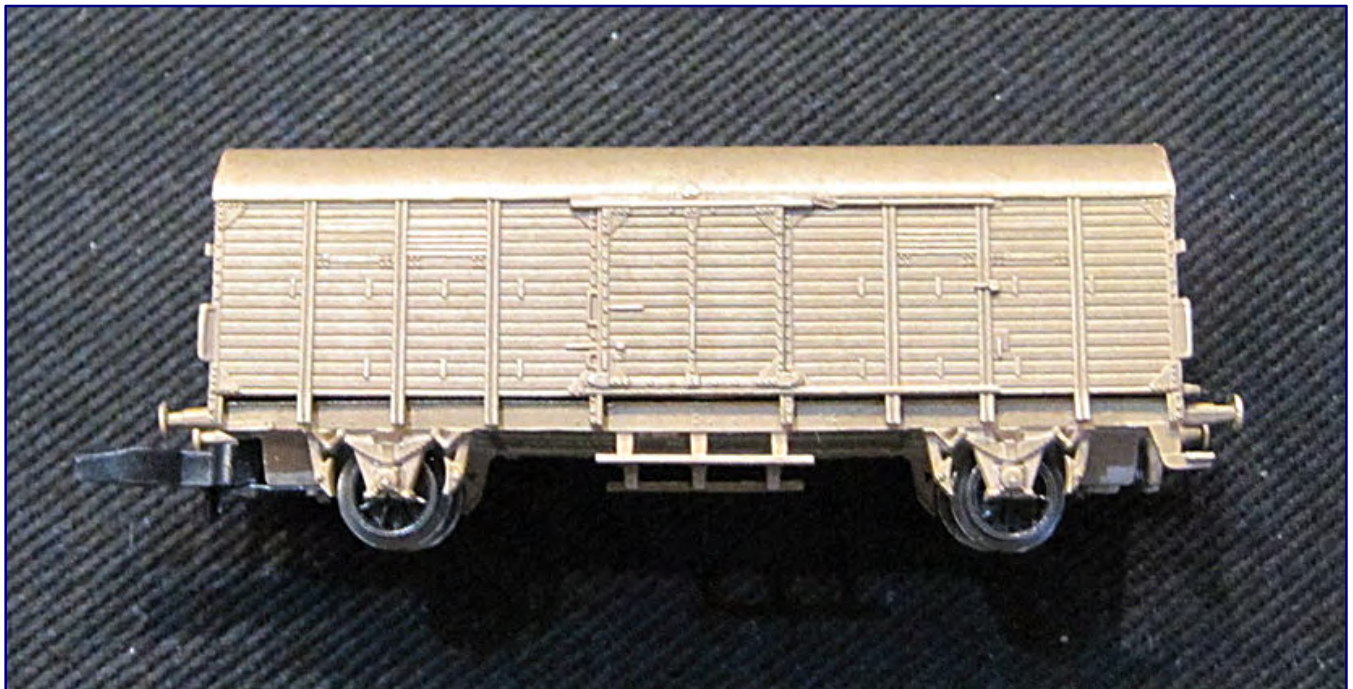
This letter to the editor was about the various controllers by Jörger and Passmann. There is a top specialist for Z-controllers. Have a look at them: <https://klaus-steuer.jimdo.com/>.

He is very competent, friendly and helpful, and also builds according to special customer wishes. My grade is 1+! Let's see what you can report.

Eckehart Tijssen, by E-Mail

These are Märklin's autumn new releases 2019:

As expected, among the autumn new releases, presented at the Göppingen exhibition is the 101 064-4 of the Deutsche Bahn AG (Art.-No. 88677) with its anniversary design. We had already made a that assumption in the last issue. Like the second model, which is to be presented at the same time, it has a bell-shaped armature motor and direction-dependent LED lighting on both ends.



The chassis and body are cast in bronze for the autumn new release of the covered car of the genus "GI Dresden" (Art. No. 82170). In contrast to the original, the model has spoked wheels, and these are insulated on both sides. Photo: Torsten Schubert

With the bronze cast crocodile Ce 6/8III (88565) Märklin starts a new fine casting series which is based on the experience the manufacturer had gained with the 80 series for Insider members. The heavy model is intended to appeal to collectors and is given a transparent protective lacquer.

This also applies to the covered freight car "Gl Dresden" without handbrake (82170), whose chassis and body are also cast and which appears in the same fine casting series.

This year's Christmas car (80629) also serves the collectors' clientele. It is a festively designed low side car (base 8610) with a group of snowmen made of transparent Plexiglas on the loading area.

This Christmas car 2019 is presented in a transparent snowman with one half of this tree trailer transparent orange. It can be attached to the fir tree with a cord.

The service railroaders will also be provided with a "side dump car" (82436). The three Fas 126 side dump cars of Deutsche Bahn AG (DB AG) are labelled in accordance with Era VI and carry "gravel piles" load inserts.

A green eye-catcher is now on its way:

It was listed in the collector's catalogue under the serial number 1795: We are talking about the annual train car 2018 of the Z-Freunde International e.V. (Z friends international), which could also be acquired by Z forum members who are not members of the supporting association.



The Z friends international have made a very special model out of their annual car 2018 (Art.-No. 8612.046) with a lot of effort: The green-painted ascension ladder, walkways and railing, as well as the yellow UIC rope clasps, together with the multi-coloured printing, bring it very close to its historical original.

The entirely green painted tank car with the white inscriptions BfB (Federal Monopoly Administration for Spirits, Art.-No. 8612.046) has been manufactured and printed by Märklin. A unique 150 copies of this beautiful model have been produced.

The model, which was elaborately painted and extensively printed, including the ascension ladder and railing, is modelled on a two-axle tank car of the former BfB, which was operated by the German Federal Railways.

Since July, the models have been on their way to their owners, who will certainly be pleased about this eye-catching, but also true to model, splash of colour on their layouts.

Faller new products 2019 for Z gauge now official:

With its novelty brochure 3/2019 Faller has officially announced what we had already announced in the February issue. For gauge Z, two new architectural kits made of laser-cut hardboard will be released this year.



The church “St. Johannes Baptist” (Art.-No. 282778; left) and the Cinema Kandelhof (282795; right) are the new releases of Faller 2019, which were officially announced only now, in autumn.

The Kandelhof Cinema (Art. No. 282795) is a typical cinematic theatre of the economic miracle era, housed on the ground floor of a four-storey, municipal tenement building with a hipped roof. The kit includes lettering and billboards for completing the entrance area.

The current series of townhouses will also be supplemented by the Church of St. John the Baptist (282778), a hall building with a saddle roof and apse. The tower with the pointed steeple is set at the side. Both buildings were modelled after real buildings.

The following new products have been released by Märklin:

The orient red diesel hydraulic locomotive 218 286-3 in experimental design for the colour scheme of the DB product colours introduced in 1986 (art. no. 88780) was the special model for the Toy Fair 2019. It shows the operating state of its model around 1987/88, when the first machines of other series were already repainted.



The 218 286-3 in orient red test paint (Art. No. 88780) also received slight traces of operation in the form of soot deposits in the roof area.

Now the locomotive, packed in a real wood box, has arrived at the dealers and is attracting attention with its good driving characteristics. The reason for this is the conversion of their chassis to the bell-shaped armature motor, which has become the new standard.



With its large lettering, the Miele car (80329) will in future be an eye-catcher on the facilities of Insider Club members.

With a weight of 26 g, the converted model has gained 2 grams in weight, and the driving characteristics have also improved further.

The model has also benefited from larger buffer plates and changed lighting, as the peak/final signal changes between warm white and red.

This year's Insider Club 80329 with its large Miele lettering on the light grey walls of the body is also very appealing. The freight car (Era III) rented by the Bundesbahn to the

Gütersloh household appliance manufacturer belongs to the category GI 11 and is an eye-catcher in every freight train.

Discount on admission to Faszination Modellbau:

At the "Fascination of Model Making" trade fair taking place in Friedrichshafen from 1 to 3 November 2019, **Trainini®** readers can get a discount on admission. All they have to do is show the corner of the event advertisement placed in this issue when they purchase their admission ticket. This year the fair will be 25 years old and open daily from 9:00 a.m. to 6:00 p.m., on Sunday it closes one hour earlier. The Z-Freunde International e.V. has once again organised a colourful and attractive contribution for Z gauge.

New small series manufacturer makes a name for itself:

Why Björn Plutka made “Z-Doktor Modellbau” (<https://www.z-doktor.de>) his company name, he explains on the phone with great enthusiasm. In his spare time, he designed and printed 3D models on a computer.

And since they were much too good to be thrown away, but he couldn't or didn't want to keep them all, he started to give them away to his friends. Also with technical problems, he helped gladly and repaired locomotives free of charge, which were not operating.



The Eicher Königstiger (Art.-Nr. ZD-220-00002-X) shows how finely the 3D-printed models of Z-Doktor Modellbau are produced. The ladder trailer with coupling produced by him using the same production method is, so far, a unique piece.

To the astonishment of its owner, a vehicle that had already been written-off experienced a “rebirth”, and so the owner proudly thanked Björn Plutka with the compliment that he was a true “Z-Doctor”. Thus he acquired a good reputation as a helpful and competent Zettie, which finally led to a small series company.

As a free service, he also offers a limited range of simple repairs. If things get more complicated, he makes the customer an offer beforehand. His main activity, however, is 3D printing, where he seems to be exploring the limits of what is technically possible.

For example, a buggy was created for fun, which we would like to show as one of the extremes for the children's world on a scale of 1:220. The offer includes extremely precisely designed and finely detailed models of a “Formula Z” racing car (Art. No. ZD-220-00003-X), the Lanz Bulldog tractor (ZD-220-00001-X), Eicher Königstiger tractor (ZD-220-00002-X) and the tiny Holder A12 ride-on tractor (ZD-220-00004-X) with a total length of only 9.5 mm.



In order to represent the size and scale of Z-Doktor Modellbau's buggies, we already had to reach into the bag of tricks: If the coin illustrates the tiny size of the models, the added Preiser figures prove that the prams were printed in the right size.

All these models are ready varnished, unpainted, or also as light coloured defective specimens. The interior fittings for Märklin passenger coaches are also worthy of note. Already available is a four-part set for the Langenschwalbach passenger coaches (ZD-220-20001-X).

True to the motto "The best for last", we can't avoid a car model that is the only one that can be used with a wink in all the model railroad epochs that have already been defined and that can still be imagined in the future: In view of the success of the DeLorean DMC-12 (ZD-220-00005-X) we don't have to say any more words about it.

So far, the models are primarily available through direct sales. With the 1zu220-Shop (<https://www.1zu220-shop.de>), a first sales partner has already been added.

Full streets thanks to EtchIT:

EtchIT Modellbau (<http://www.etchit.de>) solves delivery problems on a scale of 1:220. Small commercial vehicles are currently on the agenda for new products in early autumn. Panel vans with a short wheelbase (Item No. ET014a_Z), a long wheelbase and a high roof (ET014b_Z).

The vehicle is also available as an eight-seater minibus (ET015_Z) and as a platform truck with short



The volume display shows all five body variants of the new, light commercial vehicle including their assemblies. They are also offered in this form as a grouped package (Art. No. ET014s_Z). Photo: EtchIT Modellbau

cabin and low side walls (ET016a_Z), as well as a double cabin for five persons and high side walls (ET016b_Z). All five vehicles are also available together at a reduced price (ET014s_Z).

The current deliveries of AZL:

American Z Line proudly announces the launch of its California Zephyr packs. The model train was jointly operated by CB&Q, D&RGW and WP from 1949. One train consisted of ten cars at first, supplemented by an eleventh from 1952.

The trains were pulled by EMD F3 units, which were assembled differently depending on the railway company: A-B-B-A (CB&Q), A-B-B-A (D&RGW) and A-B-B (WP). This can be achieved by individually combining the following locomotive packs:

EMD F3 A-B of the D&RGW (Art.-Nr. 62915-1)
EMD F3 A-B of the D&RGW (62915-2)

EMD F3 A-B-B of the WP (62916-1)
EMD F3A of the WP (62916-2)

EMD F3 A-B-A of the CB&Q (62917-1)

The two wagon packs, each containing a complete train, bear the article numbers 72100 and 72101. The additional wagon that completed the train is offered separately as 10-6 sleeping wagons (72104).

On the way to the customer are also light passenger coaches in grey design of the NYC. There is a choice of 4-4-2 sleeping (73007-1 to -4), dining (73507-1 / -2), baggage (73607-1 / -2), seating (73707-

0), pulpit (73807-1 / -2) and mail coaches (73907-1 / -2). Suitable EMD E8A locomotives (62604-3 / 62604-4) are still available from AZL.

Photos from the manufacturers of the current deliveries can be found under <http://www.americanzline.com>.

New Products from Noch for the autumn:

The accessories specialist from the Allgäu region has also presented seasonal innovations for autumn and winter, including a product that is interesting for Z gauge. 104 tufts of grass, each 6 mm long, are counted in the pack "tufts of grass dark green, medium green, brown, golden yellow" (Art. No. 07009).

New catenary line from HOS Modellbahntechnik:

At the same time as the architectural kits for the Altenbeken viaduct published at the 1zu220 shop, HOS Modellbahntechnik presented its own overhead line system suitable for installation in the bridge section and based on the DRG overhead contact line.

In the meantime, the range has grown considerably and has also reached the Re 160 DB overhead contact line, which was developed after the war for maximum speeds of 160 km/h. The new system is now also available in the form of the DRG overhead contact line. It is also the model for the Märklin system, which is no longer up to date with punched contact wire parts.

In keeping with its geometry, Heinz O. Schramm, also sold by the 1zu220 shop, therefore offers finely etched replacements that allow the pantographs to be put on in silver or dark stained and meet today's requirements. Also, we plan at present a showpiece with these elements.

One advantage is that assembled Märklin masts do not have to be removed. Suspended overhead lines can easily be removed and replaced. Nevertheless, HOS Modellbahntechnik also offers its own mast bases, which give Märklin's lattice masts a correctly shaped base.



The reseda green tower masts are available from HOS Modellbahntechnik in different equipment variants: with top light (Art.-No. OL 04; far left), in a simple version (OL 03) for attaching cantilevers (OL 17), as well as with wheel (OL 13; 2nd from right) and the older lever tensioning system (OL 05; far right).

In connection with the new presentations for the Märklin Days, gaps in the Märklin range were also closed, which make the design of the track systems really complete for the first time.

We noticed a reseda green 62 mm high overhead tower mast (Art. No. OL 04) made of 0.15 mm thick nickel silver. It is not only used to hang cross-beams, but also has an additional (LED) light to illuminate the track area in darkness.

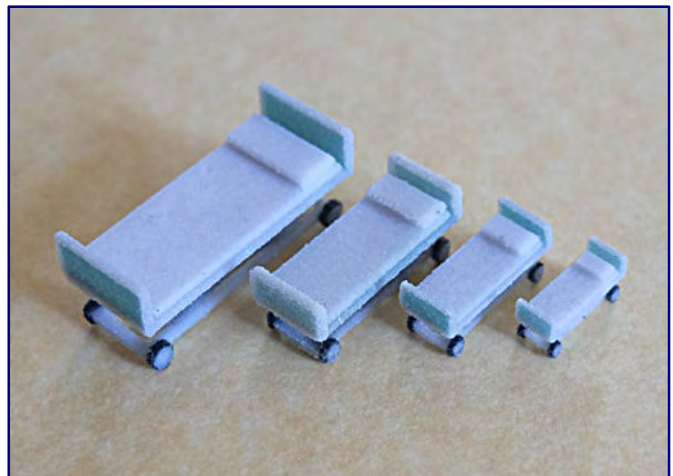
It is also available as a double pack (OL 03) without a luminaire attachment and can then also be fitted with separately offered DRG brackets (4 pieces; OL 17), as this pole type was widespread on double-track lines from around 1928 onwards.

Also important are the two masts with clamping mechanisms, as they can be found at the end of electrified sections, in order to keep the cables tight, to prevent excessively strong oscillating movements and to counteract wind forces, these important elements are usually neglected in models.

HOS Modellbahntechnik offers two versions: With the lever tensioning station (OL 05) it corresponds to the Reichsbahn designs, which remained in use even during Bundesbahn times; with the wheel tensioning station (OL 13), it fits better to the lines first electrified by the Bundesbahn.

Two new items from Schrax:

3D printing producer Schrax has introduced two rather unusual new products that are also available for Z gauge. The colorful carousel gondola fits well with modern rides and can be integrated into your own creations, because there is a lack of suitable basic models from series production on a small scale.



The illustrations show both the new carousel gondolas (picture left) and the hospital bed (picture right) in various sizes, of which the Z-gauge is the smallest in each case. Photos: Schrax

The new hospital bed in light blue on wheels appears more common place here. This is a familiar item which is known to everyone. A suitable hospital, with sufficiently large windows for a view into the interior, should be quite easy to build by yourself or by adapting kits.

These two new products are also available, as usual, at <http://www.schrax.com>.

And here's what you can get from Micro-Trains:

The farm-to-table series goes into the next round with car number 8. This time the model is a boarded refrigerator car from Mid-West-Catsup (Art.-No. 518 00 780). Boxcars with single sliding doors are also two other new models, each of which is offered with two company numbers.

Colourful are the "Cushioned Ride" cars of the Great Northern (505 00 401 / -402), which stand out with green paint and red doors. More common on the road are the ATSF (505 00 411 / -412) models with white and yellow "The Chief" inscriptions.

The eight-pack of modern TTX carrying wagons (994 00 812) has the manufacturer with rust and graffiti provided, as it is also widespread in the United States. Without such patina the four yellow passenger coaches of the CNW (994 01 240) get along of course.

Micro-Trains products can be purchased in Germany at Case-Hobbies, among others. (<http://www.case-hobbies.de>).

Eisenbahnfreunde Marsberg celebrates their anniversary:

On the weekend of 20/21 September 2019, the Eisenbahnfreunde Marsberg e.V. celebrated their 40th anniversary with an operating day. Four founding members are still represented in the circle of active members, who were honoured on this occasion even after the event closed on Saturday.



The two operating days on the occasion of the 40th anniversary of the Eisenbahnfreunde Marsberg e.V. met with lively public interest.

In the former supermarket building at Waldecker Straße in Marsberg-Westheim the club presented its H0 system and other showpieces in the same scale and for track 0. Also Z gauge was well represented. In showcases Jörg Erkel showed various models of 0 and Z gauge; Rita Kruse-Spiekermann knew how to present some dioramas with her self-made trees.

Jan Tappenbeck had come especially from the Hanseatic city of Lübeck to contribute to the success of the exhibition with two dioramas of his own. One showed a museum steam locomotive depot on the basis of a three-storey Märklin round shed with turntable.

With the second copy he also promoted his association, the Eisenbahnfreunde Bad Schwartau e.V. The object of this showpiece was the former signal box in Bad Schwartau, where this community has now settled.



Homage to the Heimatverein: Jan Tappenbeck showed the signal tower of Bad Schwartau, where his Spur-Z group meets regularly (picture above). The thatched farmhouse (picture left) and the Blankensee signal box (picture right) also have their home in the far north. But also other curiosities could be marvelled at: The Köf in the size 0 (picture below right) is a complete self-built by Raimund Meiseberg, which this Zettie built many years ago.

He had also brought along the farmhouse “Dörpeder Hof” from Archistories from the distribution of the 1zu220 shop. At this beautiful house he demonstrated how it can be fitted with a thatched roof and thus adapted to the German coastal architecture.

He was also visibly proud of his self-made “BlS” (Blankensee) positioning system, which is intended for installation in a module.

But more important were the personal exchanges.



The association had provided food and drink and invited people to linger outside in beautiful weather. The guests thanked them for it, because they came beside the closer environment to a large part from a catchment area from Duisburg to Springe, as well as from Hamburg to Reutlingen - no distance seemed too far, to join the crowd of visitors.

And so the occasion was used for a joyful reunion, showing and trying out newly acquired or handmade models and, of course, a lively exchange on railway related topics. In the end, everyone agreed that it was over much too quickly...

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Trainini

German Magazine for Z Gauge



Weathered is the dirty new look

**Norddeich Mole, final stop?
Birthday party in Göppingen**

Introduction

Dear Readers,

when I started to write these lines, the saying “as time goes by” shot through my head...

Is it really autumn and Christmas not two months away again? The dealers are looking forward to the biggest sales of the year, Märklin will then be driving special shifts again and delivering more items.



Holger Späing
Editor-in-chief

It's filthy outside - wet, but not cold (in Germany). This is what my friends at the regulars' table in Hamburg, who claim to know such a climate very well, call shitty weather. Experts say it must continue to rain until at least February 2020 in order to fully compensate for the loss of soil water after the record summer of 2018 (again in Germany).

I wouldn't object to that if we didn't all have to go to work more or less frequently through the rain, or even to go shopping. But otherwise we wouldn't miss anything outside and could turn more to the most beautiful hobby in the world.

The Märklintage (Märklin days) in Göppingen certainly provided us with enough suggestions for this, and our volunteer helper Stephan Fuchs reports on them in detail and authentically in this edition. One result of the suggestions from outside is also the Glmehs50 boxcar, which we present to you from page 4.

We detailed it a little and aged it really hard. And if you like it, we have even more of it: A Miba report on covered freight cars has just appeared, which accompanies the Carstens book reviewed today.

The magazine is full of ideas for various train cars of almost all gauges and peppered with many original models. The book “Freight Train Cars Volume 1.2” again provides the theoretical fundament with descriptions and drawings.

In the meantime, Dirk Kuhlmann has again intensively dealt with our main topic of the year, “Layout diversity”. The result of his impressions, translated into drawings and ideas, is a construction proposal based on the model of the terminal station of the Emsland line: North Dyke Pier. Once proud “steamers” and later diesel locomotives of the 220 series could be admired here.

The number of letters to the editor and messages to the editor have also increased, because - as I wrote at the beginning - Christmas is approaching. But we also want to have a look offside the tracks.

To this we have also discovered a book about a true classic of delivery traffic, which successfully portrays the Mercedes-Benz L 319. And with the Convair CV-340 from KLM, we are commemorating a milestone in aviation. It's the mix that makes the difference and I hope it's just right this time again!

And now, with that, I officially start the 2019/20 model building season...

Sin-Z-erely,

Holger Späing

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We thank Jürgen Faulhaber and the Eisenbahnstiftung for their photo support.

Date of publication of the German language version of this issue: 28 October 2019

Cover photo:

The dirty 44 1754 snorts over one of the many goods bypasses of the Ruhr area, which lead past the settlements of the industrial workers. It is hauling dirty, covered wagons, which are used for the transport of general cargo and food.

UIC standard wagon

Hard to take

When Mini Club was first presented at the International Toy Fair in 1972, a new DB covered car was included from the very beginning. And it has bravely fought its way through the programme to this day. Märklin recently gave it a product upgrade, and thus upgraded it for years to come. The new Miba Report, which accompanied the publication of a book, encouraged us to further refine the revised model, and also to give it visible everyday traces of aging

At an early stage, the young Bundesbahn began to develop new wagon types to compensate for war-related losses and to use modern materials in freight transport. After all, it played a very special role in the reconstruction process and the beginning of the economic miracle.

Without its transport capacities, a return to a normal life was unthinkable: it removed rubble, transported building materials to the destroyed cities, and provided the necessary supplies by transporting animals and food.

Immediately after 1945, the focus was initially still on the (makeshift) repair of lines of rolling stock, but it increasingly shifted to new production of proven models, partly with slight modifications to simplified war designs which were, therefore, not future-proof.



When the young DB launched its new construction programme, it turned to UIC standard types for freight wagons. The first covered wagon after their unification was the Gln(e)hs 50. The one photographed here in 1954 with the serial number 201 203 has been permanently connected to a three-axle Leig unit as a reinforcing wagon, but had not yet been redrawn. Built in 1953, it is one of the first delivery lots with board framework. Photo: Bustorff, Eisenbahnstiftung

However, new developments also began in peacetime Europe, the basic idea of which is comparable with those of the former association types of the German state railway administrations: Uniform principles, features and later, also, uniform types were to promote cross-border use.

The aim was to ensure that foreign railway workers could also easily cope with the designs of their neighbours, employ them in international trains, and operate them safely. Only in this manner was it possible to form a common pool of wagons ("EUROP") of the various European railway administrations in the 1950s.

Its aim was no longer to simply unload third-party freight wagons and send them back to the owner empty, but to integrate them into wagon circulation and use them in a sensible way. Thus a wagon from neighbouring countries was also allowed to operate in domestic traffic in order to finally start its journey back home with new loads.

If we imagine this in our imagination, it will certainly also be comprehensible which standardisations there had to be: first of all, of course, brake equipment had to be harmonised in such a way that it also functions reliably with other freight wagons.



The Tnohs 59 heat protection wagons for the transport of bananas were prepared from covered freight wagon of the genus Glmhs 50. They had a superstructure with synthetic resin wood panels, which was to become standard in the future. When the car with the company number 568 321 [P] of Transthermos GmbH Bremen was registered in 1957, it was still almost brand-new and showed few traces of operation. Photo: Bustorff, Eisenbahnstiftung

But train and bumper equipment or running gear must also meet the requirements of third-party administrations. Coordinated clearance profiles seem almost even more important in order to rule out the risk of an accident from the outset.

And uniform characteristics also had to be established in terms of traffic and traffic tariffs. If we think of the constant increase in size of many types of freight wagons, then it is clear what problems would arise for a consignor if, for example, instead of a four-axle Eaos 106, only a two-axle O 11 was available for the type of consist.

Thus it was also necessary to have regulations and agreements on typified loading areas, loading space volumes and load capacities of the various types, including a type designation agreed throughout Europe from which the most important characteristics could be read.

Deutsche Bundesbahn was involved in this process at an early stage and adapted its conversion and new-build programmes to these requirements. Some of the later UIC standard designs can therefore be traced back to DB, not only in the passenger coach sector, where it established the "UIC Type X" with a length of 26.40 m above buffers, which is still prevalent today.

In the case of the boxcars, the coordination resulted in several designs with different lengths and capacities. An early child of this development is the Glm(e)hs 50, which is one of the most spacious vehicles, as expressed by the sub-genre symbol "l."

In the beginning the DB still chose board formwork for the superstructure, and also their simple tin brake houses were to be found on the hand-braked models. But soon the young state railway was turning to compacted synthetic hardwood panels, which were to characterise all modern cars with covered superstructures in the future.

For a long time, the Glm(e)hs 50, which is now considered here, remained the longest type of covered freight wagon in DB's inventory. The Gms 54 was the first newbuilding from the ongoing conversion programme and was even shorter.

In retrospect, it remains incomprehensible why the Bundesbahn did not accept the even longer UIC standard design for many years. It was only at the beginning of the sixties that it was turned around, until then the Glm(e)hs 50 found successors based on the same layout as a result of further developments.

These include, for example, the Glimmhs 57 as well as the almost unchanged Tnohs 59 heat protection wagons for banana transport, which were insulated on the inside.

An approach and first thoughts

In recent years, Z gauge enthusiasts have also been increasingly concerned with fine detailing and ageing of locomotives and wagons. With its Zpur® brand, KoMi-Miniaturen can certainly be regarded as a pioneer of the finest detailed models: There doesn't seem to be anything that isn't in the range there.

Also, FR Freudenreich Feinwerktechnik sells kits of its miniatures and individual parts like buffers or couplings, which enthusiasts need again and again for their projects, customizations and other projects.



Our model should have been in service for a few years already and show signs of operation. Suitable photographs were found on the Internet or in technical literature. At the same time, we took the opportunity to give the model some equipment.

It is our concern today to introduce our readers to such refinements. Michael Richter, one half of KoMi-Miniaturen, goes so far even on a small scale to omit no detail and even to install functional screw couplings.

We deliberately do not want to go quite that far at this point in order to be able to offer a broad introduction and to keep the level of difficulty manageable. Everyone grows with their tasks, but the courage to take the first step is certainly the biggest hurdle.

And so our activities find their limits where our eyes would be overwhelmed. So our fingers and tools won't reach where they can't look in the company.

With applied aging and weathering it looks a little different. That they are fully in line with the trend can also be seen in the programs of Märklin and Micro Trains in the United States.

For years, MTL has been offering freight car models that are covered in rust, dirt and graffiti. And Märklin, too, was less hesitant recently: simple aging is applied ex-works, for the Dortmund Messewagen 2017 (Art.-No. 80960) and the courage was even sufficient for large-area paint graffiti.

Accompanying Miba Report 1/2019 on the topic:

To supplement the information contained in the book "Güterwagen Band 1.2" (Discussion in this issue). The first conference on this topic will take place on 17 October 2019 as VG Bahn announced in the Miba report.

Well-known specialist authors have contributed articles to painting and aging work on models of covered freight wagons of various designs and all common gauge widths from I to Z the one who's going to be Stefan Carstens provides his role model for the and stimulating templates.

Z scale is also available there for the first time with its own article. Since our editor-in-chief Holger Späing is a native, we cannot do without a separate review, and we will leave it at that hint for the time being for those of you who are looking for further specialized reading.



But we are not that far yet, because our theme is Epoch III, a time in prototype, where foreign properties still prevailed among the entire population.

But there are also "natural" limits to be respected: If our model gets too thickly applied or just too many colour applications, its fine engravings are lost. They are covered over and lose their sharpness. "Less is more" will therefore be our guiding principle. The use of a spray gun is therefore the first choice compared to the considerably thicker application of paint by brush.

Before you can get started, it is not only in layout construction that you need to plan the steps to be taken. Nothing would be

more dangerous than to go to work without prior considerations and "coming to grips" with the model with paint. Key questions for the work steps to be determined can be as follows:

- If modifications are to be made to the model prior to the aging adjustments, such as, for example, preparation or superstructures, which of the following must be performed?
- In which operating condition should the wagon later appear, with simple operational damage, slightly weathered or almost ready for retirement?
- What kind of operating traces should be shown (dirt, rust, mechanical damage, etc.)?
- Are there suitable templates on which the work can be based?

Suitable examples can be found in large quantity in the literature or also on the net in most diverse photo galleries. They offer many suggestions and sometimes also unusual approaches for unusual manifestations.

Let's now look at our Z scale model, but then it's time to step back: Our observer's perspective corresponds to the view of a model from at least ten metres away. Not everything that adheres to its surface can be perceived by our eye from this distance.

And what we don't see isn't worth implementing either. This saves, especially in comparison to the larger gauges, some paint applications and even meets our previously described requirements. What seems essential and perceptible is also reproduced on the model, and the rest is done by the imagination of the later audience.

Optical and functional enhancement

According to the described ideas we would like to design the Glmehs 50 with the company number 201 364, for which the two covered freight wagons from the Märklin wagon package 86000 offer suitable starting points. You have already received a product update, which saves us from having to correct the steps below the sliding doors.

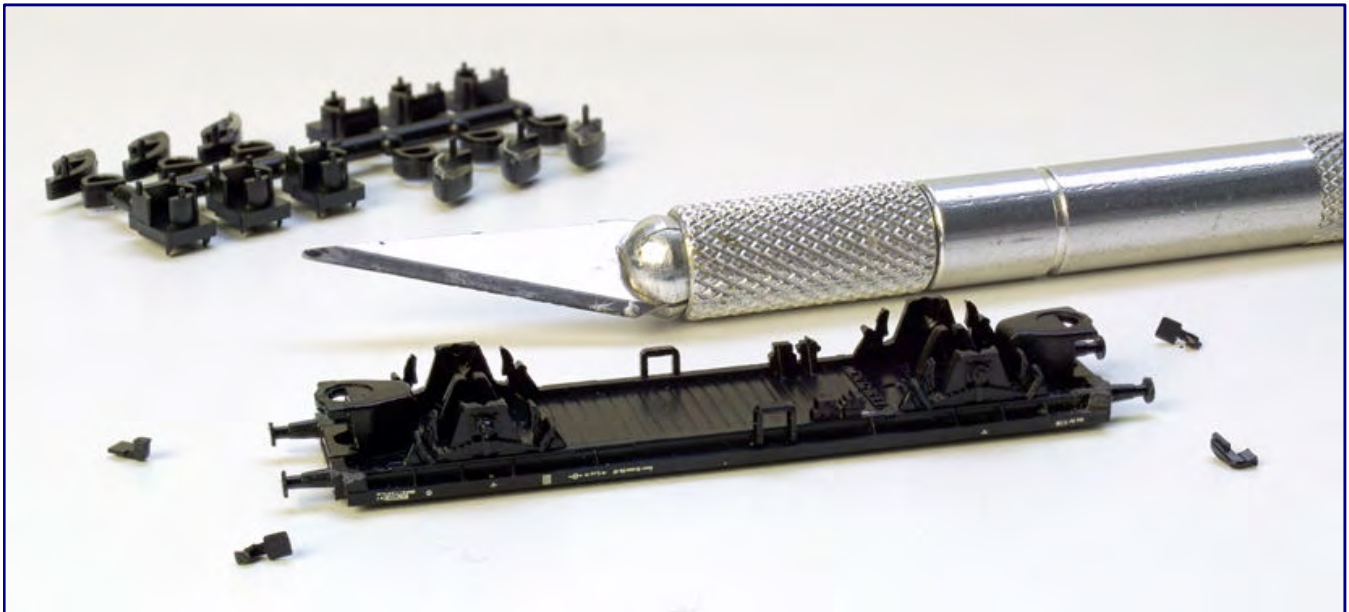


The wagon from the Märklin freight car package 86000 has been carefully dismantled, most of the parts (Zettelkästen (destination and labelling sign holders) are still missing) that we want to exchange or attach are ready. The fine detailing and aging can begin!

Nevertheless, these models, whose basic construction dates from 1972, are not perfect. They have features that do not fit together in this combination: We see a total of four UIC steps at the ends of the chassis, two at each end to the right and left of the buffer.

Only the first series were equipped with rope eyelets (for fixing on ferries), but the reinforcing plates injection-moulded onto the outer box struts did not fit, as these were features that were only to be found in the later models equipped with UIC rope hooks.

Therefore, we first have to take action and make corrections before we can start to age the wagon. This is how we opt for targeted retrofitting with Zpur® parts from KoMi-Miniaturen.



Four UIC steps on our model are correct, but it is much finer than with the factory molded parts. And that's why they have to be removed (photo above)! The FR Freudenreich coupling hooks (photo below) made of polystyrene are glued on the underside with a small iron metal bracket to prepare the wagon for operation with Jörgger uncouplers.

We cut off the four injection-moulded UIC steps, which look far too coarse for today's requirements, with a scalpel, smooth the interface with key files and then continue the filing work on the reinforcing plate replicas.

We can then use Bindulin instant adhesive to apply four new UIC shunting corner steps (ZW 6), which first need to be bent into the correct shape, and rope eyelets (ZW 3) connected to the outer box struts. Two etched Zettelkästen (destination and labelling sign holders) (ZW 4), which are glued on instead of the factory pad printing, also provide a further optical improvement.



The fine detailing parts from KoMi-Miniaturen are attached and emphasize the typical character of the chosen type of car, where Märklin once implemented a mixed type. We have already remedied this by sanding the outermost box struts. The removal of the old company number with tampon printing remover, unfortunately, did not take occur without damage to the paint this time, as can be seen in the photo.

We would also like to exchange the Märklin coupling hooks for identical parts from FR Freudenreich Feinwerktechnik. The background is the desired attachment of a magnetized metal strip made of a staple with eagle owl hard plastic adhesive to the underside of the coupler hook.

It will later make it possible to separate the car from its neighbour as if by magic using Jörgen uncouplers. So that the new equipment feature does not immediately (disturbingly) attract attention, we paint the metal frame with black PU primer from Badger with a few brush strokes.

We had the already mentioned company number including the genre name produced by Andreas Nothafft as an individual sliding image address for Era III. However, we can only dissolve and apply them with water after the data printed at this point have been removed.

For this we use the Tampondruckentferners (pad printing remover) from Prehm Modellbau, which is always very reliable. In this case, unfortunately, it also removes the base coat, so that the brown plastic also is revealed. But the dilemma is quickly solved with the help of a fine hairbrush and some varnish in the colour RAL 8012 reddish brown.

This is followed a short time later by the application of some adhesive base and the floating and pressing of the decals treated with plasticizer. After drying, the model is ready for the planned colour work.

Aging in fast motion

These will now begin with the coloring of the newly added parts. All are coated with some isopropyl alcohol to degrease them reliably. Otherwise there could be adhesion problems, because the finest grease layers may already have been left behind during adjustment.



A whole lot of tools, different paints and primers are also required to weather a Z-scale model. Only what was used in this project can be seen. Only the old Revell colour container with the number 383 (chestnut brown RAL 8015) was replaced by the correct RAL 8012 red-brown from Oesling Modellbau.

Then it should be Badger's black polyurethane primer again, which is dabbed with a brush onto all three types of attachments. In principle, any other primer is suitable as long as it is compatible with the chosen lacquer.

In the case of the product mentioned above, we have not been able to detect any incompatibilities with the water-based acrylic paints of the various manufacturers, which we also happily use.

The rope eyelets, which are to be painted reddish brown (recommended: RAL 8012 silk matt from Oesling Modellbau) after the primer has dried, also require a second working cycle. The dark colour base always has an effect on the luminosity of the opaque colour, which is not subject to any special requirements here. It also prevents the metal surface from shimmering through if there is any subsequent paint damage.

The newly mounted Zettelkästen (destination and labelling sign holders) were not originally painted when the original was presented, which is in line with our wishes, because they stand out better from the brown background as blank parts and are therefore more conspicuous.



After removing all grease and oils, the etched parts are primed in deep black (picture above). As a further preparatory activity, we lay out the brake system adjustment device with Tamiya XF-2 white (picture below), red frames and levers will follow. The Zettelkästen (destination and labelling sign holders) has already been sanded down as described in the text.

This is how we carefully remove the primer from your surface with fine sandpaper or sandpaper: the black paint only remains in the recesses, and makes the fine parts look really three-dimensional. The brake reversers, which could also be exchanged for other Zpur® accessories, are then laid out in matt white (Tamiya XF-2), and their levers are then set down in fiery red (Revell Aqua Color 330).

We start applying the patina in the roof area and on the ventilation flaps. As they represent the brightest areas of the wagon, the luminosity of the substrate also affects the adhering dirt. We create a first base with a fine hair brush, because the rust texture from the series Mecha Weathering (69.821) by Vallejo cannot be sprayed.



The rust traces with “Mecha Weathering” from Vallejo are applied with a brush, since the rust texture cannot be sprayed due to coarser particles.

It contains fine particles that can be felt and could clog or even damage our nozzle. However, the still very strong rust tone is damped again during the following work and does not have to worry us at this point.

Another quality is the contamination of the front and side walls: Especially the box struts provide places with little air movement and form run-off edges behind or under which dirt is neither carried away by wind nor rain. These corners and edges are shaded areas and therefore appear darker.

Black acrylic varnish “Engine Black” (16-01) from Bagder, which is sprayed finely and precisely onto struts and corners, provides the right prototype impression here. The finer the nozzle of the spray stylus, in our case 0.2 mm on Gabbert's Triplex I, the better it looks in the red-brown environment.

continued on page 15



With “Engine Black” from the Badger paint program, applied with the spray gun and fine nozzle, the dirt adhering to the struts and corners is applied in the second aging step (picture above). A cotton swab soaked with cleaner helps to remove or blur traces that are too strong. This has a particularly convincing effect on the elevations of the box struts (picture below). Some cotton fuzz still reveal the activity carried out here. The processed undercarriage is loosely attached for the now pending, common sepia spray application.

In the next step we break the radiance of the colours on the roof and ventilation flaps. Here a glaze colour in the sepia tone of Vallejo (73200 "Sepia Shade") is the first choice for us. All parts already mentioned are repeatedly sprayed with it, but always fogged over very thinly until the desired colouring is created.

A cotton swab soaked in airbrush cleaner helps us to reproduce rain drainage marks on the roof. We paint from the middle to the outside and blur the still fresh glaze a little. Traces of fog of the black varnish contribute their part to the results.



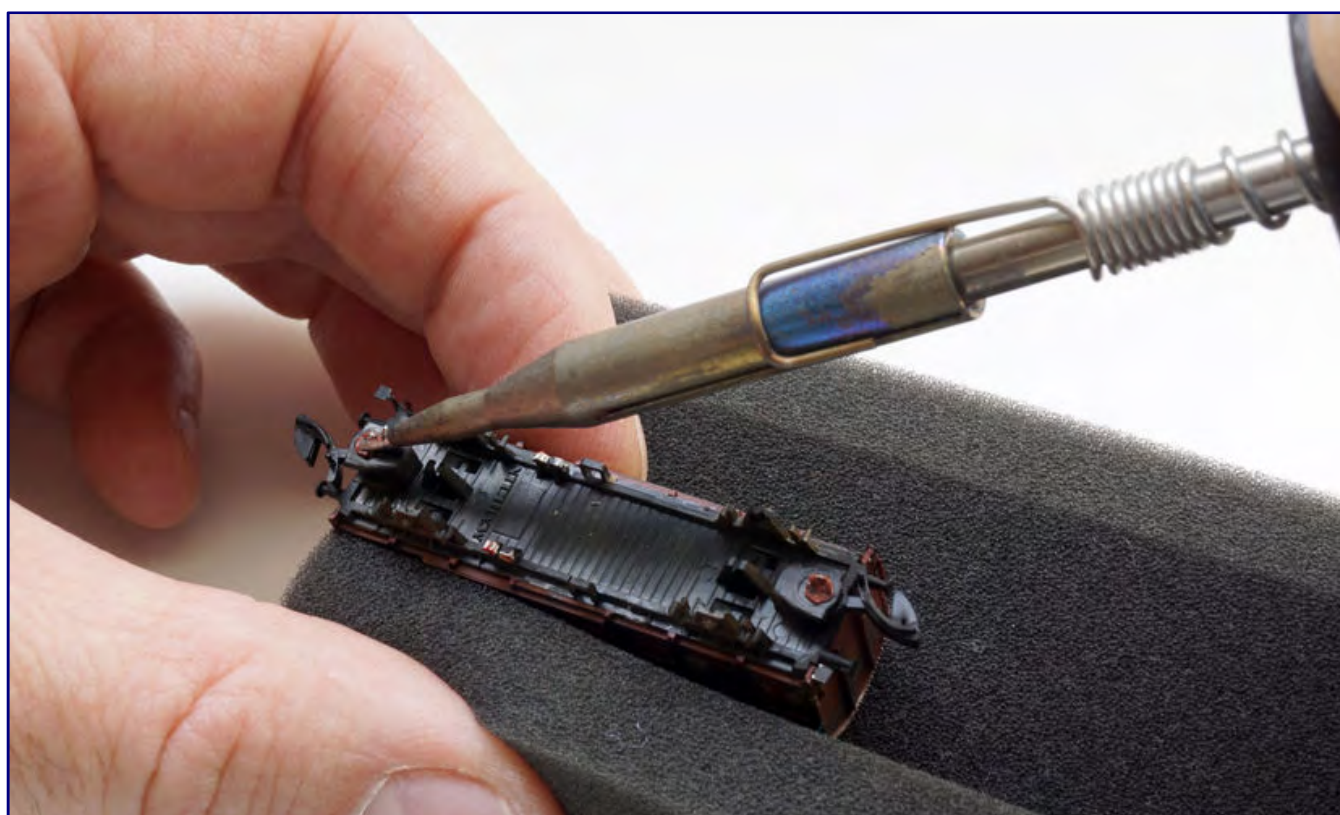
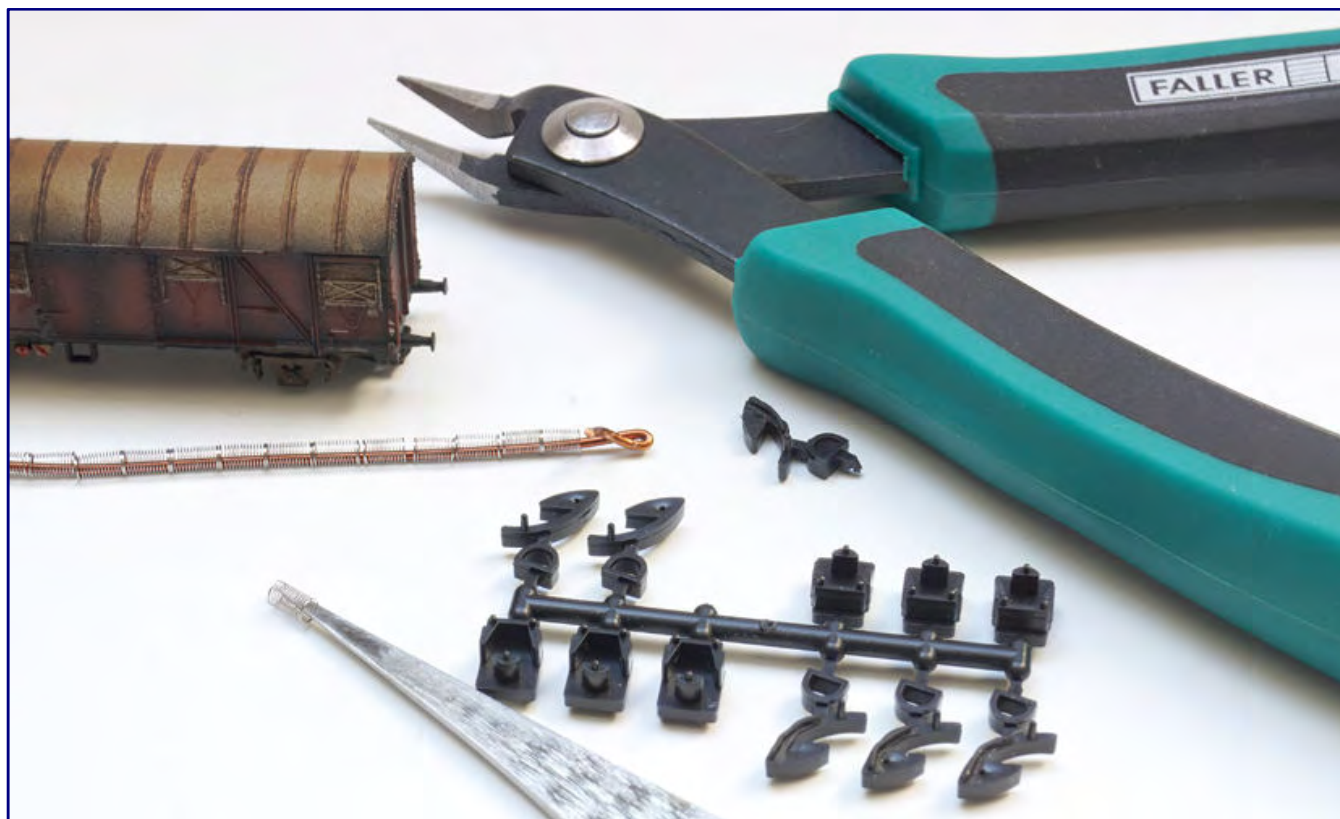
The wheel discs, which are barely visible in layout operation, can also be aged: They are inserted into a self-made polystyrene stencil and fogged several times with Vallejo's sepia glaze until the image has turned into a metal wheel covered with flash rust.

The brake reversers also receive a fine sepia coating and thus blend harmoniously into the overall picture. After cleaning the fuel tank we fill the paint container with "Basic Brown" by Vallejo (73004) from the Train Colors collection. It forms the first base coat for the brake dust adhering to the chassis. We counteract a possible monotony with some "Basic Rust" (Vallejo 73001) as a supplement.

And the work is already nearing its end. What is needed now is a uniform gloss level before the chassis and its body are reassembled with the couplers and springs from FR Freudenberg Feinwerktechnik.

"Ultra Matte Varnish" (AK 183) from AK Interactive is the method of our choice here, because we did not know of any clear lacquer that dries up matt, and at the same time doesn't tend to cast grey veils on the model. It also makes the edges of the new markings almost disappear.

continued on page 17



The first step in reassembly is to insert the (new) couplings, on which the black painted metal bracket is no longer noticeable (top photo). We also exchange the compression springs for those from FR Freudenreich Feinwerktechnik, because they allow softer coupling.

The housing can then be pressed back onto the chassis. If the melting points were carefully bent up at the beginning and not destroyed, they can now be pressed wide again with the soldering iron (picture below) and provide a firm new hold. We advise against fixing with superglue or the like.

While it dries, we turned to the wheel sets: Also on the dark nickel-plated Märklin specimens some colour is required. Using a self-made polystyrene template with a drilled hole (the cut out from drilling through polystyrene) in the diameter of the wheel discs, we can spray the “Sepia Shade” on them. However, the application is carried out in considerably more layers so that it appears darker.



The Vallejo Traincolors also have a suitable colour for the slightly shiny grease on the buffer plates of our matt freight wagon: “Grease & Oil” is dabbed precisely onto the ends of the impact devices using a micro brush.

The finished model in operation can be seen at the top of page 18. Ranked between a (factory) aged locomotive and also dirty, other freight wagons, our “patient” knows how to convince the viewer well.

The only thing that shines on the ends when our aged model is returned to the layout is the grease on the buffer plates. Using a micro brush, we carry a swab of “Grease & Oil” (73002) from the Vallejo traincolors to the relevant places. Finished!

Basic model:

<http://www.maerklin.de>

Used parts, adhesives and colours:

<https://www.bindulin.de>

<https://www.kuswa.de>

<http://intl-trade.eu> (Importer Badger)

<http://www.komi-miniaturen.de>

<https://www.modellbahndecals.de>

<https://www.oesling-modellbau.com>

<http://www.prehm-modellbahn.de>

<https://www.revell.de>

<https://www.tamiya.de>

<https://www.uhu.de/de>

Vallejo paints with 10 % Trainini® reader discount:

<https://www.modellbaukompass.de> (Key word: Trainini)



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Convair CV-340 of KLM

Historical border crosser

After a long time we would like to take up a marginal theme of the model railway with the aviation depicted in detail, even if it is undoubtedly attractive. The Herpa delivery of a classic of the early post-war years has induced us to devote some space to it again today, especially since the miniature really deserves this name and has layout friendly dimensions.

The Convair CV-340, equipped with conventional piston engines, made its maiden flight on 5 October 1951. It and the shorter basic version CV-240 are considered to be the first short-haul aircraft in the world to be fitted with a pressurised cabin in the circular fuselage as standard.

The low-wing aircraft was developed as a 1.37 m extended version of the CV-240 according to United Air Lines' requirements. Two doors with built-in stairs were to make the aircraft independent of ground facilities and speed up passenger handling.



The Convair CV-340, christened "Pieter Brueghel," was the youngest aircraft in KLM's fleet in the first half of the 1950s at the time of its introduction, and made its contribution to quickly phasing out the older Douglas DC-3s.

Compared to the exit type, one row of seats found more space in the cabin, allowing 44 passengers to be carried in 11 rows of seats, sufficient capacity for a short-haul aircraft at that time, because flying was still a luxury. But also the wings and the quite striking vertical stabilizer, which rises at the tail, were changed.

The rudder and rear wing edge made of fibreglass-reinforced plastic were certainly among the special features of the time. The landing gear with nose wheel of this aircraft type was retractable, the piston engines worked at a fixed speed on the two three-blade variable pitch propellers.

Economically the Convair CV-340 became a success, although the jet age after the Second World War started quite fast, but at first for only long distances. After all, the American manufacturer built 246 units, of which 209 were sold to airlines.

The German Lufthansa also used four aircraft of this type between 1954 and 1968. Many "Convairliners" were technically upgraded to propeller turbines with conversion kits from their manufacturer, and ensured that the aircraft was used for a long time.



The Convair CV-340 was the first short-haul aircraft to have a pressurized cabin, but was still a classic model with piston engines, the cylinders of which worked star-shaped on the drive shafts of the two three-bladed propellers. The propellers always turned at the same speed, and the desired propulsion was set by the angle of attack of the blades, similar to the rotors of helicopters.

However, from the mid-sixties onwards, the large companies gradually had to clear the field for the Boeing 737, which also had a significantly higher capacity. In some cases, however, they had a second career in the Third World, and even in the USA, individual examples were active until 2008.

For our model discussion this time, we have selected a copy of the Dutch airline KLM, which is to receive a brief tribute to its one hundredth anniversary this year.

The "Koninklijke Luchtvaart Maatschappij" (Royal Dutch Aviation Company) was founded on 7 October 1919 and is thus the oldest airline in the world, which is still in existence today and operating under its original name.

Due to one of its two founders, KLM once had a close relationship with the former manufacturer Fokker from its own homeland. From the thirties of the last century, however, it also procured DC-3s from the American aircraft manufacturer Douglas. After the Second World War, it even shifted the focus of its fleet procurement policy to American models.

In 1948, twelve copies of the Convair CV-240 also entered the KLM inventory. In 1953, seven machines were added to the further developed CV-340 version, which finally allowed the last Douglas DC-3s to be taken out of service.

With the Convair CV-340, KLM also dared something new with her: the fuselages of her airplanes had previously been left metallic, but with this pattern she now started to paint the machines from the lower edge of the window hinge.



With the aircraft type presented today, KLM also introduced a new, much friendlier design: From the lower edge of the windows, the fuselage was now painted white instead of bare metallic aluminium. The rudder is painted in the colours of the Dutch national flag.

The large white area made the aircraft look much friendlier. If it was bordered at the bottom by a dark blue decorative line that widens towards the nose and surrounds the nose, a second one above the windows was considerably wider. On it were a white printed “The Flying Dutchman” (left side) and “De Vliegende Hollander” (right side).

The seven CV-340s were used between 1953 and 1964 in European short-haul flights. They were thus in direct competition with the Trans-Europ Express trains, whose idea also originated in the Netherlands.

With these trains, the railway administrations in the slowly reuniting states of Europe tried, as is well known, to stand up to the increasing air traffic. It is precisely this aspect that makes the Convair CV-340 from KLM a particularly interesting object for the model airport at the edge of the facility.

In 1964, KLM withdrew the prototype for the Herpa model from the KLM fleet and the company Martinair, which was also a Dutch commercial airline, acquired it. There it was converted to turboprop engines, and, thus, technically upgraded to the CV-600 standard.

Evaluation of the new product

In terms of quality, the new cast metal model seamlessly follows on from the younger predecessors of the Wings series in 1:200 scale. Due to the slightly different scale from Z gauge, the aircraft is, of course, slightly too large, although this is not really noticeable here.

So it can easily be combined on the apron with road vehicles of our correct scale, especially since historic airport vehicles are still in short supply in the Herpa range.



The labelling has been printed very precisely, which is particularly evident in the logos and fine lines in the rear area. The lacquering is also free of dust inclusions: The metallic blank underside of the fuselage even reflects the baggage carts and trailers.

Its tiny size by today's standards meets the requirements of model railroaders very well. Air traffic enthusiasts often wish to link the different modes of transport, but an airport that would otherwise be too large can only be allowed a peripheral area on the layout.

The dimensions and proportions of the familiar pattern have been correctly observed and reproduced. Herpa's quality standards today are reflected in the fineness of the details in the area of engines, propellers and chassis. Again such features are applied that cannot be integrated into the casting of the fuselage and wings. This can be seen here on the two contemporary antennas on top of the fuselage.

The Convair CV-340 was announced by KLM (Art.-No. 559393) with the registration PH-TGD and the christened name "Pieter Brueghel" after the famous Renaissance painter already for November / December 2018. So it now has appeared with a delay of about ten months, but what lasts for a long time will be good.

Die wichtigsten Daten und Maße zur Convair CV-340:

	<u>Prototype</u>	<u>1:200</u>	<u>1:220</u>	<u>Model</u>
Length	24,13 m	120,7 mm	109,7 mm	123,6 mm
Wing span	32,12 m	160,6 mm	146,0 mm	159,8 mm
Height	8,58 m	42,9 mm	39,0 mm	43,4 mm
Take-off weight	21.320 kg	---	---	134 g
Cruising speed	457 km/h			
Engines	2 x Pratt & Whitney R-2800 CB16			
Propeller	Three-blade variable pitch propeller (fixed speed)			
First flight	5 October 1951			

It now also impresses with a clean paint finish without inclusions, which is particularly good for the shiny metallic underside.

Position lights, logos and labelling as well as the square cabin windows or windscreen wipers have been elaborately highlighted using the pad printing process, as is customary with this manufacturer.

The early fifties were characterised by the rather small lettering “KLM,” which could be found above the rear door (left side) and slightly larger on both sides next to the front door. Much more striking were the long lettering in majuscules above the window hinge, which had already been mentioned in the original part.



Due to its small size, the Convair CV-340 is ideal for designing a manageable airport section at the edge of a4 layout. One conceivable option would be a piece of runway that would be undercut by a railway line. On such a runway, our model follows a pilot vehicle (Tempo Matador “crumple face”) to its parking position.

Even the old company logo with a crown is hardly noticeable from a small distance, while the colours of the national flag on the rudder cannot be overlooked. By the way, the crown emblem was missing from the graphic representation at the time of the announcement.

What remains to be clarified is how the low-wing aircraft with the tail sloping down under the rudder can best be positioned? Since the prototype had integrated stairs, it might seem overloaded with prototype passenger stairs.

It is certainly more enjoyable to wait on the runway for the release of air traffic control while trains are passing under it through the tunnel, or when following a pilot vehicle ("follow me") over the apron to its parking position.



With successful proportions, fine engravings and attached parts such as chassis or antennas, the Herpa model offers a good basis for integration into a Z-gauge environment. This scene has also been supplemented with well-known 1:220 scale accessory materials, without causing any problems.

Naturally, it is also conceivable that, suspended from invisible nylon threads, it floats above the layout shortly after lift-off. The extended chassis of the fine model fits perfectly to this. In any case, a decision should not be put off for a long time, because the historic aircraft is already sold out at some dealers.

Webpages of the model manufacturer:
<https://www.herpa.de>

Queen of the rails in the spotlight **A catwalk for beauties**

Especially in recent years, many beautiful and very well done Z scale models have hit the market. This poses the challenge of putting these models in the spotlight and creating an appropriate setting for them which would stand out even at fairs and exhibitions. In the context of our “layout diversity” focus theme this year, Dirk Kuhlmann tackles the planning and detailing aspects of building a worthy stage for your models.

By Dirk Kuhlmann. Before explaining the slightly strange title of this article, let me thank our readers for the many positive reactions we have received since the first part of our series on layout planning was published in the June edition. The presentation of track plans with a twist seems to come at the right time.

In our second instalment we literally head north to the city of Norden by the German northwestern coast of East Frisia and its offshore pier “Norddeich Mole”. This is an area where I often spend my holidays, and when there, a visit of this terminus is a must. It is the perfect place to spot all types of passenger trains, with the exception of ICEs.



When our editor was visiting Norddeich Mole with his father in August 1974, the two of them perhaps also spotted train E 2730 pulled by 012 061-8, which is waiting for its departure on August 11 of that year. During the holiday season, the parking lot for the ferries to the islands of Norderney and Juist was always well filled, a possible scenic detail for a layout, especially if one would like to also create an effective stage for car models. Photo: Prof. Dr. Willi Hager, Eisenbahnstiftung

I had been reflecting for a long time about the feasible dimensions of a layout which would make for a reasonable representation of this scenery in Z scale, preferably in an era IV setting. Choosing this era would allow for the prototypical operation of class 012, 042, 043, 044 and 050 steam locomotives (which ran until approx. May 1975) and “old red” class 212, 216, 220 and 221 diesel engines.

Together with my father, I visited Norddeich Mole in August 1974 solely for the following reason: to be able to have one more close look at the class 01¹⁰ oil steamers in their last summer of operation.



On the same day, a delayed ferry connection caused a 20-minute delay in the D 730 with its Pop Art coloured coaches so popular with model railroaders. Then 012 082-4 and her train made just 100 m before being delayed again due to a signal malfunction. By the time it could move on, daylight and the opportunity for a decent photo had almost disappeared. Photo: Prof. Dr. Willi Hager, Eisenbahnstiftung



It is an amazing experience when such a "monster" slowly stops in front of the buffer. When all passengers got off the train, the locomotive had to push its train backwards into Norddeich main station, which is located on the inland side of the dike.

Left:

After the passengers got off, the locomotive pushed its train back to the Norddeich main station, which, at the time, still had separate stabling tracks to the left of the platforms. Then it uncoupled, turned, was supplied and was coupled to the other end of the train. The "Nnw" signal box and the station building were demolished a few years ago.

The steam locomotives then uncoupled, drove onto the turntable and positioned themselves on the other side of the train after taking up supplies. Once the time had come, the locomotive pushed the whole train backwards onto the pier and the passengers for the return journey could board the train.

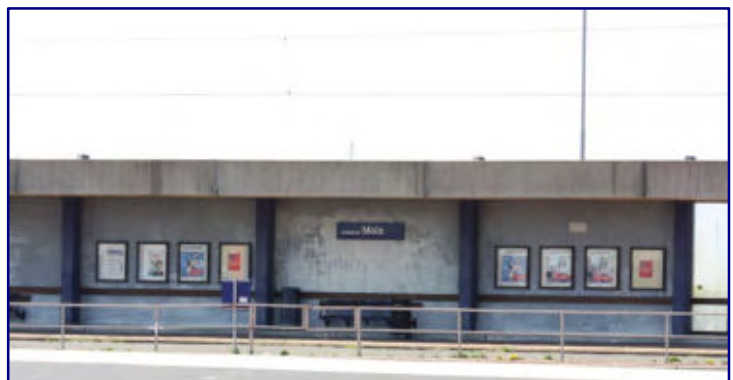


In 2009, the former platform from the historical photos to the right of the track was still there. In the meantime, however, this platform has long since gone, as has the version of the canopy that can be seen here. The shelter in this idiosyncratic form, however, can serve as a model for a credible reproduction on an era IV / V layout. The shelter is also a part of the original building.

So this is how operations could also run on the model railway layout, but please without the complications of installing a turntable or long stabling tracks. Another condition was to stay essentially within a size of 250 x 70 cm.

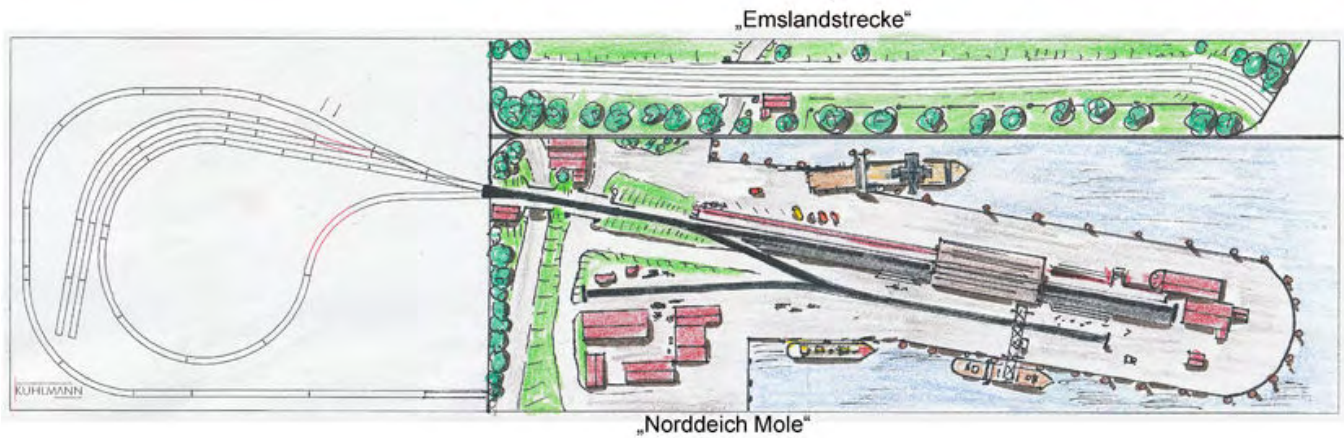
Right:

For those interested in a reproduction, we also show this front view of the platform canopy. This should help to understand the elements and proportions of the structure.

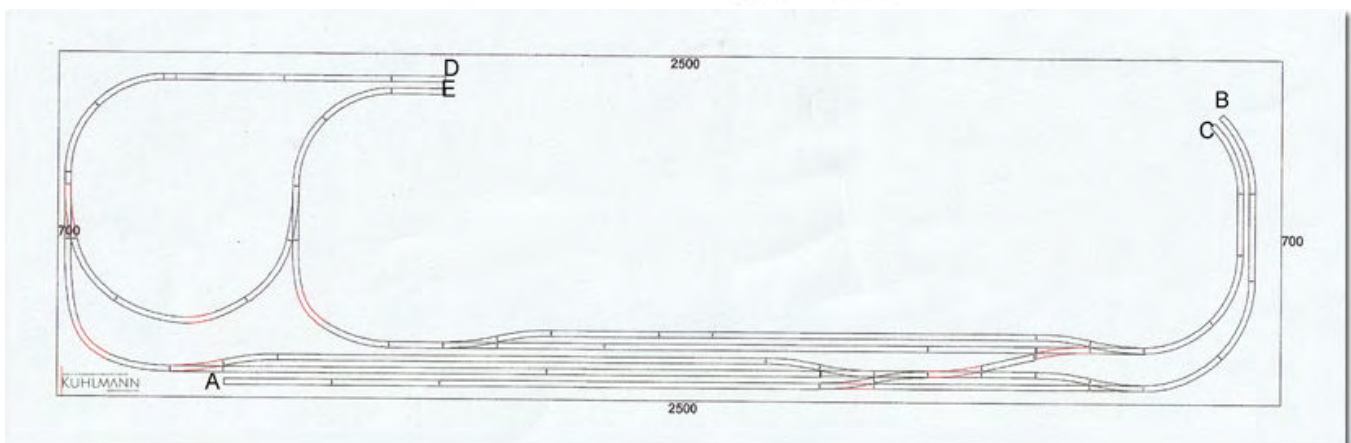
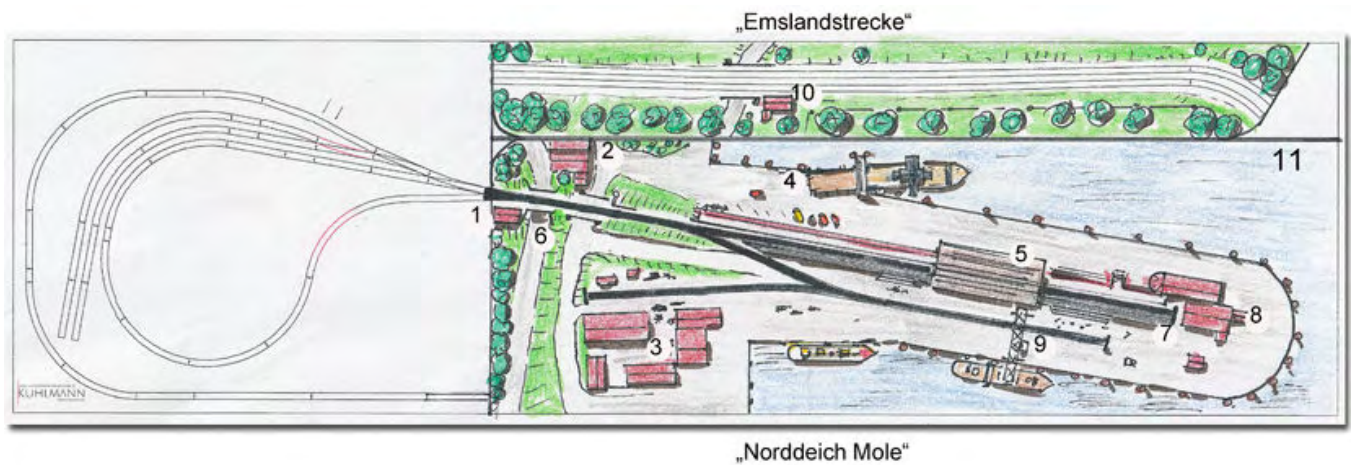


Transposing reality into the model

The solution is quite simple: a reversing loop is sufficient to all of our requirements. To see how it works, just have a close look at the track plan below. As an addition, there is a small staging yard which can be used, for example, to stage trains with class V 100 engines or also local freight trains.



The track arrangement also allows for shunting trips on the pier, which opens the layout to a truly manual operation. On the internet there is plenty of photo material available on prototypical operations.



Track plan including upper and lower level fiddle yards and identification of structures:

- | | | | | |
|------------------------|--------------------|-----------------|----------------------|-------------------|
| 1 Signal box Norddeich | 2 Hotel | 3 Business park | 4 Car ferry terminal | 5 Platform canopy |
| 6 Underpass | 7 Shipping company | 8 Kiosk | 9 Crane | 10 Signal box |
| 11 Scenic divider | | | | |

But friends of running long trains should not miss out either. On its lower fiddle yard level, the layout therefore gets a piece of the "Emslandstrecke", the railway line connecting the city of Rheine with the East Frisian coast. Both parts of the layout are separated by a scenic divider, of course each with its own backdrop scene to simulate as much depth as possible.

You will certainly have noticed the railway wye on the left half of the lower level. I don't want to say many words here either. Just retrace the tracks in your mind.

Ultimately, this arrangement allows you to send any train, no matter from which direction, to the pier. Or you let an automated system run different trains on the lower level only, without affecting operations on the upper level. The only connection between the two parts of the layout is a long ramp.



Norddeich is the only spa and resort on the East Frisian coast directly connected to the railway network. On 9 January 1967, i.e. one year before the introduction of a computerized train classification system, 01 1072 awaits departure in the immediate vicinity of the ferry terminals. Photo: Detlev Luckmann, Eisenbahnstiftung

Would such a layout be well received at an exhibition? You bet! This showpiece offers three different views for the audience:

1. The straight line track section of the "Emslandstrecke" can be used to showcase prototypical train formations, such as long iron ore trains, repeatedly sold by model railway manufacturers under the name "Langer Heinrich", or various express trains. For prototypical operations, make sure to install the double tracks at a distance of 19 mm.
2. The unscaped area around the reversing loop could inspire the purist, who is only interested in seeing rolling stock in action.
3. The pier, on the other hand, is the catwalk area of the layout. Here your most beautiful models find a suitable stage for showing off their appeal. The slow speed ride of a digital model, equipped with fully detailed and moveable running gear and possibly also with sound - we refer to something like the class 012 announced already years ago by Bahls Modelleisenbahnen.



Our layout proposal is not limited to a specific era. As proof we show this double traction formation consisting of 220 064-0 and 220 081-4, waiting for departure from Nordeich Mole in April 1977, two years after the decommissioning of the class 012 steam locomotives. And soon after that, the entire Emsland line was converted to overhead lines. Photo: Joachim Claus, Eisenbahnstiftung

With all these operational and scenery possibilities assembled on one layout, model railroaders dabbling in larger scales will now probably retreat to their cave to cry and moan!

A layout like this offers pretty much everything one may wish for. And I do not want to enter into a further discussion of modelling specific railway eras. After all, everything is possible with some imagination and creative thinking.

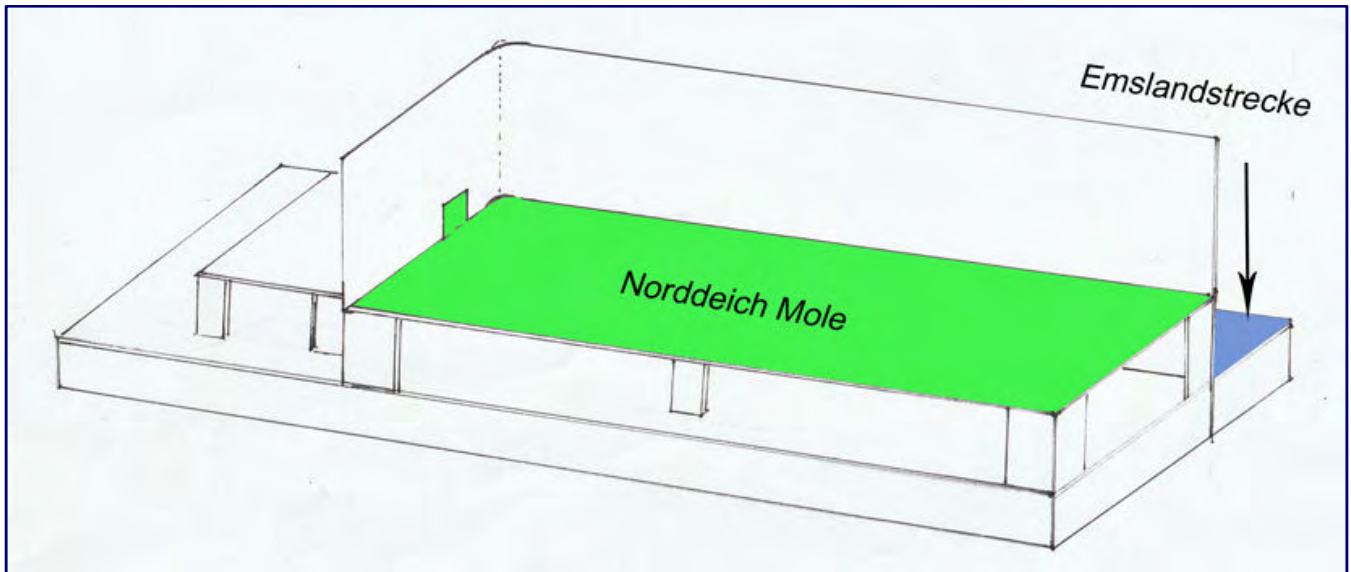
Even the installation of realistic looking overhead lines is now possible in Z scale. Finely detailed catenary poles and equipment from the companies HOS Modellbahntechnik and FR Freudenreich Feinwerktechnik invite for a trip into present times or to Scandinavia.

The basic structure - a wooden box

Below you will find a general sketch of the baseboard and its superstructure for this type of layout. This is meant to be just a visual presentation without dimensions, leaving the exact size up to your own specific needs and plans.

The inclined hobbyist still make further modifications, in particular by adding a proscenium arch or box on top of the layout to give it that modern “picture frame” look, often seen now at exhibitions.

Buildings and other structures for this type of scenery are unfortunately somewhat rare on the market. We’ve done a little search for you amongst relevant manufacturers and of course found something (see page 32). There is also the option to order tailor made models from specialised suppliers. And, as always, one can build things completely from scratch.



A general depiction of the baseboard and superstructure. This is only to illustrate the basic idea for this type of two level layout and we therefore refrain from giving precise dimensions.

Hence our tip: come and visit the Z-scale-weekend in Altenbeken, which takes place again from 28 to 29 March 2020. Almost all Z-scale manufacturers and specialist suppliers will present their products and be available for discussions and suggestions.



A further proof of the era-independence of our layout proposal is provided by this photograph from June 1961. Arriving from Emden, 38 3279 and its train enter Norddeich Mole station, with its passengers transferring to the ships to the nearby North Sea islands. In the background, next to the platform canopy, a portal crane can be seen. See position 9 on Dirk Kuhlmann's track plan for where he would place the crane on the layout. Photo: Quebe, Eisenbahnstiftung



Archistories 114111



Archistories/ HRT 109181



Laffont Z1401



Laffont Z1601



Lütke 73201



Lütke 73212



MBZ 16387



MBZ 16423

Photos: Manufacturer

Literature on the topic:

To those who would like to learn more about the Norddeich region we recommend the book „Norddeich - Eine Zeitreise in Bildern“ (Norddeich – an illustrated trip back in time) by Sigrun Müller.

ISBN 978-3-00-057301-9 (out of print, only second-hand).

Z-scale models of suitable ships are also very rare. Here I can recommend only two websites, which are given in the box at the end of this article.

And Peter Nolan (N Scale Ships) has said that he is active again and builds also Z-scale ships. He has become known in Germany with his shrimp cutters and ferries for the extraordinary “Kniephaven” and “Helenensiel” Z-scale layouts.



The simple but beautiful station of Norddeich is now also a thing of the past. For those embarking on this layout build it may be worth, however, to reproduce this typical East Frisian architectural style in the model.

Final remarks

As on other occasions, we do not want to express a preference for any specific brand of Z-scale tracks. Also, we think that the decision to go digital or analogue is a matter of personal preference, as much as the choice of electrical components.

Even the scenery does not have to be modelled after the German North Sea coast. Let your imagination run free, as already mentioned in connection with the possible addition of an overhead line. But one basic principle always applies: A good illumination of the layout is especially important; after all, our beauties love being in the spotlight!

Buildings and structures:

<https://www.archistories-shop.com>
<https://www.luetke-modellbahn.de>

<https://www.mbz-modellbahnzubehoer.de>
<https://www.modellbau-laffont.de>

<https://www.1zu220-shop.de>

Model ships:

<https://www.kartonmodellshop.de/shop/>
<https://savyboat.com>

<https://nscaleships.com>

Grenzenloser Modellbahnspaß in 1:220

**8. Internationales
Spur-Z-Weekend**



ALTENBEKEN

28. und 29. März 2020

Samstag: 10.00 - 18.00 Uhr

Sonntag: 11.00 - 17.00 Uhr

Eggelandhalle, Gardeweg 8

33184 Altenbeken



Z-Freunde International e.V., Brandenburg 6, 56856 Zell/Mosel

www.z-freunde-international.de

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Gedekte Güterwagen nach 1945 Unter neuem Dach in Ost und West

Mit Spannung erwartet wurde Band 1.2 aus der Güterwagen-Buchreihe der VGB. Durch die viele neuen Informationen und Quellen, die Eingang in die Überarbeitung der zum Standardwerk aufgestiegenen Literatur fanden, ließ sich ein Aufteilen des ursprünglichen Bands zu den gedeckten Wagenbauarten nicht vermeiden. Umso neugieriger waren die Besitzer des ersten Teils, was die Geschichte dieser Wagengattung bei DB und DR an neuen Erkenntnissen bringen würde.

Stefan Carstens / Paul Scheller
Güterwagen Band 1.2
Gedekte Wagen DB + DR

Verlagsgruppe Bahn GmbH
Fürstenfeldbruck 2019

Gebundenes Buch
Format 21,8 x 29,7 cm
192 Seiten mit ca. 550 teilweise farbigen Fotos und 135 Zeichnungen

ISBN 978-3-89610-653-7
Best.-Nr. 15088145
Preis 40,00 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Wenn Stefan Carstens ein neues Buch auflegt, dann sind die Erwartungen groß. Nicht umsonst ist seine Güterwagen-Buchreihe zum Standardwerk dieses Themenbereiches geworden. Inzwischen hat er begonnen, seine ersten Werke zu überarbeiten und zu erweitern sowie sie strukturell an die jüngeren Bände anzupassen.

Das sorgt für Einheitlichkeit, aktuelle Daten und hoffentlich auch neues Wissen, das hier Eingang finden kann. Keine Frage war es auch beim neuen Band 1.2, der zusammen mit dem bereits vorgestellten Teil 1.1 das einteilige Ursprungswerk von 1989 ersetzt, das die Latte sehr hoch lag.

Auf diese Höhe hat sie der Autor selbst gelegt und wir waren gespannt, ob er sich selbst schlagen kann. Bevor wir Inhalte und Struktur überfliegen und bewerten, möchten wir ergänzen, dass sich „SC“ wieder tatkräftiger Hilfe von Paul Scheller bedient hat, mit dem er seit längerem erfolgreich zusammenarbeitet.

Als Band 1 vor nun bereits 30 Jahren erschien, war von vornherein klar, dass es keine Eintagsfliege sein sollte und eine ganze Reihe geplant war, die irgendwann einmal den gesamten Güterwagenbestand der deutschen Staatsbahnen behandelt haben würde.

So war auf der Rückseite einst zu lesen: „Dieses Buch beschreibt – als erster Band eines in mehreren Fortsetzungen erscheinenden Standardwerks – die gedeckten Güterwagen in Regalbauart von der Jahrhundertwende bis zu den letzten Entwicklungen der Deutschen Bundesbahn.“



Mit dem Fokus auf die Deutsche Bundesbahn gab es eine bedeutende Lücke, von der noch niemand ahnen konnte, dass sie sich später schließen lassen würde: Nicht enthalten waren nämlich die für die Deutsche Reichsbahn der DDR gebauten oder von ihr umgebauten und modernisierten Wagen.

Nachdem Band 1.1 die Entwicklungen und Bauarten bis 1945 zum Thema hatte und uns zu begeistern wusste, soll Band 1.2 jetzt die Geschichte in Ost und West fortschreiben. Außen vor bleiben die erst von der Deutschen Bahn AG beschafften Güterwagen, denen bereits ein eigenes Buch bei der VG Bahn gewidmet ist.

So umfasst die vor einigen Monaten erschienene Buchneuheit alle gedeckten Güterwagenbauarten, die die Eisenbahnen in den Besatzungszonen und ab 1949 DB und DR beschafft haben. Behandelt werden auch alle Wagen, die hier wie dort aus Vorkriegswagen umgebaut, modernisiert oder auch rekonstruiert wurden.

Veranschaulicht werden sie mit 135 Zeichnungen und annähernd 550 Fotos. Einen wichtigen Hinweis verdient hier die Tatsache, dass der frühere Bildbestand zu einem großen Teil ausgetauscht und erweitert wurde. Das stellt einen weiteren Kaufanreiz dar, macht aber auch das alte Buch zu schade zum Wegwerfen oder Verkaufen.

Dank hervorragender Wiedergabequalität von Abbildungen und Fotografien ist auch das hier vorgestellte Buch wieder eine gelungene Vorlage für alle Modellbahner, die Umbauten oder Umgestaltungen wie auch Alterungen zu ihren Vorlieben erklärt haben.

Doch der Modellbau kommt auch im Buch selbst nicht zu kurz, denn vorgestellt werden dort auch mehr als 35 Modelle von Güterwagen der DB und DR, die namhafte Modellbauer umgebaut oder farblich nachbehandelt haben.

Einziger Kritikpunkt ist im Modellteil die kompromisslose Haltung, die der Hauptautor zu seinem Credo erhoben hat. Dort liegt nicht der Schwerpunkt auf Betriebstauglichkeit im Anlageneinsatz, sondern tadelloses und in jeder Hinsicht maßgenaues Aussehen.

Auch das ist eine spezielle Richtung und sehr interessante Ausprägung des Modellbaus, doch sollte die Toleranz gegenüber der Mehrheit der Modellbahner, unter denen Märklin-Kunden die Mehrheit stellen, insgesamt deutlicher zum Ausdruck kommen. Unser Hobby soll schließlich verbindend und nicht teilend wirken.

Wer mit anderen Ansichten gut zurechtkommt und sie würdigen kann, der wird aber auch mit diesen Abschnitten kein Problem haben und diesen Titel sicher schnell ins Herz schließen. So viele und so gute Vorbildinformationen wie auch Zeichnungen bietet derzeit kein anderes Werk auf dem Büchermarkt.

So ist auch „Güterwagen Band 1.2“ ohne Zweifel ein weiteres Standardwerk und damit kommen wir auf unsere (nicht ausdrücklich gestellte) Eingangsfrage zurück: Paul Scheller und Stefan Carstens haben es auch dieses Mal geschafft, sich selbst zu übertreffen und den Anspruch auf Informationsfülle und Wissenstiefe wie -umfang noch einmal zu erweitern.

Wir nominieren diesen Titel für die Neuerscheinungen des Jahres 2019 in der Kategorie Literatur.

.....
: Publishing pages with reference possibility:
: <https://shop.vgbahn.info>
:

Transportergeschichte(n) mit Stern **Mercedes auf der Überholspur**

Wo sich der VW-Transporter als zu klein erwies, da konnte Mercedes-Benz mit dem L 319 punkten. Das ungewöhnliche Auto mit der recht platten, wenn auch ausgerundeten Front und klarem Markengesicht wurde zum Urahn der mittleren und großen Transporter, der auch im Maßstab 1:220 eine wichtige Rolle spielt(e). Ein neues Buch setzt ihm jetzt ein würdiges Denkmal.

Andreas Gaubatz / Jan Erhartitsch
Mercedes-Benz 319
Der legendäre Mercedes-Transporter 1956-1967

Motorbuch Verlag
Stuttgart 2019

Gebundenes Buch
Format 21,0 x 28,0 cm
144 Seiten mit 180 teilweise farbigen Fotos

ISBN 978-3-613-04199-8
Titel-Nr. 04199
Preis 19,95 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Der Mercedes-Benz 319 ist wohl nicht irgendein Transporter. Nicht ganz so populär wie der Dauerbrenner von Volkswagen, der erst kürzlich Thema in unserem Magazin war, ließ er seine direkten Konkurrenten jedoch schnell hinter sich.

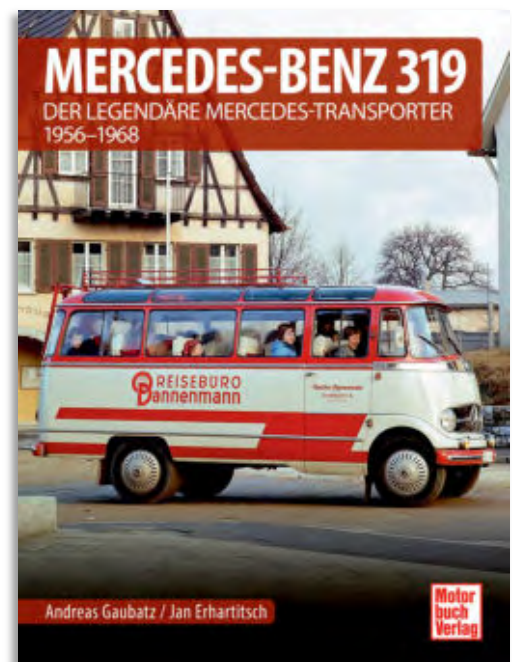
Und dies ist umso erstaunlicher, als dass dieses Fahrzeug nicht mit revolutionären Innovationen punkten konnte, die seinen Wettbewerbern um Längen voraus gewesen wären. Es war einfach seine schlichte Eleganz und Ausführung als Frontlenker, die ihn auffallen ließ und seinen hohen Praxiswert ausmachte.

So trat er quasi die Nachfolge des legendären Opel Blitz an, den Daimler-Benz auf Geheiß der Reichsführung in Kriegszeiten fertigen musste, weil er sich dem Mercedes-Pendant überlegen zeigte. Die Marke mit dem Stern wollte nach der „Stunde Null“ ein Vollsortimenter werden, denn noch schien der Markt an Nutzfahrzeugen kleiner und mittlerer Transporter sowie der schweren LKW fest aufgeteilt – jede Klasse bediente ein anderer Hersteller.

Wenn ein Unternehmen so breit investieren möchte wie der Stuttgarter Autopionier zu jener Zeit, dann sind keine allzu großen Sprünge möglich. Damit ist die Vorgeschichte des mittleren Transporters, der den ersten Baustein für die heutige Dominanz in der „Sprinter-Klasse“ legte, schon zu einem großen Teil erzählt.

Wer mehr dazu erfahren möchte und Wert auf Details, exakte Beschreibungen und einen Blick auf die konkurrierenden Marken, wie etwa Vidal & Sohn mit dem Tempo Matador („Knautschgesicht“) oder Ford mit dem Transit, legt, ist mit dem vorliegenden Buch gut beraten.

Ausdrucksvolle und auch anspruchsvolle Aufnahmen aus der Zeit dieses Fahrzeugs und heutiger Restaurierungsobjekte, auch aus dem MB-eigenen Bestand, unterstreichen die umfangreichen Textausführungen dieses Bands.



Der Autor Andreas Gaubatz, seit frühester Jugend glühender Anhänger französischer Klassiker, hat ein Herz und eine Leidenschaft für historische Fahrzeuge aller Art. Zusammen mit Jan Erhartitsch hat er eine außergewöhnliche Lektüre geschaffen, die keine wissenschaftliche Abhandlung darstellt, sondern anspruchsvolle Unterhaltungsliteratur für Gleichgesinnte sein will.

Wer alte Autos liebt, möchte schließlich Spaß am Lesen haben, wenn er sich mal in Buchform mit ihnen beschäftigt. Technikgeschichte und wichtige Konstruktionseigenschaften, aber auch Schwächen, dürfen dabei dennoch nicht zu kurz kommen.

All das weiß dieser Titel in beeindruckender Weise in sich zu vereinen. Anschaulich wird erzählt und berichtet, wie der L 319 als erster Mercedes-Transporter der Nachkriegszeit auch zur Basis für die Kleinbusse des Unternehmens wurde.

Seine robuste Bauweise mit blattgefederten Starrachsen waren keine technisch herausragenden Merkmale und ließen Fahrkomfort vermissen. Gleichzeitig gewährleisteten sie aber auch das Durchhalten unter den harten Beanspruchungen von Handel und Gewerbe. Wenig aussagekräftig blieben jedoch die nach außen nicht nachvollziehbaren Typenbezeichnungen, die keinen Rückschluss auf Nutzlast oder Motorisierung zuließen.

Dies wurde 1963 geändert, Daimler-Benz übernahm Hanomag und erhielt dadurch Familienzuwachs. Letztendlich erwies sich das als Startschuss für internationale Karrieren und Entwicklungen, die nicht vorauszusehen waren.

Zuerst in Sindelfingen und später in Düsseldorf gebaut, reisten auch Teilesätze zur Montage ins spanische Werk Vitoria und sogar nach Argentinien. Nachfolger des L 319 wurden der „Düsseldorfer Transporter“, aber auch die geschmähten MB 100 und die vielseitige Reihe des 207.

Zum großen Klassiker wurde aber erst ein anderer Nachfahre, weil er inzwischen fast das ganze Spektrum der Nutzfahrzeuge unterhalb des 7,5-Tonner-Einstiegs beim LKW abzudecken weiß: Der bis heute gebaute und schon in mehreren Generationen existierende Sprinter ist heute so alltäglich wie sein Urahn.

Im heute besprochenen Band wird der „Methusalem“ in all seinen Facetten gezeigt und gewürdigt. Auch Feuerwehrautos, kommunale Fahrzeuge und Sonderaufbauten werden hier thematisiert. Anzutreffen waren die Fahrzeuge mit verschiedenen Pritschen, Kästen und in Bus-Ausführungen bei Reiseunternehmen, Gewerbetreibenden und auf Baustellen.

Mit Blick auf Modelle für die Spur Z, die in der Vergangenheit angeboten wurden, lohnt sich das Lesen und Nutzen der Informationen. Sie helfen, einen unverzerrten Rückblick zu erhalten und die frühere Gegenwart glaubhaft dazustellen, wie sie sich oberhalb des VW-Transporters darstellte.

Und wer selbst ein Herz für nostalgische Straßenfahrzeuge hat, der wird zusätzlich noch mit den Geschichten von Wiederauferstehungen verschlissener, verrosteter und verrotteter Relikte verwöhnt, die heute als hochglanzpolierte Vorzeigeobjekte mit H-Kennzeichen zu Treffen mit Gleichgesinnten fahren.

Publishing pages with reference possibility:
<https://www.motorbuch.de>

25 Jahre

Faszination Modellbau

*Internationale Leitmesse
für Modellbahnen und Modellbau*

1.-3. NOVEMBER 2019

MESSE FRIEDRICHSHAFEN

Öffnungszeiten: Fr. und Sa. 9.00–18.00 Uhr, So. 9.00–17.00 Uhr



Willkommen zum 25. Jubiläum des weltweit größten Modellbau-Events! • Miniaturen aller Modellbausparten • Indoor-Flugschau • Outdoor-Flugschau „Stars des Jahres“ • FPV Drone Race • RC-Car-Offroadshow • German Open RC-Car Masters • Truckparcours • Roadworker • 400m² Wasserbecken • Modellbahn-Anlagen aller Spurweiten • uvm.

WWW.FASZINATION-MODELLBAU.DE



ZEITGLEICH:

www.Echtdampf-Hallentreffen.de



That was Märklintage 2019 Exhibition in Göppingen

Between 13 and 15 September 2019 the time had again come: IMA, Märklintage and the Southern Germany Z-gauge Convention called the model railroaders to Göppingen. With a lot of role models at the station, big company participation and worth seeing facilities, it was again very interesting this time. And unlike what was announced two years ago, Märklin didn't have to look for a replacement for the shipyard hall. Stephan Fuchs reports for our readers.

By Stephan Fuchs. Like every two years the now 12th Märklintage in connection with the 36th International Model Railway Exhibition and a new edition of the Z Convention South Germany called to Göppingen from 13th to 15th September 2019.

Contrary to some fears and the original announcement, not only the shipyard hall, but also the Dr.-Herbert-König-Platz were available again in Stauferpark. Thus, it was once again possible to fall back on the proven venues Stauferpark, EWS-Arena, Bahnhof, Leonhard-Weiß-Areal as well as Stammwerk with the first part of the Märklineum.



The crowds were large for the Märklintage 2019 at all venues, but above all, of course, again at the vehicle show at the station.

The Märklin Museum, including the factory outlet and old treasure trove on Reutlinger Strasse, was closed in mid-2017. Since September 2018, everything has been located at the main factory, accessible from Reusch Strasse. However, the new museum, with its historical exhibition and exhibition facilities on the site now called Märklineum, has not yet been opened.

According to current plans, this second construction phase will be inaugurated in May 2020. As a small appetizer, a view of the H0 layout already in the hall was possible for the first time during Märklintage.



For the first time, visitors were able to take a look at the shell of the large layout currently under construction at Märklineum. And work on it continued before their eyes.

Free buses were again used to quickly transfer guests between the individual exhibition venues. As always, this was very well organised, even though some buses were in danger of bursting at the seams. Or in short: I felt right at home...

But it started for me already on Thursday, because I wanted to capture the special train from the Bavarian Railway Museum in Nördlingen in both moving and static pictures. Like the visit of the Märklintage, this has been a fixed date in my calendar since 2011.

The train even arrived halfway on time, which was not necessarily the case in previous years. It's always great to see and hear how the machines, some of which are more than a hundred years old, are puffing, steaming and rattling. There were again more photographers in Uhingen than at the last edition of the event, a trend that I have been noticing since 2011.

On Thursdays, most manufacturers and exhibitors set up their stands, which is why I went to the shipyard hall to see the Zetties right after my special train photo session. The western adjoining room, which was occupied several times with exhibits of our small track until 2017, was no longer available this year for the first time.



Certainly not everyone gets to see that: Before they can start, the exhibition vehicles arrive by special train. Our author was present in Uhingen and was able to photograph the train from Nördlingen in the very best weather.

And so almost all Z gauge trains now found space in the south foyer of the shipyard hall. Only Rainer Tielke with the double-decker bridge of Bullay and Heckl small series were not in the side wing, but in the shipyard hall itself. Like his work, most of the other layouts in our community were already known from other events and reports.

In the south foyer, however, many beautiful exhibits could be seen in any case, such as the four seasons layout from Trafofuchs. Birgit Foken-Brock also used it here to show off her craftsmanship to the public.

Gerhard Maurer set the mood for autumn, which is not far away, with the yellow and red colours of the vegetation on his dream loops. His most recent layout had recently received animal growth in the form of a flock of sheep, which led to more life and new scenes to explore.

Photos on the right:
Motif of the four seasons layout from Trafofuchs (above) and new scene on the "Traumschleife" by Gerhard Maurer (below).

His friend Manfred Forst presented agricultural motifs with the



“Schweineanlage”, surprised with his “Kohlenkiste”, but also with a wonderful staging of the Hunt’s large coaling plant from the Märklin program.



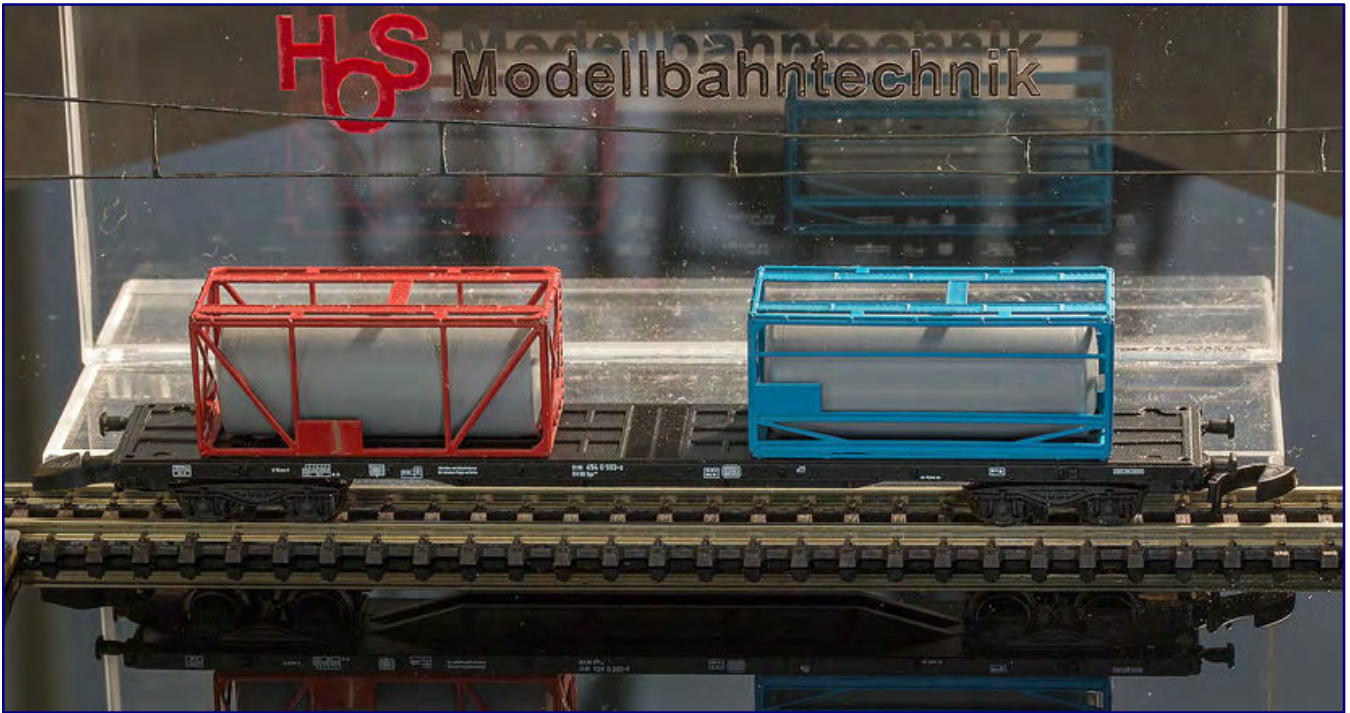
Cardboard model making expert Manfred Forst had perfectly staged Märklin’s Hunt’s large coal handling system for the exhibition (top photo). Jürgen Walther is waiting for the big onslaught behind his “Hirschsprung” layout, which has since received many more trees (photo below).

By the way, the word “box” should be taken literally here, because Manfred likes to build closed eye-boxes that are limited to the bare essentials. I am fascinated by these dioramas precisely because of their purism.

German Magazine for Z Gauge

Not to be missed were the Vienna Stammtisch layouts, Karl Sinn's "Kesswil" railway station, which has since been completed, and the illuminated effect dioramas and vehicles by Z-Lights (Torsten Schubert). The layouts of the Berlin Stammtisch were not (any longer) recognizable as such.

Guests not familiar with the track gauge can be impressed again and again and especially with the Z-Car system from KK Produkcja, which was awarded the **Trainini®** as the best new release of the year 2017. This worked again in Göppingen. The dwarf train by Ingo Sindermann, which runs on Z-gauge tracks and was already presented in the Köln (Cologne) report of the previous year, is more aimed at children.



Above the new tank containers (photo above) from HOS Modellbahntechnik you can also see the new overhead contact line of highest filigree, designed for the Märklin system. On "Drachenstein" a hippie bus has now arrived at Harald Hieber (photo below).

A further eye-catcher was a suitcase system with moving buses and a camera, Railex showed earlier Feinode, which are known to be sold out. It wasn't quite enough for Peter Friede, who actually wanted to demonstrate a new state of development from his wood grindery on the Lech. So the anticipation for the next exhibition remains, because in the meantime it was already going well, as he let us know.



The club table "ZIST 1 Berlin-Brandenburg" had again united many club members' table modules with and without tracks into a large L-layout (photo above). Karl Sinn from Spur-Z-Atelier is waiting in front of his new railway station in Kesswil (photo right) for visitors to enter.

Photo on page 46:
One of the most impressive exhibits of this exhibition was certainly the outstandingly staged Neuschwanstein Castle on a scale of 1:220 by Ulrich Günther.

The Z-Customizer was also represented in the shipyard hall. Our "dirty pig of the service," Sascha Braun, showed beside his own great products, which once began with realistically reproduced graffiti smithies, also the still young layout "Crazy Nugget Mining".



About HOS-Modellbau was to be read only in the last issue in the messages. Heinz O. Schramm had planned the premiere of his new catenary parts, suitable for the Märklin system, but much finer etched and burnished on request, for the Spur-Z-Convention South-Germany.

He has also developed his own fastening elements for Märklin's line masts, which make it possible to dispense with the clamps on the catenary that are not prototypical. A novelty ready for exhibition were his tank containers for Märklin wagons with finely etched supporting profiles around the massive tanks.



A selection of his innovations is currently also being processed in a **Trainini®** project and will soon be shown. Because of the Black Forest theme, this leads me to Jürgen Walther's "Hirschsprung" layout, which has been further re-forested and has changed its "face" even more - simply great to view!



Jürgen Wagner's "Mittelerde" layout was also staged with videos by our journalist colleague Peter Pernsteiner.

Also there was the layout "Mittelerde" (Middle Earth) by Jürgen Wagner, which the journalist Peter Pernsteiner, himself an enthusiastic "Einser" (1er), presents with a film portrait on his Youtube channel (webpage address at the end of this article in the info box). Among the Zetties, this layout is now just as well known as the black-and-white viaduct in Raffaele Picollo's picture frame, designed after an old postcard.

Our Italian friend from Genoa had also brought along the comic book layout he had presented last year in the Land of the Rising Sun on Osaka-Z-Days. To the diorama "Drachenstein" by Harald Hieber, an almost perfectly designed layout, it can only be said that a portrait of the layout seems long overdue...

Here, the railway seems to be built into the landscape, with many facilities, rather the other way round. This showpiece is also spiced with humour in many places, but above all an illuminated hippie bus now seems to attract attention. If a few suitable figures are added soon, they will certainly make you smile.

Idyllic photo motifs, for dreaming and because of the realistic effect also amazement, were offered by "Schloss Neuschwanstein" by Ulrich Günther, who also had a "used exhibition module" in his luggage. Those who, as Zetties, indulge in a passion for collecting, could once again not get past the special wagons. There were three different ones on offer again: an old MHI tank car with historic Märklin lettering



(Art. No. 86117), a heat protection car according to G-10 drawings for the event with 160-year motifs (80922) and roof printing, and a red, dust silo car (86666) as a souvenir of the Z gauge convention.

Good mood

What I noticed right away and this impression remained throughout my entire presence: 2017 was



Three special Z-gauge cars were available at the Märklintage: Old tank car with historical lettering for the MHI (Art.-No. 86117; picture top left), 160-year special car with roof printing (80922; picture left) and a dust silo car for the Z gauge convention (86666; picture right). Photos: Jürgen Faulhaber

somehow a strange mood during the Märklin days. I couldn't really grasp it then, but others had the same impression. Fortunately, in 2019 there were no further such signs - on the contrary! It was just a good meeting.



No matter whether at the cash desks, in front of the factory or during the inspection of the assembly line: long queues were already forming everywhere on Friday. The visitors had to bring a lot of patience with them.

Friday was the first day with and for the audience. There were lines of equal length before the ticket sale. It was no different at the main factory, where many people stood in line for a tour of the factory. I hadn't expected that on the Friday.

For me this meant thirty minutes of waiting, later it was even more for the masses at the gates. Also in the factory itself it was already very crowded around 11:00 o'clock, and it was hard to get through.

And who wanted to acquire there the special car, had to plan further 45 minutes waiting time. Therefore some criticism seems to be appropriate here: A single cash register is simply not sufficient for the car sale. The fact itself that there was such a strong demand at all, however, I consider a very good sign. I think it's a pity that the heavy class 44 freight steam locomotive still has a somewhat loveless existence in the inner courtyard and has to wait for a new paint job.



In front of the EWS-Arena the narrow gauge Meyer steam locomotive 99 1594-3 made a lot of steam.

Little time was spent on the Ilm track, i.e. the garden railway. What was impressive about the EWS-Arena with its various LGB participations, however, was at least, because it was visible from afar, the steaming Type Meyer steam locomotive 99 1594-3, which was placed on a rail joint in front of it and was once placed on the 750 mm track as a Saxon IV k.

At the station I was able to admire a real “crocodile” for the first time, that was very impressive. This Swiss museum locomotive does not have a German train protection system and is, therefore, not approved for operation on German tracks.

Without further ado the former Austrian E 94 088 of the Elektrische Zugförderung Württemberg GmbH was used to bring the unequal sister to Göppingen. The journeys in the driver's cab of the German

counterpart were very popular, without a break, the trains departed almost every minute.



Of course, Märklin's “heraldic animal” could not be missing for the anniversary: From Switzerland, the Ce 6/8II arrived in Göppingen in 14305, which was only recently implemented in various scales.

A special highlight was then on Saturday the appearance of the E 03 001. I came earlier still into the benefit to drive in the tow of a 103 in the Intercity. I had never seen a pre-series locomotive E 03 in the

original. Accordingly, the photos turned out to be more of a snapshot, because I hadn't expected the "lady". This is simply a great locomotive!

At the Leonhard-Weiss-Areal on Saturday the 75 1118 was standing, because there was a defective brake line and could be welded there again. The locomotive including the shuttle train had to be towed by the Bavarian Railway Museum on the route between Plochingen and Göppingen by the V 100.

The special train over the Geislinger Steige and back with the S 3/6 3673 and 44 2546-8 from the Bavarian Railway Museum passed here with a strong plume of smoke, but there were so many photographers on site that it was a matter of luck to photograph only the train without arms or hands of third parties in the picture and above all not blurred. The impression on the stairs in Göppingen station was almost even better and more moving.

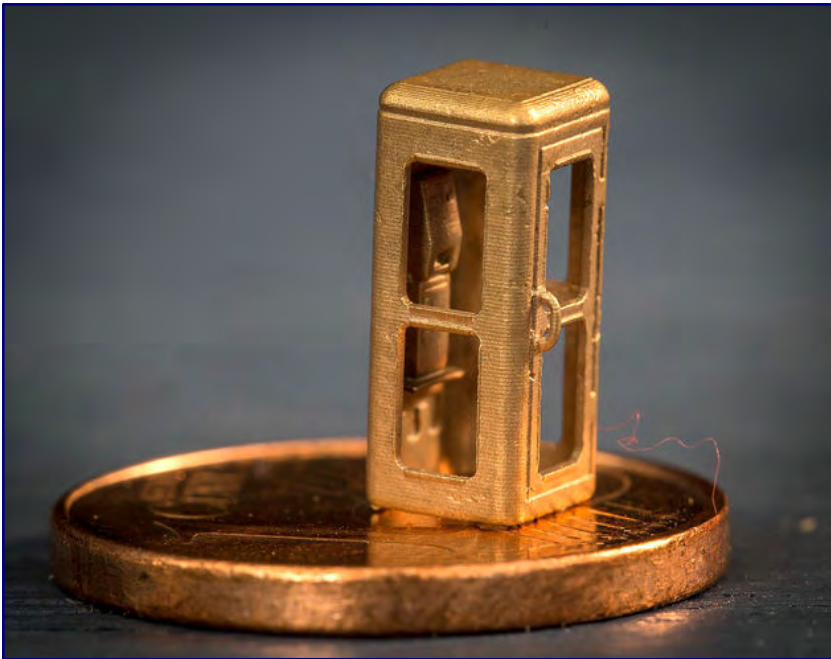


The special train over the Geislinger Steige with S 3/6 3673 and 44 2546-8 (as a push locomotive at the end of the train) from the BEM Nördlingen was also closely surrounded. This was not only true for the platform, but also for the stairs, on which every centimetre was occupied.

Back in Stauferpark I was then able to search for new products for our gauge. At Märklin there were various bronze models to see as an innovation. The original models for lost forms are created from 3D prints, which are then filled with bronze.

An example of this was a telephone box in various scales. I captured the last design of the telecommunications division of the former Deutsche Bundespost at the casting boom in Z scale, of course. The only thing missing from this model is the telephone receiver inside the cell. This could not be easily produced in such a small scale, but only in the larger ones.

In the second tent on the Stauferpark in front of the shipyard hall, private installations of various nominal sizes were again exhibited, including a Belgian Z gauge layout by Toon Versnick. Unfortunately, taking



A demonstration of the skills: Märklin demonstrated the possibilities of brass fine casting using 3D printing and lost moulds using a telephone booth for Z gauge, as an example.

pictures there was almost impossible, because the heavily swaying wooden floor ruined almost every photo. However, I managed to take a few photos.

If you wish have a fuller impression, you will soon be able to see this layout up close. Toon is planning to take part in the Z scale weekend in Altenbeken 2020. And in all probability, this should work out.

Furthermore, a 40 year old shop window system of the Z gauge could be discovered in tent 1, but more of our scale was not present there.

However, the Association of Swiss Digital Model Railroaders (VSDM), which visited one of its installations abroad for the first time and had already sought contact with our magazine, certainly deserves a brief note.



This layout comes from Belgium and was demonstrated by Toon Versnick. It captivates by a “green hand” and many self-built details like the arms of the catenary masts or the finely detailed catenary system.

Interesting for the Insider Club Belgium is the special approach of having identical H0 systems built in the track diagram and then programming a digital operation for them. This program, I was told, takes place

because there are few model railway exhibitions and events in Belgium. In order to keep the Insider Club interesting for the public, this program was begun.

At such a meeting, like the one in Göppingen recently, the discussions among each other are of course very important. This time I also remembered the audience, some of whom were very knowledgeable. So I can look back on a great event with many impressions and look forward to the next time. See you then 2021!



That's it for 2019! We will continue with the next edition of Märklintage, the International Model Railway Exhibition in Göppingen, as well as the Spur-Z-Convention South Germany, in two years.

PS: Crocodiles have to go back into the enclosure. That was the case on Monday, which I was able to record. And with this picture I say goodbye for today!

All photos (when not marked otherwise): Stephan Fuchs

Exhibitors mentioned in the text:

- <http://www.forstberg.de>
- <http://gerhardingen.de>
- <http://www.heckl-kleinserien.de>
- <http://www.hos-modellbahntechnik.de>
- <https://www.maerklin.de>
- <https://www.picollo.it>
- <http://www.rainer-tielke-modellbau.com>
- <http://www.spur-z-atelier.de>
- <http://www.trafofuchs.de>
- <http://z-car-system.de>
- <http://www.zcustomizer.de>
- <http://z-freunde-international.de>
- <http://www.z-lights.de>
- <https://1z220.de>

Videobeitrag zur Anlage „Mittelerde“:

- <https://youtu.be/6ansJmg8GRw>

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Here, joy reigns over the portrait of his diorama:

Thank you very much for the great article you have written about my diorama. Great photos which you have also added and the picture descriptions are just great. Many thanks for this honor.

Keven Horat, per email

The originator of the ghost car construction report writes to us:

Cordial thank-Z for publishing my article and for all the effort you have put into correcting and completing the photos. I am very happy with the result.

If the article can be an inspiration for one or the other Zettie to take the challenge of reconstruction, that would be very nice.

Greetings to your whole team with great compliments and thanks for the wonderful work, which is shown again and again in every edition of your great magazine!

Thomas Heß, per email

This reader writes about the model of the 86 457:

Many thanks for another great Trainini magazine! Every time I am excited about the many, yes loving and certainly with passion published edition of Trainini!

Here and now I would like to make a brief comment about what disappointed me a little when I read it: This is the article about the BR 64 and the BR 86.

I noticed the following with the description of the BR 64 and 86: In the DB model, the 86 457 is steaming up; in the Z model, it received further exemplary details under the capable hands of Michael B. as a Märklin model; plus the number of its big sister, 86 547. But that's not right, is it?

I mean, there were no two different BR 86 models with the end number 547 at the DB. Not for that reason alone, because they are two different types: the "big" DB BR 86 547 with its shortened feed water boilers is a - later - series 86; while the model of the BR 86 in Z is from Märklin from the early series, with the long feed water boxes and the round "control lids"!

The "model converters" of the Märklin-86, which was correctly reproduced from the early series, would have found it easy to change the feed water boxes, wouldn't they? Sand down, saw off, fill again, finished.



It would have been consistent with the model locomotive of the DB and thus become a prototype model. An unused chance...

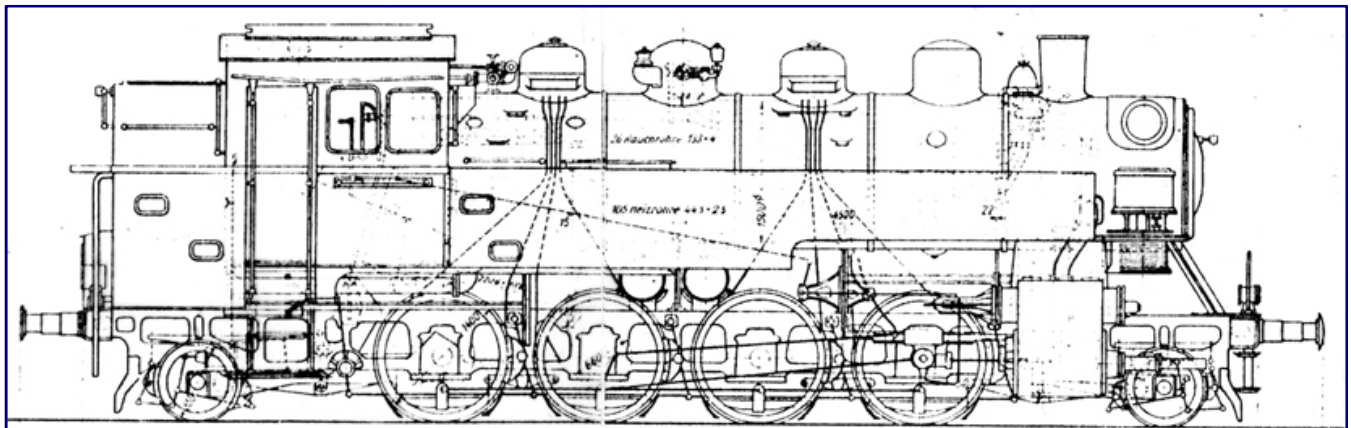
Rainer Spohr, Berlin

Answer from the editors: In order to make this letter comprehensible for all readers, we would first like to explain that the article deals only with the 86 457 in both the prototype and the model. Company number 547 can therefore only be a transposed number, which is irrelevant for the further explanations.

Our reader refers to a design change to the 86 series, which was carried out in 1941 with the company number 378. The water boxes were then shortened and further cut out in sections. On the model, taking this feature into account would have meant shortening Märklin's castings by about 1 mm. We considered and still consider this to be an unnecessary effort, especially in comparison to the number plate on the smoke chamber door, which is too low.

For the work on this model we and afterwards Michael Bahls only had a very tight time window for the chassis, because the locomotive had to be finished on time for an article in a professional magazine. Otherwise, our activities might have included another feature with which the locomotives from 86 252 (year of construction 1936) were equipped. Starting with this model, the water boxes were no longer riveted but welded. Since Märklin reproduced the rows of rivets very finely, sanding and varnishing work would have drawn much more notice here.

Of course, we take the letter to the editor as an incentive to raise the bar every now and then. At the same time we also want to ensure that the majority of model railroaders can and will find themselves in our articles. Therefore, not everything that is possible will be implemented. As editors, we also ask for your understanding.



This drawing shows the original version of the class 86 at the German Empire Railroad Company (with double compound air pump), which still carries long water boxes without cut-outs, as they can also be seen in the Märklin model. The museum steam locomotive 86 457 belongs to a later series in which the water boxes in the model would have to be almost 1 mm shorter. Drawing: German Empire Railroad Company, Archive **Trainini®**

Priceworthy Noch trees available:

In our trade fair edition 2/2019, we had also pointed out the new Noch tree packs, which can be used to afforest large areas at very reasonable prices. Of the 16 announced packs, seven have now been delivered, in which the Z-gauge versions can be found:

- Deciduous trees, 8 pieces, 10 – 14 cm (article number 24600),
- Deciduous trees, 16 pieces, 10 – 14 cm (24601),
- Mixed forest, 4 deciduous trees+ 4 firs, 10 – 14 cm (24620),
- Mixed forest, 8 deciduous trees+ 8 firs, 4 – 10 cm (24623),
- Firs, 16 pieces, 10 – 14 cm (24641),
- Firs, 16 pieces, 4 – 10 cm (24643) und
- Snow-covered firs, 16 pieces, 4 – 10 cm.

The blanks of these standard trees are turned by hand, then cut into shape, colour-designed and flocked with a specially developed material. Each specimen has a root foot.

ZettZeit takes a time out:

Jens Wimmel starts on a one-year journey across the American continent. Many readers noticed already for a long time that his earlier well maintained site (<http://www.zettzeit.ch>) had not been updated for a long time.

Now he has announced to archive the pages and to take them off the net. Whether and how it will go on, he wants to decide and announce only after his return. We are sure and confident that even the long journey through America will not cure him of the "railway bacillus."

Therefore we are looking forward to a new start and look forward to seeing what impressions will shape and influence his hobby in the future!

With Full Throttle it continues:

WDW Full Throttle has delivered new models and resumed distribution. In September, a bright red 34-foot wooden refrigerated car with yellow sidewalls of the Grand Trunk Pacific was delivered, which is offered in a double pack (Art.-No. FT/B-9021-1).



Photo: WDW Full Throttle

New figures at C-M-K in October:

Creativ-Modellbau Klingenhöfer opens a new Eco-series, which is supposed to accommodate customers in terms of price. The articles of this series are simply painted and can be offered cheaper due to the previous similar items.

zettzeit macht Auszeit



Wie schon vor längerer Zeit angekündigt, verabschiede ich mich Ende Oktober 2019 für ein Jahr von Europa und bereise mit meiner Partnerin Süd-, Mittel- und Nordamerika mit dem eigenen Camper. Die Vorbereitungen für diese Reise und die Auszeit, das Anpassen des Wohnmobils, das Erlernen und Erfahren des rollenden Heims und das Eintauchen in die Camper-Community beanspruchten den Grossteil meiner Freizeit. Für das Spur Z-Hobby blieb oft keine Zeit und meine persönliche Z-Zeit beschränkte sich auf wenige Stunden.

1999 hatte mich der Z-Virus in Form einer Startpackung erwischt und was danach passiert ist, dürfte bekannt sein. Seit dieser Zeit habe ich unzählige tolle Erlebnisse gehabt, wunderbare Menschen kennengelernt, Freunde gewonnen, mit viel Freude Messen und legendäre Z-Treffen besucht. Ja, die Spur Z hat einen ganzen Lebensabschnitt mitgeprägt. Ich denke gerne an diese Jahre zurück und die Leidenschaft für die kleinen Modelle besteht auch weiterhin.

zettzeit.ch bzw. die Vorgängerseite ist inzwischen seit über 19 Jahren online. Dass der Webauftritt Faszination, Inspiration, Ansporn und Unterstützung verbreitet, das war mein Ziel. Erreicht habe ich es wohl, unglaublich viele positive Rückmeldungen in all den Jahren zeigten dies. Was die Welt aber nicht braucht, ist eine ungepflegte, vor sich hin dümpelnde Webseite, die sich nicht weiterentwickelt. Ich habe mich daher entschlossen, den zum 23.10.2019 auslaufenden Webhostingvertrag nicht zu verlängern. zettzeit.ch geht an diesem Tag vom Netz. Die Domain bleibt reserviert. Die Daten werden gesichert.

Was die Zukunft bringt, ob und wie es weitergeht, das ist derzeit nicht zu beantworten. Somit bleibt mir am Schluss, euch zu danken für diese grossartige Zeit, es war mir eine Freude und eine Ehre!

Gerne dürft ihr die Reiseabenteuer auf vanamericana.ch mitverfolgen.

Jens

For the time being we only have this farewell photo on the (already cancelled) page. Originator: Jens Wimmel / ZettZeit

And there are now two more double packs of the same construction type with different operating numbers available. These red-brown, reconstructed cars (FT/B-9006-1 / -2) once belonged to the stock of the Canadian Pacific Railroad.

Products from this supplier are distributed in Germany by Case Hobbies (<http://case-hobbies.de>), among others.

The start is made by “3 scooters simple” (ECO-VE01) in plain basic paint, which can be selected from red, green, brown, blue or yellow.



The three simple scooters (Art.-No. ECO-VE01; photo left) and the three swans with mounted feathers (ECO-WA19; photo right) open the new Eco-series at C-M-K. Photos: Klingenhöfer

The "man on motor scooter" (VE01), on the other hand, also uses this type of moped, but like the figure it is painted more elaborately and belongs to the usual program. At home is the moped rider from the fifties to the present, because his vehicle, which originated in Italy, has almost attained cult status.



The woman with cat (TK03) closes a figure gap in the Z gauge assortment. Photo: Klingenhöfer

The “woman with cat” (Art.-No. TK03), who confronts her pet with a transport box and a cuddly blanket, may also have what it takes as a model.

The black tomcat fits seamlessly into the unique animal figures of this supplier and thus enlivens every layout scene.

The “3 swans with upright feathers” (ECO-WA19) should also be a successful product. The snow-white birds require only a few operations and have therefore been included in the Eco series.

Since water scenes are very popular on installations, there is no way around adding them. Of course, nobody wants to get involved with the proud birds.

You can order new products as well as the regular catalogue programme on the pages of the website at the following website: <https://www.klingenhoefer.com>.

Märklin deliveries in October:

It is already noticeable that the Christmas business is approaching, as the number of new products delivered is on the rise, as is the number of those whose planned date is soon to come. Many customers will probably be delighted with the two Green Cargo articulated tank cars (Item No. 82532) of the Zacns type for Era VI.



Long awaited were the attractive "Green Cargo" articulated tank cars (Art. No. 82532), which Ermewa SA has discontinued in Sweden.

Many Zetties had been waiting for their release for a long time, as they still belong to the spring novelties of the year 2018. The colourful models attracted an unexpectedly great amount of attention.

Their modern role models from Ermewa SA transport kerosene to the airports in Sweden, and the models use close couplings to ensure a good appearance in trains.

Märklin does not currently have suitable locomotives in its range, but customers will find them in the NOHAB from FR Freudenreich Feinwerktechnik.

Also, the latest innovation, the class 103 (88544) high-speed locomotive, which was designed to power the latest Rheingold set, was also delayed.

The beige-purple-red series locomotive now rolls forward with a bell-shaped armature motor and warm-white LED tip lighting depending on the direction of travel.

The switchover screw for overhead operation is invisibly attached, but for the first time a model in this series also features disc inserts in the engine room. The propriety of the new engine room lighting in analogue mode could be discussed in detail: When stationary, it remains functionless, while operating it illuminates the engine room -- exactly



Der Frankenzucker-Staubsilowagen (86667, Bild oben) und der Aral-Kesselwagen (82324, Bild unten) bedienen die Epoche III.

the opposite of what we can see in prototype.

At home in epoch III, on the other hand, was the dust silo car of the genus Kds 54 by Frankenzucker (86667), which was fitted with advertising addresses and was put into service at the Deutsche Bundesbahn. Its two chambers for fine bulk goods were used for transporting foodstuffs.

Longingly expected by the editorial staff was the old tank car of the BV-ARAL-AG from Bochum (82324), also employed by DB, to be classified in the same period. This grey wagon with disc wheels is only a lettering or colour variant of earlier models, but offers some variety.

For the first time, the logos of a mineral oil company have not only been printed on the boiler, but have also been reproduced as attached signs, as in the former models. We take this novelty as an opportunity to go into detail on the subject of mineral oil supply in the Wirtschaftswunderland (economic wonderland) and its reproduction in the model in the next issue.

Large calendar selection for the year 2019:

As the autumn approaches, the major publishers present their various calendar motifs for next year and vie for customers' favour. This applies to the VG Bahn or Geramond as well as to the EK-Verlag, whose programme we would like to briefly present here.

Here you will find a large selection of various theme calendars on the model and model: Steam locomotives 2020, North America 2020, Germany 2020 or Alpine railways 2020, for example, can be selected in the model area.

With Mythos Rheingold, Feldbahnen, Sauschwänzlebahn, Harzer Schmalspurbahnen or Mollis there are also very special themes on offer.

The appealing calendar "Series 218" in honour of the most successful DB line diesel locomotive certainly deserves a special mention, with many colourful and highly varied motifs showing the variety of this design.



Anyone interested in other technical details should also find what they are looking for, as trams, lorries, buses and tractors have their own editions dedicated to them. The same applies to the large track 1 in the model railway area.

The other sizes can be found in "Modellbahnen 2020" (Order No. 5844 / ISBN 978-3-8446-5844-6; 11.80 EUR), which we have looked at on behalf of the listed editions. Even the title page is very inviting here. The content, which can be seen in small pictures on the publisher's pages (<https://www.ekshop.de>), also lives up to its promise.

Unfortunately, this time (for the second time in a row) it didn't bring a Z scale motif into the selection, whereby the content is mixed seasonally, according to epochs and also cross-country. That's why Zetties will certainly enjoy the thirteen calendar pages on spiral bound art paper.

If you still don't want to do without a Z-scale motif, you can take the photo calendar "Traumanlagen von Modellbahnprofis" (ISBN 978-3-95843-859-0) from Heel-Verlag. In this calendar, created in cooperation with the "Modelleisenbahner", a motif of the "Kniephaven" layout by Dirk Kuhlmann is shown in May.

Model railway manufacturers sound the alarm together:

“Vision and mission are in place; now the implementation follows, after students of the Stuttgart Media University have set the conceptual and strategic course,” begins a current press release of the German Association of the Toy Industry, Model Railway Section.

The manufacturers that make up this group have apparently recognised that their true competitors are not other suppliers in the same segment, but lurk outside their own industry. They are therefore now blowing their guns together across competitive boundaries and want to promote the most beautiful hobby in the world jointly and effectively.

The formal starting signal was given by the model hobby show at the beginning of October in Leipzig, the leading trade fair in the leisure sector. With it a bundle of marketing measures starts, which reaches from the common web appearance up to the campaign with a self-satirical advertising short film and announcements.

Using the motto “Wir Modellbahner” (We Model Railroaders) (at <http://www.wirmodellbahner.de>) and a six-figure campaign budget, a seasoned “biker” will initially be campaigning for model railroading from the beginning of October in order to bring it back to families, children, people in their prime, baby boomers and all other friends of this hobby, with wit and humour.

It is commendable that not only advertisements in paper magazines are used, but also modern and social media. Unfortunately, the public's own demands and perception differ widely, and not only are the manufacturers allowed, but the followers should take an active stand against this.

After all, we model railroaders are also often portrayed in the daily press as cranky, old-timed crackpots. How creative and technically as well as electronically demanding this hobby is, and what challenges it poses are often overlooked or deliberately ignored.

Therefore we would like to actively promote the International Model Railway Day in the week of December 2, 2019. Talk to like-minded people and use it to show publicly and as broadly as possible that model railways are by no means dead or dusty!

Z gauge at Faszination Modellbau:

There are only a few days left before the fascination of model building opens its doors at the Friedrichshafen Exhibition Centre. Just in time we can now announce which exhibitors and exhibits will be represented at the exhibition area organized by Z-Freunde International:

Module “Traumschleife” (Gerhard Maurer)
Hunt'sche Großbekohlung und Auto-Schaukästen (Manfred Forst)
Layout “Heinzenberg” (Jürgen Tuschick) - Premiere –
“Moselbridge Bullay” (Rainer Tielke)
Cuba-Anlage (Sascha Braun)
“Wood grindery on the Lech” (Peter Friede) - extension of a new module -
“Bahnhof Kesswil” (Karl Sinn)
“Drachenstein” (Harald Hieber)
Light effect dioramas and soldering demonstrations (Torsten Schubert)
Z-Car-System (Oliver Kessler)
Railex (Roland Kimmich)



This gives you the opportunity to take a personal look at well-known and appreciated exhibits in the border triangle, to profit from the knowledge of well-known model railroaders and to discover new things.

A discount of 3 Euro on the admission price granted by the organizer to our readers is intended to increase the incentive for a visit. If you print out the discount corner of our advertisement and present it at the box office, you will be charged the reduced admission price of 12 instead of 15 Euros.



The two-axle chassis with MAN cabin of the F8 (Art.-No. XD033_Z) opens up various self-construction options. Photo: EtchIT

Vehicle substructure for individualists at EtchIT:

New at EtchIT Modellbau (<http://www.etchit.de>) is a truck cabin of the MAN F8 in medium length design with two-axle chassis (Art.-No. XD033_Z) for own truck trailers. Zetties who are eager to rebuild might have waited for this conversion, especially since they promise creative solutions and variants for the Z-Car system as well, if it is combined with their chassis.

Challenges for Velmo:

How often have we talked to Claudius Veit on the phone and asked about new decoder solutions? It will hardly be possible to reconstruct it, although every single conversation was interesting and informative. No question about it, the heart of this digital specialist beats for the model railway and for making full use of its technical possibilities.

Märklin's new bell armature motors often caused headaches at Velmo (<https://velmo.de>). In the future they will require split circuit boards, but also safe and reliable electrical connections like all previous decoders.

A decisive criterion here is always a light and customer-friendly installation solution; economically sensible batch sizes are indispensable. And so it took a long development period and extensive practical testing to be able to present the new product presented here today ready for series production.

Velmo has now also found a digital solution for Märklin's still young E 41. It offers the well-known functions and accepts the protocols DCC and SX. The decoder with the designation "VELMO LDS18061" for this model is for the first time not available as a kit or individual part.

This cannot be done because of its challenges due to flexible conductor paths and a not quite so simple assembly, including installation. This is done by the sales partner 1zu220-Shop (<https://www.1zu220-shop.de>).

Only if the installation risk is assumed can it be purchased separately. In any case, we advise you to do your research adequately, well in advance of your order, so as not to risk any failure, here.

Ongoing AZL deliveries at the beginning of autumn:

At the most important US manufacturer for gauge Z, this month's trend continues seamlessly with the light passenger coaches. They are now available in grey designs from Pullman and Union Pacific (Overland), with two-colour and single coat finishes.

The following models are available in two shades of grey interrupted by white decorative lines: 4-4-2- (Art.-No. 73002-1 to -5), 6-6-4 sleeping cars (73102-1 / -2 & 73144-1 to -3) and a matching combined mail and luggage car (73944-1). The passenger train is complemented by three dark grey 6-6-4 sleeping cars (73101-1 to -3) whose single paint finish is kept in the same colour as the window ribbon of the aforementioned cars.

Photos of the manufacturers of the current deliveries can be found at <http://www.americanzline.com>.

Another first Artitec delivery:

The Dutch accessories specialist Artitec impresses the Z-gauge world again this month. The "bridge wagon" (Art. No. 322.028) trailer for the transport of hay and straw bales in rural areas has just been delivered.

In its usual fine and at the same time detailed design, this model surpasses all previous offers of similar shape that the Z gauge had previously offered.

Since it is also quite timeless, the vehicle can be used without hesitation from epoch III to the present. After many tractor models, we want to deal with the model in more detail in one of the next issues.



Recently delivered by Artitec was the bridge wagon (Art.-No. 322.028) for the transport of hay or straw bales.

From Amsterdam the upcoming first delivery of the "Auto-Schutters" or "Dumpers" (322.026) was also announced. You can find the manufacturer site at <http://artitec.nl>.

Sale of Saller models:

Günther Saller has announced the discontinuation of his Spur-N and Spur-Z range. We find this decision very unfortunate, because on a metal casting basis and with the help of further materials, great model displays were created with Saller models for many years, which found a home on the layouts of the Zetties.

These include the extensive range of Zf size light rail vehicles as well as a furniture transport trolley with a wooden body from times when horses were still harnessed, and car and trailer models that were manufactured on behalf of third parties.

If you still have open requests for Saller models (<https://www.saller-modelle.de>), you should hurry up and use the current sale to place an order.

View of the Trainini Photokalender 2020:

For fans of our magazine there will also be a **Trainini Fotokalender 2020** with thirteen motifs dedicated exclusively to the scale 1:220 and trying to capture all the facets of this size.

It will be made available on our website around mid-November 2019, i.e., shortly before the next issue of the magazine is published.

In order to shorten and sweeten the time until then, we are showing our readers the cover photo today.



And this is what Micro-Trains offers:

The Farm-to-table series at MTL now continues with car number 9 (art. no. 518 00 790). Once again, it is a brown refrigerator car with a wooden body and grey sides. Advertising for Nash Mustard appears on it.

60-foot long flat cars with high end walls are now available with two company numbers each for the BNSF (527 00 171 / -172) and CSX (527 00 181 / -182). They are loaded with replicas of steel profiles.



This is what this year's Christmas car (Art.-No. 507 00 670) from MTL looks like. Photo: Micro-Trains

Long before the Christian feast, this year's Christmas car (507 00 670) will be on sale, again decorated with a drawn motif with the company's own micro-mouse.

It is realized on a covered 50-foot standard trolley with sliding doors in red paint, which makes the model very appealing.

Micro-Trains products can be purchased in Germany at Case-Hobbies (<http://www.case-hobbies.de>).

Herpa Wings innovations for the beginning 2020:

At Herpa, due to the usual long lead time, the time has already come to announce the first new products, which are to be delivered in the beginning of 2020. From the 1:200 scale aircraft models, we have again selected those for European installations that do not exceed a total length of 30 cm.

The offer of the Wings series covers several epochs, but has a more modern focal point:

KLM Cityhopper Fokker 70 – PH-KZE (Art.-Nr. 570640),
Air Baltic Airbus A220-300 – YL-CSJ “Estonia” (570657),
Königl. Niederländische Luftwaffe Lockheed Martin F-35A Lightning II – F-001 (570671),
Deutsche Luftwaffe Mikoyan MiG-29A “Fulcrum” – 29✖18 (570688),
Balair / Internationales Rotes Kreuz Transall C-160 – HB-ILN (570701),
Franz. Luftwaffe Airbus A400M “Atlas” – F-RBAL (570718) und
Sukhoi T-50 (SU-57) Prototyp – 510 blue (570732).

In the Snapfit series we have to report the Airbus A321LR with the identification 4X-AGH of the Arkia Israeli Airlines (612524). Due to its great success, the “BVB Team Airbus” A320 from Eurowings (611312) will be relaunched unchanged. Snapfit models are reproduced with the chassis retracted and in a simplified form compared to the Wings collection.

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German Magazine for Z Gauge



Large container traffic on the Bundesbahn

The big festival in Friedrichshafen
A little Christmas story

Introduction

Dear Readers,

With giant steps we are now approaching the Advent season and thus the peak of the model railway season. Many novelties are traditionally on sale at this time of year and promise excitement.

But for us it seems to become rather quiet this fall. Some of the models that were eagerly awaited have been postponed and do not seem to reach dealers until 2020. Surely we can cope with it, but with all possible understanding we can also express our displeasure, can't we?

Remember when we were kids, how we pressed our noses against shop windows? There were train layouts with shuttle train controls in every larger department store and at the model railway dealers, who were so commonplace at that time.

We also want to pass on a little of this feeling to the younger generation. That is why we always visit smaller and larger exhibitions and offer hands-on and join-in activities for the youngest. In this issue you will also rediscover that.

This brings us to the International Model Railroading Day 2019: On 2 December it's time again. And if it becomes a success, it can also be thanks to your creativity and energy!

An example of creative design is a winter layout presented today, small and fine, but with a special history. Alexander Mack has transformed memories into three dimensions, honours "heirlooms" and has invented on top of this an exciting Christmas story for his children.

And we know what we're talking about! Our reviewer is nine years old and was very fascinated. She reported excitedly and in detail about the book and only knew how to recommend it warmly. That's when we got bitten by the bug and we agree with this judgement.

At the same time, we look back on the anniversary of the trade fair in Friedrichshafen, where the Z-gauge systems were certainly among the best and most demanding. This superlative does not apply to the train models presented today, whose model history we likewise examine in detail.

In this way, however, we can help to classify the new models correctly, to demonstrate ideas and perspectives and to use the freshly delivered models credibly and successfully. They are compromises, but usable and useful.

With a view to the upcoming holidays, we also thought about the gifts that could be given to model railway enthusiasts and prototype friends. We have found two books that differ in price and quality and therefore might cover different tastes.

Finally, this edition is rounded off by a large number of reports. Enjoy the reading and the upcoming Advent season!

Sin-Z-erely,

Holger Späing



Holger Späing
Editor-in-chief

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We thank Eisenbahnstiftung for their photo support and Alexander M. Mack für his contribution.

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Cover photo:

V 60 107 has shunted a train of container cars to the loading track. Individual large containers with steel extension walls are now picked up by delivery vehicles and brought to their customers, mostly local coal traders without railway sidings.

From house to house on new paths

Many compromises and gaps

The year 2019, which is slowly coming to an end, is showing itself to be a slow one in terms of new wagon models based on new designs. One of the few exceptions is the container wagon BT 10 from Märklin, which brings at least one new container type with it. We take this as an opportunity to take a closer look at the models, to point out program gaps and to present an approach to a solution in the form of a self-modification.

Märklin would like to commemorate the great era of door-to-door traffic with its freight car packaging (Art. No. 82329), which is being discussed today. In keeping with the high point of this mode of transport, the models also carry revision data from 1959/60, thus, complementing the Insider 2010 annual car (80320) with a different company number.



The freight wagon package “From house to house” (Art. No. 82329) consists of two BT 10 three-wheeled carrying wagons with open containers Eosakrt 031 and a Kaelble delivery vehicle with another large container of the same design.

Similar to the prototype, where the undercarriages of war-damaged freight wagons of types G 10 and G(r) 20 were used for the conversion, Märklin also reused an existing chassis. Although this reduces the development and production costs, it also means some significant compromises, as we will see in a moment.

But today we only want to take a short look at it, because it has remained unchanged compared to the former annual car, which has already been discussed in more detail in **Trainini®** 5/2011. Again inscribed as BT 10, the only difference this time would be a missing steam heating pipe.

Our considerations have a decisive difference compared to the article eight and a half years ago: This time we use the model dimensions as a yardstick, which should apply according to the wagon labelling. Previously, we had used the ones that best matched the different dispenser trolleys.



As usual at Märklin, both cars have received different numbers than earlier editions. This time both models are marked as BT 10, i.e. as a car without steam heating pipe. The first model (picture above) has the company number 010 355, the second (picture below) the 010238. The mounted tanks are new because of the steel walls.

The compromises and limitations resulting from the incorrectly selected chassis are now very clear. The model is thus flawless in all relevant dimensions, which can be easily recognized even without remeasuring. The wagon is about 1.5 mm too narrow, and at the same time more than 4 mm too short. And so the wheelbase is almost 3 mm too small.

As a result, everything here seems extremely stocky. At the end of the vehicle, without the handbrake, there was no space left for the shunting handrails, which catches the eye of the observer. The thick metal

weight plate also distorts the picture, because in its place there are graceful rails, perceived as raised, into which the Pa containers have been inserted.

But they are indispensable; otherwise, the model would not be safe to operate. There is no (reasonably) suitable zinc die-cast chassis as an alternative in the existing moulds. And so, the basic chassis provides the reason why the two wagons should only be loaded and viewed from a slightly greater distance. If this puts their weaknesses into perspective, and they form only two units in a train, their individual issues are noticeably reduced.

On the other hand, the clean paint finish in original colours and the multi-colour pad printing of trolleys and containers are impeccable, with the latter again on all four sides. The two wagons of the type BT 10 have the serial numbers 010 355 and 010 238, the other labelling can only be deciphered with a magnifying glass.



The many compromises on the wagon become particularly clear at the hand-brakeless end: Here, two retaining brackets actually belong on the loading area, for which there was no room on the too short model. Even the substructure of the containers on the longer side does not exactly match the prototype.

As the table of dimensions shows, the three Pa containers, each with a different operating number, deviate from the original by half a millimetre to just under one millimetre. However, since the proportions are consistent, this hardly matters.

More importantly, the models were derived from the Eoskrt 022 Pa container, which was supplied with the 2010 annual car, in contrast to the prototype. This is relevant for the appearance of the side walls, which are characterised by two, instead of one profile, and do not have a design specific flap. The converted deviations from the original dimensions, on the other hand, are irrelevant, which is why Märklin probably made this compromise.

Data and dimensions of the carrying wagon BT 10 and container Eosakrt 031:

	<u>Prototype</u>	<u>1:220</u>	<u>Model</u>
Genus BT 10			
Length over buffers	9,800 mm*	44.5 mm	40.1 mm
Width	2,965 mm	13.5 mm	12.0 mm
Wheelbase	4,500 mm	20.5 mm	17.8 mm
Container Eosakrt 031			
Length	3,072 mm	14.0 mm	13.1 mm
Width	2,295 mm	10.4 mm	9.8 mm
Height	2,152 mm	9.8 mm	9.1 mm
Years of construction (tank)	1955/56		
Quantity procured	ca. 200		
Retirement (container)	ca. 1975		

* depending on the respective origin wagons of the types G 10 and G(r) 20 also 9,100 mm possible (models are labeled with 9.8 m length over buffers)

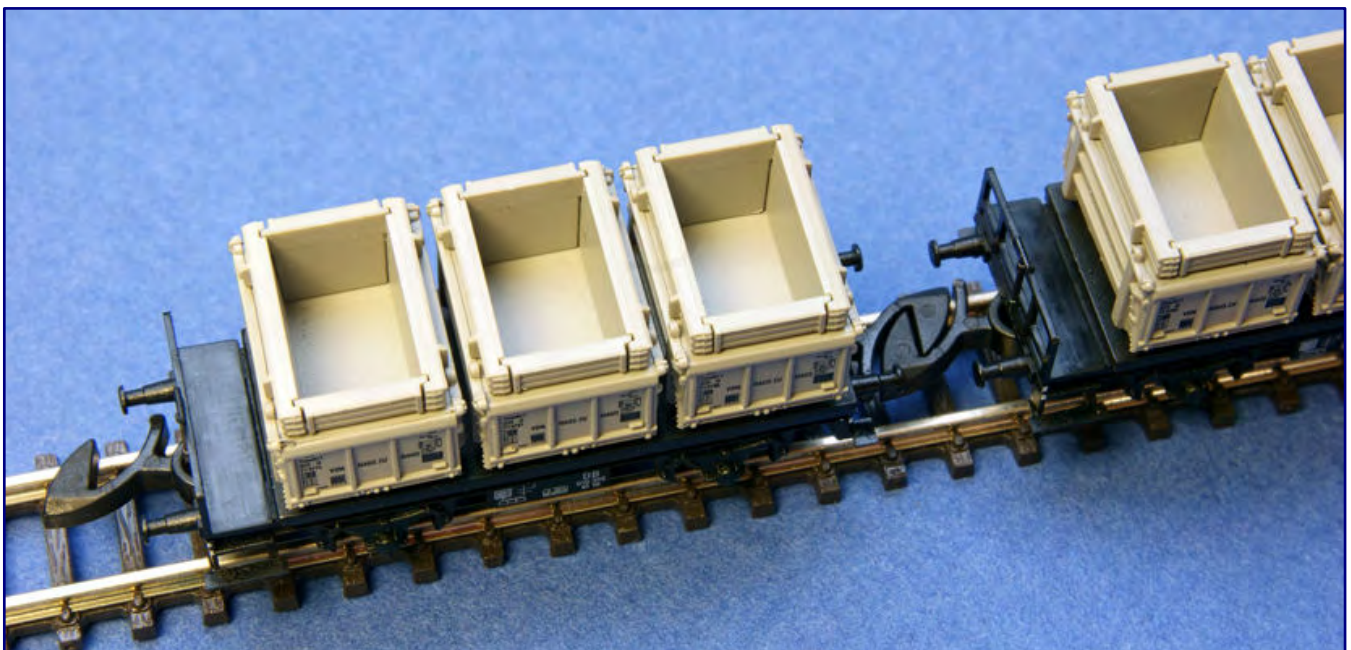
The model height of the steel extension walls was 384 mm for the model and was reproduced in a visually harmonious manner. If we look into the containers from above, we can see that these four walls are loosely inserted in the model.

Caution is therefore called for, because if they are removed or fall out, their reinsertion requires a lot of patience and calm fingers.

What we also notice when we look at them is the material thickness of the small walls, which is not exactly dainty as a result of plastic injection moulding.

Since the top walls extend down, they noticeably reduce the volume of the containers.

Our recommendation at this point is, therefore, to fill all seven centre containers found in the package with suitable cargo inserts. Spur Z Ladegut Josephine Küpper from Aachen reacted quickly, where our favourite coal (Z-82329-K), but also gravel (Z-82329-S) and broken bricks (Z-82329-S), are offered.

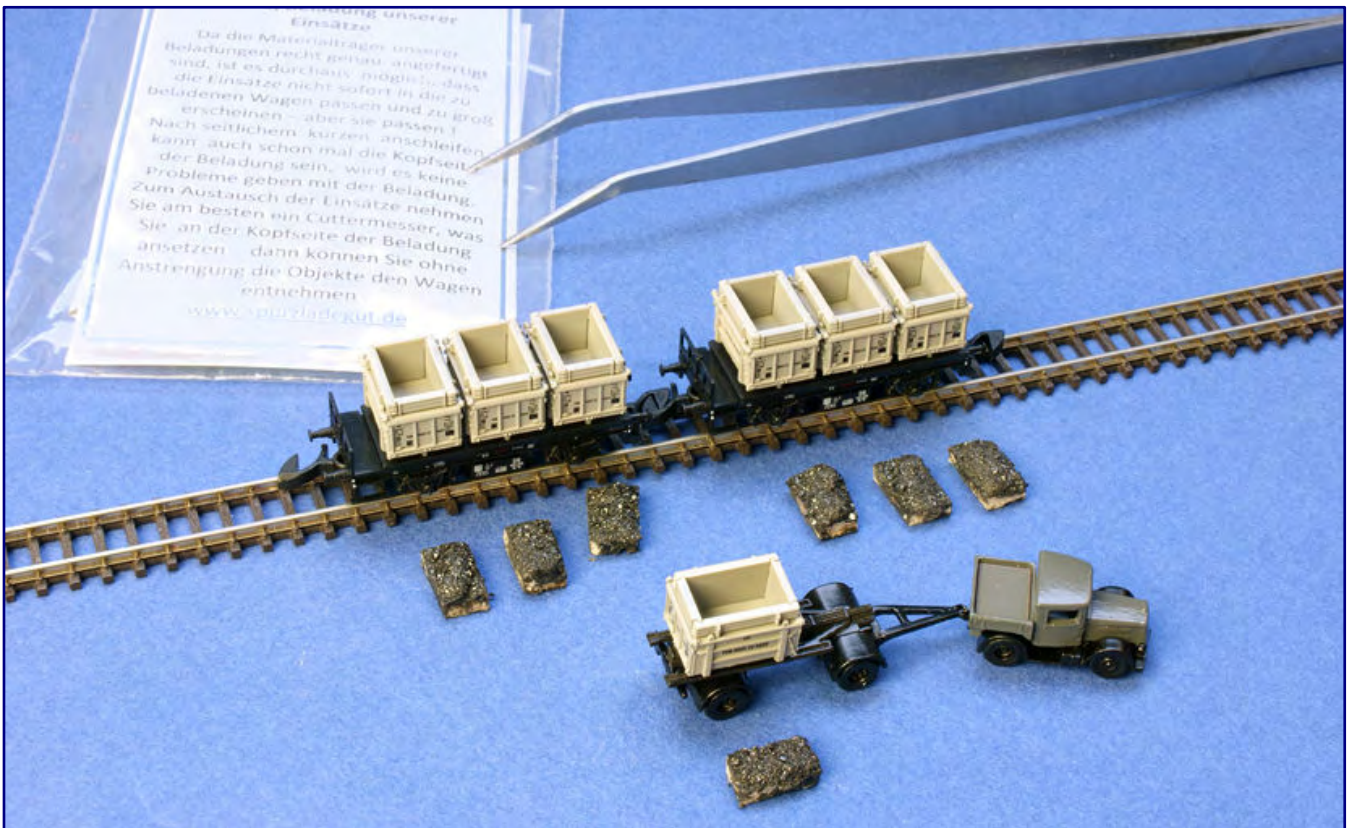
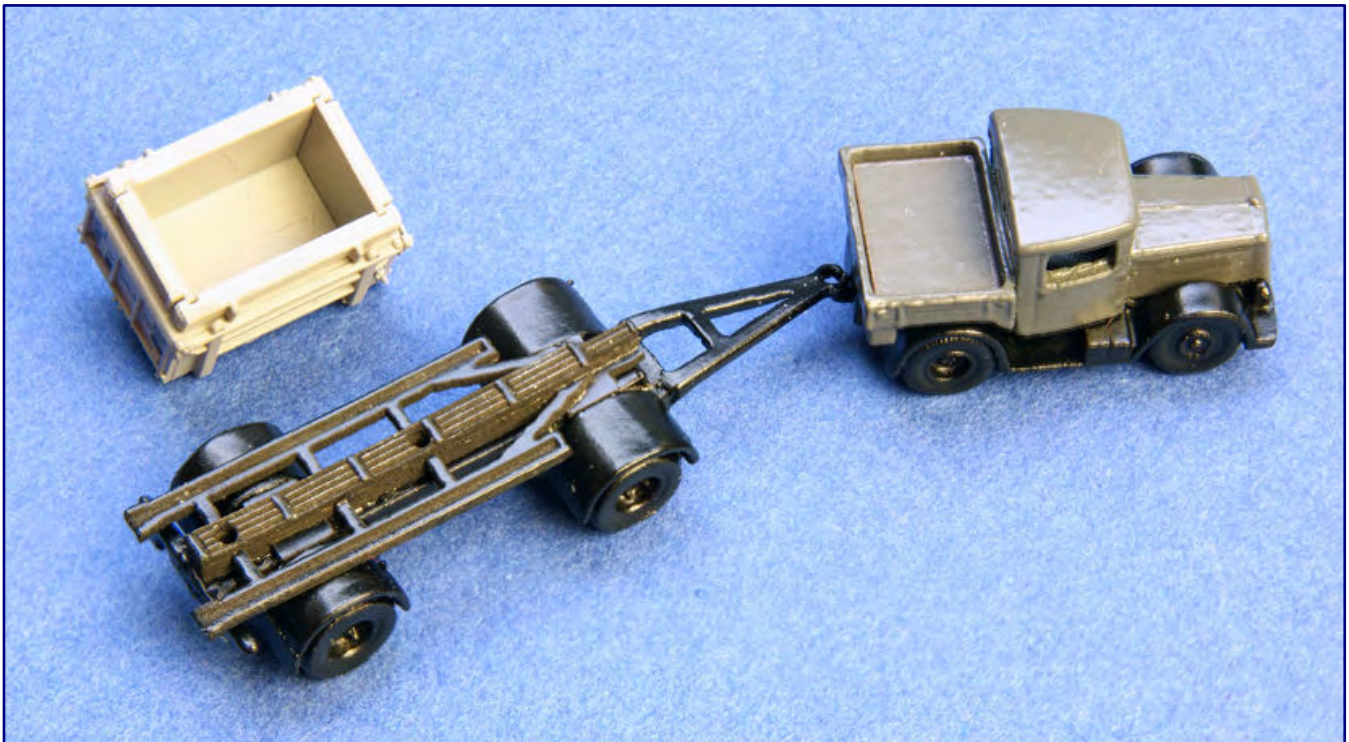


The view from above shows the attachment walls loosely pushed together from the four parts and the material thickness owed to the production process, which is somewhat annoying to the viewer.

The products are manufactured as usual from the respective real material and thus guarantee a realistic appearance. Since they are offered individually and not as a seven-piece package, they can also be mixed and can thus take up one of the original ideas of small quantities in door-to-door traffic.

If you are wondering about the number seven for two three-wheeled cars, please refer to the delivery vehicle enclosed with the car package at the end of this model discussion. The metal model is a black-

grey painted Kälble-Zugmaschine (Kaelble tractor) (without DB lettering) and a matching two-axe trailer on which a further Pa container Eosakrt 031 fits.



The trailer of the delivery vehicle has been designed according to Lienen (picture above), which differs fundamentally from the Ackermann vehicles. As suitable inserts Spur Z Ladegut Küpper already offers coal (Z-82329-K; picture below), gravel (Z-82329-S) and broken bricks (Z-82329-S). Seven inserts are required for this pack, each weighing less than 0.1 grams.



With Märklin's wagon pack and Küpper's cargo you can create outstanding loading scenes. Here, Pa containers with hard coal are driven to the nearby fuel dealer.

They take up road transport with vehicles that were once developed by the company von Lienen as an alternative to the well-known Ackermann trailers. Only then is the unaccompanied, intermodal transport really round, and virtually complete in its model.

Closing the gap in self-modification

It was not enough for our reader Dirk Rohwerder to be able to use only halfway scale tank wagons from the DB conversion programme. For this reason, he quickly started to modify the BT(r) 10 type to replace



The five-position Lgjs 598 was too modern for Dirk Rohwerder and so this model served as a donor car for a conversion project to the once widespread BTms 55 with four stands.

the weight plate in the delivery form with imitations of the model rails. A particular problem here is to keep the wagon sufficiently heavy to avoid any risk of derailment.

More spectacular, however, is his somewhat larger modification project, which is based on the Insider 2002 annual car. This five-passenger Lgjs 598 with Königsbacher beer containers does not fit into his favourite time as an Era III fan, and therefore seemed dispensable.

He removed the Pa containers and roughly dismantled this wagon into its most important components: Cast chassis with unmixed coupling shafts and the two injection-moulded parts with the axle bearings.



This photo of the dismantled trolley shows the cuts and milled parts required to modify the car into the missing desired model. A newly manufactured brass plate holds the chassis halves together in the future. Photo: Dirk Rohwerder

It was now possible to work on the chassis with a saw, file and milling cutters. And so the outer beams were first cut off in such a way that the guide rails reproduced above were not damaged. Then the running gear could be shortened to the length of a BTms 55.

The model for its conversion had a buffer length of exactly 11,000 mm with an axle base of 7,000 mm (Lgjs 598: LüP 14,800 mm / axle base 8,000 mm). The model developed by SEAG in 1955 was by far the most frequently represented type among all tank wagons, and 2,438 units were produced.

In order to give the shortened chassis sufficient stability, Dirk Rohwerder milled an attachment from a brass plate, which was glued to the bottom middle (from below) as an additional weight and additionally connected both halves. At the same time, it also offers mounting and contact edges for the long beams to be reattached to the outer sides.

After the axle bearings had been reattached and the colours had been repainted, the four-wheel car was ready. Only new labelling matching the original was still missing, but with a view to its tiny size it is also quite dispensable.



The converted wagon with exchanged large containers and self-made stage railing is not recognizable: as BTms 55 of the first lot, it fills a large gap in the Z gauge model range and cuts an excellent figure. Photo: Dirk Rohwerder

The beer containers Dkr/Ddikr were not reused on this model. Instead, four open containers were used, as they were found for the first time on the Insider 2010 (80320), in order to be used together with the modified model.

Concluding observations

Door-to-door transport with large containers was a popular and widespread form of transport in the early years of the Deutsche Bundesbahn (German Federal Railway). Open wagons still accounted for the highest quantities in the DB stock lists, which also led to high quantities of open Pa containers. So even on the model railway in epoch III it is not possible to do without them.

Märklin's collection of moulds still includes the aforementioned five-passenger car of the type Lgjs 598 with metal chassis, which is much closer to the model, but had its most important uses in ISO container traffic because the Pa containers had long since been withdrawn when it appeared.

Thus, for the heyday of this form of transport, only the wagon packaging considered today remains, which, however, demands great willingness to compromise on the part of the customer. However, in view of the rather small numbers of the BT 10 as a prototype model, this is acceptable, for the time being.

The topic “from house to house” is still considerable and can be expanded further: Many still unproduced container types are waiting to be reproduced, including the closed box types or those for compressed air discharge for cement. They were still on the road for a long time when general cargo was almost exclusively transported in ISO containers.



Here we have once again staged the new wagon pack (82329) with other Märklin models, including the Insider 2010 annual wagon (80320), and filled with Küpper loads: The topic “from house to house” now has a certain variety to offer, but with more containers and, above all, accurate wagon models from the time of this mode of transport, it can still be expanded considerably.

Other types of car are then also required. And here we mean that development costs should rather flow into the once so ubiquitous and widespread BTms 55 with four stands than into a full-scale BT 10.

The conversion to your own model cannot be a permanent solution and is also not suitable for every model railway enthusiast. As you can see in our rebuild example, the Zetties should expect an almost perfect result when using a metal undercarriage, similar to the Lgjs 598. In this respect, we make a clear appeal at this point to Göppingen.

Manufacturer webpages and references of the model:

<https://www.maerklin.de>
<https://spur-z-ladegut.de>

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Significant development spurt from 1946 Large container traffic at the DB

The use of large containers on rail did not really gain momentum until after 1945. At the same time, special freight wagons were developed for the first time to enable fast and easy reloading. Today, we look at these forerunners of container transport, follow their developmental history, and the operational peak of their career.

Although container traffic did not begin until after the war, its origins go back much further. The first known container wagon in Germany dates back to 1924 and was presented at the railway technical exhibition in Seddin.

This “tank car with removable tank containers” built by Krupp ran for the dairy Bolle A.G. from Berlin and were used to transport coconut fat. It was designed as a flat wagon with four rolling boilers which could be moved onto the trailer of a truck for road transport.



The basic idea of Pa container traffic could probably not be illustrated more appropriately in one picture. The central containers of Trilux, the light source manufacturer from Sauerland, have arrived by rail in Neheim-Hüsten. There they will now change to the Ackermann trucks at the loading dock

It was and is essential for the unaccompanied intermodal transport justified by this that only containers without motor vehicles are loaded. The Deutsche Reichsbahn-Gesellschaft intensified its own truck traffic in the thirties of the last century. In contrast to other countries, however, it initially concentrated on transporting smaller quantities of goods instead of large containers.

Containers of up to 20 m³ in size only appeared in Germany in the mid-1930s. At that time, they were transported on converted freight wagon bogies of various types. From this time on, various standardized small and large containers were in use, in which liquid or solid goods were transported. These pre-war containers finally disappeared from the inventory records by 1967/68.

The idea received a new impetus immediately after the end of the war. As far as the large containers were concerned, however, it was necessary to decide between three procedures practised in neighbouring countries: In Great Britain, detachable and stackable large containers that could not be rolled were in use. France preferred containers that could be driven on the road. The detachable and rollable large containers with 3 to 6 m³ capacity that were common in the Netherlands were tried and tested.

In 1949, the decision was therefore taken by the Bundesbahn to adopt the Dutch container system and test it themselves. It was also to assert itself in the young Federal Republic and became known under the catchy brand name "from house to house."

According to international agreements, the large containers were called 'pa,' an abbreviation derived from French ('porteur aménager'). They enjoyed such rapidly growing popularity and distribution that further basic designs became necessary.



The container wagon BT 10 010 138 marks the new beginning and the expansion of door-to-door traffic driven by the German Federal Railway, based on the model from the Netherlands. The Ekrt containers loaded in August 1960 were built in this form in 1950 and belong to the original types of this system. When this photograph was taken in Braunschweig Rbf, the lashing of the Pa containers by eyelets on the wagon frame was no longer common practice. The advent of special containers meant that this form of transport securing was no longer possible. Photo: Reinhard Todt, Eisenbahnstiftung

In addition to the open and closed containers of the first hour, there were also special types for liquids, powdery and moisture-sensitive bulk materials, foodstuffs and even frozen goods as well as cement containers to be unloaded by compressed air.

The number of such containers also grew rapidly. As early as 1955, DB owned more than 6,000 Pa containers of various types. Container traffic reached its peak in the sixties with around 25,000 units and around 5,700 large container transport wagons. On the way they were partly in a specially set up city express service.

The system passed its zenith when, in 1966, competition came to Germany for the first time with the standardised ISO overseas containers, which were able to quickly establish themselves. Now the large containers were suddenly small and had to be satisfied with the designation "Mittelcontainer" (middle containers, abbreviated to MC). As a result, they were hardly procured until the last one was finally added to the stock in 1974.

By further specializing in various container types, however, it was possible to compensate for the shrinking importance of door-to-door traffic for several years to come. The total number of Pa containers shrank slowly but steadily, while the number of types increased.



The Bundesbahn advertised its "house to house service" in a targeted manner and gladly demonstrated it on a wide variety of occasions, such as here on 7 September 1951, in order to win new customers for it. It wanted to counter a dwindling volume of freight traffic by rail, which she succeeded in doing well for almost two decades. Photo: Willi Marotz, Eisenbahnstiftung

In 1983 there were still about 10,000 MC in the inventory, with over 33 types. In all the years of their use, they bore their own generic markings, which indicated their functions and technical characteristics.

The open containers of the genus Eosakrt 031 recently picked up by Märklin belong to the basic types with which the system once started, but they are not the original basic form. It has been slightly modified over several years and the external dimensions have also changed slightly.

The concrete model corresponded to the Eoskrt 021, whose capacity could be increased to 9.6 m³ by 384 mm high steel extension walls. Around 200 copies of this design were use on the rails in Germany from 1955/56 onwards. Between 1953 and 1965, almost 100 similar containers Eosakrt 032 were procured, whose support walls were made of laths. 1975 both types can be proved for the last time in the DB stock.

continues on page 19



The comparison of the long side of this Pa container Eoskrt 021 with the Märklin model on page 6 reveals the differences in construction (top photo). The Eikrt freezer containers photographed in Braunschweig in September 1964 are among the first special types. Photos: Slg. Eisenbahnstiftung (top) / Reinhard Todt, Eisenbahnstiftung (bottom)



The cement tanks on a BTms 55 in Letmathe (March 1965; photo top) and the spherical tanks for compressed air discharge Ddzkr 551 (1961; photo bottom) are further examples of the many special tanks. Photos: Reinhard Todt, Railway Foundation (top) / Helmut Först, Eisenbahnstiftung (bottom)

Development of the carrying wagons

While the DR's large containers were all rebuilt in the Bi-Zone, the first carrying wagons were built in 1948/49 on chassis of older types from the damaged wagon stock of the war. It was not until around 1950 that the company switched to new construction. Although they were not part of the Reichsbahn stock in their form for container transport, they were still assigned to the Offenbach district until 1951.

Common to all wagons used in inland transport for Pa containers is the fact that they are flat wagons with support rails placed transversely to the direction of travel or integrated in the underframe. The MCs were set down and secured on rails with a track width of 1,400 mm. Their height above the upper edge of the rail was uniformly 1.185 mm.



The container wagons BT 10 were created by conversion on old chassis of covered wagons of the types G 10 and G(r) 20. Two such donor wagons of the first type are located in Kassel in March 1961 next to and behind the wagon with the serial number 010 235. Photo: Reinhard Todt, Eisenbahnstiftung

They were secured for the journey with lashing spindles, wheel chocks and initially also diagonal braces, which were given up without replacement with the appearance of the first special containers. The lashing spindles were attached to brackets in such a way that they could be tightened with nuts as soon as they were hooked into the eyelets of a large container.

Container wagons had a normal underframe, running gear with two axles without exception, common pulling and pushing devices with a continuous pull rod and brake equipment. Some of them were fitted with a handbrake, which in the oldest designs was initially installed in a metal brake house.

The first large container wagons were the BT 10, built in 1948/49 on the two-axle undercarriages of the covered freight wagons G 10 and G(r) 20. Parallel to this, the BT 30 type was developed on the undercarriages of covered Gms 30 "Oppeln" and open Omm 34 "Klagenfurt". Both types were designed for three Pa containers.



Photo top:

The tank wagons BT(hs) 30 were also built by conversions and were based on chassis of the "Oppeln" or open wagons of the type "Klagenfurt". The car with the company number 010 297 was on display on 1 July 1962 in Holzminden with open centre containers of the original design. Photo: Reinhard Todt, Eisenbahnstiftung

Photo bottom:

The container wagon BTs 50, here with the open centre container Edkrt 72, was one of DB's first new developments after the war. Here, too, the first cars still had metal brake houses on the handbrake platform (1952). Photo: Bustorff, Eisenbahnstiftung

The first new construction of the Bundesbahn was the BT 50, introduced by SEAG in 1950 and built in series from 1952 to 1954. Its 1,873 units could also accommodate three containers, but belonged to the fast runners due to their more modern running gears with double-shock hangers and brakes.

For tariff reasons, they were soon coupled in pairs and, referred to as BTmms 51, treated like a four-axle wagon for six containers. The background was that the permissible weight limits could not be used for many goods such as malt or oats. In the structure of the valid freight rates, cost advantages arose for customers through the combination into fixed (two-wagon) units.



Photo above:

The BTmms 51 photographed in Neuhaus in March 1962 with the company number 013 691 does not belong to any independent type, but is only one half of two BTs 50 coupled for tariff reasons. As here, they often travelled with bulk containers Efrk, whose loads could not usually make use of the permissible weight limits. Photo: Reinhard Todt, Eisenbahnstiftung

Photo below:

The Rmmso / BTmms 33 was only a makeshift on the basis of stanchion cars, whose side walls were partially dismantled like this one. Märklin would have a suitable basic shape in the Z-gauge portfolio. (July 1960). Photo: Reinhard Todt, Eisenbahnstiftung

However, the 150 cars of the BTms 33 type, which were converted from Rmms 33 stake cars to multi-purpose cars, were only a makeshift. In some cases they ran without stanchions and side kerbs, otherwise they had to be folded down and the stanchions stowed in the pockets on the undercarriage.

With this makeshift design, the mounting rails were recessed into the floor and welded to the long beams. They could be covered when not in use, the covers formed loading thresholds when opened, and when rails or profiles were to be transported, the lashing spindles were sunk into the ground when the wagon was not running in container traffic.

The transition to four-wheeled cars took place in 1955 with the BTms 55 type, which had also been developed by SEAG. Compared to the BTs 50, its loading weight could be significantly increased to 26.5 tons by using high-quality steel St 52, while its own weight increased only slightly.



The tank wagon BTms 55, built in series from 1955, here the copy with the company number 016 314, marks the transition to the four-wheeled wagon. In January 1958 it was captured in the picture with Eoskrt 21 containers, which, raised with steel extension walls, would have been the correct model for Märklin's latest model. Photo: Reinhard Todt, Eisenbahnstiftung

From the second series onwards, the cars were fitted with additional treads on the supports to make it easier to climb over at the front. Some of the 2,438 cars of the most common type among all carrying wagons were equipped with heating cables and could be fitted into passenger trains.

Five years later, the Bundesbahn finally switched to the five-column car, which formed the final stage in the development of door-to-door traffic. After two prototypes built in 1958, BTmms 58 (Lbms 589) went into series production in 1960.

By 1966, 2,100 units had been built. From 1964 they received reinforcements on the long beams and 59 t high performance buffers in the current series, existing cars were adapted 1963/64 accordingly in the context of special work.

This type is not only important in terms of numbers, but also represented the standard wagon for the Pa container until the end. Between 1963 and 1965, the first tests were carried out with wagons equipped with shock absorbers for bumper traffic.



The five-passenger BTmms 58 carrying wagon marks the final stage of development for door-to-door traffic and represents the wagons of similar design built up to 1971. It was built in series from 1960. The open (2nd from left) and closed containers (far right) were the two original types, whereby the open already belongs to a further developed design (model for Märklin). Deep-freeze containers (far left) and those for liquids (centre) as well as moisture-sensitive fine bulk goods (to the right) are the first special types. Photo: Bustorff, Eisenbahnstiftung

DB's latest developments benefited from this experience in the form of the Lbgjs 598, suitable for shock-sensitive goods, which was built between 1966 and 1971 in 530 series cars plus a prototype.

After the ISO large container was introduced, DB urgently needed wagons for its transport. In order to be able to use them to full capacity for the time being, however, they were also to be able to transport centre containers. This new development was equipped with so-called quick-release locks for five containers and additional locating pins for 40-foot containers. From 1983 they became pure container wagons with the expansion of the large container facilities.

DB was also forward-looking when designing the BTmms 59, later referred to as Lbms 591. These specialists, procured from 1963 in a number of 180 cars, were based on the BTmms 58 / Lbms 589 and, in addition to container traffic, were also suitable for the new truck swap bodies, which first appeared in the early 1960s.

In addition to five MCs, they could also accommodate one swap body from the Ulm system or two from the Wuppertal system. They remained in use until the mid-1980s, when they were virtually a door opener for another successor system.

We did not consider here experimental designs and prototypes that did not achieve any operational significance, including a three-pedestal car from 1957 that was suitable for ferry traffic with Great Britain. However, we would like to take a look at the delivery vehicles that took over the containers from the carrying wagons.

Delivery to the customer

Pa containers were delivered to customers either by rail, if there was a siding, or individually using special delivery vehicles procured by the Bundesbahn. Special tilting devices, stands (for cement containers) and lifting rollers were also created to steer remote units into factory premises.

The special vehicles for the road were available in many different forms, which also underwent constant further development. The most modern among them were semi-trailer tractors. Some of these were also able to transport two large containers at the same time.



The simple semi-trailer shown here (German Federal Patent DE814275B), which accommodates a closed Ekrt container (built 1950) in 1951, was developed by the Bochum bus, bus trailer and vehicle manufacturer von Lienen and deviates considerably from the widespread Ackermann solution. Photo: Below, Eisenbahnstiftung

The containers were transferred between wagon and truck via two articulated rails over which the MC could be rolled. An important basis was therefore the uniform height of the rails on the carrying wagons.

Two fundamentally different systems were used. With the design of rails and also the trailers of the AG-Weser, the vehicles were shunted at right angles to the wagon, which required a high degree of precision and also required a lot of space on the loading road. In most cases, the containers could be lifted slightly on one side and thus better guided.

The better-known Ackermann type worked with special semi-trailers whose body could be rotated. Here the vehicles drove parallel to the wagons and then swivelled their attachment towards the wagon to be able to reload the container more easily.



In August 1961, the loading of an Etkr container for fine bulk material by an Ackermann delivery vehicle was recorded in a sequence of pictures at the Koblenz-Ehrenbreitstein railway station. The vehicle with the registration number DB 49-596 drove parallel to the train on the loading road and swivelled the trailer 90 degrees over to the container wagon BTs 50. The Pa container is then rolled on rails onto the trailer and lashed there (top photo). Once it has been fixed securely for transport, the trailer swivels in again (bottom photo) and assumes its travel position. Photos: Slg. Eisenbahnstiftung

In general, both types could not be used elsewhere, because of their special features, and had only a low load factor. This finally also led to the fact that the Bundesbahn moved away from door-to-door traffic due to the constant decrease of the transported quantities and cost-intensive special containers.



This semi-trailer, like the one on the photo on page 22, comes from the Bochum bus, bus trailer and vehicle manufacturer von Lienen. The photograph taken in Bochum in 1961 shows us how this solution required the vehicle to approach the train and the Pa containers to be transferred. Photo: Barnarius, Eisenbahnstiftung

After years of no investments, no new customers were added and existing customers increasingly turned their backs on DB, which, at the end of the 1980s, stopped delivering its own vehicles and sold them or phased them out. The few remaining Pa containers were then only transported privately.

Suggestion to the topic (film preview of a DVD):
<http://www.historischer-filmservice.de/media/video/293.mp4>

Winter scenery for a Christmas story

Exciting adventure on a scale of 1:220

One day, whilst looking at his 34 x 47 cm winter layout, long-time Mini-Club enthusiast A. Matthias Mack had the idea to turn it into a story for his two children. The result is an exciting Christmas adventure for young and old which allows us to complement our "layout diversity" focus theme this year with a description of this little layout and the corresponding paperback book.

By Alexander Matthias Mack. My enthusiasm for model railroads goes back to my infancy. I inherited it from my father Günter Mack who was born in 1940 and had already had a Märklin model railway since his childhood. This was an absolute rarity during the post-war period and something which my grandfather Richard could only obtain through certain connections.

Many years later, after founding a family and building a house (obligatory for any self-deserving Swabian), my father took this H0-layout into his new home in 1972 and supplemented it with a marshalling yard with sidings and turning facilities.



When I myself was about thirteen years old, my father decided that his layout would have to be completely rebuilt and modernised. This was pretty much at the time when Märklin introduced its digital system. And



since my father was technically adept and had business dealings with electronic controls, he decided to bring the layout up to the state of the art.

So we dismantled the old M-tracks and I designed a track plan (at that time of course still with stencil and pencil). When the new K-tracks were finally laid, the overhead lines plugged in and the points connected, the Mack family became probably one of the first owners of a Märklin digital layout.

During the following years, our collection of rolling stock was expanded by various top-of-the-line models of contemporary model railway production. We certainly must have been amongst the first to run an Intercity Experimental on their layout, and, a few months later, a Württemberg Class C locomotive, also called the "schöne Württembergerin" or "the beautiful Württemberg lass".

But now to the Z-gauge layout, which I want to present here: Richard Mack, my grandfather, wanted to have again a model railway of his own, after his son Günter had taken the trains with him after moving out.

This was still at the beginning of the Seventies and the dealer recommended him to go with the then brand new 1:220 Z-gauge. My grandfather therefore bought a starter kit with a class 216 diesel locomotive and the so-called Pop (Art coloured) cars. Soon, he also added an Intercity towed by a class 103 and a small class 24 tender steam locomotive.



We grandchildren always played with this Mini-Club railway, which my father Günter assembled into a small and simple layout, when we visited our grandparents. When my grandma then moved house at the beginning of the Nineties, she did not want to take the layout with her, but passed it on to me.

The layout essentially consisted of only two simple circles and I had just started designing a new track plan (still with a stencil), when my brother-in-law came by with a gigantic cardboard box and the news

The book about the layout:

The book "Rauhnacht, Weihnacht, stille Nacht" Twelfth Night, Christmas night, Holy Night) by A. Matthias Mack is available in bookshops under ISBN 978-3-86870-848-6 for 8.95 Euro (fixed book price and in German only).

Our nine-year-old reviewer praised the title as a good read and found the story around the mystic key figure "Witch Astrafania" exciting and full of suspense. In her view the book is interesting for both girls and boys, even those older than herself.

To that we can add that the book also makes for a good bedtime story and will be also entertaining for adult readers.



that his neighbour had just broken up his Z-gauge layout. As he could not get the buildings sold, he wanted to throw them away.

This came at exactly the right time for me and felt like Christmas and Easter on the same day: The box contained almost the complete Z-gauge collection from the Kibri catalogue, fully assembled and hardly damaged! I only had to redesign the layout once more to integrate some of the new structures.

In the following years, I gradually added to my fleet, and the layout had to be expanded again and again to create sufficient space for all my trains. Meanwhile it consists of four independent tracks, three of them



with overhead lines. One of the tracks is equipped with an automatic train change system in which two trains, controlled by switching tracks, operate in an alternating fashion.

My long-cherished dream, however, was to have a model railway winter layout. I finally realized this dream on my 40th birthday. Due to lack of time I bought a small pre-fabricated layout from Noch, and adapted and decorated it according to my needs, for example by including two alpine houses still left over from the aforementioned box.

While looking at this small, Christmas landscape, I had the idea to write a story about it for my two children, which would be set in the small mountain village of our Z-scale winter railway. After almost three years, I was able to publish my book "Rahnnacht, Weihnacht, stille Nacht" (Twelfth Night, Christmas night, Holy Night) in October 2015 (ISBN 978-3-86870-848-6).

The book, in which a class 798 rail bus plays an important role, is about two boys whose village is cut off from the outside world by an avalanche at the end of the 1970s, shortly before Christmas.



In the following “Twelfth Night” period around Christmas, extremely mysterious things suddenly happen which pose dangerous riddles for the two boys in the last days of Advent.

But I do not want to reveal more at this point - except: The whole thing finally finds a very Christmassy and happy end, of course with a model railway placed under the Christmas tree for the main character of the story.

Have fun reading and Merry Christmas!

Book mentioned in text available for order at:
<https://www.rediroma-verlag.de/buecher/978-3-86870-848-6>

All photos: Alexander Matthias Mack

Grenzenloser Modellbahnspaß in 1:220

**8. Internationales
Spur-Z-Weekend**



ALTENBEKEN

28. und 29. März 2020

Samstag: 10.00 - 18.00 Uhr

Sonntag: 11.00 - 17.00 Uhr

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www.z-freunde-international.de

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Eisenbahnpionier im Ruhrgebiet **Die Köln-Mindener Eisenbahn**

Seit 140 Jahren ist die Köln-Mindener Eisenbahn (KME) Vergangenheit, und doch ist sie den Menschen vor allem im Ruhrgebiet in guter Erinnerung geblieben. Sie hinterließ wichtige Verkehrsadern, die auch heute noch Bestand haben. Ein gelungener Band aus dem Sutton-Verlag hält Erinnerungen wach.

Bernd Franco Hoffmann
Die Köln-Mindener Eisenbahn
Schienenwege durch Rheinland und Ruhrgebiet

Sutton Verlag GmbH
Erfurt 2019

Gebundenes Buch
Format 17,0 x 24,0 cm
160 Seiten mit ca. 130 teilweise farbigen Fotos und Abbildungen

ISBN 978-3-95400-972-5
Preis 24,99 EUR (Deutschland)

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oder im Fach- und Buchhandel

Autor Bernd Franco Hoffmann ist Journalist, der für verschiedene Tageszeitungen und Fernsehsender gearbeitet hat. Literarisch hat er durch mehrere Sachbücher im Bereich Sporthistorie, Verkehrs- und Regionalgeschichte sowie Reiseführer auf sich aufmerksam gemacht.

Dieses Mal präsentiert er uns eine kurzweilige Reise in die bewegte Geschichte der Köln-Mindener Eisenbahn. Bereits 1843 gegründet, zählte sie zu den Pionieren des deutschen Eisenbahnwesens. Im Westen Deutschlands gehen auch wesentliche Teile des heutigen Streckennetzes auf diese Gesellschaft zurück, die 1879 verstaatlicht und zum 1. Januar 1886 endgültig aufgelöst wurde, um in die preußische Staatsbahn überführt zu werden.

Nachdem wir uns bereits mit einem Werk zu ihrer wichtigen Konkurrentin BME (Bergisch-Märkische Eisenbahn) auseinandergesetzt haben, die wegen Stilllegen vieler ihrer einstigen Strecken teilweise in Vergessenheit geraten ist, lag das Würdigen des hier besprochenen Titels eigentlich nahe. In Dortmund und dem angrenzenden Ruhrgebiet standen die beiden Bahnen einst in einem harten Wettbewerb.

Besonders für Dortmund war dies eine schicksalhafte Begegnung, denn ihre Strecken trafen sich dort, wo sich heute der Hauptbahnhof befindet. Die Bahnen sorgten für Aufschwung und Industrialisierung, die nach dem Ende des dreißigjährigen Krieges längst vergessene Hansestadt blühte, wie auch Oberhausen, zu einer Metropole auf.

Die KME schuf aber weitaus mehr als die namensgebende Abfuhr- und Handelsstrecke zwischen den wichtigen Häfen von Minden an der Weser und Köln am Rhein über das aufstrebende Ruhrgebiet. Den



Grundstein zum Unternehmen und erfolgreichen Strecken hatten Visionäre wie Friedrich Harkort und David Hansemann gelegt, die besonders im Großraum Dortmund bis heute unvergessen geblieben sind.

Noch heute sind Personen- und Güterzüge zwischen Köln und Hamburg, Arnheim und Gießen oder Dortmund und Duisburg auf Trassen unterwegs, die auf dieses Verkehrsunternehmen zurückgehen. Sein bei der staatlichen Übernahme rund 1.100 km Streckenlänge umfassendes Netz verband das Rheinland, das Ruhrgebiet, Ostwestfalen, den wichtigsten Elbe-Hafen, die Niederlande und Ostfriesland.

Diese Geschichte wird im Buch anschaulich, überraschend ausführlich und spannend erzählt. Geschichte muss kein trockenes Thema sein, wie hier anschaulich bewiesen wird. Gerade das Konkurrenzverhalten zu anderen Bahnen, vor allem der BME, verspricht spannende Episoden, die in dieser Tiefe doch ungewöhnlich für Sutton-Zeitreise-Bände sind.

Dennoch wird sich niemand davon abgeschreckt fühlen, weil das Erbe der KME so packend und hinreißend präsentiert wird. Für Eisenbahnfreunde ist es eine Fundgrube auch wegen des historischen Bildbestands, der enthalten ist.

Rund 130 teilweise zuvor unveröffentlichte Bilder dokumentieren die eingesetzten Fahrzeuge, die Entwicklung von Strecken und die wichtigen Bahnhöfe. Der Bildband wird dazu zum Pflichtprogramm nicht nur für alle, die sich für die Entwicklung des Eisenbahnverkehrs in Deutschland interessieren. Wer an der regionalen Geschichte des großen Einzugsbereichs der KME interessiert ist, kommt eh nicht an diesem Titel vorbei.

Für Modellbahner aller Spurweiten bietet sich hier wieder ein authentisches Objekt zum Studium zeitgenössischer Zugbilder und des Lebens im Bahnhofs- und Streckenumfeld. Dies ist für Puristen, die eine stimmige Modellumsetzung abliefern wollen, sicher unverzichtbar. Wer kann sich im Rückblick – selbst bei eigenem Erleben – heute noch so gut und vor allem unverfälscht erinnern?

Lobenswert sind in diesem Zusammenhang auch die gelungene und tadellose Bildwiedergabe der Farb- und historischen Schwarz-Weiß-Aufnahmen. Doch bevor es so weit ist, steht ja auch eine Bildauswahl an, denn es gilt, die textlichen Ausführungen gelungen und treffsicher zu dokumentieren und zu unterstreichen.

Auch dies hat ohne jeden Tadel oder Anlass zur Kritik geklappt. Unter Berücksichtigen eines Rückblicks auf einen Zeitraum von immerhin locker 175 Jahren ist auch das keine Selbstverständlichkeit. So wird dieser Titel zu einer von zwei Buchempfehlungen für das bald anstehende Weihnachtsfest und erfährt auch eine Nominierung für die Neuerscheinungen des Jahres 2019 in der Kategorie Literatur.

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Serienmaschinen der BR 218 Eine vollendete Trilogie

In zwei Vorgängerbänden wurde sehr detailliert und gelungen die Entwicklungsgeschichte der V 160 aus dem ersten Typenprogramm der Deutschen Bundesbahn be- und über die späteren Baureihen 210, 215, 217, 218⁰ und 219 fortgeschrieben. Die Serienmaschinen der Baureihe 218 bildeten den krönenden Abschluss einer fast zwanzig Jahre währenden Entwicklung. Sie sind Thema des dritten und letzten Bands aus der Reihe von Baureihenportraits im EK-Verlag.

Josef Högemann / Roland Hertwig / Peter Große
Die V 160-Familie
Band 3: Die Serienlokomotiven der Baureihe 218

EK-Verlag GmbH
Freiburg 2019

Gebundenes Buch
Format 21,0 x 29,7 cm
424 Seiten mit 642 teilweise farbigen Abbildungen

ISBN 978-3-8446-6014-2
Art.-Nr. 6014
Preis 49,90 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Die Diesellokomotiven der Baureihe 218 stellen den technischen Höhe- und auch Schlusspunkt einer überaus erfolgreichen Familie dar, die von der Deutschen Bundesbahn begründet worden war. Und auch nach gut vier Einsatzjahrzehnten sind sie bei der Deutschen Bahn AG noch unverzichtbar, auch wenn sich ihre Reihen inzwischen deutlich gelichtet haben.

So wurde es Zeit, dieser erfolgreichsten und am längsten beschafften Lokomotive aus der V-160-Familie ein literarisches Denkmal zu setzen, ihre Entwicklungs- und Beschaffungsgeschichte zu analysieren und niederzuschreiben.

Dafür verantwortlich zeichnen wieder drei bekannte Autoren, die sich auch mit vielen anderen, in diesem Verlag erschienenen Titeln einen Namen gemacht haben. Das Trio führt diese Trilogie also gekonnt und punktgenau über die Zielgerade.

Blicken wir zurück, wie eng getaktet die drei Bücher über die bis heute wichtigste Baureihenfamilie unter den Verbrennungsmotoren sind, dann staunen wir, wie sorgfältig und ohne Hektik dies vonstattengegangen zu sein scheint.

Verblüfft stellen wir beispielsweise fest, wie akribisch genau die Dachausrüstung der Baureihe 218 mit unterschiedlichen Anordnungen von Kuckuckslüftern, Kühlerformen, Lüftungs- sowie Auspufföffnungen und Hutzen recherchiert und festgehalten wurde. Selbst nach Abschluss der Arbeiten fanden sie noch temporäre Sonderformen, die in einen Nachtrag aufgenommen wurden.

Unterstützt von zahlreichen DB-Dienststellen und vielen Eisenbahnfreunden haben die drei Autoren so viele Informationen zusammengetragen, dass nicht nur ein gewohnt ausführlicher Band mit dem bekannten, fachlichen Tiefgang herausgekommen ist, sondern dieser mit immerhin 424 Seiten auch einen unerwartet großen Umfang beansprucht.



Themen der einzelnen Kapitel sind ausführliche Technikbeschreibungen mit Fokus auf alle wichtigen Bestandteile und Komponenten, Umbauten und Sonderarbeiten an den Maschinen, eine zusammenfassende Chronik der Baureihe sowie einzelne Lebensläufe und natürlich auch der Einsatz an den verschiedenen Dienststellen von 1970 bis heute.

Eingangs werden Entwicklung und Bau aber erst aufgeschlüsselt und nach Baujahren, Herstellern und Serien aufgliedert. Das erleichtert das Verständnis vor allem mit Blick auf die Kapitel der jüngeren Betriebsgeschichte.

Immerhin war die Baureihe auch von einem Verkaufsverbot an Dritte betroffen, anschließend beschränkten sich Abgaben auf ältere Exemplare der ersten Serien. Die klare Strukturierung und verständliche Aufteilung der Fakten erleichtert dem Leser das Verstehen des Werdegangs nach der letzten Jahrtausendwende.

Denn während die Einsätze bei DB Regio weiter rückläufig sind und auch die Bedeutung für den Fern- und Sylt-Verkehr sinkt, haben zahlreiche Maschinen in anderen Bereichen oder privaten Bahnunternehmen neue Aufgaben gefunden.

Dazu gehören ICE-Abschlepplokomotiven von DB Fernverkehr, der Sylt-Shuttle-Verkehr der vorherigen DB AutoZug, bundesweiter Bauzugdienst, Sonderverkehre und der Güterverkehr bei privaten EVU. Damit sind die Einsätze der Baureihe 218 heute so vielfältig wie nie zuvor. Auch Museumsmaschinen und grenzüberschreitende Einsätze werden im vorliegenden Titel ebenso wenig vergessen wie alle Lackierungen und Gestaltungen, mit denen mindestens eine 218 jemals unterwegs war.

Erinnert sei hier nur an die kieselgrau-reinorange strahlenden City-Bahn-Exemplare des Bw Hagen-Eckesey oder die Einzelgängerin 218 217-8 in purpurrot-elfenbein, die oft als „TEE-Lackierung“ fehlinterpretiert wurde. Ein Blick wird auch auf bekannte Unfälle mit „Zwo-Achtzehnern“ geworfen, sowie die Aufgaben des verantwortlichen AW, allen voran das Werk in Bremen, wo Regeluntersuchungen und Bedarfsausbesserungen durchgeführt worden sind.

Ein besonderes Kapitel stellt das Betrachten von Projekten für Nachfolgebaureihen dar. Während die Bundesbahn auf Bestellungen der V 320 von Henschel verzichtete, kam sie zugleich nicht umhin, in vielen Dienstplänen unwirtschaftlichere Doppeltraktionen von Maschinen der Baureihe 218 vorzusehen.

Die Hersteller versuchten wiederholt, die DB von der Bestellung einer stärkeren Baureihe auf Basis dieser Lokfamilie zu überzeugen und beteiligte sich am Aufruf zu Entwürfen nach vorgegebenem Lastenheft. Unter ihnen finden sich einige weniger bekannte Kuriositäten, aber auch die Doppellok aus zwei um je einen Führerstand eingekürzte 218-Maschinen auf gemeinsamem Mittendrehgestell. Diese Idee der Kieler Firma MaK dürfte vielen Lesern durchaus bekannt sein.

Eingerahmt wird das wieder von vielen äußerst aussagekräftigen Aufnahmen und Zeichnungen, die wohl alle Zeitabschnitte und Betriebsepochen abzudecken wissen. Wie vom EK-Verlag gewohnt, sind sie auch wieder hervorragend aufbereitet und gedruckt worden. Versüßt wird das noch mit einem umfassenden Bildteil, der inzwischen wohl unverzichtbar in EK-Baureihenportraits ist.

So reiht sich auch der dritte und letzte Band der kleinen Reihe nahtlos an seine Vorgänger an. Wir nominieren ihn für die Neuerscheinungen des Jahres 2019 in der Kategorie Literatur und halten ihn für ein perfektes Weihnachtsgeschenk im Bereich der höherwertigen Bücher.

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25 Years of Faszination Modellbau Jubiläum in Friedrichshafen

Nearly 400 international exhibitors from 15 nations and around 2,000 non-material participants among the exhibiting associations and other communities will be represented at the official fair. On the visitor side, we noticed an unprecedented crowd on Friday, a full Saturday and a rather subdued Sunday. The 25th year of Faszination Modellbau will be remembered as a success we can look back on today.

By Stephan Fuchs and Joachim Ritter. It's hard to believe, but this fair on Lake Constance with nearby Switzerland and Austria has existed for 25 years! Accordingly, this event was strongly promoted from 1 to 3 November.

However, what we did find a bit difficult with these advertising texts: an unbelievable number of Anglicisms. Whether in the headlines of the preview with "immer im Flow" (always in flux), "It's a Classic!" or "Celebrate good times!", this "Denglisch" of official trade fair information also shows how much the German language has gone out of fashion in this country.



It almost hurts to have to read texts in which foreign language terms are distorted, English grammar is disregarded or cultivated English is distorted.



The 25th anniversary fair was well attended and also the handicraft offers for the youngest ones were well and above all gladly accepted.

It is possible that the trade fair management wants to make a modern and youthful impression, i.e. “hip” and “trendy” in its own language, but it is still not clear to us why this should not also be possible with a little more thought in the German language. And we are sure that the many Swiss and Austrian visitors would also understand, and appreciate, it in their mother tongue.

But this should only be a thought-provoking impulse, which we hope will be heard and the responsible people will think about. Our topic today should be the report of the event itself, the exhibits shown and our impressions.

In the end, the anniversary edition of the fair was very well attended. With over 53,000 visitors on three days, the conclusion of the trade fair management can certainly be signed. The Catholic holiday of All Saints on the opening Friday certainly contributed its part to this. It was full as never before on a Friday. Interested trade visitors, mixed with families, literally burst all the halls.

This rush also caused streams of visitors at the large Z-Freunde International e.V. stand. These were, because of the quality of the exhibited equipment, also highly deserved. Many astonished looks and shouts like “What, this is Z gauge?” or “Unbelievable, how detailed” were the order of the day.

The parade of Z gauge systems, which was again excellently put together by trade fair representative Sylvester Schmidt, captivated the visitors. A Swiss couple, who were about to retire, had specific questions for each exhibitor, so that, their own project of the 1 “Pensioner-Z layout” should also succeed.

continues on page 41



Trade fair representative Sylvester Schmidt (2nd from right) had once again put together a successful mix of a wide variety of systems. Meanwhile, Oliver Kessler (KK Produkcja, left) and Axel Hempelmann (ZFI Chairman, centre) presented the latest developments in the Z-Car system. Photo: Stephan Fuchs



Sascha Braun has impressively managed to conjure up the atmosphere of a Cuban sugar cane railway on the plant. And after the latest revision, his work "San Jose" has gained even more details. Photos: Stephan Fuchs

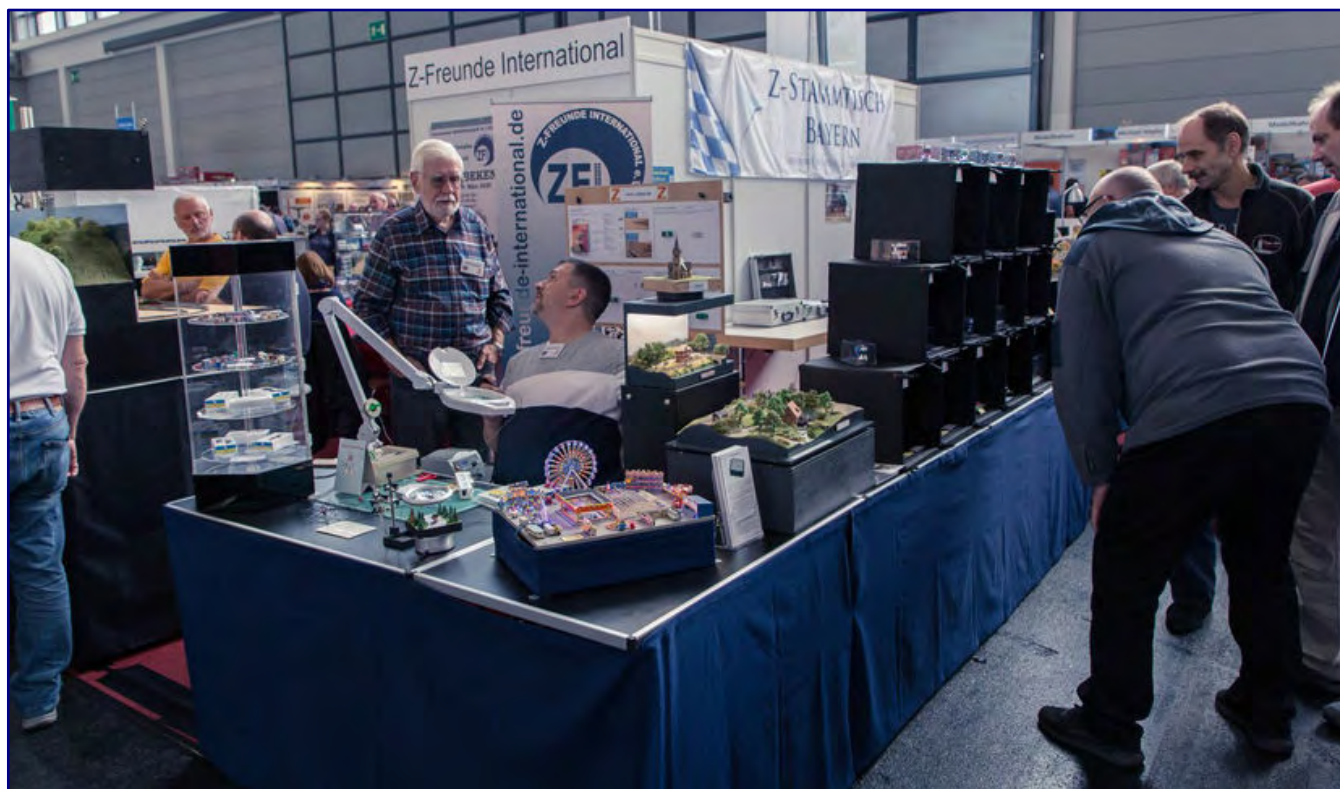
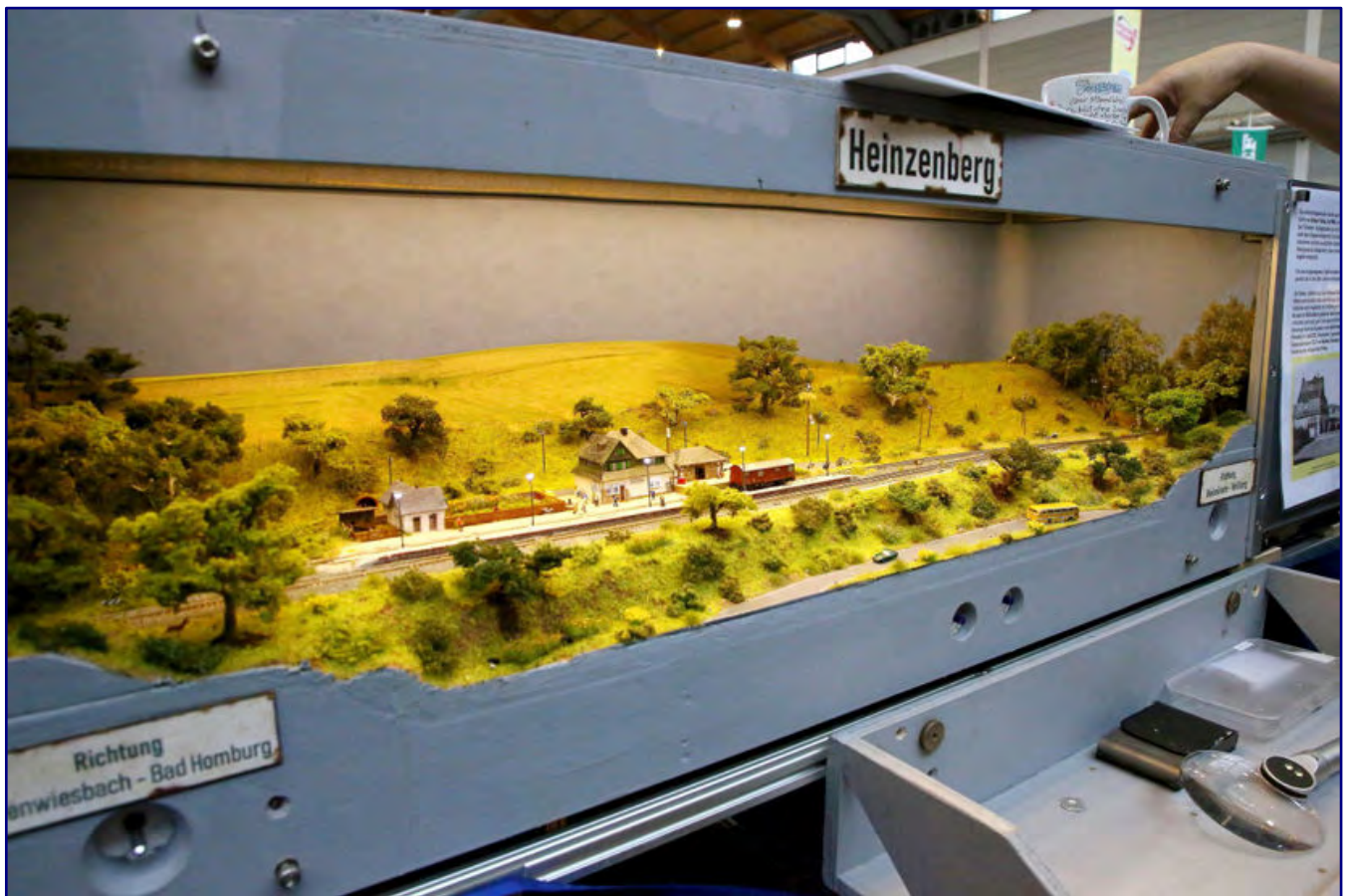


Photo top:
Gerhard Maurer provided, suitable to the season, with his “dream loop” again for autumnal mood. And finally we can show this layout in an overview photo almost over its entire extent.

Photo bottom:
In addition to his soldering demonstrations, Torsten Schubert showed all sorts of illuminated exhibits, which have long since not only dealt with the scale 1:220. Their presentation in the small darkrooms (right) is effective. Photos: Stephan Fuchs

So, very nice conversations developed, it was violently, long and extensively talked shop. The couple spent half the day in the Z gauge exhibition area. Wonderful layouts were on display, be it Sascha Braun's exotic and freshly restored Cuba facility or Gerhard Maurer's "Traumschleife" (Dream Loop), which shone in autumn. The visitors found again and again new details.

There, as well as at the "Heinzenberg" complex by Jürgen Tuschick or the "Bullay" double-decker bridge by Rainer Tielke, they were gladly informed about the history of the originals. Anyone familiar with Jürgen's latest installation based on US motifs might have been astonished at the contrasting effect of his latest work. But for him, like everyone else, it's always the individual handwriting that makes each exhibit so unique to read.



The "Heinzenberg" layout by Jürgen Tuschick celebrated its exhibition premiere, lovingly designed by him according to a real model. His wife ran the children's facility next door, where young visitors were allowed to ride trains themselves.

Once again, Manfred Forst's "Kohlenkiste" (coal box) was a fine peep-box; here, too, there was always a dense crowd to catch a glimpse of even the smallest detail.

Karl Sinn was to be seen for the last time with his installation "Kesswil", because after this exhibition its sale is on the agenda. Peter Friede was represented with his Lechschleiferei (Lech grinding shop), which had progressed considerably, but was still not completed.

There was also an age-appropriate offer for the children in the form of a small layout on which they were allowed to play. And there was always something going on here as well. On "Drachenstein" by Harald Hieber they were amazed at the diesel locomotive guided mouse train, while their parents were amazed at the many details.



This Swiss motive from "Kesswil" we will probably not see any more at exhibitions, because Karl Sinn has subsequently sold this layout.



Märklin left it to a pure showcase presentation of the current new products.

Harald was happy to tell how his plant has developed in recent years. He liked to draw attention to areas that can hardly be found in other gauges of this fineness.

But also the demonstrations, in which Torsten Schubert used the soldering iron not only to "illuminate" his models, but also the audience with fine tricks, were always closely surrounded. At the same time, the Z-Car system also attracted considerable interest.

Awkward insertion of batteries was a thing of the past here thanks to magnetic recording, and a new model impressively demonstrated how the small vehicles could easily climb even steep gradients.

All in all, this resulted in a show of performance that emphatically showed many sceptics how sophisticated, technically mature and visually impressive our small track gauge has developed recently.

At Märklin's large exhibition stand, we would have wished for a Z gauge system again. But here time seems to have stood still: old ham, abstract acrylic or pure showcase presentation.

This time, those responsible for the latter have decided to exhibit the latest new product

It was pleasing to note that the playing area between the stands of Märklin and Falter was always densely besieged.

Children as well as parents lay on the floor and played together to their heart's content. Also the Bastelecken (children's construction corner), at which model houses could be assembled, were well visited.

The N-gauge layout "Zeitreise - Zwischen Ostsee und Mittelgebirge" (Journey through time - between the Baltic Sea and the low mountain ranges) stood out particularly among the larger gauge systems.

Here there was a seaport with sailing ships, the fully functional Henrichenburg ship lift and an airship port with a hangar and a zeppelin. It seems as if it is always the little ones who have to judge it. This connects the Zetties with the "Nines."

TV presenter Hagen von Ortloff was also there again. Together with Lutz Mäder and Stephan Kraus, he presented a gigantic showcase set in the 1950s and 1960s entitled "Modellbahn- und Verkehrsgeschichte" ("Model Railway and Traffic History").



It's nice when model railroaders can help each other. With this Dutch H0 layout, Stephan Fuchs, as a balloonist, was able to explain that the pilot was standing in the basket at the start and not next to it. This was promptly corrected on site. Photo: Stephan Fuchs



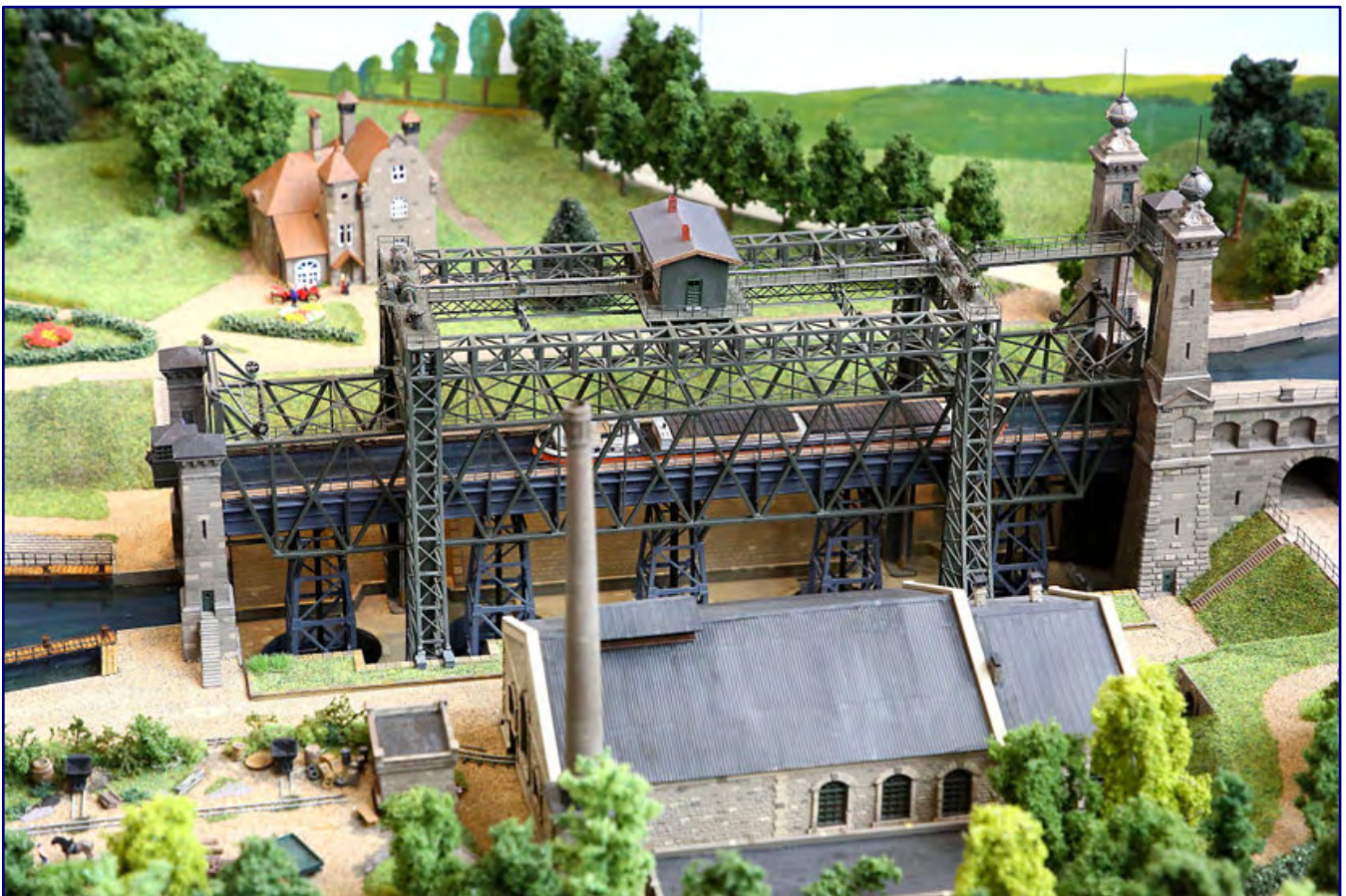
The sheet metal tracks built by Hagen von Ortloff and his comrades-in-arms also enjoyed good popularity and a steady stream of visitors. Old model railways know how to impress even without exact model fidelity. Photo: Stephan Fuchs

He himself said to us, “the layout is pure megalomania” and with it to have built up everything that can be found in the depths of his mind. A few tracks were added a few days before the fair and made it possible to present this small layout.

It is fascinating to see how the model railway has developed in the last seventy years, but also how much fun you can have playing with these old treasures today. “Yes, you can become a child again and relax wonderfully”, said a smiling Hagen von Ortloff.

The Echtdampftreffen (live steam meeting), which takes place every two years in the Friedrichshafen fair calendar, was also very popular. Here it smelled of real steam and oil, each visitor was close here and could look over the shoulder of the locomotive drivers and their helpers. The attendee trains were always packed, because whether big or small, riding along was an absolute must!

So we could see with joy: Model making is still modern and very popular. In all halls there was a lot to see, crowded (at least on Friday) the visitors moved through the halls.



One of the highlights of the exhibition was the N-gauge complex “Zeitreise - Zwischen Ostsee und Mittelgebirge” (Time Travel - between the Baltic Sea and the low mountains). in the style of Epoch II with many moving scenes. For example, ships enter and leave the Henrichsburg ship lift on the Dortmund-Ems canal, invisibly moved by the Faller-Car system.

They didn't just leave it at looking, they also bought a lot of things in addition to the many conversations they had. Many bags, crates or boxes were carried to the car, so that new ideas and achievements could be made and played at home.

As always, we will keep Friedrichshafen as a trade fair location with its excellent organisation, even the huge rush to the parking lots was mastered with ease, to the best in memory. However, we mourn the

small series manufacturers, who used to have small, but fine stands, and who can no longer afford the expensive exhibition stands today.

We were still missing a few “Aha experiences”, which in earlier years still spiced up the trade fairs by Schall events. Especially the small suppliers, with whom there is otherwise only e-mail and internet contact outside the trade fairs and exhibitions, were like the salt in the soup.

Official organizer pages:

<https://www.faszination-modellbau.de>

Exhibitors mentioned in the article:

<https://www.faller.de>

<http://www.forstberg.de>

<http://gerhardingen.de>

<https://www.maerklin.de>

<http://www.rainer-tielke-modellbau.com>

<http://www.spur-z-atelier.de>

<http://z-car-system.de>

<http://www.zcustomizer.de>

<http://z-freunde-international.de>

<http://www.z-lights.de>

<https://1z220.de>

<http://www.1-220-modellbahn.de>

Event advertisement

14. Modellbahn-Ausstellung Zeller Adventsmarkt

**Zell an der Mosel
Zeller-Schwarze-Katz-Halle**

Sonntag, 08. Dezember 2019, von 11 bis 17 Uhr
Veranstalter: Z-Freunde International e.V., 56856 Zell (Mosel)

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

With questions this reader turned to us:

I like to read her magazine regularly and I like it very much. I have even looked through all the old issues bit by bit. Now I have a question.

I would like to convert some locomotives to a 5-pole or bell-shaped armature motor incl. maintenance and DCC decoder. Do you have any information on who I could contact for the purpose of the conversion?
Jens Drews, Ammersbek

Answer from the editors board: First of all we would like to question the requirement of a conversion of the respective engine. Although the five-pole, but above all the bell armature drive, offers significantly more driving refinement, this does not automatically justify such a conversion. A locomotive to be digitized should run well in analogue, because the decoder cannot compensate for a bad engine, or a not smoothly running gearbox. The five-pole also requires more frequent maintenance because it has smaller collector gaps, which clog more quickly. A higher current consumption is the result, which can also be dangerous for the decoder.

With these preliminary considerations we would like to answer your question as follows:

- 1.) Bogie models can be digitized easily and even with a Velmo decoder exchange board (velmo.de).
- 2.) The article numbers of matching five-pole motors can be found in the exploded views of technically identical models on the Märklin webpages (incl. availability display). They can be ordered there or through any dealer.
- 3.) If you would like to convert to bell-shaped armature motors and do not have the appropriate tools and experience for the milling work to be carried out, specialised suppliers who can also carry out digital conversion according to your wishes are available. As suitable we recommend Michael Bahls (bahls-modelleisenbahnen.de) or Oliver Passmann (passmann.com).

Trainini® at the Bergkamen Railway Friends:

On 2 and 3 November 2019, the Bergkamen railway enthusiasts again invited to the Day of the Model Railway enthusiast and presented their annual club exhibition, which took place for the second time in the Schützenheide Bergkamen (district of Unna).

Several installations of the sizes N, H0, 0, I and II f could be seen and admired. Dealers and small-series manufacturers offered fine and sought-after items, among them Modellbahn-Union from nearby Kamen with a large sales stand.

Highlights of the exhibition were a remote-controlled steam crane in track 1, which swivelled a bridge over the railway line, and the "Klosterstollen" mine on a scale of 1:22.5 (track II f). The model for this today museum-like operation can be found in Barsinghausen near Hannover, where the builder also lives.

Trainini® also took part again with an offer that was aimed at the children: At the magazine stand interested young model railroaders were allowed to graze on a beer mat, design it, learn something about model making and in the dry breaks steer a Shinkansen train from Rokuhan themselves over an oval track.



Two Shinkansen trains in the Z gauge invited the participating children to their own taxes (photo above). At times, short queues formed behind the control desk, from the photographer's point of view on the right next to it, handicrafts were carried out (photo below left). The most unusual object of the exhibition was the excellently designed "Klosterstollen" (photo below right).

Since this offer was taken up and highlighted by the press in the (preliminary) reports, the number of children on Saturday was already considerably higher than the total number of the previous year. The opportunity was in great demand and was actively used. Addressing children of roughly the same age has proved particularly effective. We are glad to have contributed to a very special family event.

We also had some exhibits in our luggage: A golf course on a scale of 1:220 was shown to demonstrate different grass fibre lengths, the summer diorama presented this year and the youth work "Auf der schwäb'schen Eisenbahn" (On the Swabian railway) in H0 scale. Many nice conversations with numerous Zetties rounded off the successful event for us.

Christmas campaign "Tortenschlacht" (Cake Battle):

What does a cake fight have to do with model railways? Märklin is trying to show exactly that in its latest advertising campaign at the start of the Christmas season. It is addressed in the form of a short and amusing film on the Youtube platform or at <https://www.maerklin.de/de/lp/2019/tortenschlacht/> near the bottom of the webpage.

But it is not only done with the video. “The video is the focus of a campaign that addresses the memories of the target group”, reports Jörg Iske, Marketing Manager at Märklin. “We want to reach the youngest by playing together with their grandpa, which has been a tradition in many families for decades - Christmas time is Märklin time”, he says.

Anyone who has experienced the magic of model railroading in his or her own childhood should immediately have bright eyes, even if he or she no longer has anything to do with model railroading today. “And this is exactly our target group”, Iske continues.

Own Z-gauge program of the Modellbahn-Union:

The Modellbahn-Union (<https://www.modellbahnunion.com>) takes things seriously and expands the range of its own brand for Z gauge quickly and powerfully. The new products, which we present here, are manufactured using the 3D printing process. They are delivered in neutral grey as plastic models and require colour painting by the customer.

The first is the “Eiserner Schutzmann” (Iron Protector) emergency call pillar (Art.-No. MU-Z-A00214), which used to stand at important road crossings and which every model railway enthusiast, who experienced the time before the mobile phone, will remember well. Two examples are included in package.

The turnout heater with 2 propane gas tanks (MU-Z-A0011) is also supplied in two units and protects against operating faults caused by severe frost in winter.

The two three-door lockers (MU-Z-A00008), which can be supplemented by two workbenches (MU-Z-A00007), on



The “Eiserne Schutzmann” (Iron Protector) (picture above right) and the interior fittings for a lever interlocking system (picture below) are just two of the new products in the steadily growing Z-gauge series of private label products. Photos: Modellbahn-Union

which even a tiny vice can be seen, also find many applications in workshops, industrial areas or in the locomotive shed.

This topic is further complemented by a lathe and a modern workshop trolley (MU-Z-A00006). Anyone who designs a signal box will also find the suitable equipment with the seven-part interior consisting of two lever benches as well as a desk, block box, stove, cupboard and locker (MU-Z-A00063).

The four mobile toilets (MU-Z-A00016), which can almost equip a complete concert in this quantity, serve human needs. Further new products will be subsequently added to the line, and we will continue to report thereon.

New animal figures from C-M-K:

Creativity is Klingenhöfer's program and so the C-M-K program now includes more animal figures for a wide range of uses. Fans of Scandinavian models would like to have their rolling stock safely transported in a suitable setting.

To make this credible, the animals of the north cannot be omitted. And so an elk bull and an elk cow (art. no. TE09-1) will expand the product range in the future.



Like all new animal figures, the moose bull and moose cow (Art.-No. TE09-1) are also very convincing C-M-K models all round. Photo: Klingenhöfer



The three new geese (TG02) are so small and yet so finely designed. Photo: Klingenhöfer

Putting Nils Holgersson from Astrid Lindgren's novels in the limelight could be a bit difficult on a scale of 1:220. Martin, the house goose that joins the wild geese, on the other hand, would find a suitable template in the assortment in the future.

New at C-M-K (<https://www.klingenhoefer.com>) are also three geese (TG02) with white plumage and orange beaks, which know how to populate any farm. But please don't put it close to Christmas time...

Multipliers urgently needed for model railway hobby:

The Modellbahnverband in Deutschland e.V (Moba) published a search on 30 October 2019 as a call for help, which we initiated on the part of the editorial staff. Two OGS (open all-day schools) in Schwerte (Ruhr) run by the Johanniter would like to (again) offer a model railway opportunity for their primary school pupils.

One of the two schools already has experience in this area and also has model railway equipment. Here, the former director of this offer had to retire for health reasons, and left a painful gap among the children willing to do handicrafts.

A second school would like to make this offer itself because of the good acceptance of such an AG, but needs an experienced model railroader who likes to work with primary school children and knows how to deal with children of this age.

Whoever can imagine such a thing here, honorary or also in minor employment, may turn gladly to our editorship. This also applies to questions which need to be clarified in advance and which may be relevant for a decision.

Our editor-in-chief Holger Späing has been in charge of such offers for several years and has also been active in Schwerte at a third Johanniter-OGS for over a year. The great success with boys, as well as girls, proves him right.

“Time and again there is talk of the extinction of the hobby of model railways. On the other hand, I experience how great the children's interest and thirst for knowledge really are. It regularly requires organizational talent and improvisation in order not to have to cancel a child registration”, he reports. “Now it is up to all of us, including you, to give this hobby a perspective and a future! And that's not so difficult”, Holger Späing sums up.

You find our contact data in the imprint, the call for help is published in the net under <http://moba-deutschland.de/hilferuf/>.

Märklin deliveries in November:

During the reporting period of this issue, things remained surprisingly quiet as far as newly delivered Märklin models are concerned. For many of the new products announced, delivery dates have recently been postponed, often resulting in a delay into the new year.



The new hardboard kit for the Himmelreich railway station (Art. No. 89709) at the Höllentalbahn in the Black Forest was delivered at the editorial deadline.

However, the freight train package "From house to house" with new Pa containers (Art. No. 82329), which we treat separately and in detail in this issue, has found its way onto the shelves of dealers.

The hardboard architecture kit for Himmelreich station (89709), for which we are currently preparing a multi-part report next year, is now also being delivered, and is, therefore, on schedule.

Distribution in North America now licensed:

The Hello Kitty Shinkansen from Rokuhan, about which we have reported extensively in **Trainini®** 2/2019, has now been licensed to be offered by our distribution partner **Ztrack** in the United States of America.

This means that both the exclusively designed and assembled starter pack with a three-part train (Item No. G004-3) and the individually available three-part basic unit (T013-6), the four-part extension pack (T013-7) and the Shorty model (ST008-2) are now included in the product range.

In addition, the shortened unit of the Shinkansen 500 Kodama in standard paint (ST008-1) will be added. The sales pages for end customers can be found at <http://www.ztrackcenter.com>.

On the road in East and West with etchIT:

etchIT-Modellbau (<http://www.etchit.de>) has published another truck for the nominal size Z (Art.-Nr. XD076_Z). The model is the Ford Transcontinental. Technically, it was once (years of construction 1975 - 1984) popular among heavy trucks and in western Germany, and was very popular in freight forwarding fleets, as well as used in the cross-border transport with the DDR by Deutrans.



The Ford Transcontinental (Art.-No. XD076_Z) was on the road in East and West, as it was also used across the border by Deutrans. Photos: etchIT

The detailed kit is manufactured as usual using the high-resolution dotHIRES 3D printing process. In the near future a matching two-axle turntable trailer with platform and tarpaulin will follow.

And that's what's new at Micro-Trains:

The farm-to-table series will be continued this month with car number 10 (art. no. 518 00 800). The wooden refrigerator car in the yellow colour has been set up at the Northern Refrigerator Car Company and has been on the road for the "Zion Bakery" since the late twenties.

In the standard brown of the Union Pacific a covered freight car of 50 feet length with double sliding doors (506 00 290) is on its way. The same design in the blue colours and yellow lettering of the Chesapeake & Ohio (506 00 391 / -392) is being released with two different car numbers at the same time. The sliding doors are also clearly set off in yellow.

Now also the diesel locomotive EMD SD40-2 "Desert Storm" of the Union Pacific (970 01 300) with the company number 3593 in spot camouflage paint is sold.

The models are distributed in Germany by Case Hobbies (<http://case-hobbies.de>), among others.

The **Trainini Photo Calendar 2020** has been published:

Also for friends of our magazine there is now an own calendar! The **Trainini Photo Calendar 2020** with thirteen motifs was published on 22 November 2019, and is available for download and printing on the download page.

The new calendar can be printed for hanging at up to DIN A3 format. As every year, we have selected the motifs in a colourful way and as appropriate as possible to the seasonal mood.

We also tried to combine all types of traction, different epochs, and as many railway administrations and countries, as possible, into a successful mixture.

An English version for the US holidays will follow by mid-December at the latest in cooperation with our correspondent magazine **Ztrack**.



Ongoing AZL deliveries at the beginning of autumn:

American Z Line has the famous Southern Pacific Daylight on its agenda for November 2019. Its red-orange carriages are some of the most striking and well-known wagons on America's rails.

AZL is delivering the following cars this month: Dining cars (Item No. 73547-0), T&NO baggage cars (73647-1 / -2), seating cars (73747-0), pulpit cars (73847-0) and mail cars (73947-1 / -2).

Photos of the manufacturers of the current deliveries can be found at <http://www.americanzline.com>.



Observation car (photo above) and mail car (photo below) for the "Southern Pacific Daylight". Photo: AZL / **Ztrack**

Soon it's International Model Railroading Day again:

On 2 December 2019 is again International Model Railroading Day. Meanwhile, it is also popular abroad and receives active support there, so that it can now, without exaggerating, be called the International Model Railway Day.

In order to provide sufficient space for actions and exhibitions that serve to promote and promote the most beautiful hobby in the world, the days before and after 2 December 2019 will also be included and extended to an action week.

At a time when children can no longer press their noses against shop windows of model railway dealers, such high-profile campaigns in cooperation with industry, their interest groups, small series manufacturers, clubs and individual model railway enthusiasts are important in order to excite third parties.



Model Railroading Day

2. December

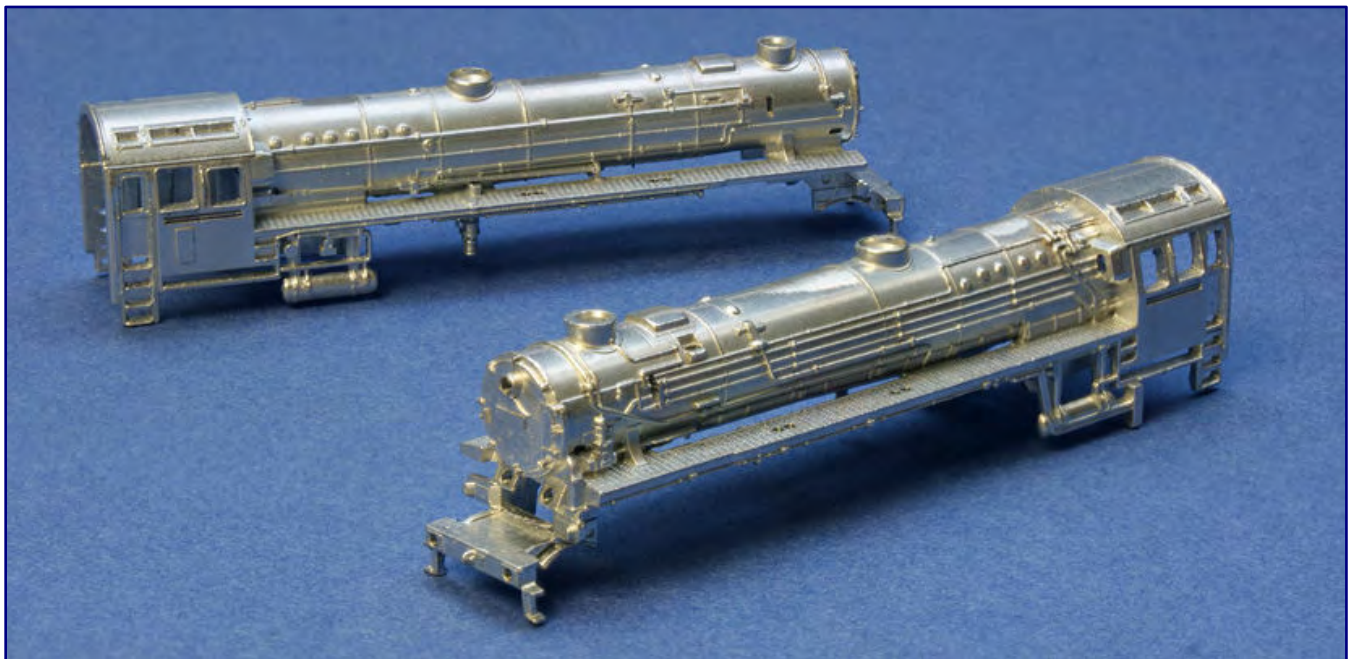
On the campaign pages (<https://www.tag-der-modelleisenbahn.de>) all registered events and much more information can be found. At the same time there is the possibility to register your own events.

Trainini® will again make its contribution to the achievement: During the Advent meeting in Zell (Mosel) on Sunday, 8 December 2019, during the opening hours of the model railway exhibition, we offer free beer mat designing with small landscapes for all children visiting this exhibition.

This is made possible by the support of our sponsors Conrad Electronic, Modellbahn-Union, Noch, Uhu and the 1zu220-Shop.

Announcement of Insider Models 2020:

On 27 November 2019 from 18:00 Märklin would like to announce its Insider Models for 2020. For the first time, this is planned as a real-time campaign in which only registered members can participate and thus get back a piece of exclusivity that was once characteristic for premium customers and has, unfortunately, been lost in our fast-moving times.



Insider members simply log into the club area with their access data on the Märklin pages and can then participate in the exclusive video presentation. After the presentation there will be the possibility to chat with Märklin employees.

Those who can't see what Märklin has come up with for its loyal regular customers on a scale of 1:220 will find out here shortly before Christmas.

While the Z-gauge model for 2017, a DB diesel locomotive V 80, is still waiting in the wings due to a previously communicated tool defect, and will have to be re-introduced into production, there is growing evidence that the express steam locomotive of the series 03¹⁰ with the new boiler (Insider model 2019) will find its way to its customers by the end of the year.

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That's gotta roll...

Shinkansen into the light
Theatrical layout design

Introduction

Dear readers,

It is hard to believe that it is already another year (almost) around! The 15th year of our popular magazine is finished with this December issue.

I can still well remember how, in August 2005, we virtually out of emergency pulled **Trainini®** out of the ground. Only the name of what is now by far the world's leading magazine especially for Z-gauge had been known for a long time.



Holger Späing
Editor-in-chief

Only it had not been thought up for a magazine. A distribution platform for small series manufacturers was to be built up under it. At the beginning of 2005 our gauge seemed well positioned in the media: We had long been well networked and visible at the important exhibitions, knew how to draw attention to ourselves and impress third parties.

What was missing from my point of view at the time was the sales platform. It was supposed to relieve small-series manufacturers of the effort of picking, shipping and accounting and to create free space for the development and production of models. On the customer side, postage costs would have been better exploited, because products from different manufacturers could have been ordered in a single process.

But as the people so often say: "First of all, things don't happen the way you think they will." When, from one day to the next, Z gauge was lacking an information medium, quick action was required. Throwing the almost finished business plan overboard, I spontaneously decided to fill this gap on a voluntary basis and to look for helpers.

I could rely on editorial experience with a membership magazine of the German Lifesaving Society (DLRG) and my professional past in the field of marketing including conceptual responsibility, which in today's manufacturing industry is usually defined as product management.

I also found the necessary helpers quickly and can still count on them today. Their number has grown over the years and I can only list those who are regularly involved in **Trainini®** in various ways and can also partially be found in the imprint:

Bernd Knauf (†) as proof-reader and later editor, Joachim Ritter as author, correspondent, proof-reader and editor, Dirk Kuhlmann as model manufacturer, author and editor, Stephan Fuchs as photographer and author and Torsten Schubert in the field of trade fair reports, solder works and model making.

Michael Etz runs a Facebook group that uses the name **Trainini®** under license, and the licensed archives don't want to be forgotten either. Occasionally, Jörg Erkel, Götz Guddas, Dirk Rohwerder, Andreas Petkelis and Peter Scheele help us in various ways, also in an advisory manner. Stephan Bauer plays a similar role in the area of internet presence.

I am also particularly proud of our three translators Alexander Hock, Christoph Maier and Martin Stercken. Christmas is now the time to thank all our readers and all the helpers - even those not mentioned by name here - and to wish them a Merry Christmas!

Holger Späing

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We thank Ulrich Biene, F.-J- Huwig, Historisches Archiv BP / Aral and the Eisenbahnstiftung for their generous support with texts and pictures.

Date of publication of the German language version of this issue: 23 December 2019

Cover photo:

In the years of the German economic miracle, the demand for fuels increases enormously year after year. 80,032 tank wagons are shunting on the tracks of a junction two tank wagons, which will go on a long journey as a wagon group within one of the next freight trains.

Tank cars, filling stations and mineral oil logistics

A small step with a big effect

Märklin revitalizes mineral oil logistics with a new Aral tank car on a scale of 1:220. We took a look at the ground breaking Era III model and thought about what suitable accessories are or were to be found on the right and left in order to successfully and effectively present the topic of fuel supply in the model. And this topic is more colourful and diverse than initially thought, so we will stick to a few suggestions

“That's one small step...”, began the famous quote from the first moon ascent by American Neil Armstrong in 1969, and the conclusion of a Märklin new product for gauge Z could begin fifty years later.

The fact that we are reporting on them with our own article may seem surprising at first. The model would have remained just one of many colour variations, if its manufacturer had not decided to give it a small shield that makes the difference and also shows perspectives.



The old-style tank car from Märklin is one of the better realized models. The finely reproduced details include the rows of rivets on the tank and the wooden structure of the running boards next to the filler dome. In the latest edition, it appears as a private car of the BV-Aral Aktiengesellschaft (Item No. 82324), and is used by the Deutsche Bundesbahn (German Federal Railroad).

And so we expect that this small step will mark the beginning of a new era of more prototypical tank cars. We take this as an opportunity to take a closer look at the new model, to place it in its epoch, and to venture a small market overview on the subject of filling stations and crude oil logistics, but without claiming to be complete.

We will first focus our considerations on the recently delivered Aral tank car of the replacement type with brakeman's platform (item no. 82324). According to the operating labelling of Deutsche Bundesbahn, it is registered as a private car of BV-Aral Aktiengesellschaft and has the road number 503 316 [P].

It is no longer assigned to a genus district, but the area from which his road number originates comes from the one which BD Essen was allowed to use for such cars. This is in keeping with the Bochum-Riemke home station that was written: local wagons had to be returned directly to the home station after emptying.



Painting and printing of the new item are impeccable, as we are used to from Märklin. But a big step forward is the separately applied shield for the Aral diamond, undoubtedly a small detail with great effect.

Its capacity of 194 hl according to the inscription seems to us quite small, and we could neither prove nor disprove that Aral had such a small car in use. Its load is indicated by the attached signs with "petrol, benzene, toluene, spirit and oil".

Best new releases of the year 2019

For the best new releases of the year, we nominate new models that were delivered to retailers or end customers in the respective year of our magazine, and submitted to the editorial team for personal evaluation.

In the case of new products based on already known parts, it is decided on a case-by-case basis whether the level of creation or degree of innovation is sufficient to be nominated for the awards.

The model considered here is a good example of how, even with relatively minor innovations, it is sometimes possible to achieve great effects, and tangible steps forward.

The Aral tank car (item no. 82324) from Märklin will therefore be nominated as the 2019 best new releases in the category of cars.

The riveted cauldron resting on the jet-black chassis with old steps and railings is painted light grey, and bears the ultramarine blue Aral diamond for the first time, as a separately attached shield on the right-hand side of the cauldron.

And this is precisely the decisive innovation to which we would like to refer. On earlier tank cars, such as the Bundesbahn model of the same design with silver-coloured tank from a DB starter pack, the company logo was always printed directly on the tank.



Mineral oil logistics has many facets, including the transport of heavy oil for oil-fired steam locomotives in service tank cars (82060). Our Aral miniature is shunted here together with a brother belonging to VTG, which, like the four-axle Gasolin wagon, comes from an older freight train package (82518).

Directly comparable would also be the older model with a cream-white boiler and brakeman's cab, which Z Club 92 offered as a museum car in 1997. There was a prototype model from the DGEG railway museum in Bochum-Dahlhausen. This wagon bore labelling as a private car of the BV-Aral Aktiengesellschaft, which was registered with the Deutsche Reichsbahn-Gesellschaft.

They all looked appealing and largely followed real-life examples, but the company emblems printed on the boiler were not correct.

In the days of the Reichsbahn and even the Bundesbahn, signs were usually screwed on separately with brackets, which is why the company emblems stood distinctly away from the boiler, and did not follow the boiler's curvature.

Märklin has now reproduced this on a Z scale model for the first time.

But there is a small hitch, because the model plate has the plug connection to the boiler at its middle connection. The model had two beams (right and left) on which the sign was fixed and which were angled around 90° so that they could be riveted or welded to the boiler.



The 1997 Z Club 92 museum car produced and printed by Märklin had a real prototype model, but only had a printed company emblem (in the 1952 design).

If you also value this detail on a scale of 1:220, you would certainly be right with the relevant small-series manufacturers, especially as there are comparable super parts for larger tracks. We don't want to criticize the new product at this point and are happy about this effective feature, which fits perfectly to the written operating condition between 1958 and 1961.

By the way, the same applies to the chosen boiler colouring in light grey. However, the wagon under consideration here stands in stark contrast to the four-axle standard tank car with a sheet metal brakeman's cab (82073), which the Insider members were able to purchase at a reduced price in 2008 as compensation for an unusual catalogue.

The body of this car was painted completely in ultramarine blue and also carried the Aral logo from 1952 on the right side of the boiler. But which colour scheme is right for a company-owned Aral tank car?



Photo above:

In the Aral stock, it seems certain today, there were also blue tank cars. So the four-axle Insider car (82073) probably did not originate from Märklin's imagination. However, its emblem, unlike the new product, was only printed on it. Photo: F.-J. Huwig

Photo below:

Many colourful tank cars in the history of model railways had no prototype model in their design. With the three Olex cars (82311) from the transition period to BP (see colour scheme and logos next to the lettering), this probably looked different.

After all, many colourful tank car designs of the seventies and eighties allegedly originate from the imagination of product managers, as can be read and heard again and again? We agree with this impression, because only very few realizations of that time can be exactly or approximately documented with photos.

At Aral, however, it is certain that some of the tank cars were painted blue, as can be read in "Güterwagen Band 7" by Stefan Carstens. Anyone who would like to take a closer look at this subject, should, therefore, refer to this excellently researched book.

A final note, however, is that Aral was the first crude oil company to give up its own tank car fleet, whose cars ended up with the lessors VTG and Eva. We are thus already touching on the era of uniformly grey tank wagons, which, when leased to the same company for a longer period of time, at best referred to the crude oil brand with an attached sign.

How much more colourful was the situation before the Second World War? Märklin once tried to remind people of this with a three-part tank car pack (82311), two yellow and one blue of which were on the road for the OLEX and some of which already had the later BP logo next to them. The yellow Shell cars, which are well remembered by so many model railroaders, also date from that period.

Suitable road vehicles

Since crude oil logistics is a very interesting subject area for model railways, it is not surprising that it has always been popular with accessory manufacturers. The DAF tractor from the Kibri range is classic Z.

Currently, it can be found in the programme under article number 36980 with a modern tanker trailer, which is designed in the colours and with the emblems of Aral. The mentioned vehicle combination is one of six different truck-trailer combinations of the mentioned product combination.



This Aral tanker truck in a still quite contemporary design is included in the Kibri 36980 package and can justifiably be called a classic in scale 1:220. Photos: Kibri / Viessmann

Also already a classic, because it has long since ceased to be available, the Opel Blitz is available as "Tanker Shell" (Item No. 35) from Limized, at that time still trading under the name "Live Miniatures". This small vehicle is a typical fuel transporter for the early post-war years, when the successful Opel Blitz model was still being built, and, therefore, fits perfectly into the early Era III. It can certainly also credibly bring heating oil to households.



The Opel Blitz as a Shell tanker still comes from the era of the former “live miniatures” (35), while the Ford Canada 60, built as a military transporter from 1944 onwards, was already produced in the BP-Olex design (4310074) under the Limized brand. Today this small series manufacturer is unfortunately no longer active.

For many Zetties, the Ford Canada 60 probably looks unusual. Its long part number (4310074) refers to the later years of Limized, which unfortunately are already a thing of the past. It was designed as an OLEX vehicle in blue and yellow, and, with its strange looking shape, it is quite unique to most Germans.

This truck was built from 1944 by Ford in Canada as a unified military vehicle, which was used in large numbers as a load carrier in the war against Germany. Since gasoline and diesel fuel were compulsory in Germany at that time, and were only sold as unbranded standard fuel, it cannot be a prize vehicle.

The original vehicle was probably a truck that came into civilian hands after the end of the war, and had a second career in the stock of the oil company that was contacted. Like the aforementioned Opel Blitz, it belongs to Era IIIa.

However, due to the merger of OLEX in 1950 and the change of name to BP with a subsequent change of company colour to their green/yellow, the active period for this vehicle is more limited than for the Shell tanker.

A still quite young model also belongs to this long gone time and deserves to be reissued in the catalogue programme with inscriptions of a well-known oil company. We would suggest Gasolin (a company name, not the product), as we know of some suitable model photographs.

We are talking about the supplement to the Märklin museum car 2016 (80027). It was dedicated to the company Zeller + Gmelin, whose red corporate color radiated in an almost brilliant way. The enclosed vehicle model was a Büssing truck with rubber tires.



The Märklin Museum Car 2016 (80027), a four-axle standard tank car designed in the corporate colours of Zeller + Gmelin, contained an interesting addition: The loading area of the rollable Büssing truck was fitted with a demountable tank, which was often used by coal dealers, who had discovered mineral trading oil as an additional line of business.

A grey container was loaded on his loading platform, as was the case with many coal merchants for a long time, which opened up a further source of income by trading in fuel oil. If we think of Gasolin in this model, this now leads us to three designs of our own, which fit perfectly to today's topic.

The first model is a VW Transporter T1, which comes from MBW. We painted it white, set it off in red and gave it a window grey roof. Supplemented by the emergency labelling of the classic Gasolin lettering on the sides and a logo on the front (in place of the VW emblem), it exactly reproduces one of the many minibuses that were actually on the road in such an effective advertising design.

The DAF 1300 (220.DAF.01.02), built in 1959, which is not very common in Germany, and the tanker semitrailer (220.TRL.02.02), built around 1960, make a visually and chronologically appropriate tractor-trailer combination.



In view of the popularity and awareness of the former brand Gasolin, we have designed two vehicles of this chain based on templates with decals by Andreas Nothaft. The tractor of the tank truck is a less common model in Germany.

We have also painted these two models in the house colours of Gasolin. While the tractor is painted completely red, the trailer has all three colours (red, white and window grey). On the long side of the boiler the traditional Gasolin lettering is emblazoned, while the doors of the tractor unit show the then valid logo. Both originate from lettering sets by Andreas Nothaft.



While the Gasolin vehicles only required painting and lettering, the Aral articulated lorry was created by a conversion. Here you can see all three models after applying the last colour refinements such as indicators, tail lights, headlights and number plates.

Aral tanker truck in self-construction

However, it was somewhat trickier with a comparable supply vehicle from Aral. Of course, it should not only differ in colour, but also be typical for the Germany of the sixties.

Since there was no alternative for the tanker trailer, the only remaining option was to replace the articulated lorry. But for our purpose no suitable models could be found at A2 Models. So we went, similar to Dirk Rohwerder (see photo on page 19), the way of converting of an existing German model.



For the conversion, the Märklin base vehicle must be disassembled as described in the article. The printed labelling on the doors is first removed (photo above), and then the rear wall of the driver's cab is filled (photo below).



Before the filled area can be repainted in the cabin colour, the surface must first be sanded smooth.

While our idea generator used the Krupp Titan and was on the road before 1952, with BV-Aral labelling, and the old BV logo, our focus was on the first half of the sixties.

Since there was no alternative for the tanker trailer, the only remaining option was to replace the articulated lorry.

They were already on the market at the beginning of the fifties, but were not built for the German market until 1967 via the Planet Series and the type S 6500. Their wide distribution made them perfect for our project.

We found a model for the tractor unit in the Märklin program: The supplement to the 2009 museum car was a blue Magirus platform truck with tarpaulin. Together with the blue chassis it seemed perfect, as long as it could be converted into a semi-trailer.



The chassis of the semitrailer truck is already reassembled. The semitrailer attachment was supplied as a 3D printed part by EtchIT-Modellbau. Compared to the unpainted DAF 1300 with rear trailer, it is easy to see how much the base paint already makes up the overall impression.

The platform attachment could be removed after the connection point between the chassis and the body had been drilled out from below. Now the chassis had to be shortened to the length of a tractor unit. To do this, a suitable piece was sawn out between the tank container and the rear axle, and the shortened frame was then reassembled with two-component adhesive.

In order for this connection to hold, a frame reinforcement was still missing to increase the bonding surface. This could be realized in a prototypical way with a truck seat post including its plate surface, which was produced as a 3D printed part by EtchIT Modellbau.

It was actually taken from the construction kit of one of our own models, and fitted perfectly to our new model. In the front they were supported by a thin strip of black 0.5 mm polystyrene plate from Evergreen. A silky matt spray paint coat in deep black completed the work on the chassis.

But also the driver's cab still required some modifications. The paint colour was a little lighter than the ultramarine blue, which comes closest to the historic Aral colours. But this could not be changed because to do so, the front imprints of headlights, radiator grille and Magirus signs would have been lost.

We therefore accepted a slight colour deviation at this point and explain it with the company colours which were not yet definitively set at that time. At the doors, however, the tampon (pad printed) imprints were still disturbing, because the Aral diamonds were later to be placed there. Electric eraser and the Tampondruckentferner (pad printing remover) from Prehm Modellbahn made the labelling disappear without a trace.

In addition, the rear wall of the driver's cab also had to be reworked because a saw cut had to separate it from the platform, which no longer belonged together. So the surface was carefully smoothed, joints and cracks were closed with modelling putty "Revell Plasto", and, after drying, they were sanded smooth.



The finished labelled vehicle is waiting for its clear lacquer finish for permanent protection of the decals. Following photo templates, the Aral tank truck has also been given a company emblem at the rear of the trailer.

The glossy enamel paint "Revell 52" proved to be an exact match for repainting. The last change needed to be made now only on the roof, which was white on all Aral tankers we saw in historical photos.

With 10 mm wide Tamiya-Abklebeband (“Masking Tape”) from the dispenser, in our opinion the best product on the market, due to sufficient adhesion and good removability, we protected all parts of the cab that should not be allowed to get paint mist. The spraying unit then sprayed the exposed surface RAL 9002 grey-white.

The trailer, a super-resolved 3D print model of A2 Models, was also spray painted with colours from Oesling-Modellbau: basic colour RAL 9002 grey-white, contrasting surfaces in RAL 5002 ultramarine blue and black wheels and chassis in RAL 9005 jet black.



The finished Aral tanker truck cuts a fine figure on the site. It represents the generation of typical trucks that were on Germany's roads at the time chosen by most model railroaders.

At this point, we would like to give you a tip for working with masking tape: Pin-sharp dividing lines can only be achieved if the tape is applied to the surface without joints. The separating edge is then first sprayed briefly so that the paint could undermine the tape. However, the colour that would be found under the taped surface is chosen. Sprayed thinly, this layer dries so quickly that work can continue immediately with the target colour.

Now all that was missing were the company addresses before this model could be sealed with clear varnish and allowed to be installed on the layout as a finished piece. The scope of the symbols and writings to be applied included small Aral diamonds in the 1952 design (used until 1973) on the cab doors, on the right and left sides of the trailer, and in a larger form on its rear.

Between the logos on the sides, we only chose the lettering “Aral,” although we did not find a secure template for this. The reason for this is that we have chosen the mid-sixties as the time shown: Aral completely deleted the “BV” from its name in 1962, so that, newly built and labelled vehicles from that time on, should also be without it.

Self-constructed Kits

In view of the listed road vehicles and many more passenger cars waiting for fuel in the model, we did not want to forget to also take a look at the no less interesting filling stations.

Finally, they provide surfaces for the presentation of car models, for interesting figure and light scenes during work in the workshop, or also for easily visible interior design in the lavishly glazed checkout area.



Wolf-Ullrich Malm has built a replica of the Olex filling station, which was built in Lübeck in 1924 (picture above). The once typical combination of a workshop or village smithy (blacksmith) with a small filling station (picture below) has been converted by MBZ into a kit.

This did not yet exist in the model for Wolf-Ullrich Malm from 1924. On his "Alt-Lübeck 1905" layout, he built his own model of the OLEX filling station in Lübeck, which at the time was one of the first of its kind in the country.

MBZ-Modellbahnzubehör reminds of the time, when gas stations were often connected to repair shops as a sideline business, with its laser cardboard construction kit “Alte Schmiede mit Tankstelle” (old blacksmith with filling station; 16388), which is in good hands in village areas on branch lines and could be found there for some years longer.



Torsten Schubert transformed the typical appearance of a former large Gasolin petrol station with mushroom-shaped roof support column, petrol pumps, oil cabinet, maintenance and care hall, as well as the mast with the trademark on the road into the model. His prototype model was the preserved petrol station Brandshof in Hamburg.

A sample of small post-war petrol stations can be found in the same programme: The kit, simply called "Tankstelle" (16389), has an Esso station with only two petrol pumps as a model, but they are already covered away from the cash desk.



Photo above:
The small Esso filling station of MBZ (16389) has only two columns and no annex for refreshment room or vehicle care. It probably represents the smallest type of petrol station which was common at that time. Photo: MBZ-Modellbahnzubehör

Photo below:
Luetke Modellbahn has chosen a more modern architecture of free petrol stations (73240). Although there are only two fuel pumps under the roof, this model has considerable maintenance and repair capacity as well as a car wash. Photo: Luetke Modellbahn

Completely different were the large filling stations of Gasolin, which, unfortunately, are not available as a kit. Torsten Schubert nevertheless fulfilled his wish, and built the impressively illuminated vintage gas station Brandshof in Hamburg, with the help of parts Ratimo had made to order.

As a starting point for your own construction projects, the gas station (73240) of Luetke Modellbahn is also a good place to begin. It has a station to the model and has besides the cash room on the left and also a wash hall, and on the right a larger workshop with tire service and lifting platforms. All these are more modern features that fit well into the seventies and eighties.



The hatchway kit was adapted and redesigned for Dirk Rohwerder's plant (picture above). Now it bears the colours and design of Aral. The associated supply vehicle (picture below) was also made from a Märklin model and a trailer from A2 Models, but still bears the labelling valid before 1982. Photo below: Dirk Rohwerder

It could be changed or “flagged” to one of the well-known brands without much effort. A suitable colour scheme and topping the mast with the encircled T do wonders here. Dirk Rohwerder has also demonstrated this very impressively: He reduced the size of his kit by the larger hall and left it at the maintenance and washing halls.

An ultramarine blue support column for the roof above the columns and equally blue doors on the sides mean that even without a logo notice, every viewer can immediately see who is selling fuel here. As with the models of that time, however, he also placed a pole with the Aral diamond on the street.



Another aspect of crude oil logistics is addressed by Faller with the Aral tank farm (282747). Here, the focus is on the storage of fuels and lubricants and their loading onto road and rail. Photo: Faller

At the end of these observations, one facet remains that we have not yet considered at all. Many years ago, a tank farm and a filling plant for tank wagons and tank trucks were already included in the Faller programme.

In 2014, the Gütenbach-based kit producer has reissued this polystyrene classic and combined it to form the “Aral Tank Farm” (282747). In its modern, distinctly lighter shades of blue, this facility, including warehouse and rolling tire drums for lubricating oils, cannot be overlooked as an eye-catcher.

Manufacturer of the model new products:

<https://www.maerklin.de>

Company addresses of the market overview:

<https://www.a2models.nl>

<https://www.faller.de>

<https://www.luetke-modellbahn.de>

<https://www.mbz-modellbahnzubehoer.de>

<https://viessmann-modell.com/kibri>

3D printing, labels and materials:

<http://www.etchit.de>

<https://www.modellbahndecals.de>

<http://www.oesling-modellbau.com>

<https://www.revell.de>

<https://www.tamiya.de>

Replica of the Gasoline filling station:

<http://www.z-lights.de>

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Fuel supply in retrospect

In the beginning there was coal

In order to be able to correctly classify the Märklin model of the tank car and the road vehicles also presented in the previous article, we also take a look at the prototype. The development of the filling station chains with a focus on the Aral brand, which is the leading brand in Germany, should help us to understand how gasoline and diesel fuels reached their current importance and the supply of land became more important.

No matter which epoch model railroaders turn to, there is probably no way around tank car and car models after the end of epoch I. While motor vehicles were still a rarity on German roads in the twenties of the 19th century, their importance grew rapidly with the advent of the first trucks and buses.



Mineral oil transport was and still is a popular topic among model railroaders. Tank car trains (here the gag 65680), here pulled on 23 March 1976 by 042 106-5 and coming from the Emsland oil refinery, we also find as model reproductions. Photo: Peter Schiffer, Eisenbahnstiftung

Those who could afford it switched from horse and carriage to the modern form of mobility. Then as now, the car was above all a status symbol. And with the increasing number of cars, fuel supply also gained in importance.

In the beginning, fuels such as petrol or kerosene could only be purchased in pharmacies. The first “filling station” in the world in this sense is now the Stadt-Apotheke (City Pharmacy) in Wiesloch (Germany),

where Bertha Benz acquired the light petrol Ligroin (white spirit) during her overland journey by car from Mannheim to Pforzheim in August 1888.

However, with the increase in motorised traffic, tanks were eventually set up, often at car repair shops, locksmiths and blacksmiths, who maintained or repaired the vehicles, in equal measure. The pumps used there were usually simply barrel pumps, as they had also been long used in garages.

Later, high hand pump columns appeared, with petrol tanks embedded under the road or pavement surface. These were often found in restaurants that sold fuel as a side-line business and invited the car excursionists of the time to stop by.



This BV-Aral petrol station from 1924 gives an idea of the first stations that emerged in this decade. Diesel fuel was not available there at that time. Photo: Historisches Archiv BP/Aral



As shown here in 1942 at the Engelsburg restaurant, petrol was often sold as a side-line for many years: Anyone who had to fill up the tank of his car on a weekend trip, stopped here. Photo: Historisches Archiv BP/Aral

In those pioneering days, the petrol stations initially offered light petrol and kerosene, later benzene and petrol. Diesel fuels were by no means part of this, because compression ignition engines were only found in (a few) trucks and these were usually filled up at a depot.

A first unit type for a so-called “large filling station” was developed in 1917 by the Standard Oil of Indiana. Behind the term is a fuel dispensing operation as we still know it today in its essential features: The fuel dispensers are located under a free-standing canopy roof that protects personnel, vehicles and customers from rain.



An early representative of the large filling stations is this BV-Aral station in 1939, with its high hand pump columns and the still outdoor lifting platform for work on vehicles. Photo: Historisches Archiv BP/Aral

The cashier's office is a few metres away and is glazed towards the columns. A price pole is set up so that the daily prices can be seen from the street. The first filling station in the German Reich according to a comparable scheme was opened by OLEX, a predecessor company of Deutsche BP, at the end of 1922 at Raschplatz in Hannover (Hanover).

Following the American model, the station was filled with petrol by means of a nozzle instead of canisters, and from August 11, 1927, OLEX was also operating in Hamburg. It was not untypical for the time before the Second World War that fuels of different brands could be bought at one and the same filling station. The brand specialization and strict separation is a development of the post-war period.

The cornerstone for this had already been laid by the forced management of fuels during the Third Reich. The resulting brands had now become meaningless due to the form of the levy under state control, and the crude oil companies could in fact no longer appear under their own name.

Leuna had previously taken on a special role, being marketed as Deutsches Benzin (German Gasoline) in a business heavily dominated by foreign companies. I.G. Farben marketed this gasoline, initially produced in the Leuna plants, mainly through its own marketing company, Deutsche Gasolin Aktiengesellschaft.



Bundesarchiv, Bild 111-098-075
Foto: o. Ang. | 1936

Leuna took on a special role in the German petrol station market, with its red-and-white glowing stations dispensing fuel obtained by hydrogenation of coal. In 1936, this filling station also clearly advertised the term "Deutsche Benzin" (German gasoline). Photo: Bundesarchiv, Bild 111-098-075 / CC-BY-SA 3.0

From 1936, however, this gasoline was also produced in hydrogenation plants in other coal liquefaction manufacturing processes, as well as by other companies that did not belong to I.G. Farben.

Crude Oil Transport

For decades coal was and remained the most important fuel, not only in Germany. As a classic bulk cargo, its transport by rail was almost without alternative.

At times, open wagons represented the highest number of units of all wagon types.

The situation was quite different with tank wagons, in which other goods in liquid aggregate state were also transported.

If we restrict our focus to petrol, benzene and diesel fuel or heating oil, they together fell far behind coal.



The view over the Esso tank farm in Hamburg in 1959 shows not only a company-owned mineral oil tank car but also a lot of rolling tire drums that were stored in the open air. Photo: B. Rieckhoff, Eisenbahnstiftung

If we were able to look at long trains of open wagons, tank wagons were at best reserved for groups of wagons in mixed trains. This changed with the Second World War, because now fuels for the war effort had to be transported in large quantities and over long distances.



In 1956, tank cars were also delivered individually to customers, if necessary, also by road. Here, an Esso private wagon is loaded onto a DB road scooter unit at Hanau station (photo above). Like all big brands, Aral once had its own tank cars, here two two-axle and one four-axle version. They were mostly grey, some probably blue, painted and also had brand name plates set in the late fifties. BV-Aral was finally the first company to give up its own fleet of tank cars and rely entirely on rented cars. Photos: Historical Archive BP: Paul Trost, Eisenbahnstiftung (Photo above) / Historical Archive BP /Aral (Photo below)



This is what petroleum transport looks like today: 145 020 of the RBH will be on the road with modern articulated tank cars from four different manufacturers near Nordheim/Württ. on 3 September 2019. Photo: Zeno Pillmann, Eisenbahnstiftung

After the war, mass motorization with the Volkswagen (Beetle) quickly set in, which led to an increasing demand and also to higher transport volumes. The number of tank wagons increased, larger new wagons became necessary, and, at some point, entire trains of these types also became part of the everyday life of the railway.



In the sixties there were also still quite small tankers on the road to supply the filling stations, as we know these tankers today from heating oil deliveries. Photo: Historisches Archiv BP/Aral

At the same time, however, it was also necessary to guarantee the transport from crude oil depots (and sometimes refineries) to the filling stations and heating oil suppliers emerging from former coal traders. This was usually done with tankers, which soon grew in size.

In the beginning, there was usually a removable tank on the platform of the two-axle truck, which could also transport coal to customers when the tank container was not needed.



As demand increased, so did the amount of fuel to be transported. Tankers therefore also made use of the weight permitted by the registration regulations. In 1965, articulated lorries, like this Mercedes short bonnet with Aral semi-trailer, had long been commonplace. Photo: Historisches Archiv BP/Aral

The simple, two-axle trucks were then converted into trailers on tractors that could drive larger quantities and, in divided chambers, also different types of fuel. After the war, trucks were initially in a class of usually only two to three tons, but today heavy 40-ton trucks are standard.

The market development after 1945

When the economy picked up again after the war, the number of motor vehicles increased rapidly, as described in the previous section, and the network of filling stations also grew enormously. All companies and brands were striving for the highest possible market shares and tried to create unique selling points and high recognition value.

This was the birth of the modern branded petrol station. Now the task was to create easily identifiable brand emblems, to cover the company with the clearest possible company colours and also to create ad design for the petrol stations' architecture of the future that was as independent and uniform, as possible, throughout Germany.

Customers were wooed with a clean and courteous appearance, good service and small promotional gifts. At that time, the range of petrol stations, included not only fuels, but also lubricating oils, spark plugs and accessories, and, sometimes, also tyres.



Hand pump columns, here at a BP station, disappeared quite quickly after the Second World War and made way for more modern types of construction. Photo: Sammlung Ulrich Biene

Connected to a petrol station was often a washing and maintenance hall, and a workshop was rarely found. On site, windows were cleaned, oil level, radiator water level, the antifreeze content of the coolant and the air pressure of the tyres were checked, the lighting was checked, and defective lamps were replaced.

At that time, fuel dispensing was still inclusive of service, the term "tank attendant" refers to his function of maintaining the tank, i.e. refilling fuel and collecting the amount due for it. In 1952, this became an apprenticeship in the Federal Republic of Germany with a three-year training period.

When the market was saturated, a shake out set in. Familiar brands disappeared overnight after companies were acquired and the nets were re-flagged. Size was now the key to being able to withstand the incipient price pressure and maintain the necessary structures. Customer service, which had previously been a matter of course, increasingly receded, and promotional gifts and awards for brand loyalty lost their importance.

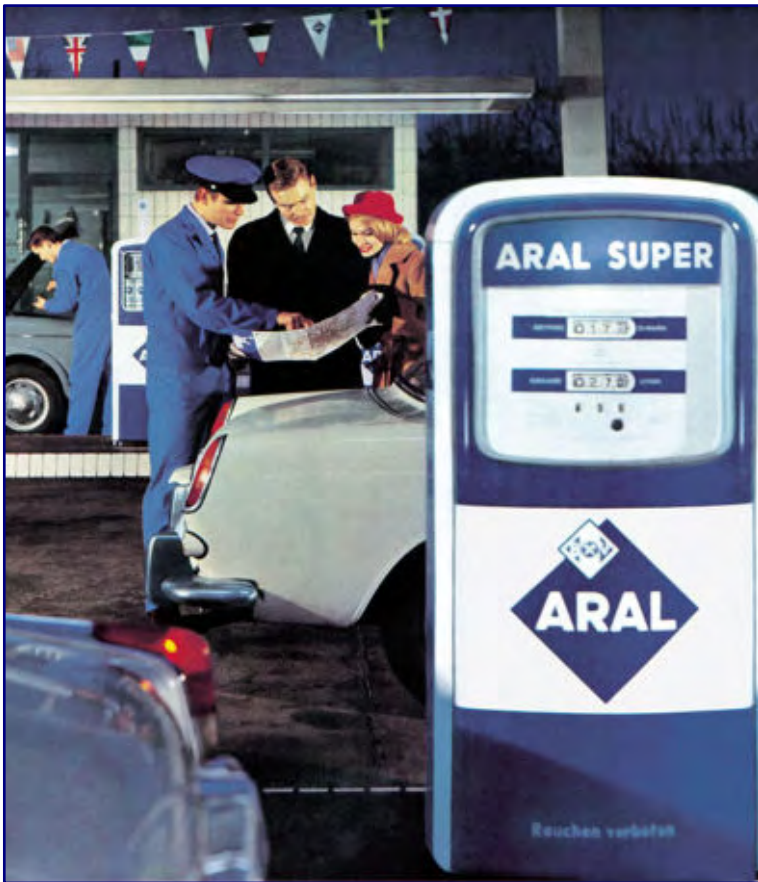
continued on page 31



Filling stations connected to restaurants, as here in Soest, also disappeared visibly. A similar fate befell many once well-known brands, such as Rheinpreußen here, which were lost through mergers and takeovers. Photo: Sammlung Ulrich Biene



Tank attendant became a three-year apprenticeship in the Federal Republic of Germany. Here we see a further training course for tank attendants at Esso, from a time when a clean uniform was still part of the obligatory job description. Photo: Sammlung Ulrich Biene



In 1964, customer service at German petrol stations was still a top priority, as it was at Aral. Photo: Historisches Archiv BP/Aral

And so, on the customer side, too, there was a long-lasting but constant change in thinking. Customers became increasingly price-sensitive and, as a result, also more independent. Cars were gladly maintained and smaller repairs were sometimes carried out by the customers themselves. Successful book series like "Jetzt helfe ich mir selbst" (Now I help myself) reflect this trend.

The filling station business began to change, familiar sources of income disappeared and even earnings from the core fuel business fell. Free filling stations and self-service stations emerged, and served exactly the needs of those times.

Instead of a filling station attendant, a motor vehicle mechanic was now needed to be able to do important business alongside the petrol pump.

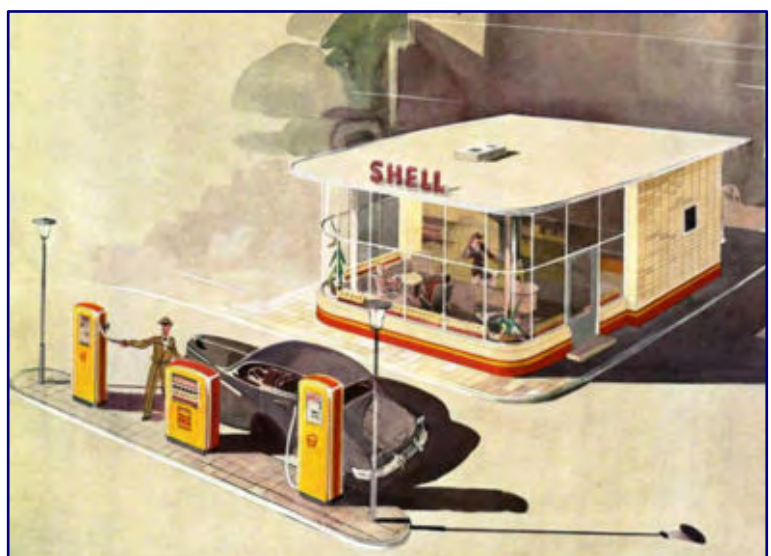
More and more frequently, repairs were now being carried out at the filling stations or used cars were being sold. The number of petrol station brands dropped to hardly more than a handful, and the number of stations also fell, with the remaining ones tending to become larger.

If we look at the current sales offices, it is easy to get the impression that the sale of petrol and diesel fuels has been assigned a secondary role.

The former refreshment rooms have disappeared; instead, the cash desk area gives the impression of a small supermarket. Here you can buy beer and spirits until late at night, and in the morning warm rolls are waiting for early risers.

If you want to build a filling station on your model railway, you should therefore familiarize yourself with these developments and choose and design it to suit the chosen era.

In any case, the topic is attractive, because hardly any other place allows bringing together so many everyday cars and dream cars in a small space.



A company-specific architecture was also characteristic of the post-war history of petrol station chains. At first, Shell did without a canopy and let the glass front of the cashier's office take effect for itself. A typical stylistic element was the lanterns that bordered the tank island. Photo: Sammlung Ulrich Biene

Aral – a German brand

The Aral brand, which has been the market leader in Germany for decades, has played and continues to play an important role in the development described. Since 90% of Germans spontaneously know the name Aral and can describe the trademark, the so-called diamond, it is worth taking a short look back at the history of this Bochum-based company, not only with regard to Märklin's tank car innovation.



The change of the Aral trademark in the course of time: The oldest version (far left), contrary to the proof, dates from 1918 and was changed to the Bochum city colours in 1927 with the introduction of black and yellow road signs. The most important version for the majority of model railroaders is the one from 1952, in which an Aral logo appears for the first time. Photo: Historisches Archiv BP/Aral

In 1924 the chemist Walter Ostwald had developed a novel and engine-friendly fuel from a benzene-petrol mixture and christened it B.V.-Aral. It consists of the two initial letters of the chemical group of aromatics (benzene) and aliphatics (petrol). 85 years later almost every German knows this brand name.



The company has been based in Bochum, a city in the Ruhr area, since its establishment in 1898. Here, you can see the historical main building of the sixties with a brand name filling station in front of it. Photo: Historisches Archiv BP/Aral

However, the history of the company is even longer: On 28 November 1898, 13 mining companies founded the West German Benzene Sales Association in Bochum, which later became the Aral Aktiengesellschaft.

Its task was the marketing of the benzene obtained from the coking of hard coal, e.g. for the production of paints or also for lighting purposes. From 1918, after restructuring, the company finally traded as Benzol-Verband, or B.V. for short.

The B.V.-Aral, developed in 1924 and already mentioned, is the first super fuel in the world, which also helps to promote the accelerated development of the company's own filling station network from 1926. Just one year later, the B.V.-Aral changed its corporate colours from black and yellow to the Bochum city colours blue and white. The reason for this is the newly introduced traffic signs in Germany, which would otherwise have created a risk of confusion.

In 1929, lubricants were also added to the range, and one year later Aral dyed its fuel in the corporate colour blue - a measure to protect the brand. From 1932, B.V.-Aral also offers diesel fuel for the first time. At the same time, the first mobile car test benches are used for free adjustment of the carburetors.

Competition comes to a standstill with the introduction by the state in 1939 of the compulsory management of standard fuels. Now only quantity counts to keep the war machine running. Increasingly, petrol is now also produced from coal, instead of oil.

It was not until 1951 that the fuels could again be offered under their own name. The refineries are now gradually converting their processing operations back to oil. Nevertheless, Aral had already introduced the first electrically operated petrol pump in Germany two years earlier.



Symbol of German wanderlust in the economic miracle: A Mercedes-Benz W120 stops at an Aral filling station in Kirchdorf / Tyrol to refill fuel. Aral is already an international brand in these years. Photo: Paul Walde (1899-1970); in the public domain

In 1952 the name Aral appeared in the company name for the first time. Kohlenwertstoff AG is broken up and B.V.-Aral Aktiengesellschaft is founded as a successor company. Between 1956 and 1961, the filling station network is expanded considerably, increasingly, also in other European countries. In 1968 it finally reaches its peak with 11,000 filling stations.

When Aral withdrew from the Benzin-Benzol-Verband (gasoline-benzene association) in 1962, the letters B.V. were also dropped from the company name. The company was now called Aral Aktiengesellschaft. The sales products receive new designations: Aral is the regular petrol, all others are prefixed with the brand name, like "Aral Super" for the super fuel.



Aral took account of the market development towards self-service petrol stations from 1969 onwards. Here, we see a station with this offer and the reference to the cheaper price in the seventies. Photo: Historisches Archiv BP/Aral

New services are being added to the range, including car care or the small shop in the ticket office. Aral responded to the trend towards self-service from 1969 with the first self-service filling station: customers received a discount of 2 pfennigs (approx. 1 euro cent) per litre, if they do not need to be served by the filling station attendant.

Increasing price competition caused the network of filling stations to shrink from 1970 onwards and favoured concentration on large filling stations. Aral also maintains its market position by taking over Gasolin, which suddenly disappears from the market in 1971. The Aral brand, under which all petrol stations will appear in future, is given a new design.

This is followed by the introduction of the trademarked Aral font, which also characterizes the brand lettering in the new "Diamond", from which the suffix "B.V." now finally disappears. The company colours are brighter and, above all, in 1973 are made binding. Blue sky and white clouds flow into the advertising presence.

In the same year, Germany is also hit by the “oil shock” as a result of the first oil crisis. Sunday driving bans are suddenly part of the agenda, and Aral feels compelled to look for alternatives to oil, and to push ahead with his research. By the time it celebrated its 100th birthday in 1998, the company is finally able to present some innovations and pioneering achievements. With still 2,418 stations, Aral remains the market leader in the third largest petrol station market in the world.



The extent to which petrol stations have also changed at Aral is easy to see in comparison with this modern station: The brand image has been further developed and adapted to contemporary tastes, but above all, petrol stations today are usually more distinct than thirty or forty years ago. The range of products on sale in the cash registers has come much closer to that of bakeries, cafés and supermarkets in terms of size and range. Photo: Historisches Archiv BP/Aral

Aral has also experienced shifts or changes in its shareholder structures in the course of its company history. We have only touched on a few of them briefly. The takeover of all shares by VEBA OEL AG in 2000 was significant. The new company name was now Aral Aktiengesellschaft & Co. KG.

But this was only of short duration: on 1 April 2002 Deutsche BP AG took over 51 % of the shares in VEBA OEL AG and on 1 July the remaining 49%. Aral was therefore integrated into the BP Group. The resulting crude oil company had to sell four percent of its market share, on condition that it was sold by the EU.

Because Aral was and is the market leader in Germany and enjoys a high level of brand awareness, all stations in Germany were rebranded to Aral by 2004. The BP brand disappeared from the petrol station business, but remained active in the aviation fuel sector, for example. Similarly, the service stations in other European countries now operate only under the BP brand.



Aral's modern brand image continued to exist even after the takeover by BP. The modern shades of blue and white are reminiscent of a cloudy sky, as this tanker truck impressively demonstrates. Photo: Historisches Archiv BP/Aral

The German BP headquarters is located at the Aral headquarters in Bochum and construction begins on a new office building to accommodate the increased tasks. In 2006 it is ready for occupancy and not only national business activities, but also parts of the European divisions have been based in Bochum since then.

A short review of Gasolin

Gasolin (a company name, not the product) was a German petroleum company with its own filling stations from 1920 to 1971. From 1926 onwards, it was primarily intended to market the synthetic Leuna petrol produced by Leunawerke. The fuel obtained via coal hydrogenation was successfully marketed as "Leuna Deutsches Benzin" (Leuna German gasoline).

In 1929 Gasolin was one of the big five oil companies operating in Germany by balance sheet totals, and in 1935 also by number of its filling stations. The consequences of the beginning of the war and its forced management were drastic.

From 1943, the Deutsche Reichsbank (German Central Bank) in Berlin had been Germany's only central securities depository bank where the company's shares were deposited. At the end of the war it was in the Soviet sector.

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Two examples of Gasolin: Large filling station in Cologne at night (picture above) and in Wuppertal-Elberfeld according to company architecture. These included the well-kept landscaping strip along the street and the mushroom-shaped supporting column of the canopy. Just as typical were the oil cabinet between the petrol pumps and the display on the street. Photo: Collection Ulrich Biene



Although Gasolin became an independent gas station company in the West (registered in Berlin-Charlottenburg) with the breakup of I.G. Farben in the West, with the loss of its eastern holdings, the shares were no longer accessible. Leunawerke, previously the largest supplier of gasoline, was also located in the territory of the later DDR.

With the Securities Settlement Act of 1 October 1949, Gasolin's shares were declared invalid and replaced by a global certificate. The demonstrable owners became shareholders again and received ownership of this global certificate.

In the mid-1950s, the market share of foreign petrol station chains in the Federal Republic of Germany was about 40 %, and the share of the large German companies about 36 %. To strengthen the German share of the petrol station market, Wintershall and DEA became co-owners of the BV-Aral association in 1956, contributing their distribution subsidiary NITAG (Wintershall), their petrol stations (DEA) and their respective shares in Gasolin.



Photo above

The Büssing LS 11 pulls an already quite modern trailer with a four-chamber tank that could hold a proud 17,000 litres of fuel.

Photo on page 37:

The Gasolin bullis (nickname for the VW bus) with an oversized record oil dove on the trailer were used as advertising media for Gasolin. Photos: Sammlung Ulrich Biene

Subsequently, NITAG with its approximately 800 filling stations was merged with Gasolin with approximately 2,000 filling stations to form Deutsche Gasolin-Nitag AG. This meant that the NITAG brand had finally disappeared from the market.

As was already the case with other brands, the interests of the shareholders and major shareholders of Gasolin were later changed by market pressure. As a result of such developments, Rheinpreußen was also absorbed by DEA, which in turn was later swallowed by Texaco. Decades later it was re-established and soon afterwards merged with Shell.

It was similar with Gasolin, which was integrated into the Aral in 1971. Within only a few months, all petrol stations were then re-flagged from red, white and black to Aral blue. The Gasolin brand seemed to have suddenly, and surprisingly quickly, disappeared from the everyday picture. However, the Gasolin refinery in Dollbergen had already closed in 1969.

Here too, consumer interest in the Gasolin brand had not diminished, but the increased competitive pressure in the German petroleum industry, which had intensified considerably in the 1960s, had taken

its toll. After years of economic boom, fuel sales have been declining since the 1960s and the price war instigated by the first independent petrol stations has increased the cost pressure on the companies.

Official website of Aral AG:

<https://www.aral.de>

Historical Archive of BP / Aral:

<https://www.aral.de/de/global/retail/ueber-aral/geschichte/historisches-archiv-bp-aral.html>



Like a small theatre

Theatrical layout design

In addition to a small installation and some dioramas from our circle of readers, this year we have submitted our own proposals for projects of comparable size. With this issue, our annual theme "Layout diversity" comes to an end. Our editor Dirk Kuhlmann uses this closing point to promote an effective idea and also shows well-known showpieces that follow this concept.

By Dirk Kuhlmann. On your visits to various model railway exhibitions and trade shows you may have noticed already a trend towards small Z-scale layouts featuring a picture frame or "shadow box" design with a display area of perhaps 60 x 20 cm.



Miniature theatre stage in the living room: The impressive diorama "Le Viaduc sur la Valeé" is an eye-catcher in Dirk Kuhlmann's dining room and captivates every visitor.

Inside and under bright lights, highly detailed landscapes on a scale of 1:220 can be found. Of course, many visitors might think at first that they are witnessing the inglorious reincarnation of a TV set from the cathode-ray tube era - but far from it!

The "Marienfeld" layout by Dietmar Allekotte, for example, has attracted quite some attention at recent exhibitions. At present, it is the "Kohlenkiste" layout by Manfred Forst, a highly topical representative of this genre, which is going on an exhibition tour.

The charm of these types of layouts lies in their concentration on one specific theme within the smallest possible space, without sensory overload, but with a corresponding selection of rolling stock that complements the scenery. They often consist of a continuous run track oval with or without a staging

yard, and are sometimes operated by an automatic shuttle train controller. What are the other advantages of such a box?

First and foremost, it allows the inclined hobbyist to build and finish a presentable model railway layout within a reasonable period of time. It is actually also the right type of project for a beginner or somebody getting back into the hobby.

And for those of us having already a larger layout with a, let's say, German Federal Railway era III theme, a small picture frame layout would allow us to dabble in other eras or even areas of the world. Moreover, such a jewel should surely find its place in the living room without causing a major outcry of our spouse.



Another peep-box as a decorative object: The narrow "Süderheidetal" even attracts attention with an automatic shuttle train.

In order to further showcase all the advantages of this type of theatrical layout design, I present you with a special picture frame layout design project in this Christmas edition of **Trainini®**. For this we are leaving Germany and go to the Northeast of the United States. But don't worry, the track plan and the proposed scenery can be used universally.

For example, a German "Wupper River Valley" theme would also go well with this scenery. In addition, I will avoid the special jargon of the die-hard US model railroaders as far as possible. To make sure we are all visually involved in the game, you will find links to two videos and a website in the info box at the end of this article: So I recommend that you first have a look at that before reading on.



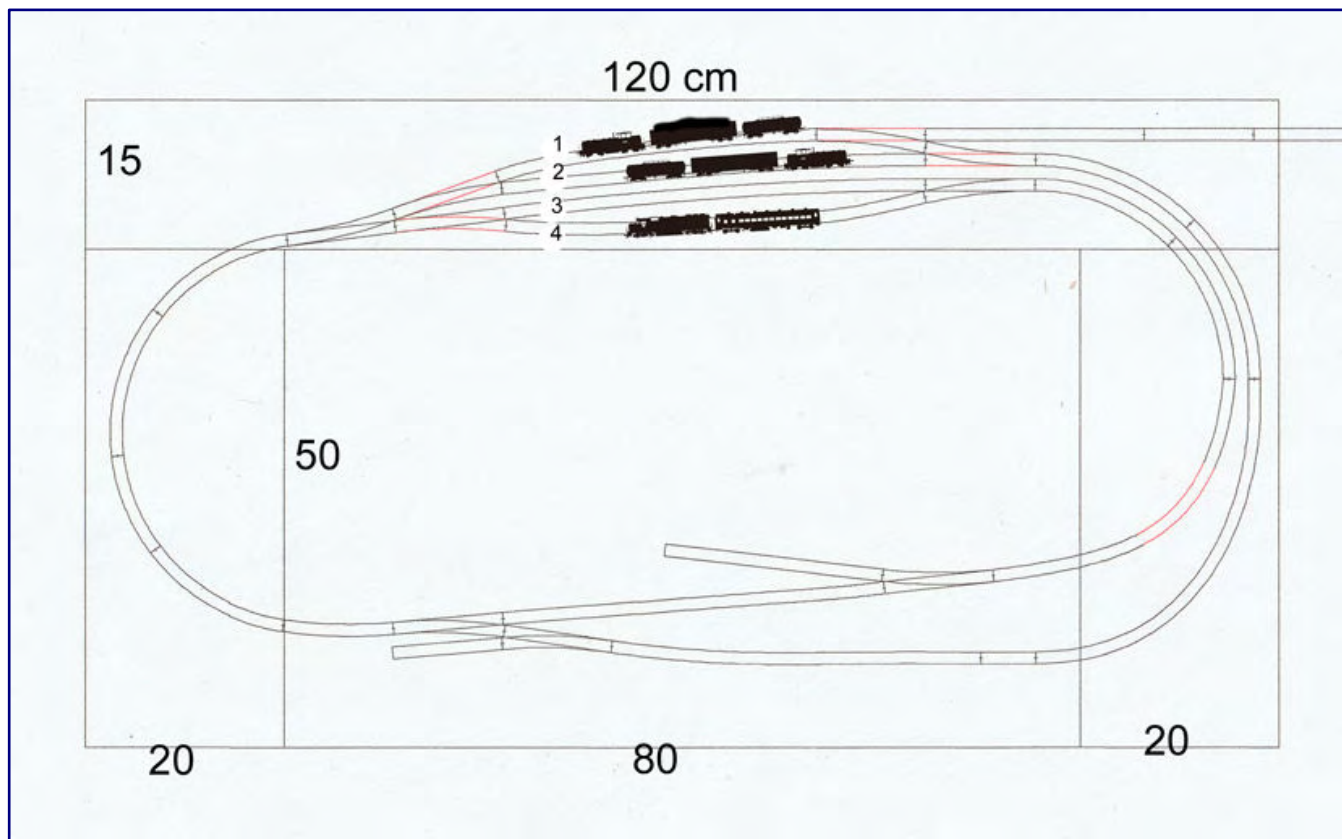
The viaduct diorama also allows for the use of American rolling stock. Here, for example, a Budd Rail Diesel Car (Budd RDC).

A North-American branch line theme

Many model railroaders usually connect the United States with ultra-long freight trains which are pulled by several diesel locomotives. That is great if you have enough space in your home or your model train club plays along. A reasonably realistic implementation of such a theme will easily require a layout measuring 5 meters in length or more.

However, modelling interchange or switching operations of a so-called “short line” is feasible and attractive even in the confines of a small space.

continued on page 45



Simple track plan (top) with the different sections measured in centimetres. Bottom: scenery proposal with 1 depot, 2 loading siding, 3 farm house, 4 industrial access.

Just imagine a situation with a remote branch line serving several industries. Usually, a shunting locomotive (“switcher”) with two or three wagons in tow will pass by (from track 1) and deliver materials to a company.

Sometime later the train will pull out of the industry siding without its cargo (from track 2), and without the model railway operator or his assistants doing any offloading! How does that work?

Well, it is an old trick, which just requires two sets of the same locomotives and wagons with identical markings, one with and one without cargo. In a non-visible area of the layout, the trains are started, controlled by an automatic system, and with a corresponding reversal of polarity.



Here is an example of suitable American rolling stock (photo above). These are products from AZL, MTL and open wagons from FR Freudenreich Feinwerktechnik. Some of them were repainted and re-labelled. As explained in the article, having one train loaded, and another identical one unloaded adds operational interest which will surprise spectators. We also found some typical American houses (photo below), amongst them in the middle a brand new item from Archistories, which is further presented in the news section of this issue.

For even more fun, our proposal includes two additional tracks in the fiddle yard (tracks 3 & 4). This allows us to operate additional trains, such as the weathered “Budd RDC” (shown on the viaduct photo above), the American equivalent of the German rail buses.

Finally, there is an extra behind-the-scenes siding which provides the possibility to connect the layout to an external staging area for additional trains.

The tracks are preferably sourced from MTL or Atlas, which come closest to the prototypical spacing of American sleepers. To ensure a high level of operational safety, a radius of 195 mm is used.

continued on page 47

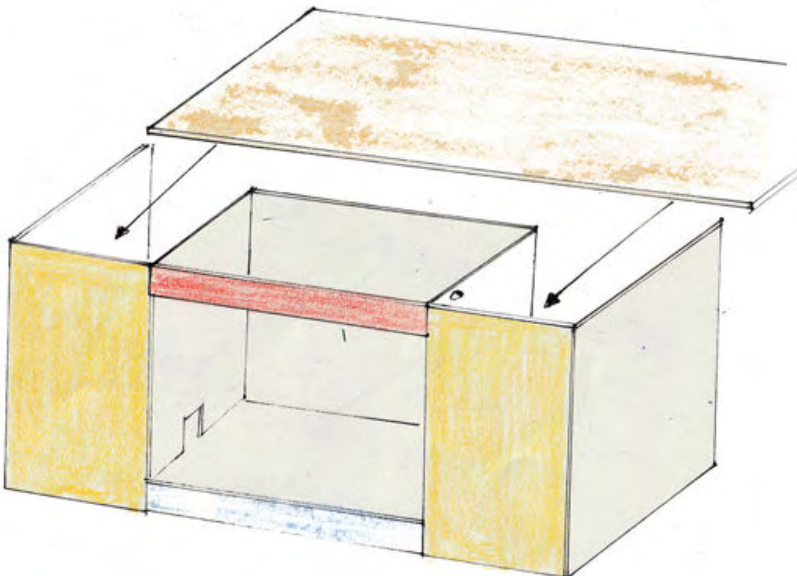
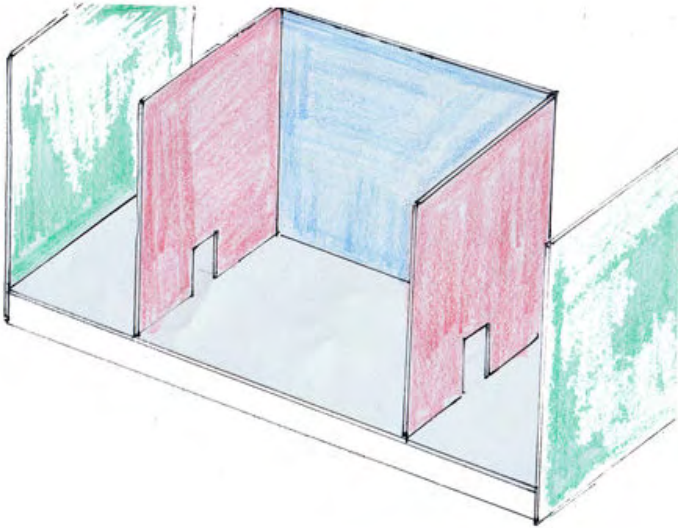
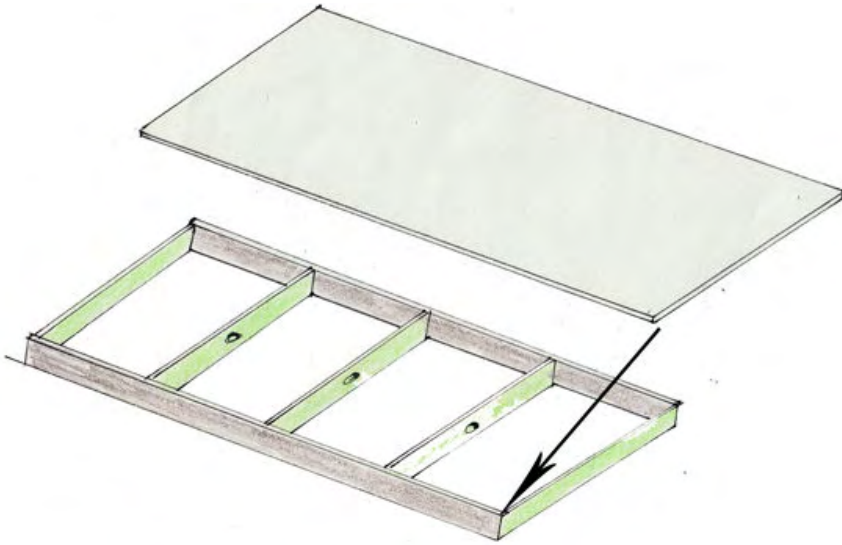


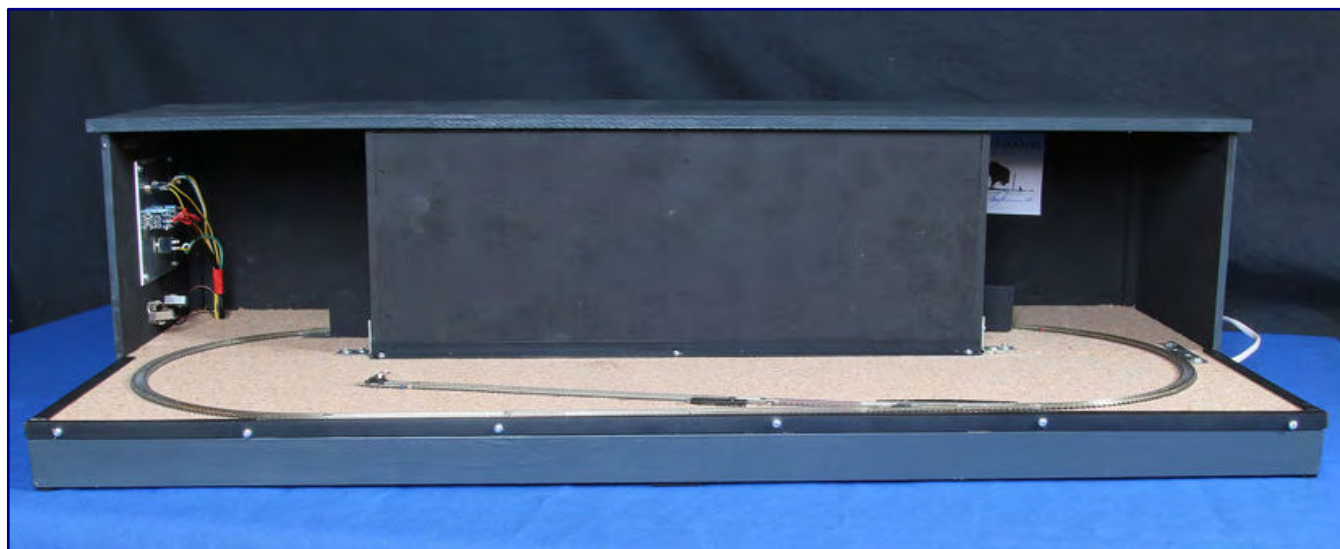
Diagram left:
The basic structure of a photo frame display layout.

Suitable buildings

The choice of available American Z-scale houses and other buildings is, of course, limited here in Europe. A look across the pond and at the pages of the dealer and manufacturer "Walthers.com" offers at least a first visual orientation (see also illustrations on page 45).

The brand-new series of US-American houses by Archistories (presented in the news section of this edition) is, however, more than suitable for our project.

A small platform is easily and quickly built from scratch and a search amongst various manufacturers will surely yield a small depot building. A large gate entrance with enclosing walls and a gatehouse are enough to hint at a large industrial plant.



Photos above and below:

Here you can see the basic construction of the "Catharinen Tief" and a view of the finished backside, which the viewer normally does not get to see or was not allowed to view.

The inclusion of only a few selected buildings provides us with sufficient space for some effective landscape design. Already the small valley with its river makes for quite some visual variety and interest.

Hoping to facilitate an easy start for such a project, I have also included some sketches for building the basic structure of a picture frame display layout. And, as always, a look at a diorama under construction is still the best visual guide.



Some inspiration for starting your own functional Z-scale diorama project: Landscaping work on “Le Viaduc sur la Valeé” diorama. On page 49 in our final picture you can see our author’s latest work entitled “Strandgatan,” which will soon have its exhibition premiere at the Lahnstein Model Railway Show (see below for further information).

As always, a suitable backdrop and an effective illumination of the scenery complete the showpiece.

Visit tip for the exhibition in Lahnstein
If you can sacrifice desire and a little time on February 29, 2020 or March 1, 2020, you should visit the “International Model Railway Exhibition” in Lahnstein.
Here, for the first time in public the swiss theatre stage “Strandgatan” in scale 1:220 is presented. Further information in the profile on page 49.

This brings us to the end of the third instalment of our small series of articles on layout design over the course of 2019. I wish you a lot of fun building your own “theatre stage”!

The **Trainini®** editorial team is of course available to answer questions from our readers, but I would also like to mention the 8th International Track Z Weekend in Altenbeken on 28-29 March 2020. Many of the small plants

mentioned will be on site and the individual builders will certainly also be there to give advice.

Let me finish by saying: “Ladies and gentlemen, please welcome to the stage...”

Video recommendations:
<https://www.youtube.com/watch?v=9t1qAMlhUHQ>
https://www.youtube.com/watch?v=4H10g_dmaUI
Website recommendation:
<https://www.american-rails.com/shortlines.html>
Author’s website:
<http://helenensiel.com>



Vormerken!

- 18. Lahnsteiner Modellbahntage
29.2. / 1.3.2020
10:00 - 17:00 Uhr (So. ab 11:00 Uhr)
Stadhalle am Lahnhofplatz
56112 Lahnstein
- des MEC Lahnstein-Koblenz
<https://www.mec-lahnstein-koblenz.de>
- mit Beteiligung von Dirk Kuhlmann und
seinem neuesten Werk „Strandgatan“

Shinkansen E6 Komachi

Outside wow, ...inside too!

Japanese high-speed rail has its special attractions: this type of transport has its origins in the technology-obsessed land of the rising sun. And it is here that even the most modern trains in the world seem to race along the elevated tracks. We take the model of the still very young E6 series and equip it with Rokuhan interior lighting.

The Shinkansen trains of the E6 series, which are now our demonstration object as a model, have been in service with JR East since March 16, 2013. Like the E5, which they resemble in their design, and with which they are coupled to run on partial routes, they emerged from the two prototypes called FASTECH 360Z.



JR East's E6 series Shinkansen, which can travel at up to 320 km/h on the Komachi line, is the latest development among the high-speed trains currently in service. On May 16, 2016, this train will be stretching its nose towards the photographer in Tokyo.

At the time of the first commercial use on the Super Komachi connection, the maximum speed was still limited to 300 km/h. Compared to the predecessor E3 (V_{\max} 275 km/h), however, they made it possible to reduce travel time between Tokyo and Akita by 10 to 15 minutes.



The Shinkansen E3, which was also in Tokyo on the same day, only ran at a maximum speed of 275 km/h. The six-piece set had the same space capacity as its successor, which was longer by a middle car.

In order to be able to drive coupled with the E5, it was necessary to increase the maximum permitted speed to 320 km/h, which was done one year after the start of these most modern Shinkansen trains. At that time the delivery of the E6 was completed, which meant that the E3 could be taken out of service.

The connection between Tokyo and Akita driven by the E6 was changed back from Super Komachi to Komachi with this complete change - the connection name is also found in the official product name of



Not all Shinkansen lines stop in Oyama and so the trains can be observed here on the middle tracks even when passing through at top speed. However, this double unit consisting of an E5 (rear) and an E6 (front), which could be stopped in full length, made an intermediate stop on 14 May 2016.

Rokuhan. Due to the high running demands on curves, the trains are equipped with a tilting technology in the prototype, which can tilt the car body up to 1.5° in the curve.

Like the E5, which also runs in regular service at 320 km/h, the E6 series has also been designed to be consistently streamlined. This leads to a 13-meter-long nose with a "duck's beak" on the end cars, which is intended to counteract the tunnel bang.

For this reason, the new trains need seven cars to achieve the same space capacity as the predecessors of the E3 series, which only had six cars. Rokuhan has taken this unit of an E6 from the prototype and divided it into a basic (end car and a motorised middle car; item no. T029-1 / Still 7297675) and an extension pack (four middle cars; T02-2 / 7297676).

The exterior design of the prototype and model is dominated by white car bodies with a silver-coloured decorative stripe and red roofs and fronts. The trains look sleek, attractive and very modern when entering the station.

View of the model

The models of Rokuhan look very successful: Shapes and proportions appear appropriate, the paintwork is clean and free of dust inclusions, and even the rather sparse labelling of the model can be easily read under a magnifying glass.

The only special feature we stumble upon is a decal sheet enclosed with the bilingual instructions (Japanese / English). It contains the car numbers for the two end cars, which are to be applied by the customer himself after cutting out and loosening in a water bath.



The logo of the railway administration, which Rokuhan also reproduces on his models, is emblazoned on the middle car between the two end cars.



The long and typical nose shape of the Shinkansen E6 has also been well met in the model. The individual car parts have short couplings, give a closed train image even in curves and also show the breaks in the otherwise closed image of the prototype apron.



The two train sets, in the picture the basic unit (Item No. T029-1 / Still 7297675), are delivered safely packed in plastic cassettes with the bilingual printed instructions (and here also the decal sheet for the car numbers of the end cars).

This does not pose a problem, but is also for Rokuhan rather unusual. The two end cars are absolutely identical in their shapes and so additional print templates were not necessary.

But do all five carriages really show different shapes, at least in the smallest details? These have been completely pad-printed.

If you already own the Shinkansen 500, you may find it a bit strange at first sight that the E6 carriages are quite short and have a rectangular cross-section.

But this is a correct reproduction of the original, because the layout is typical for all Shinkansen trains with the one exception - and that's exactly what Rokuhan once did.

Also the length of the car body does not reveal any shortening, which would probably also be superfluous on a scale of 1:220. This is shown by the conversion of the original dimensions and re-measurements on the end and middle cars: Rokuhan has kept all important original dimensions absolutely exactly! The cars of the similar E5 are also about five meters longer in the prototype.

Dimensions for Shinkansen E6 Komachi

	<u>Prototype</u>	<u>1:220</u>	<u>Model</u>
Length	23.075 mm (End wagon)	104,9 mm	104,9 mm
	20.500 mm (Middle wagon)	93,2 mm	93,3 mm
Height	3.650 mm	16,6 mm	16,8 mm
Width	2.945 mm	13,4 mm	13,8 mm

In order not to impair the comfort and space available to passengers, the high-speed trains have rectangular cross-sections and generous widths. This “trick” is necessary to ensure that the E6 Komachi Mini-Shinkansen, which also runs on converted old lines (see **Trainini®** 10/2014), does not exceed the permissible clearance gauge.

Placed on the track, the good impression is confirmed: the train gently starts moving as soon as it receives power through the track. In keeping with its role model, it is also suitable as a real “racehorse” that loves excessive curves.

The drive is again hidden in one of the middle cars - under the reproduction of the interior design. It is the E628-5 car equipped with a pantograph, which is of course included in the basic package. There a flat, but sufficiently strong bell-type armature motor, works via cardan shafts and worms on the gear wheels of all four axles.

Compared to the models of the Shinkansen 500, the E6 Komachi is however considerably louder in the higher speed range. We don't find this annoying, but it is also not to be ignored. A little grease on the worms (made of metal) and the actually self-lubricating POM gearwheels at least provides some relief.

Standard at Rokuhan is the double-sided current collection on all axles of each middle and end car. Since the wheel grinders that grip the axle tips do not produce any measurable rolling resistance, this seems astonishing when a wagon is unknowingly turned over and checked for the first time.



The separation strips and a few operating addresses are printed accurately on the trolley (photo above). Only the railway logo and the pictograms for car number, disabled access and 1st class (green cloverleaf symbol) are multi-coloured. The buffers and bumper plates, which lead to the apron cut-out on the prototype, have been reproduced as engravings, but not darkened. Typical for this train is also the mounting of the pantograph with surrounding wind deflectors (photo below).

It is a technical masterstroke to make this electrical connection on moving parts in such a way that contact is secure and at the same time low friction: If a wagon standing on rails is pushed lightly, it will continue to roll unperturbed, after being released.

In comparison to the three model variants of the 500 series so far, we have noticed another change with pleasure: Instead of yellow LEDs for marking the tip light - in Japan a dual-light tip signal - the E6 Komachi now uses cool white LEDs. They reflect the original prototype much better.

Installation of the interior lighting

Cold-white SMD light-emitting diodes are also used for interior lighting, which we want to install in the Shinkansen E6 Komachi for testing purposes. This requires seven circuit boards, because no part of the car should remain dark.

Technically, they are all prepared for this ex works, so a Rokuhan product can be installed without any problems and, as we would like to show, very easily. We have to get a total of seven lighting sets, which are needed in two technical versions.



Two interior lighting units for Shinkansen end cars (A030 / Noch 7297432) and five “Type A” (A009 / Noch 7297409) are required to illuminate the (equipped) interiors of the entire train. Motorised and non-motorised intermediate cars therefore use the same type.

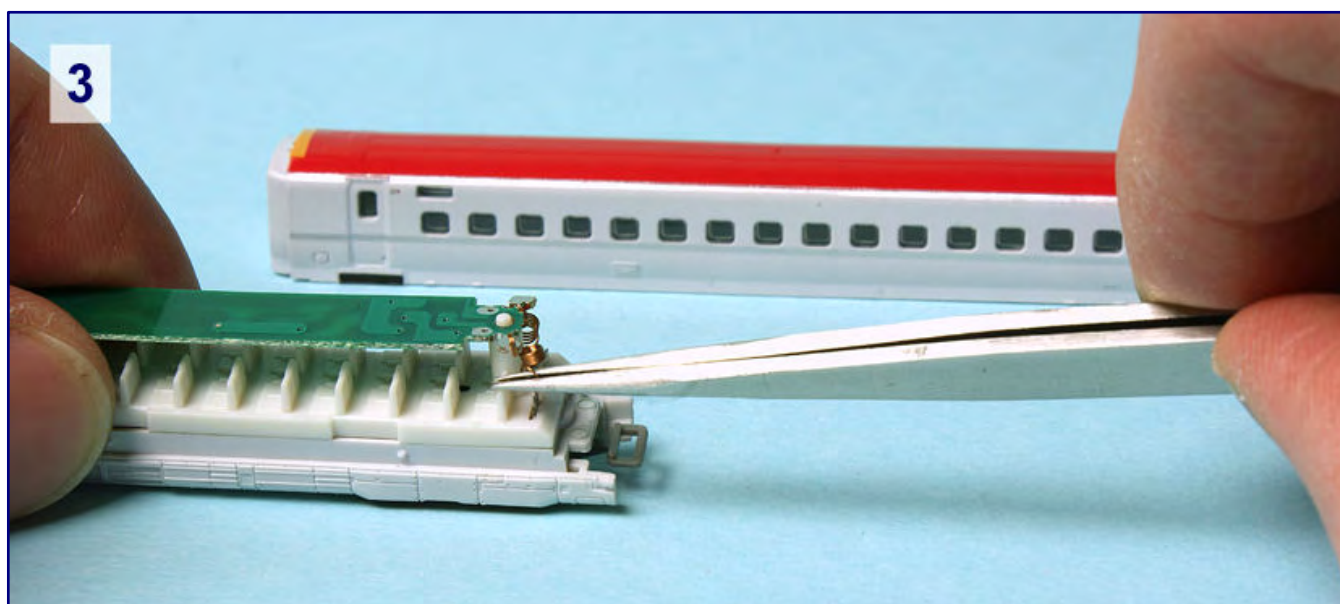
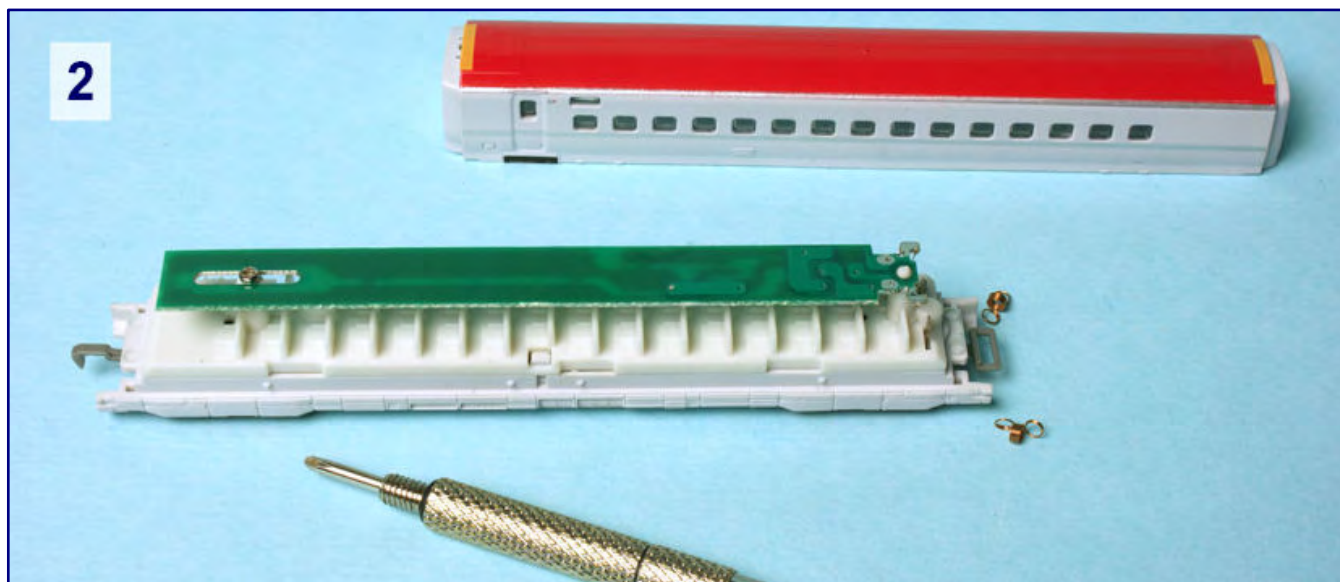
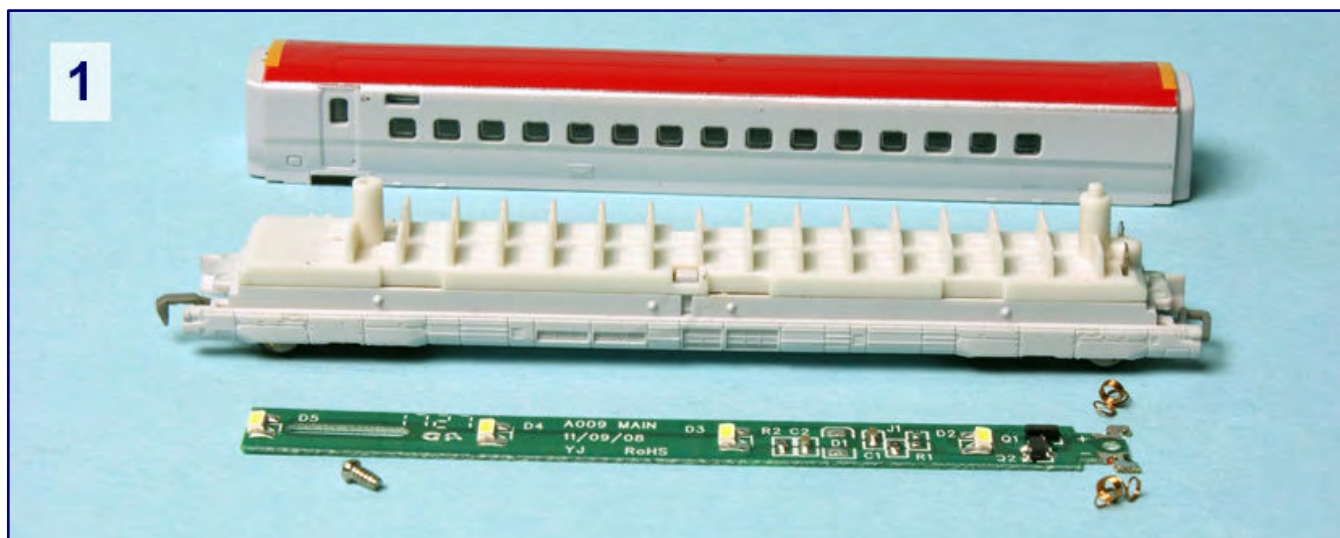
The “Interior Lighting Type A” (Item No. A009 / Noch 7297409) fits into the middle cars, of which we therefore need five, while the two end cars can only cope with the shorter “Shinkansen (Front) Interior Lighting” (A030 / Noch 7297432).

In addition to the actual circuit board, each package contains four tension springs and two small screws - only half of these are required for installation, provided no part is lost or damaged.

To complete the installation, we first have to separate the chassis and body of the vehicle part to be retrofitted, which means different procedures. This is explained in the instructions that come with the vehicle, not the interior lighting.

Fortunately, the pictorial representations here are very “informative”, because Rokuhan's instructions are limited to Japanese and English descriptions. We will start with the non-motorized middle cars of the supplementary package according to the procedure described.

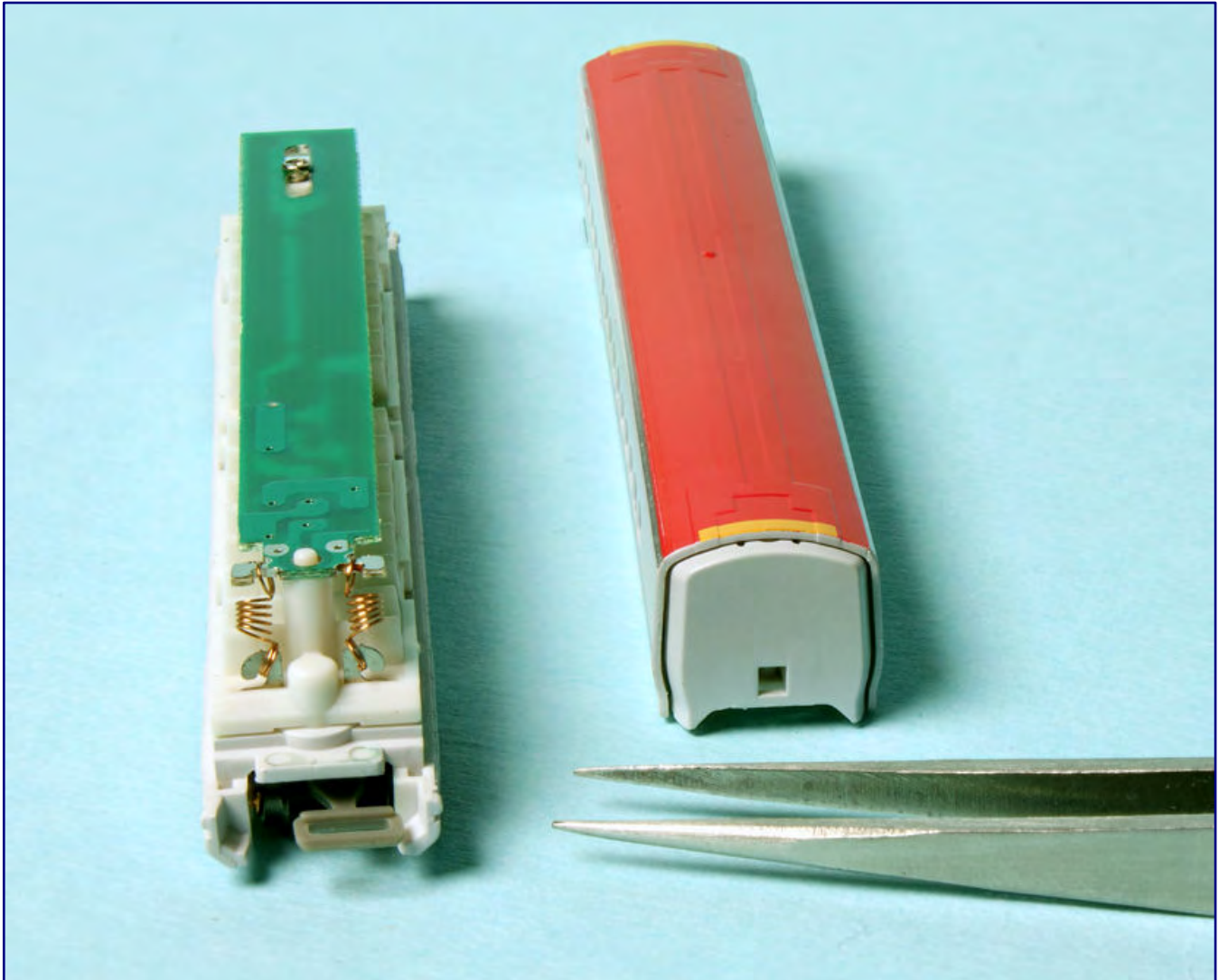
continued on page 57



Easy installation of the Rokuhan interior lighting in three steps: Getting the PCB, the two springs and the fixing screw ready, then spread and remove the housing (1), put on the PCB with the LED pointing downwards, put on the screw and tighten it, (2), then hook the two springs with the help of tweezers (3) – done!

With these, the body is only attached to the chassis with lugs. By slightly spreading the body, it can be pulled upwards, as we know it from Märklin bogie locomotives.

An interior design that reproduces correct 2+3 seating, but is too narrow in the model to place figures on the seats, comes to light. At first this seems to be an obstacle, because later on we will use the light to focus on the interior.

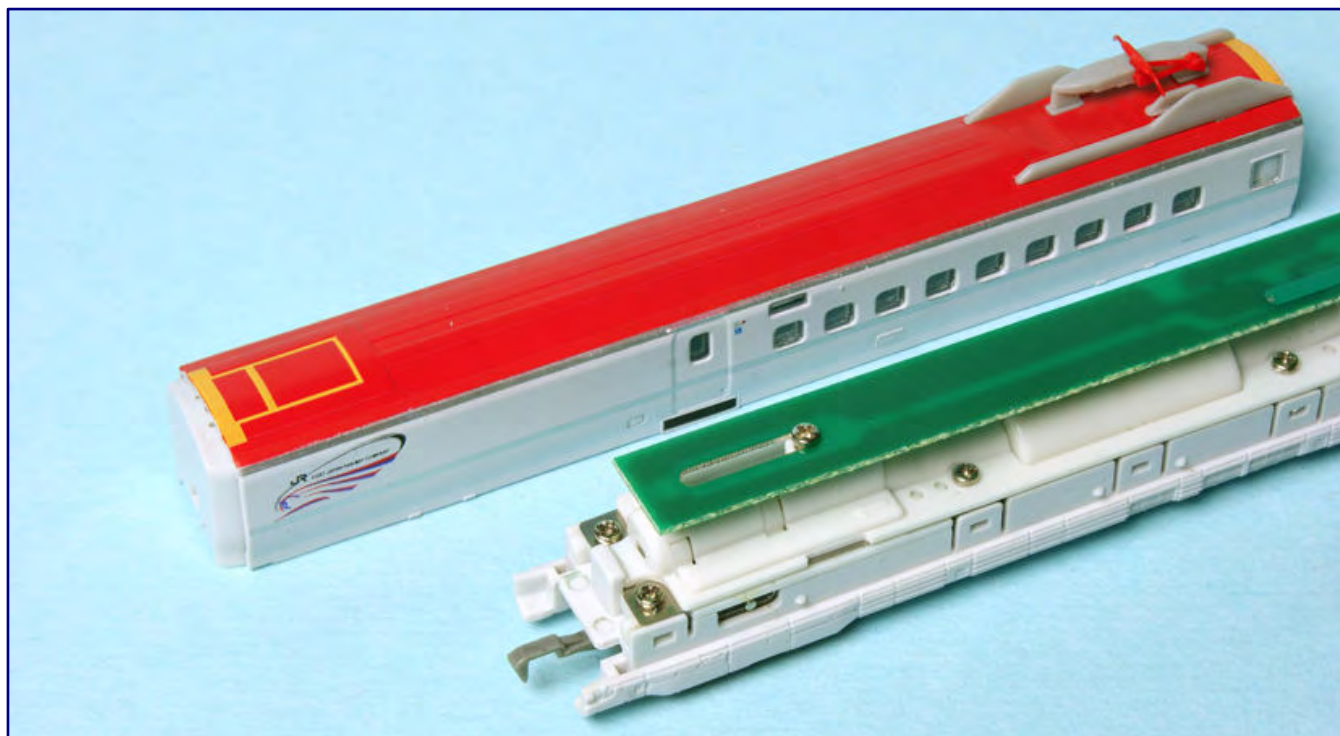
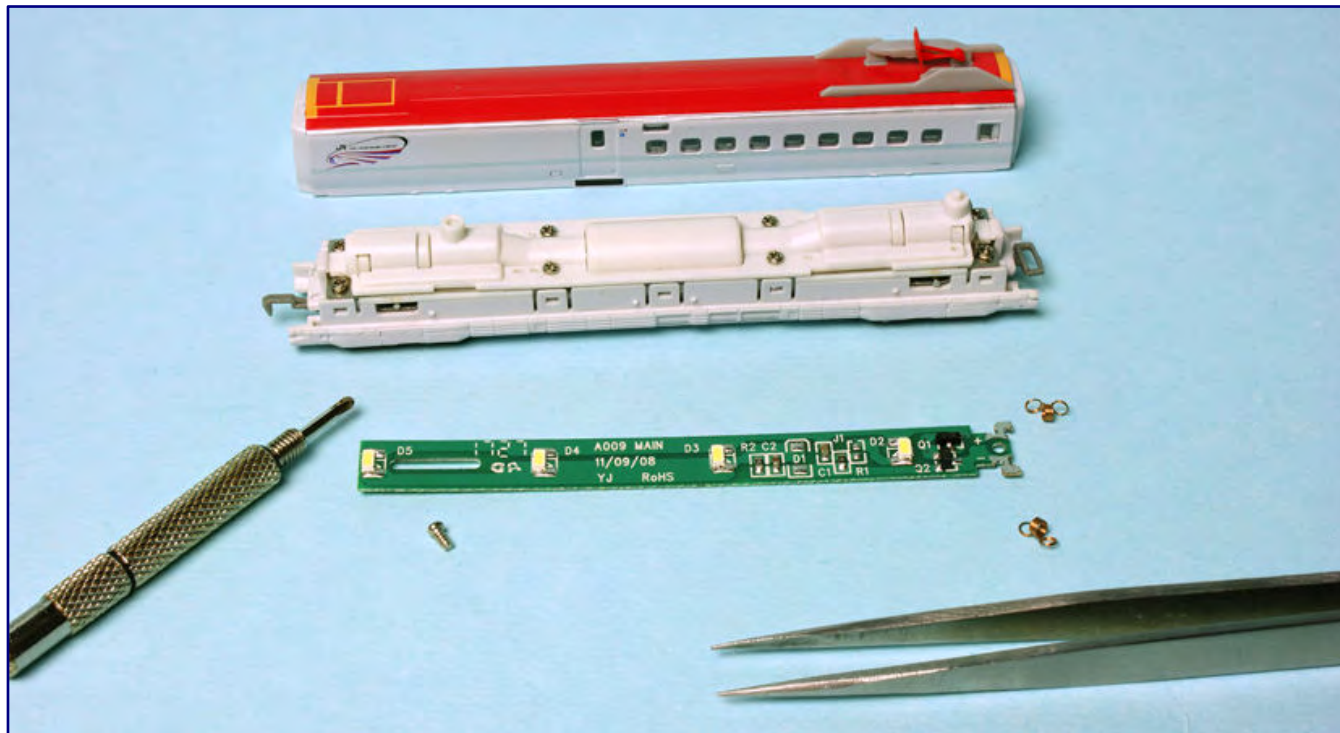


Simple, but effective: Here you can see how the safely suspended springs electrically connect the current collection of the chassis and the light board. The tension springs must not be overstretched during installation. There is no electrical coupling between the carriages. The carriages are connected again by a simple hook-and-eye coupling (in the picture the eyelet formed as a slotted hole), which allows a safe and quick uncoupling.

But since the Shinkansen trains have very small windows, as is similarly the case with airplanes, this is not noticeable. So we will continue right away! We inspect the model and find that the installation direction is clear and unambiguous.

On one side, the circuit board, whose LEDs must naturally point downwards and then illuminate the interior, is placed on a bridge with a nose. The opposite side at the other end shows a slotted hole under which the second web with a screw hole is placed. Here, we screw in the enclosed screw with a watchmaker's tool and fix the board on its brackets in this manner.

Once this is done, it is necessary to connect to the live parts of the chassis. At the plugged end we find two metal brackets and on the circuit board above it two corresponding suspension points. They are connected with the enclosed tension springs, which transmit current to the board.



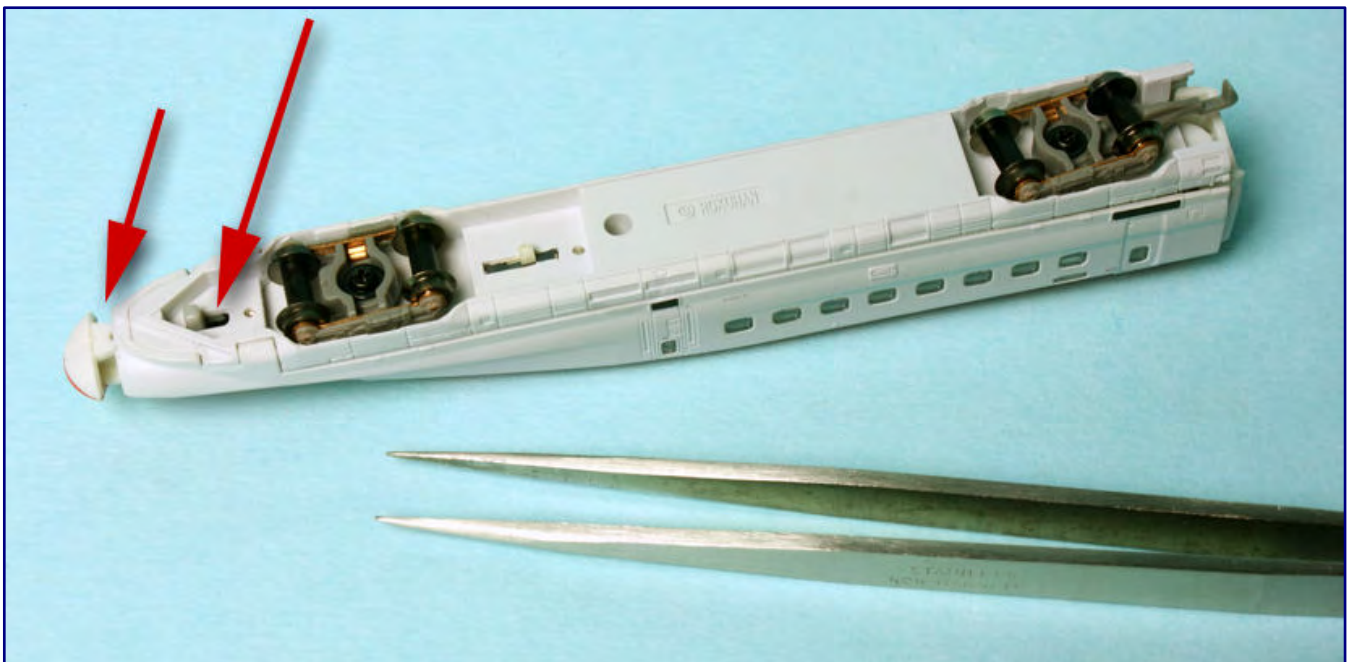
The assembly in the driven middle carriage shows no difference to the others (photo above). The same type of board (A009 / Noch 7297409) with four LEDs is also used. In comparison to photo 2 on page 56, however, it is now possible to understand why the position of the tightened screw is such that its feed-through in the PCB was designed as an elongated hole (photo below).

With pointed tweezers, steady hand and skill, the springs are first attached to the bottom and then to the top of the board. They should be gripped in such a way that the pulling forces do not act towards the open end of the tweezers.

Otherwise, you will quickly understand why Rokuhan attaches four instead of the only two required. But a loss of parts can also result from the fact that too much force is exerted on them when the springs are hooked in, stretching them so much that they no longer develop sufficient holding power and get unhooked again. This is also warned by the text of the enclosed instructions – but unfortunately not in German.

If this step is also completed, a function test can be carried out and the housing can be put back on. In our case, this construction step has to be repeated three more times before we work on the first part of the basic package with the motor vehicle.

Again, the procedure is identical to the four previous intermediate cars. Differences exist only in that the motor vehicle does not show any replicas of seats, because the cover of the flat, longitudinally installed bell-shaped armature motor is too high for this. This is also not disturbing from the outside, it is simply not recognizable, especially not when driving past.



The only tricky part of our exemplary equipment with interior lighting is processing the end cars. Here the body cannot simply be removed. Before that, the wagons have to be turned over and with the help of a blunt but sufficiently narrow tool the pin marked with the right arrow has to be pushed forward. Then the front fairing (left arrow) moves in the same direction and releases the body. Here you can also see the fine current consumption at the axle tips, which are contact-safe without creating rolling resistance.

But if you pay attention, you will find out what a slotted hole on the circuit board is for: The screw is screwed in at a noticeably different position. So Rokuhan has designed its light boards in a way that they can be used in as many products as possible.

Nevertheless, the parts used so far do not fit into the two end cars, which are longer than the middle cars, but have a shorter passenger compartment due to the elongated noses. The selected installation parts are correspondingly shorter, but their assembly is identical.

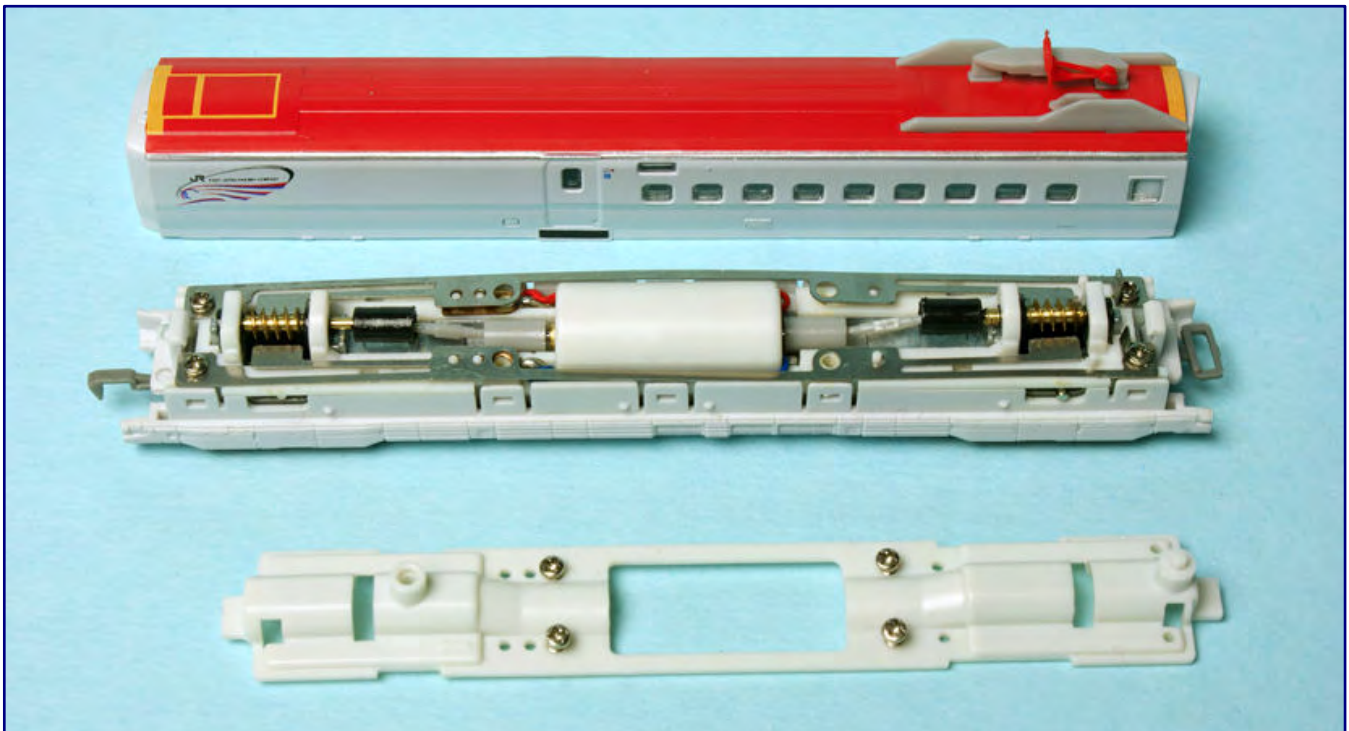
A bit more complex and also more difficult is the lifting of the housings to get access to the interior. It is not possible without consulting the diagram in the instructions: The end car to be fitted with the lighting

board is placed upside down on the table – a locomotive foam tray can do a good job here. The front cover of the nose, under which the automatic coupling is hidden in the prototype, must now be removed.

To do this, the grey pin in front of the front bogie has to be moved towards the tip of the nose. This is best done with the blunt end of a pair of tweezers or a wooden pin. It is better not to use a screwdriver, because if it slipped off, a noticeable scratch could occur.

When you move the pin, which has a quite firm fit, the cover slips out a little at the front and can then be easily removed with the fingers. Now the body can also be braced and pulled off the end car, paying special attention to the area between the bogie and the slot (for the pin), because here the body reaches far around the bogie.

With a strong but sensitive pull the body finally detaches from the chassis, and we can turn the chassis over. Now, we can insert the PCB, screw it to the chassis at the other end, and hook in the contact springs. After carrying out these steps on the other end wagon, our train is fully illuminated, and can go on the layout.



The drive shows the construction method of all multiple units known from Rokuhan: A low-profile bell-type armature motor sits deep in the chassis and drives two metal worms via cardan shafts, which sit above the bogies. POM plastic gears transmit power to both axles.

Impressions from the test drives

Four (A009 for the middle carriages) or three SMD light emitting diodes (A030 for the middle carriages) are installed by the manufacturer per light circuit board. They are spaced quite far apart with a distance of two centimetres. They are surrounded by further SMD components such as series resistors for reducing the track voltage to a level suitable for LEDs.

All four LEDs are located between the two retaining bars, which hold their plates at the correct height in the wagon. Only in the end cars is one LED located in front of the retaining pin, viewed in the direction of the nose, the other two are also located between the bars.

We are therefore curious to see how well the light is distributed, what the light colour might look like and how reliably the interior lighting works at all. As soon as the train is completely placed on the track and the Rokuhan controller is turned up slightly (constant light function), the interior lighting starts working.



The advantages of interior lighting are particularly evident in night-time operation, as we have chosen here. The likewise white top lighting also catches the eye pleasantly.

If we turn the knob back to zero after a few laps, the light is visibly dimmed, but remains active. If, on the other hand, we turn the speed control knob off, it goes out immediately. The products have not received a stronger buffering by capacitors.

Nevertheless, the interior lighting works reliably and flicker-free. So Rokuhan has done a good job here, and has matched its solution well to his own speed controllers. We are particularly impressed by the contact reliability of the pantographs at the axle tips.

So far we don't know of any comparable Märklin product in this area: The double-deck cars of the previous editions with comparable current collection showed a high rolling resistance, but the LED interior lighting of the Lufthansa Airport Express flickered noticeably. With Rokuhan, the interplay fits.

The only small weakness is the distribution of the cold white light in the interior of the individual cars. This is particularly noticeable with dimmed light in constant light mode. The areas on the LEDs appear much brighter than the spaces between them, each 1 cm away from them. If we increase the track voltage, however, this effect recedes noticeably.

All in all, we rate the retrofit kits as a smart and easy to install solution. We welcome the fact that Rokuhan's multiple units, and coaches are prepared at the factory for later installation of interior lighting.

At the same time, the buyer who does not wish to choose this option will not be charged additionally. Rokuhan thus manages to satisfy very different customer interests on an equal footing. It is also fortunate that the PCBs can be designed in such a modular way that all passenger trains can be illuminated with just a few items – so far only three in all.

Sales pages for Germany:

<https://www.noch.de>
<https://www.rokuhan.de>

Manufacturer website:

<http://www.rokuhan.com/english/>

Grenzenloser Modellbahnspaß in 1:220

**8. Internationales
Spur-Z-Weekend**



ALTENBEKEN

28. und 29. März 2020

Samstag: 10.00 - 18.00 Uhr

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www.z-freunde-international.de

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Der Kunde war König **Tanken mit Handkuss**

Lange vorbei sind die Zeiten, in denen die Dienstleistung rund um ein Produkt im Vordergrund stand und Firmen durch tadelloses Auftreten und zuvorkommendes Handeln Alleinstellungsmerkmale zu schaffen versuchten. Heute zählen Selbstbedienung und möglichst günstige Preise. Das folgende Buch lässt uns in eine Zeit eintauchen, die viele nicht mehr kennen und doch im Kleinen auf ihrer Modellbahnanlage nachzustellen versuchen.

Ulrich Biene
Gasolin
Nimm dir Zeit – und nicht das Leben
aus der Reihe „Bewegte Zeiten“ (Band 26)

Delius Klasing & Co. KG
Bielefeld 2018

Gebundenes Buch
Format 24,6 x 21,7 cm
112 Seiten mit 278 farbigen Abbildungen

ISBN 978-3-667-11246-0
Preis 16,90 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Ulrich Biene kennen unsere Leserinnen und Leser als Autor des Buches, mit dem die Firmen- und Modellgeschichte von Fallers aus Gütenbach zu deren Jubiläum erzählt wurde. Doch ist der Verfasser jenes Buches auch auf anderen Gebieten unterwegs, beispielsweise bei nicht minder geschichtsträchtigen Wiking-Automodellen.



Hauptberuflich ist er Pressesprecher eines Getränkekonzerns, nebenbei literarisch und fachjournalistisch tätig. Und hier ist er erheblich breiter aufgestellt als sich nur auf den Modellbau zu beschränken. Im vorliegenden Buch hat er ein großes Stück deutscher Wirtschafts- und vor allem Nachkriegsgeschichte aufgearbeitet und wiedergegeben.

Schon kurz nach der Währungsreform holte die deutsche Wirtschaft mit Riesenschritten auf. Der VW Käfer ist ein Synonym für das deutsche Wirtschaftswunder geworden. Er läuft und läuft und läuft. Doch damit er das kann, braucht er Kraftstoff. Und der kam nicht selten aus einer Zapfsäule von Gasolin.

Der vorliegende Band erinnert an die markante Architektur der „Großtankstellen“, rot-weiße Zapfsäulen und den bis heute noch nicht vergessenen Werbespruch „Nimm dir Zeit – und nicht das Leben“. Gehört hat ihn schon jeder, doch kaum jemand wird ihn 48 Jahre nach dem Ende dieser Kette noch zuordnen können. Damals prangte er an der Rückseite vieler Laster und mahnte angesichts steigender Unfallzahlen zur Gelassenheit am Steuer.

Ulrich Biene holt Erinnerungen zurück, erzählt Geschichte und auch Geschichten. Die Geschichte von Gasolin geht bis in die zwanziger Jahre zurück, weshalb auch das Buch beim Motoröl Motanol und dem früheren Leuna-Kraftstoff ansetzt. Wie aus einem einstigen Logo und dem bekannt gewordenen Benzinamen die Wort-Bild-Marken von Gasolin wurden, wird in diesem Verlauf beschrieben.

Der Autor hat in akribischer Arbeit zeitgenössische Dokumente und Bilder gesichtet und die Markengeschichte recherchiert. Als Ergebnis informiert er über die Historie bis hin zu den Relikten, die es heute noch zu entdecken gibt.

Einen weiteren Schwerpunkt streift der bekannte Markenspruch selbst: Der Firmenauftritt hinsichtlich seiner Farben, seiner Sprache und Bildmotive sowie auch der Architektur ist über viele Jahre mit einem Namen fest verbunden. Deshalb findet sich auch dieser besondere Aspekt der Gasolin-Geschichte im Buch zu Recht wieder. So zeitlos der berühmte Satz war, so vergänglich zeigte sich sein Urheber.

Für jeden, der der das typische Lebensgefühl der Blütezeit von Gasolin und das Verständnis jener Jahre im Modell wiedergeben möchte, ist ein Werk wie dieses einfach eine Pflichtlektüre. Tadellos saubere und perfekt sitzende Uniformen, höchst zuvorkommend agierende Tankwarte, ein Handkuss für die Dame und eine aufgeräumt und sortiert erscheinende Verkaufsstelle prägen das einst erwartete Erscheinungsbild.

Wer an den an Autos anhaftenden Schmutz und Schmierflecken von Betriebsmitteln denkt, erkennt sofort, wie gut oder schlecht Anspruch und Wirklichkeit oft zusammengepasst haben dürften. Das ist weiterer Stoff für spannende Kapitel, die sich in diesem Buch wiederfinden.

Es erzählt nämlich auch die Geschichte und Erfahrungen von Unternehmern und Unternehmen, die mit Gasolin verbunden waren. Sogar das sich wandelnde Bild von Zapfsäulen taucht darin auf. Eine besondere Rolle spielten damals Werbemittel. Seien es Straßenkarten für die steigende Zahl an Wochenendausflüglern und Urlaubsreisenden oder kleinere Modelle für die mitfahrenden Kinder.

Gasolin war damals omnipräsent und hatte nach 1945 doch bei Null anfangen müssen. Als das Auto zum Allgemeingut geworden war, war das Tankstellennetz so dicht geflochten, dass Gasolin bis zu 3.500 Stationen mit dem technisch modernsten Standard betrieb.

Ein Tankwart säuberte damals noch unaufgefordert die Windschutzscheibe und kontrollierte auf Wunsch auch den Ölstand und den Reifendruck. Das Wort Dienstleistung war da noch wörtlich zu verstehen und meinte wirklich noch aus den beiden wörtlich zu nehmenden Hälften Dienst und Leistung.

Doch die Zeiten änderten sich, aus einem Profiteur der Wirtschaftswunderjahre wurde ein Opfer der Marktkonsolidierung. Unverändert geschätzt und beliebt bei den Kunden, verschwand Gasolin 1971 quasi über Nacht.

Ulrich Biene erzählt mit profundem Fachwissen und unterstützt von einem hervorragenden Bildarchiv die Gasolin-Geschichte. Teile des ebenso gut ausgewählten wie auch wiedergegebenen Bildmaterials waren zuvor unveröffentlicht.

In geradezu bewegenden Bildern erlebt der Leser Aufstieg und Niedergang von Gasolin in jedem Detail. Vergessene Welten werden darin sogar für diejenigen lebendig, die zu jung sind, um sie selbst erlebt zu haben. Waren das einst bessere Zeiten? Auf jeden Fall waren es billigere: Ein Liter Benzin kostete 1970 gerade mal 55 Pfennig. Fangen Sie gar nicht erst an, das in Euro umzurechnen...

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Tankstellen im Wandel der Zeit

Sammelpunkte der Mobilität

Ging es im zuvor vorgestellten Buch um die Geschichte einer einzigen Kraftstoffmarke, so sind sie hier alle gleichermaßen berücksichtigt und vorgestellt. Mit ausschließlichen Fokus auf Nordrhein-Westfalen erleben wir auch hier ein Stück Wirtschafts- und Mobilitätsgeschichte. Geschickt vermittelt der Autor das sich wandelnde Erscheinungsbild der Tankstellen und flechtet ein, welche Rolle sie in der Wahrnehmung der Gesellschaft spielten.

Ulrich Biene
Bitte volltanken!
Tankstellen-Kultur in Nordrhein-Westfalen

Klartext Verlag
Essen 2019

Gebundenes Buch
Format 27,6 x 22,4 cm
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Und wieder Ulrich Biene... In der Tat greift auch die zweite Buchbesprechung dieser Ausgabe einen Titel desselben Autors auf wie der vorherige Beitrag. Von Haus aus Journalist und in der Pressearbeit tätig, beschäftigt er sich seit vielen Jahren mit Themen zur Nachkriegsgeschichte und gesellschaftlichen Entwicklungen im Wirtschaftswunderland.

So erscheint es beim zweiten Hinsehen wenig verwunderlich, dass er als „Kind des Ruhrgebiets“ auch hier verantwortlich zeichnet. Deshalb besteht kein Grund zur Sorge, dass sich Inhalte wiederholen oder überschneiden könnten. Nur ganz wenige Bildmotive finden wir in beiden Titeln wieder.

Die Botschaft des Autors liegt auch hier wieder in der Authentizität seiner Reportagen, Zeitzeugenberichte und Dokumente. All das fügt er gekonnt zu einem wahrheitsgetreuen und reich illustrierten Bild der Wirtschaftsgeschichte zusammen.

Auch das in diesem Jahr erschienene „Bitte volltanken!“ ist ein Werk geworden, auf das Modellbahner nicht verzichten sollten, wenn sie Tankstellengeschichte auf ihrer Anlage authentisch wiedergeben und umsetzen wollen.

Genau darin liegt auch der Grund, warum wir mutig waren und Ihnen heute gleich zwei Bücher vorstellen, die auf den ersten Blick nichts mit der Eisenbahn zu tun haben. Wo die Berührungspunkte liegen und wie allgegenwärtig sie auch im Kleinen sein sollten, wird der geneigte Leser bei Lektüre dieser Ausgabe aber sicher schon festgestellt haben.

Tankstellen entfalteten bald nach dem Krieg eine Strahlkraft wie weithin sichtbare Leuchttürme. In vielen Farben erstrahlten die Symbole und großen Buchstaben der Mineralölketten von den Masten am



Bürgersteig. Doch bis hierher sind die Ausführungen noch gleichermaßen für alle Regionen der jungen Bundesrepublik gültig. Was also macht dieses Buch aus, dass sich auf Nordrhein-Westfalen fokussiert?

Das ist zum einen natürlich der größte Ballungsraum Deutschlands, in dem viele Kulturen, Gewohnheiten, Charaktere und auch Autos aufeinandertreffen. Hier begegnen die Motorräder und Kleinwagen der an Wohlstand gewinnenden Arbeiter an der Tankstelle den großen Limousinen der Zechenbarone und Stahlwerkseigner.

Und wie nirgendwo sonst suchen junge Ehepaare ihr berufliches und finanzielles Glück als Pächter einer Tankstelle. Innerhalb von nur zwei Jahrzehnten entsteht in Nordrhein-Westfalen mit beträchtlichen Investitionen das dichteste Tankstellennetz Europas. Das ist in dieser Form einzigartig und liefert den Stoff, mit dem dieses Buch so eindrucksvoll geschrieben wurde.

Wir begegnen Firmen, die auch heute noch im Ruhrgebiet bestens bekannt sind, nur kaum jemand wird sich noch erinnern, dass ihre Ursprünge im Tankstellengeschäft liegen könnten. Hier treffen wir Sonntagsausflügler, die in Werl an der Bundesstraße 1 Halt an der Großtankstelle machen, um ihren Kraftstoffvorrat aufzufüllen.

Arbeit und Vergnügen, Alltag und Erholung – alles liegt so nah beisammen und zeigt auf, wie wichtig die Mobilität im wiedererwachenden Deutschland war. So bunt wie das Bild der Menschen, ist auch jenes der Tankstellen.

Ob groß, ob klein, ob in einen bestehenden Betrieb integriert oder nach den Architekturplänen der Konzerne errichtet: Ihre Vielfalt findet sich im vorliegenden Werk wieder und liefert Anregungen für den Modellbau in Hülle und Fülle.

Dabei fasst dieser Titel die Entwicklung der Tankstellen von der Gehwegpumpe über die rasanten Wachstumsjahrzehnte bis zur Ölkrise detailliert und unterhaltsam zusammen. Alle Ketten, die nach 1945 in Nordrhein-Westfalen bekannt waren und auftraten, sind hier vertreten und ihre Geschichte, die häufig in einer Fusion oder Übernahme endet, wird anschaulich beschrieben.

Die Erkenntnis daraus lautet, dass es nicht immer Aral, Shell oder Esso sein muss, was auf den Modellen unserer Landschaften angeschrieben wird. Aufmerksamkeit gewinnen auch längst verblichene Namen, die mit heute eher ungewohnten Farben um die Gunst der Kunden warben.

Eine Tankstelle als Modell war auch damals übrigens schon sehr beliebt. Am Ende des Titels finden wir einige Modelle, die bei bekannten Herstellern erschienen, als Werbegeschenke verteilt wurden oder an der Station selbst gekauft werden konnten. Selbst der Papierbausatz zum Ausschneiden fand sich darunter. Die Faszination des Nachbaus gab es also auch schon vor fünfzig Jahren.

Freilich lässt sich all das nun Beschriebene nicht ohne Bilder zu den Lesern transportieren. Und so haben wir hier in erster Linie einen Bildband statt eines Fachbuches mit großem Tiefgang vor uns.

Ulrich Biene ist der Spagat zwischen sprechenden Fotos und nicht zu knappen oder auch zu umfangreichen Informationen sehr gut gelungen. Und da die über 400, bestens und passend ausgewählten Fotos auch tadellos im Druck wiedergegeben werden konnten, lassen sie die nordrhein-westfälische Tankstellengeschichte in den Gedanken der Leser wiederaufleben.

Wir nominieren dieses Buch für die Neuerscheinungen des Jahres 2019 in der Kategorie Literatur.

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Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Christmas greetings from Switzerland:

Unfortunately, the snow this year was slow in coming, and, therefore, it was quite late for a suitable holiday photo, but perhaps it is still enough for a contribution in the dialogue section. Many thanks to the editors for the great work you do for us readers every year, month after month.



Another version of SBB's „Roten Pfeils“ (“Red Arrow”) from SMZ's production, which is not just a colour or lettering variant, was the biggest surprise 2019 for our reader. Photo: Simon Wernli

To the photo: The biggest surprise from a Swiss point of view this year was probably the revised version of the „Roten Pfeils“ (Red Arrow) of Sondermodelle Z, this time in the short original version.

Happy holidays and a happy new year! Hearty Christmas greetings from Switzerland.

Simon Wernli, by E-Mail

Container carrier wagons in the Herz:

Thank you for the interesting issue, the topic "container wagons" appealed very much to me. I'm curious if Märklin will publish car types based on the Rmms 33, like the BTmms 51 (or the Rmmso), as Z-models. I have already registered interest there.

Happy Advent season, and all the best for the New Year.
Jörg Endreß, Bremen

Inspiring contribution in Trainini® 11/2019:

Thank you very much for the detailed report on the container wagons. Many of the photos were new to me. Also many thanks for the contribution to my conversion to BTms 55.



The title theme of the last issue was extremely well received by the readers, as confirmed by the feedback we received. Even the cover picture suggested that the scene depicted on the loading line of the company's own plant could be recreated with similar models. Photo: Dirk Rohwerder

I liked the cover so much that I re-created this scene. The photo is attached.

Dirk Rohwerder, Sprockhövel

Advice to LED exchange base from Great Britain::

I was searching on Ebay and saw an item for the replacement of the original bulbs on Z locos by LEDs.

Search term: CKLED Märklin

There's also a video on Youtube, same search term. Perhaps a topic for Trainini (the best Z Magazine)?

Hans Prudon, by E-Mail

Answer of the editors' board: Thank you very much for this notice. We have inquired under the e-mail address given in the film contributions and asked for further information including sources of supply outside the auction platform. At the time of going to press, we had not yet received a reply to our inquiry.

Modern night trains thanks to Sondermodelle Z:

SMZ has recently introduced models of the Nightjet, which are based on well-known Märklin models, in their well-known good paint and lettering quality. One Ellok "Taurus" locomotive and four passenger coaches drive up in the typical ÖBB night train livery. Three of them are based on a Eurofima car, the fourth one is a compartment car.



The five-part ÖBB Era VI night jet of Sondermodelle Z was created on a Märklin basis and impresses like everything else that has been realized by the Viennese inventors so far. Photo: Jörg Erkel

Deliveries of new products from Noch:

A whole series of new products were able to make it into the Christmas business on time. We would therefore like to take this opportunity to highlight the products relevant for Z scale.

Among the deliveries are also tree packs of the standard series for Z-scale. The 16 deciduous trees (Art. No. 24603) with a height of 4 to 10 cm, sprinkled with specially developed flocking, and provided with a root base, but certainly also the 16 mixed forest specimens with a height of up to 14 cm (24621) are suitable for cost-effective reforestation.

In our discretion, the 10 to 14 cm high conifers of the following combinations are also usable: 8 firs (24640), 8 snow firs (24680) and 16 snow firs (24681). We also offer single trees of this series with 8, 10 and 12 cm high specimens in dealer assortments: 100 deciduous trees (25963) as well as mixed deciduous and coniferous trees (25964).

New loads at Modellbahn-Union:

The Modellbahn-Union (<https://www.modellbahnunion.com>) has launched no less than ten load inserts for Märklin's models of the modern, open freight car type Eanos-x 052 as new products. They are manufactured in our own Issum plant using laser cutting technology.

They are delivered as kits, which can be quickly assembled according to the instructions and can be realistically designed and coloured, as desired. You can choose between "planed boards dark" (art. no. MU-Z-F00003) and "light" (MU-Z-F00004), "sawn boards dark" (MU-Z-F00005) and "light" (MU-Z-F00006), "gravel fine black" (MU-Z-F00007) and "grey" (MU-Z-F00008), "brick" (MU-Z-F00009) and "brick" (MU-Z-F00010).

Märklin deliveries in November:

The Märklin catalogues 2019/20 are now available for collection (free copy for Insider members) or purchase from dealers. They list all known models of the delivery program including the new models introduced or announced this year.



Unfortunately, the new catalogue does not offer a special experience. This is also due to the lack of surprises, but above all to the very compressed and "listless" presentation of Z gauge. It has to do almost completely without motivational shots, which are actually supposed to set the buying incentives.

Among the few motifs we find, we could not discover anything new. In any case, there is potential for improvement here, which we have already indicated to Märklin.

Many customers will certainly also regret that some other models have slipped further back in the delivery schedule and have thus been pushed into 2020, including the local transport package with another shape variant of the 141 series. This means that the Christmas business on a small scale will, unfortunately, remain quite manageable.

But there are also pleasant exceptions: For example, the class 0310 express steam locomotive was delivered to the German Federal Railways with a newly built boiler (Item No. 88850). The Insider model 2019, driven by a bell-type armature motor, features excellent detailing, a closed driver's cab with side window panes and also tender lantern lighting.



The recently delivered express steam locomotive 03 1001 of the DB with new boiler (Item No. 88850) has a side window on both sides of the driver's cab and also drives up at the tender with direction-dependent headlights.

We will present this model in detail and test it in the next issue.

The X 05 low side car with brakeman's cab (82334), which is delivered with a large diesel engine mounted on a loading frame, has also found its way into the dealer shelves. The oversized engine on this small wagon will probably have exceeded the load limits of the prototype, as well.

Shortly before Christmas, Era VI will also be served with a pack of cars in service around 2015. The three side-tipping cars of the Eamos type (82435) registered in Austria, bear the orange RTS Rail Transport Service design and even have fine frame lettering.

Regulars' table special models Rhine-Neckar 2019:

In cooperation with FR Freudenreich Feinwerktechnik, the Rhein-Neckar regulars' table has once again this year had special models issued to alleviate the shortage of vehicles for Era IIIa, i.e., the period when the Reichsbahn was in the occupation zones, and before the successor companies were re-established.

The special packs (art. no. 49.014.02), produced in a one-time edition of 40 pieces, contain an open freight car "Klagenfurt" of the DR in the USSR zone with real coal load insert and a covered car G 19 of the DR Brit-US zone.

Both cars were in service around 1947/48 in the lettering shown, but can still be used beyond that, because experience has shown that it took several years to completely relabel the stock as DR and DB.



A removable load and operating traces of blurred chalk markings and torn labelling further enhance the attractiveness of the two cars (Item No. 49.014.02) for the period between 1948 and the early 1950s.

The open car has the wooden doors typical only of the Reichsbahn remaining in the east. In the case of the Bundesbahn, these were replaced early on by metal doors and the opening area was reinforced by an upper belt inserted above the doors.

The models were given a light patina at the factory, and the "Kassel" also has wiped or blurred chalk writing. Narrow rubbing stripes are intended to reflect residues of address labels. This design is protected by a thin matt lacquer coating. The handcrafting makes each trolley unique.

The car package was offered for the first time at the Advent meeting in Zell (Mosel), the first issue price offered there (155.00 EUR / from two packages 150.00 EUR) remains valid until 24 December 2019. Interested parties please contact Volker Töpfer directly (info[at]kurfalz220.de).

If there are any remaining stocks after this date, they will be offered at the regular price (159.00 EUR) from the 1to220-Shop in 2020. As an exception, we will mention concrete prices here, because there are no other sources of supply other than those mentioned here, and, therefore, these prices are considered fixed.

The current deliveries of AZL:

In December 2019, American Z Line will be offering sleeping cars from the ATSF light passenger coach series. These are Pullman sleeping cars, whose series has been given the product name "Valley" by the manufacturer. The latest deliveries include no fewer than thirteen matching cars.

The mentioned 6-6-4 sleeping cars were the first streamlined vehicles with smooth outer walls, which were used by the ATSF. They were put into service in June 1942, and while they were initially painted green or two-tone grey, they were later also given a "shade dress" in aluminium colour, which visually matched other types with beading under the windows.



Thus they were used both in the transcontinental Chief trains and in lower category passenger trains. At AZL, the new items include nine specimens, both grey and with painted beading (item nos. 73128-1 to -9) and four matching baggage cars (73628-1 to -4).



Manufacturer photos of the current deliveries can be found at <http://www.americanzline.com>.

In these two designs, the passenger coaches have now been launched for ATSF. Photos: AZL / *Ztrack*

East German commercial vehicle from EtchIT-Modellbau:

The IFA L60 KT truck, offered as a kit in high-resolution 3D printing, was the standard vehicle in the commercial vehicle sector of the former GDR. The model of the all-wheel-drive prototype has a fuel tank (Item No. XD153kt_Z) and is supplied in three parts (chassis, tank tank and driver's cab), and assembly is simple.

Manufacturer EtchIT-Modellbau has classified the assembly into the easiest of five levels of difficulty. However, the customer must bear in mind that the parts must be painted before assembly. It is a great advantage that the well-detailed model still consists of the individual components mentioned.

The novelty is available on the manufacturer's website (<http://etchit.de>). It may seem tempting here that until 30 December 2019, shipping is free of charge for orders with a relatively small minimum order value. So it is not worthwhile to order other models from East and West, as well?

Archistories Exclusive kits for Ztrack:

Exclusively produced by Archistories for **Ztrack** are the new kits of a US-American farmhouse from the 19th century ("19th Century Farm House"). The multivariable kit (Item No. 406191) features typical design features from the rural regions of the United States.



A picture is worth a thousand words: Here you can see all five selectable building colours and also several of the possible variations by arranging the elements differently for the American farmhouse of the 19th century. Photo: Archistories

The individual components of the building, consisting of main building, porch, annex and portal, can be combined completely freely. In addition, the house can also be built in a mirrored form, which is why the five colour versions currently available can be combined to create an enormous number of variations which together form a lively settlement. This is also ensured by the fact that the components of the kits are interchangeable.

The kits are made of high-quality, solid-coloured hard cardboard, as is usual for Archistories. The dimensions of the building, assembled according to plan, are approximately 46 × 36 × 34 mm.

At the end of the year there is also something from Micro-Trains:

The Farm-to-table series is now entering lap 11 and is now focusing on a wooden refrigerator car in brown basic colour with yellow side walls, as it was used for the Horvitz Brothers (Item No. 518 00 810).

Covered bulk freight cars with two outlets on the car floor appear in both the grey of the BNSF (531 00 301 / 302), and the former green of the Burlington Northern (531 00 311 / 312).

Painted in two shades of green are the four passenger coaches of a Northern-Pacific composition (994 01 250), which consists of three four-axle cars with smooth outer walls and a six-axle pulpit car after its modernization.

The models are distributed in Germany by Case Hobbies (<http://case-hobbies.de>), among others.

News about the Insider Locomotive 2020:

Märklin has sent us a new product picture of the 042 series (item no. 88275), which already shows in the photo montage how the circulation on the later model should appear. The hand sample shown with the announced one still showed the body of the 0310 series, where only the closed cab was exchanged for the open version of the standard cab.



In the latest representation of the steam locomotive class 042 (item no. 88275) the typical course of the circulation on the side with the bend at the transition to the smoke chamber has now been implemented. Product illustration: Märklin

As this can lead to irritation in view of an earlier model with a completely unsuitable structure, we have asked for this in order to achieve security for our interested readers. In addition to a positive answer, we also received the product photo shown here with the correct circulation, which, slightly lowered, completely surrounds the smoke chamber.

Free model addition of the 1zu220-Shops:

The 1zu220-Shop (<https://www.1zu220-shop.de>) will continue this year's campaign to include a model car free of charge with the Märklin Insider model for those who order it. Whoever orders the oil-fired steam locomotive 41 356 with a tender can look forward to this gift, which is not available separately, and thus, should keep its exclusive character.

In 2020 it will still be a truck model with a red chassis and blue painted cab. This time, however, the Krupp Titan SW L 80 with platform and tarpaulin, which is so popular with model railroaders, was selected and implemented by Wasp models, after the Magirus tipper from the current year was so well received.



The 1zu220-Shop offers this model of a truck with platform and tarpaulin of the type Krupp Titan as a free addition to the Insider model 2020 in Z gauge.

A matching trailer with three axles and a Krupp 980 front control arm in open platform design have also been completed and delivered. However, they are to be added to future models, or offered via other promotions.

Review of the International Model Railroading Day 2019:

Also, in 2019 many events and exhibitions occurred on 2 December as well as on the weekends before and after, which should actively draw attention to our beautiful hobby. As with every year, the companies Faller and Noch were present.

Due to limited capacities only with advance reservation, a look behind the scenes of the Wangen-based accessory manufacturer was possible. Every hour on the hour, 16 people were given the opportunity to be guided through the halls and to gain impressions of the production of model landscapes, the production of kits using laser-cutting technology, or the fully automated warehouse in operation.



The International Model Railroading Day 2019 on 2 December and the weekends surrounding it are now the occasion for many events aimed specifically at children, newcomers and re-starters.

Meanwhile, the press had rediscovered the 30 x 15 m H0-gauge layout of singer Rod Stewart, already known from the American "Model Railroader" for years, and reported about it. This showed how well rock music and this supposedly quiet hobby go together, when the same people usually like to spread the image of the aged, and fallen out of time, senior.

A reader survey conducted by the company Market Research (<https://www.tag-der-modelleisenbahn.de/de/fakten-rekorde>) paints a different picture, because according to this survey around 1.5 million German citizens are very interested in the railway. Almost half a million of them are also actively involved with model railways in their leisure time.

Special models for the Advent meeting:

For the Advent meeting in Zell (Mosel) Jörg Erkel presented two matching special models, which were initially offered exclusively on site. Remaining stocks were intended for subsequent sale in the 1to220 shop.

All models of the ocean-blue Deutsche Bundesbahn workshop car 611 (item no. 49.334.211), which was exclusively produced by FR Freudenreich Feinwerktechnik in a limited edition 40 units, have already found a new owner on site, which means that the original plan has already been fulfilled.

The Westheimer series, based on covered G 10 cars and printed exclusively with Märklin prints, found a seasonal continuation: The latest motif for "Westheimer Winter" (98166) was only available in Zell (Mosel) together with a sample bottle of the beer advertised on the car.



The latest beer trolley from the Westheimer series from the 1zu220 shop shows a very attractive design. The motif presented in Zell (Mosel) (Item No. 98166) advertises Westheimer Winter and was once again produced and printed by Märklin.

But the 100 unit model also has a lot to offer in terms of design: The iron-coloured front and side walls have a refreshingly different look and are reminiscent of the earlier design of Italian pointed roof cars. The red roof, which also has a slightly metallic appearance, stands out pleasantly warm.

Scale models of the operating lettering and printed brake corners also allow the railroad employees to add this car to their own stock and integrate it into freight trains without losing a realistic appearance. Anyone interested in the Westheimer winter car should not wait too long: One-time edition models are still available (<https://www.1zu220-shop.de>), but the older wagons of the series, which are already sold out, prove the good demand.

The rail bus models for Era IV by Märklin (88167), digitized in cooperation with Oliver Passmann, also meet with great interest. Jörg Erkel has already been able to bring some of them to Zell. The vehicles, which are already equipped with interior lighting and interior fittings ex-works, are driven by a bell-type armature motor, which provides a good basis for this DCC variant.

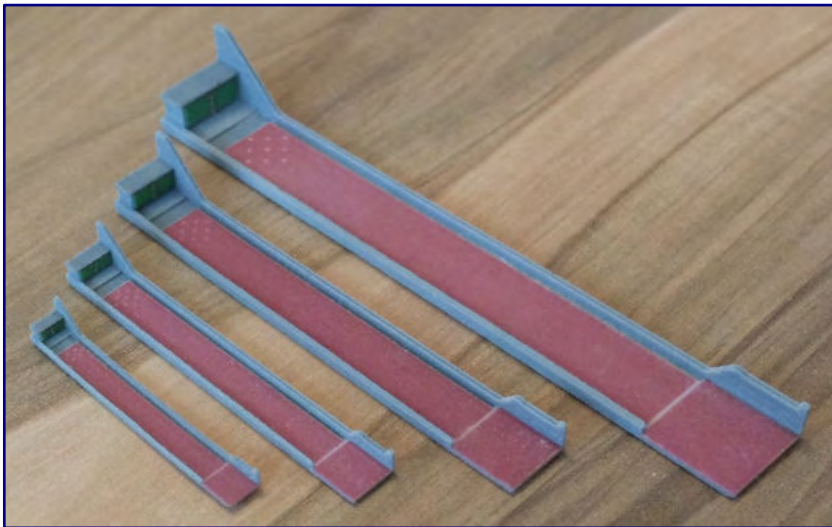
Proven decoders from Doehler + Haass provide the digital control option: In the motor car, a DH05C motor decoder controls the motor decoder, in the control trailer, the FB05B function decoder is useful.

Sweet fruit new at WDW Full Throttle:

WDW Full Throttle will be presenting a yellow wooden refrigerated truck with red front walls and roof in December 2019, which was modelled for the Fruit Growers Express and provided an attractive model. This type of car is available in a double pack (Item No. FT/B-9022-1), which is distributed in Germany by Case Hobbies (<http://case-hobbies.de>), among others.

Schrax presents something different:

An unusual idea came from the 3D printing accessories manufacturer Schrax (<http://www.schrax.com>). One of the bowling alleys that used to be so popular in the past, and which were by no means only found in enclosed spaces, was realized here on a scale of 1:220. The novelty seems suitable for the epochs I to IV.



The new bowling alley is also offered as a 3D printed model for Z-gauge (far left in the picture). Photo: Schrax

The template is a typical “pub railway.” In the days of non-automatic tracks, balls and cones were locked in a steel cabinet at the end of the track. After paying the rent for the track, the athletes received the key.

While they were playing, someone had to stand at the end to put the knocked over cones back on their positions and lift the ball onto the ramp so it could roll back to the players.

Children were happy to earn a few pennies, a bockwurst or even a barrel shower as a skittles boy. With the Schrax novelty this long gone time can be reproduced on the model railway in the future.

Exhibition outlook for next year:

Fascination Modellbahn would like to kick off the public exhibitions in 2020 and be the first to show the new products presented in Nuremberg directly to model railroaders.

The venue for the event is once again the Maimarkthalle in Mannheim, where the gates will be open from 13 to 15 March 2020 from 9:00 to 17:00 hours. Information about the fair will again be available at <https://www.faszination-modellbahn.com>, and will be updated regularly.

Intermodellbau in Dortmund, which shows all branches of model making, will follow between 23 and 26 April 2020. In the field of model railways, the organizers and the Modellbahnverband in Deutschland e.V. (MOBA) will then celebrate the 25th anniversary of their successful cooperation.

But before all other events, Zetties is looking forward to the 8th International Gauge Z Weekend in Altenbeken, which is heading for a new record in international participation. On 28th and 29th March 2020, all those of rank and name on a scale of 1:220 will meet again in the railway town in the east of Westphalia for an exchange of ideas, presentation of new products and unusual exhibits.

But there was still one horror to digest: As the railway press service Ferpress reported, the 37th International Model Railway Exhibition in Köln (Cologne) was cancelled. The reason given was that the Lego exhibition, which would otherwise have taken place at the same time, also would not take place and thus synergy effects were lost.

In addition, according to the report, the City of Köln (Cologne) insisted that new model railway articles on the Sunday of the fair should not be sold until the afternoon - a condition that was probably not reasonable for the commercial exhibitors in view of their personnel and stand costs.

Fortunately, Messe Sinsheim, as the organiser, reacted quickly and chose Faszination Modellbau in Friedrichshafen (30 October to 1 November 2020) as an alternative venue. There, the IMA will extend the already extensive model railway section even further, as Ferpress announced.

It remains questionable, however, whether the end of the fair in Köln (Cologne) is solely due to the city: It was planned to be held on the Sunday National Day of Mourning 2020, a public, Protestant holiday. It is one of the silent holidays in November, the month of mourning, when other commercial events may not be permitted. Should the organizers really have overlooked this?

So the question arises whether the International Model Railway Exhibition in Cologne should not may have been deliberately set up to fail. Since Messe Sinsheim took over the organisation it lost its attractiveness in the opinion of many model railroaders and recently also showed a strong Visitor fade-out. Critics expressed similar opinions about the earlier fair in Bremen, which had a similar development with a quick end.

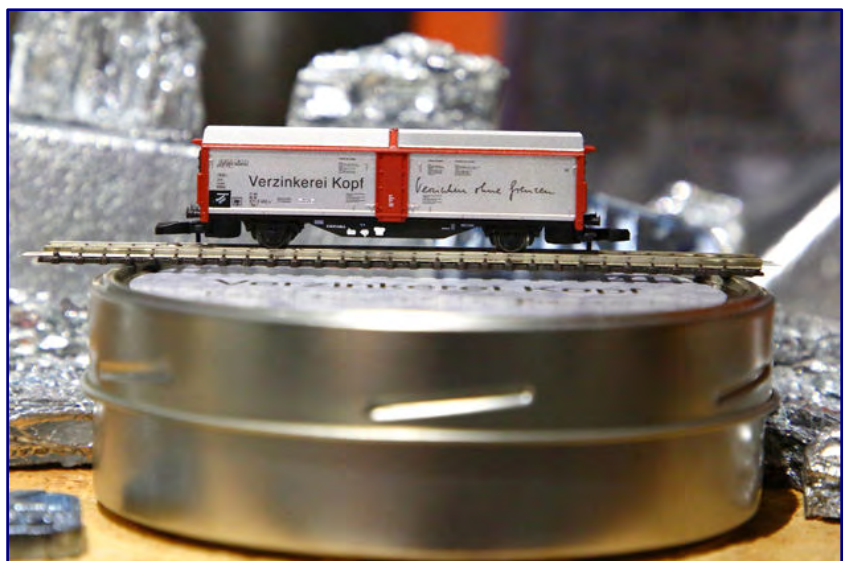
Such criticism will hardly be able to be disproved and dispelled. One thing is certain for us, that Friedrichshafen cannot be a substitute for Cologne: The catchment area of the three countries Germany, Switzerland and Austria has already been developed. It remains to be seen how far it can still be developed.

This contrasts with three Benelux countries, which, in addition to the direct catchment area of the only megacity of North Rhine-Westphalia are now completely undersupplied. A replacement location in the immediate vicinity such as Düsseldorf or with European neighbours would be necessary. Perhaps, Eurospoor, held in Utrecht, the Netherlands, could in this respect and even more strongly be developed into an international event?

Presentation of the museum wagon 2020:

As is tradition, shortly before Christmas Märklin again invited guests to the museum car presentation at Märklineum. Managing Director Wolfrad Bächle welcomed the numerous guests and had researched a lot of detailed information about the partner companies of the Museum Wagon 2020 and their company history.

The partner for Märklin cars is Kopf Holding GmbH, which was founded as a galvanizing plant in 1973. A connection to Märklin can already be established through the material zinc. For the Trix and LGB brands, the partner is once again from Nuremberg, this time the traditional company Lebkuchen Schmidt GmbH & Co. KG.





The two Märklin Managing Directors Wolfrad Bächle (left) and Florian Sieber (right), together with representatives of the two affiliated companies, present the Museum Cars 2020 for the Märklin, Trix and LGB brands.

Under a galvanized palm tree and a fine decorative arrangement, the curtain was opened to showcase the models that are officially only available in the Märklineum. The Z scale model is a Tbis 850 sliding-roof side-wall car with a sliding roof and advertising design by Verzinkerei Kopf, Schlierbach, packed in a round metal box.

This evening also provided the setting for the ceremonial signatures for the sale of the Märklin tower room collection. The Chairman of the Board of Management of Kreissparkasse Göppingen, Hariolf Teufel, District Administrator Edgar Wolff, the two Managing Directors of Turmzimmer GmbH, Ingo Putschkat and Joachim Müller, as well as Märklin Managing Directors Florian Sieber and Wolfrad Bächle, all attended.



Presentation of the new museum cars in the Märklineum for the museum car presentation (for 2020).

Märklin is investing around 11.3 million euros in the construction of the new adventure world called Märklineum. 5.3 million euros of this amount will come from the aforementioned sale of the Märklin tower room collection. Additionally, there is a commitment to the long-term preservation of this unique collection.

Herpa airplane models for Spring 2020:

Already in March and April 2020, the aircraft new products recently announced by Herpa will be available on a scale of 1:200. We have looked at what can be installed in overall sizes suitable for plant operation on aprons, taxiways and runways with a European character.

We first came across the Airbus A220-300 "Lithuania" of Air Baltic (Item No. 570770), which completes a series of three aircraft with special designs for all three Baltic EU states, which had already started with the May/June new products of this year.

However, holidaymakers may also be familiar with the same aircraft type as the Egyptair (570787), which will then perhaps be available next to the Embraer E190 from KLM Cityhopper (557580-001) with a new designation. For the Hungarian Malév, is the Fokker 70 (570763), which completes the civil new items.



In dieser Gestaltung flog die Fokker 70 einst für die ungarische Malév. Herpa bildet sie so als Modell (Art.-Nr. 570763) nach. Abbildung: Herpa

The special design "Fulcrum Farewell Tour 2003" carried a Mikoyan MiG-29A of Fighter Wing 73 (570794) which had been transferred from NVA stocks to the stock of the German Air Force. The Snapfit model of the Airbus A320 neo "Padre Américo" of the Portuguese TAP (612593) concludes this list.

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