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Trainini

German Magazine for Z Gauge



Award Winning New Products 2020

Review of a Difficult Year
Shifting Technology Freshly serviced

Introduction

Dear Readers,

First of all, I would like to take this opportunity to wish you a Happy New Year, also on behalf of my editorial colleagues. May it be a better and happier year for all of us than the last one was.

As much as I am happy to have more spare time for my hobby, I also miss many personal contacts with fans, relatives and hobby colleagues. I'm certainly not alone in this either.

Traditionally, our January issue in particular is a review of the past twelve months. I have already mentioned what they were dominated by. But, even in a year as difficult as 2020, there were good news and positive memories that remain.

In our review of the year, we summarised all that once again and were surprised ourselves at some points. An equally cherished tradition is to discuss the outstanding new releases in detail, to compare and evaluate them in order to finally award our title "Best New Releases of the Year 2020" in different categories.

In this process, it was interesting for us to have a look at who had won the awards in previous years. This also tells us about a change in the market, but also about outstanding developments that make some providers seem almost like "regulars" on the winners' podium.

But after the turn of the year, let us now rather look ahead: The first innovations from mass production have become public, in the meantime. This sometimes means disillusionment when a personal dream model is (once again) not included, but great joy for the people next door because a dream has finally come true.

It is important to us that we also keep an eye on newcomers and those returning to the hobby, because there are many of them at the moment. We welcome all those who are reading **Trainini®** for the first time!

Please feel invited to let us know your wishes and information needs so that we can prepare appropriate articles. Z gauge may not be a very large community, but it is a close-knit and "sworn community" in which, you too, will find your place—we promise!

Today, we kick off with part 9 of our series "Maintenance & Care of Small Trains," which has been running since 2013 and always provides valuable knowledge for rather inexperienced Zetties. Our reader Michael Pleiner's contribution to the construction of a "half-luggage and half-coach" wagon can also be performed with little technical skill.

All in all, we hope it will be another colourful, versatile and worth reading edition. We are now eagerly waiting to see what new products will soon be announced to us so that we can share them with you next month. In the meantime, have fun reading!

Sin-Z-erely,

Holger Späing



Holger Späing
Editor-in-chief

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We thank Michael Pleiner for his contribution and Matthias Wistrach for his photo support.

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Cover photo:

"King Steam" still reigns as we meet V 80 008 in its depot, as can be seen from its facilities. But that will be a thing of the past in a good two decades. In the model, it will certainly not take so long for the design principle of this locomotive to become established.

Our Editor's Choices for 2020

New Products of the Year

A difficult year lies behind us and the number of candidates for our editorial award is lower than usual. Nevertheless, it was a competition and exciting in the end. After long discussions, speeches of praise, exchanging arguments and promoting personal favourites, everyone was, nevertheless, largely in agreement.

What an exciting and extraordinary year lies behind us! We will probably remember it for a long time, although we would rather forget it quickly. The few positive things it left behind were the increased and more time-consuming dedication to the most beautiful hobby in the world.

It is only stupid and difficult for us when, despite great demand, not all wishes can be fulfilled promptly. By this, we do not mean that someone would have to interrupt their landscape construction for lack of paint, scatter (landscaping) material or kits, but rather the unfortunately slow delivery of new products.

When factories are affected by temporary closures and production is temporarily interrupted due to a lack of parts and components, it is impossible to make up for lost time later in the year.



Due to the temporary shutdown of economic life (not only) in Germany, there were only a few, new-form wagon and locomotive models in 2020. One of them was Märklin's V 80 008 (item no. 88803), which was delivered with a delay of about three years.

Modern business management demands that machines are fully utilised, and that over-planning is always carried out to avoid downtimes, in order to be able to compensate for failures. Only in this way is efficient operation possible in the long term.

With the events of last year, this naturally creates situations in which an expensive production stop nevertheless occurs and at the same time no time window can be created to make up for all failures through extra shifts or overtime.

Strangely enough, we also noticed that our magazine seemed to be temporarily empty only at the very beginning of the pandemic because of postponements regarding planned reports. Only a few weeks later, we could no longer save ourselves from topics and articles.

Whatever we had planned and now had to reschedule, we were then faced with the problem that our issues almost threatened to burst in size even without completely new products. At the peak of this development, we even exceeded our anniversary issue 100 from November 2013 by one page with 101 pages!



The motorised and thus powered Kliv 20 (88025) from Märklin was also in our selection. It proves that it is not simply a question of the number of new shapes, but that unusual ideas can also make up the special charm.

Fortunately, such a situation is unique in the history of our magazine, so we don't want to complain, but look forward and enjoy the new models and products that have reached us.

Of course, in view of the problems described above, the selection was smaller than usual, but in no category did any applicant stand alone to “walk away” with our coveted prize. So, it was once again a competition and the editors had a lot to discuss, evaluate, and vote on for the award.

It was not easy this time either, although in the end, when all the arguments had been exchanged, almost all the decisions were unanimous. Traditionally, it is not simply the perfection of a model or the price of a new product that counts, but the balance of many individual criteria.

The size of the gap in the market that was filled for the first time by a new design continues to play a major role, as does the market significance of a product in a comparison of all candidates. What is meant here is the estimated size of the target group that can be served by a model.

The more exotic and specialized an item is, the fewer people who would buy it. Our rating would then only be a recommendation to buy for a very small circle of customers, and, for example, newcomers and beginners would be left out, although they would certainly be grateful for advice and help here.

Hopefully, this also makes it clear that no criterion is absolute and that there is always a relationship between the individual candidates. Thus, we have deliberately refrained from using a predefined weighting or a fixed points system.

Every decision is subjective and should consciously be so. The necessary distance in the sense of objectification results solely from the fact that everyone votes for themselves, and an uneven number of votes is collected. So far, this has always ensured a clear majority and also captures the different tastes and preferences of the editors.

With that, it is now time to choose and honour the winners of 2020. We warmly congratulate all the winners, who are announced below.

Category Locomotives

In the most important category for many readers and also for the manufacturers, three models competed against each other: the DR class 120 "Taiga drum" from Zmodell (art. no. 99120), as well as, the railway master's car Klv 20 (88025), and the DB mainline diesel locomotive V 80 (88803) from Märklin.



The third and last locomotive to enter the race for the editors' favour in Zmodell's most important category was the class 120 "Taiga Drum" of the Deutsche Reichsbahn (99120). Photo: Zmodell

The DR's class 120 may be an important locomotive for Reichsbahn enthusiasts, especially since this market segment is only very sparsely served. Creatively, many series parts from Märklin were mixed into its running gear, resulting in a convincing overall construction. The many detailed solutions on the superstructure also deserve great recognition.

But, in order to win the prize, this was still too little for us in a field with strong competitors. On the one hand, the chosen solution also leads to a high price, which strongly limits demand. And, here we are also dealing with one of the smallest niches in Z gauge, where only extremely small quantities can be produced anyway, which is why the significance for the overall market is simply too small.

The situation was quite different with the Klv 20 of the Bundesbahn. Due to its tiny size and the fact that it also falls into the most popular epoch III, it seemed, at times, almost like a storm for the first delivered models.

And Märklin fiddled for a long time until a really layout-suitable vehicle came out. In addition, the manufacturer outdid itself in the printing of the company inscriptions. Many important details that would have been much too fine for engravings have also been reproduced in the pad printing process.

We also found evidence of the sometimes controversial shape of the front for the selected operating number, as well as pictures of loaded roof racks and (open) boxes that were loaded on them.

Even if they did not originate from the DB's operating time, it should be clear that such equipment is not unrealistic.

But the Klv 20 has a small catch that kept us from the award. While it may be forgivable for the viewer's astonishing use on the layout, because it will be noticed by very few, the implementation scale makes it pretty much exactly in the middle between the nominal sizes N and Z our problem:

It is "a delicacy, but that is too little," as editor Joachim Ritter put it in a nutshell.

So we give the award in this category to the class V 80 from Märklin (88803). "Its shape is not to everyone's taste, but it is a previously missing link in the development history of diesel traction," said Editor-in-chief Holger Späing, praising the choice of prototype.

Visually, too, it is right up there with its equally excellent predecessors, the Kittel steam railcar and the E 41 series unit locomotive, which also boasted a great deal of detail. The successful propulsion system is also on a par with them.



We also received a lot of reader feedback on this, which editor Dirk Kuhlmann recorded as follows: "In the case of the V 80, it should be noted that many railroaders attested to the model's fine design with good operating characteristics - a good hit from the house of Märklin."

Indeed, we have received unanimous approval that the prototypical slow running of this model with a wide control range is explicitly desired and the right direction to take. The problem on the layout is not this model, but the "racing sedans" of earlier days, as Holger Späing likes to jokingly call them.

This makes this locomotive our 2020 prize winner, and Joachim Ritter sums up the laudation in a single sentence as follows: "We have waited long enough—now it is here, and it is convincing."

Category Wagons

Here, we would have liked to include the Märklin SSy 45 heavy goods wagon in the vote, because we have high expectations for this model. This will be at least the fourth Z gauge model, but, so far, none of them has been able to completely convince us. One of them also appeared long before the first issue of our magazine.



But, we will probably not be able to test and evaluate it until 2021. We also did not physically have the "Villach" type open goods wagon from FR Freudenreich Feinwerktechnik, which has, so far, only appeared in a DR version.

In this respect, it unfortunately had to be omitted, although we are sure that it would have been a great favourite.

The decision was, therefore, reduced to a choice between the open freight car Omm 32 of the DB 49.339.02), also from FR Freudenreich Feinwerktechnik, and the covered freight car GGths 43, for the DB (96051) from Zmodell.

What made this decision difficult was the fact that both manufacturers have delivered really good models with excellent detailing, painting and lettering, as well as almost perfect scaling.

They also share points that could speak against them: The segments of open and covered cars have been very well and actively served in the last ten years. So there is little need for new models here, at present.

And in such situations, even small things count to tip the scales in some direction. In the case of FR, it was the fact that of two possible designs, precisely the one that leaves fewer options for the future was chosen, while Zmodell chose DB's only four-axle high-capacity freight wagon that has a unique selling point under the main class sign G.

This is precisely what has made it a significant desired model that has been on many wish lists for a long time. And the high demand, which Alex Mark has not yet been able to meet conclusively before turning to variants for DR, ÖBB and PKP, also speaks volumes.



The well-done GGths 43 has successfully occupied a niche and probably brought Zmodell a breakthrough as a small-series supplier. We also honour this model with our award.

For Zmodell it was a breakthrough from tinkerer to small-series manufacturer, against their will? If we had initiated a readers' poll, this wagon would have emerged as the winner by a landslide in our estimation. And that is exactly why it receives the award as the best new release of the Year 2020 in the category wagons.

We even have to add one more point: It also has a narrow lead over its competitor in terms of detailing. This concerns the fine but quite stable railings on the brakeman's platform and above all the design of the wagon floor, of which the separately, also coloured shifters, are the only fine details that are also visible on the re-railed wagon.

Category Accessories

This category required long discussions and we were torn. The choice here was at least between three new products that are comparable in that their models were all (also) on the road.

The decision had to be made between the slurry tanker with cows (NF 6.1) from KoMi-Miniaturen, the Schi-Strä-Bus NWF BS 300 (WM-HRT09-001) from the 1zu220-Shop and the Krupp-Dolberg excavator (322.025) from Artitec.

Only the Schi-Strä-Bus was eliminated early, because the model is very popular and fills a gap, as Holger Späing noted, the very low implementation quality was not appropriate for the price: The track bogies fit



A very promising candidate in the accessories category was the slurry tanker with pulling cows by KoMi-Miniaturen (NF 6.1). It was only narrowly beaten by the winner in the voting discussions.



Artitec serves the previous gap in the programme of cable excavators from the economic miracle era with two models that threatened to cannibalise each other. Here, you can see the Dolberg crane.

poorly on the track and even with a lot of reworking, it is not possible to conjure up a top model from the rather rough housing.

But, of course, it fulfils the purpose that Jörg Erkel from the 1zu220 shop had assigned to it. It should be cheap, i.e., affordable, and thus appeal to a large and certainly also price-conscious target group. But, it will hardly convince a big fan of the historical model.

The slurry tanker with cows (KoMi) and the Krupp-Dolberg excavator by Artitec, on the other hand, were evenly matched. Not one of the editors wanted to make the decision too easy.

Manufacturers delivered perfect models and the only question is, therefore, what the market needs more of at the moment. And that tipped the scales narrowly in favour of Artitec, who successfully defended their title from last year.



When it comes to agricultural vehicles of Era III, the Zetties have been quite well catered for in recent years, and not only in the case of the tractors that are almost over-represented here.

However, apart from the Fuchs 301, there were no excavators for this era. Larger versions with chain drive, as they partly characterised the reconstruction, but in any case the economic miracle, were not on offer anywhere.

Especially the classic Menck cable excavators were on the wish lists for a long time, says Holger Späing. Artitec has now delivered models of comparable size for the same purpose and thus closed a large gap in the market.

In this context, the question should almost have arisen sooner as to whether the now excellent Krupp Dolberg excavator or the similar Dolberg crane from the same company would be awarded a prize.

In view of the importance for Z gauge, which is also shown by the out-of-print first edition, we nominated the excavator. If both had competed, they might have cannibalised each other.

The final words of praise go again to Joachim Ritter: "With this excavator, Artitec has realised a dream model (for me) and inspires with great implementation. This is exactly what we need for a credible environment for rolling trains. Fine details, coherent appearance - simply perfect."

Category Technology

For a long time, Ratimo was the great favourite here with the "wagon crossing with plug" (16028; addition to Märklin's VT 08.5). The open "tubes" were the significant weak point of this beautiful train, and after there was a visually appealing solution for Velmo-digitised models, Rainer Tielke must have been approached with many wishes for many analogue running examples.

And, it doesn't always have to be the big innovation to win in this category. It is also the smaller and, at first glance, more inconspicuous things that can achieve great things. Rainer Tielke knows this too, which is precisely why he was the winner in this category in 2013.

The fact that he narrowly missed the podium this time in a majority decision, the only category without a unanimous vote, was probably only due to the fact that Märklin made decisive improvements to its winner.

The idea for the winning WGmh 824 (87210) with effect lighting excited us from the start and the shortcoming of the unbuffered and therefore voltage-sensitive electrics could only be partially tolerated here. Since Märklin has retrofitted capacitors, this problem has also been eliminated and the model now attracts attention, exclusively, in a positive sense.



A hitherto unique idea in Z gauges was the WGmh 824 lounge car with its disco lighting (Märklin 87210). After this was reworked in the factory, it was able to convince us.

The fact that the window arrangement is not quite correct in one place (and only on one side) and that the additional ventilation openings have been reproduced by pad printing can be forgiven. This is more than outweighed by the fact that this vehicle can also withstand digital voltages and was, therefore, able to appeal to larger group of customers.

It should also be explicitly mentioned here that this model was nominated in the technology category, because it was all about the "inner values", after all, the basic model was not new.

As the winner of this category, Märklin would certainly be well advised to continue its creative search for ways to further enrich Z gauge with smart ideas.

Category Literature

We missed a lot in this category last year. It seemed to us that, as a result of the pandemic, far fewer titles were published, than usual.

The takeover of VGB by Geramond has certainly shifted some things in the market and may have had an impact here. In addition, we only found quite a few books worthy of titles that we held in our hands for the first time in 2020.

So here, too, it came down to a duel. The book "Die Vorkriegs-Elloks der Reichsbahn" (978-3-8446-6037-1; "The pre-war Reichsbahn electric locomotives") from EK-Verlag was the winner and gives a well-structured and valuable overview of the many el-locomotive designs of the Reichsbahn. They are presented here in due brevity, but nevertheless in sufficient detail.



In addition, the book also complements two volumes on old construction machines in the service of the Bundesbahn to form a meaningful trilogy.

The fact that it was not enough for the title is only due to the fact that this book is more suitable for railway historians and prototype fans of electric traction.

It offers far less added value for Zetties, because it deals with vehicle types that play almost no role in 1:220 scale.

This might have been different if the portrait of the 01 series, which we will be presenting in the next few months, had been available in time for us to evaluate it here.

In any case, the two volumes "Straßenroller der DB" by Volkhard Stern published by VG Bahn were a hit.

They went far beyond the topic mentioned in the title and also paid tribute to the anniversary of the Deutsche Reichsbahn last year.

We have nominated the volume that is the more important of the two for users of nominal size Z: "Straßenroller der DB - Band 1" (978-3-8375-2239-6).

Only the regular services shown in it can be reproduced with the few Z gauge models and, at the same time, show which new products would be conceivable and sensible for our scale.

The readers' reactions also show that the topic is a moving one. There is still potential here, which I hope the small-series manufacturers in particular will recognise and take up. If the 1u220-shop allows the sample of a more modern Kaelble tractor to be worked on in the areas of the bonnet and radiator and to go into series production, a start would already have been made.

Then the conclusion from Holger Späing's praise certainly fits: "The book helps to use the few models correctly and to depict this interesting chapter." The other two editors could also agree with this.

By the way, EK-Verlag, as a permanent contender for the title and VGB, have now drawn even with each other with four awards. We combine this with the hope that under the new owners, the up-and-coming segment of sophisticated and valuable railway books of Verlagsgruppe Bahn will be continued and perhaps expanded. Otherwise, EK-Verlag would probably be almost without competition here in the future.



Category Exhibits

The proposal for this new category was put forward by editor Dirk Kuhlmann and immediately found favour with the entire editorial team. Our first prize-winner was also already decided, because he had made a deep impression on all three editors.

But first things first - why should we continue to honour only manufacturers? At best, private individuals could also be honoured for their merits and works in the category of complete works. At trade fairs and exhibitions, of which, there were unfortunately far too few in 2020, model railway layouts are the crowd pleaser.



The first winner in the new category Exhibits is Wilfried Pflugbeil from Chemnitz with his exhibition installation "Wiesenthal". Photo: Wilfried Pflugbeil

The demands on the exhibited installations have changed enormously in the last ten to twenty years. The most captivating layouts are those that are narrowly and recognisably limited to one theme, and present that theme as coherently and conclusively, as possible.

Good and well-thought-out landscaping with no overloading with tracks is in demand, and having your own lighting helps enormously. Not to be forgotten, today, is a suitable background that creates realistic impressions and provides depth.

Spectators should be able to immerse themselves in a layout and dream themselves into it. Whether it is a perfect world or exciting scenes is of secondary importance. A good model railway is more like 3D cinema: in the middle of it instead of just being there!

Moving elements, functions or even button-pressing actions can of course increase the attention and increase the time spent. This describes what we consider worthy of an award and would like to bring to the public's attention here. The size of the layout does not matter, which is why a module or diorama can also be victorious here in the future.

The first layout we are going to award today fulfils all the above criteria and does so almost perfectly. Its builder Wilfried Pflugbeil from Chemnitz was not originally a Zettie. He knows his craft and has sought the challenge, and, now, has also passed his master craftsman's examination with flying colours.

His "Wiesenthal" layout is simply a work of art that is also set in the rarely shown Era II. It is therefore refreshingly different and in addition to the excellent design quality, there are also the many buildings that were handcrafted according to original models.

And his sometimes witty but always entertaining features never fail to inspire, which is why he has made no exception with this layout. But they also inspire with their technical understanding, craftsmanship and great expertise.

What should we add now? Let's keep it short, because as if from the same mouth, the three of us here were told early last year: "Willi, you've earned it!"



Complete Works

We have been awarding our last category, which is still missing for this year, since 2010. Our first award winner was digital pioneer Velmo (Claudius Veit). Our current award winner has also been adept and almost at home in the digital world. He has also shown pioneering spirit when it came to appealing and convincing solutions.

We are writing in the past, as you will notice. And indeed, today we have a posthumous tribute to pay. We have deliberately called this section complete works because we do not want to honour people at the end of their work or even their lives, but rather to rejoice with them.

Unfortunately, this was no longer possible for the second time. Joachim Ritter commented: "We would have been happy to postpone the award for many more years or at least to award it during his lifetime - but, now it is time to remember his always outstanding work in this way once again, and to pay him our respects.

We are talking about Oliver Passmann, who left us far too early, and far too young, in January 2020. The gap he left in Z gauge is large and has not been filled yet. Unforgotten are his operationally safe wheel electrical (contact) sliders, which did not affect the rolling capacity of wagons at all.

But even these he knew how to improve over the years. This also applied to his interior lighting (also with tail lights), which set standards, were flicker-free and frugal. Adapted to the interiors of another supplier, they later found their way into pioneering small-series models designed and produced by FR Freudenreich Feinwerktechnik for the Z-Bahn GmbH.

And who still remembers the legendary Passmann controller? It made an ordinary Märklin class 050 steam locomotive with a three-pole motor creep along in such a way that its movement was hardly perceptible. With this, he had vividly demonstrated that the well-being of the teddy was not solely dependent upon the motor, but much more on the track supply.

As an experienced electronics expert and digital connoisseur, his work also included customer conversions. He counted many digital enthusiasts among his customers, for whom he was always able to make the best offer for their model and their ideas.

But even that was not enough for him. Exclusively for the 1zu220-Shop, he made Märklin's latest railbus edition (with the standard interior) truly contemporary and a top digital model.

Demand was immediate and in large numbers, but this model remained rare because it was to be his last: Even before he could finish the agreed series, he died surprisingly and completely unexpectedly.

Many fans of our gauge will have already missed his products, which we have mentioned as examples, as well as probably the personal conversations with him.

Therefore, we would like to set an important precedent here and pay a deserved tribute to Oliver Passmann, the quiet, kind and serious, but easy-going model railway enthusiast. It is a pity that we can no longer tell him in person...



Webpage offerings from our winners:
<https://artitec.nl>
<https://www.maerklin.de>
<https://www.vgbahn.shop>
<https://www.facebook.com/Zmodelltrains>

Advertisement

An advertisement for Trainini magazine. On the left, the headline 'Unsere Nummer 1!' is written in a large, red, cursive font. Below it is a white rectangular box containing the 'Trainini' logo in red and the slogan 'Dafür wird Spur Z gemacht.' in blue. On the right is a thumbnail image of the magazine cover. The cover features the 'Trainini' logo, the text 'Praxismagazin für Spurweite Z', and a photograph of a train crossing a bridge in a scenic landscape. Additional text on the cover includes 'Ausgabe 98 | Juli 2008', '4. Jahrgang', 'Kostenlos, privates Online-Magazin für Freunde der Bahn im Maßstab 1:220', 'www.trainini.de', and 'Leserantage Jürgenstadt Zu Besuch im Mittelburgenland'.

Reaching the goal by the simplest means

Do-it-yourself Half-luggage and Half-coach Wagon

For years, Zetties have been waiting in vain for passenger coaches from new casting moulds. Even in 2021 it does not yet look as if Märklin will listen to the wishes of its customers here. Among the railcar models that are at the top of the wish lists are “half-luggage and half-coach” wagons. Our reader Michael Pleiner likes to tinker with paper and cardboard. So he has now also found his way to his big dream model.

By Michael Pleiner. “Half-luggage and half-coach” wagons were still a frequently used railcar type for express trains in the second half of the 80s. But Märklin has never produced such a wagon for Z gauge, and I do not know if a small series manufacturer has dealt with this topic.

But for N gauge this railcar can often be found in catalogues and new product brochures. This gave me the opportunity to reproduce this wagon for Z gauge, which I would now like to describe using the example of the ocean-blue/ivory-coloured version.

My basic railcar is a second class express train car of the same colour (item no. 8721). I used the very good illustration of an N-gauge model as a template.



The lack of such a frequently required half-luggage wagon at Märklin for almost 50 years now made our reader go to self-help.

First, I made a scaled copy on the colour copier, making sure that the window spacing of the compartment side exactly matched that of the base car. The second working template is then a mirrored copy.

Then, I took out the inserts of the window panes and glued the unnecessary windows on the luggage side flush with pieces of cardboard. Then, I cut out the necessary windows of the compartments and boarding doors on the copy with a scalpel and a graver stencil.

The graver stencil was a piece of cardboard in the size of the filling pieces of the window openings, which was fixed to the window surface with a drop of photo glue and additionally pressed on with the shaft of a pair of tweezers during the engraving process. After that, I checked the positions of the windows of the copy and the wagon. In order to emphasise the folding doors of the luggage section a little more, I traced the door gaps with a fine black ballpen.



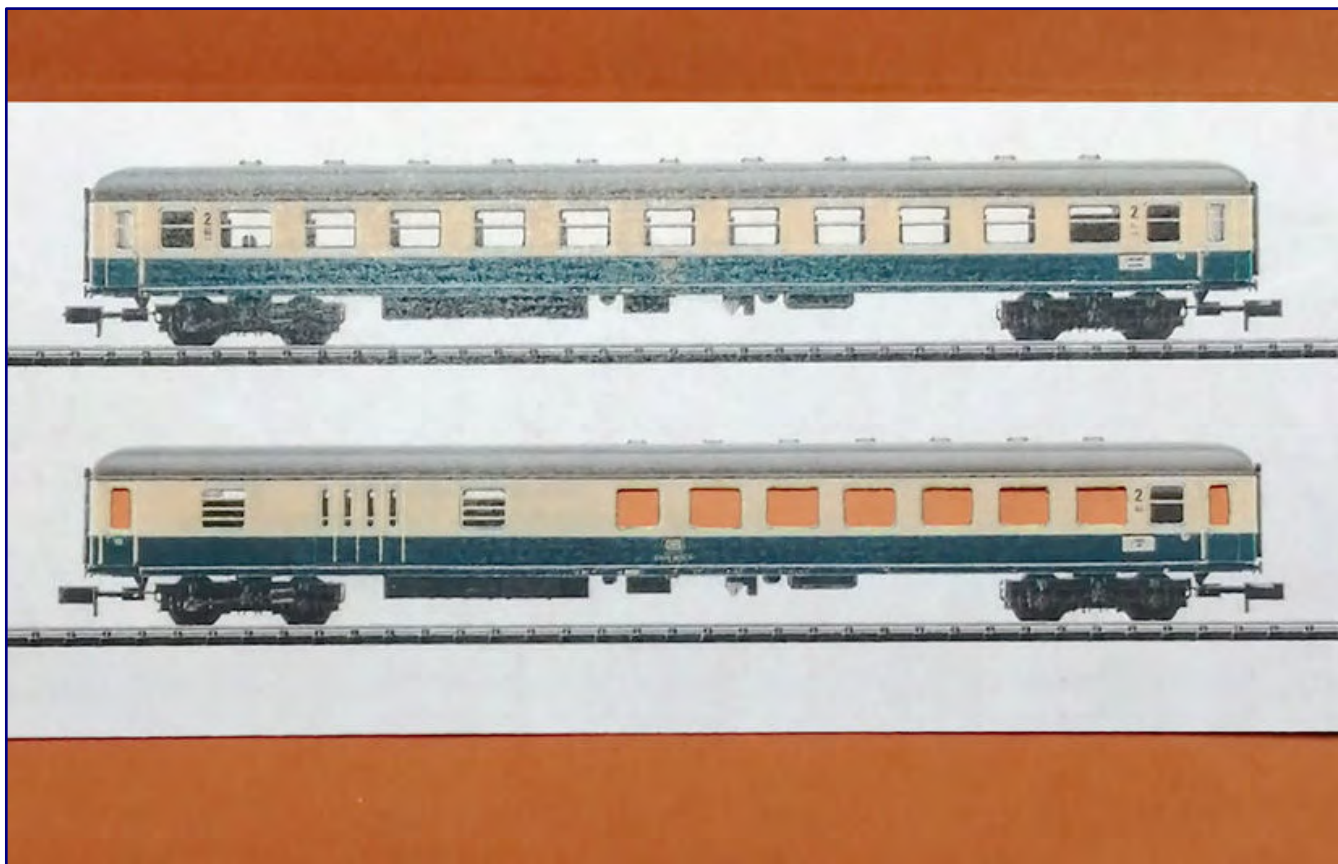
A 2nd class compartment wagon Bm 234 from Märklin (item no. 8721) and a scaled N-gauge catalogue illustration (at the bottom of the photo) formed the basis for the unusual do-it-yourself solution.

Then, I cut out the sidewall prints along the edge of the roof and directly below the beige-coloured dividing line between the wagon floor and the wagon body and glued them onto the railcar. Photo glue has the distinct advantage that it can be applied very thinly and over a wide area, and the print is removable. Drops of glue that escape when pressing it on can also be rubbed off easily.

Furthermore, the advantage of the optically reinforced gap of the folding doors and the use of prints becomes apparent at this point at the latest: Depending on the photocopy template, the new side wall of the railcar can be longer than required.

In this case, it is possible to cut the sidewall print with a scalpel along the gap that is as closest to the centre of the railcar and to shorten the rest on the compartment side accordingly. This is not visible after gluing the compartment side, because the black marked door gap made in advance makes the cutting line invisible. And, if something went wrong, a new print could be made quickly.

continues on page 20



Do-it-yourself in three steps: The template printed in the correct wagon length is cut out in the area of the windows already present on the model (photo above), while the window openings not required on the model in the future are closed with cardboard filler pieces (photo middle). After the template has been cut out along the roof and frame edges, it can be glued to the wagon body with photo glue (photo below). The wagon is now ready and does not require repainting.

After the new side wall prints have been glued onto the coach, the window inserts are put back in place. On the luggage side, a compartment window and the window insert of the door must be glued in separately, as the previously filled openings no longer allow the use of the complete window insert.



In the same vein, Michael Pleiner has built two more BDms half-luggage wagons, of which, the one in pop colours, is particularly appealing.

Contrary to the original, I have left the ventilation attachments on the luggage side of the roof, otherwise I would have had to repaint the roof. Nevertheless, I am very satisfied with the result of my redesigned car. Even at arm's length, it is no longer recognisable that it is merely an optical illusion.



This trio in various colour schemes now further complement the layout fleet of our reader and eliminates the decades-old lack of suitable scale models in 1:220.

So now another attractive vehicle complements my ocean-blue-ivory-coloured rolling stock. But as is well known, every coin has 3 faces: heads, tails and edges. And the half-luggage wagon is also available in chrome oxide green and pop colour paint in N scale, so I had to take action two more times.

All photos: Michael Pleiner

Manufacturer of the basic model:
<http://www.maerklin.de>

Maintenance & Care (Part 9)

And Then Postponed for a Long Time

It has been about one and a half years since the last article in our maintenance and care series, which is intended especially for beginners, and those returning to model railways. Today, we would like to show how dusty Märklin transfer tables can be cleaned and restored with a few simple steps and skilful intervention. After that, they can transfer our locomotives again for a long time.

In times when people feel cooped up and unable to pursue other habits and passions, model railways are once again finding great popularity. Manufacturers as well as clubs report that the questions addressed to them have greatly increased. Our editorial team can also confirm this impression.

However, it is also noticeable that these questions increasingly come from inexperienced hobbyists. This proves the need for qualified help, because new fans are indeed finding their way to model railways and want to create something beautiful within their own four walls.

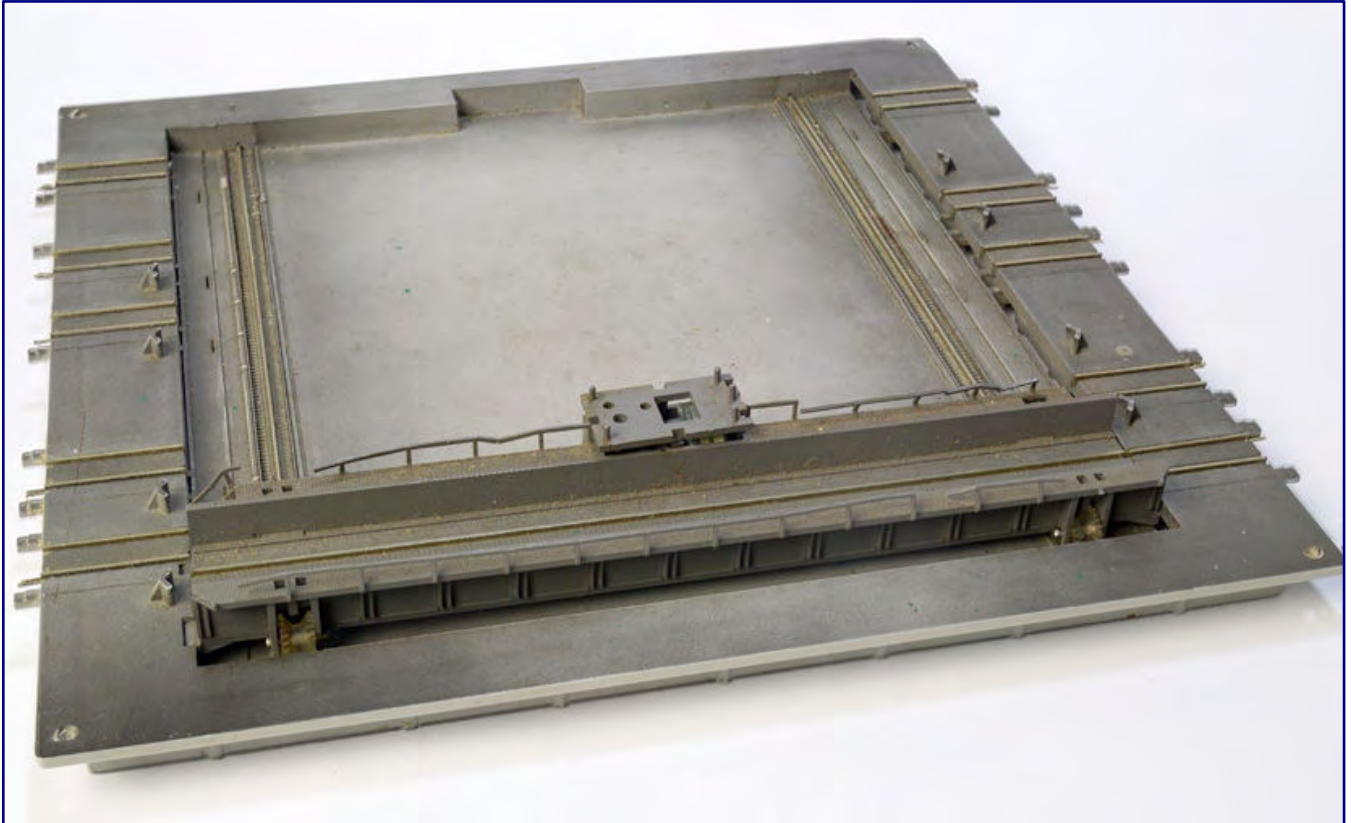


The appearance of Märklin's transfer table is long out of date, but with a few paint jobs on the railings and especially the guard's house, its face is transformed. And, so, it is also worthwhile to restore a model that has become very dirty for everyday use. Photo: Matthias Wistrach

Often, the questions do not revolve around landscaping techniques or the planning of track layouts, but, rather, around how to clean and maintain one's own rolling stock or accessories. Especially in Z gauge, this often brings beads of sweat to the foreheads of newcomers the first time around.

“What do I have to disassemble to get to the motor and gearbox parts without breaking anything?” is a typical question we are usually asked. We have already provided a lot of help in our series, which we have compiled chronologically at the end of this article.

Today, we would like to devote ourselves to Märklin's 8994 (1977 - 2002) and 89941 (2003 - 2014) transfer tables, which were certainly the only editions offered with the classic motor technology. In case of a new edition, Märklin will, certainly, also convert this model to a bell armature drive. Without additional modifications to the superstructure, this accessory would, also, no longer appear to be current.



This incomplete and badly damaged Märklin transfer table (item no. 8994) is to be cleaned and serviced today, so that it can again provide faithful service for many years after (future) superstructure work.

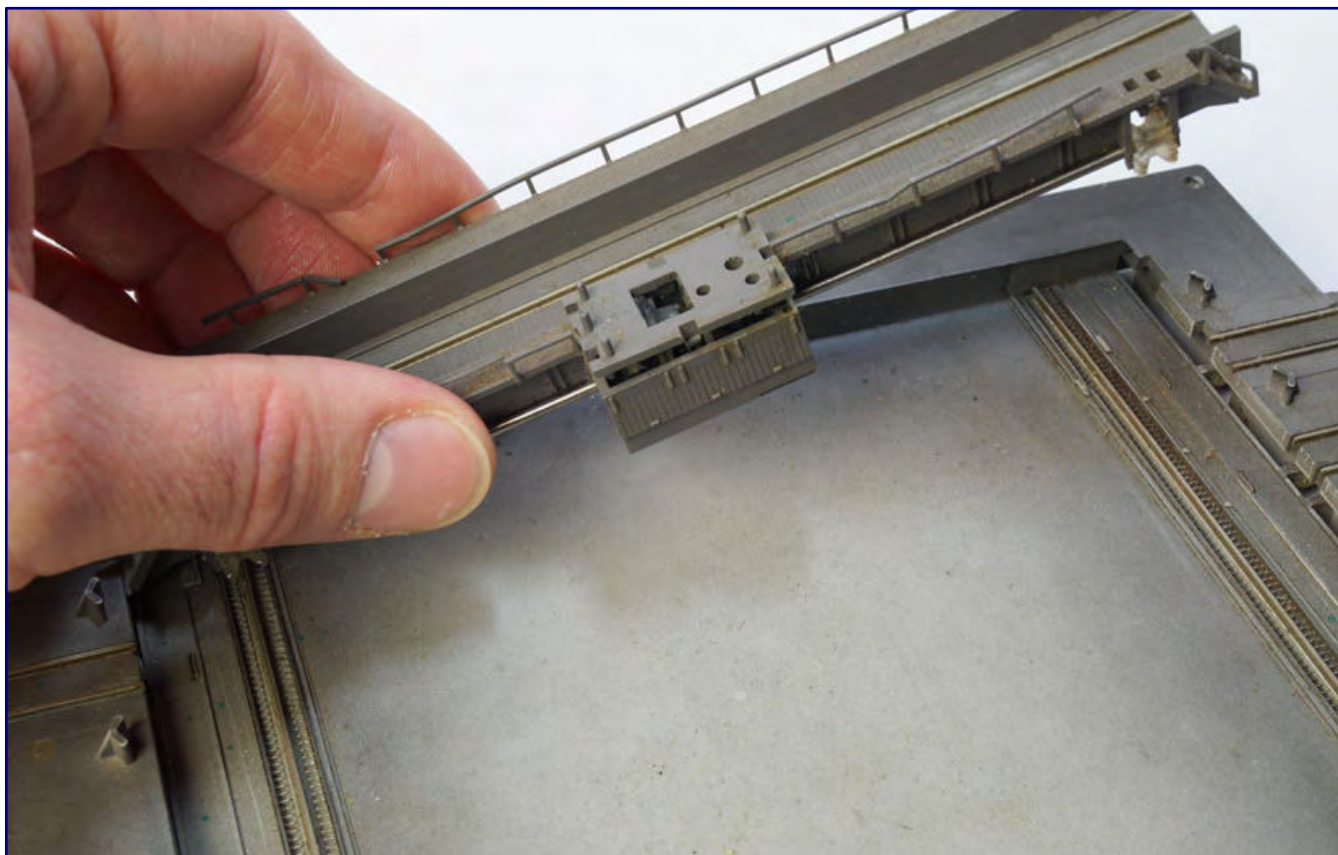
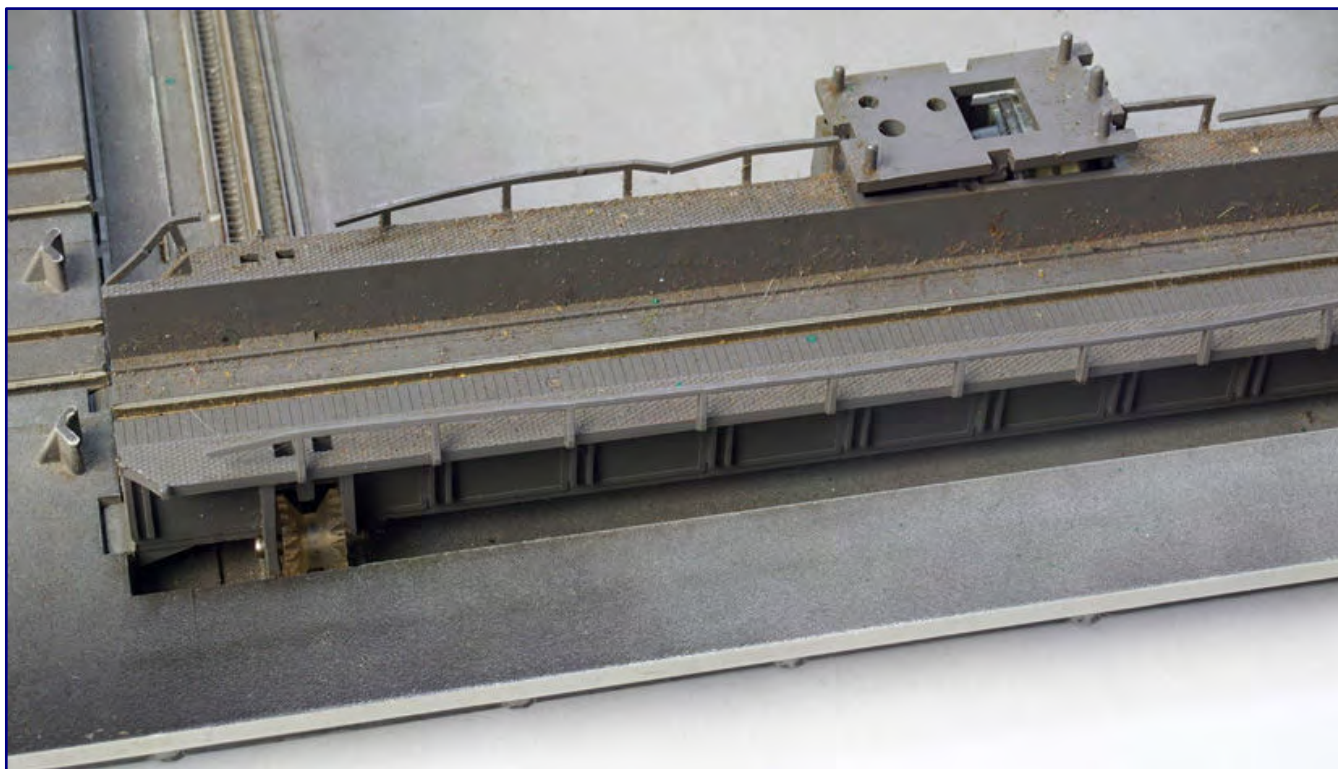
With this, we now turn to the three conceivable maintenance and servicing tasks that can be performed on this model:

- Cleaning of dirt and dust contamination,
- Re-greasing the gear parts and shafts, and
- Replacement of worn brushes.

Dust lying on the platform can be easily vacuumed off. Care should only be taken that the hose and nozzle of a Hoover do not damage or pickup parts, as they could disappear into the bag.

A nozzle that is as fine as possible and having a second person guide the hose can help prevent this. With the help of a bristle brush, you can sweep out corners and then let the vacuum of the vacuum cleaner capture the dust that has been stirred or swept up.

continues on page 24



The transfer table we worked on is extremely dusty and also dirty (photo above). Such traces show after years of use on a system that is not protected from dust during breaks in operation. The platform can be removed from the transfer table pit for cleaning, if it is moved to the end (of the transfer table) opposite to the guard house. Then, it can be raised at an angle on one side and lifted off in this position (photo below).

However, despite all caution and also a check for possible loose parts before starting work, it can happen that the suction power of the vacuum unit causes the finest parts, especially figures placed near the transfer table, to disappear into it.

An old trick, well known among long-time model railway enthusiasts, prevents this: We pull an old nylon stocking over the nozzle or the Hoover pipe. Fixed tightly with a rubber band, it does a good job, because its fine pores allow sufficient air flow while effectively holding back models, small screws, or even figures.



An old trick, well known among long-time model railway enthusiasts, prevents this: An old nylon stocking is placed over the nozzle or the Hoover pipe. Fixed tightly with a rubber band, it does a good job, because its fine pores allow sufficient air flow while effectively holding back model parts, small screws or even figures.

A fresh bag in the vacuum cleaner can also prove its worth: If something does disappear in it, the contents can be poured out and the manageable contents searched, as long as there are no house dust allergies.

Another proven tool was the dust witch, patented in 1984, and once distributed by Poscher. This is an attachment for the Hoover tube that directs the air so that it swirls up the dust in front of it as a nozzle at a distance from the surface and only then sucks it in.

But this device is no longer available new, and is also very rarely offered second-hand. Lux-Modellbau had announced a reissue at the 2017 Toy Fair. Since then, however, the delivery date has only been postponed from year to year, and due to a lack of feedback from the manufacturer, nothing seems to be in sight.

Do not provide a stage (platform)

Up to this point, it makes no difference whether the transfer table is installed in a layout, or it is dirty due to unprotected storage, and we want to prepare it for installation.

But we would also like to present a way to remove more stubborn adhesions without damaging the electronics and drive. This only succeeds if we can remove the moving platform from the pit, and it can also be helpful with the steps presented before.

The instructions for the accessory part, which can also be called up electronically from the product pages at Märklin, if missing, indicate a defined end position in which the part can be removed: To do this, it must be moved completely to the end of the pit opposite the control's house. On this side there is a siding on one side only.



The arrows in the photo indicate the guide lugs on the platform (left) and the corresponding recesses at one end of the pit (right), recognisable by the lack of a track exit there, which enable removal by tilting.

Once there, the platform can be removed diagonally upwards with a simple handle (and later reinserted in the opposite direction). We will also carry out maintenance work on it in a moment. Before that, however, the plastic pit should be completely cleaned.

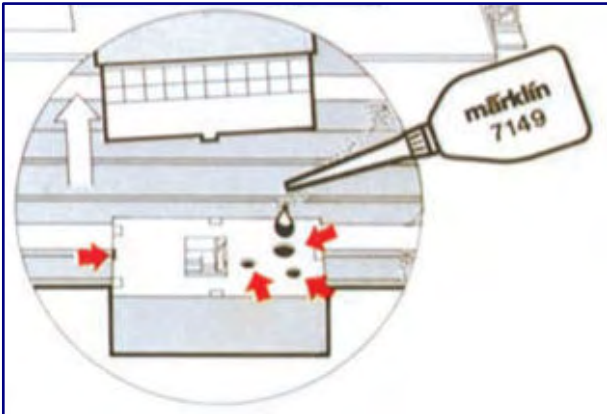
In our case, with an incomplete transfer table, and with parts damaged, as well as heavily soiled, as it is often called an "attic find," vigorous cleaning work was necessary. After removing the platform, an old toothbrush was used.

In combination with a mild cleaner (neutral soap or a dishwashing detergent), all dirt can be removed and the model looks the same as it did when it was new. Be careful with the platform insert, because water must not get into the engine, and we do not want to allow rust to find a foothold.

However, the surfaces can be cleaned carefully with a little moisture in a similar way, because the loose part can now also be turned, so that no water can run into the housing under the control's house.



With an old toothbrush and a suitable cleaner, we removed dirt adhering to our transfer table (photo above). The subsequent look at the bristles (photo below) and the visibly soiled cleaner indicates how required this form of intensive care was in our case.



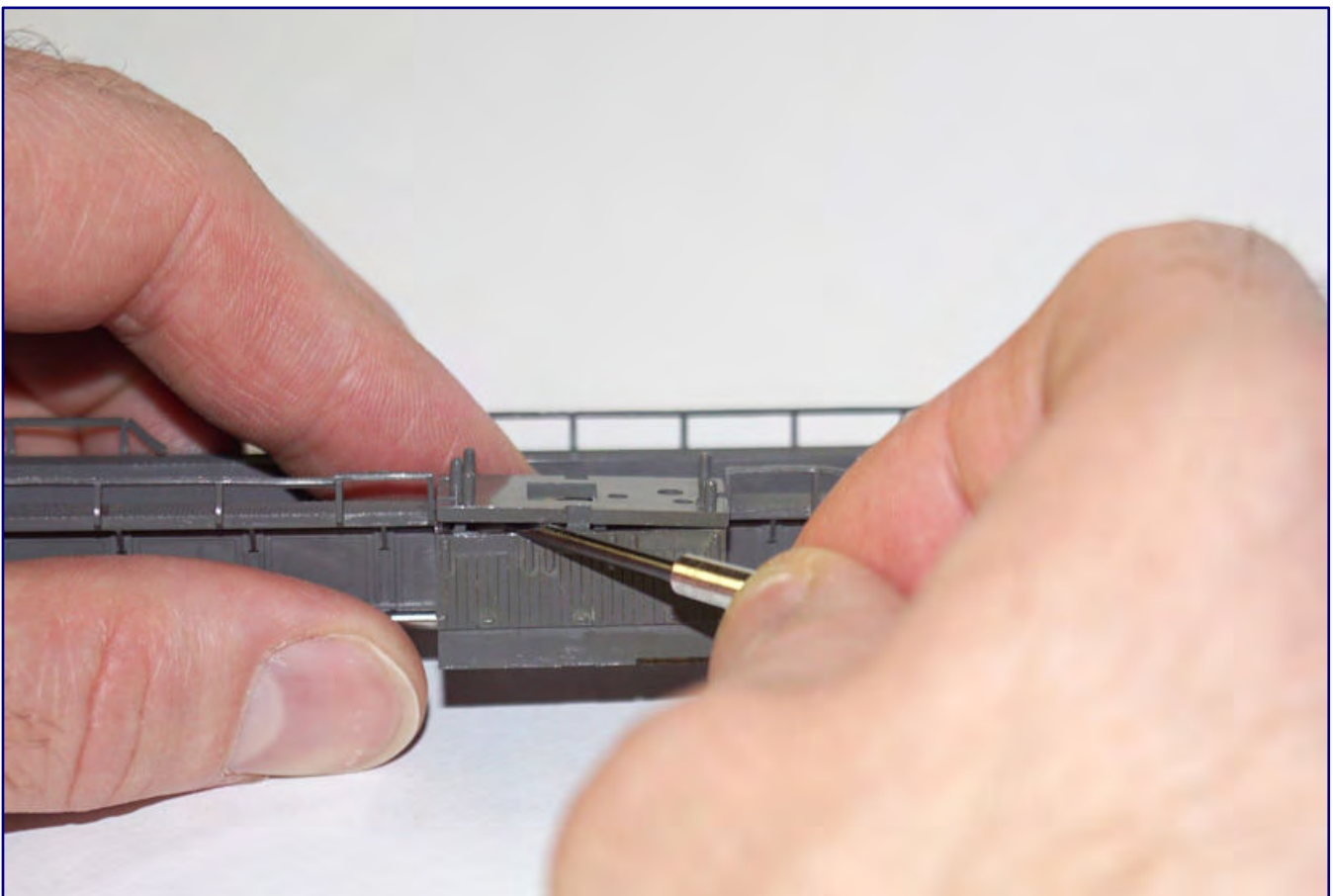
The control house is simply pulled off upwards, as the instructions enclosed with the product show. Märklin also marks the openings for inserting new lubricant there. Illustration: Märklin

carefully applying pressure several times and from all sides, we obtain the desired access.

But, before we start reassembling, we would like to service the drive. It is enclosed on all sides in grey plastic and sits under the control house. The housing is simply and carefully pulled upwards, according to the operating instructions.

After that, the interference suppression capacitor is already visible, as well as openings through which the lubricant is introduced according to the instructions. But, that is not enough for us, because, here too, we expect dirt and possible grease accumulation.

So the cover has to be removed. It can also be pulled straight up, but, at first, it is hard to grip. The blade of a watchmaker's screwdriver (slot) helps us here by penetrating the tiny gap, and using it as a lever. By



The drive is located in the grey box under the control house. To get to it, we have to lift off the cover, which is held in place by guide pins and latches. With a fine watchmaker's screwdriver, applied sequentially in different places, we carefully prise it loose, until we can also pull it upwards.

continues on page 29

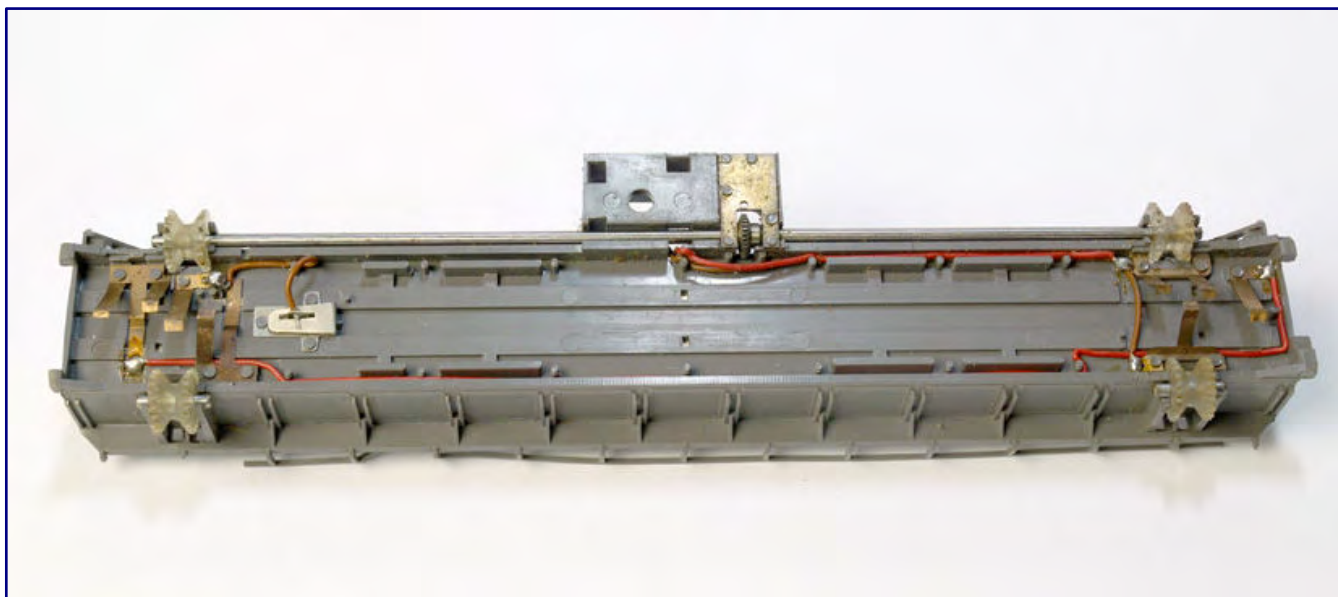
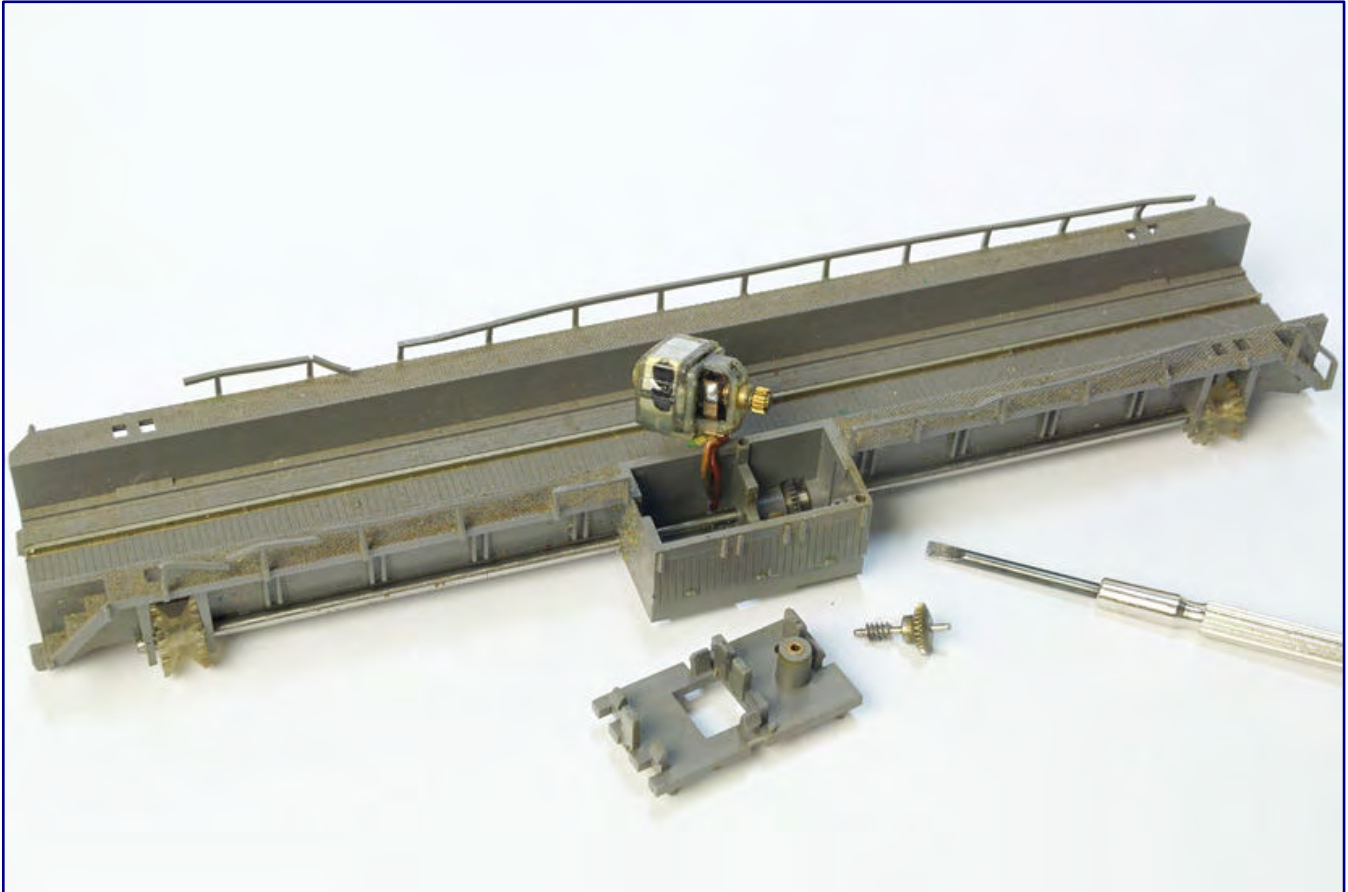


Photo above:
As expected, the underside of the transfer table is much less dirty than the surfaces visible from above. Nevertheless, we naturally also include them in the cleaning and maintenance steps.

Photo below:
We could remove coarse dirt in the manner already described, but special care and attention are always required for the sensitive contacts, which must not be bent or broken. With the help of cotton swabs previously dipped in spirits or isopropyl alcohol, we remove the dirt without using much force.

At this point, a souvenir photo in the assembled state would be useful for a beginner. Then it's time to disassemble and dismantle. We take out all shafts and pinions, as well as the motor soldered to the wire strands.



If you are dismantling a vehicle for the first time or, as in this case, the transfer table, you should memorise the structure of the gearbox or take souvenir photos of how the parts are correctly installed. This also applies to the bearing of the three-pole motor, which has to be removed to check the brushes, change them, and clean the armature and commutator gaps. But, it is also particularly important to know how the vertically positioned shaft with worm and pinion (in the photo to the right of the cover with its inserted metal bearing) is to be reassembled later.

The gears can be cleaned in an ultrasonic bath or with the help of a toothbrush. We should just not give them a chance to disappear in the bottom of the sink never to be seen again.

The cleaning work is similar to that of a locomotive model, which is why we leave it here with a reference to the earlier parts of this series. We always see the locomotive cleaner (6856) from Viessmann as a good help, which also strongly degreases the parts, and thus frees them from old, possibly resinous, lubricant.

Motor care for a long life

The commutator gaps of the motor deserve special care, because abrasion in combination with lubricant can lead to short circuits between the individual poles here and permanently damage the motor. The five-pole motors of the newer 89941 transfer table are particularly at risk here, because the gaps are smaller in this type of motor and can be bridged even more quickly.

To get to them, we have to remove the brushes, whose wear and condition we also check and assess, at this time. If both copper blocks are still sufficiently thick and evenly worn, they can continue to be used after any abrasion has been removed.



We displace possible residues of moisture or the locomotive cleaner spray with compressed air from the compressor. This is done with the help of the spraying stylus, which of course contains no paint! If you do not have such a tool at hand, you should allow sufficient time for drying.

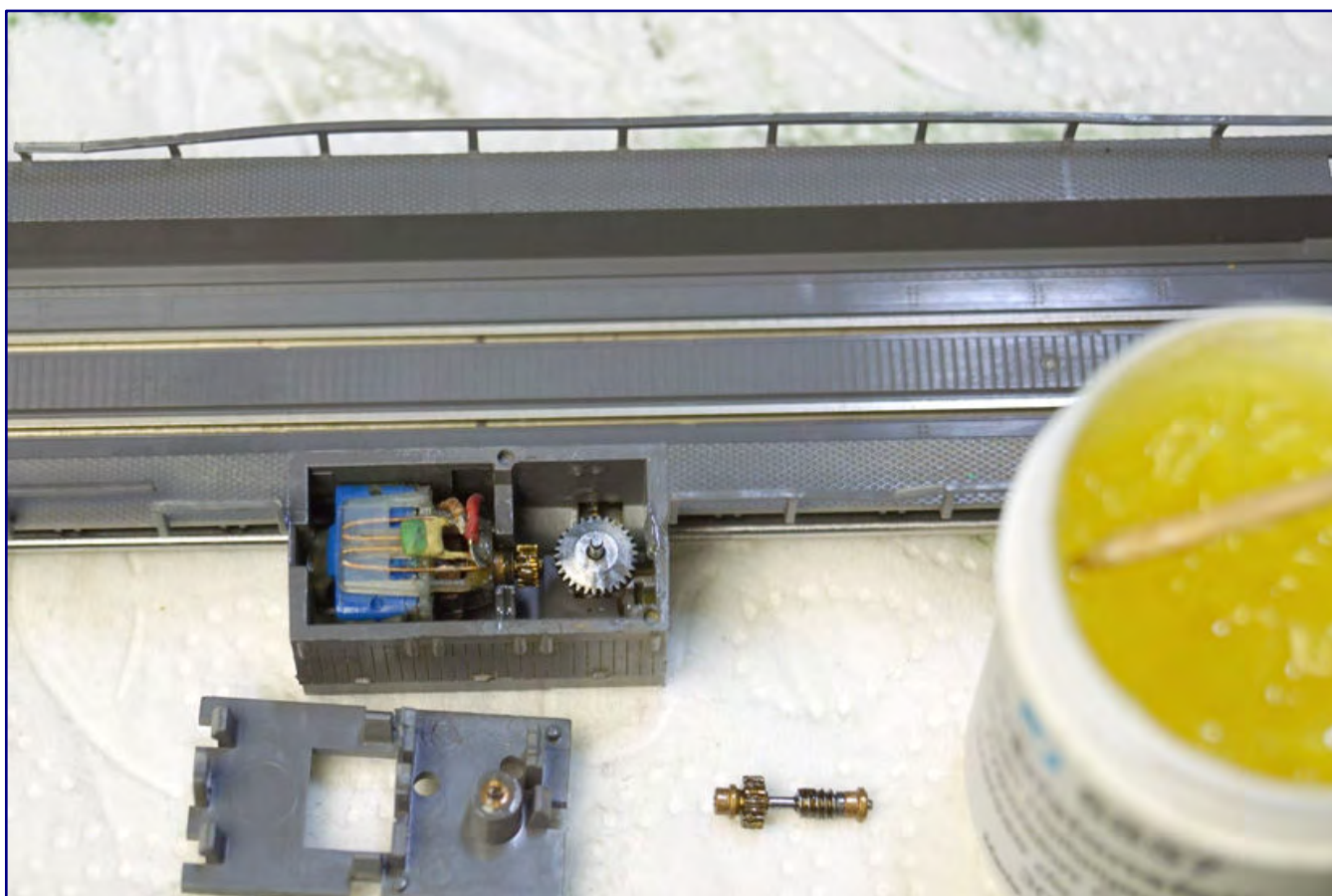
If burrs have formed on the outer sides, we carefully grind them off, or replace the brushes in case of doubt, certainly, also depending on personal experience. Too large a contact surface, as a result of such wear, also causes short circuits and reduces the service life of the motor.

If a replacement is required, the correct and suitable brush type must be selected. Märklin itself does not describe a replacement in this case in its instructions, because this is simply required much less often than with a locomotive model. After all, the transfer table is not an endurance runner.

The brush type of the old model 8994 with three-pole motor had the article number 8989. This type has not been produced for years and is, at best, still available from old stocks at dealers. However, it may only be used for a three-pole motor.

Later five-pole motors, such as model 89941, need an alternative of the same basic design, but in a narrower version, because five smaller instead of three larger poles had to be accommodated on the same circumference. These were given the item number 89891 and are equally suitable for both types of motor. With them, even an inexperienced Zettie has made the right choice.

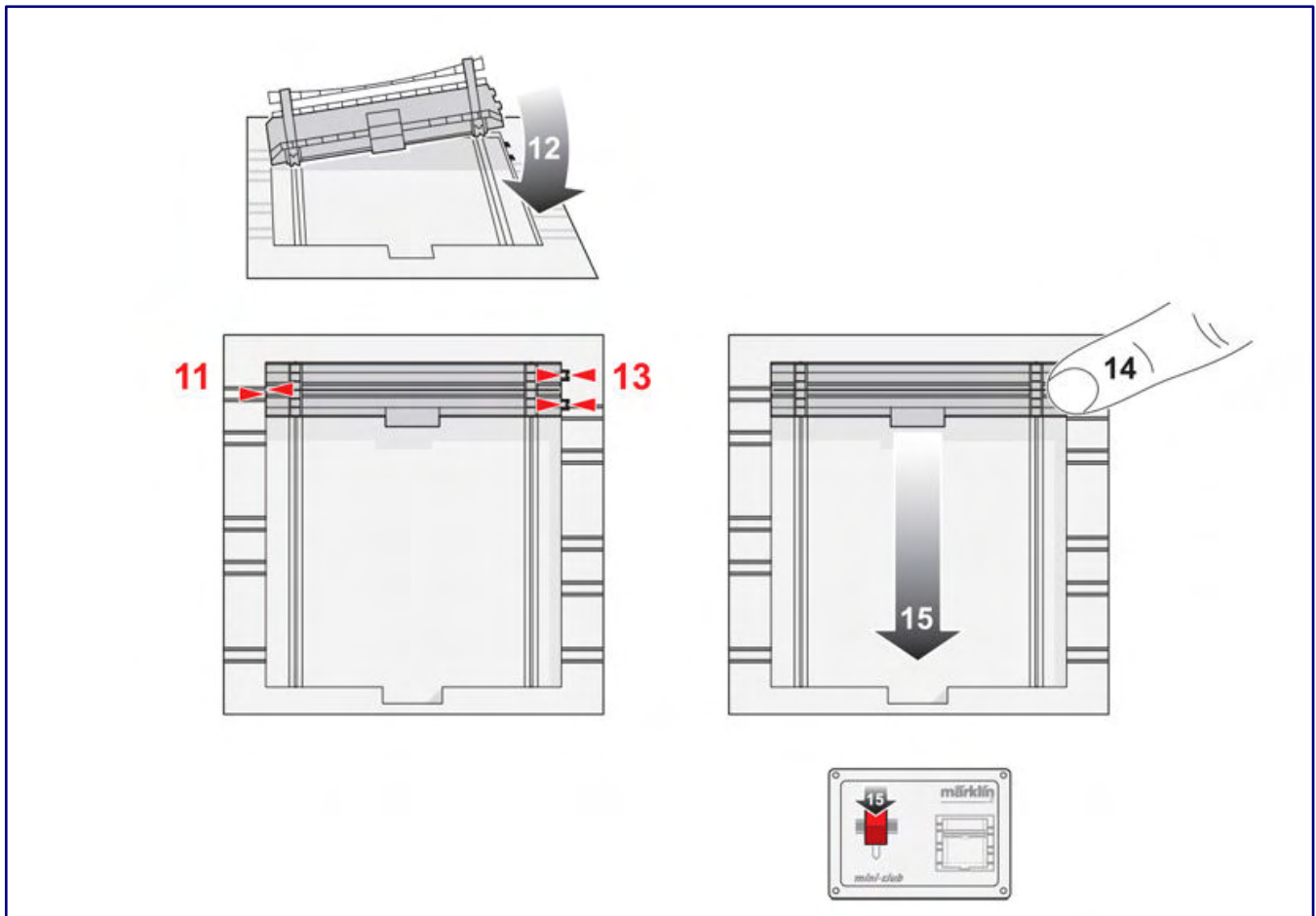
continues on page 32



We lubricate the motor bearings with a drop of Lubra metallic each, which spreads well in them and stops possible screeching noises of an armature running dry in them (photo above). We spread a little precision grease on the pinions and worm with a toothpick tip (photo below), which will then spread evenly over all the gearbox components during operation.

Again, the locomotive cleaner spray is the agent of choice. We can pick up loosened dirt with a cotton swab, but we should make sure that no fibres remain anywhere. So, a lint-free cloth on a fine wooden stick would be better. Finally, we scrape the collector gaps free with only light pressure using a toothpick.

Now, it is time to safely remove possible spray residues. This is best done with compressed air. You can get it in spray cans, but it is much cheaper to get it from a compressor, if you have one. Therefore, we use an air brush without paint. Its dry spray jet also displaces any residue of the locomotive spray.



Märklin's instructions from the sliding platform 89941 show best how our freshly maintained platform is put back into the pit: At the point marked with the red 11, it is placed at an angle to one side, and then lowered with movement shown under "12" until the guide lugs under "13" slip into the recess in the pit that matches them. A slight finger pressure on this end (14) fixes it there again. Afterwards, it (15) can be moved again with the control panel.

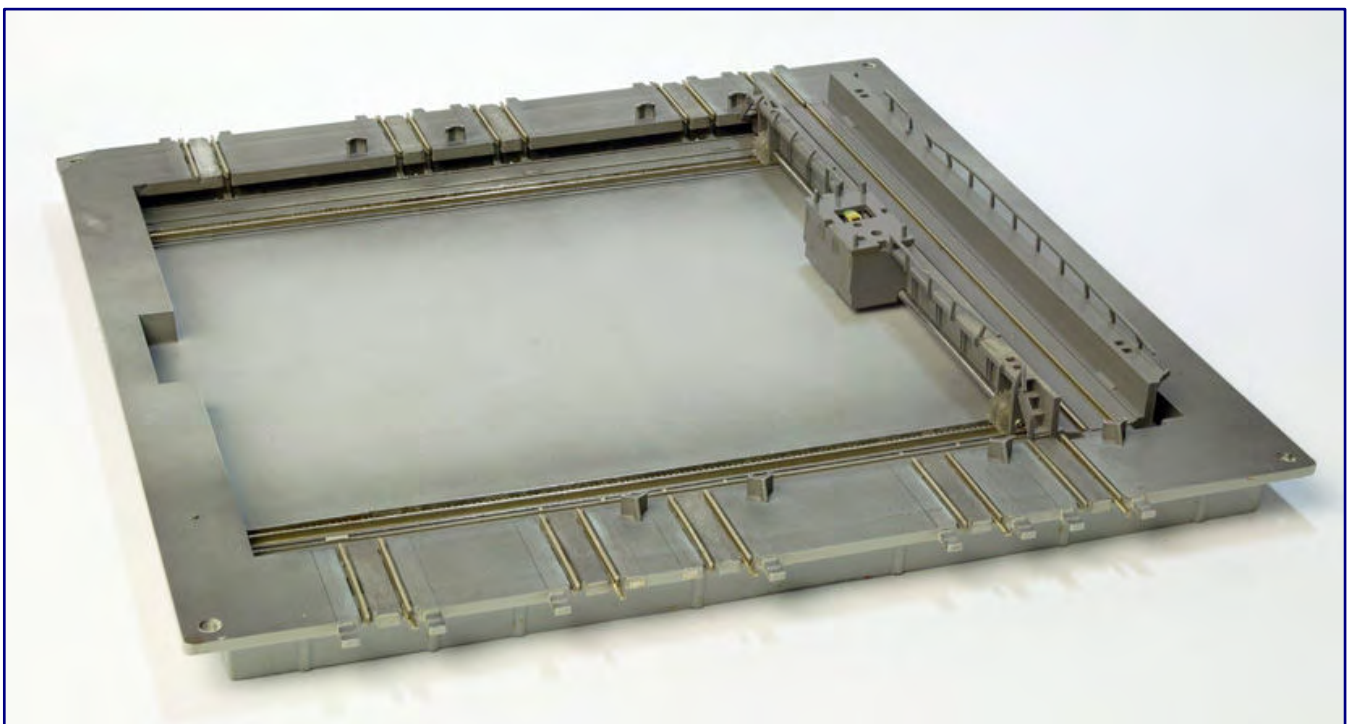
Now, the freshly serviced engine and all the gearbox parts can move back into their old positions. Before we do this, we only grease the bearings that we would otherwise not be able to reach. At this point, we are referring primarily to the engine bearings. They need a lubricant that creeps well, which we drip on the spot with a blunt cannula.

This time we chose the classic Lubra metallic in the form of a care oil stick, which we obtained from Werkzeuge Peter Post (alternatives are available). It is important that an acid-free and non-resinous lubricant is used; otherwise, our happiness will not last too long. All too many "expert tips" that can be read somewhere on the internet, should always be taken with a grain of salt.

Worm and gear wheels do not run well with oil, and it would be spread or worn off when operating, and then these parts run loud and dry until they are worn out. In principle, a good lubricant has to fulfil the same properties here, as previously explained. However, it should have a consistency that allows it to adhere well, but, also, not be so tough, that it inhibits the parts.

Precision grease (6857) from Viessmann has proven itself in our work. A small but sufficient amount is applied to the parts with a toothpick, and the grease is spread evenly and automatically during operation.

Now our model is reassembled: Carefully put on the cover, after making sure it is properly seated, carefully push it down, put the control's house back on, and reinsert the table at the removal point at an angle. First, the side facing the siding is lowered and put into place, then the other.



Technically, our model is again in largely mint condition after all work has been completed. The plastic surfaces also look very attractive again. Now, we can proceed with a replacement of the guard's house, and a new construction of the railings.

Done! Connected and energised by the control unit, it should now do its job again and move to the desired position. Hours of shunting in the depot are now safely guaranteed!

Articles published so far in our series "Maintenance & Care small railways"

Part 1 (Trainini 8/2013)
4-axle bogie locomotives reworking

Part 2 (Trainini 6/2014)
Workshop (basic) equipment

Part 3 (Trainini 6/2014)
Track cleaning with the track cleaner

Part 4 (Trainini 7/2015)
EMD E8: Replace bogie with covering plate

Part 5 (Trainini 9/2015)
FR couplings on the "Oppeln"

Part 6 (Trainini 8/2016)
Wagon (wheel) maintenance

Part 7 (Trainini 10/2018)
6-axle bogie locomotives

Part 8 (Trainini 6/2019)
Locomotive couch aid (from Noch)

Transfer table manufacturer:
<https://www.maerklin.de>

Lubricant cover:
<http://www.peter-post-werkzeuge.de>
<https://viessmann-modell.com>

Spare parts and replacement brushes:
<https://spur-z-ladegut.de>
<https://www.ersatzteile-1zu220.de>

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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

G 12 auf vier Kuppelachsen Abgespeckte Dampflok

Wegen des Mangels an Güterzuglokomotiven im Ersten Weltkrieg entstand aus der G 12 eine um eine Kuppelachse und einen Kesselschuss verkürzte 1'D-Lokomotive, die als Gattung G 8³ eingereiht und später als Baureihe 56¹ bezeichnet wurde. Nach einem Verzicht auf das Dreizylindertriebwerk wurde der Vierkuppler als G 8² (Baureihe 56²⁰) weiter in Dienst gestellt. Ein EK-Baureihenportrait zeichnet ihre Geschichte nun ausführlich nach.



Hans-Jürgen Wenzel
Die Baureihen 56¹ und 56²⁰
Die preußischen Gattungen G 8³ und G 8²

EK-Verlag GmbH
Freiburg 2020

Gebundenes Buch
Format 21,0 x 29,7 cm
144 Seiten mit 251 teilweise farbigen Abbildungen

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Best.-Nr. 6046
Preis 29,80 EUR (Deutschland)

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Eine echte Premiere stellt das hier und heute vorgestellte Buch dar: Wurden umfangreiche Baureihenportraits auch schon zu vielen Dampflokomotiven veröffentlicht, waren doch die preußischen Gattungen G 8² und G 8³ nie darunter. Allenfalls als „kleine Schwestern“ der G 12 wurden sie bisweilen erwähnt.

Auch dieses Buch entstand eher ungeplant: Autor Hans-Jürgen Wenzel hatte sich 1974 und 1986 in verschiedenen Artikeln mit den beiden Heißdampf-Vierkupplern beschäftigt, 2019 war es auf Basis heutigen Wissens ähnlich als Mehrteiler für den Eisenbahn-Kurier gedacht.

Sinnvoll wurde das angesichts des Umfangs, den die Arbeiten zum Zeitpunkt der Entscheidung angenommen hatten. Seit den letzten Fachaufsätzen hatte sich immerhin auch ein Zugriff auf Archive in Ostdeutschland, Polen oder auch Russland möglich gezeigt. Und das förderte jede Menge zuvor unbekanntes und nicht ausgewertetes Material zu Tage.

Aus diesem Zusammenhang wird indirekt deutlich, warum dieser Band der EK-Baureihenbibliothek trotz gleich zwei behandelten Dampfloktypen mit 144 Seiten gerade mal den halben Umfang anderer Titel dieser „blauen Reihe“ einnimmt: Bleibt das Material auf der einen Seite überschaubar im Umfang, war es für Beiträge in einem Magazin hingegen doch schon deutlich zu viel.

Viele unserer Leserinnen und Leser werden die Baureihen 56¹ (85 Exemplare) und 56²⁰ (846 Exemplare) bislang nicht kennen, schon gar nicht aus eigenem Erleben. Viel zu früh waren beide Baureihen von den Schienen zumindest im Westen Deutschlands verschwunden – anders als in der DDR. Doch das bedeutet nicht, dass sie Splittergattungen gewesen wären oder sich betrieblich nicht bewähren konnten.

Im Falle der Baureihe 56¹ ist dies leicht zu erklären, denn nur fünf Exemplare wurden 1945 in der britischen Besatzungszone gezählt – kein sinnvoll zu erhaltener Betriebsbestand für eine Staatsbahn, weshalb sie 1946/47 an die per Fusion entstandene OHE (Osthannoversche Eisenbahnen) verkauft wurden. Die DR hatte hingegen mindestens 58 Lokomotiven auf ihrem Gebiet.

Bei der Baureihe 56²⁰ waren über 500 Maschinen in den westlichen Besatzungszonen verblieben. Für die spätere Bundesbahn erlangten sie jedoch nicht mehr die große Bedeutung, die sie vor 1945 einmal besaßen. Anders als die DR im Osten konnte die DB auf einen (viel zu) großen Bestand an Güterzugdampflokomotiven zurückgreifen.

Mit 65 km/h Höchstgeschwindigkeit waren die ehemaligen preußischen G 8² für Hauptbahnen bald zu langsam, mit einem Achsdruck von 17,5 Tonnen für die meisten Nebenbahnen hingegen bereits zu schwer. Mangels sinnvoller Aufgabengebiete verabschiedete sich die Bundesbahn daher bald (nicht nur) von ihr und zog die in großer Zahl vorhandene Einheitslok der Baureihe 50 vor.

Unser Leser Dirk Rohwerder hat der Baureihe 56²⁰ ein Denkmal im Maßstab 1:220 gesetzt und sie auf ähnliche Weise nachgebaut, wie auch ihr Vorbild entstanden ist. In **Trainini**® 9/2015 war ein Bericht dazu zu lesen.

Der vorliegende Titel zeichnet die hier kurz skizzierte Geschichte ausführlich und in der seit Jahren aus EK-Baureihenbänden bekannten Struktur nach. Selbstverständlich fehlen hier auch die Entwicklung, historische Einordnung und Technik ebenso wenig wie Ausführungen zu den Kohlenstaublokomotiven oder dem Betriebseinsatz bis 1945 nicht.

Für die Nachkriegszeit werden neben OHE und DB selbstverständlich auch die DR und ausländische Bahnverwaltungen behandelt, die solche Maschinen gekauft, als Beutelokomotiven übernommen oder auch ähnliche Konstruktionen selbst beschafft hatten.

Eingerahmt und abgerundet wird das von einem umfangreichen und bestens passenden Bildteil voller historischer Aufnahmen in hervorragender Wiedergabequalität. Dazu gehören auch einige Dokumente und Einsatzpläne. Sogar ein kleiner Farbteil ließ sich einbauen, der allerdings nur Lokomotiven der TCDD (Türkei) zeigt.

Kritikpunkte an diesem Buch gibt es nur zwei: Besonders im Mittelteil erwecken viele Wortfehler den Eindruck, dieses Werk sei unter großem Zeitdruck entstanden und nicht durchs Lektorat gegangen. Der zweite Ansatz hat den wiedergegebenen Umfang der Betriebseinsätze zum Thema. Im Vergleich zu anderen EK-Titeln dieser Reihe sind sie nur sehr kurz und wenig detailliert wiedergegeben worden, was durchaus der Quellenlage zuzuschreiben sein mag.

Wir haben aber dennoch nicht das Gefühl, dass uns hier relevante Informationen vorenthalten wären. Diesem Eindruck können sich bestimmt viele Käufer anschließen, die das Buch ebenfalls schon gelesen haben.

Und das scheinen nicht wenige zu sein, denn überraschenderweise ist die Erstauflage dieses Titels bereits verlagsseitig vergriffen. Wer das Glück hat, bei seinem Händler oder Bahnhofsbuchhandlung noch ein Exemplar zu ergattern, sollte nicht zögern. Allen anderen bleibt nur die Option, auf einen Nachdruck zu warten.

.....
: **Publishing pages with reference possibility:** :
: <http://www.eisenbahn-kurier.de> :
: <http://www.ekshop.de> :
:

Bodensee und Donautal in Bildern Abschied von der Dieseltraktion

In der Bodenseeregion und dem Allgäu ist derzeit vieles im Umbruch: Strecken werden modernisiert, Brücken und Signale ausgetauscht sowie Oberleitungsmasten gesetzt. Das führt zu Streckensperrungen und Umleitungen. Höchste Zeit schien es da, den gewohnten Dieselverkehr in Bildern festzuhalten und den Wandel in der Zeit der Deutschen Bahn AG zu dokumentieren. Doch es bleiben darüber hinaus auch noch schmerzliche Lücken.



Christoph Riedel
Dieselstrecken zwischen Donau und Bodensee
Mit der Eisenbahn vom Südschwarzwald zum Foggensee

Sutton Verlag GmbH
Erfurt 2020

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Der passionierte Eisenbahnfotograf Christoph Riedel, der schon einige Bücher bei Sutton veröffentlicht hat, die wir teilweise auch hier besprochen haben, hat sich mit einem weiteren Titel gemeldet, der Eisenbahnstrecken in den südlichsten Zipfeln Deutschlands gewidmet ist.

Seine Streifzüge zum Sammeln des erforderlichen Materials führten ihn durch Landesteile von Baden, Württemberg und auch von Bayern. Dabei verlässt er die Gegend von Bodensee und Donautal nicht unerheblich, weshalb der Titel in Teilen irreführend scheint.

Doch darüber sehen wir großzügig hinweg, denn es hat dieses Buch für uns eigentlich sogar unterhaltsamer, informativer und aufschlussreicher gemacht. Durchweg unveröffentlichte Aufnahmen der schönsten Strecken dieser Regionen machen diesen Bildband liebenswert.

Diesem Eindruck kommt zu Gute, dass die Aufnahmen nicht nur passend ausgewählt und an wirklich geeigneten, weil äußerst ansprechenden Orten gefertigt wurden, sondern auch hervorragend reproduziert worden sind.

Zu sehen sind in diesem Werk die meisten Dieselfahrzeuge, die in den letzten Jahren und noch aktuell 13 Strecken(abschnitte) am Bodensee, in Oberschwaben, im Allgäu und nahe des Schlosses Neuschwanstein im Personenverkehr dominieren.

Der meist kaum noch vorhandene Güterverkehr wird hingegen nahezu vollständig ausgeblendet: Eine einzige Aufnahme zeigt eine Diesellok der Baureihe 212 mit einem zweiachsigen VTG-Kesselwagen sowie einem Schwerlastwagen samt Panzerladung in Füssen. Dieses Foto wurde vor beinahe 25 Jahren aufgenommen.

Rund 50 Jahre haben nach dem Abschied der Dampfloks Diesellokomotiven und -triebwagen die portraitierten und besuchten Strecken dominiert. Vielerorts ist damit bereits oder bald Schluss, denn umfangreiche Elektrifizierungsarbeiten werden das Bild der Strecken und eingesetzten Fahrzeuge nachhaltig ändern.

Dies macht den vorliegenden Band so wertvoll, obwohl an derselben Stelle auch ein Kritikpunkt stehen muss: Christoph Riedel wirft den Blick zurück auf eine Epoche, die in den meisten Fällen kurz vor ihrem Ende steht.

Dazu bedient er sich durchweg eigener Aufnahmen, die, wie er schon im Vorwort selbst erläutert, aus der Mitte der neunziger Jahre und einem Zeitraum zwischen 2012 und 2018 stammen. Sie zeigen also den Betrieb nach dem Übergang auf die Deutsche Bahn AG und der jüngeren Vergangenheit.

Zwei bereits sehr unterschiedlich wirkende Einsatzräume werden so gekonnt gegenübergestellt und zeigen den Wandel der Zeit: Wir sehen Diesellokomotiven der Baureihen 212, 215 und 218 sowie Dieseltriebwagen der Baureihen 627 und 628 von den ersten Prototypen bis zur letzten Bauserie und rund 20 Jahre später modernere Triebwagen wie die Baureihen 611, 612, 642 oder 650.

Das ist spannend und vor allem im geschilderten Kontext hochaktuell, zumal der Autor seine Bildkapitel stets mit einem (sehr kurzen) Streckenportrait einleitet, aktuelle Entwicklungen und Aktivitäten einflechtet und auch Erläuterungen zu seinen Fotostandorten mitliefert. Schon bald wird vieles davon historisch sein.

Genau hier ist es aber schade, dass es dem Autor nicht gelungen ist, verbliebene Bildlücken zu schließen. So fehlen beispielsweise die Lokomotiven der Baureihe 234 und 245 sowie die Triebzüge der Baureihen 622 (LINT) und 644 (TALENT), um das behandelte Zeitfenster rundum abzudecken.

Zumindest die Mehrmotorenlok 245 war zum Zeitpunkt der jüngsten Aufnahmen in der Region auf mehreren Strecken im Einsatz. Dies zeigt, dass hier eher auf Ausflügen entstandene Urlaubsaufnahmen um einen neuen Buchtitel gelegt werden als gezielt Strecken oder Region zu portraituren.

Das ist nicht verwerflich und schmälert auch nicht den Nutzen für viele unserer Leser, die nun mit einem Kauf liebäugeln dürften, schafft aber ein Problem für andere Verlage und deren fachlich sehr viel anspruchsvollere Titel.

Weit über einhundertfünfzig Jahre Eisenbahngeschichte in dieser Region werden sich vorerst kaum noch auflagenstark in einem Fachbuch behandeln lassen und hinterlassen für lange Zeit eine schmerzliche Lücke.

So werden wir die Einsätze der Baureihe 210 in den Siebzigern mit ihrem Turbinengeheul ebenso wenig genießen dürfen wie der leider nur kurzzeitige Einsatz des RAm TEE der SBB/NS im Allgäu, zu dessen bitterem Ende leider das Zugunglück von Aitrang im Februar 1971 gehört. Auch das nahezu vollständige Fehlen des Güterzugverkehrs spielt in diesem zeitlich deutlich weiter gefassten Blick eine größere Rolle.

Doch immerhin schaffen Bilder und Inhalte einen engen Bezug zu unserer Spurweite Z, weshalb wir einen großen Nutzen für viele unserer Leserinnen und Leser sehen: Wir erleben Vorbildeinsätze vieler Märklin-Modelle und eines von Hagemodell.

Besonders auf ihre Kosten kommen werden aber diejenigen, die sich glücklich schätzen, Fahrzeuge von Sondermodelle Z (SMZ) wie die Baureihen 627 oder 628 sowie den Regioshuttle RS 1 ihr Eigen nennen.

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Our annual review of the year 2020

Losses and prospects

2020 was a mixed year in several respects. A prevailing sense of cheerfulness at the beginning of the year soon gave way to the consequences of the spreading virus. And our community also suffered some painful losses. But the many restrictions also led to a veritable rediscovery of the model railway hobby within the quiet confines of our homes. Both, hidden activities and the deadly silence perceptible on the outside, contributed to this mixed picture.

Tradition holds that the first issue of the new year should be heralded with a review of the previous one. Unlike hardly any other, 2020 will probably go down in history as an extremely difficult year. Nevertheless, many things about it may appear comparatively harmless to us, if we compare them with the hardships suffered by earlier generations.

While our grandparents had to save their lives through two world wars, the parents of today's working population still remember the flight from bombing raids, possibly also displacement, mass murder or terror, as well as the privations of the early post-war years.



Our first topic in 2020 was the newly delivered express steam locomotive class 0310 of the Deutsche Bundesbahn with a new boiler. The cover picture of the [Trainini Photo Calendar 2021](#) shows it on the Diemeltal layout, which also played an important role in our articles of the past year.

This is not intended to gloss over the current situation or even to play down the health risks, but to help people to put into perspective what they have experienced and to give them courage. Despite all the hardships and restrictions, we have also noticed that many people are turning more strongly and intensively to their hobby, or have even (re-)discovered model railways for themselves.

And, so this review will not only show the low points and blows of fate of the past year, but will also present highlights and rays of hope. One such highlight was certainly the very extensive reports on the Obere Ruhrtalbahn (Upper Ruhr Valley Railway), which are spread over the entire 2020 edition of **Trainini®**.

It all started in January with the presentation of the Märklin class 03¹⁰ model. This Insider Club model was based on the DB express steam locomotive without streamlining, but with a high-performance boiler.

Its great and last refuge was the Upper Ruhr Valley Railway, as we have explained in detail in the prototype article and also have documented with many photographs. The prototype and model were an excellent prelude to the theme of a layout created by Dirk Kuhlmann on behalf of Jörg Erkel.

The “Diemeltal” exhibition layout was actually supposed to have had its premiere in Altenbeken, and is now in the showroom of the 1zu220-Shop, where it can be viewed by customers, when the current restrictions allow.



At the Nürnberg (Nuremberg) International Toy Fair in late January/early February, many trade visitors first became aware of the danger of the impending Corona epidemic, which in terms of its spread and consequences now certainly has many similarities to the Spanish flu of 1919/20. The fair is a place where many exhibitors from China and buyers from Europe traditionally meet. In Wuhan, China, the disease was already spreading particularly strongly at this time

As a substitute for having missed out on experiencing the original layout, we started a series of reports on this layout. In April, we presented the idea and track plan, and in the following month an article about benchwork and track laying.

The series continued after a longer summer break in October, with a report on model landscaping. In the same issue we also presented a portrait of the actual Upper Ruhr Valley Railway. As a consequence we did not revive our earlier annual theme, but focused the reports on complementary, but equally important concerns.

This also became clear in the fourth and, for the time being, last part of our small series on the exhibition layout (November 2020): Instead of writing about techniques for a successful application of static grass, we focused on a close observation of reality, how to transpose real life situations into a model, and how to achieve a more natural looking placement of trees and other vegetation. All too often, model railway layouts lack the randomness and chaos created by Mother Nature.

In February 2020, the first signs of the soon to begin pandemic were slowly making themselves felt: A conspicuous number of visitors and exhibitors from the Far East wore medical masks at the Nuremberg International Toy Fair, reminding everyone of what the media had already been reporting for some time from China.



Our community had to mourn three prominent deaths in 2020, including Wolf-Ullrich Malm. Unfortunately, he did not live to see the portrait of his layout with the station in front of the Holstentor. In his honour, we also presented one of his other layouts a little later, as part of our annual theme.

Despite the bad feeling that was beginning to spread, this trade fair was still a success, and we were able to report on many great new products, some of which unfortunately have still not been delivered. These, too, are often consequences of events later in the year.

Often, things were and still are better, in the case of the small series producers, which were not only able to present exciting products, but usually also found the time and leisure to fill the gaps that had opened up elsewhere. We are glad for this effect, which somehow saved the model railway year, not only in terms of an increasing enthusiasm for our hobby.

However, bad news reached us at the same time and was also reflected in the February issue: unexpectedly, Oliver Passmann, an accomplished small-scale manufacturer, who had previously served the technical and digital sectors excellently, passed away on 25 January 2020.

Unfortunately, this was not the only loss to occur during the year. On 13 March 2020, Wolf-Ullrich Malm died; his outstanding layout "Alt-Lübeck 1905" had also kicked off our annual focus theme on readers' layouts in March. In his honour, we changed our plans and added in our April issue a portrait of his last publicly exhibited work, his "Buntekuh" layout.

We then had to mourn a third loss on 11 September 2020 with Walter Schmidt. The owner of the former Z-Stübchen in Krefeld, a small wonderland in 1:220 scale, had succumbed to his serious illness.

The sense of disaster felt by so many of our readers fits well into this place of painful memories. The pandemic took its fateful course in March 2020, also in Germany. While the world seemed fine just a month before, there was now a constant stream of cancellations of trade fairs and exhibitions.



The only exhibition we know of that still took place and that had Z gauge participation was the one in Lahnstein. Here, the showpiece "Standgatan", designed after Swedish impressions, could be viewed. FdE Burscheid's layout of the Busch light railway also ran on 6.5 mm track of the (1:87) "Tillmanns Loch" operating diorama.

First, to be hit were Faszination Modellbahn and Intermodellbau trade fairs, which is why the manufacturers' spring new products could no longer be presented to the interested public. The fears for the much smaller exhibition in Altenbeken also ended in a cancellation, because the long-awaited meeting of the Zetties from all over the world could no longer be held, even with hygiene concepts and distance rules.

Only the Lahnsteiner Modellbahntage (Lahnstein Model Railway Days) still occurred in February, but it was already a harbinger of what was to come. Bruno Kaiser and Dirk Kuhlmann participated on behalf of their club FdE Burscheid with two 6.5 mm track exhibits. However, while “Tillmanns Loch” was a light railway in 1:87 scale, “Strandgatan” was a real Z gauge exhibit based on the Swedish model. Our editor reported on it in the March 2020 issue.

The news section of that issue also described how the ZFI club (Z Friends International) tried to set up a virtual substitute for the cancelled Altenbeken Z gauge weekend in their internet forum. We also participated in this form of presentation and showcased our large “Himmelreich” station diorama there.

Spread over no less than four issues, we reported in detail on its construction according to the Black Forest’s Höllentalbahn (Hell’s Valley railway) theme: The first part appeared in June, and described the assembly and detailing of the station building, as well as the plan of the diorama.

After the benchwork, the construction of the station road, and the work on the subterrain structure had been presented in the July issue, we turned in August to landscaping that would give the finished showpiece its unmistakable appearance.



The construction of the station and diorama “Himmelreich” (Kingdom of Heaven) station and diorama, whose landscape is shrouded in early morning mist in this photo, was presented in a four part series.

This also included the detailing of the diorama with figures and other accessories that are indispensable for a creating a lively scenery. But we were not satisfied with that yet: Since many of our readers frequently ask questions about electrical topics, we described the wiring and lighting of the diorama in an additional article in the September issue.

We also painted the wooden base and showed how important a clean finish is for an exhibition layout. In October, we added a few more photos that could not be accommodated before. At the same time, the fourth and last part described our first excursion into the world of CAD construction.

This topic, which is also gaining more and more importance in the hobby sector together with 3D printing, was simply a hot issue, and probably only needs sufficient help and instructions so that even more model railway enthusiasts try their hand at it. We had already shown the impressive possibilities of this technology with the example of Michael Hering in issue 3/2020.

But, we were also able to bring two interesting examples for traditional model vehicle construction, which can be described as classic, in one case even in a combination with 3D printing: In **Trainini®** 3/2020, our reader Michael Pleiner presented how he built the missing US diesel locomotive ALCO PA1 of Amtrak for existing Märklin wagons with simple means.

Markus Krell went a bit further in May 2020 and sacrificed the body of a Märklin EMD F7 for his “galloping goose” mounted on a Rokuhan Shorty motorised chassis. As already mentioned, the superstructure was based on a 3D-printed stationary model purchased from a third party.

In this review, we do not want to go into detail about new model launches during the year. Items that were considered to be new designs and sufficiently outstanding received a best product nomination in another article in this issue.



The new shape of the Omm 32, ex type “Linz,” from FR Freudenreich Feinwerktechnik deserved an article about new and unusual cargo loads.

Once again, however, it was time for an article on (quite) new, and sometimes unusual, cargo loads (April 2020). The impetus came from the model of the open wagon “Bauart Linz” by FR Freudenreich Feinwerktechnik, which is ideal for this purpose.

And, so, we focused on newer wagon models on the one hand, and products on the other hand, that were not necessarily designed as loads, but are, nevertheless, excellently suited for such a task. Our key

topic throughout the year, however, was our readers' layouts. Let us look back once again at the outstanding designs gracing our pages between April and December 2020.

Karl-Heinz Wobschall's "Bergheim" layout has a manageable size, but is huge in terms of visual depth and impact. We had the opportunity to visit this layout several times to take photos for calendars or articles.

Once purely analogue, the layout is now DCC controlled and had already appeared in MIBA magazine and on Märklin-TV. We had waited years for the right moment to present it in our magazine until the opportunity presented itself last year with our layout article series. The layout's creator also has proven that a good layout is never finished. Our readers could witness how the appearance of some parts of the layout changed over time.



The beautiful readers' layouts, which we presented as part of our annual theme, were highlights of the past year. Among them was the "Bergheim" layout by Karl-Heinz Wobschall from Duisburg, set in the foothills of the Alps, and digitally controlled.

Not quite at the right time of year, but in keeping with the holiday mood and dreams of long-distance travel, came the effective tiny layout "Cherry blossom in Shizuoka" by Christoph Maier (July). Christoph from our trio of translators has immersed himself deeply in the "Kuhlmann School" of layout modelling, and always delivers amazing results.

Inspired by Bernhard Stein, "Alpenidyll im Badischen" was Eckard Jehle's previously unknown home layout (August issue). Its strong points include long parade routes and a functioning cable car. It was created in the style of the nineties according to the tips, tricks and techniques of the aforementioned old master.

"Along the Rhosel" by Jürgen Wagner took our readers to the German wine valleys in October. In his unique way, Jürgen has converted many, many holiday impressions into something lasting on this exhibition layout, which was actually also to celebrate its premiere at the Altenbeken convention.



Two layouts that could not be more different: The tiny layout “Cherry Blossom in Shizuoka” (photo above), newly built as a living room decoration, was presented to our readers in July 2020, giving the magazine a touch of internationality. In contrast, the room-filling layout with Alpine motifs, long parade routes and a functioning cable car, which could be admired in the following month, was previously created in the nineties. Photos: Christoph Maier (photo above / Eckard Jehle (photo below)

Without a doubt, “Wiesenthal” by Wilfried Pflugbeil will also go down in the history books of Z gauge. We can be glad to have such a sprightly pensioner and experienced tinkerer among us in Willi. The man from Chemnitz once again managed to surprise us with amusing and technically sophisticated movement functions.



The new layout “Along the Rhosel” by Jürgen Wagner, which was actually supposed to celebrate its premiere in Altenbeken, also spread a holiday mood in times of privation. Besides the river that gives it its name, the “Kammerrund” block station is also an eye-catcher. Photo: Jürgen Wagner

Harald Hieber concluded our series with his “Drachenstein” (Dragon Stone), which has been rebuilt and extended several times. We had it on the agenda for a long time, but several times we had to wait and see what Harald might be up to again and what he would create here.

Finally, after several postponements, plans by the print press got in the way. But good things come to those who wait: “Drachenstein” was given an accompanying insert explaining stalactite caves as interesting motifs, and inspired with many appealing shots.

Let's go back a few months: The closer summer came, the more it seemed as if normality could return to our lives, a fallacy, as we know today. When we reported in May that the opening of the Märklineum would be postponed until autumn, it proved to be a reflection of much that was to come as a result of the shutdown of economic and social life.

In the meantime, we have arrived in the year 2021, experience for a second time what is behind the foreign word “lockdown” and realise that we will have to continue waiting for open doors at the Märklineum. At least, Miniatur Wunderland was able to reopen between May and autumn 2020 with severely limited capacity restrictions, followed in July by a milestone for the operators, with the installation of the glass bridge over the canal.

In late spring, it became known that the Munich publishing house Geramond wanted to take over VGB, a publisher of railway modelling magazines, which occurred some time later after approval by the cartel authorities. This resulted in a hitherto unique dominance in the magazine and book sector of railway and model railway publishing, the consequences of which slowly became visible by the end of the year.

On the other hand, this has no effect on our work, because we operate in a niche of the model railway sector that seems to be mostly uninteresting for the print press, which is why this does not result in any competition.



The consequences of the Corona pandemic have made many scheduling plans obsolete, and even put companies' business models into question or at least put them to the test. Market leader Märklin also had to scrap its plans for the opening of the new Märklineum: A new date has not yet been set.

What has changed, however, is the way of working together. Irregular video conferences of editors and translators promote personal contact in times of social distance and allow an interactive exchange that was not possible in the same form with the previously preferred media of telephone calls, conference calls and e-mail exchanges.

Two small model highlights, which are not equally worthy of awards, characterised the reports of last summer. The first was Märklin's WGmh 824 lounge car, which only became really layout-suitable with subsequent improvements at Märklin and was later completed with a pre-series class 120 locomotive, and a three-part special train car pack.

However, more of a product highlight was the special train "Deutsche Weinstraße", which was launched in editions for two different model eras and in parallel by Märklin itself and by the 1zu220-Shop, in the latter case, by means of a special edition produced by Märklin in a one-off series.

We also presented the matching interiors created by Torsten Scheithauer, and showed how wagon models from the eighties, which can be procured inexpensively with minor reworking, can extend the two

1zu220-Shop special models to a train close to the prototype. All in all, this resulted in a very suitable and worthy topic for prototype considerations, as well.



Special trains were certainly a focal point of the 2020 programme at Märklin: In addition to a WGmh 824 club car with additions, the start of the “German Wine Route” was the talk of the town. The latter also included the very successful special model that was produced exclusively for the 1zu220-Shop. We were pleased to welcome some new photo authors for the prototype reports.

This gives us the opportunity to thank some new image authors, who have contributed here and to other topics once or now repeatedly: Norbert Schmitz, Sven Ullrich, Andreas Schmidt, Manfred Britz and Peter Pfister. We were particularly pleased that among them were regular readers of our magazine who wanted to contribute and will continue to do so in the future.

In June we also had an article that may have caused some people to rub their eyes in surprise at first: We presented Noch's new laser cut cardboard building kits, which they produce at their Wangen site and which are available in three kit combinations.

The houses looked strangely familiar, and, yet, they were new products. How was that possible? In addition to our experience, we therefore also reported on the background to their creation, and explained why they initially seemed like copies of their own or other firms' kits.

What seemed more like a marginal topic was something that was indispensable in the prototype and certainly played a central role in operation: telecommunications along the rail line. What is done today with the GSM-R mobile phone standard from the moving train once needed telephone boxes or telephone booths at the edge of the track.

We used a kit designed by Modellbau Laffont for Märklin to draw attention to the topic in a prototypical way and as colourfully as possible. Because we think that these relics should not be missing on model railway layouts, we explained the assembly, presented other products, and do-it-yourself scratch-built solutions.

One of the highlights of 2020 was our article on the Schi-Strä-Bus (Rail-Road-Bus) of the German Federal Railways. We have prepared the model in the low-price segment, realised for the 1zu220-Shop, for use on the layout and have also prepared a detailed report on the prototype.

The article hit a nerve and resulted in more reader reactions and calls to the editorial office than any other article in years. At times even the editor-in-chief's wife complained because the lines were busy every day and for hours at a time. What had happened?

On the one hand, there were reactions from readers in various forums who were probably rather unfamiliar with the subject, saying that the article was quite nice, but that it really contained new information. On the other hand, other historians, some of them after years of their own research, were delighted to discover that this article had for the first time succeeded in proving the existence of a second prototype, V4, which had been changed in important details.



There is still plenty of material for the future on the subject of rail-road-buses. Here, the NWF BS 300 bypasses the Holtgaster Feld reception building to reach the grooved track it needs to transfer to the track wagons.

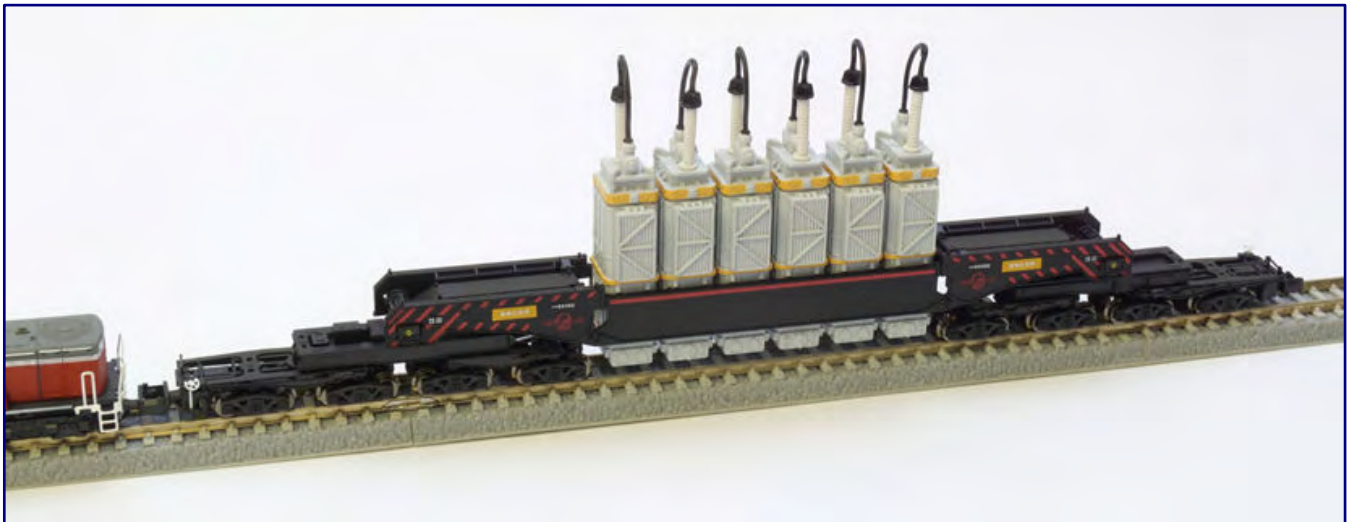
It seems that the history of this vehicle is far from being conclusively known. Thus, our report led to further scientific research and the formation of a research community that would like to evaluate at least three sources in different places in Germany, and compare them with known and proven facts.

Only the restrictions of the pandemic have so far prevented this work from continuing as planned and findings from being exchanged. A new article by our editor-in-chief has been published in the current edition of the "Eisenbahnmagazin" 2/2021, which has also incorporated sections of the discontinued "Eisenbahn-Journal".

It provides a reorganised summary of what is known about the subject and asks the questions that are still open. Readers who can provide relevant information or who have previously unknown material or missing information are invited to support the research. It seems that, even today, many people are still interested in this curious and unique vehicle.

But we must not forget the highlights of outsiders. And so we would like to mention two special models here that did not make it into our nominations, but are very important for foreign markets, which we also address in our *International Edition*.

We are thinking here of the models of the ALCO RS-2, RS-3, RSD-4 and RSD-5 types, which were distributed by AZL in the summer of 2020, and the sixteen-axle SHIKI880 transformer transport wagon from Rokuhan, which was presented in August.



We must not forget those new products that are important for international markets. For example, we would like to mention the Japanese transformer transport wagon SHIKI800 by Rokuhan, which is an impressive eye-catcher, and, yet, not too heavy.

An anniversary that almost went unnoticed in 1:220 scale was “100 Years of the German Reichsbahn”. Since the fans of epoch II are becoming fewer and fewer, Märklin was conspicuously restrained with new products, although there were also a few announcements during this period



In addition to the already mentioned “Wiesenthal” layout portrait, we also honoured this 100th anniversary with a report on the very interesting special exhibition in the DB Museum in Nürnberg (Nuremberg) and, with prototype and model articles on the Culemeyer transports, also addressed a pioneering invention of the Reichsbahn at the time.

It showed like no other the challenges this railway company was facing: wartime reparations, competition from road transport and the later take-over by the German Wehrmacht.

But we were also able to show that this is still a largely untapped field, which is extremely interesting and versatile, especially for epoch III.

In addition to a large prototype anniversary, there was also a small one that we did not mention. Only one reader seemed to have noticed it and congratulated us punctually by a letter to the editor.

Things became too political in the opinion of some readers when we compared travelling by long-distance bus and ICE high speed train in October, thus picking up not only on advantages of travelling by train, but also on the smart business strategies of a long-distance bus provider.



The Culemeyer road scooters were an exciting topic: as an invention of the Reichsbahn, they were a perfect fit for its 100th anniversary and reached their all-time high in the times of the Bundesbahn. This offers many perspectives for new small series models in a still largely “untapped field.”

However, our magazine “for friends of the 1:220 scale railway and prototype” aspires to also keep an eye on real railways, which unfortunately have a lot of problems and has been in danger of falling behind for many years. In addition, the railway and buses here also provide some visual inspiration for Era VI modelling projects.

The end of the year is fast approaching and with it a spectacular new product that has caused a lot of discussion: Märklin's KlV 20 rail bus is not exactly to scale, but is now nevertheless the smallest powered Z-scale rail vehicle in serial production. At the same time, it was the first new design in our scale based on metal-filled plastic.

Because of all the excitement and attention about this tiny model, we also included an article about the prototype. This way, our readers were also able to learn that, contrary to all gloomy predictions, the mould constructed by Märklin for the chosen prototype is correct.

In November, on the one hand, we were able to present Märklin's new insider model 2021 and were pleased that the double diesel locomotive V 188 achieved almost sensational pre-order figures, but, on the other hand, it was also important to announce, that this difficult year with its restrictions, has caused a great deal of confusion in Märklin's delivery planning.

Especially the forced interruption of production in spring had led to delays that could not be made up during the rest of the year. This hurts the manufacturer, especially, in view of the fact that demand for model railway articles has picked up strongly again during times when we have to remain at home.



The fire brigade is ready for action! Craft topics are particularly popular; especially, when they can also provide some market overview and show the possibilities of 3D printing. We tackled the red rooster on the roof in December 2020, and closed last year's final issue with an article that appealed to many readers.

And, fortunately, the average age of model railway enthusiasts is also falling again, so the hobby is becoming more modern and contemporary again. Nevertheless, an effective public presentation of the hobby is currently almost impossible. Even the annual Model Railway Day, which is specifically intended to draw public attention to the hobby, was hardly used or actually almost abused, as our commentator Dirk Kuhlmann noted.

In any case, we wanted to make the hobby more colourful, which is why we also published from time to time topics that are somewhat off the rails. For example, we presented the Mercedes-Benz O 305, the most important standard road bus in terms of number of units, as a model, and a few months later, in December to be precise, we dealt with Mercedes-Benz fire engines.

Our impression is that these topics were well received by our readers with respect to the chosen scope and focus, which is why we want to continue with them. Unfortunately, the topic of road transport had recently suffered somewhat over a few years and we are glad that some new suppliers are currently enriching the market enormously.

At the end of each year's review, we look ahead: we don't want to be frightened by the current state of the pandemic, we want to be considerate and simply devote ourselves, as intensively as possible, to the hobby.

This distracts from reality, gives pleasure, and promotes creativity until hopefully normal life becomes possible again, and we can also maintain contacts at trade fairs and events. In order to not lose sight of each other until then, the editorial team has decided on a special annual focus topic for 2021.



At the end of this article, a ten-year anniversary should unite the past and the future: Rainer Tielke celebrated a round anniversary as a small series manufacturer with his Ratimo-Z brand in 2020. Portraits, such as the one of his work in Trainini 3/2020, will form our annual focus in 2021.

Instead of calling it simple, let's just take one last look back at **Trainini®** 3/2020: Titled "Round Anniversary in the North", we paid tribute to ten years of Rainer-Tielke-Modellbau in March. We took advantage of this to present him and his wide-ranging work.

It is manufacturer portraits like these that are to be a main topic for us in the new year, also to avoid that forced distancing does not lead to forgetting. And that is why we have found an appropriate title for our new series. It is short and sweet: "Staying in touch - small series manufacturers".

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Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

New Year's wishes 2020/21:

Your posts were something I could look forward to every month. This year, the little escapes from reality were something I appreciated very much - thank you very much for that.

My annual picture with best wishes for the new year this year depicts a goods train from 1990. The locomotive is the Ae 6/6 11428 (Märklin 88501), Hbis 299 (82151), Tbis (prototype) of the SBB (from 8623), Eaos SBB (8651), Hbils-vy SBB "Hundert Jahre Monte Generoso" (8657.901) and 3 x tank car BP (8628).



The goods train in the photo for the already traditional New Year's greeting was based on a real model that was featured in the Railway Journal. Photo: Martin Zimmermann

The train is shown in this order in the railway journal 1/2020 "Gotthard Railway", page 52. I have only omitted the second locomotive.

Martin Zimmermann (Switzerland), by E-Mail

More wishes for the turn of the year 2020/21:

I wish you and your family, as well as your Trainini editorial colleagues, all the best for the coming year! I hope with you that you stay healthy (and especially Covid-19-free), and that you get through the lockdown well in every respect, even if it lasts longer than the politicians intended.

Jochen Brüggemann, by E-Mail

Idea from the last issue implemented immediately:

Many thanks for the detailed report on the V 80 in the current issue. Since there was talk about an idea, here is the implementation:



Even small things have a big effect: Our reader's V 80 has been given a locomotive driver, as we suggested in the test report. Photo: Dirk Rohwerder

“Before we get to the technical evaluation, we will conclude this section with an idea, of gluing a torso figure into the driver's cab, which can be seen through freely, in order to recreate the locomotive driver and to further strengthen the prototypical impression?”

Greetings from Sprockhövel, Merry Christmas, a good transition into the New Year and stay healthy!

Dirk Rohwerder, Sprockhövel

Question on Part 4 of our Digital Series (Trainini 9/2018):

My name is Peter Taubert, I am 66 years old (still working part-time), and I am building a digital Z gauge layout (4.00 x 0.65 m). I decided to use the Rokuhan track system after a test circuit with MTL tracks.

On the subject of digitisation, I have read various articles on the net and also in literature (e.g. LDT, "Digital-Profi warden") and have always gained new insights. This is also the case with the loose series on digitalisation in **Trainini**.

Now my specific question about the article mentioned in the subject line: Why was the entire test layout isolated in two rails by the author, Mr. Andreas Hagendorf? Only to test the insulating connectors offered by Rokuhan or for reasons of later digital expansion?

Enclosed is another experience I had to make with Rokuhan tracks when I wanted to solder them. The track connectors are made of metal (magnetic), and cannot be soldered. I also wanted to avoid this kind of additional feed points. Now I have to think about it again. But, maybe, the author of the article has already had more experience with the durability of the simply plugged-in track connectors.

Peter Taubert, Schwerin

Author's answer: The two-sided isolation is a personal preference, it is rarely really technically necessary, as described in the series when using several boosters. Sufficient for busy signals or braking sections is the one-sided isolation of the track. The rule of thumb is "right is right (in the direction of travel)". But in my opinion, this is where it gets difficult: what is the direction of travel on single-track lines? So it is easier to separate on both sides and to do the appropriate control via cables. The test track is, therefore, prepared for all digital circuit variants.

The tracks are made of nickel silver (a copper-zinc-nickel alloy) whose surface sometimes resists soldering stubbornly. The use of soldering flux (e.g. Stannol no. 1V, art. no. 114033) at the soldering point can help. Whether this is also the case with rail connectors is beyond my knowledge, as I have not yet done this.

However, from experience with my regulars' boards, which are changed from time to time, I can say that the connectors are very robust and conduct electricity perfectly. Should contact problems, nevertheless, occur, careful bending is sufficient. In stubborn cases, you can simply pull them off and replace them. For stationary layouts, soldering the tracks seems to me to be rather unsuitable; if tracks or points have to be replaced, it could become problematic.

Christmas surprise from the Creative World Record Team:

I would like to thank you very much for the wonderful figure. The "Smallest Stelter in the World" makes me very proud. I also emailed it directly to my former editor from NRW-Duell. Thank you very much and all the best for the coming year, which will hopefully be better than the last one.



Comedian and TV presenter Bernd Stelter (photo left) is happy about miniaturisation. It found a place of honour in the display case next to his Bambi, which he received in 1998 for "7 Days - 7 Heads". Photos: Bernd Stelter



Exactly based on a photo template showing Bernd Stelzer standing in front of a Christmas tree in the stage set, his replica was created on a scale of 1:87. The view from the corner of the eye seems to fall on his even smaller replica (in the same posture) on a scale of 1:220, which corresponds to the scale to which Birgit Foken-Brock (Trafofuchs) had previously also scaled down Carmen Nebel.

P.S.: As you can see, the figure has been given a “prominent” place on my shelf.

Bernd Stelzer, per E-Mail

Christmas Eve with the Z Gauge:

“The Smallest Christmas Tree in the World,” recognised and since April 2007 reigning world record holder, had a prominent television appearance on the Second German Television (ZDF) in prime time on 24 December 2020, and in this way also brought model railways back into the public consciousness a little.

In the popular Christmas programme “Christmas Eve with Carmen Nebel” at 9:21 pm, the well-known presenter presented, among other things, some bizarre and record-breaking Christmas trees with the help of superimposed photos. Only the tiny one from the Kreativ-Weltrekordteam (“Creative World Record Team”) was also physically present on the set.

Wrapped like a Christmas present and decorated with gold-coloured ribbon, it was placed between Carmen Nebel and her guest Bernd Stelzer on a glass table to be unveiled by her and presented and introduced in detail along with the backdrop. She described it as “precious cargo” that she had received on loan.

Then Bernd Stelzer, himself a television presenter and also a carnival performer, comedian, actor and singer, suddenly interrupted his surprised hostess and addressed what many may have already thought:

“All the people here always get something as a gift, only Carmen has to borrow something and give it back afterwards.”

He didn't want to let that stand and so, as the only person on the set to be initiated by the editorial team, he presented Carmen Nebel with a figurine in a jewellery box as the godfather of the Creative World Record Team: Created by Birgit Foken-Brock (Trafofuchs), it showed the presenter on stage in the clothes of her previous appearance, complete with microphone and presentation card.



Also based on a photo template from her last TV show, the presenter Carmen Nebel was created in Trafofuchs' workshop, holding a microphone in her right hand and a presentation card in her left. She knew nothing about her Christmas present until the moment Bernd Stelter presented it to her: only he and the show's editorial team were privy to it.

Of course, she was scaled down to our scale of 1:220 and yet, thanks to her stature, posture and hairstyle, she was instantly recognisable. The surprise was written all over her face and she was delighted to thank our editor-in-chief again personally after the shoot was over.

Meanwhile, the community of six people behind the record-breaking tree did not want to leave it at that. Contacts were quickly made and photo templates sent to create a souvenir for Bernd Stelter as a thank-you.

Based on a photo template, he was finally miniaturised by Trafofuchs himself and inserted by Holger Späing into a small replica of the set on location. There he can be seen exactly as in his photo in 1:87 scale, his gaze falling on his smaller replica in 1:220 scale.

Of course, he did not know about this beforehand and so his wife was allowed to surprise him at Christmas with the “smallest Stelter in the world.” In the letter to the editor before this report, he expresses his gratitude for this himself.

Manufacturer's response to the letter to the editor from Trainini 12/2020:

Our reader Michael Werstein had reported in **Trainini®** 12/2020 about his frustration with a model of the EMD GP38-2 made by AZL. Our North American correspondent Robert J. Kluz, editor-in-chief of our partner magazine **Ztrack** and distributor of AZL responded as follows:

"I believe you are missing quite a lot of information on the chassis issues with the AZL GP38. First AZL has very much acknowledged the issue. We have encouraged customers to contact us when they experienced failures. Since 2015 we have been replacing broken chassis free of charge. These chassis were sourced from the first run of GP38s (2013 series).

In May of this year, we announced an all new exchange program. For a nominal fee, customers can return their first run GP38 chassis to us and it would be replaced with a 2020 series chassis. This program is for anyone who has an original chassis, regardless if it is defective or not. This information is on the AZL forum. It is pinned to the top of the AZL Knowledge Base category.

Here is the link: <https://azlforum.com/thread/1626/2020-gp38-chassis-exchange-program>.

This exchange program has been extremely popular and we have handled returns from all over the world, including Germany. In addition to the link above, we have the exchange parts and details of the program listed on AZL Direct. Here is the link: <http://www.azldirect.com/parts>."

AZL is offering to customers who have first generation GP38-s an opportunity to upgrade the chassis and running gear to the current 2020 chassis. The 2020 chassis features an upgraded split frame chassis, improved motor, AZL standard PCB, and newer trucks.



This program is only for first generation GP38-s. To be eligible for the program, customers are to place their order online via www.azldirect.com.



Once the order is placed, the customer will be required to return the original chassis, including motor, and trucks. For those with a damaged chassis, this program covers those chassis as well. Return the chassis including motor and trucks to the following address:

AZL has meanwhile launched a second exchange programme for the known problem. These two chassis are also offered in this programme: Item no. 62599-1 (photo above) and 62599-3 (photo below). Photos: AZL / [Ztrack](#)

Ztrack Distribution
c/o AZL Chassis Exchange
6142 Northcliff Blvd
Dublin, OH 43016
USA

Please do not (also) return the body (housing).

Upon receipt of the chassis, AZL shall send a replacement to the customer. The cost of the replacement includes shipping to US addresses. The cost for international replacement is higher to account for increased shipping costs.

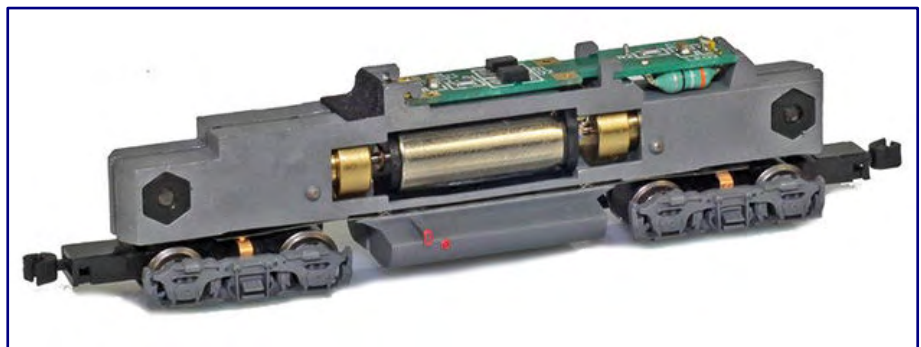
The following exchange packages and eligible locomotives are:

- GP38-2-Chassis with black tank and bogies (Art.-Nr. 62599-1) for BN (62502), BNSF (62503), CN (62504), CP (62505), CSX (62506), NS (62507), SOO (62510), MILW (62511), GTW (62512) and unmarked (62513)
- GP38-2 chassis with grey tank and bogies (62599-2) for SP (62501) and UP (62508)
- GP38-2 chassis with black tank and silver bogies (62599-3) for ATSF (62509).

This reference was supplemented by the following notes:

“We can allow dealers to buy chassis, but that would be at the same cost as for the customer,” he said.

We can arrange for dealers to purchase chassis, but it would be at the same cost as the customer. I hope that you understand our position. We are trying to keep the replacement cost as low as possible for those who have these existing chassis.



For the UP version of our reader this chassis with grey tank and bogies (62599-2) is the suitable replacement part. Photo: AZL / [Ztrack](#)

If German customers would like to arrange a bulk shipment, we definitely can work with them to do so. We would require the chassis to be returned in bulk and we can send the replacements out in one package.”

New products delivered from Oregon:

Micro-Trains will deliver some new items in January 2021, but they come from only two different casting moulds. For example, there are New York Central cattle cars in a pack of four (Item No. 994 00 110) and open freight cars with different loads and two different service numbers each for the Western Pacific (522 00 361 / 362), Southern (522 00 371 / 372), Union Pacific (522 00 381 / 392) and PRR (522 00 391 / 392).

Micro-Trains products are distributed by Case-Hobbies (<http://case-hobbies.de>) and others.

Model railway featured positively in the daily press:

Several daily newspapers ran headlines, probably partly prompted by an interview with Märklin's managing owner Florian Sieber, with the headlines “The return of the model railway” or “The model railway is back”.

The tenor of these reports was unanimously that the model railway was a crisis winner. People who have devoted themselves to this hobby are spending more time with it again and using it because other employment and entertainment options have fallen away.

This trend can also be seen in the increased demand for model railway articles. Manufacturers also predominantly report that they are increasingly confronted with consumer questions and concerns—especially from beginners. It fits in well with this that Märklin is also reporting an increasing number of younger beginners and that the average age in this toy segment is falling again.

We combine these developments with the hope that “old hands” as well as manufacturers, associations and clubs will seize the opportunity to welcome the newcomers, to help them and to make age- and target group-appropriate offers so that they can enjoy this creative and challenging occupation in the long term.

Unusual New Year's message 2021:

As usual, the staff of the Miniatur Wunderland Hamburg have dealt creatively with the current challenges and created an unusual New Year's message. With the help of over 100,000 matches and sparklers, they symbolically banish the Corona virus from our lives and our daily routine. But see for yourself: <https://youtu.be/8nK34JqSqJU>.

Boom at NoBa-Modelle:

“Work begets work” is an old saying from the trade. This apparently also applies to NoBa models (<https://www.noba-modelle.de>), because after a veritable flood of new designs as a result of the time windows gained from the events of last year, the two specialists from Aichtal can hardly keep up with the orders.

For this reason, the current new products are somewhat shorter than usual. We start with new products that are perfect for the cold season or already give a glimpse of spring: 3 sledges, (item no. 10508R), 1 team tent (10408R), 2 house tents (10407R), 3 two-man tents (10406R) and suckling pig on the barbecue (10259R).

All products are printed in resin, including the following ones:



The large crew tent (item no. 10408R) is an example of various new tents that can also be used at scout camps or in disaster relief operations. Photo: NoBa-Modelle

Battery-powered railcar ETA 150⁵ (5211R)
Control car ESA 150 (5210R)

Diesel locomotive Series V 320 without drive – Finished model (5016RF)
Kit for a Diesel locomotive Series V 320 (5016R)
Accessory kit for Diesel locomotive Series V 320 (5016.1R)
Double-decker wagon DByg 546 (5105R)
Accessory set for double-decker wagons DByg 546 (5105.1R)



Double-decker wagon DByg 546 (5105R). Photo: NoBa-Modelle



The V 320 has now also received prototypical bogies and can be set in motion with a ghost car in this form. Photo: NoBa-Modelle

It should be explained at this point that an ETA 150 (under a different art. no.) had already been in the programme for a while, but could now also be supplemented with a matching powered trailer. The V 320 was supplemented with newly designed bogies, which also correctly reflect the asymmetrical axle base.

Having previously been able to be operated with an extended Märklin running gear, it now becomes a model without drive but capable of rolling, which can be set in motion with the help of a ghost wagon.

Japanese Virtual Meeting 2020:

The Japanese Z gauge fans of Baden-Tani9 (<https://www.baden-tani9.com>) held their annual meeting on the weekend of 5 and 6 December 2020 under the title "Osaka Z Day 2020."

Our reader Raffaele Picollo from Genoa had participated in an earlier meeting and also reported on it here. As it was not possible to hold the event in the usual form due to the pandemic, the model railroaders held a digitally Zoom meeting.

They report on it in text and with videos at the following address—please set the language as desired in the top right-hand corner before clicking through: <https://www.baden-tani9.com/event/osaka-z-day-2020-online/>.

A creative start:

Accessories supplier EtchIT-Modellbau has taken off creatively after resuming its activities. The current new products include three passenger cars that do not coincidentally resemble popular prototypes.

For example, the prototype of the Sapporoshez 968 (item no. ET006_Z) was on the road in the former GDR, while the Triumph TR-4 (ET033_Z) and Aston Martin DB-5 (ET034_Z) revived the myth of English sports cars in the western hemisphere.

All three models come as unpainted kits, but due to their design they are quite easy to colour and have amazing surface quality and detailing. At a glance, all new products are on the following page: <http://easy01.bplaced.net/data/etchIThorne/index.html>.

New in circulation from Märklin:

After the end of the Christmas business, Märklin has now delighted the fans of Era I with two new deliveries. The first train set reproduces a goods train of the Royal Prussian State Railway (art. no. 81302), and consists of a P 8 steam locomotive and a Pwg goods train accompanying wagon, a beer refrigerator wagon of the group type, an acid pot wagon and an Rm stake wagon with a load of slab board stacks.



In January 2021, friends of Era I will be able to enjoy delivered New products with a Prussian (item no. 81302; photo above) and a Württemberg train set (81390; photo below). Photos: Jörg Erkel (1zu220-Shop)

Especially the beer refrigerator car looks very appealing. The steam locomotive is state of the art in terms of propulsion, lighting and detailed control. This also applies to the second set for the anniversary "175 years K.W.St.E" (81390).

It reproduces a goods train with passenger transport (GmP). It is hauled by a class T 9 tank steam locomotive, for which the similar model of the class 74 had to be used. It has a 3rd class through coach, a beer refrigerator car, an open goods wagon loaded with coal and an acid pot wagon on the hook:

Well-known railway historian deceased:

As reported in the internet forum of "Drehscheibe online", railway historian and book author Wolfgang Diener passed away on 3 January 2021 after a serious illness at the age of 76. He became known for his research into the paintwork and lettering of railway vehicles.

His well-known, but previously long out-of-print works were reissued a few years ago in three volumes by VGB in a revised and updated form. They were published there between 2012 and the end of 2017. We had reviewed the third and last volume in **Trainini®** 3/2018.

His expertise was recognised and, according to our information, he was also in contact with the documenters at Märklin. In this respect, Zetties may also have profited from the results of his research, mostly without knowing it.

January New products at WDW Full Throttle:

This US small-series manufacturer has introduced a new wagon double pack (item no. FT-2058-1). Both 33-foot bulk freight cars with outside box braces have a light grey paint scheme and Burlington Route inscriptions with the advertising slogan "Everywhere West."

They are available in Germany from Case-Hobbies among others (<http://case-hobbies.de>).

EM reader's choice now also for Z scale:

For the first time, the publishers and editors of the railway magazine have decided to award the "Model of the Year" award, which is based on a readers' vote, in a separate Z gauge category (for rolling stock).

We are extremely pleased about this step, as it gives the fans of our scale the chance to vote for their own new design, which was particularly well received by the readers.

Outstanding new accessories, on the other hand, will remain in the familiar category and will have to compete against those of the larger gauges.

We will keep a close eye on the results and compare them to see to what extent our assessments correspond with the readers' impressions.

Therefore, we call on you to participate actively and to lend weight and continuity to this important readers' choice for our scale.

By the way, you don't have to vote for other categories if you don't feel confident enough to do so.

All important information and the voting card can be found in Eisenbahnmagazin 3/2021, which will be available from your dealer or station bookshop, as well as, from the publisher from mid-February. As usual, many attractive prizes will be raffled among the participants.



Cover page: Alba Publikation / Geramond.

AZL deliveries for January 2021:

The ALCO RS-3 will be released this month with three different service numbers in the attractive Burlington Northern colours (item numbers 63311-1 to -3) and twice in SSW design "Cotton Belt" (63313-1 /-2).



The EMD GP38-2, which is the first model once offered by AZL, reappears in its current state (with new chassis) for the Milwaukee Road (62511-4R) and the popular Union Pacific (62508-1R to -4R) in limited editions.

The additional letter at the item number indicates that these are newly upgraded first edition housings.



The recently introduced R-70-20 refrigerator cars now appear as the second version for the Golden West (VCY) as a pack of four (904804-1) and two (914834-1) as well as a single car (914804-1).

The ALCO RS-3 looks very appealing in the BN colours (item no. 63311-2; photo above). The still almost new refrigerator cars wear colours and inscriptions for the Golden West Service in January (914834-1; photo below). Photos: AZL / [Ztrack](#)

Better known, however, are the Gunderson MAXI-I container freight cars, which now again feature the BNSF Herald logo, but are loaded with Genstars containers (906509-1GE to -4GE).

Manufacturer photos of the current deliveries can be found at the following address: <https://www.americanzline.com>.

Sad news to end on:

Shortly before the editorial deadline, we received the sad news that Graham Jones of the Z-Club GB, passed away at the age of 70 as a result of the Covid 19 disease. Graham was the most important "asset" for our small scale in the British Isles.

With his layouts, mostly built after English models, and often self-made models, he repeatedly took part in fairs and exhibitions in Germany, above all, the biennial meetings in Altenbeken. Thus, he was well known to the local public and appreciated by the visitors.

Our editors' last contact took place on 22 December 2020 by e-mail. At that time Graham was still in good health and also proudly reported on his two grandchildren, whom he proudly saw before him as future model railway enthusiasts, just like his son Damian.

At the moment we cannot comprehend that Graham, such an accomplished, competent and likeable person, a true English man of honour, has passed away so suddenly. Our thoughts are with his wife, son, daughter-in-law, grandchildren and all relatives.

Graham Jones will be irreplaceable for them and for us. We bow to him and his life's work and would like to remember the good times we had with him.

Picture supplement to the New Year's greeting:

Jochen Brüggemann added the following picture motif to his greetings and wishes to us (see page 56 above), for which he sent the following information:

“50 622 passes through Etzelwang with a photo goods train on 28 December 1985 as part of the closing event for the anniversary ‘150 Years of German Railways.’ I took the photo (...) myself, in wet and cold winter weather with wet snow (...).

In general, this event was excellently organised by DB. Participants in the event (i.e. passengers of the special trains) could, if interested, take photos, get off at a station before Etzelwang and travel ahead to Etzelwang by two buses (and DB tour guide).

Only after the tour guide had positioned all the photographers in such a way that none of them could be seen from behind (which would have put them in the way of other photographers or in the photo), did he give a signal to the dispatcher to call off the special trains (the passenger trains and the additional goods train) one after the other. The snow flurries were wonderful, with the wet snow also sticking to the locomotives...!”

We would like to share his memories with all our readers at this point.



Photographs like this one from 28 December 1985, when 50 622 with a photo goods train passes through a small stop in front of Etzelwang station in a snowstorm, are unlikely to be repeated. The setting would suggest that this photo was taken before 1968. Photo: Jochen Brüggemann

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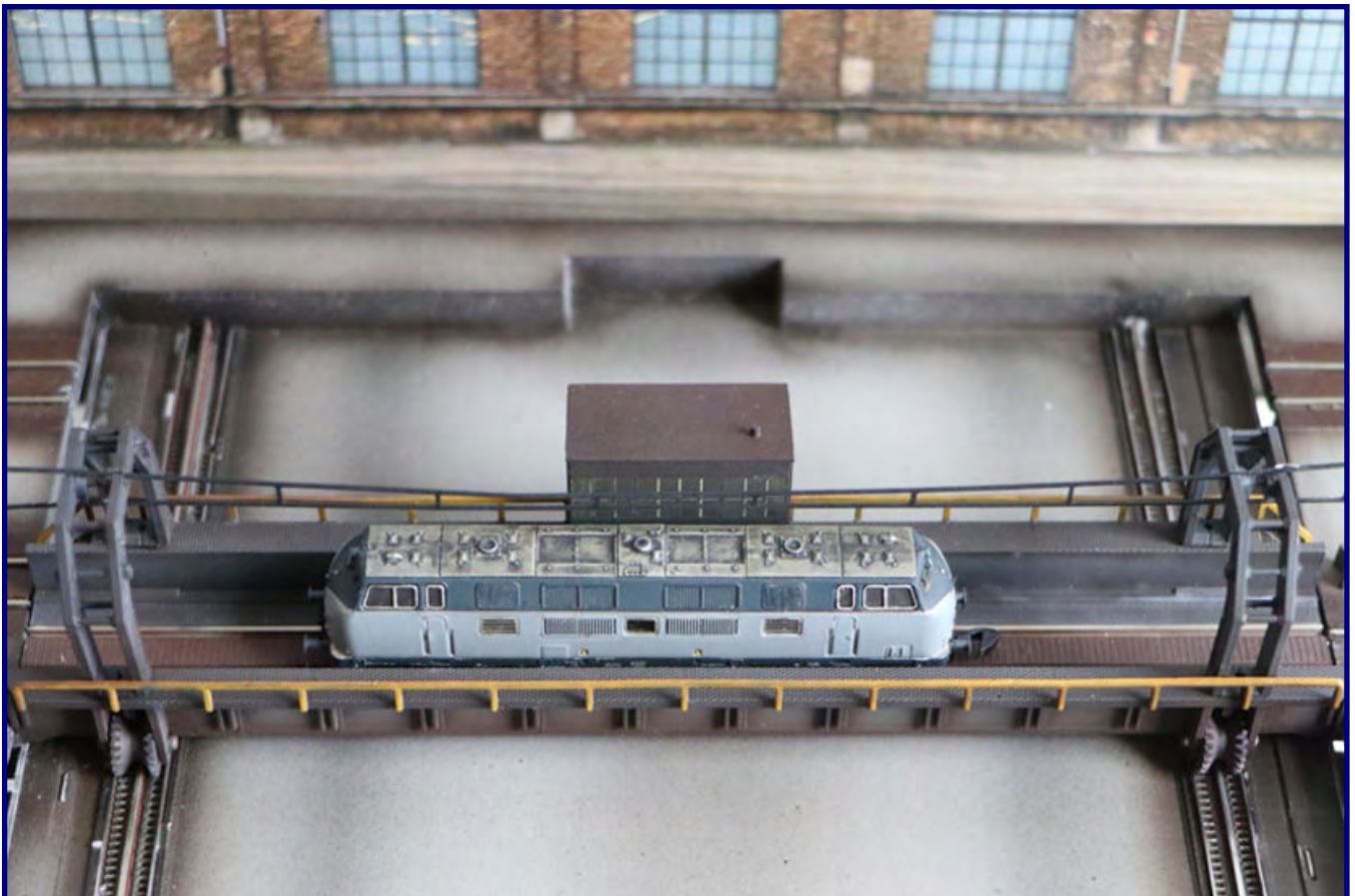
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Spring New Products 2021

Altering Hardboard Kits
A Maritime Emergency off Sylt

Introduction

Dear Readers,

At the time of publication of this issue, the Nuremberg International Toy Fair would actually be long behind us again, but here, too, everything is different in 2021: the leading fair has been postponed and will probably have to do without model railways altogether.



Holger Späing
Editor-in-chief

We have our doubts as to whether it will take place at all, because the developments of the last few weeks do not allow us to make a reliable forecast. Of course, we too would like to be able to meet fans and family again, or to exchange ideas with like-minded people at events.

But for a while we will probably still be busy alone at home. We are lucky to have an extremely creative and versatile hobby that many people envy us for: If you don't have one, boredom literally falls on your feet.

Not so with us, because we have been busy crafting again in the last few weeks and have started some new projects. The worry of being able to complete everything as planned within a fixed time frame is more likely to spread.

The risk of delivery delays remains, and all too often we have to react to current circumstances that do not always make everything plannable. So, we touch on the subject of trade fairs once again: With Faszination Modellbahn exhibition, the first one has already been cancelled, and important association exhibitions have not even been planned.

And nobody knows yet what will happen to the Intermodellbau in Dortmund. But we are prepared, and, thanks to our readers, we are well equipped. Günter Falkus and Hermann Kammler are proof of this today.

Here, two tinkerers with very different interests and focuses have joined forces to create great things. This is model building in its perfect form: a good idea as the initial spark, analysing strengths and weaknesses, finding suitable building partners and then realising it together!

The most beautiful things are not created by simply putting together purchased parts. Model making means planned action and often enough also construction. And what I can't do myself, someone else can. Friendly turns and "neighbours helping neighbours" are virtues that also carry us through difficult times.

Now let your thoughts roam, enjoy the many new products that we have been able to collect and let yourself be inspired by what wonderful things can come out of them. No matter how it will look individually, there will certainly be room for your result in this magazine.

Trainini® also depends on participation and mutual sharing, sometimes with the help of many people. And we are proud of that! And now my colleagues and I hope you enjoy reading.

Sin-Z-erely,

Holger Späing

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Currently no items

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Currently no items

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We thank Günter Falkus, Hermann Kammler and the DGzRS for information, contributions and photos.

Date of publication of the German language version of this issue: 22 February 2020

Cover photo:

The grey-painted V 270.09 of SGL, a locomotive of the former class 221 of the German Federal Railways, was chosen by Märklin as the “Messelok (trade fair locomotive)” 2021. Of course, our reader could not yet know this when he designed it in the model himself. And yet, he provided the decisive motif for this issue. Photo: Matthias Wistrach

Model railway scenery construction (Part 1)

And what does not fit ...

...is made to fit. A German craftsman's proverb perfectly describes what this report will be about: We have a specific modelling situation and an architectural kit that seems ideally suited for it. However, since it cannot be used without modifications, we first have to adapt it for our purposes.

This article is intended to be a first prelude to documenting the creation of a diorama for magazine shots and to give valuable modelling tips over the course of several episodes. After all, our project does not differ from the classic approach to diorama engineering; only the use of the finished piece will differ in some respects.

In our case, it is about building a 66 cm long photo diorama for taking photographs of longer train formations. Its width, height and cross-section of the terrain are already given by other dioramas which we want to be able to combine this new piece with. In order to prevent the showpiece from appearing boring later on, it should offer the viewer some visual interest and variety along the track.



We liked the modular railway arcade elements from Modellbau Laffont so much that we also wanted to use them in a modified form as retaining walls on a railway embankment. In this article we show how this can be done.

Even before construction began, this general plan gave rise to the desire to visually break up the long railway embankment at one point. Inserting an arcade wall section seemed to be well suited for this purpose, and we found what we needed with the two-track elevated brick and masonry railway arcade elements from Modellbau Laffont (art. no. Z2411).

However, building the 11 cm elements according to instructions and arranging them in a single or in multiple rows would not have fit the given dimensions of the diorama. We were therefore faced with a classic model railway problem that, fortunately, could be solved quickly.

Theoretically, we could have chosen alternative products from other manufacturers, but we had already been committed to the aforementioned arcade since it appeared as a new product last year: It had to be a modern hardboard kit, and Stefan Laffont's new product had a unique selling point on the market when it first appeared.

The only solution was to not build the kit exactly according to instructions, but to adapt it to our purposes. Many long-time model railway enthusiasts have been in similar situations when working with polystyrene sheets or classic building kits, which can be cut to size with a sharp craft knife or a circular table saw. Today, we would like to show you that the same kit bashing techniques can also be applied to cardboard kits.

The first step is to take measurements. A wooden strip with a rectangular cross-section will be used as a base for the elevated track has already been prepared. To avoid accidentally measuring over the wrong side, we start by marking its underside, which will not be visible later.



We do not need for our project the kit's interior parts, nor the lower and upper sides of the elevated arcades. The masonry parts (on the left in the photo) and later on the stones are sufficient. The marked wooden strip (at the back), which will later form the railway embankment, and a railway arcade element built according to the instructions (on the right) serve as aids for measuring and determining the necessary cutting lines.

We take the parts of the kit with masonry replicas out of the package and lay them out on our work bench. We will only use the long sides, but not the inner core and the cross sides. Since our arcade section will be 22 cm long, and given that it will be visible from one side only, we can take the two long sides from one element, and place them next to each other.

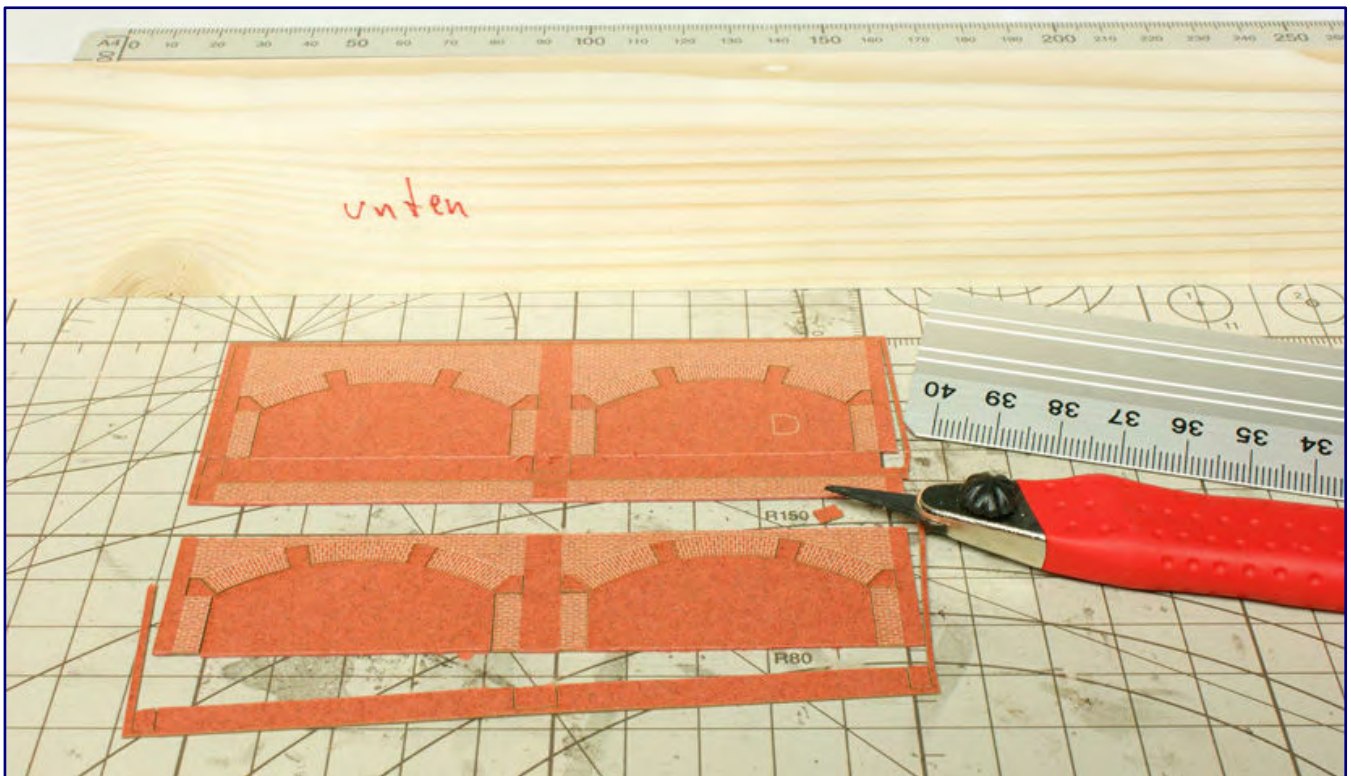
With the help of these parts, we now have to determine where and how the long side of the arcade can be divided horizontally, or shortened in height to obtain the desired appearance. A placement test with an elevated track element built according to the plan helps us to judge the looks of it.

Masonry cut to size

We find that material must be removed from the lower side in any case, because otherwise the characteristic arch could not be preserved. This would not only result in a less satisfying visual appearance, but would also violate the underlying principle of an arcade arch. After all, an arch that is interrupted at the top cannot dissipate the pressure forces on it into the foundation and would collapse.

We, therefore, decided to make a first cut at the lower stone facing. We began our work on the thinner cardboard that will later rest on top and is easier to cut because of the thinner material. A cutting mat from Tamiya serves as a secure base.

A steel ruler and a suitable craft knife are now the tools of choice. What is required is a good grip that allows safe and pressure-filled guiding, a sharp blade that does not tear out the material at the cutting edge, and also its stability during the cutting process, so that it follows precisely the cutting direction.



The wall panel is trimmed at the top and bottom with a sharp cut along a steel ruler. The choice of tool here should not be a matter of chance, because it is important to ensure that the finely engraved cardboard does not tear!

This is not the first time we have recommended the precision knives from Mozart, which have made such a lasting impression on us. The P2 T knife we are using here is currently available from Peter Post Werkzeuge (art. no. 01345) in two special offers with replacement blades (see news reports and supplier pages).

We then have to make another cut above the arch before testing against the wooden strip that everything fits, and confirming that the pieces have been measured and cut correctly. Only, we make the further work a little easier, and glue the shortened part onto the thicker masonry board.



To glue the thin, already cut attachments to the thicker backing cardboard, we use Noch's Laser-Cut Adhesive this time, whose dispensing tip is completely sufficient for this purpose.

Since, for this project, we prefer to spread out glue evenly over the entire surface, we don't need an extremely fine dosing needle, but a regular white glue would possibly lead to too thick of a coat. The glue that perfectly fit the bill for our purpose was therefore Noch's Laser-Cut Adhesive (61104). It is very fluid, spreads well, and can be applied in a very thin coat.

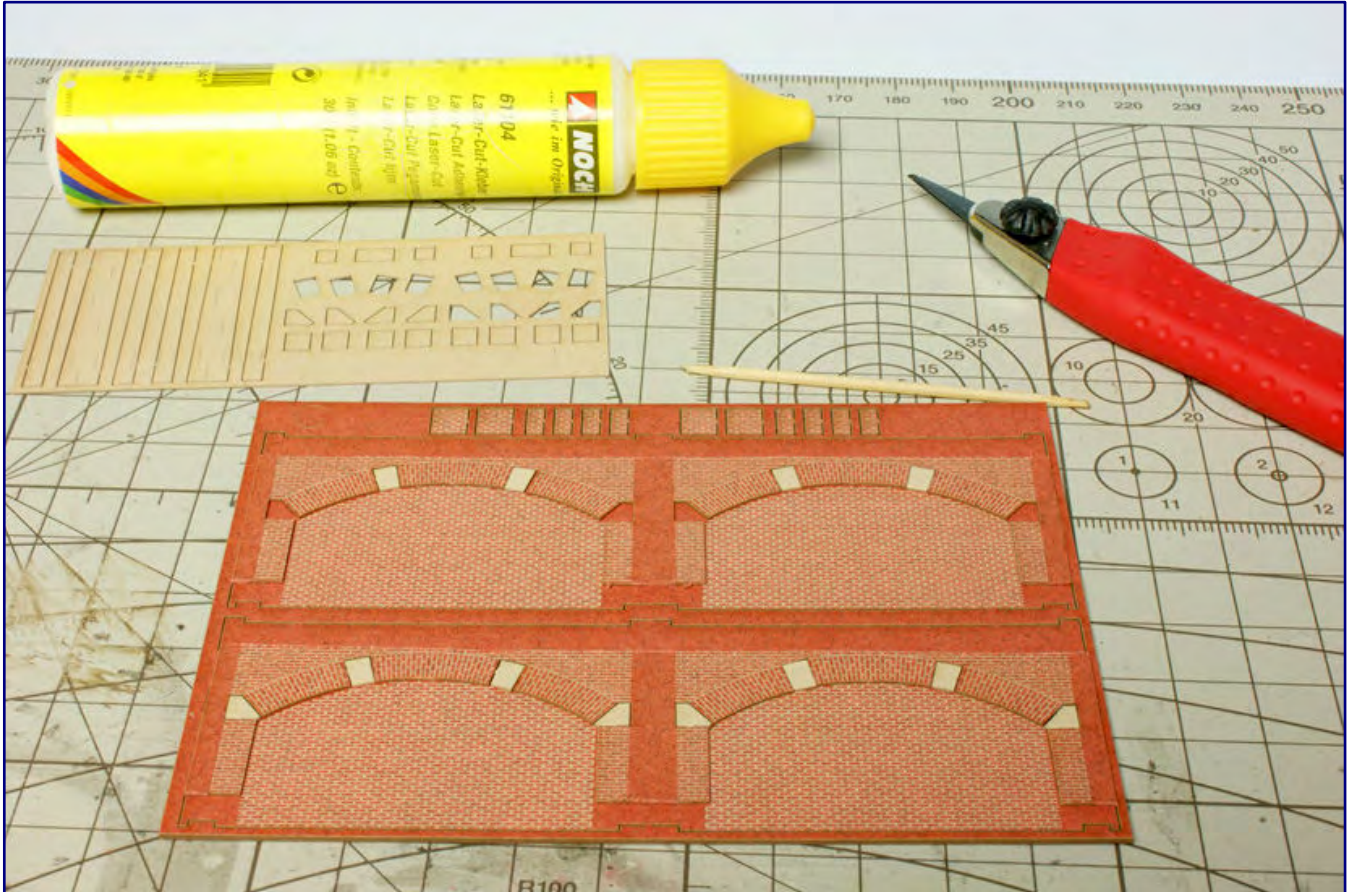
Placing the glued part on the other cardboard base must now be done precisely and with pinpoint accuracy, because subsequent shifting is only possible within fractions of a millimetre at best. Anything else would smear the glue and leave ugly marks.

If, on the other hand, a tiny drop of glue does escape from one edge, it can still be picked up with the tip of a toothpick without leaving detectable traces. We now give both parts a few minutes until the glue sets. Only when we are sure that the parts will no longer move against each other, do we proceed to the next step.

Next, we insert the intermediate stones into the arches, following the kit's instructions. This increases the stability and is the most comfortable way to do it. We dry fit each piece to make sure that we have cut out the right stone from the beige sheet and that it is aligned correctly.

Then, we place a small drop of laser-cut adhesive on the intended spot. To prevent any drips, this can again be done with the tip of a toothpick. Another toothpick, the tip of which we moisten a little with our tongue, allows the stone to adhere to it long enough for us to use it to insert it with pinpoint accuracy until the adhesive forces of the glue become sufficiently strong to hold it permanently in place.

After drying, it is now also time to trim the thick cardboard sheet horizontally at the top and bottom edges. The glued-on trim already marks the cutting edges where the steel ruler is to be placed, thus avoiding the need for measuring again.



After drying, we insert the decorative stones into the arches. But the rows of facing stones between the individual arches will have to wait!

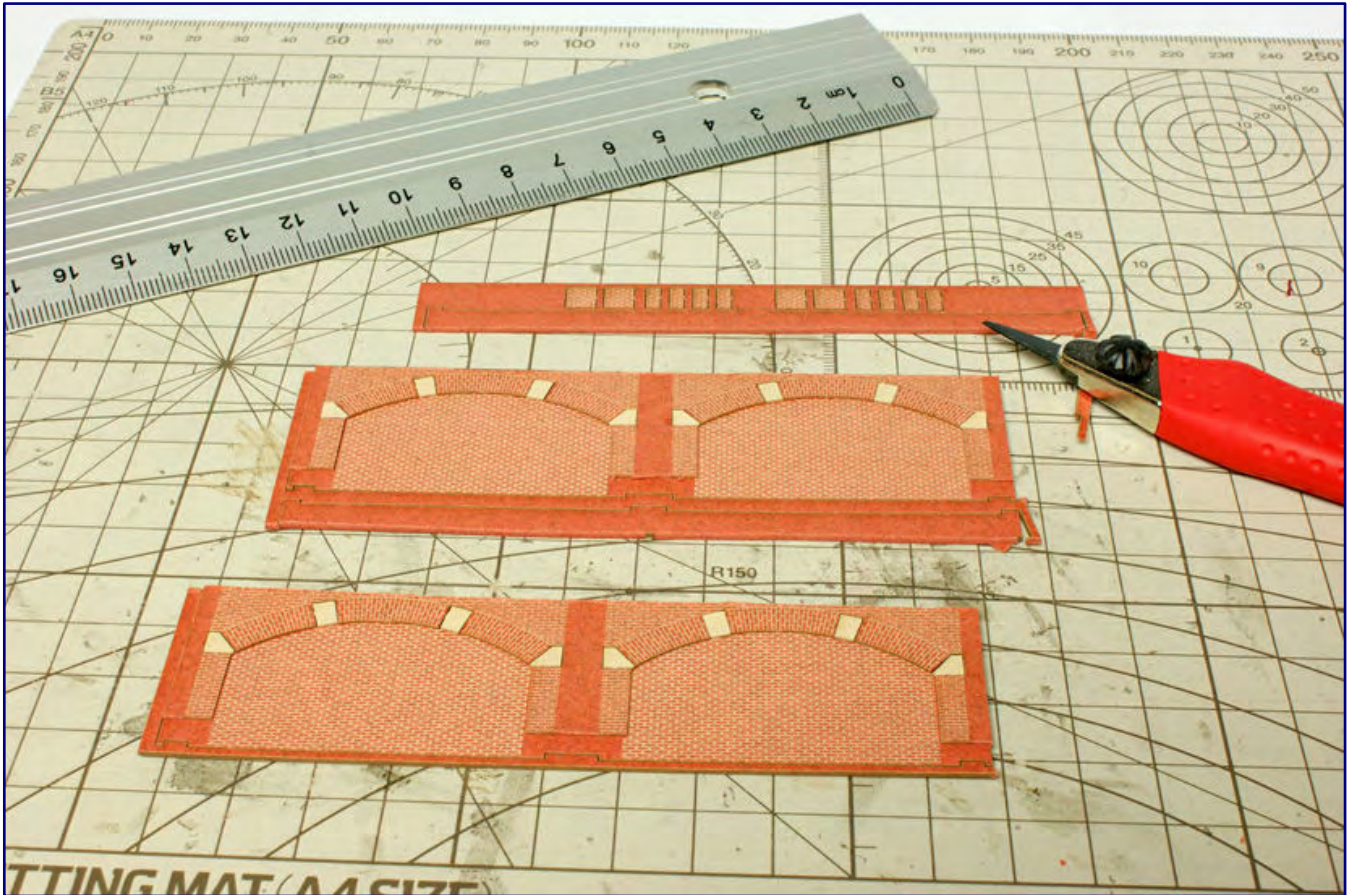
The only thing that is needed now is precision in placing the ruler precisely, and holding it securely so that nothing can go wrong! Carefully pull the blade along the ruler's edge through the cardboard material. The cut does not have to go through the entire material in the first run.

We simply repeat the movement with gentle pressure until the pieces are separated. The first cut makes it easier to guide the blade for each subsequent cut and, thanks to its sharpness and durability, no material tears out. The result is convincing.

A seamless connection

Now our work is slowly coming to an end, but before that, a small challenge awaits us: we now have two arcade walls in front of us, which are to be installed next to each other.

A visible separating edge between the two would, therefore, be disturbing. So, we have to find a way to camouflage the transition, as best as possible. But, a joint would always be visible, no matter how precisely the walls are fitted next to each other.



The attached cardboard parts of the arches (with the decorative stones) show where to make the cuts in the thicker bottom part: Now it's all about a sharp cut and a blade that works just as precisely. The knives from Mozart surpass everything we have ever used before.

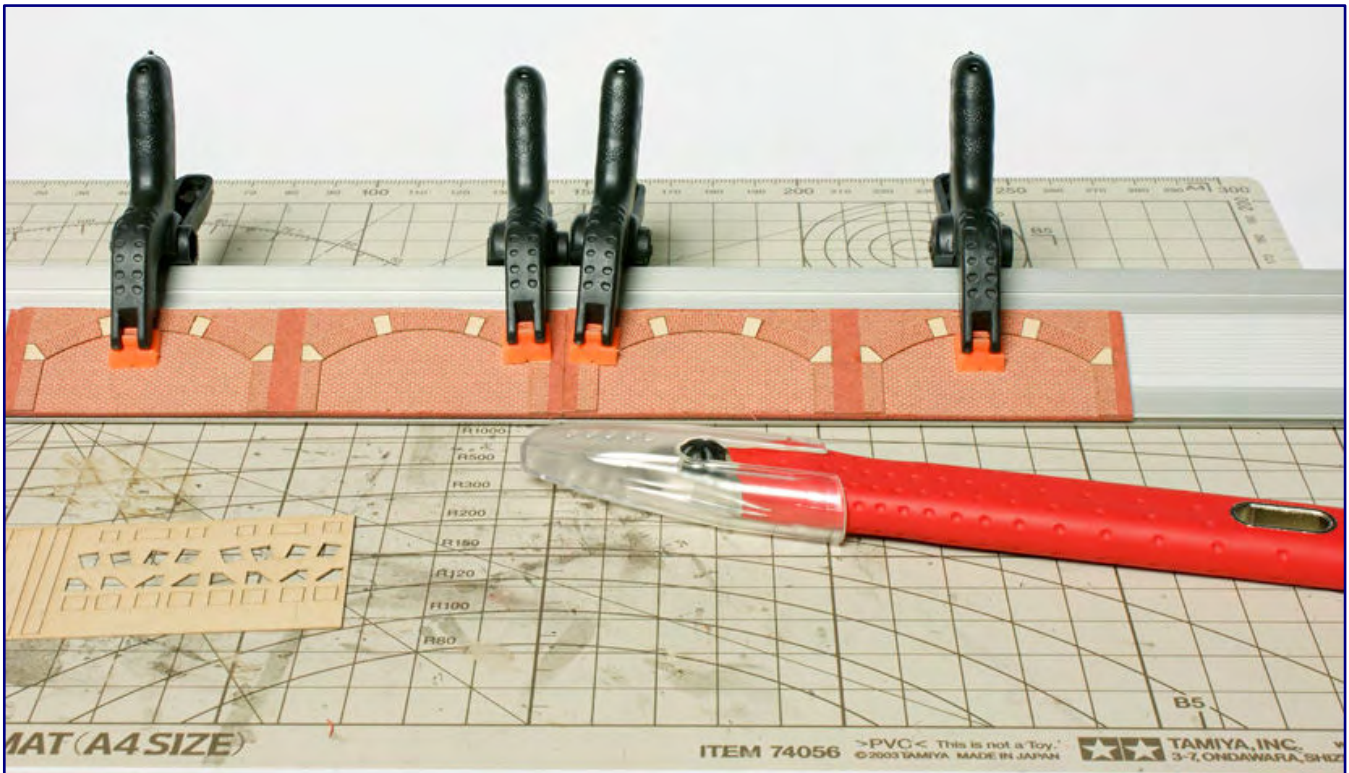
What we need to ensure is a firm and permanent connection of the two walls, for which, however, only a very narrow adhesive edge is available. Additional support will therefore also have to come from at least one long side.

So first we check how precisely the two walls can be joined together with the help of the steel ruler so that they line up neatly at the bottom and there is no kink between the walls. We are encouraged by the result and only need to support them long enough until the glue has hardened.

This task is done by four model clamps from Revell (39070). After drying, the glued seam is the first one on which we apply the column of facing stones from the beige cardboard sheet. Here, we choose one of the two wider rows that were actually intended for the area between the two arches of one of the two arcade pieces.

This then provides the desired, lasting stability. By the way, this is also the point where we decide whether to finish flush with a full stone at the top or bottom of the wall. We opt for the top edge to be flush, given that the edge of the bottom stones will disappear anyway a bit into the ground later on.

We repeat this over all columns of facing stones in such a way that the top edge and offset are coordinated and fit together. A second strip of greater width remains for the next step. After that we still have four narrower strips to do, two of which have to be pushed together to form a wider column and be glued on.



The two arcade parts created from the kit must be glued together at their narrow end walls in order to be able to conceal or close the gap between them. We carry out alignment tests on the steel ruler to place them exactly at the same height and without any kinks in the run. Revell model clamps fix the parts in place. Once the desired outcome has been achieved, glue is applied and the parts are finally aligned and fixed until the glue has hardened.

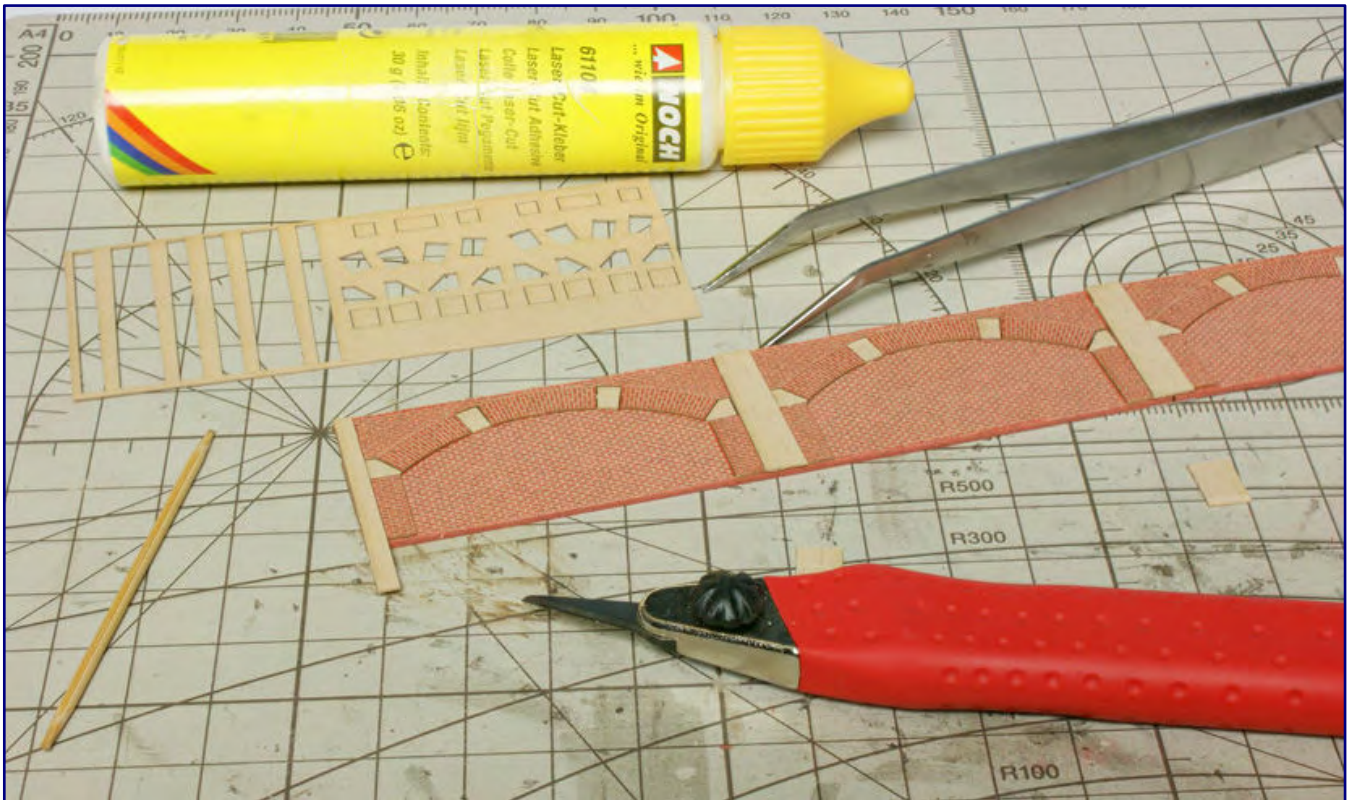
This, too, is so successful that only a small number of people will notice it later. There is no need to use fillers, and we have been able to conceal the centre with a barely visible joint. As soon as everything is dry, we turn the cardboard wall over and also cut the overhangs of the blind rows flush with the Mozart precision knife.

We measure the wooden base strip once more and are satisfied with the result. There is only one more thing to do now. Every wall needs a finish at the top, which has both visual and functional reasons. It can also prevent rainwater from penetrating the masonry and bursting open during frost.

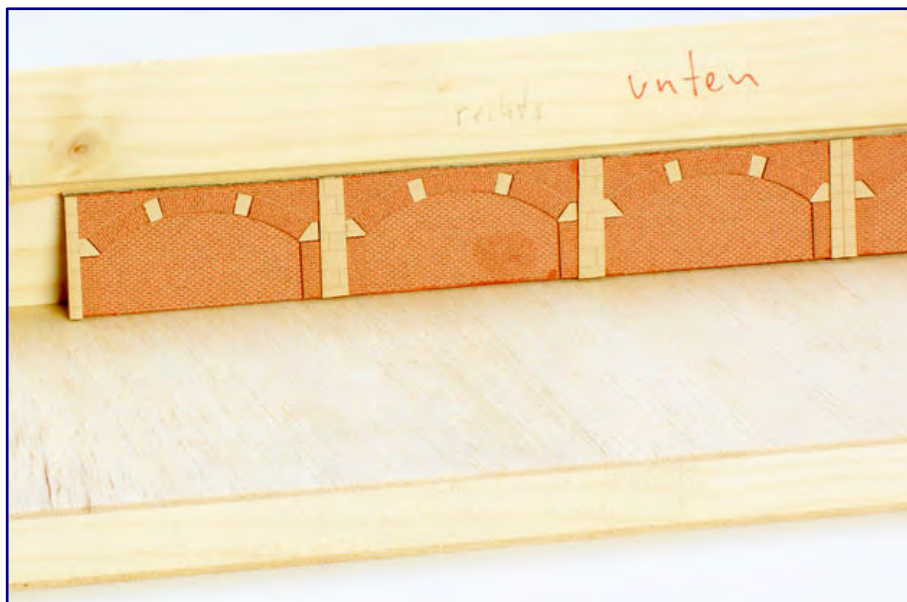
Such terminations or wall crowns can consist of additional bricks or stones, which are placed transversely to the direction of the wall, or, as suggested here, as a concrete base. This could also take over the function of an anchor and distribute forces more evenly over the entire length.

Such a concrete base can be built quickly and without effort. All we need is a sufficiently long piece of drawing cardboard. Cut a strip from it along the steel ruler and glue it on with the laser-cut glue.

Its width is so small that it is flush at the back (with the embankment), and only has a tiny projection of half a millimetre at the front. Converted to the prototype, this would be a 11 cm overhang, which is almost not visible in the model.



The penultimate step is to cut out, align (uniform stone columns) and glue on the decorative strips between the arches. They are shortened to fit only after they have dried.



This brings us to the end of our task today.

We have finished a nice little after-work craft session and can look forward to the landscaping part of our project.

That, however, will be the topic of part 2 of this little series.

Photo left:

The final step is to cut out a skirting board from cardboard and glue it to the top of the wall. The alignment test against the railway embankment proves that everything has worked out fine.

Manufacturer of the basic kit:
<https://www.modellbau-laffont.de>

Suppliers of glue and tools:
<https://www.noch.de>
<http://www.peter-post-werkzeuge.de>
<https://www.revell.de>
<https://www.tamiya.de>

Marine emergency deployment as a model **Rescue from the Greatest Distress**

The ship and crew members of the DGzRS are heroes of our time. Supported by state-of-the-art technology, they go out when others come in – to save the lives of their fellow men. Often enough, they do this at great risk, some men even kept to themselves by the sea. Two model builders remind us of this hard work on roaring seas and show us what we don't get to see, otherwise.

The German Maritime Search and Rescue Service (DGzRS) has often been the subject of reports in this magazine. The reasons for this are probably manifold: On the one hand, maritime themes fascinate many model railway enthusiasts, because they offer a wonderful contrast to the widely represented mountain motifs.

And in recent years, thanks to Dirk Kuhlmann's layouts and dioramas with a touch of the North Sea, they have also been swimming on a wave that has produced appealing accessories or loads. But it may also be due to the good reputation of the aid organisation itself, which works without tax money and with a large number of volunteers, and has done so for over 150 years

What's more, thanks to their very familiar colours and design, the rescue cruisers are immediately noticeable in any harbour. And that means a sure eye-catcher for modelling after all. Günter Falkus and Hermann Kammler have used exactly that and combined it in their own way with movement, excitement, and the model railway.



The Theodor Heuss (KRS 2) founded a new era of sea rescue and plays the leading role in the diorama presented today. Archive photo: Die Seenotretter - DGzRS

But, before we get to the main character of her diorama, which we present here, we would like to take a look at how it came into being and its history: Christened in early 1957, the “Theodor Heuss”, named after the first Federal President of the Federal Republic of Germany, bore the internal registration KRS 2.

The abbreviation stood for “Küsten-Rettungsboot – Seenotrettungskreuzer” (coastal lifeboat – rescue cruiser), the number is a consecutive numbering system that was retained and continued without interruption, even after the introduction of a new designation system in 1992.

So, it quickly becomes clear that this ship is almost at the beginning of a development. It stands for a new beginning in rescue on the high seas and took up the experience gained with the first post-war newbuilding. The concept of a rescue cruiser with a daughter boat, which went into series production with the “Theodor Heuss”, celebrated its breakthrough with this ship, and has proven itself to this day.



The H. H. Meier (KRS 4) also belonged to the Theodor Heuss class. After the ship that gave it its name was decommissioned, it and its dinghy took on the name of the ship for a few years and then moved to the Deutsches Museum in Munich, as exhibits without giving it up again. Archive photo: Die Seenotretter - DGzRS

An entire class was named after this ship, which consisted of three more ships. The basic concept was constantly developed further, and continued in more than 30 successors of different sizes and classes.



In June 1987, the Theodor Heuss, former H.H. Meier (KRS 4), makes a stopover on the banks of the Rhine in Cologne on the first leg of its journey to München (Munich). Hermann Kammler has his picture taken on her, and his enthusiasm is aroused. Photo: Hermann Kammler

Good seakeeping and the best manoeuvring characteristics distinguish these boats in all operations and even under the toughest conditions, which our two model builders also demand of their replicas in their models.

Elementary design features are her net rib system, watertight transverse bulkheads, a double outer skin (with fuel tank between them, water cells and air tank containers that act as buoyancy bodies.

The superstructures can be closed on all sides and increase protection against sinking.

They are also protected from the consequences of capsizing in many ways, by further design refinements of the hull and superstructure. A wide keel protects against grounding or sliding over shallows.

Prof. Dr. Theodor Heuss, who gave the ship its name, took over the patronage of the DGzRS, which has since become a tradition for all his successors in office.

By the way, the dinghy of the rescue cruiser named after him was given the name "Tedje", a North German short form of his first name.

The prototype ship was stationed at the Borkum station until 1963 and then in Laboe, until it was decommissioned in 1985. It was then sold privately, renamed and converted into a cabin cruiser.

The original names of ship and dinghy were taken over on 1 August 1985 by the former "H.H. Meier" and her daughter boat, both belonging to the same class and only slightly newer. This duo remained in the DGzRS's inventory as reserve stock for two years before it was transferred to the Deutsches Museum in München (Munich), where it was exhibited as a milestone in the history of technology in the outdoor area.

Emergency Marine Deployment as a Model

Let us now listen to the report of Günter Falkus, who describes how he came to the idea and its implementation: "The rescue cruiser Theodor Heuss was stationed in Borkum at the beginning of the sixties. I was lucky enough to experience the launching manoeuvre of the dinghy from the beach during a holiday in Ostfriesland (East Frisia), a very lasting experience.

After many, many years as a model ship builder, I then got into Z gauge in 2015. My first work was the well-known quarry with many functional scenes, which I also displayed at the Advent meeting in Zell (Mosel) in 2017 and 2019.

Finally, after a holiday on the island of Sylt, I had the idea of how I could link the Z gauge with sea rescue: In a small diorama I wanted to stage a fictitious emergency operation! Sylt in 1961, the Hindenburgdamm, a thunderstorm, a churned-up North Sea and an accident victim in the surf at the Hindenburgdamm – many ideas popped into my head.”



Dramatic scenes were to take place in front of the Hindenburgdamm, which Günter Falkus imagined in his mind, in order to later transform them into a model. The happy end of the rescue operation was never in doubt. Photo: Hermann Kammler

So, here we now describe dramatic scenes that will have a happy ending, but were not recorded in any logbook: The sky over the German North Sea around Sylt is black as night. A heavy storm brings rain, lightning and thunder, transforming the previously peacefully undulating sea into raging masses of water with waves several metres high.

A cutter, unable to manoeuvre, is pushed against the Hindenburgdamm by these masses of water, temporarily aground and badly damaged. Water breaks in, the small ship takes on a heavy list and now threatens to sink. The crew desperately fights for their lives and is no longer able to send a distress call.

But the staff of a passing train fortunately notice the emergency, stop and make the decisive emergency call to the rescue control centre. The DGzRS sea rescue operation begins, and only a few minutes later the Theodor Heuss arrives to help the victim.

The German Armed Forces, which are involved in the SAR service ("Search and Rescue"), also send help. Despite the danger to its crew, its rescue helicopter ventures to the scene of the accident. The train's personnel set up lanterns at sea to detect people who may have gone overboard and to guide the rescuers.



The dinghy Tedje is launched and sails into shallow water, while the German Armed Forces naval aviators help from the air with a Sikorsky rescue helicopter.

The dinghy Tedje is launched and sails into shallow water towards the largely destroyed wreck, where the heavy waves and foaming spray continue to pull. In the meantime, the Bundeswehr helicopter has also arrived and ropes down a rescuer with the winch. He is finally able to rescue the skipper from the shipwrecked vessel and brings him on board the rescue helicopter.

As the heavily damaged ship disappears into the waters of the North Sea, all crew members are safe. Done in, but satisfied, rescuers can make their way to safe land with the rescued. This time everything went well, the damage was limited to the material.

The road to implementation

But, this is by no means a matter of course. Even though fatal accidents during rescues on the high seas have decreased significantly, the German Maritime Search and Rescue Society has had to mourn human losses in its long history.

Günter Falkus and Hermann Kammler have found a way to capture the drama of this difficult work, and make it visible in the model without gloating over the suffering of other people. The viewer is moved, hopes, and feels sympathy – as probably do the relatives in such an emergency on land.

Günter Falkus, who has been a ship enthusiast for decades, describes the further path from the idea to the final screening piece as follows: “Moving waves, lightning, the roar of the storm and the sounds of the helicopter were to round off the scenes.

And, all this was to find its place in a diorama of manageable size. In the end, it turned out to be 55 cm long, 38 cm wide and 48 cm high. I already owned the very detailed model of the rescue cruiser in the appropriate scale.



Günter Falkus had already built the sea rescue cruiser KRS 2 “Theodor Heuss”, Now he was looking for a useful purpose for the planned diorama – far away from his prototype station Borkum.

Dynamics was set for this diorama, but who could support me with the technology required for it? Well, I came across Hermann Kammler, known to many Zetties as “Bagger-Hermann,” a gifted, functional modeller, at the Z Gauge exhibition of the Stammtisch Untereschbach.

He accepted the challenge and realised the technology. My friend Michael Hess helped us with the Arduino programming. In the area of special challenges, I turned to the water surfaces, which had to be flexible and move with the ship as it swayed in the raging sea. Otherwise, they would not have been able to look like water.

I used acrylic (paintable), silicone (not paintable; colour-repellent) and household disposable cloths. I modelled with them until the churning sea with its splashing and foaming wave crests including spray was right in front of me.

In the tripartite division of the complex and also very different tasks, I see a prime example: such a model builder cooperation proves to be ideal even outside of previous presentations and publications. If one strives for a certain perfection of what is shown, the individual quickly reaches his limits.

On the other hand, if you look for like-minded people with different skills and competences and bring them together, you can combine their strengths and knowledge to create something completely new and extraordinary.”

Functionality and procedures

The diorama of the three tinkerers is controlled with four Arduino Nano. Several light-emitting diodes for the lightning on the background scenery, gear and stepper motors for the movements, two sound modules for helicopters and thunder, create the heavy thunderstorm atmosphere.

The stepper motors make the Theodor Heuss swing, her dinghy Tedje is moved by Magnorail (magnet guided by a magnetic chain). The Havarist, which stomps up and down in the high seas, is also moved by a stepper motor.



A cutter is in distress in heavy seas off Sylt. The heavy thunderstorm over the North Sea pushes it off the Hindenburgdamm, the hull bursts, and the ship's hull loses stability. Photo: Hermann Kammler

Furthermore, the light of a flickering LED simulates the firebox of the steam locomotive class 24, figures with lanterns and the crew of the rescue cruiser complete the depicted scene. By pressing a button, a 4-minute loop is started, which starts the entire sequence of the rescue operation and shows it to the viewer.

continues on page 20



The stoker of a passing train happens to notice the accident, which leads to the decisive alarm of the Bremen Maritime Emergency Centre. Meanwhile, the train crew tries to illuminate the scene of the accident (photo above). The Theodor Heuss (photo below) is sent to help the victim. Photos: Hermann Kammler

I have attached great importance to authenticity and true-to-scale models. So, everything is built in the exact scale of 1:220. The masterpiece and, for me, the highlight of this arrangement is the Sikorsky S-58 (H-34) helicopter from the German Armed Forces Naval Air Service.

Hermann built it by hand and the result is a marvel of engineering on the smallest scale. It “flies” by being moved along a rod. It then stops precisely at the position of the victim to lower a rescue diver down.



The dinghy Tedje is the first to rush to the sinking cutter to rescue the crew members. Photo: Hermann Kammler

After a successful rescue, the helicopter is hoisted up again by the winch and disappears into the scenery. Synchronised to this, the characteristic original sound of a helicopter can be heard, accompanied by lightning and thunder of the heavy storm. Once the mission has been successfully completed, the SAR helicopter returns to its zero position behind the backdrop to wait for a new start at the push of a button.

This complexity demanded by Günter Falkus could only be achieved with four Arduinos working in parallel, some of which also had to communicate with each other for this purpose: Arduino 1 controls the rolling and pitching of the Theodor Heuss in the heavy sea, while Arduino 2 controls the total of 36 LEDs of the lightning installation, as well as, the storm and thunder sounds.

Arduino 3 controls the horizontal movement of the helicopter and synchronises its operating sounds with the movement. Arduino 4, on the other hand, controls its vertical movements including the movements along the rod. So, it is these two that are constantly communicating with each other, to always know exactly the helicopter's position.

Only the drive of the dinghy is analogue, as is the movement of the average man in the surf. Combining the high school of Arduino programming with the necessary mechatronics was certainly one of the biggest challenges, and, also, a new one for “Bagger-Hermann”.

Günter Falkus is in complete agreement with all the spectators so far that he has mastered this challenge brilliantly: “The finest technology in a tiny space”, is the conclusion that sums it up, so well.



In the meantime, a rescue helicopter of the type Sikorsky S-58 (H-34) has also arrived, sent by the SAR service of the German Armed Forces. It too is able to rescue a crew member on board with the winch. When the capsized average sinks, all people are off board and safe. Photo: Hermann Kammler

In this respect, it should come as no surprise to our readers that a number of dates have already been fixed for “the time after Corona.” So far, the presentation has been limited to the last Advent meeting in Zell an der Mosel and reports in “Köln Echo - Bergischer Bote,” “Volkszeitung Oberberg” and “Längsseits” (DGzRS magazine).

Today, the functional diorama is at home or stationed in Gummersbach. There, the showpiece is maintained, repaired and brought to exhibitions. Meanwhile, the next joint projects of the “G-&H-Team” are already in preparation.

We, and I am sure our readers as well, are all the more curious to see whether “Bagger Hermann” Kammler will now be joined permanently by “Boat Günter” Falkus. In view of his former passion for model building, his “new love” for Z gauge and the ambitions of this duo, this seems very likely to us. So, let's see what they will develop next.

Information about the DGzRS:
<https://www.seenotretter.de>



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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Eisenbahn im Bergischen Land Bedeutender Brückenschlag

Remscheid(-Lennep), Opladen, Solingen und Wuppertal sind Ortsnamen, bei denen nicht nur Ansässigen der angrenzenden Regionen sprichwörtlich „die Ohren klingeln“. Der wirtschaftliche Aufstieg der Region, der besonders die recht kleine Klingenstadt Solingen weltberühmt machte, ist eng mit der Eisenbahn verwoben. Die besondere Geschichte dieses topografisch herausfordernden Städtedreiecks zeichnet nun eine EK-Chronik genauestens nach.



Zeno Pillmann
Eisenbahnchronik Bergisches Land
Das bergische Städtedreieck Wuppertal – Remscheid – Solingen
Band 1: Strecken und Bahnbetriebswerke

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Über 150 Jahre ist her, dass der erste Zug, vom heutigen Wuppertal kommend, in Remscheid eintraf. Dies war ein Meilenstein in der Eisenbahngeschichte des Bergischen Lands und gleichzeitig auch seiner Wirtschaftsgeschichte.

Heute kennt jeder die Messer- und Klingenstadt Solingen, ebenfalls zum Bergischen Land gehörend, doch das wäre sicher in der heute bekannten Form nie eingetreten, wenn es die Eisenbahn nicht gegeben hätte. Die Region hat ihren Namen nicht von den Geländeformationen, die sie prägen, sondern von den Grafen, die hier früher herrschten.

Und dennoch stellte die topografische Lage der Städte im wirtschaftlich bedeutenden Dreieck die Ingenieure jener Zeit vor große Herausforderungen: Große Kunstbauten und Rampenstrecken waren nicht nur erforderlich, um Remscheid per Schiene zu erreichen.

Kaum noch vorstellbar ist heute, wie die Rohstoffe früher Schmieden, Schleifwerke und andere Gewerbeeinrichtungen der Region erreicht haben. Klingen wurden in Körben auf dem Rücken von Menschen zu ihren Verbrauchern oder Logistikzentren, die damals freilich noch nicht so hießen, geschleppt.

Autor Zeno Pillmann macht alles richtig, indem er genau dort einsteigt und zunächst die wirtschaftliche Ausgangslage der drei heutigen Städte Remscheid, Solingen und Wuppertal schildert. Er charakterisiert

das Gewerbe, das dort vertreten war, dessen Aufstreben und die Grenzen, die eine fehlende Bahnanbindung dort aufzeigte.

Wirtschaftlich drohte die Region ins Hintertreffen zu geraten, denn wichtige Abnehmer im Rheinland waren über den großen Strom leichter und günstiger zu erreichen als über das damalige Straßennetz. Und so war es wirtschaftlicher, Waren aus dem fernen England zu importieren als aus einer Region, die nahtlos angrenzte.

Auch das Ruhrgebiet als Lieferant von Kohle, Koks und Stahl schien zu weit weg, solange es keine Bahnstrecke gab, die beide Zentren miteinander verband. Der Buchautor begleitet Nöte und Entwicklungen vom Streben der heimischen Wirtschaft, über das Zeitalter der Privatbahnen bis hin zur preußischen Staatsbahn. Die Reise geht anschließend über Reichsbahn und Bundesbahn nahtlos bis in die Gegenwart weiter.

Der vorliegende Band 1, ein zweiter zur Betriebsabwicklung ist inzwischen auch erschienen, beschreibt dabei die spannende Entwicklung des Streckennetzes samt den baulichen Anlagen in Form der Bahnhöfe und Bahnbetriebswerke. Auch dem Jahrhundertbauwerk Müngstener Brücke von 1897 ist ein eigenes Kapitel gewidmet.

Immerhin ist dieser Stahlkoloss, der in den letzten zehn Jahren zunächst mit halbherzigen und beinahe missglückten Sanierungsarbeiten durch die Deutsche Bahn AG von sich reden machte, bis heute Deutschlands höchste Eisenbahnbrücke. Auch fernab ist ihr markantes Bild, das auch den Buchdeckel schmückt, deshalb bestens bekannt.

Beispielhaft möchten wir aus den Buchthemen noch Lennep herausgreifen: Aus seinem Durchgangsbahnhof wurde mit dem Bau weiterer Strecken nach Wipperfürth / Marienheide, Opladen und ins Tal der Wupper nach Radevormwald ein bedeutender Knotenbahnhof.

Das dortige Bw Lennep beheimatete neben preußischen Dampflokomotiven bald auch die damaligen Wittfeld-Akkutriebwagen. Zur Reichsbahnzeit waren hier die ersten Maschinen der Baureihe 62 beheimatet.

Deutlich wird schon dadurch das bunte Bild des Fahrzeugparks, der in diesem Buch zu sehen ist. Wie auch viele zum Nachbau anregende Brücken, Stadt- oder Landschaftsszenen aus allen Zeiten sind diese Bilder durchgängig schwarz-weiß und hervorragend wiedergegeben worden.

Weiter illustriert wird dieses hervorragende Buch durch Kartenauszüge, Skizzen, Fahrpläne und andere zum Thema passende, wie auch geeignete Abbildungen. Dabei stehen die gewohnt hohe Qualität und der fachliche Tiefgang der EK-Bände zu keinem Zeitpunkt in Frage.

Autor Zeno Pillmann zeigt in einer umfassenden Gesamtdarstellung die bemerkenswerte Geschichte des Schienenverkehrs im bergischen Städtedreieck so eindrucksvoll und kompetent, dass dieser Titel nicht nur ein Muss für Eisenbahnfreunde des Bergischen Lands ist.

Auch Modellbahner werden hier beispielhaft viele Anregungen und Ideen finden, dazu auch Verständnis für historische Zusammenhänge gewinnen, die beim Hobby nützlich sind: Wer glaubhaft und überzeugend Modellbau betreiben möchte, muss schließlich auch in die Lage versetzt werden, den „Atem der Geschichte“ zu spüren, aufzusaugen und in seinem Werk zu vermitteln.

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: **Publishing pages with reference possibility:** :
: <http://www.eisenbahn-kurier.de> :
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Anerkannte Seenotretter

Das deutsche Vorzeigewerk

Fast jeder Deutsche wird das Kürzel DGzRS in seiner Bedeutung auflösen können. Die Hilfsorganisation aus Bremen ist bekannt wie auch einzigartig. Sie erfreut sich deshalb auch höchster gesellschaftlicher Anerkennung. Ihre farbenfrohen Kreuzer sind Botschafter und auf Fotos gleichzeitig beliebte Urlaubssouvenirs. Ein Typenkompass schafft dabei Überblick über ihre Vielfalt.



Hans Karr
Typenkompass Seenotrettungskreuzer
Geschichte – Technik - Schiffe

Verlag Pietsch
Stuttgart 2018

Taschenbuch mit Klebebindung
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Wenn sich ein Fregattenkapitän a. D. anschickt, einen Typenkompass über die Seenotrettungskreuzer der DGzRS zu erstellen, dann dürfen die Erwartungen sicher zu Recht hoch sein. Deshalb waren wir besonders neugierig, was das hier zu besprechende Werk beinhalten würde.

Es hat in den Jahren seit 2013 bereits zwei Aktualisierungen und Erweiterungen erfahren. Die vorliegende, dritte Auflage von 2018 ist das jüngste und damit vollständigste Werk dieser Reihe. Persönlich begeistert von Tätigkeit und Schiffen der Deutschen Gesellschaft zur Rettung Schiffbrüchiger, ging der Rezensent folglich mit großer Spannung an seine Arbeit.

Doch bevor sich unsere Eindrücke dem Inhalt dieses Bands zuwenden, möchten wir kurz auf seinen Autor eingehen: Hans Karr ist heute Redakteur bei einer Marinezeitschrift. In den vergangenen Jahren veröffentlichte er daher in Fachzeitschriften zahlreiche Beiträge zu Themen der Marine und Seefahrt. Auch bei diesem Verlag gehört er zu den Stammautoren und so stammt auch eine ganze Reihe an Typenkompass-Bänden zur Schifffahrt aus seiner Feder.

Einen weiteren zur Hochseerettung aus Seenot zu schreiben, wäre aber mit einem reinen Portrait der Seenotrettungskreuzer zu kurz gegriffen. Das ist für Außenstehende zuvor sicher etwas näher zu erläutern.

Die Geschichte der DGzRS geht zurück bis ins Jahr 1865 und war bis dahin keine Selbstverständlichkeit. Zuvor waren Schiffbrüchige häufig auf sich allein gestellt und in Küstennähe eher ein Opfer von Plünderung als Nutznießer von Rettungswilligen.

Ein organisiertes Rettungswesen entstand erst durch den Zusammenschluss verschiedener Rettungsstationen und -vereine zur „Deutschen Gesellschaft zur Rettung Schiffbrüchiger“. Die ersten Rettungsmittel, die für viele Jahre zum Einsatz kamen, waren Ruderrettungsboote. Von Pferden auf

Fuhrwerken ins Wasser bewegt, eilten sie nach dem Aufschwimmen mit reiner Muskelkraft den in Not Geratenen zur Hilfe.

Später kamen Raketenapparat und Hosenboje als sinnvolle Hilfen hinzu, um Schiffbrüchige von sinkenden Havaristen zu retten, bevor auch Brennkraftmaschinen die harte, körperliche Arbeit auf An- und Abfahrt ersetzen konnte. Doch erst nach 1945 war die Zeit reif für einen Seenotrettungskreuzer (SRK) moderner Prägung.

Die Schiffe „Bremen“ und „Hermann Apelt“ lieferten die Erkenntnisse und Erfahrungen für den Serienbau eines in jeder Hinsicht auf Einsatzzweck, Anforderungen und Bedienbarkeit abgestimmten Rettungsmittels in Form eines Spezialboots: Der moderne Seenotkreuzer war geboren.

Ihre Geschichte, Entwicklung und Zusammenstellung der fast 50, bis heute gebauten Einheiten, sortiert nach Klassen und Reihenfolge, ist das zentrale Thema von Hans Karr in diesem Buch. Um dies verständlich zu machen und einsortieren zu können, stellt er die erwähnte Einführung in die Entstehungs- und Entwicklungsgeschichte des Seenotrettungsdienstes voran.

Die verschiedenen Schiffsklassen, jeweils bezeichnet nach ihrer Länge und auch mit dem Namen des jeweils erstgebauten Schiffs versehen, folgen der aus der Typenkompass-Reihe bekannten Form: etwa eine Seite pro einzelnes Schiff samt Fotos (meist mehr als nur eines).

Vorgestellt werden technische Details, Einsatzzweck und häufig auch Beweggründe für die Entwicklung einer Klasse, sowie jeweils Bau, Indienststellung und Einsatzgeschichte jedes einzelnen Schiffs. Am Ende des Buches werden auch die Rettungsboote (bis zu 10 m Länge) zusammenfassend beschrieben und mit Bildern gezeigt.

Eine Tabelle am Anfang jedes neuen Kapitels fasst auch hier wieder die wichtigsten Kenndaten klassenweise zusammen: Klassenname, -bezeichnung, Wasserverdrängung, Länge, Breite, Tiefgang, Stärke der Besatzung, Antrieb samt dessen Leistung, Geschwindigkeit und Angaben zur Ausrüstung sowie ggf. besondere Anmerkungen.

Bedingt durch sein Auflagedatum ist dieser überarbeitete Band (beinahe) auf aktuellstem Stand: Das Neueste aus dem Jahr 2017 fehlt ebenso wenig wie Angaben zur ab 2015 in Dienst gestellten neuen 28-m-Klasse von Seenotkreuzern.

Zum Zeitpunkt der Drucklegung waren drei von fünf geplanten Schiffen bereits im Dienst, die „Hamburg“ (SK 40) im Bau und ein fünftes noch ohne Namen (SK 41) beauftragt. Es ist inzwischen fertiggestellt und traf am 31. Januar 2021 auf seiner Station in Grömitz ein.

Es soll dort den Seenotkreuzer Hans Hackmack ersetzen. Sein Name wird, wie es eigentlich Tradition ist, erst zur Taufe bekannt gegeben. Und was bei Erscheinen des Titels noch niemand ahnen konnte, ist der Auftrag zum Bau eines sechsten Kreuzers (SK 42), der am 12. März 2020 bei der Fassmer-Werft auf Kiel gelegt wurde.

Er soll dieses Jahr in Dienst gehen und den Namen Nis Sanders erhalten. Das zeigt, wie die Geschichte hier fortgeschrieben wird und immer wieder Stoff für neue und wieder erweiterte Auflagen bietet. Nicht nur Freunde der ansprechend gestalteten Boote werden daran ihre Freude haben.

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Large series new products 2021 for Z gauge New, even without a Show in Nuremberg

The Nuremberg International Toy Fair 2021 will not take place until the summer, unless the still ongoing pandemic also throws a spanner in the works of these plans. While there will be a change in the board of directors at the operating company due to age, model railways may be on the verge of leaving for good. Who has been there, at least, so far, is listed by us this time in a familiar place.

Who would have thought it a year ago! The International Toy Fair with over 50 years of tradition in Nuremberg cannot take place at the scheduled time. A year ago, the epidemic was already hovering over the visitors like a sword of Damocles, but still seemed to be a purely Far Eastern problem.

Twelve months later, we know how much the waves of disease are limiting people worldwide, and so new solutions had to be found for many things. Now the International Toy Fair has been postponed, and the model train manufacturers will probably not participate, at least the majority of them, according to our expectations. This will certainly determine the timing and manner of announcements for years to come.



Even without a winter International Toy Fair, many appealing new products have been announced, among them the crimson class 212 (item no. 88214) together with its matching track construction train (87761) on the part of Märklin (for a description of new products, see the following section). Photo: Märklin

Not all manufacturers have already published their new products at the usual time; some are still working off backlogs from the shutdown of economic activities. However, we have collected the new products that have been announced and present them in two separate reports in the usual form.

+++ Artitec +++

Artitec has published a new product brochure that omits Z gauge. On request, we have been confirmed that announcements for our gauge will follow with a time delay. As many may have noticed, there are still some backlogs from last year.

Many of these are due to the special situation of the pandemic consequences. The focus is currently on working through these backlogs, which is why it does not make sense to publish further new products at this time.

Artitec has explicitly stated to us that they want to shorten the time window between announcement and delivery. This sometimes give the impression that no new products are planned, even though production of such new products has already begun.

The fact that new products were announced, at all, is due to the expectations of the trade press for their special issues and reports on spring new products. We will provide our readers with the requested information later.

www.artitec.nl

+++ Beli-Beco +++

As we know, this lamp specialist does not officially carry any Z gauge models in its range. And yet his handcrafted models are unsurpassed in filigree. What is designed here for the scale 1:160 proves time and time again to be so fine that it can be used in the nominal size Z without any problems, and no one will notice.

This also applies to a new platform light (item no. 155501), which, with a height of 38 mm in Z gauge, reproduces an 8.36 m prototype – a typical dimension, we think. The shade, which is mounted crosswise at the top, is illuminated by two SMD light-emitting diodes with 3 V operating voltage. Like all other lamps, this model is also delivered with series resistor and protective diode for connection to 16 - 19 volts AC or DC voltage.

www.beli-beco.de

+++ Busch +++

The range of new products suitable for Z gauge remains manageable at Busch this year. But we have found a few little things here as well: The tree pack "Reforestation" (item no. 6392) contains 15 young spruces with heights of 20 - 25 mm and ten tree roots.

It can be combined perfectly with the other spruces from the Busch assortment, and can also recreate a forest area that is being reforested on a scale of 1:220. The trees are then just a few years older. The "Mini-Set Spruce Cones" (7759) contains a total of 230 specimens of these fruit cones to stick to the tree material.

The "Lightning Spruce" (5431) promises attention and excitement at the same time. With its 160 mm height, converted to Z gauge, it has a prototypical height of 35,20 m. Connected to 14 - 16 V DC or AC



The new platform light from Beli-Beco for nominal size N (item no. 155501) is also a very good choice for Z gauge. Photo: Beli-Beco.



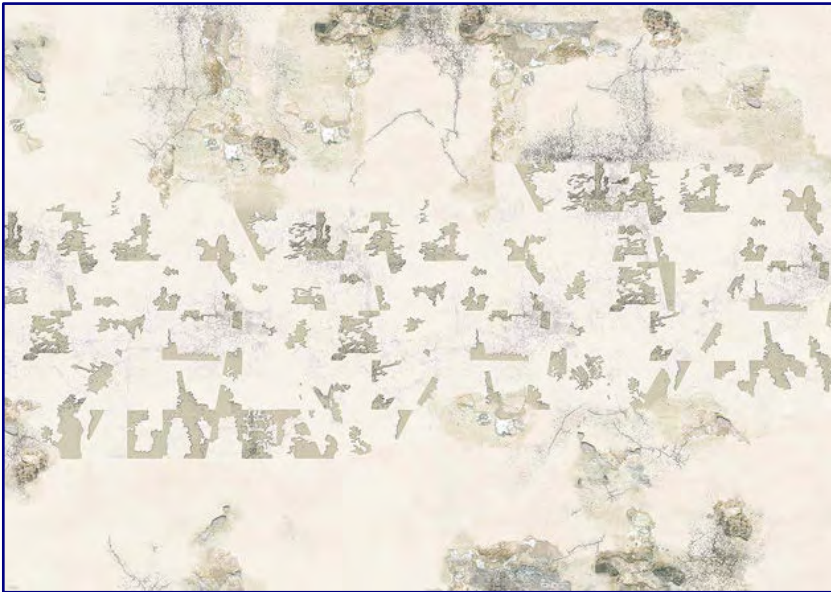
The lightning spruce (item no. 5431) has, like most of the trees from the industrial programme, a height that perfectly matches the Z gauge. So, it can become a successful eye-catcher even in small scale. Photo: Busch

voltage, LEDs integrated in the tall trunk simulate a realistic glowing fire effect after a lightning strike. With cotton wool in the treetop, the rising smoke is reproduced.

On the product photos, the 48 x 25 cm “Rape Field” (9771), which can be worked with scissors or a knife, looks a bit garish to us. On the photos shown so far, we miss the green part of the plant stalks, which is quite noticeable in the prototype. Interested parties will only be able to get a decisive impression of this when this new product is delivered.

The new wall and decorative panels in the “matt look” are the only ones intended for the Z scale from the factory. According to Busch, they have a particularly realistic surface, almost like patinated.

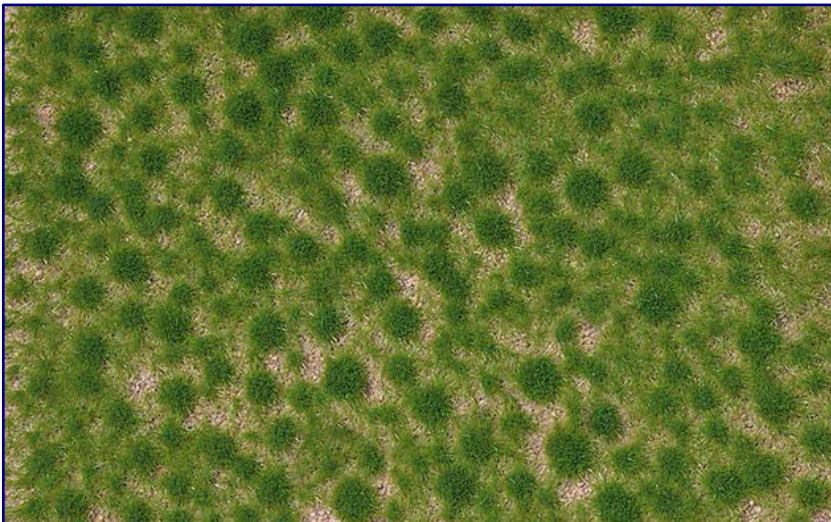
Therefore, they can be used directly and achieve a special effect in terms of look and texture. The decorative panels in 21 x 14.8 cm format made of printed cardboard are intended for use in the design of walls, buildings or house walls.



However, we consider at most two of the four new plates suitable for our purposes, as the brick replicas of the others cannot cover the gauges from H0 to Z in a scale approximation: weathered plaster (7436) and weathered wall (7437).

By the way, new hand-made natural pure deciduous trees with particularly realistic foliage appear to be very interesting.

It will be worthwhile to see whether smaller specimens with finer leaves will be added to the range in the next few years, which could then also be used for Z gauge.



The new grass tufts of the Natur Pur series can basically be used in all gauges, can be combined with each other in any way due to their harmoniously coordinated colours, and which still have their limits when it comes to credible use due to their length and their effect on the layout.

We count 16 different packs with grass fibre lengths of 2, 4 and 6 mm. Each of them contains up to 100 tufts of grass, which are applied to a 13.8 x 8.8 cm carrier foil and are taken off and stuck on individually.

Provided that the prints suit our scale sufficiently, the decorative plates "Weathered Wall" (7437) and "Weathered Plaster" (7436; photo above) could become interesting for scratch-built projects. The grass tuft mats, in the photo below "Spring" (3511),, as well as, the other vegetation variants definitely help in designing. Photos: Busch

We do not use the products with 6 mm fibre length, because this would correspond to grass with a growth height of more than 1.30 m. We also offer the products with 2 mm fibre length.

The following products are available: "Spring" (3511), "Late summer" (3513) and "Autumn" (3514) with a height of 2 mm and "Spring" (3516), "Late summer" (3518), "Autumn" (3519), "Spring, bicoloured" (3531), "Late summer, bicoloured" (3533), "Tufts of weeds, spring" (3541), "Late summer" (3543), "Tufts of flowers, spring" (3546) and "Late summer" (3548). Further new products will be announced for summer 2021.

www.busch-model.com

+++ Deluxe Materials +++

At this supplier from the United Kingdom we found a UV adhesive as a new product, which is offered under the product name "Roket UV," and is supposed to cure within one to three seconds. It is offered in containers of 5 and 20 grams.

Unfortunately, the pages do not provide any information about the composition: pure UV adhesives have not yet been able to establish themselves in model making due to the lack of transparency of the parts to be glued, as they often could not develop sufficient adhesive strength.

However, as it is listed under cyanoacrylate adhesives on the supplier's website, it is probably a mixture with superglue that has eliminated this problem. The new products can only be purchased directly from the manufacturer within the United Kingdom; shipping outside the country's borders is not permitted.

www.deluxematerials.com

+++ Donau Elektronik +++

This supplier, which is a subcontractor for retailers, also lists some interesting tools and aids in its new products brochure that will be of interest to our readers.

The first is a high-quality micro-open-end spanner (wrench) set (art. no. 980-SET) with ten V2A open-end spanners (wrenches) between 1 mm and 4 mm width across flats. The gradation between the individual sizes runs in alternating steps of 0.3 and 0.2 mm up to a width of 2 mm, and from then on in 0.5 mm steps to 4 mm, plus 3.2 mm. The tools are packed in a practical storage box.



The high-quality micro-open-end spanner (wrench) set (art. no. 980-SET) provides good service for do-it-yourself projects. Photo: Donau Elektronik

A six-piece hook set (RM16) offers a total of nine different shapes for scoring and modelling in model making as well as electronics (hooks on one or both sides of the handle).

The 140 mm long tools made of chrome vanadium steel are supplied in a bag for storage.

There is a clever idea behind the giant cutting mat size A1 (MS-A1): The self-healing tool with centimetre grid offers various, printed angles and shapes, but also shows length measurements in typical scales down to 1:220.

In addition, thanks to TPU coating on the back, it is slip-resistant and foldable, which makes it considerably easier to transport!

There are three new products in the field of stranded wires for various applications: Plastic-insulated copper stranded wire with a cross-section of 0.14 mm² will be available in eight 5-metre coils (119-885) in the colours red, blue, grey, black, yellow, green, brown and white.

Twin wires with 0.25 mm² cross-section in a very flexible design, 5 and 50 m rings are offered in the colour combinations red-black (225-01 & 225-010), red-blue (225-02 & 225-020), red-brown (225-08 & 225-080) and oiled-brown (225-38 & 225-380).

The copper strand RGB LEDs are four-core, also insulated with plastic, flat and flexible. They show a helpful colour combination of black-red-green-blue for connecting RGB LED strips, which can be helpful in the model building sector for adjustable layout lighting, for example. With a cross-section of 0.25 mm², 5-metre (419-005) and 10-metre (419-010) rings are available here, supplemented by 10 metres in a ring with a cross-section of 50 mm² (450-010).

The 23-piece "Lighting connection set" (755-SET), consisting of ten dwarf plugs each with cross hole and screw connection at 2.6 mm diameter (yellow and brown), one 10-metre ring each of stranded copper wire with 0.14 mm² cross-section (yellow and brown), as well as a matching distribution strip with ten connections, and 36 cm supply cable including dwarf plugs at the end, offers a useful combination.

shopware.donau-elektronik.de

+++ ESU electronic solutions ulm +++

The quadruple magnetic article decoder Switchpilot 3 (art. no. 51830) is intended for stationary use on the layout, is multi-protocol capable and can switch double coil point machines, light signals, magnetic uncouplers, incandescent lamps or other stationary consumers.

For easy configuration, it has a new operating concept consisting of a four-line, illuminated OLED display and three input buttons. As an eight-fold decoder it is called Switchpilot 3 Plus (51831), but a Switchpilot 3 Servo for controlling up to eight RC servo motors is also offered.

www.esu.eu

+++ Faller +++

The manufacturer from Gütenbach in the Black Forest has become a synonym for building models with its company name. In 2021, it will celebrate its 75th anniversary and therefore has new products for our gauge in its quiver for the first half of the year.



These summer dressed passers-by (art. no. 158052) belong to the spring new products in the Faller anniversary year. Photo: Faller

The main new product for the scale 1:220 is the Oberneulander Mühle (art. no. 282789), a Dutchman windmill with an octagonal ground plan and a surrounding gallery. Its prototype is a listed building in the Bremen Oberneuland district.



The main new product for the Z gauge is the hard cardboard kit for the Oberneulander Windmill (282789), which has its prototype in Bremen. Although the model looks appealing overall, we are not impressed by the pointed cap of the prototype with its clumsy and clunky shape. Product photos: Faller

The hard cardboard laser-cut model can also be powered with the Faller gear motor (180722), which can be purchased separately. Unfortunately, this can also be seen in this model, because especially compared to the original, the mill cap (bonnet) does not look very convincing in its shape and proportions. It seems as if this part of the building was “constructed around” the given windmill.

Of course, this windmill can be used from epoch I onwards. With its extension it requires a base area of 112 x 80 mm and a height of 132 mm. With the sails (blades) the width increases to 100 mm and the height to 157 mm. This new product will be available from May 2021.

A six-piece figure set “Passengers” (158052) has been announced as a second new product for specific gauges. Further new products are for landscaping, and can therefore be used independent of the construction scale.



A new 2C model water (171656) is intended to provide deceptively real results when designing water surfaces. Clear casting resin and hardener component can be dosed, mixed and stirred in just a few steps.

The finished compound can be applied to almost all substrates, including foam PS panels. With stirred-in dry pigments, the water can also be coloured, immediately.

Faller promises clear wiring under the layout surface with the brown distribution plate, (180807). The small current distribution plate offers 2 x 10 sockets and is supplied with ten plugs (2.5 mm). Alternatively, an identical product is available in the colours red (180801), yellow (180802), blue (180803), green (180804) and black (180805).

At <https://faller-create.de/>, many functions have been expanded in recent months. The supplier therefore also points out that it is now possible to build freely and with millimetre accuracy even in scales smaller than H0. Even if Z scale was not explicitly named, it might be worth taking a look at this offer for individualists.

A new 2C model water complements the Faller range. Photo: Faller

www.faller.de

+++ Heki +++

New trees appear in the Super Artline series that are also suitable for Z scale. With heights of 4 cm, the five deciduous trees (art. no. 19130) rather reproduce young specimens on a shelter, while the five apple (19131) or plum trees (19132) already have a normal growth height. But, also, the two hornbeams (19113) remain within the scale range with a height of 9 cm.



The five apple trees (art. no. 19131) are not too big for the scale of 1:220. A height of just under nine metres should not be too generous for fully grown fruit trees. Photo: Heki



What was written before also applies to the five plum trees (19132), which are based on the same tree blanks, but flocked in different colours. Photo: Heki

In the Realistic model tree series, the four birch trees (19120) at 11 cm seem just about usable, as do the same number of poplars (19110) at 13 cm, because this tall and slender tree is depicted far too small in almost all gauges.

www.heki-kittler.de

+++ Herpa +++

In the field of 1:200 scale aircraft models, new moulds for two Russian military prototypes were announced: the Tupolev TU-22M3 strategic bomber and its modernised version TU-22M3M.

The supersonic aircraft, built between 1967 and 1997, is equipped with pivoting wings like its prototype.

Of course, this only makes sense if it does not always have to travel with its landing gear extended. The landing gear is therefore also designed to be removable.

There are also two new designs for the simplified Snapfit models in flight presentation. In both cases, the European aircraft brand Airbus is being considered: from about May/June, the first models of the Airbus A220 can be expected, and for the third quarter of 2021, the Airbus A330-700, better known as the wide-bodied transport aircraft "Beluga XL".



The "Ed Force One" is reproduced in the form of a Boeing 757-200 by Herpa in the Snap-Fit series, seen here as the "Somewhere Back in Time World Tour 2008" version (Item No. 613 255). Photo: Herpa



The strategic long-range bomber Tupolev TU-22M3 also gets swivelling wings and removable landing gear as a model to be able to display it as if it is in flight. Photos: Herpa

In addition, an expansion of the diorama accessories was also announced for this year. However, we are now looking at the next new products, which have already been announced with concrete models, but limited to machines with a maximum model length of 30 cm and usability in Europe.

Fans of "Iron Maiden" will be especially pleased, because their Astraeus-Boeing 757-200 "Ed Force One" will appear as a Snapfit model in the designs "Somewhere Back in Time World Tour 2008" (art. no. 613 255) and "The Final Frontier World Tour 2011" (613262).

In the sports sector, Eintracht Frankfurt supporters can look forward to their "SGE-Express" from Sun Express. The Boeing 737-800 also appears as a Snapfit model (613200). All other new products are regular Wings models with represented landing gear.

The Alrosa Tupolev TU-154M (571388) commemorates the last commercial flight of the prototype, the CSA Czechoslovak Airlines Ilyushin IL-18 (571333) is another aircraft of former Soviet production. The Boeing 707-400 of Israel's El Al (571432), on the other hand, established the era of jet aircraft in the West.



Israel's El Al Boeing 707-400 (571432; photo above), Douglas DC-4 (571357; photo below left) and British Airways Helicopters' Boeing 234 "Chinook" (571418; photo below right) are other new products scheduled to appear by mid-2021. Photos: Herpa

Much smaller are the Iberia Lockheed L-1049G Super Constellation (571395), and the Swiss Air Lines Douglas DC-4 (571357) from the last heyday of piston engine propulsion. The "banana helicopter" Boeing 234 "Chinook" (571418) is flying for British Airways Helicopters.

Concluding this line-up are two military aircraft, the Lockheed Martin F-35A "Lightning II" of the Italian Air Force (571371), and the Russian Sukhoi SU-27SM Demonstrator.

www.herpa.de

+++ Kuehn +++

This manufacturer states on its website that due to the postponed International Spielwarenmesse (International Toy Fair) and currently closed shops, new products will not be presented until later in the year. Those interested in the digital solutions from this company should therefore continually follow the entries on the company website.

www.kuehn-digital.de

+++ Lux-Modellbau +++

The long-awaited Lux Staubhexe (Dust-witch) is finally scheduled for delivery in the 3rd quarter of 2021, according to information now published.

www.lux-modellbau.de

+++ Märklin +++

We would like to certify that Märklin is the market leader with a very colourful programme of new products geared to a wide range of interests. Even if the first impression is that the Göppingen-based company is rather restrained with new moulds, it is neither boring, nor unattractive, what they have put together.

Consequently, we have heard words of praise throughout the series so far, which more than confirm our impressions. It also has to be taken into account that a big anniversary (50 years of Z-gauge) is coming up next year, and, that as a result of the factory closures in Győr and Göppingen, as well as failures in the supply chains in spring 2020, delivery dates have changed significantly: Backlogs could, at best, only be partially worked off through the present.



Some information regarding the product equipment of the double diesel locomotive V 188 001 a/b has been added since it was announced as the Insider Annual Model 2021. The order deadline for this model was extended to 31 March 2021 shortly before the editorial deadline. Photo: Märklin

And so, we are pleased to have a well-balanced compilation, which we would like to present in more detail below. Let's start with this year's dealer's gift (art. no. 80131), which is otherwise known as the "Messewagen" ("trade fair wagon").

Märklin uses it to honour this year's anniversary "50 years of Intercity", which is also printed on the roof of the model. A Donnerbüchse had to be used for this, which was painted with the long-distance livery of the Deutsche Bahn AG and its logo.

The next step will be this year's models for the Insider Club members: Known since November 2020 was the new-form double diesel locomotive V 188 001 a/b of the Deutsche Bundesbahn (88150), about which we could still clarify some details.

For example, it is correct that this model will have a change between warm white LED headlights and red tail lights depending on the direction of travel. Furthermore, the rods that firmly couple both halves are designed to be pluggable. As delivered, the model runs through the radius R 1 (145 mm), but can also be coupled more closely, thanks to two additional coupling rods that are enclosed. One of these exchange parts is for R 2 (195 mm), the other for prototypical display in the display case.

A privately used class Uc powder silo car was announced as the annual car (80331), which belongs to the former chemical company Hoechst AG from Frankfurt (Main) in its operating condition around 1994. It is painted dark grey and bears the name and logo of its owner on the left container.

So, it's fitting that we're on the subject of MHI special models. The first edition for this dealer initiative in 2021 will be Railroad Maintenance Car Set (87761) for track construction. It contains two ocean-blue painted conversion three-axle cars as a living/workshop car (conversion from BD3yg) and a lounge car (conversion from B3yg). Both models are not identical with the wagons of the earlier pack 8776 from 1991.

This set is completed by a chrome oxide green equipment wagon (former G 10), an open freight wagon Klm 441 with ten rails as a load and an open freight wagon E 037, which has a ballast insert.



The crane with protection and low side wagons (86572) optionally extends the track construction train seen on page 27. Photo: Märklin

This set can be extended with a supplement (86572) consisting of a Krupp Ardelt crane together with a protection wagon and a low side wagon (ex X 05), which is equipped with a load "tools" made of resin. All three wagons are painted chrome oxide green and belong to the Deutsche Bundesbahn Railroad Maintenance Car train (Era IV).

The matching locomotive for the complete train is a class 212 crimson diesel locomotive in the familiar design, which means that this extremely popular locomotive is being offered individually for the first time in its original colour with lettering for Era IV.

Another wagon pack is intended for Era III and is recommended as matching the V 188: We are talking about three old construction tank cars (82326) with attractive designs of the companies "Hobum" and "F. Thörl," which captivate with their blue paint. More inconspicuous is the car of the lessor Eva with its old logo; however, it is the one that once belonged to the daily scene.



The three old construction tank cars bring (82326), in addition to two attractive and picture-covered colour variants, also, finally, an early Eva version. Photo: Märklin

Besides the diesel locomotive for the club members, there is only one other mould new product this year: This is the sliding roof/sliding wall car of the type Tbes-t-68 of the Deutsche Bundesbahn, which can fall back on an existing running gear. It shows the prototypical details, which also include attached platforms on the front sides.

It is offered as a set of two (82153) in operating condition around 1963 (epoch III) and as a set of four as Tbis 871 (82155) with touch-up patches for the eighties (epoch IV).



In the eighties, the Tbis 871 is on the road as a four-pack with touch-up spots (82155). For Era III, the new sliding wall car comes in parallel as Tbes-t-68 in a pack of two (82153) with reproductions of unlacquered metal doors. Photo: Märklin

The bronze investment casting edition for collectors continues with two new products. One is the small cattle car Vh 14 (86606) with brake house; the second is a diesel locomotive V 200 (88207) with window inserts.

For the locomotive, the housing fastening had to be changed, as the metal cannot be spread. It is packed in a wooden box.

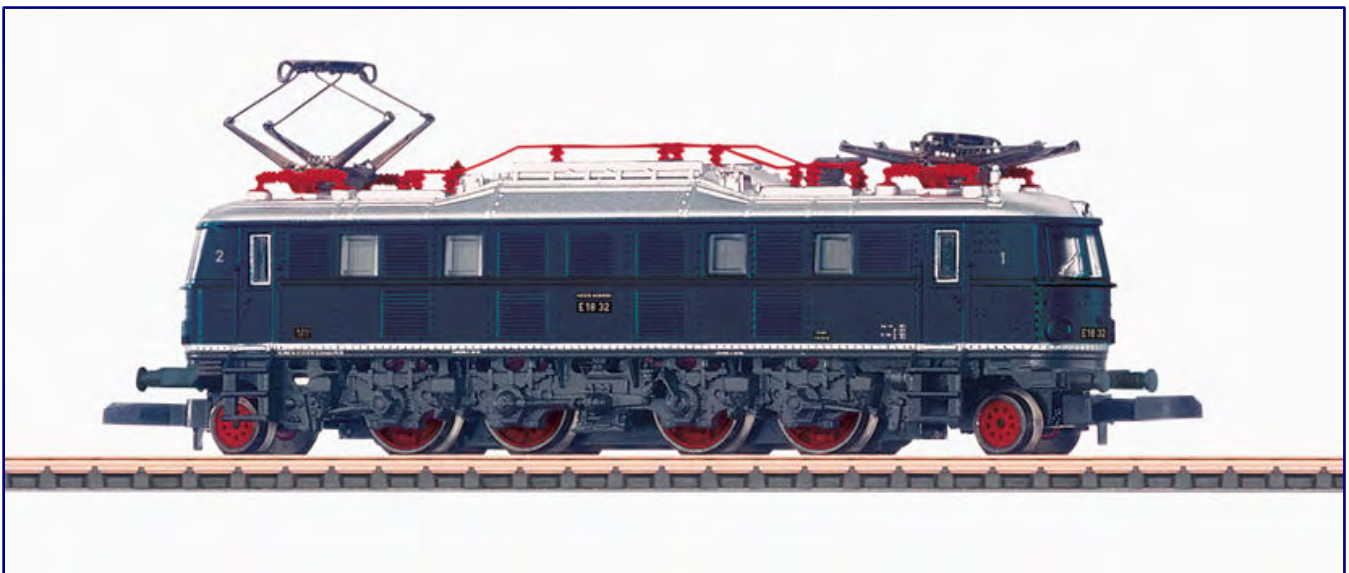
Fans of Era II will also be very happy this year.

The shapely electric locomotive of the class E 18 (88083) will receive a product update, and will be converted to have a bell-shaped armature drive and warm white LEDs. Modified in this way, it has been running for the Deutsche Reichsbahn since 1937.



With the class V 2000 (88207) we continue the bronze investment casting edition for collectors. Photo: Märklin

But the German Federal Railways can also rejoice here, because a version with the latest technical standards is also being released for them: In Era III, it has steel blue paint and a light-coloured decorative stripe between the frame and the superstructure (88088).



The shapely E 18 appears in no less than two versions after its product update: Deutsche Reichsbahn from 1937 (88083; photo above), and in the never-before-offered standard steel blue livery of the Bundesbahn (88088; photo below). Photos: Märklin

Thus, after almost twenty years, the E 18 finally appears in the design which everyone probably remembers, and how it, consequently, belongs on an electrified model railway line!

But let us return to the pre-war period. The important locomotive new product is complemented there by a five-piece apron car set (87352), which was already offered in a similar form: one AB4ü (1st/2nd class), two C4ü (3rd class), and one ABC4ü (1st/2nd/3rd class), supplemented by a Mitropa dining car WR4üe. All coaches have close couplers.

And also the freight traffic of epoch II was thought of, because here three hinged cover cars K Wuppertal (86010) for 15 t load weight each with brakeman's cab and spoke wheels roll forward. The open freight wagons O 10 (82327) appearing at the same time are packed in pairs, bear signs of operating use, and bring a three-wheel box wagon Tempo A400, as an extra.



The small theme pack "Coal Loading" (82337) consists of a loaded freight car O 10 and a Fuchs 300 excavator with a high cab. Photo: Märklin

The latter type of wagon will also make it back into Era III this year, but there individually as part of a "Coal Loading" themed pack (82337).

Here, it no longer has a brakeman's cab, but an open platform, but it does have a coal load insert. It is completed by a Fuchs 300 excavator with a high driver's cab and coal shovel.

We also consider the individually available Donnerbüchsen for Era III based on the DB model to be a good idea.

Although there have been many wagons of this type in recent years, a real gap has now been closed. The wagons were in use in the first half of the fifties, therefore, painted in the

dark bottle green again, but do not bear the DB logo, which was only introduced in 1955.

This period was also before the class reform, which is why only the 3rd (main class C) or 2nd and 3rd class (BC) is written on the models.

With different road numbers are offered here: a BCi (87501), two different Ci (87511 & 87512) and a pack wagon Pwi (87521). The lower case i indicates wagons with open platforms.

The class 64 tank steam locomotive (88744) with likewise early Era III markings or the V 36 108 diesel locomotive from the museum locomotive series (88772), which comes in a wooden box, are a good match.

Märklin remains true to its latest line on another point: when the moulds are ready for production, several variants are produced from them at once in order to be able to work more economically. This also provides fans of the German Reichsbahn with interesting models in Era IV.



Donnerbüchsen appear for the late DR and the early DB, here two Bundesbahn examples before class reform, still without logos (87501; photo above / 87512; photo below). Photos: Märklin

The Donnerbüchsen also are bottle green with the distinctive DR text logo and computer numbers. The four car set (87513) consists of a Daai baggage car and three 2nd class Baai through cars. The class 37 steam locomotive with tender appears at the same time as the matching locomotive (88032).



The "Steppenferd" last bore the class designation 37 at the Deutsche Reichsbahn of the DDR. A Märklin new product (88032) for the DR diesel coaches now appears in this design. Photo: Märklin

It was known as class 24 before the renumbering, its actual prototype was later sold to the West to procure foreign currency. The model is from the revised design without the standing boiler dent, which was once taken from the H0 model that was the inspiration in the seventies.

As all good things are known to come in threes, a goods train set (82268) is not missing for the Eastern Reichsbahner, consisting of a trio of two covered wagons Gbkl (ex-Dresden) and a low side wagon Klms [3430].



The V 36 108 (88772) continues the series of museum locomotives and is therefore packed in a wooden box. Photo: Märklin

The special feature of this Era IV set is the yellow painted sliding door of one of the wagons, which is also marked with danger and hazardous materials information: Tetra-ethyl lead, which was once added to petrol to make it anti-knock, was transported in it.

Let's stay in Era IV for a short while, but switch back to the DB, because here we are still missing three important new products: Surprisingly quickly, an oil-fired multi-purpose steam locomotive of class 042 (88276) appears in the normal programme.

With the road number 042 096-8 indicated that it comes from the Bw Rheine, which in this case still belonged to the (soon to be dissolved) BD Münster. Otherwise, it is identical in every respect to the Insider Model 2020.

A missing variant of the V 2000 was this locomotive in its original purple colour scheme with computer number (and corresponding Ege biscuit on the long sides). It now appears as 220 085-5 (88206) from the Krauss-Maffei lot in the condition of the early seventies.



Surprisingly quickly, the Insider locomotive 2020 (not yet delivered) also appears in the normal programme, namely for Era IV as 042 096-8 of Bw Rheine (88276; photo above). The class 220 in purple also closes a gap in variants for this class (88206; photo below), and we hope that the shape of the more pointed decorative lines of a KM locomotive on the front will be correctly implemented this time. Photos: Märklin

As promised, the “German Wine Route” theme continues with a train set (81306) that expands on last year’s pair of cars: two more pairs of WG3yge/WG3yge cars, and a matching class 2184 mainline diesel locomotive in ocean blue/ivory extend the train, which was already begun.

We hope it will be well received by the Zetties, and will then be completed in the anniversary year 2022 by new bogie express coaches with double end doors, which also travelled in this special train and represent a large gap in the 1:220 scale programme.

Slowly, we are now getting a bit more international, as the DDm 915 auto transport wagon for passenger trains is not returning to the range in DB design. Unloaded and single it comes in chrome oxide green (87094), which means that it was already part of the range except for the short coupling that is now installed.

International trains can also be formed with the car transport wagon pack (87095) based on the ÖBB model. These two examples of the DDm in pure orange and with the ÖBB flag on the pillars (Era IV) also have close couplings and were used in international traffic.

The sliding tarpaulin wagons of the type Shimmns(-tu) 718 of DB Cargo are also being reissued. Individually for Era V (86357) with slight signs of ageing or as a duo for Era VI (86356), they show different designs, whereby, especially the two examples from the double pack, with and without green advertising banner, stand out clearly from each other.



The eye-catching DDM car transport wagons in pure orange (87095) could also be seen outside Austria when they crossed the border into Germany on international auto transport trains. Photo: Märklin

Also of interest, is a container car pack (82665) from the transitional period from the Bundesbahn to the Deutsche Bahn AG. The six wagons are of the types Sgs 693 (two) and Lgjs 598 (four).

They are loaded with various 20- and 40-foot containers, as well as Hoyer tank containers, which are no longer available.

The orange Hapag-Lloyd containers and the grey examples still labelled "Maersk Sealand" are certainly striking splashes of colour.

A DB-TFG container in pebble grey with a strawberry-red longitudinal stripe seems to be a bit out of the ordinary, as it was certainly a residual wagon, about to be taken out of service, and had not been redesigned at that time.



This sliding tarpaulin car Shimmns(-tu) 718 from a set of two (86356) serves the area of the modern Deutsche Bahn AG. Photo: Märklin

A suitable train locomotive for the presented freight wagons is the extremely popular "Ludmilla" class 232 in bordeaux-red original livery (of the DR) and markings of DB Cargo (88136), as it was often seen in the mid-nineties.



In its original livery, but already sporting the DB AG Dürr biscuit, class 232 (88136) now appears as a suitable diesel locomotive for DB Cargo's modern freight service. Photo: Märklin

There is also the class 285 (88378) from the TRAXX family, a modern diesel locomotive in Press blue, and the grey "Messelok" V 270.09 (ex class 221), a Bundesbahn classic that has found a new home at Schienen-Güter-Logistik GmbH (SGL), stylishly packaged in a wooden box (88205).



Things are also becoming more international in the programme and on model, railway tracks with the three SNCF sliding tarpaulin wagons (82427). Photo: Märklin

The three SNCF type Rils sliding tarpaulin cars (82427) stand out as popular models for Era V international freight traffic. These European standard wagons carry colourfully designed advertising for three French mineral water brands.

However, those who value colour and want to be guaranteed that their trains will not escape the notice of onlookers on the layout will not be able to avoid another set of five. PKP Cargo's Falns self-unloading wagons (86311) can be used to form block trains that travel with coal traffic between the Silesian coalfield and Germany, including the Ruhr area.

The special thing about these wagons is that queues of wagons formed from a single type appear much longer to onlookers than they are. In this particular case, the empty wagons running back east with close couplings, benefit from the fact that their yellow-blue livery makes them the most striking reproductions of this type of wagon.



The PKP self-unloading wagons (86311; 5 examples) are certainly the most colourful representatives of their type and are therefore guaranteed to stand out on any layout. Their prototypes can be seen frequently in Germany.

Last but not least, the fans of the SBB and ÖBB get their money's worth. The SBB's Re 420 electric locomotive from the LION modernisation project (88595) can be seen today mainly in Zurich S-Bahn traffic, but can also be used with many other rolling stock from the Märklin range. The features of its visible modifications on the long sides are reproduced by pad printing.

Märklin has added a private Hbbins sliding wall car from the Swiss paper paint company Saber AG (82385) with blue advertising for Tela-Kimberly on a white car background as a new product. It is modelled on its prototype from 2012.

The three four-axle sliding wall wagons of the type Habbiillnss (17) from an Austrian-Swiss wagon pack (82418) are undoubtedly travelling internationally. One each belongs to SBB, Transwaggon Zug/Switzerland, and Rail Cargo Austria. This type of wagon is the first colour variant of the new design that was previously sent on its journey for the Swiss postal service.



One of the new accessories is the MCI-43 makeshift personnel carrier (89012), which earns its living as a shed or material store. Photo: Märklin

For the rolling stock we are now only missing the Easter wagon 2021 (80421), which should be delivered quite soon. The chosen car is a light blue R 10 stake car with an Easter-like decoration and a load of three colourful, laser-cut Easter eggs. It is packed in a transparent plastic Easter egg for hanging.

The spring new products are rounded off by an architectural kit (89012), which is based on a makeshift MCI-43 passenger car that has been stripped of its running gear and is used as a shed or material store, and an individually packaged vehicle set consisting of eight different coloured Tempo tricycles with box or flatbed (from new construction). This set is declared as “display” (89024) and can be used in many eras.

www.maerklin.de

+++ Microrama Miniature +++

Microrama has launched its own grassing machine, the “bFlock 50” model, which is now available in various cases, graded according to the desired range of equipment. The combinations differ according to the number of enclosed sieves and attachments for different uses.



The Trainini owned Electrostat "bFlock 50" was delivered in a practical case with different equipment, here the maximum configuration. Photo: Microrama

Compared to the pre-series, some changes have been made to the internal test inserts to improve its handling: The switch has been moved 8 mm lower, the grounding (earth) cable has been extended to 1.25 m and made thicker, but, at the same time, more flexible.

The material for the battery cover has been changed and now proves to be more durable. Further changes have been made to the electronics, which make the unit more powerful, and at the same time easier on the battery.

www.microrama.eu
www.microrama.online

+++ MKB-Modelle +++

Unfortunately, we have not yet found any new products in scale 1:220, but we think it is worth mentioning various building boards for the Z gauge made of 0.2 mm thin, coloured photo paper. They are very well suited for creative scratch building, and, therefore, allow individual buildings in modern construction.

Various roof coverings made of tar paper and shingles, wooden planks and slats as well as brick masonry in two colours are offered.

www.mkb-modelle.de

+++ Modellbaukompass +++

According to the website, their distribution has been closed since 1 January 2021. We do not have any exact information yet.

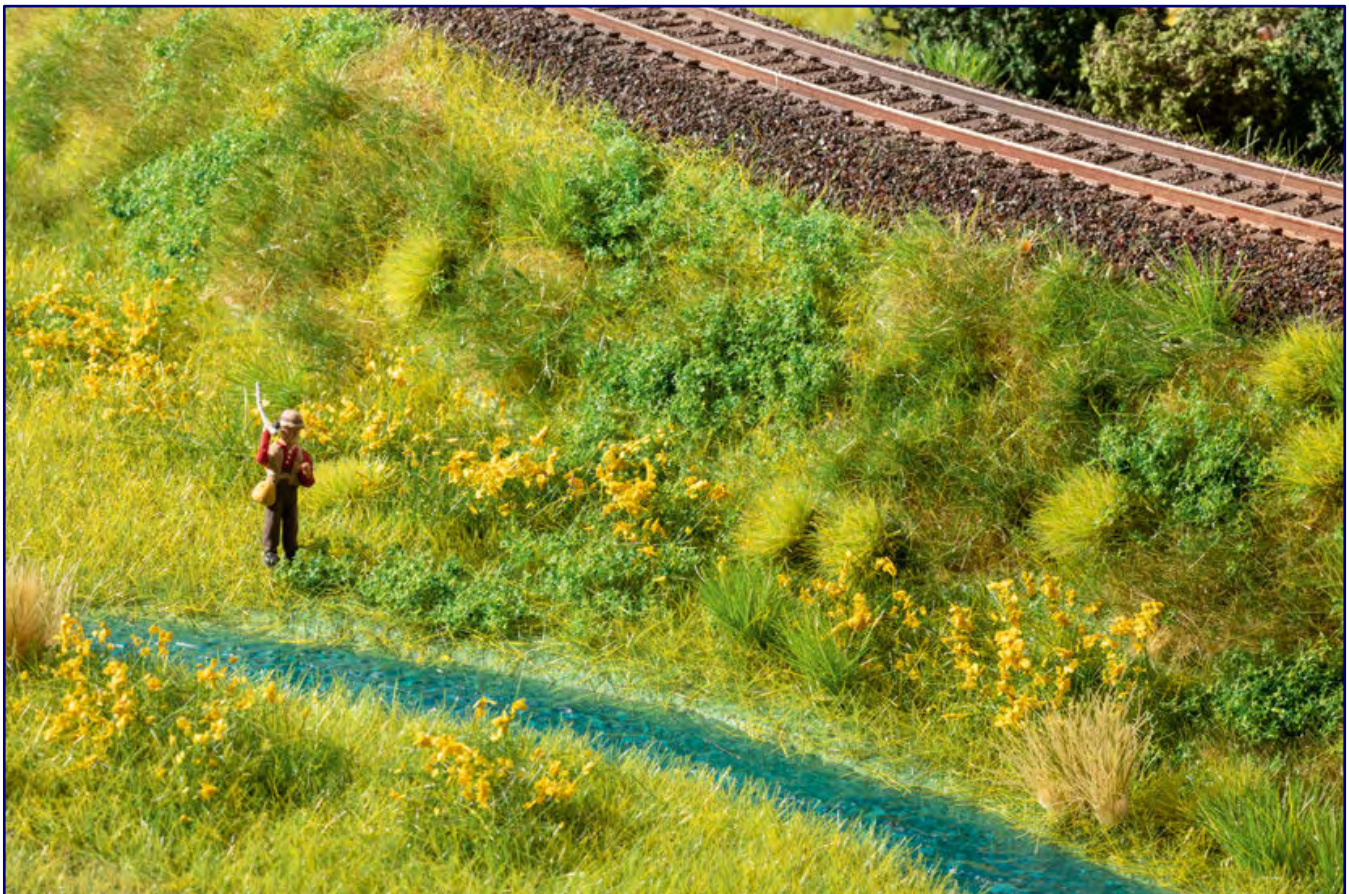
www.modellbaukompass.de

+++ Noch +++

Landscape design specialist Noch has also put together some new products in 2021, which can be used on future Z-gauge layouts. The two vegetation starter sets "Meadow" (art. no. 60771), "In the forest" (60772), "By the wayside" (60773), "On the bank" (60774), and "By the railway embankment" (60775) begin the series.

They offer a price advantage over buying them individually, and each set consists of a selection of scattered grass, material, flock or tufts of grass to match the theme.

The Natur+ series is further expanded with the following meadow and field mats in 25 x 25 cm format (two mats per pack), each with multi-coloured grass: Lush Meadow (07470), Reed Meadow (07471), Natural Meadow (07472), Heath (07473), and Grass Steppe (07474).



Design example for the new ground cover foliage in medium green (07250), modelled in H0 scale. Photo: Noch

Wafer-thin ground cover foliage on a black, stable substrate (12 x 18 cm), easy to peel off and install, either completely or in pieces, offers the following new products: medium green (07250), olive green (07251), dark green (07252), green/beige (07253), green/brown (07254), as well as meadow yellow (07255), white (07256) and red (07257).



The large landscaping basic modelling package (60780) has it all. In terms of price, those who do the math and see their needs covered will achieve a noticeable price advantage over the individual items. Photo: Noch



The detail markers (61155) could prove to be a clever idea, but their tip width is not specified in the brochure. Photo: Noch

The scatter grass assortment is expanded with 4 mm long fibres in a 20 g bag in the shade medium green (08364). The much too small reproduction of most trees also allows the Zetties to use three southern European templates as fully grown specimens: pine trees (21992) 8.5 and 11.5 cm high, olive trees (21995) 6 and 9 cm high and cypresses (21998) 10, 11 and 13 cm high.

The large landscaping basic modelling package (60780) certainly has the price of a steam locomotive model, but also actually contains a “full equipment” of materials for terrain construction, and designing a landscape with greenery, water and rocks. It is aimed at beginners and offers a price advantage compared to purchasing the products individually.

The water colour set (60878) is intended for basic design with colour and can be poured over with the “water drops” because these colours are heat-resistant. The new model water “colour” (60877) is a coloured variant of the long-known product.

The six detail markers (61155) in the colours black, white, silver, yellow, red and brown bring a new approach, for example for



The shady pine tree (21992) with its characteristic umbrella crown is the pine tree of Italy. Like almost all trees, it is clearly reproduced too small and is therefore perfectly suited for the Z gauge. Photo: Noch

“rusting” rail sides. Our readers should compare this product with the well-known Edding paint pens.



It is not the pigment powders that make up this new product, but the mixing bench (61169), as a tool holder complete with inserts for mixing powder colours. Photo: Noch

“Ageing gracefully” jokes Noch when introducing its Patina Powder with Mixing Bench (61169) and Mixing Insert (91951) as consumables to be purchased separately. The mixing bench is a kit that is equipped with three mixing inserts and seven colour pigment jars.

www.noch.de

+++ Peter Post Werkzeuge +++

The new precision wire stripper S (art. no. 37535) enables easy, as well as fast, and even stripping of flexible and solid wire conductors. It is ergonomically shaped and has an automatic sensing system that determines the cable diameter.

This is what makes it so valuable when working on the layout, as it prevents damage to the inner conductors. The working range covers diameters from 0.12 - 0.8 mm. With the adjustable scaled length stop in the working range from 5 to 45 millimetres, the desired stripping length can also be quickly, and reliably, set.

Two long-lasting, precisely adjusted torsion springs ensure comfort. They act symmetrically on the pair of blades and automatically cut the respective insulation with the appropriate pressure. Incidentally, the wire stripper is suitable for both right-handed and left-handed users.

As mentioned in the construction report of this issue, there is currently also a special offer for the Mozart precision knives (art. no. 01345) with ten standard blades (01346), and as a bonus ten additional replacement blades of one of the two other blade shapes (01347 / 01348).

These sets can also be purchased at a reduced price together with the Working-Station (01344), a stable and safe working tray for the sharp blades.

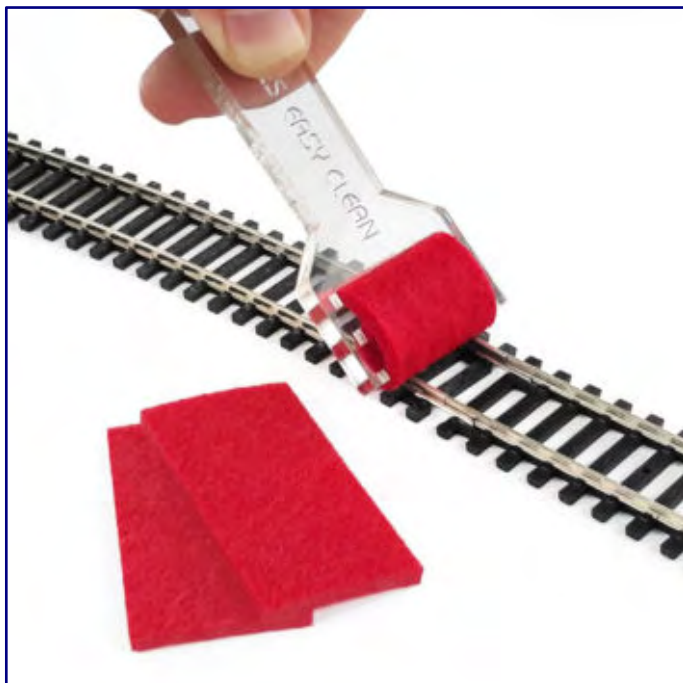


The background to this accelerated offer is the ongoing development of blades for this knife, which is intended to make it even more attractive for model makers.

The precision wire stripper S (art. no. 37535) can help avoid frustration when removing cable insulation. Photo: Peter Post Werkzeuge

However, the additional blade shapes still have to be made ready for series production and tested for their use in different application areas. It is, therefore, not yet possible to make a statement about the effectiveness of the knife.

www.peter-post-werkzeuge.de



"Rail Track Cleaner" for all track gauges. Photo: Proses

+++ Proses +++

On the pages of this Turkish supplier, we found a track cleaner "Rail Track Cleaner" (art. no. TC-001) suitable for all tracks, which is delivered including two spare cleaning felts. It can do its job even on a small scale, at least on tracks with good accessibility and without overhead catenary wires. Five matching replacement felts (TC-002) can be purchased separately.

www.proses.com

+++ Rokuhan +++

Rokuhan customers in Europe always depend on what its European distributor Noch markets. This year, the Wangen-based company is focusing on the Shinkansen 500 models of the Japanese model railway manufacturer.

They have now succeeded in acquiring licensing rights for the European market for the Hello Kitty Shinkansen, whose prototype and model we already presented in detail in **Trainini®** 2/2019.

This means that nothing stands in the way of now also selling in Europe the three-part basic unit (art. no. T013-6 / Noch 7297867) and the five-part extension (T013-7 / Noch 7297868) as well as the starter pack with basic unit (G004-3 / Noch 7297926).



The popular Shinkansen series 500 in the extravagant Hello Kitty design is now also distributed in Europe. It is divided into a three-part basic unit (art. no. T013-6 / Noch 7297867) and a five-part extension. (T013-7 / Noch 7297868).

A switch back control system (C006) that automatically sends trains back and forth between two end points was already announced in the United States some time ago. Trains are detected at the end point by a photo sensor, slowed down, stopped and reversed in their direction of travel.

Also announced there was an automatic block power switch (C007) for analogue operation. Like their proprietary point switches, it is simply plugged into the left-hand side of the RC02 or RC03 train controllers.

www.rokuhan.de

+++ Schreiber-Bogen Kartonmodellbau (Aue-Verlag) +++

If you want to devote yourself to a historical topic close to Z gauge (scale 1:250), you can create the Stone Age Stonehenge (art. no. 790) in cardboard model construction. No less impressive is the 1-metre-long aqueduct "Pont du Gard" from ancient Roman times (791), in scale 1:300.

From the MTP studio Thomas Pleiner comes the miniaturisation of St Paul's Cathedral in Münster, Westphalia (S118), which despite the scale of 1:300 still has a length of 36 cm. Therefore, despite the not exactly fitting scale, it is certainly a sensible compromise solution for all those who are looking for a large church that should not stand too close to the front edge of the layout.



St. Paul's Cathedral in Münster (art. no. S118; photo above) and salvage tug "Seefalke" (3435; photo left) are two new cardboard kits that are sufficiently close to Z scale. Photos: Aue-Verlag

Fans of harbour scenes can look forward to the salvage tug "Seefalke" (3435), which has been published by the Hamburger Modellbaubogen-Verlag and can be built up in even more detail with a laser cut supplementary set (4435). The conversion scale in this case is 1:250.

www.schreiber-bogen.de

+++ Silhouette Modellbau +++

Silhouette has also announced nine different types of deciduous trees in four different, seasonal representations each and two conifers for Z gauge. The following very beautiful miniatures are available from the Profiline series:

Hanging birches for spring (art. no. 211-61), summer (211-62), early (211-63) and late autumn (211-64) as well as poplars (213-61 to -64), aspens (215-61 to -64), beeches (220-61 to -64), apple trees (226-61 to -64), sycamore trees (233-61 to -64), weeping willows (240-61 to -64), linden trees (242-61 to -64) and oaks (280-61 to -64) in the same versions. They are complemented by a spruce (273-62) and a fir (276-62).

Loose flowers (898-30MS) can be scattered, in most scales, while for the flower assortments in five colours (898-29MS), as well as unspecified coloured flowers (998-30MS & 998-30S), suitable planting options should be sought.

Last year we introduced new grass mats on carrier film with short fibres at 3 to 5 mm thickness. This programme is now being expanded with smaller mats in 14 x 9 cm format (art. no. ending in MS). Unfortunately, the information on new products does not give any indication of the fibre lengths in the following list:

Cattle meadow spring (713-21MS), summer (713-22MS), early (713-23MS) and late autumn (713-24MS) as well as karst meadow (719-21MS to -24MS), soil overgrown (735-21 MS to -24MS), forest soil (740-21MS to -24MS), and forest soil overgrown (741-21MS to -24MS) in the same seasons.

Also, the cattle pastures (713-11MS to -13MS) and sheep pastures (715-11MS to -13MS) from spring to early autumn, as well as, floodplain meadows (733-11MS to -14MS) for all four seasonal representations with short fibres, which we explicitly listed last year, are now available in MS small format.

www.mininatur.de

+++ Uhlenbrock Elektronik +++

A Loconet servo module for four servo drives (art. no. 63500) was announced, which will be delivered with a Loconet cable. Operation is also possible without a central unit and the new product offers the presetting of the rotation speed and of four stop positions. Various switching outputs and also inputs, as well as freely programmable movement profiles, allow a wide range of applications at semaphores, barrier systems with light safety installations or also for swinging up and down or side to side.



Also new is the affordable 2.2 Ampere Booster Power 22 with wall plug power supply and Loconet cable (63210), which is intended by the manufacturer for H0 to N scale because of its output voltage of 15 volts, but will certainly not prevent it from being used in Z scale as well.

The more powerful model Power 40 (63220) with 3.5 amps is declared as "now also for Z gauge" because of selectable output voltage between 11 and 17 volts.

With 11 V for our small gauge, the supplier follows the recommendations of the NEM, but not practical experience, which rather ranges with the voltages that are also used in N gauge.

The booster Power 40 (item no. 63220) is now recommended by the manufacturer for Z gauge as well. Photo: Uhlenbrock Elektronik

A 35 VA wall plug power supply (20230) is also available separately, which outputs 16 volts on the secondary side. Both the MP3 sound module (38110) and the 3-way MP3 sound module (38130) are building blocks for playing sounds on the analogue or also digitally operated model railway layout.

Regardless of whether they are already on the computer, downloaded from the Internet or created by the user, they can be played via a USB interface. One such sound can be stored per module and played once or in an endless loop as soon as it is triggered by a button, switch or reed contact.

www.uhlenbrock.de

+++ Unique Scenery Products +++

The Dutch supplier now markets its own grass glue under the Minitec brand, which has been in its care for years, designated as a highly flexible grass flock glue (art. no. 59-0522-00), which is offered in a 250-gram can.

www.sceneryproducts.eu

+++ Viessmann +++

Two sound modules from the Viessmann new products could acoustically enliven self-designed Z-gauge scenes: Lovers (art. no. 5566) and a chicken yard (5569).

www.viessmann-modell.de



Grass flock glue from Minitec (Art.-Nr. 59-0522-00). Photo: Unique Scenery Products

+++ Walter Merten Miniaturplastiken +++

After almost yearly new and repeatedly passed delivery dates for the reissues of the Merten figures for Z-gauge announced in 2015, we have no current information. The most recent date was the third quarter of 2020, which has long since passed.

www.preiserfiguren.de



Fire extinguisher for various scales, including Z gauge (item no. 6868). Photo: Weinert Modellbau

+++ Weinert Modellbau +++

For several years now we have been supplied with beautiful new products from Weyhe-Dreye. The excellent reputation of this kit supplier also applies unconditionally to Z scale.

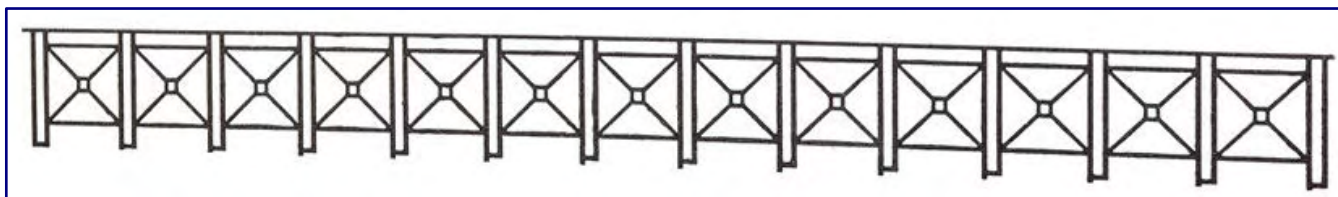
The small diesel filling station Dörzbach (art. no. 6866) made of cast brass is sure to strike a chord with customers immediately. With a prototype capacity of approx. 5000 l, it supplies small locomotives (Köf), narrow-gauge diesel vehicles, or railcars.



One of the outstanding new products of this year will probably be the small diesel filling station Dörzbach (6866). Photo: Weinert Modellbau

A useful addition there as elsewhere can be the fire extinguisher (6868), four of which are offered in the form of cast brass parts. Bronze etched parts on the other hand are the fine grids for bridges and railway area (6867; 32 cm) and the hunter fence (6864; 27 cm).

www.weinert-modellbau.de



Jäger fence (6864; photo above) and grids for bridges or railway areas (6867; photo below) are also new products. Photo: Weinert Modellbau

Bestellschluß verlängert

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Small Series Manufacturers New Products 2021

The Salt in the Soup again

Among the manufacturers who traditionally do not appear at the Nürnberg (Nuremberg) fair are also larger suppliers for the 1:220 scale such as AZL or Micro-Trains, which have their core market overseas. We would like to report on their new products as well as those of European suppliers in the small-series sector, after a lot has happened in recent years, especially in Portugal, Ukraine, and Austria.

As usual, we have also scoured the pages and publications of the well-known small-series manufacturers for our readers, as well as processing information with which new suppliers have actively contacted us. What is known after this long work and what we have been able to collect, you can read in this article.



An impression with dawn at the beginning of this report: The windmill 'Am Geestenvveen' by Archistories is certainly one of the most impressive small-series new products in 2021. But the beautiful trees are also quite something, because they come from Avantgarde Modellbau, another supplier whose new products we present here. Photo: Archistories

+++ A2 Models +++

The sale of the existing stock has not been completed to date. There are still models in 1:220 scale that can be purchased with a 20 % discount.

www.a2models.nl

+++ Álvaro Cortes +++

Portugal is also suffering and has recently suffered particularly badly from the consequences of the pandemic. Due to the massive restrictions on public and economic life, Álvaro Cortes is currently also making progress in small steps. In one or two months, he expects, there should be more new products to announce.



New decals for fire brigade, rescue service and ADAC road service vehicles (from left to right) are among the new products at Álvaro Cortes. Photos: Álvaro Cortes

Until then, there will be new sliding panels for rescue service and fire brigade emergency vehicles as well as for the ADAC road service. There is also a version of the Mercedes-Benz Sprinter, as a light tipper (light weight dump truck or tilting bed truck).



With the AC Cobra (photo, above left), the Jeep Willys (photo, above right), the Audi A6 Allroad (photo, below left) and a new tipper version on the Mercedes-Benz Sprinter (photo, below right), the new products are still manageable for the time being. Photos: Álvaro Cortes

While the Jeep Willys, built from 1941 to 1945, was more likely to be seen as an occupation vehicle and later an enthusiast's vehicle in Germany, it may have been a familiar sight in North America. The AC Cobra (Shelby Cobra), built from 1962 to 1969, was also one of the more extravagant sports cars in Europe.

That's not the case with the latest new product we have the pleasure of presenting: The Audi A6 Allroad, which has been in production since 2019, is sold and appreciated worldwide, but has its home market in Germany and Europe. All vehicles are made of resin, as is customary here.

[Acortes\(at\)leya.com](mailto:Acortes(at)leya.com)

<https://www.facebook.com/pages/My-Z-scale-models/1648628058694887?ref=hl>

+++ Archistories +++

After the great success of the "Marienfehn" windmill, Archistories has finally heeded the call to follow up with a windmill of a more recent and widespread design. Previously, our editorial staff had repeatedly suggested to this supplier for several years that they develop a gallery windmill and even passed on models.

The result of our persistent efforts is now the Galerieholländer 'Am Geestenvveen' (art. no. 702181), which is probably the archetype of a windmill for many Zetties: A wooden shingle-roofed cone with a thatched mill cap and many effective details rises above a solid brick base.



The front and back of the Galerieholländer 'Am Geestenvveen' (art. no. 702181) give hope for the delivery, which is to start at the end of March, and will, in any case, close a gap in the Z gauge assortment with a great model. Photos: Archistories

At the rear is the linkage of the so-called wind direction tracking with shank and reel. Of course, this mill is again equipped with a high-quality geared motor, which allows for a prototypical, unhurried rotation.

During product development, Archistories worked closely with Björn Plutka from Z-Doktor Modellbau on a detail that was complicated for laser cutting because of its shape, but very important. This is how a mill cap came into being, the result of countless hours to find the perfect shape of this typical windmill cap.

The kit will be available in the Archistories shop and from authorised dealers from the end of March 2021. As usual, it is made of high-quality, solid-coloured hardboard of varying grammage with an elaborate surface finish. The diameter at the base is 42 mm, the height to the cap 75 mm and to the wing tip 103 mm.

At this point we would like to add a photo of the rural US village church (403181) in wooden construction, which was exclusively issued and which we presented last year (still without a photo). In the meantime, three more colour variants have been added to the series of rural US houses in turn-of-the-century style: olive green (404201), ruby red (405201) and dark blue (406201).

www.archistories.com

www.archistories-shop.de

+++ Atelier Dietrich +++

Ten years of crumpled rocks: to mark the anniversary in 2021, the Andreas Dietrich model-making studio is expanding its popular product range with a new series of US crumpled rocks. Designed after original photographs from the USA by photographer Peter Metzker-Giessauf, the different rock variants in the sizes 30 x 17 cm, 45 x 25.5 cm and 61 x 34.5 cm will be available for order from 22 February 2021.



For the anniversary, there are now also crumpled rocks based on American models. Photo: Atelier Dietrich

For Z gauge, Andreas Dietrich recommends using only the 30 x 17 cm size, with which the inventor of this design technique will certainly not be off the mark!

www.atelier-dietrich.at

+++ Avantgarde Modellbau +++

For Avantgarde Modellbau, model maintenance is the most important thing at the moment. The trees for Z scale are also largely given self-developed, true-to-scale foliage. This includes different sizes and colours of foliage material, but also different leaf shapes. The jagged finger structure of the maple is particularly striking.



The convincing leaf structures can be seen very well on the old lime tree (photo above). But other trees such as the ash (photo below left), the spruce (photo below centre) and the pruned trees (photo below right) also benefit from more realism. Photos: Avantgarde Modellbau

Together with a few other improvements to details, the new foliage brings another significant leap in terms of realism. As a popular ornamental tree in the garden, the fan maple is only a logical consequence as a new product.

The new vegetation mat "Fern" is used to plant larger areas comparatively cheaply and easily and is an addition to the etched ferns in the range. The so-called ruderal vegetation, scrub on railway facilities and fallow land, was announced last year, as were the bare winter trees, and according to current plans is to become available this year.

www.avantgarde-modellbau.de
[motewe\(at\)gmx.de](mailto:motewe(at)gmx.de)

+++ AZL (American Z Line) +++

AZL traditionally announces new models at the most important exhibition in the United States of America, the National Train Show. Last year, unfortunately, it also had to be cancelled, but preparations were still communicated.

That is why the new products to be reported here are first of all those that are currently being delivered. Among them is the ALCO RS3 in the eye-catching design of the Seaboard (item numbers 63310-1 to -3), an specimen from a still very new mould.



The locomotive, like all the other versions of this type, has been modelled exactly on the large prototype in terms of its appearance and details. This is why the parallel model for the Canadian Pacific (63302-1 to -3) is particularly striking.



The models of the R-70-20 refrigerator cars for fruit transport are still slightly more recent. In orange they now roll for the extremely popular Pacific Fruit Express (PFE) and are available in this design individually (914800-1), as a set of two (914830-1), and as a set of four (904800-1).

The railcars for the PFE are released in a two-pack (914830-1) as well. Photo: AZL / Ztrack



The wagons for the PFE appear among others in a pack of two (914830-1). Photo: AZL / Ztrack

Already in operation for some time in 1:220 scale are the sets of five MAXI-I container freight cars, which now carry the new TTX logo (906503-2HL to -4HL) and compete with 40-foot containers for Hapag-Lloyd.

The EMD GP38-2 diesel locomotive for the SOO Line is being reissued with old chassis on new bogies from 2020. The quantities of the four operating number variants (62510-1R to -4R) are correspondingly small.

A current project is the development of a new 8,000-gallon tank car with insulation, which was shown in twelve different designs in a presentation of new products (<https://youtu.be/-4bcWpXYXPno>). Production samples already exist for these railcars. Deliveries are planned in the usual way for singles, twos and fours.

The “Super Chief” express train has now also been delivered. The full-length train is divided into two packs (72200 / 72201) with eight different cars each. To complete the train, two four-car sleeper packs (72210 / 72211) are offered. As matching train locomotives, EMD F7 are recommended as A and B units, for which space is already provided in the boxes.

Matching ATSF A-B-B-A units in Warbonnet livery (63001-1_SET / 63001-3_SET) as well as two single F7A (63001-1 / 63001-3) were therefore also released. Unfortunately, all these models are already sold out at the factory and at most dealers. A second edition has already been announced. Otherwise, customers could opt for the EMD E8, which was also used to pull the “Super Chief.”



The EMD E-7 A and B was an obvious choice for AZL and has now been announced as a new development. It will certainly also appear in the New York Central livery shown here. Illustration: JAGRAFXWIK (CC-BY-SA-4.0)

If we touch on both types of locomotives at this point, it is more or less obvious to also realise the EMD E7 as a historical development step. So, it too has now been announced as an A and B unit and initially presented with pictures of no less than nine planned versions. Owner-specific details, such as resistance brakes or different horns, are taken into account in each case.

The EMD SD70A is to be relaunched in the versions of the railway companies already taken into account, but with new service numbers and detail modifications around the driver's cab. Co-owner Hans Riddervold has also announced that up to twelve more railway companies will be available for the ALCO RS3 and RSD-5.

The last projects mentioned to be currently in progress are a shunting locomotive, which is to be presented this year, an as yet undisclosed new locomotive and the EMD SD40, but it is still unclear whether it will actually be able to be available this year.

www.americanzline.com

+++ Bahls Modelleisenbahnen +++

Contrary to what we read on the supplier pages at the time of going to press, the static distant signals and track blocking signals are currently not available from stock. New products will not be announced for the time being because there are still some orders to be processed first.

This also includes orders for three variants of the “Gläsernen Zugs” (glass or clear train) class 491 (art. no. 5291) in three colours, which are a contemporary reissue of models from the former supplier Schmidt from Hamelin.



Among the orders currently to be processed are the orders for the “Gläsernen Zugs” (glass or clear train) in three versions. Photo: Bahls Modelleisenbahnen.

Quite new in the programme, however, are wheel sliders, which we had not discussed so far. They are similar to those that could be found as a version with the lowest rolling resistance from the small-series manufacturer Oliver Passmann, who died suddenly in 2020. They can be ordered from the 1zu220 shop (see address there).

www.bahls-modelleisenbahnen.de

+++ Creativ-Modellbau Klingenhöfer +++

In demand on every rural model railway layout are the new 3D-printed chickens (art. no. TH05), which are sure to quickly become widespread.

They do not necessarily have to be used on a chicken farm or a farm: Often, escaped chickens also wander around outside their coop. The fox, which has been available in the range for some time, can then become a danger to them.

For alpine layouts, there will be more ibexes in the programme in the future, which will provide variety and certainly also have their own territorial claims. This can be seen in the two fighting ibexes (mountain goats with curved horns) (TS10-2).



Chickens probably belong in every rural area. They are now available at C-M-K (art. no. TH05). Photo: Creativ-Modellbau Klingenhöfer.

The single ibex with long horns (TS10-1), on the other hand, certainly does not want to challenge a fellow species to a duel. The standing ibex (TS10-3) can serve as a silent spectator in such scenes or also watch the layout viewers.



Three new ibexes are new in the programme: with long horns (TS10-1; photo left), fighting (TS10-2; photo middle) and standing (TS10-3; photo right). Photos: Creativ-Modellbau Klingenhöfer

With the Arctic-like temperatures prevailing in the northern half of Germany at the time of writing, the reindeer (TS10) also looks almost familiar.

Otherwise, it is more familiar from the zoo or a wildlife park. Those who build near the Arctic Circle according to the Scandinavian model will also know how to use this figure in its natural environment.

In addition to the new products already announced, this manufacturer also informed us that their focus this year will continue to be on expanding the range of zoo animals.

www.klingenhoefer.com

+++ Der Schienenreiniger +++

Without cleaning the rails, no one will enjoy their layout for long. Dust and dirt that settle must be removed from the track profiles just as gently as thoroughly as abrasion and traces of lubricant that can also get here.



The reindeer is certainly not only a good choice as a draft animal for Santa's sleigh. It will also be indispensable on layouts based on Scandinavian models. Photo: Creativ-Modellbau Klingenhöfer

Also, a supplier of track cleaners in Z gauge is Lothar Emmerling, who operates as Der Schienenreiniger. His products are not actually new products in 2021, but they certainly deserve to be brought into focus at this point.

The core of track maintenance are the tools known as "Schienenmop" (track mop) and "Schienenmover" (moving track cleaner). The first product has a long handle that guides the cleaning base past the overhead line and into inaccessible areas of the layout. The "Mover," on the other hand, has an ergonomic handle and is guided with the fingers.

The consumables and wearing materials are available separately: 20 cleaning cloths and track cleaning fluid in 100 ml and 250 ml. Both types of cleaning tools are also available together as a bundle at a lower price.

For removing metal parts from the track, there is also a magnetic head that can be exchanged with that of the rail mop, and can then use its long guide rod. This tool can also be used to test the function of magnetic items, such as relays or reed contacts.

schienenreiniger.de

+++ D&H Doehler und Haass Steuerungssysteme +++

This digital supplier for command stations, and with an excellent range of Z gauge decoders for European locomotive models, is using the year 2021 for product updates, and is not currently announcing any new products. The company's own website will provide continuous information about the deliveries of new products that are in arrears, such as the FCCX command station that was introduced last year.

www.doehler-haass.de

+++ Freudenreich Feinwerktechnik +++

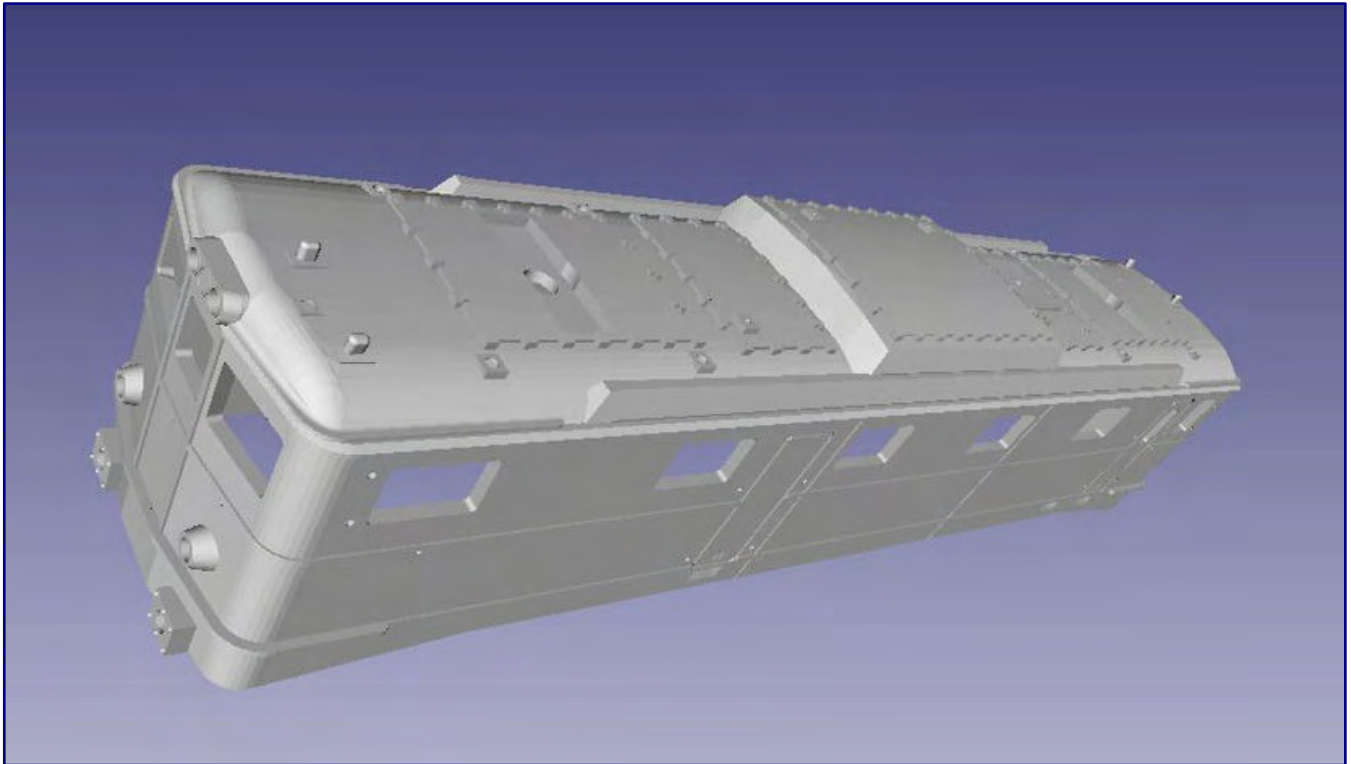
Currently in production is the V170 in blue-silver design of the BOB and NEG, another variant of the popular NOHAB diesel locomotive. It is produced exclusively for the 1to220 shop and is also mentioned there once again. This also applies to a four-part freight car pack for the German Federal Railways in Era III, which is to follow in the next quarter.

In the 4th quarter of 2021, the MY 1110 of the DSB (45.126.21) in the red-black colour scheme from its last period of service (era IV/V) is also planned. This model can also be ordered with DCC digital with operating sounds and shunting platforms above the buffers (operating state from about 1996) for an additional charge.



DSB's MY 1110 in its final colour scheme of red and black (Item No. 45.126.21) is scheduled for Q4 2021. Photo: FR Freudenreich Feinwerktechnik

Currently under construction is the six-axle electric locomotive Ma of the SJ. This representative of the Swedish railway is expected to appear in brown livery in the 3rd quarter of 2021. It, too, will be offered in either analogue or digital.



Currently under construction is the six-axle electric locomotive Ma of the SJ. It is initially scheduled to arrive in brown livery in Q3 2021. Photo: FR Freudenreich Feinwerktechnik

For the six-axle container double wagon Sggrs 80, which some customers are already eagerly awaiting, all the technical prerequisites are now finally in place for production to begin. A first series is therefore expected to be produced this year.

Finally, and quite promptly in the first quarter of 2021, the open goods wagon from the Villach district is to appear in a DB version, and with double the impact! Two examples each, labelled as Omm(r) 33, will be available without (49.338.02), and with EUROP labelling (49.338.12) in packs for Era III.

According to the current status, further new products are planned, but, are not yet ready for announcement due to a lack of reliable time schedules.

www.fr-modell.de

+++ Heißwolf Modellbahnzubehör +++

After last year's launch of the project "Serial Switch Bus SSB64," a first extension will appear on the market in the second quarter of 2021: The servo module controls four model-making servos whose end positions and setting speeds can be individually programmed via a separately available setting module.

In addition, the new servo module also has a connection for the relay module already available for polarising turnout frogs. This means that not only turnouts and shaped signals can be moved true to the model with servos.

The servo module is controlled by the "Serial Switching Bus SSB64," with which up to 64 relays or servos can be addressed via a two-wire cable. There is therefore no need for complex wiring between the layout and the control panel. The number of plug contacts between separable system parts is also drastically reduced.

www.modellbahn.heisswolf.net

+++ High Tech Modellbahnen +++

There are still backlogs and waiting times for ordered conversions. For this reason, we are refraining from announcing new products for the time being, in order to be able to use capacities in a targeted manner for customers who are already waiting.

www.z-hightech.de

+++ HOS Modellbahntechnik +++

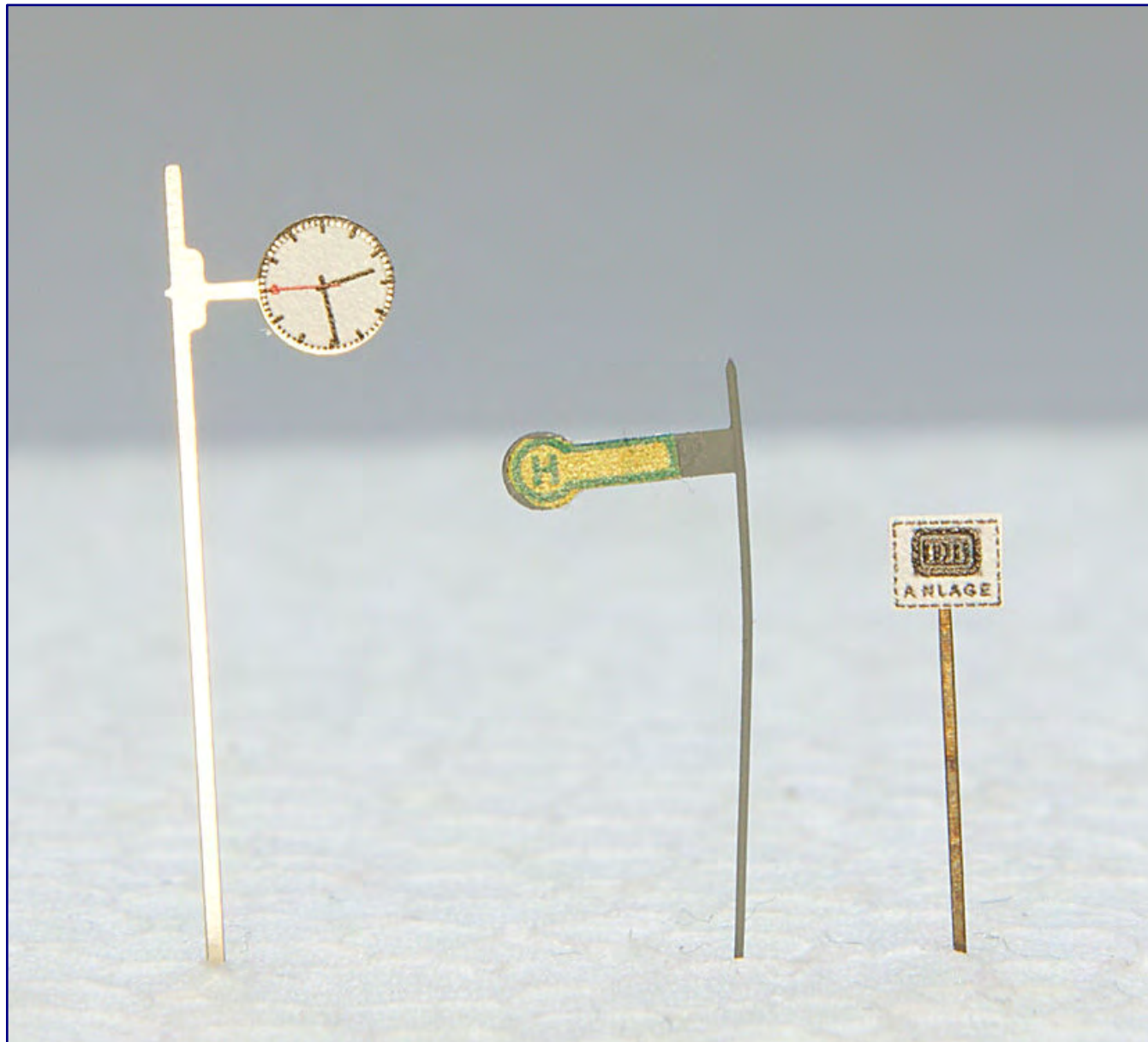
After approval by the prototype manufacturer, HOS Modellbahntechnik has transferred a small bus shelter (item no. WH1) with the designation "Wartehalle Raster 22" for epochs V and VI to the scale 1:220 according to original plans. The model is made of nickel silver and finely glazed.



The small bus shelter for epochs V and VI is officially called "Wartehalle Raster 22" (art. no. WH1). Photo: HOS Modellbahntechnik

These bus shelters can now be found in many places, also in a small version at a bus stop. Original displays can also be installed in the small showcase. Also, new is a station clock (ZU20) for hanging on buildings or poles, as required, at every train stop.

We are pleased that the “DB facility” sign (SI61) and the bus stop trowel (SI65), which we produced for our “Himmelreich” project presented last year, are now firmly in the product range. And there was also added a danger sign “Vorsicht Löschegrube” (caution, inspection pit; SI63) for steam locomotive service depots.



Station clock (ZU20), the former bus stop sign (SI65) as a "trowel" and the information sign for railway-owned layouts (SI61) are now also new in the accessory supplier's programme. HOS Modellbahntechnik.

In the meantime, the first SIG transitions for pressurised long-distance wagons have been completed. Follow-up work is now concentrating on developing them into a universal type for all possible wagon types. Delivery is planned for spring 2021.

www.hos-modellbahntechnik.de

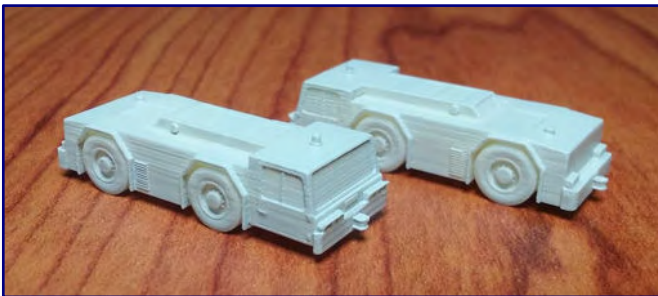
+++ JMC Scale Models +++

This Portuguese car model artist is always active, which is probably why we are allowed to report on new products throughout the year. The latest developments cater to model railroaders who have designed an airport scene on the layout or are building according to the American model.



Anyone who regularly travels by air will probably be familiar with the modern Neoplan N922-2 apron buses. Photo: JMC Scale Models

Worth seeing and finely detailed as always are the Neoplan N922-2 apron bus and the Schopf F246 pushback tractor. The airport apron bus seems to already be finished, because it is shown painted.



The Schopf F246 push-back vehicle (photo left) and the Ford Series C, rescue squad vehicle (photo right), are also well advanced. Photos: MC Scale Models

Important for American fire stations is the Ford Series C as a rescue squad vehicle, of which the first photos of the resin blank can already be seen.

jmc-scalemodels.blogspot.com

www.facebook.com/pages/category/Games-Toys/JMC-Scale-Models-341193572911400/

+++ JoWi Modellbahn-Hintergrund +++

We found five more CD data carriers with print templates for backgrounds at this supplier: "Landscapes I" (Art. No. CD-030), "Backgrounds Landscapes II" (CD-031), "Village and Small Town" (CD-035), "City" (CD-040) and "Industry." (CD-045).

www.modellbahn-hintergrund.de

+++ Kastenbahner +++

This supplier has not presented any new signals for Z-gauge, but we would like to refer you to the new products entry for the manufacturer "ZDC - Z Dream Collection," which also works with the kit parts from Andreas Herzog, and presents new products for this purpose.

www.kastenbahner.com

+++ Kreativ3.de +++

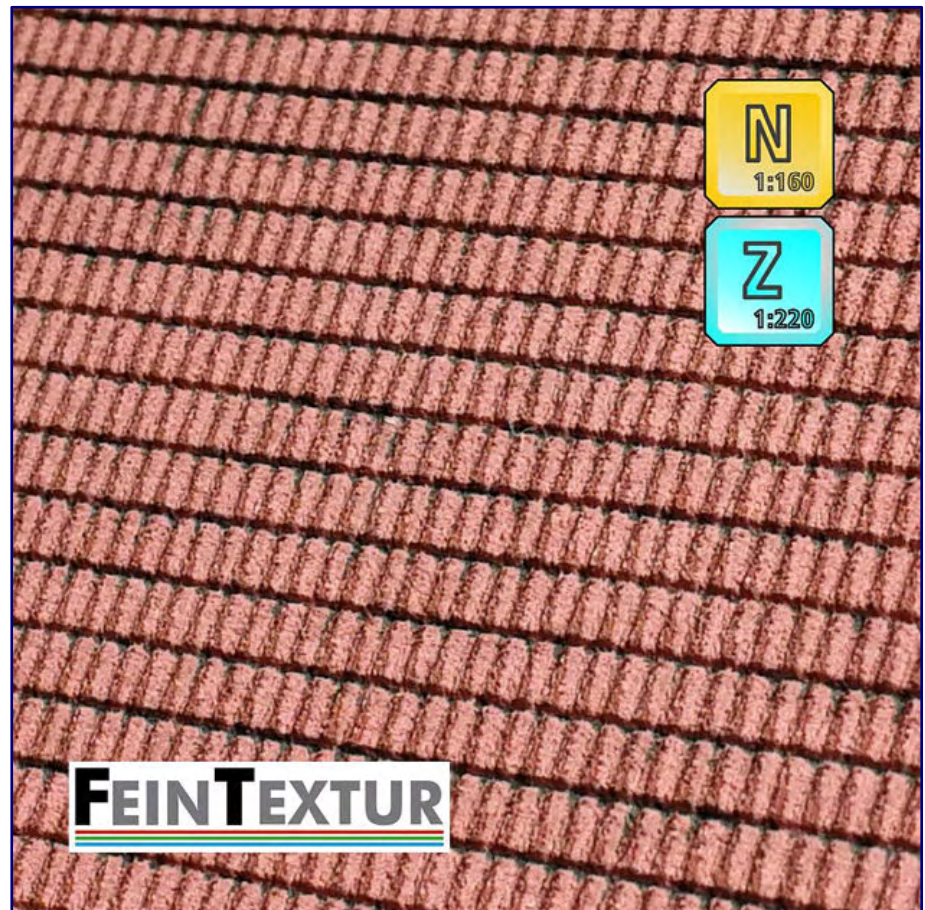
A new roof tile design can be found at this supplier: The "Frankfurter Pfanne Altdach" (art. no. MK66011) in fine texture. This cement roof tile was produced in assembly line production by the still young Braas company in Frankfurt from 1954 onwards.

To this day, they are among the most widespread concrete tiles in Germany, especially in the west of the republic.

The reproduction as an old roof by means of a 3D structure plate made of laser-engraved, solid-coloured graphic natural cardboard contains depressions with irregularly darker shades, that are caused by decomposing mosses and lichens.

The main colour of the roof is terracotta red.

www.modellkreationen.de



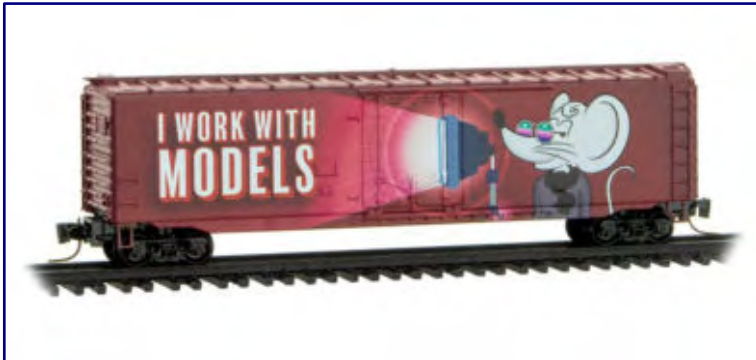
This is what the "Frankfurter Pfanne Altdach" (art. no. MK66011) looks like, which can also be used to cover Z-gauge roofs in the future. Photo: Kreativ3.de

+++ Krüger Modellbau +++

A VTG hydrogen tank car (item no. Z63000), which was offered as a ready-made model years ago, will soon be released as a kit. The pair of boilers is a white painted plastic casting with pad printing. They are placed on the chassis of a four-axle Märklin tank car to be shortened.

The prototype was built in a quantity of five by the wagon factory Kaminski in Hameln for the transport of compressed hydrogen. They wore different paint schemes and designs during their period of service.

www.krueger-modellbau.de



Printed differently on both sides of the wagon is this year's Valentine's Day car (item no. 507 00 690) from MTL. Photos: Micro-Trains

+++ Micro-Trains (MTL) +++

Bulk wagons with two discharge hoppers are currently being delivered in grey paint for the ATSF (art. no. 531 00 073 / 531 00 074), the Pennsylvania RR (531 00 321 / 531 00 322), and in yellow for CSX (531 00 331 / 531 00 332).

Painted blue and yellow is the tank of the single-domed 39-foot tank wagon (530 00 490), labelled for "Bee Hive Corn Syrup" of the adjuster St. Lawrence Starch Co. Ltd. (SLAX). Aimed at collectors is the covered 50-foot Valentine's Day standard freight wagon (507 00 690), which bears the imprint "I work with models" on one side and "Happy Valentine's Day" on the opposite side.

Other covered and bulk freight wagons for different railway companies from the recent pre-order processes are due for delivery soon or by July 2021. Further projects have not yet been announced.

www.micro-trains.com

+++ Microwelten +++

Actually, a manufacturer for the nominal size N, some products are also made here for the scale 1:220, on request. The alpine hut (art. no. 32-10) is laser-cut from 1 mm thick real wood veneer, while the scaffolding parts are made of birch plywood 0,6 and 0.4 mm thick. Placed on a wooden base if desired, the kit also includes parts such as railings, stairs and shutters as well as a PVC film for the window panes.

The series "The Wild West" (article numbers 36-11 to 36-24), also made of wood, offers seven house kits ranging from a railway station and a blacksmith's shop, to a church, various shops and a western pub. Those who are dedicated to this special theme of Era I or scenes of an amusement park will get their money's worth here.



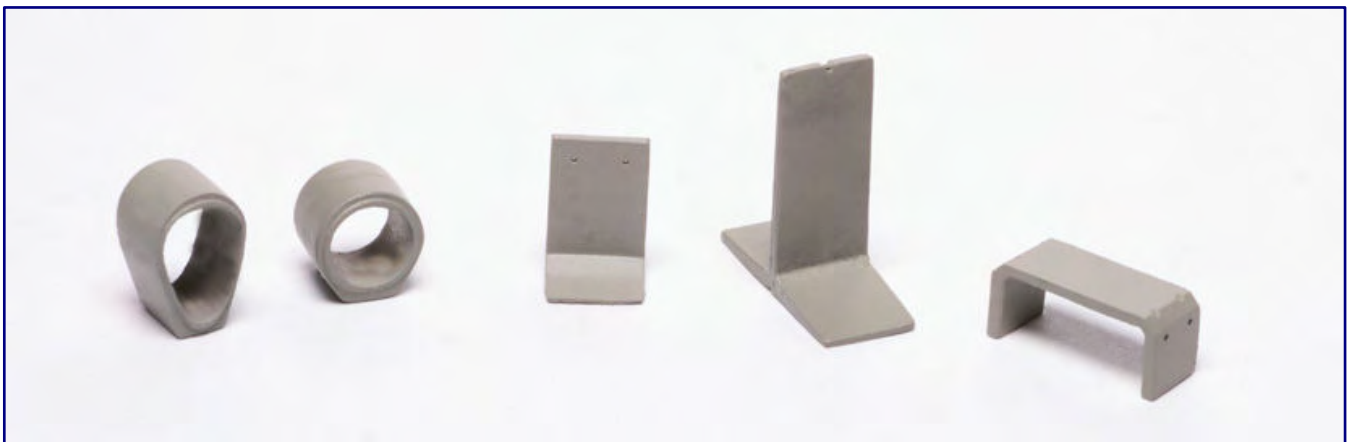
Photo on the right:
A discovery for ÖBB friends is the small Miesenbach waiting house (item no. 37-30) made of 0.8 mm birch plywood with a roof made of photo cardboard. Surely, it can also be used credibly at rural stops in Germany. Photo: Microwelten

The accessories section offers a kit for Euro pallets (31-18), 6 park benches (31-65), 6 bench-table sets (31-68), and the small bus shelter Miesenbach of the ÖBB (37-30) made of 0.8 mm thin birch plywood with a lasered roof of photo cardboard.

microwelten.de

+++ MKC Modellbahn Kreativ Chemnitz +++

At Siegfried Künzel various concrete parts, concrete pipes with round and egg-profile angle support elements (L-parts) and frame channels (U-parts), some of which were announced last year, are still in preparation or implementation. A whole series of signal boards according to the DR signal book, also announced as new products last year 2021, have been drawn in the meantime and are now available as etched film.



Without a photo, the explanations of the concrete parts would certainly be difficult to understand. The new products can be used for a wide variety of modelling projects, but can certainly also be used as loads on trains and lorries. Photo: MKC

Interested parties should write to this supplier directly, because the pages for Z-gauge products are currently taken off the net, because they are being revised and also rearranged similar to those for H0 and TT gauges.

www.modellbahn-kreativshop.de

+++ Modellbahn-Decals Andreas Nothaft +++

As usual with Andreas Nothaft, these are not announcements, but already available new products. The following sets of decals are offered for all scales, including Z gauge.



"25 years of ICE in Switzerland" can be appreciated on any scale thanks to Andreas Nothaft. Image: Modellbahn-Decals Nothaft

If you want to use the additional lettering for the anniversary “25 years of ICE in Switzerland” for epoch VI (item no. 6614), you will have to build your own or convert the basic vehicle, unless you want to use an ICE 3.

It is easier in the same era with the complete lettering for the 365 221-1 (V 60) of Rail-Adventure (6615) as well as an anniversary lettering for the 139 133-3 from the stock of Lokomotion (6616).



These warning signs (6618) can be found on every modern platform. Illustration: Modellbahn-Decals Nothaft

The markings for goods wagons (electric heating power) and refrigerator wagon strips on another sheet (6617) offer many possible uses. The same applies to the modern platform warning signs (6618), which should not be missing at the train stops in Era V and VI. Fans of Era I receive coats of arms of the Badische Staatsbahn (8183) for labelling their wagons.

www.modellbahndecals.de



Photo: Modellbahn Digital Peter

+++ Modellbahn Digital Peter Stärz +++

The new ZS2+ digital command station in a silver-grey professional housing for controlling 32 locomotive functions is a fully-fledged multi-protocol device for the Selectrix, Selectrix-2 and DCC formats. It can be used to run up to 32 SX-2 and DCC locomotives simultaneously.

An overload time setting is adjustable in steps of 1.4 seconds, and a version query has also been added. The unit is also available in black. Detailed information can be found on the supplier's webpages.

www.firma-staerz.de

+++ Modellbahn Union +++

With the Lasercut universal adhesive (art. no. MU_N-A10000), Modellbahn Union has now introduced its own adhesive for many applications. It was developed in close cooperation with Mig Jimenez.

According to the supplier, it durably bonds cardboard, plastic, resin, etched and transparent parts without leaving any residue. It is also possible to bond different materials to each other, whereby a very fast adhesion without immediate fixing of the parts is supposed to provide hold. The odourless adhesive cures transparently within six hours.

The dealer sees it as particularly suitable for large surfaces on which application by brush is also possible. Thinning and cleaning are possible with water.

www.modellbahnunion.com

+++ Modellbauatelier Steinbrecher +++

In a previous issue, we had already pointed out the very convincing and also inexpensive range of trees offered by André Steinbrecher. By collecting orders and manufacturing them by hand, this supplier achieves very favourable prices.

This is certainly of particular benefit to those model railway enthusiasts who need large quantities of trees for their layout project, but who are not prepared to compromise on their realistic appearance.

From this point of view, fixed new products are certainly not to be expected; after all, the focus is always on recreating the silhouette and foliage colour typical of each species. Special specimens such as winter versions or dead trees are also possible on request.



Photo: Modellbahn Union



Also larches in two different sizes in summer full hangings are produced at favourable prices by the Modellbauatelier Steinbrecher.



The repertoire also includes the solitary oak tree (right), which is easily recognisable as such, but also dead trees without foliage (left), which remain dead in the landscape for many years.

As an example we show such a specimen and representative for the conifers, which are always very problematic in the model, three different larches in full summer foliage – all, of course, for Z scale. We have also added a large oak as a solitary tree.

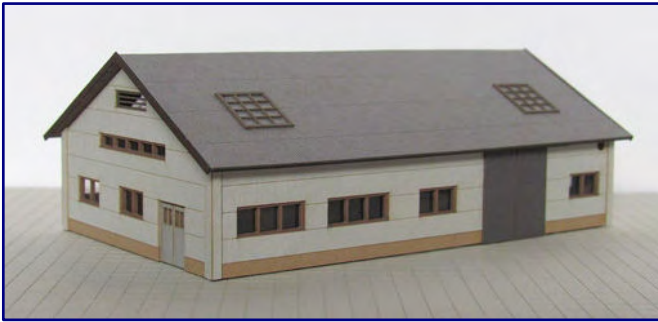
www.modellbau-steinbrecher.de

+++ Modellbau Laffont +++

Stefan Laffont has also put together a nice programme of new products for Z scale, the kits for which will be gradually implemented with instructions in the course of the year. The material for all the buildings is solid-coloured hard cardboard, mostly equipped with the finest engravings.

The first one is a boat repair building (item no. Z3201) for epochs III to VI. The rather plain commercial building is immediately recognisable in its purpose when installed close to a river or canal due to its large sliding gates. The roof has large windows which invite to furnish the interior as well. The basic dimensions are 100 x 59 x 35 mm.

Simple and of the same style, but nevertheless different in its overall appearance is the modern commercial hall with rolling gates (Z3301) for epochs IV to VI. They can be displayed open or also half-open. The basic dimensions differ from the aforementioned kit because it is not simply one variant: 108 x 65 x 34mm. Nevertheless, both halls can also be combined well in a commercial area.



The thematic focus of Modellbau Laffont's new buildings is formed by three different commercial enterprises: boat repair building (art. no. Z3201; photo above left), modern commercial hall with rolling doors (Z3301; photo above right) and a rural carpenter's workshop (Z3001; photo below). Photos: Modellbau Laffont

The rural carpentry building (Z3001) was modelled on a building in Baden-Württemberg and can also easily be used as a rural workshop or farm building.

The formative construction, suitable from Era II, appears perfect for most motifs chosen on model railway layouts. The basic dimensions are 94 x 41 x 25 mm.

The two-storey, rural apartment house (Z3101), which perfectly matches the carpentry building, also has a model in Baden-Württemberg. Its architectural style was widespread from about epoch III onwards. With basic dimensions of 46 x 41 x 40 mm, it will certainly not cause any problems during installation.



Rural apartment building (Z3101) with two storeys. Photos: Modellbau Laffont

The design also reminds us very much of factory flats, as they were readily provided by the steel industry in the Ruhr area from the 1960s onwards. Only the entrance area would have to be modified somewhat, with a door canopy and a different front door without windows.

More new products are to follow around the middle of 2021: Two-storey block stations based on models from the Palatinate Forest are then to be offered as the Rinnthal (plaster building) and Rodalben (sandstone building) signal boxes.

The Ludwigsau-Friedlos stop in semi-open timber construction south of Bebra in Hesse and the Eschelbronn station (near Sinsheim) as a two-storey reception building in plaster construction (ground floor), with timber framework on the upper floor and timber cladding in the gable area will then offer a preview probably for the end of 2021.

www.modellbau-laffont.de

+++ Modellland +++

This supplier of special accessories such as ticket vending machines from Sonneberg in Thuringia has sent us a kit for various containers (art. no. 5200-5) in sizes 20 (to scale 27.5 x 11 x 10 mm) and 40 feet (55 x 11 x 10 mm) that is already available.

This is an inexpensive cardboard sheet that will be particularly useful when many such transport containers are to be reproduced, for example, at a container terminal or a harbour quay. Even beginners will get along well with it.

Containers that look real are unfortunately rare, writes the manufacturer. With this new product, that is about to change. The customer receives exactly the realistic and true-to-scale photo as it is officially depicted.

www.modellland.de



Cardboard sheet kit for various containers (art. no. 5200-5). Photo: Modellland

+++ NoBa-Modelle +++

In the range for about a year as a 3D resin kit, the four-part class 403/404 electric multiple unit of the Deutsche Bundesbahn now also rolls out of the workshop as a painted finished model (art. no. 5207RF). Started as a hopeful for fast travel between metropolises, it was a symbol of the Intercity, which celebrates its fiftieth anniversary this year.

So when could this sleek model with the distinctive snout be more fitting than in the form of a new product in 2021? This train, also well known by its nickname "Donald Duck," has a large and loyal circle of fans, which is why it will certainly also find wide distribution in its miniaturisation.

He learned to drive at NoBa-Modelle by using parts from various Rokuhan shorty chassis. The prototypical pantographs also come from this manufacturer, but were developed exclusively for the 1812 series released by Noch.



It almost seemed as if the "50 Years of Intercity" anniversary in Z-gauge would have to be cancelled this year, but the duo from NoBa-Modelle seem to have something against it: Top new product in spring 2021 are the finished models of the IC express railcar class 403/404 of the Deutsche Bundesbahn (item no. 5207RF) from 1973. Photo: NoBa-Modelle.

But there are a few more new products in the programme: NoBa-Modelle has developed its own advertising vehicle. The "advertising car" (5315R) reproduces a DB new-build refrigerator car from the fifties and can be designed, as desired. Wheel sets, coupling hooks, pressure springs to guide them and pins to hold the couplings are not included, but can be purchased as an accessory set (5315.1).



The tinkerers from the south have now also produced an "advertising wagon" (5315R) for self-design and have provided two of these models with their own motifs as examples. Photos (top and bottom left): NoBa-Modelle



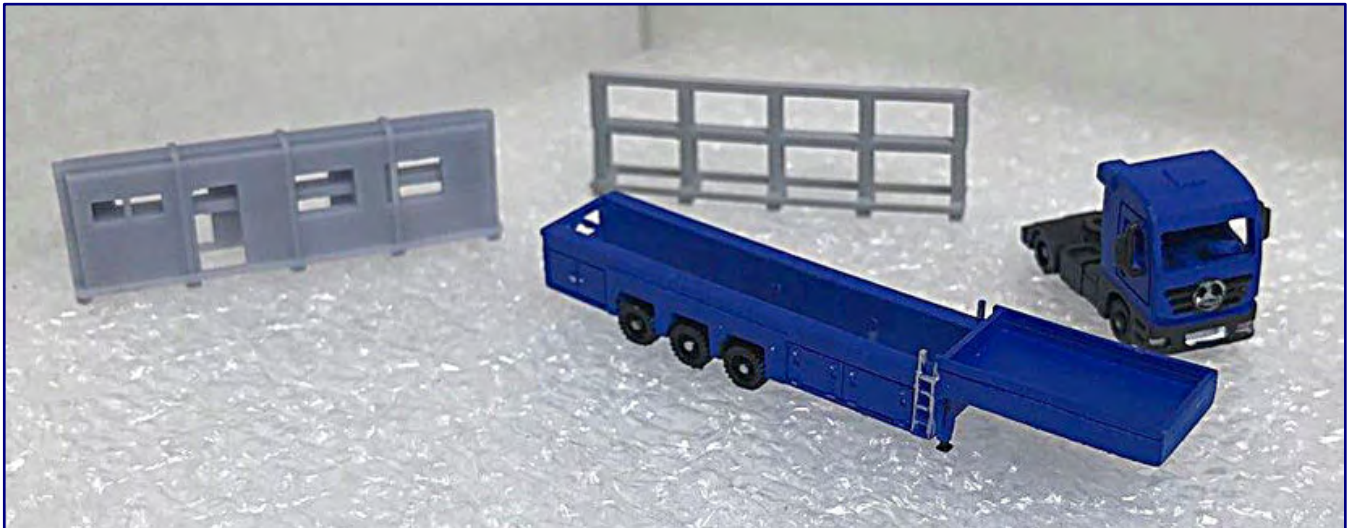
The "2 wing signals – dummy" (5501R) are an inexpensive alternative in places with limited visibility, and no need for movable signals. The two-wing lattice mast version was reproduced, which shows stop (Hp0) on one version and long travel (Hp2) on the other.

The range of tents presented in the last issue is extended by 2-3 tents (10410R) and a tipi (tepee)(10409R).

The focus in this winter new product programme is on motorbikes, which now appear in threes. Enthusiasts will be delighted with the Royal Enfield Classic (6955R), which is also available with a Steib-Zeppelin sidecar (5956R).

The new BMW motorbike (6954R) has a sidecar and additional luggage rack and trailer. And to make things really colourful, the Harley Davidson E-Glide (6958R) and another example of this brand (6957R) are not to be missed.

In the road transport sector there are even more new products. Until the 1980s, the Hatra road roller (6506R) was a common sight on construction sites. This is now available as an unpainted resin model. Winter maintenance on the layout is done by a new Unimog (6212R).



Load carrier for prefabricated concrete parts (6069R; photo above) and Büssing 8000 furniture van with trailer as a finished model in brown (6922RF; photo below). Photos: Noba-Modelle

The popular Era III is served by the tank trailer (6068R), which can itself be labelled for a suitable brand. An inside loader / load carrier for prefabricated concrete parts (6069R), on the other hand, supplies modern construction sites where three cable drums on stands (10611R) can also be used. Of course, they can also be used as load carriers.

The model of the Büssing 8000 truck, which is now available as a furniture truck with trailer in the form of a finished model (6922RF), has been around for some time. The customer can choose between three colour variants, all of which are labelled for "Gebr. Schmitz Möbeltransporte." Another ready-made model is the VW Transporter T 5 with loading area (6205RF), which can also be purchased as an unpainted model (6205R).

www.noba-modelle.de

+++ RATIMO Rainer-Tielke-Modellbau +++

Rainer Tielke reports back that there is currently no time for new developments. The current focus is on production and also on solving the last tasks left with the addresses to the models of the makeshift MDyg 986 pack wagon. So we will report further in due course.

www.ratimo-z.de

www.rainer-tielke-modellbau.de

+++ Rolfs Laedchen +++

No new products will be introduced in the spring; this is planned for later in the year. For the time being, there is also a large private order to be filled, as this manufacturer advised us.

www.rolfs-laedchen.de

+++ Scale Things +++

A stroke of luck for Zetties was a tip from Dale Schultz from the United States, who is dedicated to the H0 scale, reads our magazine and builds according to his own needs.



In doing so, he also thought of the Zetties and the accessories that are missing or compromised in our size.

The result is 3D-printed Indusi track magnets in the size 6.8 x 1.12 x 1.15 mm, which are assembled into 15 pieces and sold by his son under the trade name Scale Things.

We are currently trying to find a distribution partner for Germany.

Indusi slide magnets in scale design will be available as 3D printed products from Scale Things in the future. Foto: Scale Things

The track magnets offered here protect preliminary and main signals as well as slow speed points. In the warning position, they activate the inductive (type I 60), nowadays punctiform (type PZB 90) train control of the leading traction unit by induction via different frequencies.

This then monitors the reaction of the locomotive driver (keystroke "Indusi watchful") and his braking and driving behaviour so that the train only continues at the instructed speed, stops in time before a signal and cannot run over it by mistake when stopping. This short explanation should already explain why this component should not be missing on the modeller's layout.



A track magnet is also mounted here for demonstration purposes, but at this point we are actually talking about the hectometre stones, which are also newly available from Scale Things: To the right of the track are two already coloured stones with an odd number of kilometres, to the left a blank with an even mark of 85.6 km.

We would also like to introduce another new product, this one also in an uncoloured and painted version: hectometre stones, which were placed every 200 metres at a distance of 2.5 metres from the track in the prototype, should also not be missing on model railway layouts.

The stones are supplied with a continuous kilometre scale from 85.0 to 85.9 (in 100 m steps), which also allows an alternating installation on both sides of a single-track line. The bases are dimensioned in such a way that they can be recessed, sanded in, or shortened to find the perfect installation solution at home.

scalethings.square.site

+++ Schrax +++

Zinc tubs (made of hot-dip galvanised sheet metal) appeared from the end of the 18th century and can thus be used from Era I onwards. However, the new model made of resin can still be used in gardens today, where some people use it as a water scoop (under a hand lever pump), or even as a flower tub.

Complementing the brilliant white snow sand from the same company, a glass dust for imitating snow is now also available. Less brilliant, but with a beautiful play of light due to the many broken surfaces, it offers many attractions.

Its small grain size of less than 0.15 mm also makes it particularly suitable for Z gauge, where it can also be mixed with other materials. Supplied in practical wide-necked bottles of 0.5 (750 g) and 1 litre (1.5 kg).

www.schrax.com

+++ SMZ Sondermodelle Z +++

On the pages of this small series manufacturer, two ongoing projects have been published that we have not yet reported on. The first is aimed at the fans of the ÖBB and brings them a covered freight wagon of the type Gbs according to UIC dimensions (axle base 8.0 m / loading length 12.5 m).

The model has a mahogany brown paint scheme and Epoch IV inscriptions. The scope of delivery also includes a Steyr 91 truck of the Austrian Post and two telephone boxes typical for the country.

In the prototype as well as now in the model, the RAe 4/8 "Churchillpfeil" (Churchill arrow double railcar) is now also being produced due to its great success. The 1939 RAe 4/8 "Churchillpfeil" (Churchill arrow double railcar), which was first used as RAe 4/8 301 and later re-designated as RAe 4/8 1021, is a completely new development. It receives a sophisticated close coupling kinematics, which is why it requires a minimum radius of 195 mm (R 2).

www.sondermodellez.at

+++ Spur Z Ladegut Josephine Küpper +++

For private reasons, a creative break is indicated here at the moment. As soon as the current challenges have been overcome, it will be back to business as usual.

www.spurzladegut.de

+++ Trafofuchs +++

In 2021, there will be many new and improved offers. An important development step that has been initiated but is far from being completed is the in-house 3D CAD construction of figures.

Instead of rebuilding and modelling blanks, in future they will be created on screen to match the scene, printed out and then painted.

This step seemed logical and purposeful in order to make sic of the fluctuating and hardly calculable availability of the large-scale series figures that have been necessary for production up to now. In the end, all figures will be "100 % Trafofuchs."



The corpulent customer may bite into his hot sausage roll, which, like himself, now comes from the 3D printer. The salesman stands contentedly next to it. Photo: Trafofuchs



The hikers (photo, above), the pigs and the farmer with the feed bucket (photo, below left) as well as the Bavarian railway staff (photo, below right) are also new products that are already designed by CAD and output in 3D printing. Photos: Trafofuchs

With this introduction, we now take a look at the new figures for spring 2021: There at the snack stand, the corpulent customer bites directly into his hot sausage roll. The dark-skinned salesman at the wagon decorated with stars and stripes looks on with satisfaction.

Among the walkers, on the other hand, one half looks around to check whether his partner can still keep up, while another woman is a little impatient, holding her arms at her sides. Meanwhile, her husband is calmly looking through his binoculars.

The pigs, which used to just lie in the wallow at Trafofuchs, have now left it. The reason is obviously that the farmer has joined them with the feed bucket. The wild form of these domestic animals consists of the new composition of a pack of three wild boars with two piglets. These figures are now also considerably more detailed and thus suitable for macro photography than their predecessors.

For Era I, Birgit Foken-Brock has designed typical warehouse workers with caps, waistcoats, or braces (suspenders). The royal Bavarian railway staff for the passenger train stop placed next to it replaces the former conductors in the programme. If desired, these figures can of course also be dressed in Prussian uniforms.

www.trafofuchs.de

+++ Velmo +++

The new products presented in 2020 in ZFI's virtual Altenbeken have not yet been fully developed, which is why no further new products are planned for this year. Moreover, the order situation is so exceptionally

high at the moment that Claudius Veit does not expect to be able to resume further development at all before the summer of 2021, due to full utilisation of his production capacity.

www.velmo.de

+++ Wilhelmshavener Modelle (Möwe-Verlag) +++

When asked, the owner of Möwe-Verlag confirmed that there are currently no new products to report. However, it should also be added that the supplier pages are always kept up to date, and new products can be found there promptly.

www.papermod.com

+++ ZCustomizer +++

In honour of the Frauenau glass factory, Sascha Braun has ventured to build a Bavarian freight wagon O (open type). The glass load included in the scope of delivery comes from Gauge Z load Josephine Küpper from Aachen.



A Bavarian freight car O (open type) with old glass cargo appears new at Zcustomizer and pays tribute to the Frauenau glass factory. Photo: Zcustomizer

The car with the road number 3421 has a metal running gear and bar buffers typical of the era. Its coupler is fully compatible with those of Märklin.

www.zcustomizer.de

+++ ZDC - Z Dream Collection +++

Michael Koppensteiner has been an enthusiastic Z-gauge railway enthusiast since 1983. Under the name mentioned in the headline, he is now also starting up as a small-series manufacturer, who, in cooperation with Andreas Herzog (Kastenbahner; see there), makes finished models from the latter's signal kits.

The first model is an ÖBB distant signal as it was used from 1980. This has already been shown in the licensed **Trainini®**-Facebook group (address for registration in the imprint). Of course, new models will also be added to our own pages.

From spring onwards, further signals based on ÖBB models will follow, all of them as functioning finished models. Initially, the preliminary, main and signal combinations of the prototypes introduced in 1980, and, subsequently, those older ones from 1956, are planned.

In spring, planned for about April or May 2021, signals of the SBB will follow. In order to show what can be done with a steady hand, a combination of advance/transfer signals (2x green and 3 x yellow) will be presented first.

The entire offer is primarily aimed at those Zetties who do not have the confidence to build a kit themselves, or do not think they have the necessary skills.

Until further notice, work will be done to order, i.e., there will be no delivery from stock, but production will only occur when an order has been placed.



In cooperation with Kastenbahner, functional ÖBB light signals will be available as finished models at ZDC in the future. Photo: ZDC – Z-Dream Collection



A small foretaste of the planned programme on locomotive models with special designs is given by the Taurus for the Beethoven anniversary year. Photo: ZDC – Z-Dream Collection

Another product field is Austrian designs based on Märklin vehicles because of the limited supply of large-series models. The outlook, however, is to close gaps in the future with their own models made from 3D printing, brass or material mixtures.

Thus, within the framework of a cooperation, some Taurus models from the GySEV series have already been created as samples for the projects: Sisi, Kaiser Franz, Beethoven 2020, Liszt, and GySEV Cargo. Each model is limited to 10 units because of the licences acquired (and the costs involved).

Delivery is within four to eight weeks from the date of order. These models are delivered to the customer in a wooden box with an enclosed explanation of the significance of the production.

z-dream-collection.jimdosite.com
[zdc.austria\(at\)gmail.com](mailto:zdc.austria(at)gmail.com)

+++ ZetNa220 +++

As a result of the pandemic waves, there were delays in orders for the small cattle wagon V 23 of the Deutsche Bundesbahn, the design of which went back to the Deutsche Reichsbahn. Optimisation trials were also still underway for the sliding images and the process of attaching them to the openwork wagon body.

This is also the reason why we are not yet able to present these models in the magazine. However, interested parties should make a note of this type of car, so that they can order it while it is still available. We are also planning a report on this, which will include prototype photos as well as train formation suggestions.

Further new developments are running in parallel, but we will not announce them until later, when valid timetables are possible again.

[Zetna220\(at\)gmail.com](mailto:Zetna220(at)gmail.com)



Tests and optimisations for applying the lettering were still underway on the DB version of the V 23 cattle car. Soon, production and development will continue. Photo: ZetNa220

+++ Z-Doktor Modellbau +++

Björn Plutka is currently very busy, which is why it seems to have become quieter on his web pages. For capacity reasons, a project that we started with him will be continued together with NoBa-Modelle after both sides have agreed.

In the meantime, the Köf 1 from Z-Doktor Modellbau is ready to roll and will learn to drive itself. In any case, the model has already become more attractive. A cooperation with Archistories is already bearing fruit, resulting in additional parts and extensions to architectural kits.

The first fruits of this work are a church interior for their US model. It consists of pews and an altar with hollow candles. The altar and candles have a wire feed-through for SMD type 0201 LEDs, which can even be used to illuminate them.

The new accessory pack for the Archistories windmill is also appealing. It consists of a 3D-printed ladder truck, which we already showed at the first presentation of this manufacturer (and which now makes it into the programme), and a load of full-scale flour sacks.

www.z-doktor.de

+++ Zmodell +++

After the silver-coloured version of the US electric locomotive GG-1 from the inventory of the PPR, Alex Mark has now also tackled the blue Conrail version (item no. 99495) of this legendary machine. It is based on the Märklin model 88491, which is considered to be the “American crocodile”.

But a number of improvements set it apart from the basic design: warm white instead of yellow LEDs on a newly developed circuit board, 16 gram weight insert (metal) to increase traction and removal of the hole for the former catenary switch in the roof.



Foto: Zmodell

In addition, the locomotive was fitted with a Velmo LDS609646 digital decoder, which also has separate cab lighting (separately selectable via functions F2 and F3). An analogue version with original board, but in this case without interior lighting, is available. The markings are printed with UV-curing technology and sealed with transparent semi-gloss varnish.

www.facebook.com/Zmodelltrains/
[zmodell\(at\)ukr.net](mailto:zmodell(at)ukr.net)

+++ Z-Panzer (Andrew Hart) +++

In recent years, Andrew Hart has focused on developing exclusive models based on military models exclusively for AZL. This is also the current focus of the company's activities and in this context, for example, a heavy tank transport of the US Army was created.



This armoured heavy transport is another exclusive model for AZL, which has been the focus of its activities for several years. Photo: Z-Panzer

In the own programme the K 5 railway gun train could be completed. It comes in a stylish wooden case with a numbered brass plaque and an enclosed information booklet. The set also includes two light trucks Opel Blitz for freight and radio.





Individual of the described components of the K 5 railway gun pack can be seen in the photos above and on page 91 below. All photos: Z-Panzer

The train itself consists of the following units: K 5 railway gun as well as one ammunition feeder, one bullet (for projectiles), one cartridge (cartridges), one tempering (temperature control), one workshop, one storage and one flak wagon with mounted Flak 38. A compartment has been left free inside the cassette for a matching WR 360 C 14 diesel locomotive (almost perfectly reproduced on the basis of Märklin 88771).

www.z-panzer.com

+++ Ztrack +++

Available from our partner magazine, but also distributed in Germany by Case-Hobbies (<http://case-hobbies.de/>), is the "Ztrains Grassinator." This electrostatic grass applicator from John Cubbin has recently been improved again and now has the following product features:

- Constant direct current supply with 12 V (via plug-in power supply unit instead of battery),
- 15 kV voltage field for applying the charged fibres into the glue bed,
- extension cable for power supply,
- enclosed potter's needle for the opposite pole (more stable and steady),
- cork for safe storage of the needle tip,

- clear beaker with mesh sieve (for continuous control of the filling level),
- two additional mesh sieves are included as accessories and
- a lid for closing the beaker for storing grass fibres.

The device has already been tested in the United States by a renowned trade magazine. The report can be read here: <http://modelrailroadnews.com/grassinator-static-grass-applicator/>.

www.ztrack.com
www.ztrackcenter.com

+++ 1zu220-Shop +++

In the next few weeks, the NOHAB diesel locomotive already mentioned at FR Freudenreich Feinwerktechnik and exclusively issued for the 1zu220-Shop should be delivered as a silver-blue V170 of BOB (art.-no. 49.126.01).



Der Grassinator. Foto: Ztrains

Also, in preparation is the four-part freight car set for Era III (FR-49-SET-EP3-1), manufactured just as exclusively by the same manufacturer, consisting of the following models:

- Covered wagon "Oppeln" with platform as chrome oxide green construction train wagon,
- open goods wagon Omm32 (ex-Linz),
- FS Tipo F pointed roof car in aluminium silver with EUROP inscriptions, and
- box car Gms 54.



The Kaelble KV 632 ZB tractor presented for Culemeyer-Transporte in the November 2020 issue is also available. However, it has not yet been decided in which form it will be offered to customers.

www.1zu220-shop.de

Photo left:

It is not yet decided in which form the tractor Kaelble KV 632 ZB will be delivered. The model in the photo no longer corresponds to the manufacturer's new item condition, as it has been slightly optimised in terms of colour and subsequently labelled.



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Durchmesser unten: 42mm
Höhe bis First: 75mm
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Art. 702181

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

We are currently receiving significantly more letters from readers than usual. As editors, we are naturally very pleased about this and would like to encourage our readers to continue to do so eagerly. However, not every letter to the editor is intended for publication by its sender or suitable in terms of content.

We do not want to make our own selection here and possibly hold back individual concerns – every question will also be answered by us personally within a few days! Frequently, these are very individual concerns about special products, their handling or availability, as well as craft techniques, which can only be clarified in direct dialogue.

Trainini

Spur Z komplett

From the requests and questions that we receive, we conclude that many new readers have found their way to us who have completely rediscovered model railways.

We are happy to help them to expand this passion and to consolidate it, permanently.

An important help in mutual learning and instruction can also be the web pages of the German language Facebook group licensed by us, which is listed in the imprint of each issue. Prior registration is required for this, but the data is not collected or processed by our magazine.

Newsletter functions are missing:

I've come across your great magazine time and again and now that I'm getting back into my Z passion, I wanted to sign up for the newsletter/magazine, but I can't find a contact form. Does that mean that you have to write down the dates for the new releases of the magazine yourself?

Presumably, the GDPR will put a stop to all well-intentioned approaches here, as well.

Michael Meißner, Dasing

Editor's reply: We do not see the relevant regulations of the GDPR as an obstacle here. The prerequisite for such a use of data would be, on the one hand, that the respective reader actively enters them into a distribution list, and, on the other hand, that permission for processing is granted for a specific purpose, which can also be revoked at any time. On the other hand, we already have obligations to fulfil today, to state in a data protection statement which data may be collected and processed.

The obstacle is rather different: Our pages are in urgent need of revision and a changeover to modern technology. Since all Trainini® staff and helpers work exclusively on a voluntary basis, it has, unfortunately, not been possible to complete any of the approaches so far.

A project is currently underway in which all editors are contributing ideas, photos, suggestions and attempts to fill in content, in addition to our technically experienced person, in charge. We are therefore very confident that we will be able to present our readers with the long-awaited and already announced new appearance in the first half of 2021. This should also be accompanied by a functional expansion in some places, which will also make newsletter functions possible.



Questions about digital operation of Z gauge:

First of all, I wish you all a happy and healthy New Year.

Next, I have a wish. After the successful series “Electrics” and the test film on the digitised diesel locomotive (NOHAB from FR Freudenreich Feinwerktechnik; editor's note), would it be possible to consider a series on the digitisation of the Z gauge?

Or, perhaps to list a literature and/or supplier list if the effort is too high. I am unsure, for example, about the high voltages involved and some other operating data, and whether it makes sense at all.

Prof. Dr. Ulrich Langer, Aachen

Editor's reply: We are planning a special issue on landscaping and have also discussed another one on the topic of digitalisation within the editorial team. A smaller digital issue is still planned for this year, but it does not meet your core concerns.

However, we are always open to readers' suggestions. All we have to do is look for suitable authors, and we can already think of a few in the digital field. However, we still need some preparation, and, if necessary, prior agreement on the creation of images and the coordination or delimitation of content. However, this is hardly calculable in these months, which is why we have to try your patience at this point.

Vacuum cleaner tip from our own ranks:

In addition to our report on cleaning and maintaining a Märklin transfer table in the last issue, our translator Martin Stercken has another tip for our readers. A small keyboard vacuum with USB connection can do a good job on a Z gauge layout.

Such miniature devices can reach areas that are difficult to access and free them from dust without damaging buildings, trees, figures or vehicles. Various suction pipe attachments can certainly help them to do this.

He found a selection of such devices via the following link (there are many other suppliers): https://www.aliexpress.com/wholesale?catId=0&initiative_id=SB_20210220124008&SearchText=USB+keyboard+vacuum.

Exhibition of the Stammtisch Untereschbach cancelled:

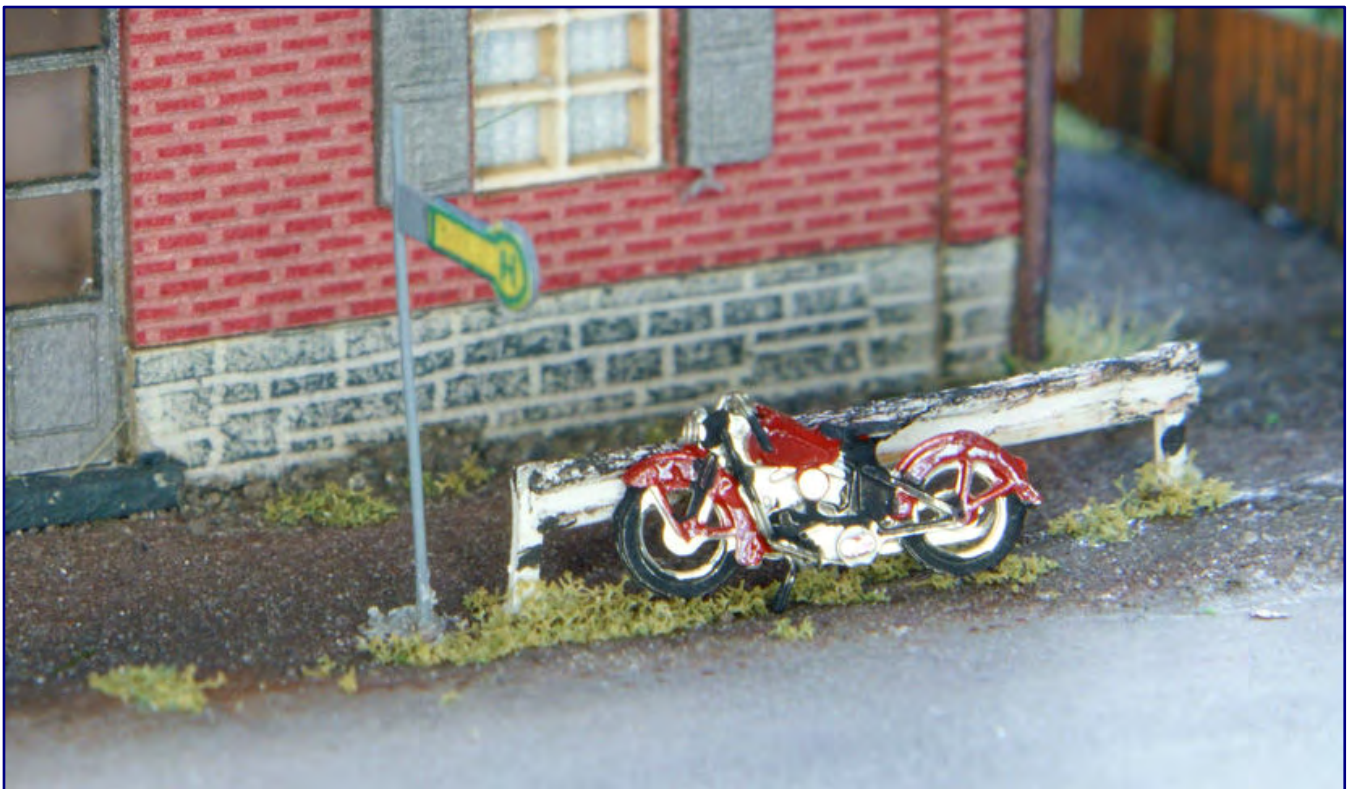
The exhibition of the Untereschbach regulars' table, which was planned for the 20th anniversary of its presentation in the railway magazine in Dieringhausen, in the "Alte Dombach" (paper museum) in Bergisch Gladbach, unfortunately has to be cancelled.

Although there is still some time to go until June, the museum management did not see any secure planning here at the moment. So, probably to everyone's regret, the difficult decision was made, putting our health first.

The association supplemented the notice of cancellation with the promise that the event, which has also been advertised in this magazine for some time, will be held at a later date, which will of course also be communicated here.

Finest models new from Amsterdam:

The Dutch accessories specialist has delivered some new items and is now delighting the Zetties again with the finest models. Among them is the civilian US motorbike (item no. 322.038), which of course also found its fans in Europe. It will find many uses on the layout, both individually (and visibly displayed) and with the addition of a rider.



The civilian US motorbike (item no. 322.038) offers many possibilities with its fine design: Whether provided with a driver on the street or, well visible to the viewer, parked in front of a railway station or a pub, it should only provide a good look at its filigree details.

By the way, the ladder set (322.016) is available again, as we need it again and again in various places. In December 2020 we showed how it can be used to equip a fire engine.

The Ford 5000 tractor (322.030) and its matching slurry tanker (322.031) are also making their debut. In view of the abundance of tractors we have been presented with in the last ten years, it is worth pointing

out that the Ford model stands out at first glance, especially in terms of colour, and is, therefore, unlikely to be overlooked.



The Ford 5000 tractor (322.030) and the attached slurry tanker (322.031) seamlessly continue the series of excellently detailed and extremely fine Artitec model vehicles. As the slurry tanker also fills a gap in the range, we nominate it for the best new releases of 2021 in the accessories category.

We probably don't need to say so much about the slurry tanker, because our tractors hardly had anything to hang on to before, the range here remained very manageable overall. The Artitec new product, however, stands out from this previously clear third programme of trailers for the stinking freight.

This is not only due to the usual detailed design with the finest details but also to the convincing colouring including lettering. We honour this particularly high-quality design with a nomination for the best new releases of the year 2021 in the accessories category.

Business development at Märklin:

At the turn of the year, the owners and managing directors of Märklin looked back with satisfaction and confidently into the new year. Despite all the problems that last year brought with it, all in all it went extremely satisfactorily.

Managing Partner Florian Sieber commented as follows: "Due to the lockdown last spring, we have to correct our forecast for the turnover of the 2019/2020 financial year from around 117 million euros to 112 million euros. Currently, we are planning to slightly exceed the previous year's revenue figures of 112 million euros for the 2020/2021 financial year."

However, this is difficult to assess due to the ongoing closure of toy shops. However, the fact that Märklin had 40 % more orders on hand at the end of December 2020, than at the same time in the previous year, is a very positive factor for this outlook.



The restrictions of the pandemic have awakened the model railway from a slumber, just as Märklin itself awakened the 44 1315, pictured here after its arrival in Göppingen in mid-September 2018, which is also unrecognisable today after restoration work has been carried out.

“Of course, the decisive factor is how quickly we can produce the goods and how the trade situation will develop in our core markets”, says Florian Sieber. For its part, Märklin has done everything to be able to meet the high demand: Investments have been made in new machines to be able to expand capacity, and the staff in Győr, Hungary, has grown by 5 %, with the same increase planned for the year just started.

According to a press release, the growth in orders does not stop at the Z gauge. This also corresponds to our impression, because the questions from readers about problems, techniques and availability of the products presented in earlier articles have recently increased to five or six times the usual volume.

The new or rediscovered love for model railways naturally pleases us as much as Märklin. That is why we add that Märklin, according to its own information, has been able to grow both in Germany, and, especially, in Benelux and the USA.

First trade fair cancellation 2021:

The first spring fair, Faszination Modellbahn, has now unfortunately been cancelled. It was planned for 12 to 14 March 2021 in the Maimarkthalle Mannheim. But, until the very end, it was not foreseeable that the restrictions imposed on the trade and industry would allow such a mass event to be held.

“We regret the decision very much, but the reasons are obvious: it is based on common sense and a sense of responsibility for the model railway community, whose health is close to our hearts. After consultation with the representatives of the industry, the current uncertainties do not allow for an event on the usual scale”, says authorised signatory Andreas Wittur.

Hopes now rest on the one hand on the 8th Faszination Modellbahn from 11 to 13 March 2022 in Mannheim, and the 19th Faszination Modellbau in Friedrichshafen, in conjunction with the Echtdampf-Hallentreffen, from 5 to 7 November 2021.

Loss in the Spur Z community:

As we have been informed, Jürgen Brehme, still known to many long-time Spur Z fans, died unexpectedly on 4 November 2020, as a result of cancer. He was 77 years old. He was a trained freelance journalist and founded the Baden-Württemberg magazine in the seventies, which presented holiday resorts and special features in the "Ländle," and also up into Rhineland-Palatinate.

In the eighties, Ideen-Magazin was created under his direction, as well as the Idee-Magazin 2000, at the turn of the millennium. He had already retired from this a few years ago. Through that time, he had also appeared at exhibitions with layouts, conversions, services, and all conceivable products around Z gauge.

His activities also included the sale of some well-known accessories, including bellows for the Märklin ICE 3 and various mobile home, caravan and car models in 1:220 scale.

According to the information given to us, he was the last survivor of his family. His only remaining kin was his stepdaughter. We wish all the people who were close to him or were his fans much strength in getting through his loss.

Märklin deliveries in February:

Since the publication of the last issue, Märklin has again been able to deliver some new items. Today, we start with the green electric locomotive series RE 14 of the SJ (Item no. 88484). With the lettering Green Cargo it complements the already delivered wagon set 82533.

Märklin has taken great pains with this model. The present housing shape does not exactly match the accident-optimised version of the TRAXX 2E that the prototype shows.

However, by printing the handrails typical of the prototype, the model could be brought as close as possible to it.

The hole in the roof for the former changeover screw (catenary or track power) has now disappeared. The lighting changes from warm white to red depending on the direction of travel, while a bell-shaped armature motor provides power.

The circuit board seems to have been adjusted as well, because it was the cause of crooked couplings in earlier models, which are no longer noticeable in the new model delivered last year.



On the model of the Swedish electric locomotive of the RE 14 series (item no. 88484) Märklin has made some minor improvements, but also has produced an almost perfect print. Photo: Jörg Erkel (1zu220-Shop)

Also available is the IC compartment car according to Eurofima design for the DB (87340) in product colour. The excellent quality of the paintwork (wash edge / end face) and the elaborate printing (door handles / seals / car signs) makes us look forward to the other cars in the series which began last year, and that are still missing.



The Eurofima 1st class compartment coach (87340) is excellently printed in the product colours of the Bundesbahn (photo, above), while the private gas wagon from Reichsbahn times (82401) impresses with a light patina (photo, below), but it is, unfortunately, already sold out at the factory. Photos: Jörg Erkel (1zu220-Shop)

The private gas wagon with brakeman's cab, set by the Deutsche Reichsbahn (82401), has a small frustration factor. This wagon with two horizontal gas tanks for carbonic acid transport around 1932 was announced with the autumn new products 2020, and has now been delivered.

As this unusual looking model presents itself with a light patina ex works, it obviously found good demand. It is already sold-out ex-works, as well as at many dealers. Those who would like to add one or multiple copies of this wagon to their trains, should therefore hurry and not miss an opportunity that presents itself.

New Full Throttle Collector Model:

The US small series supplier has put together a new collector's pack: Called "Farm Feeds" (item no. FT-COL53), the customer will receive a duo of the previously released FT-1022 and FT-1038 models, which are SHPX 62248 (Wayne Feeds) and 61323 (Purina Chows).

Vote for model of the year!

For the first time in the long history of the railway magazine, the 2021 readers' choice for the models of the year will also take place in a separate Z-gauge category "10 - Vehicles." A voting card is enclosed with the current issue 3/2021, which is currently available from the publisher, specialist dealers or station bookshops.

Likewise, readers can also vote on a specially set up website until 31 March 2021, and lend weight to their impression, as well as, the importance of this award for our size by participating:

<https://bruckmannverlaggmbhverlagshaus.survalyzer.eu/qoygmudkqq?l=de>

The following 1:220 scale models will compete against each other in category 10 (order of nomination):

1. Schi-Stra-Bus NWF BS 300 of the DB (1zu220-Shop)
2. Tenderdampflok Oldenburgische T 2 (Bahls-Modelleisenbahnen)
3. Offener Güterwagen Omm 32 „Linz“ of the DB (FR Freudenreich Feinwerktechnik)
4. Schnellzugdampflok Baureihe 03¹⁰ with re-constructed boiler of the DB (Märklin)
5. Doppeldiesellok V 188 (NoBa-Modelle)
6. Heavy duty wagon with transformer of the JR (Rokuhan)
7. Kleinlokomotive Kö 1 of the DB (Z-Doktor Modellbau)
8. Diesellokomotive Baureihe 120 „Taigatrommel“ of the DR (Zmodell)



Cover illustration: Alba Publikation / Geramond

Incidentally, there is no obligation to cast a vote in all categories. Those may also be left out in which the readers do not feel able to vote.

We, therefore, call on all readers at this point to participate actively with their vote and to lead this readers' choice of a representative picture, as well as a deserving winner!

Once again, great prizes will be raffled among the participants, among them also individually track-width related models.

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Featured Freight Cars by FR

Manufacturer Portrait from Portugal
Ten Tips for Beginners' Success

Introduction

Dear Readers,

Spring is just around the corner, but our hobby season won't be coming to an end any time soon. The other day a model railway friend said to me: "We won't get this much time for modelling again so soon!"

I should enjoy this time; I fully agree with him. But it actually hasn't really become more time, in my opinion. It's more like a deliberate shift. If contacts only take place virtually instead of in person, then I'd rather turn more to my hobby, and stay healthy.

It's all about spending spare time in a meaningful way, being happy and not being miserable. Parallel to moving my contacts into the virtual world, their number has also increased significantly.

All of us in the editorial team are probably in much closer contact with readers, but also dealers and manufacturers, than was otherwise the case. This takes time and sometimes it takes longer for us to respond – but it is a good sign!

And we would like to align the topics of this year's issue accordingly: test or introduce new products, provide suggestions from the world of the prototype and pick up many, many practical topics. We feel obligated to the many newcomers and returners who also contact us. Hopefully, this edition also shows that.

Our reader Kay Anbuhl shares ten easy-to-implement suggestions that can easily bring a model railroader's typical starting material up to date. The introduction of new car models and an aircraft model provides an overview of the current market situation. And there is a lot going on here, too, because more new products are in the starting blocks for April.

This year, we are also keen to present manufacturer portraits of suppliers from the low-volume production sector. Of course, we can't include them all here, but we have tried to create a representative picture to show the diversity of segments and gaps in the market.

Here, they should be representational, provide insights into what they do and think, and also counteract the crisis: Although model railways have been experiencing a strong upswing during the past year, this has by no means reached everyone.

Especially those who live largely from the trade fair business and their presence at exhibitions are in danger of being forgotten too quickly. This is also shown by our current announcements. In order to maintain our diversity in this area, we appeal to our readers in particular.

With these introductory words, which roughly summarise the contents of this issue, I would now like to conclude. My colleagues on the editorial board and I hope you enjoy reading this edition and we hope that you will also continue to actively participate!

Sin-Z-erely,

Holger Späing



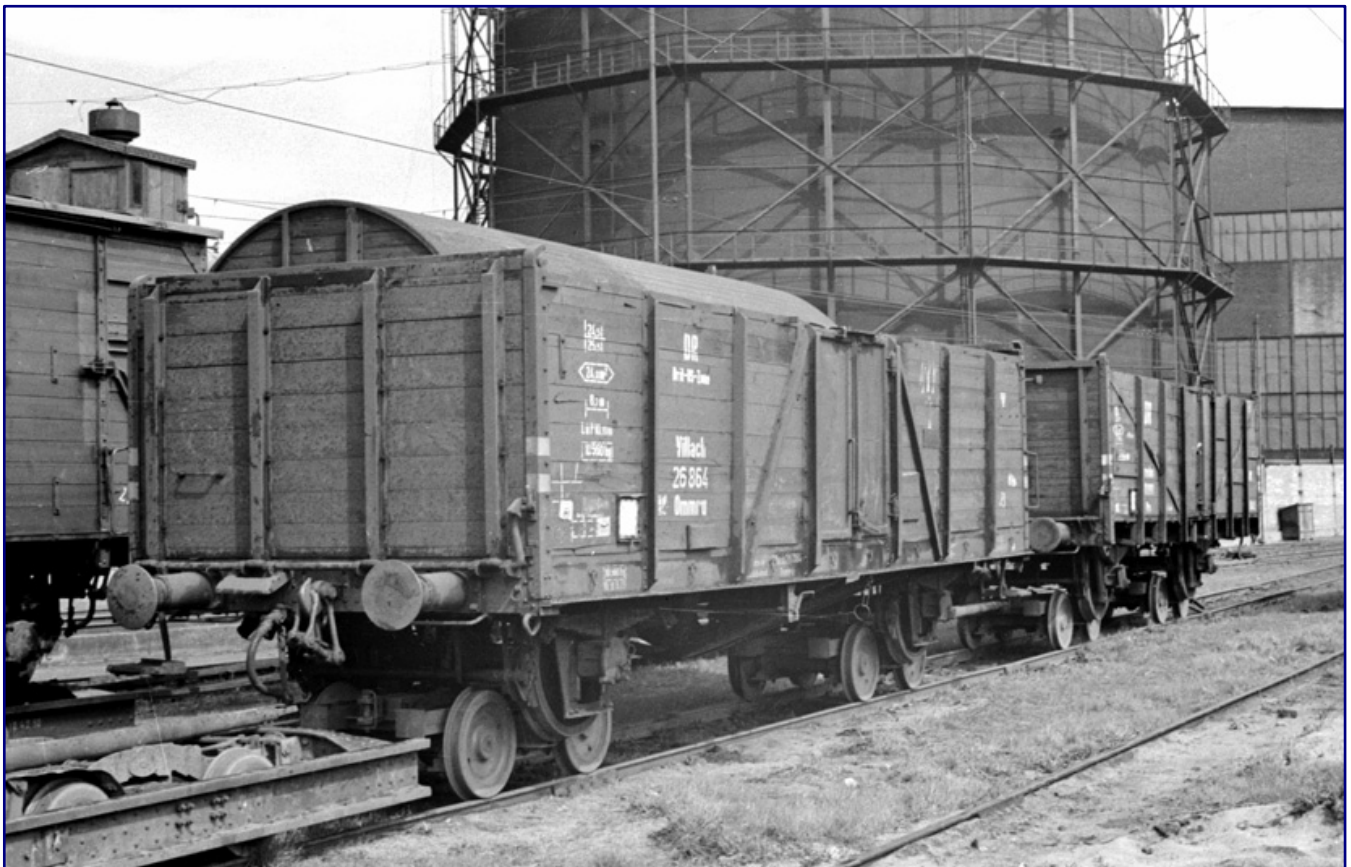
Holger Späing
Editor-in-chief

Ommr 33 from FR Freudenreich Feinwerktechnik A successful Wagon, both large and small

During the Second World War the Ommru Villach wagons were built, which were the most common type of open freight wagon in Germany after the older association types. However, models of this once so important wagon were scarce in 1:220 scale. FR Freudenreich Feinwerktechnik took on this prototype and created a miniature that is also suitable for larger quantities – just like the large prototype.

The open freight cars Ommru Villach were procured by the Reichsbahn from 1939 parallel to the Ommr Linz, whose prototype and model we had already presented in **Trainini®** 4/2020. Both car types were closely related to each other and so it was obvious for FR Freudenreich Feinwerktechnik to implement both of them at the same time.

Their main difference were the side walls: While the Linz had a height of only 1.0 metre and was removable, the Villach had 1.55 metre high side walls that were permanently attached to the underframe. They were procured only with external longitudinal girders and a spatial support system, which was also adopted for the Linz with its second series. We had already noted this in the aforementioned article.



Noteworthy is this Villach type Ommru car, which still bears the ownership markings of the Brit-US zone in 1950. It was taken on the former Haspe-Voerde-Breckerfeld narrow gauge railway, which supplied various small iron workshops. At Haspe station on the Wuppertal – Dortmund railway line was a rolling trestle for transferring standard gauge goods wagons. Photo: Gerd Wolff, Eisenbahnstiftung

The loading space of both types, which were developed at about the same time, was one metre longer than that of older Om coaches, and the loading weight had also been significantly increased. However, this could only be utilised if the superstructure of the routes also allowed it. At the time of commissioning, the maximum axle load was 16 tonnes.

The Villach was built with and without a handbrake, which was located in an all-steel brakeman's cab on a 70 cm long platform. From 1943, the platform was shortened by 20 cm to 50 cm, which was also noticeable in the length over the buffers.

Large numbers of this type of wagon were procured from 1941 onwards. Some of these over 49,000 wagons built before 1945 already had roller bearings ex-works, and others were retrofitted later. They were braked by a Hildebrand-Knorr goods train brake (HiK-G type).



After the Second World War, the wagons were scattered all over Europe and were partially rebuilt. The Ommru coach 43-72-37 remained with the Reichsbahn in the DDR and is shown there in 1959 in its original condition. Photo: Gerhard Illner, Eisenbahnstiftung

After the association designs, the Ommru Villach became the most common open freight wagon in Germany. Many of them were suitable for the transition to the Russian broad gauge, which was indicated by the secondary class symbol r. From 1938, the u meant unsuitable for carrying crews or vehicles of the Wehrmacht.

About 13,500 of them entered the stock of the Bundesbahn, which was reproduced with the present models. There they were classified as Omm(r) 33 and continued to be used as an important backbone of the car stock.

When the EUROP Park was built at the beginning of the 1950s, initially together with the SNCF, and later with other European state railways, the DB placed around 10,000 of these open wagons there.

They were only displaced by more modern wagons of the new and conversion programmes once these were available in sufficiently large numbers. Between 1957 and 1961, 5,352 examples received a body with sheet metal walls according to UIC dimensions and UIC door latches, which led to the new designation Omm(r) 43.

This rebuilding also included the replacement of the spatial by a planar sprinkler system and the conversion to roller bearing wheelsets of type 22 (former plain bearing wheelsets). The single hooks were conducive here, and so DB retained them.

With the completion of this modernisation programme, some 6,999 Omm(r) 33s remained in the Bundesbahn stock, and their withdrawal from service began in the mid-1960s. The hand brake cars had long since lost their brakeman's cabs, and most of the cars that had not been converted still had roller bearings.

The end of this type of wagon came quite quickly: While 3,168 wagons were still in service on 31 December 1966, only 241 were still in service two years later. In the meantime, the wagons were designated E 028. The last seven wagons were withdrawn from service between 1970 and 1974.

At the Reichsbahn in the DDR, the wagons taken over were to be assigned to different types depending on their gauge capability: The upgradeable models were to become Ommru 39, the others Ommu 43, however, this was not consistently done.



On model railways, too, we cannot avoid this important type of car design if we closely follow the prototype. Against this background, the decision of FR Freudenreich Feinwerktechnik is very welcome. Photo: Dirk Rohwerder

In the east, too, the car bodies were rebuilt, partly in wood similar to the "Klagenfurt", and, from 1954 onwards, for about 500 examples with sheet metal walls. Also, to be dated from 1954 is the inclusion in reconstructions, in which they were rebuilt, based on the UIC standard type. In contrast to the DB, however, they retained their old doors and the spatial explosive structure. They were designated unchanged by the DR as Ommu 43.

A differentiation was only brought about by the UIC type designation: non-converted wagons were now called EI(-u) [5560], converted ones EI(-u) [5100].

The models from Freudenreich

At the same time as the Linz presented last year, FR Freudenreich Feinwerktechnik also offered a Villach. For the time being, however, it could only be purchased as a single wagon in a limited edition within the framework of the planned meeting in Altenbeken. This special model was based on a DR wagon, a DB version did not follow for the time being.

With the spring novelties 2021, Harald Thom-Freudenreich has now followed up with two wagon packs for the Bundesbahn. With this, we would like to keep our promise from the April 2020 issue to examine and evaluate this version in detail.



Two versions for Era III of the German Federal Railways are available today for evaluation: two coaches with regular service inscriptions (item no. 49.338.02; front) and two others with those for the EUROP coach stock (49.338.12; rear).

The boxes each refer to a run of 60 packs, i.e., 120 wagons in total. However, this does not refer to each of the two product combinations, but to the total number of both DB versions: According to the manufacturer, each of the two versions was produced exactly 30 times, so they only come together to the stated 60 pieces.

We now have a two-pack as Era III version with standard inscriptions (item no. 49.338.02) and for the EUROP car stock (49.338.12). According to the box inserts, the cars are “Ommu 33 (ex-Villach)”, which would not be correct designations for the Bundesbahn.

However, before we go into the labelling further, we can already state at this point that this is a printing error and that the models are indeed correctly labelled. And so, after opening the transparent boxes, they immediately reveal the usual good impressions of this manufacturer: clean workmanship, coherent photos and successful printing.



In order to make the differences clear to readers with little knowledge of prototypes, we have compared one wagon of each version here: regular freight wagon markings (left) and DB EUROP markings (right).

Data and dimensions of the open freight wagon Ommr 33 (ex Villach):

	<u>Prototype</u>	<u>1:220</u>	<u>Modell</u>
Length over buffers	10.100 mm	45,9 mm	46,0 mm
Largest width	3.020 mm	13,7 mm	12,8 mm
Height over rail head *	2.785 mm	12,7 mm	12,8 mm
Body length	8.800 mm	40,0 mm	40,9 mm
Wheelbase	6.000 mm	27,3 mm	26,8 mm
Wheel diameter	940 mm	4,3 mm	4,4 mm
Net weight	10.500 kg	---	5 g
Years built	1939 – 1945		
Quantity produced	49.115		
Retirement	bis 1974		

* over wagon body (without fasteners)

Taking the important measurements and converting them to the prototype values also proves precision landings: There is no better way to do it. Regarding the Linz, we had said that we had never before seen such a correctly converted wagon in a test. Now we know that this model is no longer alone!

In its construction and mixed construction, the usual principles of FR Freudenreich Feinwerktechnik are continued: models with a sufficiently high expectation of quantity receive a box made of five injection-moulded parts (polystyrene) glued together, the chassis consists of etched, folded and soldered metal parts.

They again provide a sufficiently high weight and the low centre of gravity, which is important for driving stability. The tip-mounted wheel sets are insulated on both sides, which is why the undercarriage remains isolated.

The wheels are black nickel-plated and are therefore unobtrusive in appearance. The turned buffers (on one side with prototypically curved plates), the coupling shafts with pressure spring and system couplings are FR's own designs, but are fully compatible with Märklin, which is also nothing new.



View of the wagon floor made of etched metal sheets with the spatial framework structure.

First impressions confirmed

Even under the magnifying glass, both model versions keep what they already promise at first glance: fine details such as lashing eyes moulded on the outside longitudinal member, end wall fasteners and only slightly simplified door fasteners that could be operated from the ground (without a step). The box struts, plank heights and fine gaps between them have also been well defined and implemented.



The overall appearance of the Ommr 33 is accurate, and the measurements confirm this. Correct for the Bundesbahn is the black printed longitudinal beam, on which many of the wagon inscriptions and also the lashing eye reproductions can be found. The printed note box next to the chalk field looks a bit more plastic and therefore better with this lettering version.

The overall appearance is therefore not only coherent in terms of the dimensions, but also according to the subjective impression that the eye makes when looking at the layout near other wagons. The diagonal stiffeners next to the loading doors, which were indispensable for the stability of the box, have not been omitted.

As with the Linz, FR has also set off the loading doors inside the body from the (otherwise smooth) walls. This is a feature that many manufacturers do without. But it is definitely noticeable when a wagon is to run unloaded. Therefore, it is also to be explicitly acknowledged. The black-painted handle bars at the right end of the wagon also belong to such small details.

The box body is correctly painted in RAL 8012 red-brown, the chassis and the longitudinal beam, which is part of the body in the model, in RAL 9005 deep black. The pad printing came in handy on the solebar, as the areas to be painted are interrupted by the box struts.



Another feature worth highlighting are the car doors that are reproduced on the inside (photo above). The magnifier-readable printing is also of high quality and can withstand multiple magnification (photo below). Only the gloss level of the black long girder printing differs slightly from the matt black paint of the undercarriage.

A minimal point of criticism here would be at most the slightly different gloss levels: the body and chassis are pleasantly matt in appearance, the black-printed area of the longitudinal member shines a little more in direct comparison. Since it is hardly noticeable, this is to be understood as a suggestion (for improvement) and not as a defect.

The brake lever on the undercarriage is not coloured. The clean application of the paint and the printing make a significant contribution to the successful overall appearance: We could not find any dust inclusions.

All four wagons included in this test of both packs have different road numbers from the correct number range of this type of construction. What they have in common is that all four are labelled as Ommr 33. The omitted sub-generic sign u, which is mentioned on the pack, was therefore not written on. According to the r, however, all four wagons are supposed to be re-gauged representatives.



The brake lever, which can be seen on the right edge of the sprinkler, was not colour-contrasted. A handle bar was added separately at both ends and painted in RAL 9005 deep black. The superstructure is painted in RAL 8012 red-brown, which was once prescribed by the DB.

All of the (white) company inscriptions of the prototype can also be found on the models. Since the solebars are part of the car body in the model, the inscriptions have also been printed there. Differences between the two lettering arrangements have been correctly taken into account. The note box is not raised plastic, but is again part of the pad printing on both versions.

On the two EUROP coaches it is located in the area of the larger chalk field, whereas on the DB cars with normal inscriptions it is located outside this area and had to be underlaid separately in black. This was done very precisely and even looks a bit more plastic and thus more convincing in the model.

The running gear remains without any surprises, as it is identical to that of the Linz presented in **Trainini®** 4/2020, therefore our words of praise also apply to the Ommr 33 or former Villach: The elaborate sprinkler system tapering towards the centre and brake reproductions at wheel level are also particular strengths of the models here.

continues on page 13



A direct comparison of the ends shows that the two wagon ends are not completely identical: On one side, access boards can be seen at the ends (photo above). The two wagons with standard inscriptions (photo above) and from EUROP stock (photo below) are shown in comparison to each other.

The single hooks on the axle holders with roller bearings and the shunting brackets under the buffers are also correct. The floor of the wagon, which is not visible during operation, was not designed, which we again consider to be completely unobjectionable, because such expenditure, which is not visible here, would ultimately have to be paid for by the customer.

Very good overall impression

In conclusion, the Ommr 33 offers no cause for criticism. In school grades it would get straight A's, which pleases us all the more because this is an extremely important type of car of the fifties and even the sixties.

It must not be missing in block trains of that time if the model railway enthusiast wants to stay close to the prototype. This will certainly also be the reason why FR Freudenreich Feinwerktechnik has chosen a design that allows larger quantities: Further operating number variants are certainly to be expected in the future, and the customer is not doing anything wrong at this point.



The two wagons from the first set (49.338.02; photo above) bear the road numbers 814 005 and 820 180, the EUROP representatives from the second set (49.338.12; photo below) the numbers 814 012 and 814 017.

When the prototypes were still in use, open freight wagons made up the largest proportion of DB's wagon stock. They were used for many bulk goods, including coal, in particular. But ore was also once transported in them.

The "Lange Heinrich" on the Emsland line, which the model railway industry would like to sell us through the series as something else, actually consisted of such older types of wagons. It was once hauled by class 50 steam locomotives, long before the heavier class 44 made its home there.

One could also casually conclude: What the Oppeln is for the covered wagons, the Villach is for the open wagons, virtually an all-round wagon of bygone times. FR has miniaturised it impressively, and almost uncompromisingly, on a scale of 1:220.



Open wagons are particularly impressive in block trains, as here in the “Langer Heinrich” according to a historical model and not a manufacturer’s interpretation. Of course, individual wagons can also be used at the loading siding of the local coal merchant or groups of wagons can be added to mixed through goods trains.

If it hadn't already been released last year as a first design, it would now have earned a nomination for the Best New Releases of the Year 2020 in the wagon category. And, this much we can certainly share, it would certainly not have been an outsider there!

Manufacturer of the model:
<http://www.fr-model.de>

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Douglas DC-9-30 as a new product from Herpa

One aircraft with many Names

Aircraft are rather a marginal topic in our magazine and yet there are quite a few model railway enthusiasts who also collect such models or use them in Z scale. For them, we present interesting successful models and popular airlines here from time to time. Today, it is a successful model that has borne various names over the course of several decades and was on the road for an equally interesting airline.

The Douglas DC-9 made its maiden flight on 25 February 1965 and entered service just under ten months later. Series production of a total of 976 aircraft lasted until 1982, making it an outstandingly successful model. But even after these 17 years, it was far from over!

The twin-engine narrow-body aircraft was designed from 1961 to complement the four-engine DC-8 long-haul aircraft on short and medium-haul routes. It is a twin-engine low-wing aircraft with jet engines mounted at the side of the tail. Equipped with a pressurised cabin and retractable landing gear, as had long been standard for jet aircraft, the T-tail, in particular, gave it a striking appearance with recognition value.

In the course of its long history, it was to compete against the Boeing models 727, 737 and 757, but sometimes under other names. This was triggered by the economic difficulties of the Douglas Aircraft Company, which led to the merger with McDonnell to form McDonnell Douglas in 1967.



The McDonnell Douglas MD-87 (officially: DC-9-87) also flew at Aero Lloyd, and was a further development of the basic version. Photo: Ralf Manteufel (GFDL-1.2)

In 1997, another takeover by competitor Boeing occurred, where the last version of the DC-9 was modernised again and now continued to be sold as the Boeing 717 until 2006. So, the production history alone spans just over four decades.

Until the last delivery as Boeing 717 on 23 June 2006, the former DC-9 experienced a total production of 2,438 copies. They were divided into 976 copies of the DC-9, 1,191 of the MD-80, 116 of the MD-90 and 155 as the Boeing 717, making it one of the most successful passenger aircraft of all time.

Depending on the version, the DC-9 could carry 90 to 130 passengers. At the beginning of its development, the original passenger capacity was 56 to 77, depending on the seating arrangement. This shows that with the growing air traffic and changing needs, the requirements changed rapidly, which was also true for other types.

The DC-9 Series 10 (original version) also had to be stretched and adapted several times to remain successful. The first DC-9-30, stretched by 4.50 m compared to the original version to accommodate up to 115 passengers, proved to be the most successful: it alone accounted for 621 of the 976 DC-9s delivered from January 1967. This type is the model for the Herpa model presented today.

Large operators were mainly in the home country USA, but the Douglas DC-9 aircraft family also found favour in Europe and elsewhere, most notably Alitalia for later versions. Even today, some aircraft are still in the air, albeit largely only in cargo service. Military versions were also built.



The DC9-30 entered the Aero Lloyd fleet early on and was in service there for years, here photographed in January 1985 in Frankfurt (Main), in a modified but also unimaginative Air France livery. Photo: Kambui (CC-BY-SA-2.0)

Because of its importance in Italian domestic and European short- and medium-haul traffic, let's briefly discuss the different names already alluded to in the headline: Introduced to the market as the Douglas DC-9, its manufacturer was absorbed into a merged company just two years later.

In the case of newly developed designs, the abbreviation DC (for “Douglas Commercial”) was replaced by the new company name MD (McDonnell Douglas). Here, however, the designation remained, presumably because of the type certification granted. The new manufacturer was only recognisable in the long name McDonnell Douglas DC-9.

These official type designations survived even when the aircraft finally entered service in a further developed form as the MD-80, the version that was long in service with Alitalia as the “Super 80”. So, the MD-80 was officially called DC-9-81, -82, -83 and -87.

Only the MD-88 and the last version marketed by McDonnell Douglas itself, the MD-90, received official certification under these designations. The same applied to the last version, which Boeing had already sent into the race as the 717.



Two years before its sale, Herpa's model is shown here on 28 May 1992 at Stuttgart-Echterdingen Airport (STR). The McDonnell Douglas DC-9-32 with the registration D-ALLB is still in the service of Aero Lloyd. Photo: JetPix / Torsten Maiwald (GFDL-1.2)

So, what would not be apparent to outsiders from the designations now becomes all too clear with this knowledge: the DC-9 has been a once commonplace aircraft that probably everyone has encountered who has embarked on an air journey. And this brings us to the choice of airline that Herpa has made for its latest new products.

Brief History of Aero Lloyd

The tax legislation of the time in Germany in the seventies and eighties repeatedly led to the founding of so-called “dentist's shipping companies”. Shipping and air shipping companies that were already geared to loss-making results from the outset were designated as such.

High-earning private individuals could then claim these loss allocations for tax purposes. They reduced their taxable income, consequently lowered the calculated top tax rate and brought the shareholder the decisive advantage in the wallet via saved taxes.

A decisive prerequisite was the liquidity of the company, which was also the reason why Aero-Lloyd Luftverkehrsgesellschaft mbH, founded in 1979, failed very quickly. Founded by three investors as a charter airline in posh Oberursel with headquarters in Frankfurt am Main, it was granted a licence for scheduled flights as well in June 1980. At the beginning, the fleet consisted of three rather aged Sud Aviation Caravelle 10Rs in a red Air France livery.

But by 2 December 1980 it had to stop flying again. However, Aero Lloyd was not yet at the end of its tether: on 20 December, the company resumed operations after the new shareholders Deutsche Luftfahrt Leasing, and Air Charter Market bought the name from the bankruptcy estate.

They founded the successor company Aero Lloyd Flugreisen GmbH & Co. Luftverkehrs KG. On 1 April 1981, the resurrected airline resumed charter flights from Frankfurt (Main). For the time being, the three old Caravelle aircraft were still being used, while the industry was still speculating about another quick bankruptcy.



Herpa's new product of the McDonnell Douglas DC-9-30 in the colours of Aero Lloyd (item no. 571012) is a somewhat belated birthday present for the 40th anniversary of the founding of the now defunct airline.

In early 1982, it added three slightly larger Douglas DC-9-32s to its fleet, which it had acquired second-hand from Indonesia's Garuda, which is the basis for the new Herpa model, which reproduces one of these aircraft with the registration D-ALLB.

In 1986, factory-new McDonnell Douglas DC-9-83 (MD-83) and in 1988 also DC-9-87 (MD-87) followed. With them, an independent livery was finally introduced, which looked very appealing and summery warm. So, it was a perfect fit for this airline. Herpa produced the model in this 1986 livery, and chose an aircraft type that was very formative and important for Aero Lloyd.

In the summer of 1988, after efforts by Aero Lloyd, a new licence was granted for scheduled flight operations from 31 October of that year. Its main focus, however, was and remained charter traffic to Mediterranean destinations.

Nevertheless, the return to scheduled services was the beginning of the end. The poor utilisation of the aircraft of only 16% was castigated by the press at the time with the term “ghost aircraft”. The reasons given were the behaviour of the top dog Lufthansa, low frequencies and an “amateurish advertising policy”.

But the final end of the colourful planes dragged on for years. The business model was finally overturned in the aftermath of the terrorist attacks in New York on 11 September 2001.



The distinctive lettering on the vertical stabiliser, underlined by the sun colours, disappeared from the aircraft at the end of 2003. In the model, the airline lives on and allows a view here of the no less distinctive jet engine of the DC-9.

On 16 October 2003, Aero Lloyd filed for insolvency with the Bad Homburg v. d. Höhe District Court and ceased flight operations with immediate effect after its restructuring concept was rejected by the Bayerische Landesbank.

The prototype for the model had long been out of stock by then: sold to Midwest Airlines in the United States in 1994, the aircraft is now considered scrapped.

Herpa's belated present

Measured against the original start of air traffic, Aero Lloyd would have been 40 years old last year. And Herpa had planned the model for this occasion, but it was not delivered on the originally planned date.

The miniature (item no. 571012) is, like all other aircraft of the Wings collection already presented here, sufficiently close to Z gauge not to appear too large. Apron vehicles as well as figures can be used for visual comparison.

We have taken important measurements from the model to judge the reproduction quality and compared them with the prototype data, which were calculated for the conversion scale 1:200 and our comparison scale 1:220. The result highlights our assessment in this case, as well.

Size and details of the McDonnell Douglas DC-9-30

	<u>Prototype</u>	<u>1:200</u>	<u>1:220</u>	<u>Model</u>
Length	36,60 m	183,0 mm	166,4 mm	174,0 mm
Wingspan	28,40 m	142,0 mm	129,1 mm	141,8 mm
Height	8,38 m	41,9 mm	38,1 mm	41,8 mm
Max. Take-off Weight	49,94 t	---	---	244 g
Cruising Speed	ca. 900 km/h			
Range	3.030 km			
Max. no. of Passengers	115			
Engines	2 x P&W JT8D-7			
Thrust	2 x 68,9 kN			

The aircraft, which has been converted into metal, has a very high-quality appearance due to its weight and feel. This is also ensured by a clean and impeccable paint job in the correct colours of the former Aero Lloyd.

In addition, there are elaborate pad prints that reproduce small and special details or make them look more plastic. These include, for example, position lights, the window frames of the cabin and also of the cockpit as well as the frames of the

passenger doors and emergency exits. The tailgate of the prototype has also been depicted in this way, but is hardly in the viewer's field of vision on the tarmac.

But we also like the details that are represented by means of attached parts. We would like to mention such banal things as the typical T-tail, the characteristic wing shape and sweep or an attached antenna on the top of the fuselage.



With its overall dimensions, the model can be used on layouts of nominal size Z without appearing too large. The detailing also fits in excellently: In addition to the finely designed undercarriage and attached antennas, many other features have been reproduced by elaborate printing.

Absolute highlights, not only on this conversion, are the filigree and yet very stable landing gear including flaps or the two jet engines at the rear end of the fuselage: Here, if you look closely, you can also see the turbine blades. No compromises were made in the shape of the engine outlets either.

In summary it can be said that it would be a pity to hide this successful model somewhere in a back corner of the layout. The airport, on the apron of which the McDonnell Douglas DC-9-30 of Aero Lloyd is presented, should rather be clearly visible.

continues on page 23



The model also exudes special charm when viewed from the front: the eye is drawn to the framed cockpit windows with windscreen wipers and, further back, the finely produced turbine blades in the two engines attached to the side of the fuselage.



The summery dressed passers-by by Faller (158052) allow a scene that would also be conceivable on many layouts: off on holiday!

Then, you can also spread a little holiday mood with the 1:200 miniature! The bright, warm colours of the machine registered under D-ALLB in operating condition from 1986 to 1994 contribute their part to it.



Competitors on the apron: The Boeing 737 was an aircraft type that was in direct competition with the McDonnell Douglas DC-9. The big Lufthansa was also always trying to make it difficult for Aero Lloyd to survive.

And, surely, this can be underlined even more clearly with holiday travellers who want to get off the apron bus and climb the stairs to the aircraft. Even model railway layout set in domestic climes can thus add a touch of the Mediterranean, beach and sun.

Manufacturer of the model:
<http://www.herpa.de>

Ideas from reader for readers

Ten Tips for Success

Our reader Kay Anbuhl from Berlin has been, like many others, very active and busy in his hobby over the last few months. Today, he shares his ideas and examples of how to turn old models into something great with little effort, a bit of skill and patience. We hope that this will inspire regular readers, but also new ones who have taken up the most beautiful hobby in the world for the first time in recent months.

By Kay Anbuhl. Being at home much more than usual over recent months made think of making better use of the extra time. I've been also thinking especially of newcomers to the hobby who, due to the current lack of new starter packs, might rather source their first models from the second-hand market.

Most of the time, it is the inexpensive models of the first three Mini-Club decades that can be found there, rather than the highly topical new products. There is now a world of difference between the two in terms of detailing and, above all, painting and lettering.



With just a few simple steps and a little paint, even old Mini Club models can be brought largely up to date again.

But even with a few simple steps, these “old treasures” can be updated. I would like to give, therefore, a few easy tips and suggestions in the following.

Tip 1: Improved appearance of refrigerator cars

A classic in the fleet is the 54 mm refrigerator car (e.g. Märklin 8600 or 8631), which was produced as a beer car in numerous variants. In my opinion, its roof looks much better if it is also painted on the front side (ends).



The roof of the refrigerator car was repainted grey and the front ends were included for a better effect.

This can be done quite easily with a size 1 brush. Since the incidence of light on the front side is always different from that on the round roof, a deviation in the colour tone is not a particular problem. This is even less of an issue, if we also replace the often silver-coloured roof with a more attractive grey.

Tip 2: Make wheel sets visually less conspicuous

For about two decades Märklin sold its wagons with bright nickel-plated wheel sets, which are no longer convincing today, due to their unrealistic appearance (example: "Rheingold 1928" with item no. 8133).



The bright nickel-plated wheel sets of earlier generations no longer shine so brightly when their wheel discs are darkly painted.

Märklin has now solved this by using black nickel-plated axles.

The brightly shining axles can also be significantly improved by darkening their sides with black or dark grey paint.

The needle ends of the axle (tip bearing), the running surfaces and the wheel flanges should, of course, not receive any paint! Getting it right takes time and patience, but is worth the effort.

Tip 3: Removal of running boards

When Mini-Club was launched almost 50 years ago, very different freight cars were equipped with identical standard chassis in an obvious compromise between cost and prototype fidelity.

A classic case is the 54 mm chassis of the freight wagons with the item numbers 8605, 8606, 8610, 8615, 8616 and 8622. This chassis has a running board in the middle, which looks very out of place on a low side car (8610) or a container wagon (8615 / 8616). The only type of car where it did make for a prototypical look was the 8600 refrigerator car.



The standard chassis of the first-generation freight wagons had the same length - regardless of the prototype - and running boards in the middle of the wagon. On these two types of wagons, this was not prototypical, which is why they were removed with a sharp craft knife.

A simple craft knife is all that is needed to correct the mistake – goodbye, running boards.

Tip 4: Easy improvements for powdered freight silo cars



The upper part of the frame on the Ucs type powdered freight silo wagon benefits from a bit of black paint, given that it is part of the superstructure of the model and, therefore, of the same colour.

Numerous variations of the 40 mm silo cars have been produced over the years. A common characteristic of all of them is that the silos and their base share the same colour. In my opinion, this does not look very realistic.

Therefore, I painted the bottom with black paint (Revell silk matt) which is actually quite easily done with a size 0 brush.

As you can see in the photo, mistakes sometimes happen, but this should not discourage anyone, and they can usually be corrected.

For example, I forgot to paint the white floor behind the access ladder to the silos on the photographed example (82502) – I still have to work on that. Much more difficult to paint, however, are the yellow hand wheels.

One also needs a steady hand for the steps on top of the silo. But the result is convincing, because a piece of plastic has now really become a wagon with two silo containers.

Tip 5: More operational safety through modified couplers

Mini-Club was the brand under which Z-gauge was marketed when it appeared in 1972, and anyone who has been around since these early days will, like me, probably own numerous cars that are equipped with the old standard couplers. Among collectors, they are usually referred to as “Coupler 1”.

Märklin changed this in 1977 by slightly bevelling the top at the front end. This significantly improved the coupling behaviour. But, especially in the case of four-axle passenger wagons, it is not easy to change the couplings and convert them to more up-to-date ones.

Since I never got the Märklin uncoupling tracks to work safely on my layout, nothing was coupled or uncoupled. Accordingly, I had not attached any importance to the modified couplers for years. But, with the Jörger system, there are also solutions that work more reliably; and, then, suddenly there is a need for action.



These two cars with item number 8722 from two editions illustrate the aim of the modification: On the older specimen (left) the coupler has a smooth upper edge and can catch on the buffer of a coupled wagon. The coupler of the newer specimen (right) already has a bevelled surface that prevents this and which oneself can easily modify. Optical deficiencies on the front sides of the cars are also clearly visible, as the printing has not been applied around the corners.

The later, and still common, bevelled “Coupler 2” also prevents snagging with the buffers or the body of the coupled neighbouring wagon in curves. Such a snag sometimes leads to derailments.

With a sharp craft knife, I have therefore changed the 1972 couplers of my old wagons into a “1977 type” coupling with a courageous cut. It helps. It is important that the new cut surface is smooth and has no burr. The blade of the knife must, therefore, be really sharp.

Tip 6: Improving the front sides of 120 mm wagons

The next classic is the 120 mm passenger wagon in all its variants that have appeared over the course of several decades. Since their front sides are probably not so easy to print, their appearance is rather unappealing.

In my view, the greatest need for action is with the two-colour carriages (pop-coloured carriages, TEE carriages and D-train carriages in ocean blue/ivory). The front sides are painted ex works predominantly in the basic colour of the wagon; the second colour is only marginally drawn around the corner (see also photo on page 27).



Early versions of Z scale passenger carriages can be significantly improved by painting the upper colour strip around the corner and by also painting the transition plates, tail lights and rubber beads.

up to a millimetre too short. Especially on light-coloured cars, an unsightly transition is visible, which can be well concealed by black paint.

Since the reflection of light on the end is always different from that on the long side of the wagon, a small deviation in the colour tone thankfully does not really matter, which is especially helpful with the brightly coloured pop-colour wagons..

Black paint (Revell silk matt) on the rubber diaphragm and the folded-up transition plate helps to further improve things.

It is not important that the rubber diaphragm is painted perfectly, but that it gets paint on the sides, top and inside.

The brush stroke should be as straight as possible and at least somewhat vertical.

I always take off the roof to blacken the transition between the rubber diaphragm and the roof.

For reasons I don't understand, the roof of many older cars is sometimes

Finally, red paint helps the tail lights to look good. This is not as difficult as it first appears, as the tail lights are fortunately designed as a recess with an additional edge. The paint just has to be really good quality to flow properly. And we should start with very little paint. The brush tip must of course be OK, otherwise, we will paint over the edge.

The earlier conversion wagons (item numbers 8753, 8754 & 8755) can also be upgraded in this way. The photo also reveals that the slightly furry look of the pop colour car could be counteracted by a bit of dusting. In addition, the car still has a first generation “1972 coupler” which we still need to “carve” into a “1977 coupler” (see tip 5).

Tip 7: Add white toilet windows

Many of the older Z-scale wagons did not come with non-transparent toilet windows. But that, too, can be easily changed. In 1980, no search engine was available that could have helped me to identify the correct windows for the procedure.

At the time, I had deliberately not applied any white paint, but simply stuck on white insulating tape from the inside so that I could change to the correct window, if necessary, should I have made a mistake.

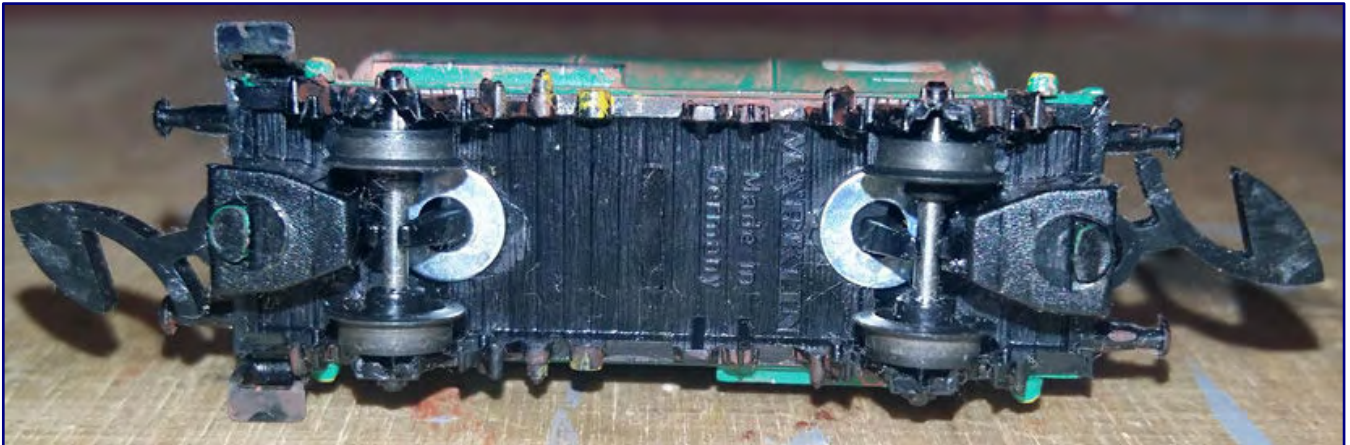


Small measure, big effect: The toilet window on the left coach has a white background - and this is even reversible in case of a mistake.

This insulating tape is still on it and remains so. Toilet windows are easy to recognise due to their smaller format and are usually located at both ends of the wagon, but only on one side of the wagon. If the roof vents are not centred, the side with the roof vents is incidentally the toilet side.

Tip number 8: Increase axle load, if necessary

When we run freight trains with more than ten wagons, it is clearly noticeable that the classic 40 mm tank wagons (item numbers 8611, 8612, 8613, 8614 etc.) derail more frequently than the 54 mm classics (8600, 8606, 8615 etc.).



Metal washers can invisibly, but all the more effectively, increase the weight of light freight wagons and provide more driving stability

The reason is quite easy to determine, because these wagons have simply turned out too light. We should not weigh every word we say, but we should put weight on every wagon! The typical „standard freight wagon“ weighs about 5.3 g, whereas the short tank wagon weighs only 3.6 g. Both are two-axle wagons, so the axle load of the lighter wagon is much smaller, unfortunately, even too small.

The remedy here is to add weights. M2 metal washers are perfect for these cars. To do this, we first remove the axles, place the washers above the inner axle holders, and finally fix the washers there with a tiny drop of super glue (e.g., Bindulin, Pattex or Uhu).

My experience is that cars weighing less than 2.3 g per axle do not run reliably. The weight can be adjusted very precisely with the washers, because two, four or even six such washers can be easily attached as required.

In this way, the short tank cars run very reliably even with the old plastic wheels, which were once considered “notorious” and, like the old couplers, were only used for a short time.

However, other wagons also have the problem being too light; e.g., the powdered freight silo cars already mentioned or the four-axle Langenschwalbach passenger cars. Even the heavy oil wagon 80317 is still too light, despite its metal chassis.

And the old 8619 long timber freight car also weighs only 8.2 g, which, for design reasons, was only distributed to four axles despite the three-axle bogies. The axle load, thus, still proves to be too low.

Tip 9: Cable pull hooks in yellow

And once again, it's all about the classic 40 mm tank cars. A bit of yellow colour helps to highlight the cable hooks. These so-called UIC hooks are also found on many other types of wagons, but not in yellow. A small brushstroke with matching Revell paint works wonders here.



A simple brushstroke literally works wonders on yellow cable hooks.

Tip 10: More realistic loads for car transporters

The DDm 902 car transport wagon (item no. 8714) came with eight unpainted rubber cars (“erasers”) which made for a rather unrealistic impression. By the way, each wagon can hold 12 cars on the two loading levels. This looks more realistic, but the wagon will weigh 18 g if loaded like this.



Two examples of DDm car transport wagons that can run on long-distance trains: up to twelve vehicles can be loaded on both floors of this double-decker wagon.

With locomotives that have a lot of traction, such as the old class 216 (8875), this is still not a problem with five 120 mm pop colour wagons and two fully loaded car transporters. A newer “Lollo” locomotive (8866) or a class 218 (8880) can get stuck in a curve with such a heavy train, and mastering gradients will be difficult. Operating tests are therefore recommended in order to determine the right locomotive for the desired train formation.

The rubber cars can attain a semblance of real cars by highlighting their windows and painting bumpers, headlights and hubcaps. Some matt black for the tyres and red for the taillights, and they will actually be usable. Job finished!

All photos (except pages 24 and 27): Kay Anbuhl

Providers mentioned in the article:

<http://www.peter-post-werkzeuge.de>
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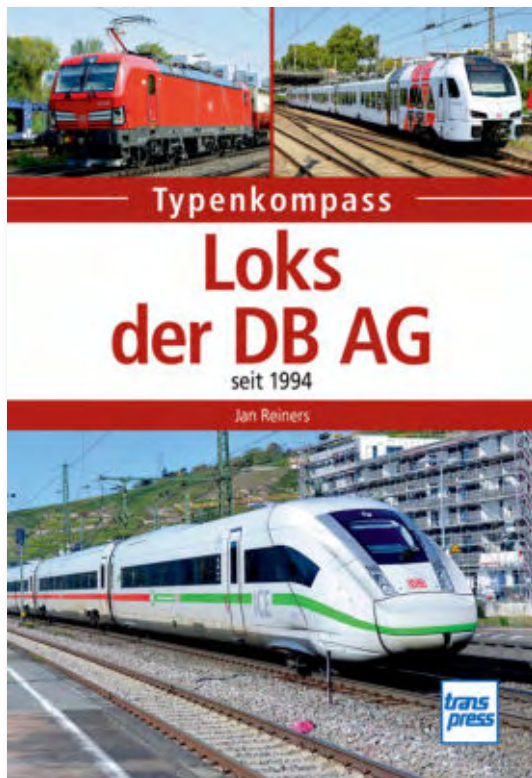
Adhesive suppliers (superglue):

<https://www.bindulin.de>
<https://www.pattex.de>
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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Lok und Triebwagen der DB AG Typenkompass auf neuem Stand

Transpress erweckt gern den Eindruck völlig neuer Bücher, auch wenn es sich tatsächlich um eine Wiederauflage handelt – mal ohne erkennbare Änderungen, mal überarbeitet und erweitert. Letzteres trifft auf den hier zu besprechenden Typenkompass zu, dessen letzte Auflage vor mehr als zehn Jahren erschien. Und in der Zwischenzeit hat sich im Bestand der Deutschen Bahn wieder viel getan.



Jan Reiners
Typenkompass
Loks der DB AG seit 1994

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Druckfrisch ist der Typenkompass, den wir heute vorstellen dürfen. Im November 2010, also mehr als zehn Jahre zuvor, haben wir seine Vorgängerauflage an dieser Stelle vorgestellt. Seitdem hat sich viel getan und nicht nur der Einband hat eine neue Gestaltung bekommen.

Weniger glücklich sind wir mit der Tatsache, dass die Neuauflage eines bereits veröffentlichten, aber längst vergriffenen Titels eine völlig neue ISBN zugeteilt bekommt, weil dies eigentlich dem Sinn dieser Katalogisierung widerspricht.

Einem Interessenten erschwert dies den Abgleich, und möglicherweise greift er zu einem vermeintlich neuen Buch, das aber bereits sein Regal schmückt. Das kann in diesem Fall nicht passieren, denn seit Erscheinen der vorherigen Auflage hat sich im DB-Lokbestand wieder einiges getan.

Das betrifft weniger, aber auch, den Bestand an Lokomotiven, vor allem aber denjenigen an Triebwagen, die ebenfalls im Buch behandelt werden. Und so sind wir dankbar für diese überarbeitete und aktualisierte Neuauflage, die wieder einen kompakten und kompetenten Überblick bietet.- nur eben auf dem neuesten Stand.

Doch darauf wollen wir gleich noch mal einen Blick werfen. Vieles, was diesen Titel charakterisiert, trifft auch auf andere Typenkompass-Bände und eben den Vorgänger zu. Wir zitieren uns beinahe selbst, wenn wir den Umfang beschreiben:

Behandelt werden Lokomotiven und Triebwagen, die seit 1994 für die Deutsche Bahn AG im Einsatz waren und sind. Eine Ausnahme bilden angemietete Lokomotiven oder Splittergattungen, die nur kurzzeitig nach Firmenübernahmen im Bestand waren, aber nicht mehr zum Einsatz kamen.

Während alle ICE-Generationen, also auch die seit Erscheinen des letzten Bands hinzugekommenen, elektrische und dieseltreibene Lokomotiven, übernommene und seit 1994 neu beschaffte Triebwagen, einige Schmalspurlokomotiven sowie die Tunnelrettungszug-Lokomotiven der Baureihe 714 behandelt werden, fehlen Instandhaltungs- und Messfahrzeuge mit der genannten Ausnahme unverändert.

Letzteres ist bemerkenswert, weil das auch 2010 schon so der Fall war. Und damals wie heute gibt der Verlag den Buchinhalt oder -umfang an dieser Stelle leider falsch an. Wir werten das als weiteren Beleg, dass es sich nicht um ein völlig neues Buch, sondern eine neue und überarbeitete Auflage handelt.

Wegen des identischen Seitenumfangs und Neuzugängen bei der DB wie den Pesa-Link-Dieseltriebzügen der Baureihen 631 bis 633, die übrigens mit völlig falschen Baujahren angegeben wurden, oder auch der nun weit verbreitete Siemens-Vectron (Baureihe 193) mussten offenbar auch Fahrzeuge entfallen, die 2010 noch aufgeführt wurden: Gemeint sind die letzten Dampflokomotiven auf Schmalspurbahnen (Baureihe 99), die kurzzeitig noch der DB gehörten.

Die Darstellungsform ist gewohnt: kurze Portraits auf einer, seltener zwei Seiten, mit den wichtigsten Daten und Angaben zur Betriebsgeschichte und dazu eine kleine Tabelle mit den wichtigsten Daten zur Technik. Das Fotomaterial, jeweils ein bis zwei Aufnahmen pro Baureihe, scheint komplett ausgetauscht worden zu sein.

Das haben wir schon beim Durchblättern als sehr erfrischend empfunden. Auch die Reproduktionsqualität der Aufnahmen ist qualitativ nicht zu beanstanden. Was uns gewundert hat, ist die mit 100 um 23 Bilder geringer angegebene Gesamtmenge.

Wir haben nicht nachgezählt, aber ein reduzierter Bildanteil widerspricht zumindest unserem Eindruck. Und der ist es ja, der letzten Endes zählt, wenn ein Kauf erwägt wird. So bietet auch dieser Band keine großen Überraschungen, unliebsame schon gar nicht.

Wenn wir diese Rezension in wenigen Sätzen zusammenfassen wollen, können wir den Verlagstext im betreffenden Abschnitt unverändert übernehmen: In prägnanten Texten und mit zahlreichen Bildern werden die verschiedenen Baureihen (hier) vorgestellt.

In gewohnter Typenkompass-Qualität halten die einzelnen Portraits der Triebfahrzeuge neben technischen Fakten auch Informationen zur jeweiligen Historie und Einsatzgeschichte bereit. Dem ist nichts mehr hinzuzufügen, es trifft alles auf den Punkt.

Angemerkt werden darf allenfalls noch, dass auch dieser Typenkompass zu den günstigsten Büchern der Fachliteratur gehört und einen guten Einstieg für Interessierte bietet. Er schafft einen kurzen, aber guten Überblick und passt auch auf Wanderungen entlang von Bahnstrecken perfekt in jeder Jackentasche.

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: Publishing pages with reference possibility: :
: <https://www.motorbuch.de> :
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Winter bei der Bundesbahn Alle reden vom Wetter...

Wer kennt ihn nicht, den wohl berühmtesten Werbespruch der Bundesbahn. Auch heute ist er noch in aller Munde und muss oft dafür herhalten, sich über die moderne Bahn und ihre immer wiederkehrenden Betriebseinstellungen lustig zu machen. Ein Buch versucht, die Zuverlässigkeit und Mühen der früheren DB im Wintereinsatz aufzuarbeiten.



Udo Kandler
Winter bei der Bundesbahn
Alle reden vom Wetter

VGB Verlagsgruppe Bahn GmbH
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Wie war sie eigentlich, die Bundesbahn? Das ist sicher eine Frage, die sich immer häufiger stellt, nachdem die Behördenbahn mehr als 25 Jahre Geschichte ist. Die Erinnerungen an sie schwinden und im Rückblick neigen wir leider dazu, uns nur an positive Dinge zu erinnern.

Eine Besonderheit im Rückblick ist sicher die Winterzeit, die wir rund sechs Wochen vor Erscheinen dieser Ausgabe noch einmal eindrucksvoll zu spüren bekamen. Das macht dieses Buch aus dem Vorjahr nun so aktuell und verleitete auch uns dazu, einen Blick hinein zu werfen. Immer im Gedächtnis bleiben werden vermutlich die Werbesprüche „Wir fahren immer“ und „Alle reden vom Wetter – wir nicht“. Doch stimmt das? Fuhr sie wirklich immer, so wie die damals Verantwortlichen verlautbarten?

Autor Udo Kandler widmet sich im vorliegenden Bildband diesen Fragen und zeigt Motive und Erinnerungen aus vielen Wintern, die aus rund dreißig Jahren Bundesbahnzeit stammen und unterschiedliche nicht sein könnten.

In hervorragender Wiedergabequalität sind darin Räumeeinsätze, Zugfahrten und Kolonnendienste aus normalen Wintern ebenso zu finden wie Bilddokumente aus sogenannten Jahrhundertwintern. Zu solchen Extremwintern gehören die Jahre 1962/63, als der Rhein zeitweise mit bis zu fünf Metern hoch aufgetürmten Eisschollen für Schiffe unpassierbar war – ein bis heute einmalig gebliebenes Ereignis.

Nicht ganz so lange zurück liegt der Winter 1978/79, der auch vielen Menschen in Erinnerung geblieben ist: Eine seltene Wetterlage sorgte im Norden Deutschlands damals für einen rasanten Temperaturabfall, verbunden mit starkem Schneefall, als feuchte Atlantikluft auf polare Kaltluft stieß. Der in Küsternähe und im flachen Hinterland starke Wind sorgte für meterhohe Schneeverwehungen und ließ die Schneeberge vereisen.

Damals ging buchstäblich nichts mehr: Weder für Autos noch Züge gab es noch ein Vorankommen. Viele Menschen saßen in Fahrzeugen fest und drohten zu erfrieren. Katastrophenalarm wurde ausgelöst, die Bundeswehr stellte Soldaten ab, die teilweise mit Panzern zu den Eingeschlossenen vordrangen, um sie

mit Schaufeln auszugraben oder mit Hubschraubern aus der Luft zu versorgen, bis der Landweg wieder teilweise passierbar wurde.

Beide Ereignisse sind, wie viele andere auch, im Buch festgehalten, doch im Gesamtkontext und -umfang bleibt ihnen eher eine Nebenrolle. Bestimmendes Hauptthema dieses Werkes ist der unbändige Willen der Eisenbahner, unter allen Umständen auch im Winter bei Eis und Schnee den Zugbetrieb uneingeschränkt aufrecht zu erhalten.

Dieser letzte Satz enthält einige Superlative und lässt die Bundesbahner rückblickend fast heroisch erscheinen. Da muss eigentlich jeder stutzig werden, doch der Autor tut genau das leider nicht. Er berichtet eifrig und begeistert in Wort und Bild, wie Schneeräumkräfte in Mannschaftsstärke ausrückten und den Bahnbetrieb sicherten.

Einfaches Handwerkzeug wie Besen, Schaufel und Spitzhacke oder handgeführte Schneeräumer belegen die harte, körperliche Arbeit. Schneepflüge an den Elektrokarren der DB oder auch Flammenwerfer zum Enteisen von Weichen erscheinen als erste Zeichen von Modernität. Bis dahin musste jeder helfen, der konnte – bei Büroangestellten meist unter dem Vorbehalt der Freiwilligkeit.

Archaisch muten die Klima-Schneepflüge und Henschel-Dampfschneesleudern heute an, die hier ebenfalls nicht zu kurz kommen dürfen. Da sie nicht nur in der Spur Z schon als Modelle nachgebildet wurden, ergeben sich hier viele Anregungen und Quellen für Modellbahnumsetzungen aller Spurweiten. Wer authentisch bleiben möchte, kommt an diesem Buch also nicht vorbei.

Aber das Buch hat auch einen großen Haken: Der Autor lässt sich dazu hinreißen, die Bundesbahn zu glorifizieren und als besseres Beispiel der Deutschen Bahn AG gegenüberzustellen. Das halten wir für eine zu deutliche Wertung, die aus dem Buch heraus nicht hinreichend belegt werden kann. Warum nicht?

Udo Kandler stützt sich auf die nachhaltig wirksamen Werbekampagnen, die wir eingangs schon mit ihren Leitsprüchen zitiert haben. Für sein Werk zieht er viele Quellen heran, die direkt von der Bundesbahn stammen, darunter vor allem das Presse- und Werbeamt, oder die ihr nahstanden. Das Bildmaterial stützt sich vor allem auf offizielle Aufnahmen der Direktionsfotografen – also durch die Reihe fast nur Eigendarstellungen des Portraitierten.

Objektive Quellen fehlen fast vollständig oder ergänzen und unterstreichen allenfalls die dargestellten Lagen. Und dennoch lässt sich auch dem Buch noch entnehmen, dass die DB zwar stets bemüht war und großen Aufwand trieb, aber keinesfalls immer und auf allen Strecken den Betrieb pünktlich abwickeln oder sogar aufrechterhalten konnte.

Darüber wird selten berichtet und hier ebenso nicht. Hintergrund ist, dass es dazu wenig Material gibt, denn die DB war nicht mal verpflichtet, derart gravierende Betriebsstörungen bekanntzugeben. In den damaligen Zeiten, Jahrzehnte vor dem Internet, erfuhren die Menschen also meist gar nichts von winterlichen Problemen und hielten sie bestenfalls für lokale oder regionale Phänomene.

Nicht zu vergessen bleibt auch, dass die DB einen öffentlichen Auftrag hatte, und zu diesem gehörte auch eine wichtige Transportfunktion im Verteidigungsfall. Flächendeckende oder größere Störungen hatten im Kalten Krieg den Charakter eines militärischen Geheimnisses und wurden nicht in den Medien ausgebreitet.

Das hätte der Autor wissen und berücksichtigen müssen, als er diese Reminiszenz auf die Bundesbahn verfasst hat. So ist es zwar ein tolles, lesenswertes und deshalb auch empfehlenswertes Buch geworden, aber es kann den Charakter einer Folklore in Teilen eben auch nicht abwerfen!

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Staying in the conversation (Part 1)

JMC Scale Models: How it all began...

João Marrafa de Carvalho is known under the brand name JMC Scale Models and has become a famous name in Z gauge with his very finely designed car models made of cast resin. The Portuguese has long since made a name for himself in our community with his great ideas and good realisations. That's why we asked him about his career as a model maker and supplier, and let him take the lead today for this year's focus topic.

By João Marrafa de Carvalho. Miniature models have been my great passion, ever since I can remember, just everything: Cars, trucks, buses, trains, planes, boats, houses... But the cars were the primary focus. Classic cars, mainly.

I remember: When I was four years old, my grandfather gave me an H0 gauge train set of the Orient Express from Lima. Apart from the fact that it has long since suffered damage, which was to be expected with a small child, I still have this set!



João Marrafa de Carvalho, the person behind the JMC Scale Models brand, painting his models from resin casts. Today, he explains, among other things, how the master moulds for this process are created.

I also remember my father assembling a 1:72 scale C-47 aircraft model kit during that time and later a Revell Airbus A320. So, scale modelling has always been present with me since my early years.

Around 1999 / 2000, at the age of 12, I left the metal cast toy cars of Majorette and Matchbox to officially start my life as a scale model builder myself. First, with H0 gauge railways, then with plastic building sets.

During this time, a colleague of my father's, a real model-making enthusiast and very talented modeller, opened up the amazing horizons of this wonderful hobby to me. I still have dozens of railway magazines, brochures and technical documents that he provided me, with along with the model kits he built.



Cars always played an important role in his modelling career. This has not changed since his orientation towards Z gauge. However, North American classics have now been joined by those from Europe, such as here the first front-wheel drive MAN, popularly known as "Pausbacke" (chubby cheeks).

One of the subjects he introduced me to was "scratch building" (making or rebuilding a model yourself, not using prefabricated parts). I was completely excited by this new kind of skilful model building and its infinite possibilities to create things virtually out of nothing. Since then, I have devoted myself mainly to this field.

In 2005 / 06, some friends started asking me, "Why don't you sell the items you make?" To be honest, I was very reluctant to do so at first. It was also during this time that I discovered Z gauge.

I started to see amazing dioramas and other accessories just being sold through internet auctions. These small works really amazed me and caught my attention.

I began to ask myself, "Why not also build small dioramas in vintage style?" At the same time, I also started to build my own vehicles. I tried to post some of my 1:220 scale works on the auction platform as well, but I was a bit disappointed: My dioramas didn't sell easily.

Sometimes they were sold, but not for the price I expected. When I look at these little pieces today, I understand why...: Skill requires experience, patience and time. So, I started to improve my modelling skills, step by step. And I am still doing that today. I just don't consider myself a professional designer. I still have so much to learn and improve.

Then, I finally stopped with the dioramas and devoted myself completely to the vehicles. In the meantime, I had heard a lot about making resin casts, and then I began casting my first self-made cars.

I found this technique simply great. Now I could make several cars from a single master form. I cast copies using resin, and then painted and weathered them. The cars seemed to sell a little better than the dioramas had before. So, with each new car I made, I tried to improve my techniques.



One of the more recent classics to emerge in Portugal is the German Henschel HS 160 city bus. Here, the excellently done model can be seen in typical layout use at the bus station next to the track layout. Photo: Dirk Rohwerder

In 2011, I finally created a blog dedicated to the Z gauge pieces I made. After a few years of inactivity, I recently brought it back to life. In 2016, I also created a page on Facebook, which is much more interactive and visible than the old blog. To this day, this is my favourite platform to spread the word about the models.

A few years ago, during my years of study, I had the opportunity to get more intensively involved with CAD programmes. In mechanical engineering, they are used very extensively today, for example Solidworks and Catia.

Solidworks is my favourite, and the programme I like to model with the most. I really like designing in 3D. Unfortunately, I don't have my own 3D printer, so I have to use 3D printing service providers to print the prototypes / master models. I then use these to make the moulds that are used for the resin casts.



The designs have long been created on the screen with the help of a CAD programme. But there are also cases in which classic manual work is required in addition: In the case of the Opel Blitz box-type lorry, the front comes from a CAD design, while the rear area was more quickly produced using classic methods.

However, working with CAD programmes also gave me the opportunity to create models much faster, more perfectly and in more detail than was possible when making them purely by hand. However, some shapes are sometimes difficult for me to construct in 3D. And then, I like to make use of some of my “scratch building” skills again.

So, it is quite common that both techniques are used in parallel when building a model. For example, I had the front part of the Opel Blitz box-type lorry 3D printed while the rest of its body was "scratch built".

A decisive turn is now still missing: Since I have been involved in Z-scale modelling, my focus has mostly been on the classics among the American cars. The reason for this is firstly that I am a great lover of the great vintage beauties, and, secondly, because American car models in 1:220 scale were previously very difficult to find.



João Marrafa de Carvalho is pleased that his models are so well received by the Zetties today, which provides sufficient motivation to continue with more classics. Some of them could already be on the workbench here

Recently, however, I have also started to pay a little more attention to European vehicles, not only because some models are still missing in Z scale, but also because there are a vast number of vehicles from Europe that I love very much and that I would therefore also like to construct.

Let's see together where the journey will take us. And stay in touch with me and my models!

All photos (except page 38): João Marrafa de Carvalho / JMC Scale e Models

Webpages of the manufacturer mentioned in the article:

<http://jmc-scalemodels.blogspot.com/>

<https://www.facebook.com/pages/category/Games-Toys/JMC-Scale-Models-341193572911400/>

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Reader feedback on our Mozart knife recommendation:

The red craft knife (meaning the precision knives from Mozart that we recommend; editor's note) feels really good in my hand.

Jan Tappenbeck, by E-Mail

“Trade fair car” 2021 on its way to the dealers:

The current long-distance paint scheme of the Deutsche Bahn AG is used on the Bi-28 standard passenger car (Item no. 80131), which continues the series of International Spielwarenmesse (Nürnberg (Nuremberg) International Toy Fair) cars. With the wagon in white paint, with red belly band, “Dürr biscuit” and basalt grey roof, Märklin commemorates this year's anniversary “50 years of Intercity”.



Even without a toy fair, Märklin did not skip its dealer gift and this time dedicates the wagon (item no. 80131) to the anniversary "50 years of Intercity."

The anniversary is also printed in white on the roof. Instead of a note “Thank you for your visit”, “Thank you for your trust” is now printed on the packaging due to the postponed trade fair date.

This gift will now be gradually handed over to the dealers who order from Märklin's new products programme when the next agent appointment is due.

Two more new products from JMC Scale Models:

This Portuguese car model supplier, which we also present in detail in this issue, had announced two more new products after the editorial deadline, which we would like to add here.

They fit perfectly into his existing product portfolio, as they have again been designed according to German and American models. The fans of North American vehicles are looking forward to a truck from 1947 ("1947 Federal Tandem Axle Truck").



With the three-axle tractor unit from 1947 (photo, left) and the Hanomag A-L 28 with box body (photo, right), two more new products have recently been announced. Photos: JMC Scale Models

The Hanomag A-L 28 with box body, a light truck for 1.5 to 3 tons payload, was modelled on a local model. The Hanomag L 28 base vehicle was their first, completely newly developed commercial vehicle after the Second World War. With its American-style alligator bonnet and headlights integrated into the front, it looked very modern in 1950.

Built until 1960, from 1955 it was fitted with front-hinged doors instead of rear-hinged doors and, in the last years of production, with a large panoramic windscreen instead of a two-part one without curvature.

The all-wheel drive version A-L 28 chosen by JMC Scale Models as a model appeared in 1958 after Rheinstahl took over Hanomag. The technically independent offshoot had a high-legged and compact appearance, but was particularly capable off-road.

It was designed as a radio, command, equipment and crew vehicle, which often had the box body that the small-series manufacturer also reproduced. This version was bought in large numbers, especially by the Federal Border Police, riot police and emergency services such as the THW. Until a few years ago, some of these vehicles were still in use by aid organisations.

Illustration still to be provided:

In the February issue we mentioned in the article on Archistories (page 62) that we would add a photo of the rural US village church (art. no. 403181), but, unfortunately, it was not possible to include it. We would now like to follow up this announcement with action.



We have not yet been able to show a photo of the elaborate rural village church in North American wooden construction (art. no. 403181). Now, we are finally able to do so. Photo: Archistories

The three new colour variants for the building series of rural US houses in the style of the turn of the century also deserve a pictorial appreciation (on page 44 above): olive green (404201), ruby red (405201) and dark blue (406201).

Two Schrax new products available:

At Schrax, among other things, tables are making their way into the programme this month. Because simple things often create the greatest effect, the round and square versions are certainly a good idea.



The US wooden houses from the turn of the century have been supplemented by colour variants in olive green (404201), ruby red (405201) and dark blue (406201). Photo: Archistories

They are 3D printed as single-colour resin models in various colours. They are usually delivered in assorted colours, but special requests can also be made.

A truck loading ramp for modern goods logistics is also interesting. The coloured model is made in one piece and represents a rolling door with rubber curtains and reflectors. It can be easily glued to a wall. With regard to the appropriate mounting height, the model railroader is guided by his models.

This new product is particularly useful when a modern logistics centre is to be modelled, whose simple functional architecture often appears unadorned. Such a hall can be considerably enhanced by adding several loading ramps.

The new products are available at <http://www.schrax.com>.

Intermodellbau Dortmund postponed:

Westfalenhallen Dortmund has announced that due to the current pandemic and the associated official measures, Intermodellbau 2021 cannot take place during the usual time in April.

The organiser is forced to postpone the fair to the autumn in the hope that there will be noticeable successes with vaccinations by then. This is intended to create greater planning security, and, above all, to ensure health protection for all involved.

The new date for the world's largest trade fair for model building and model sports is now Wednesday, 17 to Saturday, 20 November 2021. According to our current information, this means a time overlap with a model building event in Stuttgart.

AZL with design newcomers:

American Z Line will release the first new mould of this year in March: single-domed tank cars with an 8,000 gallon capacity. Built from 1917 onwards by the General American Tank Car Company, these prototypes were designed to transport bulk liquids and were not insulated.

They were used with a range of products from oil to food. The models have blackened wheel sets, fine, separately attached parts, and the usual AZL couplings, are cleanly painted as well as pad printed labelling. The bogies correspond to the Bettendorf design.

The first edition appears with silver-coloured boiler and red shadow inscription as single wagon (art. no. 915000-1) and two-pack (915030-1) for Roma Wine.



The R-70-20 refrigerator cars appear this month with inscriptions for UPFE, i.e., Union Pacific Fruit Express. The yellow cars are offered individually (914801-1), and in packs of two (914831-1) and four (904801-1). The diesel-electric locomotive EMD GP38-2 of the UP follows with the new service number 2002 (62508-8).



Two paint schemes of the EMD F7 are currently available for the Western Pacific. The silver-orange version is available as A-unit (63008-2) and AB-unit (63008-1), the dark green-orange version only as an A-unit (63015-1 / -2), but with two different service numbers available.

New in form is the tank car from 1917 in design for Roma Wine (item no. 915000-1; photo above). The silver-orange F7 are available in two versions, here as AB version (63008-1; photo below). Photos: AZL / Ztrack

Manufacturer's photos of the current deliveries can be found on the following website: <https://www.americanzline.com>.

Business closure in the crisis:

In the meantime, the mystery surrounding the supplier Modellbaukompass, which was still posed in our "Trade Fair Issue" 2/2021, has been solved. Heinz Wagner was forced to make a financial contribution as a result of a noticeable drop in sales, which was mainly due to the fact that he was unable to attend trade fairs, and was thus unable to be close to his customers who needed advice.

Also, with a view to health conditions and a lack of prospects for opening, he finally decided with a heavy heart to close his business on 31 December 2020. We regret this, but can also understand it.

Unfortunately, this means that our readers will no longer have the opportunity to purchase Vallejo products, Gabbert spray equipment, and a few other aids and tools, which were provided with an exclusive reader discount.

Fine SMZ model delivered:

Sondermodelle Z from Vienna recently delivered the 628/928 502 diesel multiple unit as a version for DB long-distance traffic. Its prototype has been operating as the "Sylt Shuttle Plus" between the island and the mainland since 2015.

SMZ has again installed its kinematic close coupling in this model, and, thus, enables rubber diaphragm to rubber diaphragm operation on straight stretches. In curves, the coupling distance is sufficiently extended to be able to drive through the curves cleanly.



The class 628/928 diesel multiple unit now also wears the colours of the DB long-distance service at the SMZ, and is, thus, on the move as the "Sylt Shuttle Plus".

In the premium version, this diesel multiple unit also has extensive digital functions, which are listed on an insert. Packed in a wooden box, it already conveys a very high-quality impression.

Move completed:

In the February issue we mentioned a temporary break in the work of Spur Z Ladegut Josephine Küpper. The background was an upcoming move, which occurred on 1 March 2021.

The load specialist can now be reached at its new address: Ellerstraße 16, 52078 Aachen. Telephone number and the internet pages with electronic sales (<https://spur-z-ladegut.de/>) remain the same.

More new products at Zmodell:

Zmodell from the Ukraine has presented another new product that had not been mentioned to us for the last issue. It is a freight car pack for the Swedish SJ, consisting of two Hbbins type cars (art. no. 92386).



Based on Märklin, Zmodell has put the SJ Hbbins sliding wall cars (Item no. 92386) on the rails as a duo. Photo: Zmodell

They are based on the Märklin models 82384, were carefully cleaned of paint and printing, and then redesigned and properly equipped. Examples are the handwheels that are separately attached to the frame. The lettering was applied with a Mimaki UV printer to the prototypically repainted housings.

Each wagon duo, with different operating numbers, is delivered in its own packaging. The models are safely stored in a firm foam bed (EVA; ethylene vinyl acetate).

All publishers under one roof:

EM Editor-in-Chief Michael Hofbauer, the new “Head of Content”, and overall head of the railway/model railway programme of VGB, Geramond and Alba, has addressed readers and staff of the magazines in a message outlining what has happened in the last few months since the VGB takeover.

For example, at the turn of 2020/21, the staff of all three publishers moved into shared office space at the publishing house in München (Munich). Some long-serving employees from the VGB environment have retired or accepted new professional challenges.

Under the umbrella of the publishing house, the (remaining) magazines are to maintain their independence, but sharpen their profile in order to avoid overlapping topics between the publications. Parts of the programme, such as their own film production or the magazines “Züge” and “Eisenbahn-Journal”, were discontinued. In the future, the trade magazine “Miba”, and also “Eisenbahnmagazin” and “Modelleisenbahner” will be of interest to Zetties.

VGB's sophisticated book programme is to be continued and clearly distinguished in terms of content from the more popular Geramond range, which is geared towards the classic book trade. The digital channels in the form of electronic publication are also to be strengthened.

Faller reissue delivered:

Already available from specialist dealers or directly from the manufacturer is the “Bystanders” pack (art. no. 158052) from Faller, which we only announced in the last issue. The set contains six figures, three each female and male, in summer clothing. This is a reissue of the figure pack with the same item number from 2016.



Faller's bystanders (art. no. 158052) were already in the programme in 2016 and are a welcome re-release.

Lots of Märklin deliveries:

March has brought many new Märklin models onto the market. So just in time with the reopened shops there is also a supply of new products. Among them are the class 216 diesel locomotives in traffic red from DB Cargo (Item no. 88791) and the Rh 2016 from ÖBB (88880), two locomotive models with the latest technical standards.

Both are not only neatly painted, but also finely printed and thus make a good impression. This also applies to the three German Federal Railways SSy 45 heavy goods wagons (82229) loaded with Leopard 1A1 main battle tanks of the German Federal Armed Forces, which we will review in detail in the April issue.



Among the models recently delivered by Märklin there are also moulded novelties with the SSy 45 heavy goods wagons. The quite elaborately printed Leopard 1A1 tank models supplied by Schuco are also among them.

Friends of the ÖBB will receive three orange-painted cars with the “Eurofima 1st/2nd class passenger car” pack (87343) at the high level of painting and printing to which Märklin has been able to improve in recent years.

Also, two more of the announced Intercity cars in product colours are now available. They, too, have many small and fine printing features, are also neatly designed around the corners, and show the once common “wash edge” at the roof transition. Delivered here were 1st class high-capacity coach Apmz 121.1 (87252) and dining car WRmz 137 (87742). The Easter wagon 2021 (80421) can also be collected in time for the highest Christian festival, now that it has arrived at dealers.

There is now a Trainini Index 2020:

The electronic tool for searching for contributions has long been missed by many readers in an updated form. When the conversion of our internet pages to the up-to-date solution currently under development is due, we will probably also be able to offer an indexing function directly in the page offer.

But until then it will take some time and therefore our reader Wolfgang Papst has created a new **Trainini Index 2020**, which can evaluate all 16 volumes and the first two issues of the current volume and is now ready to be loaded and saved (ZIP file). It works in the same way as its predecessors.

The contents, a file called Trainini.pdx and a folder called “Trainini” with two index files included, are to be placed on any computer drive. The monthly issues are then stored on the same directory level in folders separated by year. These folders are named, for example, “2020” for the most recently completed year.

The **Trainini Index 2020** is opened by starting the PDX file, which can also be done from the desktop, provided a shortcut is placed there.

In the input window that opens, the desired search term with possible additional information is then entered and the index search is started by pressing a button.

The result will be all **Trainini®** issues since August 2005 in which this term was found – individual success, therefore, depends on sensibly and precisely chosen terms and correct spelling.

Provided that the annual folders have been created and filled with the issue files, the articles found can be opened page by page directly from the index. If this is not the case, the hit list helps you to load the issue(s) from the pages of our licensed archives.

New products at EtchIT-Modellbau

This supplier was missing from the list of new products in the February issue, but a little later EtchIT-Modellbau also followed suit and announced new models (kits). The Ikarus 66 (art. no. XD066_Z) appears in the usual production method and print quality and is a bus that Reichsbahn fans will not be able to do without after the former DDR model.



Many model railway enthusiasts who build according to East German models will certainly have been waiting a long time for the Ikarus 66 (item no. XD066_Z) as a typical bus in the former DDR. Photo: EtchIT-Modellbau

It can now be ordered directly from the manufacturer at <http://etchit.de>.

NOHAB models at the 1zu220-Shop:

The NOHAB diesel locomotive V 170 in the silver-blue design of the BOB (Art.-No. 49.126.01), exclusively produced by FR Freudenreich Feinwerktechnik for the 1zu220-Shop, has now been delivered and is available as an analogue version.

With the additionally orderable item 00.125.00, the model becomes a digital locomotive with operating sounds and extensive functions. Our readers can get an impression of what this model then has in store by watching the video linked on our home page: The model shown there has the same range of functions.



The colourful NOHAB V 170 1151 of the BOB (item no. 49.126.01) is a special edition of FR Freudenreich Feinwerk-technik for the 1zu220-Shop. Photo: 1zu220-Shop

The locomotive is a scale replica powered by a Maxon RE8 motor (bell armature drive) with flywheel mass. All six axles serve both the drive and the power pick-up.

The gearbox was designed so that the top speed corresponds to the converted one of the prototype. Thanks to a milled brass main frame, the locomotive has a high weight of 47 grams, which gives it an impressively high tractive force. The lighting, which is direction-dependent in analogue mode and switchable in digital mode, is done with maintenance-free light-emitting diodes.

New products also at NoBa-Modelle:

NoBa-Modelle (<https://www.noba-modelle.de>) has also released new products. Among them are completely new designs as well as modifications of already known products. In future, road traffic will be enriched by the Hanomag F 55 with beer barrels as a finished model (art. no. 6233RF), but six beer barrels are also available individually in unpainted form (10260R).

Five small cable drums (10613R) are recommended as additional loads, one of which can also be found on a cable drum trailer with cable drum (6218R). The Actros with sweeper (6376R / 6376RF) is available

both as a kit and as a finished model, as is the T5 transporter with flatbed for tradesmen (6203R / 6203RF).



Two of the current new products fill previous gaps in the Z-gauge assortment: The sweeper (item no. 6376F; photo above) and the Neunkirch tram as an articulated multiple unit (5856; photo below). Photos: NoBa-Modelle



For a long time, no catenary repair railcar had been available for Z gauge. So NoBa-Modelle has unceremoniously added the contemporary detailed VT 55 (5212RF) to the assortment. Photo: NoBa-Modelle

The formerly ubiquitous transformer station (4077R) is helpful for designing layouts, while the five platform edges and two corners (5502R) are far more timeless in their construction. A nice little gem is the Neunkirch tram (5856), which is only one of three new track models.

The MD4ie makeshift baggage car, which is now available with NoBa bogies both as a kit (5316R) and as a finished model (5316RF), should also be mentioned here. The latest addition is the VT 55 catenary repair railcar, later class 701/702, which is also available in both versions (5212R / 5212RF).



The Bavarian cattle car 82005 with spoke wheels from ZetNa220 serves the early era I.

replacement types of the twenties. It also has additional inscriptions on the individual flaps, the decals could be applied much better than in the first edition. This model will be presented in detail in the April issue.

Deliveries and announcement at ZetNa 220:

The small series manufacturer ZetNa 220 from Catania in Sicily (Italy) has started to deliver two of its wagon models again after refinements to the wagon markings occurred.

A short freight wagon of the Royal Bavarian State Railways (K.Bay.Sts.B.) in unbraked version is offered for Era I. The green-painted cattle wagon is a very special model.

The green painted cattle car bears the road number 82005, has spoked wheels and belongs to the early era I before the turn of the 20th century.

Also available again is the small cattle car V 23 of the German Federal Railways (Era III), which belongs to the



A uniform and appealing appearance is offered by the cattle car V 23 from the stock of the German Federal Railways, which ZetNa 220 has also realised as a Z-gauge model.

The next project announced is DB's VImms 63 cattle car from their new construction programme. There is already a first model to be seen, but it still has errors that will be corrected in the further course of the

project. For example, the prototype had a smooth roof without beading, and the first model is also a little too short.

Only one new delivery at MTL:

Only one wagon is available for delivery at Micro-Trains in March: The single-domed 39-foot tank wagon (Item No. 530 00 490) with blue-yellow paintwork belonged to a series of eight similarly painted examples of the St. Lawrence Starch Company from Ontario (Canada). Used for the transport of corn syrup, they were in use until the early 1980s between Port Credit and confectionery and food companies in Eastern Canada.

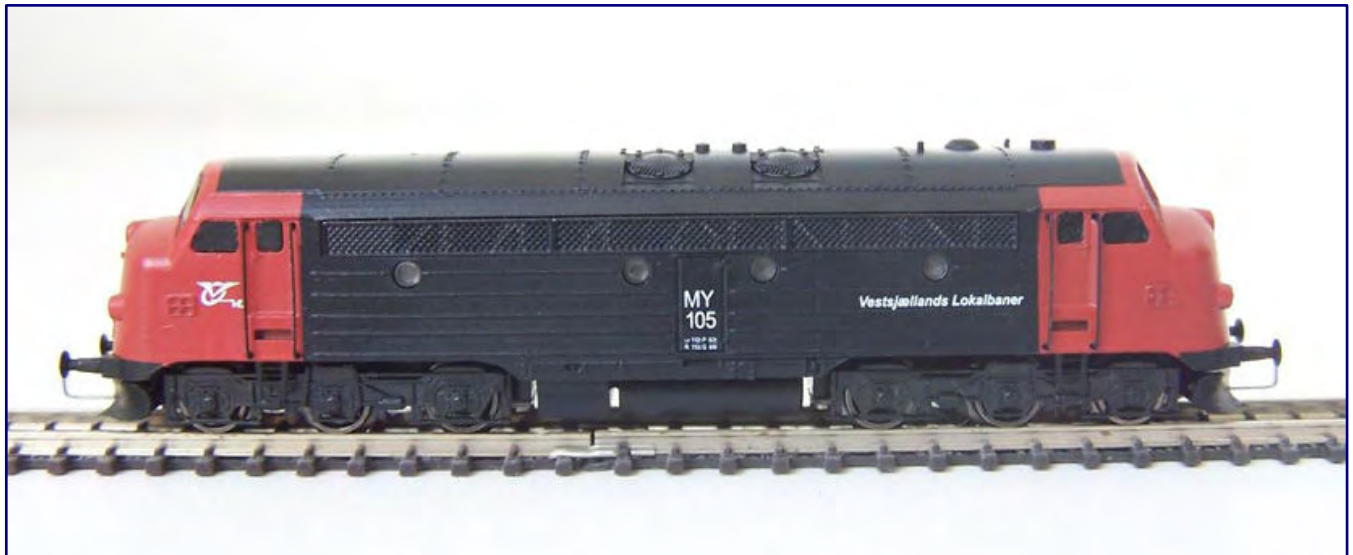


The DB's new-build wagon Vlmms 63 exists at the present time as the first prototype.

Micro-Trains products are distributed by Case-Hobbies (<http://case-hobbies.de>), among others.

Smallest edition announced by Freudenreich:

FR Freudenreich Feinwerktechnik is expected to produce a small edition of only eight examples of the NOHAB locomotive MY 105 of the Vestsjællands Lokalbåner (item no. 45.125.01) in August. The preliminary sample can already be seen in the photo.



The NOHAB MY 105 from Vestsjællands Lokalbåner (art. no. 45.125.01) is only published in a very small edition. Photo: FR Freudenreich Feinwerktechnik

Interested parties should hurry to secure one of the last four free copies (as of the editorial deadline), which can be selected analogue or in DCC sound version. The technical equipment corresponds to the previous NOHAB models, we had published a test report in **Trainini®** 5/2019.

About the prototype of this locomotive: When DSB disposed of its MY locomotives at the turn of the millennium, several machines were transferred to private railway companies. The former MY 1145 found a new home at Vestsjællands Lokalbåner (VL) in 2003 and was given the designation MY 105. However, it retained its last DSB livery in black/red, which also corresponds to the condition of the model.

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Livestock Car V 23 from ZetNa 220

**Märklin's new Heavy-Duty Wagons
Wooden Overseas Transport Crates**

Introduction

Dear Readers,

Spring has not yet really made itself felt here. The first blooms, blossoms and leaves are sprouting on bushes and trees, but that's enough for some visual guidance on model railways.

Cool temperatures invite us to continue our model railway work in the basement. That keeps us happy and, besides, healthy. As much as we like to visit "God's free nature", distancing remains the order of the day.

Happy is the one who can fade out all the limitations and keep an eye on the positive that every situation can bring. Certainly, a year ago I could not have imagined what my daily routine would be like today, but I don't feel that restricted at all.

Some habits have changed drastically, I have massively increased the amount of individual sport I do in the fresh air, and, many contacts that I sorely miss, I now and for the time being manage via video conferencing.

And that can be quite pleasant and fun. Just a few days ago, for example, we had another video conference between the editorial team, translators, and technical website support.

In addition to free conversation and showing our own work, we also discussed our ongoing joint work for a modern website: the technical framework including the design is in place, much of the content has been filled in and we are working on the finishing touches and many translations. At one point or another, we are getting permission from the manufacturers, because we would like to show their logos there, as well.

In mid-May, we want to publish what we have put together, first and foremost, of course, Stephan Bauer, who will continue to look after the technical side of the administration system. I would like to thank him very much for this, and I hope that you, as readers, will join us when our new pages see the light of day.

Celebrate with us! You won't have to wait too much longer, and until then you will have some reading material at your hands: three new model introductions, including two freight car types, another manufacturer portrait, book recommendations, and, of course, lots of letters to the editor, and news.

Month after month it remains challenging, even a huge effort, to compile, build, photograph and write a full magazine in our spare time. But it has rarely been so much fun!

We are all happy, and I am especially happy, to see how actively people are participating in the **Trainini®** "project", which has now been going on for well over 15 years. The number of helpers, readers, modellers, authors and photographers has grown and grown. And I am proud of that: thank you!

Sin-Z-erely,

Holger Späing



Holger Späing
Editor-in-chief

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We thank Eisenbahnstiftung for historic photos and Peter Sturm for his contributions.

Date of publication of the German language version of this issue: 27 April 2021

Cover photo:

V 36 237 pulls a cattle train from the loading siding. Directly behind the locomotive hangs a V 23 (ZetNa 220) shed wagon of the exchange type, one of the rarest of the types active in the fifties. Its end is approaching with the DB's rebuilding and new construction.

Märklin SSy 45 heavy goods wagon

Because all good things come in threes

Two conversions of the SSy 45 heavy-duty wagons already existed. However, they were only available in manageable numbers, and were not always convincing. This type was long overdue at Märklin. But recently these spring new products 2020 rolled in, and today they are put to the test by the Trainini® editors.

We already explained the history of the SSy 45 heavy-duty cars, their replicas and successors in detail in **Trainini®** 12/2017 when we presented models of all three frame designs. Märklin recently followed suit with its own development showing the final design with trapezoidal longitudinal beam chosen from 1943 onwards.



Two M47 tanks (still without national emblems) are shunted to the ramp by 93 754 on SSy 45 in 1957, probably in Niederlahnstein. Presumably, they are part of the initial equipment of the newly established Bundeswehr. Photo: BD Mainz, Eisenbahnstiftung

Why this choice was a good one and what options it opens up for the future will be explained in the following. First of all, however, it should be noted that this is already the third series implementation of this model.

Nevertheless, it was and is a gap in the Z-gauge programme, because the predecessor models either showed glaring weaknesses, or have long been unavailable. So, we share the assessment that such a model was long overdue in the Märklin programme, and are pleased that Zetties now no longer have to look enviously at the larger gauges.

The templates were developed and built during the Second World War for tank transport, but the four-axle cars were not equally suitable for all types of tanks in use. They could carry tanks weighing up to 46 tonnes, while the heavy “Tiger” and “Panther” battle tanks weighed 57 and 70 tonnes, respectively, and required a six-axle vehicle.

For these vehicles, the basic design of the longitudinal beam was adopted or reused. We have also known these vehicles as SSy 46 for about two decades from the Märklin programme.

It is certainly not completely off the mark when we write that the SSy 45 experienced its heyday in peacetime after 1945. The occupying armies and later also the Bundeswehr used them and their successors SSys 55 for tank transport as well, because they were quite sufficient for the M47 and M48 weighing 45 to 46 tons.

But their intended use, with or without the inserted stanchions, was much greater, and, so, they also performed valuable services in the reconstruction and export of the economic miracle period. After all, heavy engineering products also had to be transported, even if it was only to the harbour quay for disembarkation overseas.

This clearly outlines all the things that can be done with the new models. We are, therefore, not limiting ourselves to the wagon pack in its delivered form, but would also like to give you some ideas for civilian purposes. But, let's take things one step at a time, because, first, we have to look at the quality of the delivered models.

Two mould innovations at the same time

Märklin is initially offering the heavy goods wagons from the 2020 spring new products in a pack of three (item no. 82229) as the SSy 45 of the German Federal Railways for Era III. Three models of the Leopard 1A1 battle tank, which are also new in form, are included as loads. Their prototypes were in service with the German Federal Armed Forces from 1975 onwards.



New from Märklin freight wagon pack 82229 are not only the SSy 45 heavy goods wagons, but also the models of the Leopard 1A1 battle tank of the Bundeswehr, exclusively supplied by Schuco.

The tank miniatures differ somewhat from the first catalogue illustration, and, therefore, caused some irritation. In this case, the gun is affected by a thermal imaging camera on the side (as part of the aiming devices for night combat), which was part of the combat upgrade to version 1A1 in the prototype.

This modification was, apparently, still possible, because the load also had to be newly constructed, and is exclusively supplied by Schuco. The tanks are made of plastic, painted olive green and have a rotating turret.

For transport, the gun can be turned to the rear (parade (marching) position), when unloaded, it points to the front. In addition to the convincing engravings, the printing is also elaborate, including legible registration numbers, different turret numbers (321 to 323; see also, photo on page 5) next to the national emblem, guiding cross, and colour-contrasting tail lights.



Overall, the tank models look convincing and impress, above all, with their extensive printing, which is particularly clear in the rear view. Only the side chain guard seems to deviate from the prototype, the diagonal sections are only visible in the enlargement. The retrofitted attachment on the gun barrel caused irritation among customers.

The side plates on the superstructure, which serve as chain guards in its upper course, seem to deviate somewhat from the original. However, macro views reveal that this is more of an illusion resulting from the extreme reduction in size (which, by the way, can also be found on the Z Panzer model). Consequently, only a slight exaggeration of this feature could have avoided it.

It is also interesting to see how Märklin envisages attaching the tank models to the new transport wagons: It should probably be secure, removable, and, as invisible, as possible. So, each package comes with a strip of three olive-green plasticine pads that are supposed to be peeled off and lightly rolled. They can then be pressed onto the bottom of the tank and held in place with their opposite side on the floor of the wagon.



Three plasticine pads (in the photo at the front) are included in the pack for temporarily, but securely, attaching the tank models to the heavy-duty wagons. The olive-green strips should be rolled up and pressed against the underbody of the load. The enclosed stanchions can be used, when using narrower loads that can be placed between them.

But more important than the tanks and their temporary mounting are probably the cars themselves. We have therefore also taken a close look at them, measured them extensively, and compared them with the prototype dimensions. When we discuss them in a moment, we have some important information to share with you.

Review of the predecessors

Before we look at how well Märklin has actually implemented the SSy 45 heavy-duty wagons, we should take a look back at the two predecessor models from the small-series sector. Remembering their weak points certainly shows the most important areas of action that had to be taken to heart for a good large-scale production model.

So, let's start with the oldest model of this type, once offered by Schmidt from Hameln. It used Märklin bogies, but the correct or sufficiently similar ones were not yet in the mould stock at that time. So, Schmidt resorted to the widely used Minden-Dorstfeld bogies, for which the prototype can certainly be used with a trick or compromise.

But, also, the superstructure made of cast brass as long beams and loading area was not spared from mistakes: It was too short, which hardly anyone would have noticed at the time. Likewise, most owners will have overlooked the fact that the two long sides were identical, and not mirror-axis.

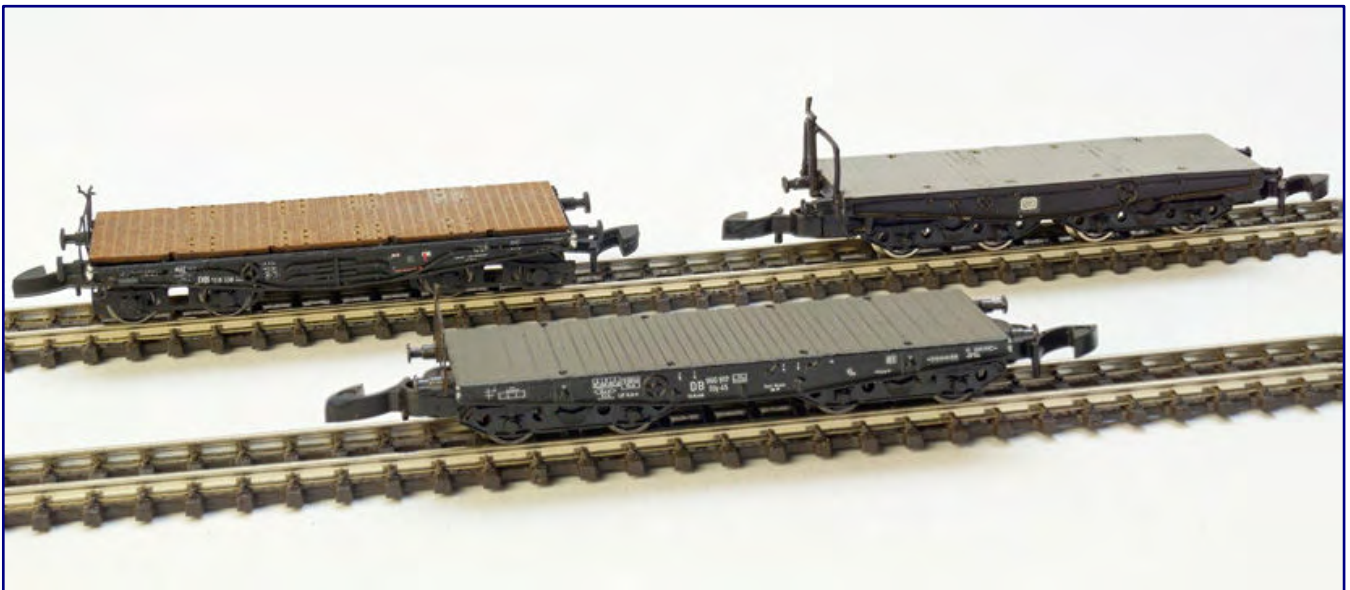
On the Schmidt model, the floor-operated hand brake wheels as well as the change-over levers of the brake system were, therefore, not opposite each other at the same end of the car, but were located at opposite ends, contrary to the prototype. The lettering was limited to the DB logo, applied as a decal, but in an incorrect negative representation.

Four years ago, Gerd Kurz, based in Australia, also ventured into these heavy-duty wagons under the brand name "eNKay-Design". Very positively, he took into account all three beam designs (fish belly / fish

belly reinforced / trapezoidal shape). Also, the loading area was considered with and without loading sills, as well as, with and without insertion possibilities for stanchions, in all variations of the prototype.

Nevertheless, it remained a unique excursion into the world of German prototypes. The reason for this was serious quality deficiencies, which led to complaints, persistent dissatisfaction, and, as a consequence, rapidly dwindling interest in buying from the German sales partner.

The weak points here were in the area of the bogies, which were made of etched sheet metal, sometimes in several layers. In order to achieve good running with these, the highest precision is required both in drawing and assembly. Axles that fell out in the tests, indicated that the required level of precision was not achieved.



Märklin's new model (front) in the company of its predecessors from Schmidt (back right) and eNKay-Design (back left). In the article we discuss their positive and negative sides, and see how the Märklin miniature does in this place.

In addition, there were parts or sheets that were falling off, paint chips or flaking, especially around the edges, and poorly applied sliding decals. In almost all tested specimens, they were infiltrated by the matt clear varnish used for sealing, and showed a disturbing grey haze that made them partially illegible.

To soften this harsh verdict, it should be added that the supplier had apparently been overrun by the enormous demand and did not want to refuse anyone. In the end, however, his production capacities could probably not be adjusted to meet the required quantities.

However, this also shows how sorely this type of car had been missed by model railway enthusiasts. There is no clearer indication from the market that a large-scale manufacturer is needed here. And that is why we now want to work out what Märklin has learned from this and put into practice.

Finally, a model from Märklin

The joy must, therefore, have been great when Märklin announced the SSy 45 in its final design, with trapezoidal girders as a spring new product in 2020. It was to be another year before delivery, but now they are finally in the focus of prospective buyers.

And, right from the start we can state that Märklin has not repeated the mistakes of its predecessors: The prototype dimensions were almost, without exception, perfectly adhered to, and only the buffer pads are



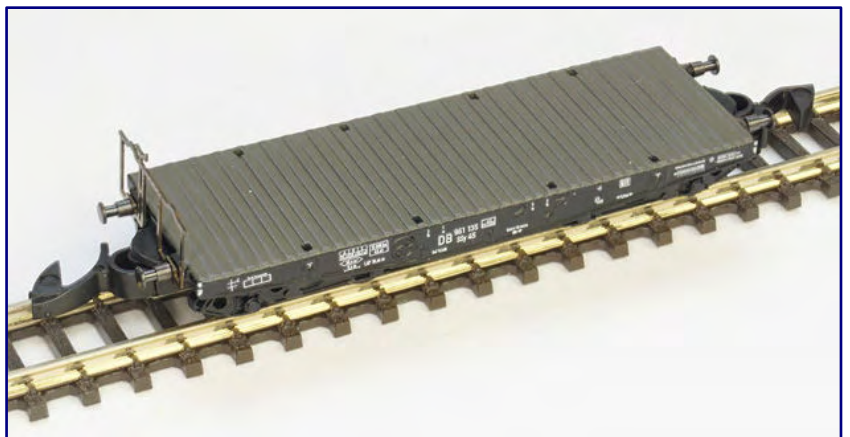
The new moulded units are delivered as a freight wagon pack (art. no. 82229) with three DB SSy 45 heavy goods wagons and three Leopard 1A1 tanks, as loads.

almost half a millimetre too small in diameter, which is due to operational reliability. We will talk about the bogie axle position and the distance between the bogie pivots, separately.

The quality of the product (engravings and detailing) as well as the production (assembly, painting and printing) meets the highest standards. Encouraged in this way, the three wagons from the package still have to be put through their paces.

The chosen structure of the girder corresponds to the prototype's design in the final design with DB-specific modifications, such as UIC rope hooks, and a loading bridge without loading sills.

No stanchion holders have been reproduced on the side member, which follows later operating conditions, but can also be indicated by prints in some cases, as on the SSym 46.



The structure of the SSy 45 corresponds to the typical condition of its time in service with the DB: loading sleepers are no longer to be found here, but insertion options for the stanchions are.

As an example, for good engravings on the zinc die-cast superstructure we would like to mention the note holders, which were not shown as details on the two predecessor models. The clean and complete printing of the models (with different road numbers) only lacks a colour contrast of the handbrake wheels and brake lever.

Data and dimensions of the heavy duty car SSy 45:

	<u>Prototype</u>	<u>1:220</u>	<u>Model</u>
Length over buffers	10,800 mm	49.1 mm	49.4 mm
Width	3,150 mm	14.3 mm	14.4 mm
Height over railhead*	1,290 mm	5.9 mm	5.7 mm
Length of the wagon body	9,500 mm	43.2 mm	43.2 mm
Loading width btw.stanchions	2,735 mm	12.4 mm	11.5 mm
Stanchion height	1,085 mm	4.9 mm	4.9 mm
Bogie pivot distance**	6,150 mm	28.0 mm	26.4 mm
Bogie axle distance	1,800 mm	8.2 mm	9.2 mm
Wheel base	6,000 mm	27.3 mm	26.8 mm
Wheel diameter	940 mm	4.3 mm	4.5 mm
Net weight	14.9 – 16.3 t	---	6 g
Years of construction	1941 – 1945 (Reconstruction 1952 – 1955)		
Quantity procured	1,930 (+ 423 Reconstruction)		
Retirement	until 1991		

* Wagon floor (not raised railing at the end).

** With bogie type 977 in the prototype (model: type 931 Minden-Dorstfeld)

The paintwork was done in the correct colour RAL 9005 deep black of the prototype, whereby the loading ramp has been sensibly set off in a shade of grey. The colour chart shows it as RAL 7022 umbra grey, which we consider a good choice.

In some cases, the railing mounted on each wagon would be dispensable for a shunter: In the prototype, it was not always mounted on the wagons and could also be lowered in order to drive over the ends of the wagons and thus be able to load or unload overhead ramps. If you want to recreate this, you can pull off the buffers, and, then, also remove the railings.

The bogies of the three new models deserve separate consideration and evaluation: They correspond to type 931 (Minden-Dorstfeld),

the prototype dimensions entered in our table belong to type 977 (pressed sheet bogies).

This points to a mistake or compromise, depending on how you look at it, on the models. In the prototype, the SSy 45, later called Rlmpmp 700, always ran with the BA 977, and we have therefore also used this for the prototype comparison. The distance between the pivots is therefore about 1.5 mm too small, the distance between the axles in the bogies a whole millimetre too large.



The railings for the hand brake operation (in the photo at the right end) would not be necessary on each of the three wagons and would then also emphasise in the model the overrunning in the area of the buffer beam. The bogies used are of the Minden-Dorstfeld type (see prototype photo on page 4), and do not match the operating condition indicated by the inscriptions.

But how does Märklin come to install “wrong” bogies, when suitable ones have long been available in the mould stock? We see the reason in closely related cars that can be derived from this new design with



For the bogies used in the model, which in the prototype only, the successor type of the SSy 45 and modernisations from 1988 onwards were used, the distance between the bogies' pivots and the bogie axle position are exactly correct. However, despite the close coupling, this leads to a large coupling distance, as can be seen from these two views.

slight compromises or minor adaptations: The successor SSys 55 (Rlmmnps 651), built primarily for crew troops and derived from the SSy 45, was equipped, as follows:

The dimensions taken from the model (prototype / 1:220 / model) for fitting bogie type 931, then also match: 5,800 mm / 26.4 mm / 26.4 mm (distance between bogies) and 2,000 mm / 9.1 mm / 9.2 mm

(bogie axle base). By the way, the replicas of the bogies have undergone modifications: Instead of being inserted by means of a pin, they are screwed to the wagon floor.



A look at the car floor bottom reveals some replicated details of the brake system and the bogies, which in this case are attached by screws.

Some of the SSy 45 replicas (Rlmp 700) still in existence at the end of the Bundesbahn era also received these bogies as part of a modernisation between 1988 and 1990. The cars converted in this way, and, also modified in the brake equipment, were operated as Rlmmp 651, which were approved for a higher speed. They were also painted reddish brown RAL 8012, as part of the work.

In use on the layout

Before we think about the use on the layout, we have to face a discrepancy and decide how to deal with it: Märklin has labelled the wagons as SSy 45 with six-digit road numbers, as they were in use until 1964 (Era III).

However, the bogies do not fit for these cars until 1988 (epoch V) and also make for an almost disturbingly large coupling distance. The tanks were used from 1975 (era IV), the M47 and M 48 of American origin would have fit better. All in all, this results in a mixture, the sense of which is not clear to us, although we are looking at successful realisations, here.

If you can overlook this, you can use your heavy-duty wagons as Märklin offers and intended. They can then also be supplemented with the older SSym 46, which were offered over many years in a wide variety of designs and are certainly available in sufficient numbers. Additional tank models can then be purchased from Z-Panzer.

Thus, even today, military transports can easily be reproduced on a sufficient scale. Fortunately, Märklin's new models, thanks to their metal construction with 6 grams of dead weight, and a low centre of gravity, run smoothly, and do not place excessive demands on the locomotive pulling them.

Of course, it is also possible to use the wagon and the tank separately. This is exactly when it gets exciting, because the wagons can be used individually, in pairs or even in threes, without any problems. In addition to engine cases (Modellbahn-Union), or transformers (Artitec), the wooden crates from Joswood, which have also just been delivered, are a particularly good alternative, in this case.



On a rain-covered summer day, two class V 100 diesel locomotives are well utilized hauling a heavy Bundeswehr tank transport train.

We describe the quick and easy assembly of the small kits in a separate article. Here, we will only deal with the use of the load: In the time depicted according to the wagon inscriptions, containers were not yet common, as transport protection.

Heavy engineering products needed to be transported on heavy goods wagons, which is why the models are highly recommended for this purpose. On their long journey to the harbour and further on by ship, however, the machines or their parts had to be protected from the weather and mechanical effects.

For this purpose, the shippers used overseas crates at that time, as Joswood has reproduced them in no less than three different sizes (suitable for Z gauge) with different inscriptions. With the versions "O&K" and "Krupp" we have chosen two well-known companies, which also offer a very different scene: On the one hand, there are three small crates of simple design and two larger ones with an additional frame for reinforcement.

This should arouse the curiosity of any observer even as they move past! What heavy things could be in them and where are they being transported? There is no doubt about their purpose: besides the company logos, they bear the usual inscriptions and notices of the time, as well as, clear markings indicating which side is up.

Let us now summarise our findings in conclusion. Märklin has fulfilled a great wish of its customers, and we are curious to see which versions of this basic model will await us in the future.

We only hope that those responsible will then consistently decide on a composition that also fits together in every respect in terms of time. For the really well-designed and, also, technically convincing models, this lack of consistency is a flaw, but it does not lead to a devaluation of the model.

On the contrary, we nominate the SSy 45 heavy-duty wagons for the best new release of the year 2021 in the category wagons. The reason is simple: Märklin has better fitting bogies in its portfolio, which would also allow for shorter coupling distances.



E 40 210 has coupled to it, a long goods train, whose destination must be one of the large seaports. On the four heavy goods wagons hanging as a group of wagons directly behind it, overseas crates of well-known German engineering firms can be found.

So, it doesn't take a feat of strength to do everything right in another edition, and, thus, come even closer to a perfect model. We, therefore, remain full of praise, and are already handing out advance praise, here.

Base model manufacturer:

<http://www.maerklin.de>

Wooden boxes as an alternative load:

<http://www.joswood-gmbh.de>

<https://lasercut-shop.de>



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A00206 - Workshop

Livestock car V 23 from ZetNa 220

Last Cattle Ride

The role of the V 23 “Altona” shed wagon remains manageable due to its rather small number of units in the stock of the German state railways. And yet this type, closely related to the Gr 20 “Kassel”, was the last significant one of this type that was still missing in 1:220 scale. With ZetNa 220, a small series manufacturer from Italy has recently closed this gap.

From about 1890 onwards, the number of covered freight wagons, still called covered (bedeck in German) until 1915, rose sharply. Live cattle were also transported from the countryside to the slaughter houses in the cities in wagons of this design.

Whereas, large animals such as horses, cows and oxen were transported in wagons of the standard design, in which the doors were locked open for air supply and a gate was hung in the opening, a special wagon was needed for small and medium-sized animals.



As this photo from 26 September 1956 shows, large animals such as cows and oxen were transported in covered freight wagons: Here, cattle bought at auction at the market in Husum are driven into the wagons. Photo: Walter Hollnagel, Eisenbahnstiftung

These were the small livestock wagons with double floors, i.e. two loading levels, which were once classified in Germany with the main type letter V (Verschlagwagen (closed wagon)). They were intended for transporting calves, sheep, goats, pigs, and also geese.

From the 1930s onwards, a process began that was to make this type of wagon gradually dispensable, although it was still able to last until 1993 with around a dozen examples: the development of the refrigerated wagon with heavy insulation and ice loading. This made it possible to slaughter the cattle beforehand and keep the meat fresh during transport.



The 2,133 units of the A8 type of livestock wagons built from 1913 onwards, designated Vh 14 by the DB, were the direct predecessor of the later V 23. This example was photographed in April 1958 at Uchte station. Photo: Reinhard Todt, Eisenbahnstiftung

Until the 1920s, the stock of wagons of the association type seemed to be sufficient, until the Deutsche Reichsbahn companies had a new interchangeable type of wagon developed in parallel to the Gr Kassel (later Gr 20).

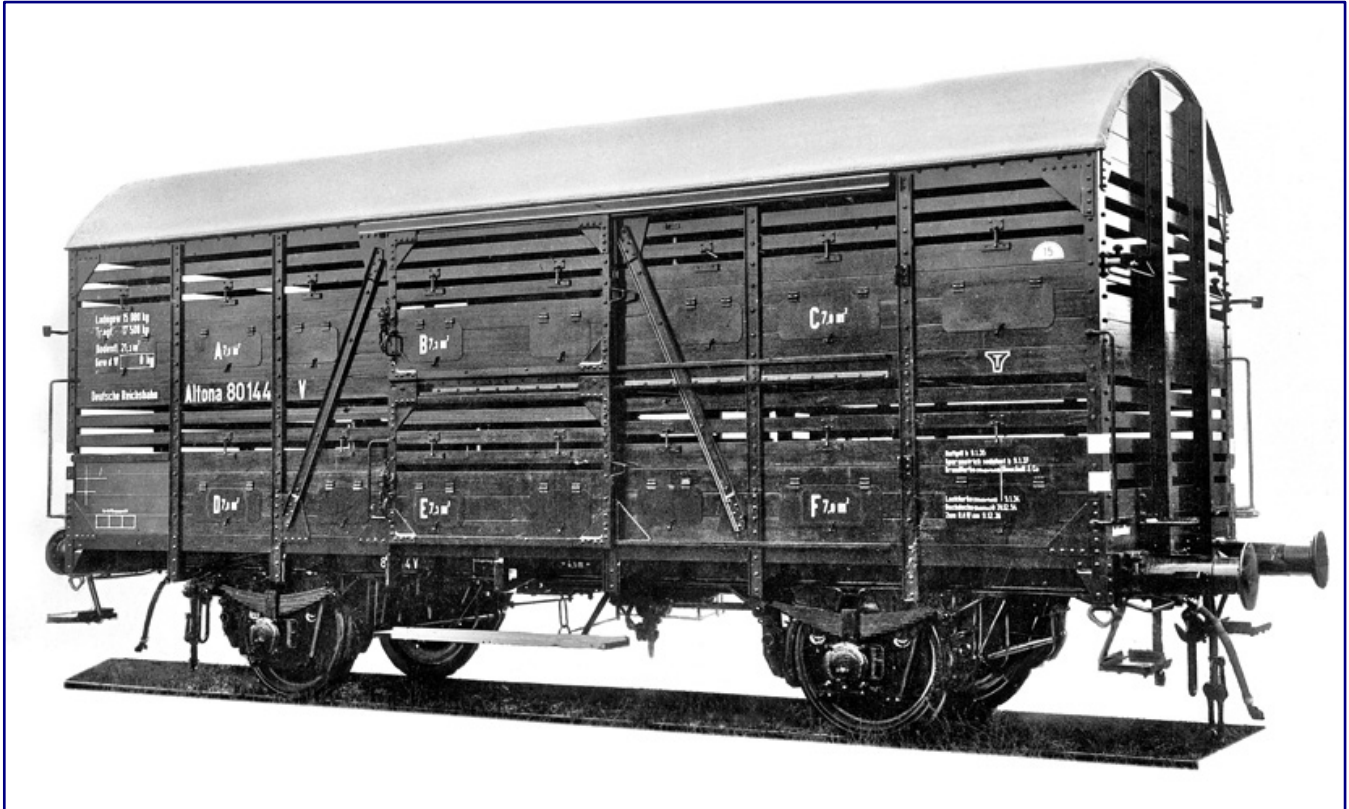
The use of as many identical parts, as possible, now had priority over the dimensions of the predecessors. In this way, the DRG also tried to achieve rationalisation effects with its wagons. Two experimental wagons were followed by a first series of ten wagons with suspended doors, two on top of each other on each side of the wagon.

In total, however, only 290 cars of this type were procured between 1927 and 1928. There was no longer a need for more, as the number of animal transports fell significantly due to bans imposed because of the occurrence of foot-and-mouth disease in Europe. Both the import of cattle for slaughter and transit were significantly affected. Only in 1934/35 was there a further procurement of 37 slightly modified versions.

As with the earlier unit designs, the wagons classified as V(r) Altona had three compartments per loading level, which could be divided by partition gates. The upper section of each level was characterised by open slatted walls, which were intended to ensure sufficient ventilation during the journey. The lower part was closed and had inspection holes that were closed by hinged sheet metal plates.

Due to the still manageable number of units, somewhat less than 100 cars were delivered to the Deutsche Bundesbahn. There they were given the designation V(r) 23 and service numbers from 330,000, which they shared with the older types of livestock wagons.

Stock inventory lists from 1953 still showed 88 units, which can be considered stable until 1960. From then on, the V 23 and also the older Vh 04 and V 14 were increasingly used for the DB's new construction and conversion programme. Profiles and other usable parts were used for the new construction of the Vmms 63.



This photo shows the Altona 80144 livestock wagon, a wagon of the type later called V 23 by the DB. It was probably photographed immediately after its delivery in 1928, because it has not yet been weighed, as the dead weight not entered in the grid reveals. Photo: RVM, Eisenbahnstiftung

When their conversion programme came to an end in 1962, only four examples were left, which went the way of old iron two years later. At the same time (Vh 04) or very close to it (V 14), the end also came for the association types.

Before that, however, some of the cars had been modernised from the beginning of the fifties: Sheet metal roofs then replaced the old, covered wooden roofs and end brace reinforcements were installed in isolated cases. On most of the coaches, the inspection holes and sheet metal flaps on the doors disappeared a short time later, often also those on the side walls.

The sudden drop in demand for small livestock wagons, which then continued to decline, should not obscure the fact that the transport of livestock was still a constant need and source of income for the Bundesbahn for a long time.

In 1953, for example, 780,000 cattle and 110,000 horses were still transported, for which a covered freight wagon was required. The wagon category considered here accounted for 2.3 million pigs, 670,000 sheep and goats, 500,000 calves, 275,000 piglets, and 85,000 poultry and other animals.

A good choice

Even though the preceding V 23 shed wagons were built in numbers far beyond 1,000 and make it look almost like a splinter genre, it is indispensable on the model railway, from our point of view.

Its appearance sets it apart from all its predecessors because of the shape of its roof and, in terms of overall length, just as clearly from the new DB model. This gives it a high recognition value and provides variety in cattle trains, which only require a short length and are therefore particularly "layout-friendly".



Well protected by a metal box with a foam insert, the ZetNa 220 V 23 closed (livestock) wagon (item no. B001) travels from Italy to its buyers.

Against this background, the decision of ZetNa 220 is to be welcomed, and we were curious to see how the small firm from Sicily would implement it. After a long wait, we were then able to hold the model (art. no. B001) in our hands in person shortly after the turn of the year.

Beforehand, there was still some work to be done in the background to ensure the best possible result. For example, the lettering on cattle cars with open, slatted frames is particularly demanding on its makers.

Pad printing does not pay off for very small series, especially since special tools have to be procured for this. And, the precise placement of the imprints on a model is not a matter of routine, either.

Transfer imprints use heat to ensure the transfer from the carrier foil to the target surface. And this is generated by friction, which creates another problem with these tiny surfaces.

What remains now are wet decals, which have a carrier foil that is also applied to the model and made largely invisible by sealing with clear varnish.

The ZetNa 220 project group also opted for this solution. An optimum of the thinnest possible film (for largely invisible edges) and sufficient thickness must be found, so that it does not tear when moved along the many edges.



The model's inscriptions proved to be a major challenge. Although the final solution cannot keep up with all the other small-series manufacturers in terms of printing sharpness, it now leaves a good impression overall, especially in view of a very moderate price.

The efforts that were made here over many months have definitely paid off. Certainly, not everything could be perfect, because some edges remained unchanged. But this is primarily due to the fact that a (slide) decal image cannot be cut exactly to the tenth of a millimetre to the width of a slat. There is nothing to criticise in terms of print sharpness, completeness, font and placement.

From the roof to the running gear, the paintwork also follows the original from the Bundesbahn stock. The colour of the body, which looks a little lighter than RAL 8012 brown-red on the colour chart, is within the expected range, which results from the definition of RAL colours.

By the way, this really slight deviation can also have another background: The colour shade used by the FS for decades differs just as slightly from the one used in Germany at the same time. On the RAL card it corresponds to RAL 3009 oxide red, so it is formally classified as a red tone.

For the model this is not disturbing, but rather welcome: Even in the prototype, not all freight wagons look completely the same! Ultraviolet rays, impurities and the effects of weathering make for quite perceptible differences that should not be ignored even on a small scale if scenes are to look realistic and not "as if, licked".

With this knowledge, let us now turn to the technical realisation of the model. The model is completely made of metal (brass). Body and undercarriage parts are etched, folded and soldered. Here, too, we find typical small-series techniques that should be very familiar to model railway enthusiasts.



We find the approach of reproducing some peepholes in an opened state, very interesting. This can be seen well here in the lower compartment on the half of the coach to the right of the sliding door. If you look carefully at prototype photos, you will often find this there, as well.

For this conversion, a car from the larger construction lot of 1927/28 was chosen that had not undergone any external changes by the Bundesbahn: the full number of inspection holes and flaps on the body and doors are therefore, unchanged. A tin roof was also not added to this model.

It should be superfluous to point out that such a model should be handled with care. After all, brass is a rather soft and easily bendable metal. What is most welcome when assembling the model makes demands on the owner of the finished model in terms of careful handling.

However, this should be done simply because the price of a small-series model is naturally higher than that of a large-series model, but, in this case, it is very moderate. In its padded metal box, the little wagon is well protected anyway, which it will certainly appreciate during its long journey to customers in Germany.

By the way, we were very impressed by the almost perfect adherence to the dimensions of the prototype. All deviations, even the otherwise often critical width, are within a few tenths of a millimetre and thus remain within the error range of such measurements.

The only deviation that remains of note is the 3 mm missing length above the buffers. The buffers can be identified as the cause. At first sight this seems quite small, which applies to the diameter of the buffer as well as to the shaft, which is obviously missing 1.5 mm each.

If this should seriously bother someone, a remedy would be easy: remove the buffers and replace them with small series manufacturers' parts, e.g., from FR Freudenreich Feinwerktechnik. However, it would probably be too much for train operation.

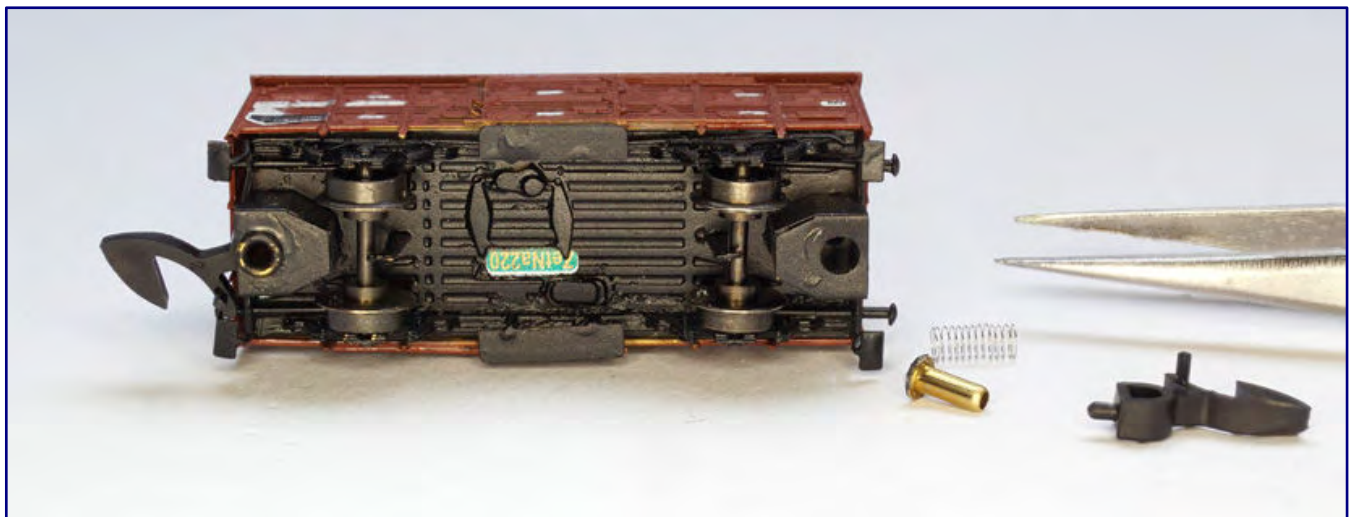
Data and dimensions of the livestock wagon V 23:

	<u>Prototype</u>	<u>1:220</u>	<u>Model</u>
Length over buffers	9.100 mm	41,4 mm	38,4 mm
Greatest width	3.050 mm	13,9 mm	13,7 mm
Height above railhead	4.100 mm	18,6 mm	18,7 mm
Length of the car body	7.800 mm	20,5 mm	20,3 mm
Wheelbase	4.500 mm	27,3 mm	26,8 mm
Net weight	12.500 kg	---	7 g
Years of construction	1927/28 & 1935		
Quantity acquired	290 + 37		
Retirement	through 1964		

The axle holders are made of several layers of brass and use Märklin wheelsets with pointed bearings. Because of only one-sided isolation the wagon is not voltage free. A central soldered bracket additionally prevents a loss of wheelsets when rerailling or lifting.

Interesting is also the coupling guide on this model, as well as on others of the same manufacturer: Here, too, parts from Märklin are used with system couplings and pressure springs, but the coupling shaft is of a completely new design.

The coupling is guided in the shaft by an inserted rivet, a solution that we had not previously seen in this design, but, still quite interesting. It offers a starting point for your own modification wishes, because it can also be removed.



For demonstration purposes we have pulled one of the guide rivets to be able to illustrate the simple change of the coupling. You can also see the two catch brackets under the axles.

If you want to change to another coupling system or use this model as an adapter car, you get easy access to this important element. Likewise, the coupling supplied can of course also be exchanged for a Märklin close coupling, or for parts from FR Freudenreich Feinwerktechnik.

The latter can make sense if Jörger uncouplers are used on the layout and a magnetisable metal strip has to be glued under the coupler. While Märklin's couplers are made of POM, which is difficult to glue, FR uses polystyrene, which is more user friendly.

Moulds and engravings now deserve a detailed look. Running boards and shunter's steps are present, the typical appearance of this type of wagon is also well captured in its entirety. This is especially true for the open-worked slatted walls, which could have been slightly reduced only at the end walls (in the roof curvature).



A convincing livestock wagon naturally includes above all the features that once gave this type of construction its name: the boarding with sufficiently wide slots for good air flow.

It was a good idea to lighten up the picture of the side view with a little trick. Most of the flaps are closed, but some are also open (see photo on page 21). From a distance this is not noticeable, for example when passing by.

However, if you park your wagon on the loading road and arrange a lively figure scene there, you will have a special feature here. In this case, the open peepholes can be used specifically for observing. What is meant here is the viewer, whose gaze now has more time to linger on the model.

Use of the V 23 on the layout

We remember that the class 41 steam locomotive was once nicknamed the “ox locomotive”. It still had this nickname from the Reichsbahn era, when the fast multi-purpose locomotive was often used in front of long cattle trains to the cities.

By the time of the Bundesbahn, this was already rare, as the number of cattle transports had long since decreased significantly. So, these trains may already be considerably shorter and can be easily broken up: groups of covered wagons of the standard type and those of the main type V suggest animal transports of different types. Märklin wagons with movable sliding doors can be placed here.

However, short trains, in particular, certainly bring variety to the layout. A single V 23 stands out especially between the V 14s from Märklin, which already existed with different road numbers, but only with a brakeman's cab. Another exception in such a train could be the Vh 04 from Heckl Kleinserien, if someone could get hold of it.



A short cattle train certainly has a particularly attractive effect on the viewer. Among the Märklin and Heckl small series wagons, the new V 23 with its barrel roof immediately stands out. The V 36 now shunts a goods train escort wagon to the train and then the group of wagons is pulled off the loading road.

Those who do not have one of the two fellow travellers need not despair, however: on branch lines, interlocking wagons could also be found as single wagons in a passenger train and therefore often had a steam heating line. When they reached the next marshalling yard, they were included in a through goods train to their destination; this is how they run alongside any other type of wagon.

In the model, too, there is nothing to be said against this. With a weight of 7 grams, the V 23 is not out of the ordinary. It is heavy enough to run safely in derailment situations, but also light enough not to be a disturbing load for the locomotive.

So, our overall conclusion is also satisfactory: Smaller points for improvement can inevitably always be found. Nevertheless, ZetNa 220 has also chosen a demanding template and has successfully overcome one hurdle here.

And since the chosen design stands out well from the rest and there are hardly any alternatives currently on the market, it can be said that this is a lucky choice. ZetNa 220 therefore really deserves to be nominated with this model for the new releases of the year 2021 in the category wagons.

Supplier of the model:
<https://sites.google.com/view/zetna220/home>

Douglas DC-3 in Pan Am Colours Candy Bomber from Herpa

Today, we are presenting a very special aircraft: We are writing about the greatest success model of all time and at the same time an aircraft to which Germans, in particular, owe a lot. Herpa has created a small memorial to it that can appeal to people on both sides of the pond.

The twin-engined Douglas DC-3 was a classic piston-engined aircraft, also known in military versions as the Douglas C-47 (USA) and Douglas Dakota (Great Britain). Developed by the Douglas Aircraft Company, it had its maiden flight on 17 December 1935 and went into production shortly afterwards.

The first examples in Europe were used by KLM from 1936 and by Swissair a year later. By 1938, 30 airlines were flying this type. Shortly before the start of the war in 1939, 90 percent of the world's air traffic was carried on a DC-3.



Most Douglas DC-3s began their service as C-47 military aircraft, and only entered civilian service after the war. This SAS aircraft operated by Flygande veterans keeps alive the memory of the most successful aircraft type of all time, captured here in October 1989 over Stockholm. Photo: Towpilot (CC-BY-SA-3.0-migrated)

These are impressive figures, but for the time being the aircraft experienced its greatest flowering in military service. There, it was used as a transporter, tow plane, ambulance and passenger aircraft and was built in enormous quantities, because of these many possibilities.

By 1945, 16,079 aircraft had been built, 10,655 of them by Douglas, and 5,424 licensed models. Only 607 aircraft were civilian models, 15,472 can be attributed to the military version C-47. To date, no passenger or transport aircraft has been able to surpass this number. This also illustrates its importance for the reconstruction of air traffic after 1945.

The majority of aircraft that became unemployed at the end of the war were sold by the American military to airlines, which had them converted into civilian models and used them on a wide variety of routes. In Germany, too, three aircraft were used by Lufthansa from October, 1955, onwards in domestic feeder services, later switching to cargo services.

Some of them are still in service today, and there are also airworthy museum specimens. The model from Herpa also belongs to this category. It follows the historic livery of Pan Am, one of the most traditional, and once, most renowned airlines from the United States.



Among others, the Douglas DC-3 and C-47 made a name for themselves as "candy bombers" during the Berlin blockade. This photograph by Henry Ries from 1948 is emblematic of this: Berliners watch the landing of a Douglas DC-4 at Tempelhof Airport. Photo: USGOV-PD

There, the aircraft were each called "Clipper" in reference to the earlier flying boats, and so the model with the US registration NC33611 also bears the appropriate name "Clipper Tabitha May", and the matching blue and white livery of Pan American World Airways (PAA) from 1940.

Hardly anyone is aware today, that it was not only large aircraft that made this US airline almost immortal in the public perception. Smaller types were also once part of its fleet, including the Convair CV-240 and the indestructible Douglas DC-3.

Between 1937 and 1966, Pan American World Airways had a staggering 90 of these aircraft in service. The new model can therefore easily be placed next to another contemporary of Era III.

In Germany, however, the Douglas DC-3 became known under a different, special name, which still concerned the military version C-47 "Skytrain". The US Air Force initially used this type primarily as a transport aircraft in the Berlin Airlift, and, so, it was the first to acquire the nickname "Candy Bomber".



This is roughly how a historic airfield scene with Douglas DC-3 as Pan Am's "Clipper" might look on a layout. Next to it, its successor Douglas DC-4, which also replaced it as a "Candy Bomber", can be partially seen at the right.

This name goes back to the pilot Gail Halvorsen, who made small parachutes out of handkerchiefs and tied sweets (chocolate bars or chewing gum) to them to drop them, like bombs, for the waiting children before landing at Tempelhof.

When this made the rounds in the Berlin press, many other pilots soon followed suit. However, the German term, which refers to raisins instead of sweets, is said to go back to the fact that a British pilot had transported a load of raisins for Christmas baking to Berlin in the pre-Christmas period of 1948.

At that time, aircraft took off almost every minute from Celle-Wietzenbruch and Faßberg (both in Lüneburg Heide), the two bases with the shortest route, to supply troops and the population in Berlin's western sectors with food and fuel.

A great challenge

When it became apparent that the Soviet Union might attempt to annex the western sectors of Berlin, considerations and planning began as to whether and how the western occupation troops, but also the civilian population, could be supplied from the air.

The doubts that this would be possible over a longer period of time were great, and yet there were also more optimistic assessments that were soon to prove true. The decisive factor was standardised and soon practised procedures that resulted in a tight cycle and rapid action from loading to unloading.

For example, limiting the Americans to only one type of aircraft simplified the procedures decisively and optimised their entire logistics: they all had the same cruising speed and flight characteristics.

Therefore, aircraft spacing could be reduced while increasing the frequency of take-off and landing manoeuvres. Maintenance and spare parts procurement also gained efficiency, crews could switch to other aircraft in case of failure, as all required the same level of training.



A number of Douglas C-47s are unloaded at Berlin's Tempelhof Airport (US sector) in 1948 during the first months of the Airlift. Photo: PD US Air Force

In the early days, as already mentioned, twin-engined C-47 "Skytrain" (known as "Dakota" by the RAF) were used. They were also supported by hired DC-3s of the civilian version, which is why the Pan Am model fits this historical event just as well.

However, the airlift could not be sustained for long with them. Their maximum load capacity of 3 tonnes was not sufficient to fly enough goods to Berlin, and the number of flights could not be increased at will. What was needed were aircraft that could carry more cargo per flight.

While the British used a wide variety of aircraft types at the same time, and only the French even used a Ju-52 of German origin for a short time, the Americans remained true to their line.

The four-engined C-54 "Skymaster" (Douglas DC-4) with a payload of 9 tonnes replaced the smaller C-47 / DC-3 and became the most important aircraft of the Airlift with 380 planes, 225 of which were for the Americans. The C-97 "Stratofreighter" and the C-74 "Globemaster", which at that time could carry an almost unimaginable 20 tons of cargo, were only used sporadically.



The Douglas DC-3 "Clipper Tabitha May" in Pan Am colours, which has been preserved in a museum and is once again airworthy, is the model for the new Herpa model (art. no. 570886), which should appeal to customers in Europe and the United States alike.

The spooky blockade of all land and water routes lasted from 24 June 1948 to 12 May 1949 – almost a year. But the Berlin Airlift did not officially end until 30 September 1949, when enough supplies for at least two full months had been flown in and stored. Fortunately, the event remained unique in German history.

The original for Herpa's model, initially a C-47 in American military ownership, possibly also took part in the Berlin Airlift, but then not in its present appearance: the design and lettering of the museum aircraft follow a Pan Am example that was lost in a crash as early as 1942, fortunately without fatalities. In any case, it was involved in the commemorative events for the 70th anniversary of the Airlift.

Even before that, it was a special aircraft: in 1945, it was one of the last C-47s to be built and was the last aircraft in regular air traffic in the United States until 2003. Only then was it decommissioned, only to be brought back to life from 2009 onwards and given the authentic historical identification of a Pan Am aircraft.

A look at the model

Let's now take a look at the new model with the item number 570886: It is made in the familiar production method, which Herpa chooses especially for smaller and therefore preferable models suitable for landing. What is meant here is a metal model that is assembled from the fuselage, wings and tail unit.

The model is cleanly painted in original colours, elaborately printed by means of tampons and in this way provided with many details. These include, for example, the position lights on the wings, and, also, fine details such as the windows and even windscreen wipers..



Traditionally, Herpa reproduces many features by pad printing. These include, for example, the windscreen wipers or the landing lights in the wings. But, also, attachments, such as the antenna or the prototypical solid-looking undercarriage characterise the overall impression.

But good print quality usually proves itself where it becomes small or varied. On this aircraft, these are the logos of Pan Am and Douglas. Both logos display a stylised globe, and can be found twice on the fuselage sides and once each on the vertical stabiliser. Very well done are again the bright surfaces, which also reflect excellently in the model.

Further distinctive features have been reproduced by attachments, because they require a three-dimensionality. For example, the radio antenna attached to the roof of the cabin stands out at first glance.

Also worth seeing are the propeller engines with replicas of the piston engines and the rotating propeller blades. A nice, but also useful little thing is the rotatable and at least limited deflectable tail wheel.

Even if this seems to be a minor detail, it allows the model to be positioned in such a way that the path it has just travelled on the apron can be traced and thus does not appear too static.

We are particularly pleased about this because this aircraft in particular allows us to recreate an important period of German history with commemorative value. At the same time, with the German-American friendship, it also emphasises a long-standing bond between today's alliance partners.

Dimensions and data on the Douglas DC-3

	Prototype	1:200	1:220	Model
Length	19.660 mm	98,3 mm	89,4 mm	96,4 mm
Wingspan	29.980 mm	149,9 mm	136,3 mm	144,6 mm
Height	5.160 mm	25,8 mm	23,5 mm	27,6 mm
Net weight	13,19 t	---	---	88 g
Cruising speed	ca. 280 km/h			
Range	2.160 km			
Max. passenger capacity	35			
Engines	2 x P&W R-1830-92 Twin Wasp			
Power	2 x 1.200 PS (2 x ca. 880 kW)			

But where there is light, there is usually also shadow: Although the miniature looks very coherent to the viewer, it shows clearer, more obvious, deviations than other Herpa products.

Fortunately, this plays into our hands, because both length and wingspan, as the most important dimensions, which are mainly decisive when looking from above, turn out to be too small. So, this model comes closer to our 1:220 scale than actually intended.

Only the height is almost 2 mm too high, which adds up to a difference of 4 mm compared to the nominal value. In view of this, we must also add that we would not have noticed this without a calipre gauge.



The slightly turned tail wheel is a small, fine detail that is guaranteed not to miss its effect on a model apron or taxiway. The good print quality, on the other hand, is proven by the globe emblems of Pan Am and Douglas in the rear fuselage area and on the vertical stabiliser of the aircraft.

Summing up our impressions at this point, we welcome this Douglas DC-3 as a tangible piece of German and American history. It should and should find its fans both here and over there, after all, Herpa is also distributed in the United States of America.

We have already frequently suggested aircraft models as enhancement of background scenery, and also find airport scenes, which are not seen too often, as special attractions of model railway layouts.



The "Clipper Tabitha May" in the historic colours of Pan American World Airways flies over Hannover, the capital of Lower Saxony. Even in the model, this Douglas DC-3 seems capable of flight and gives an impression of how it could attract attention in the skies of a model railway layout.

Such a small aircraft fits in well, of course, which is also underlined by the historical section it represents. And, so, this tiny aircraft is not only chic and nice to look at, but also something really special due to the unusual history of the pattern and the prototype.

Manufacturer of the Model:
<http://www.herpa.de>

New wooden crates from Joswood Overseas Shipping Crates

When Jörg Schmidt presented us with his first wooden shipping crates, we were thrilled: something like this had been missing from Z gauge. In the meantime, this is a thing of the past and the accessories manufacturer from the Bergisches Land region produces scale versions with many names and logos of well-known mechanical engineering companies.

In the report from the Spielwarenmesse (International Toy Fair) 2020, we mentioned wooden shipping crates from the accessories manufacturer Joswood. Provided with a prototypical load securing device (against slipping), they are a perfect load across different scales, be it for trucks, or for various types of freight wagons.

Now, other versions are available that will also appeal to Z scale enthusiasts. From the shipping crates, which were recently introduced as 2021 new products, we have chosen the small wooden "O&K" crates (art. no. 85026; 3 pieces), and the medium sized ones with a frame and a "Krupp" label (85028; 2 pieces).



The new wooden crates from Joswood also cut a fine figure in Z gauge. They are perfectly suited for Märklin's new SSy 45 heavy-duty wagons or also their six-axle siblings SSys 46 (not in the photo). Divided into several groups of wagons, they provide variety on the model railway.

Both, as well as a third version, are each only available as a hard cardboard kit. With dimensions of 15 x 11.5 x 10 and 32 x 1.5 x 15.5 mm, they are well suited for our scale and are a perfect match for Märklin's new heavy-duty freight cars, despite being being sold as N gauge items.

Their assembly is quickly completed. The manufacturer's instructions list all the tools and materials that are needed: a cutting mat, a sharp craft knife (for us, preferably one from Mozart), and wood glue. By the way, we prefer to use the recommended Bindan-RS glue from Bindulin for these types of kits. In addition, we use tweezers, a toothpick, and Noch's laser-cut glue.



The kits consist of only a few parts for the inner core, outer panels, base and lid. The Krupp crates include four additional parts for the outer frame.

Each crate is built from only twelve or sixteen pieces. The assembly starts with the core of the crates. After that, the outer panels are glued on, before the lid and finally the base plate are attached. In the case of the Krupp crates, a reinforcing frame consisting of four parts is added in between.

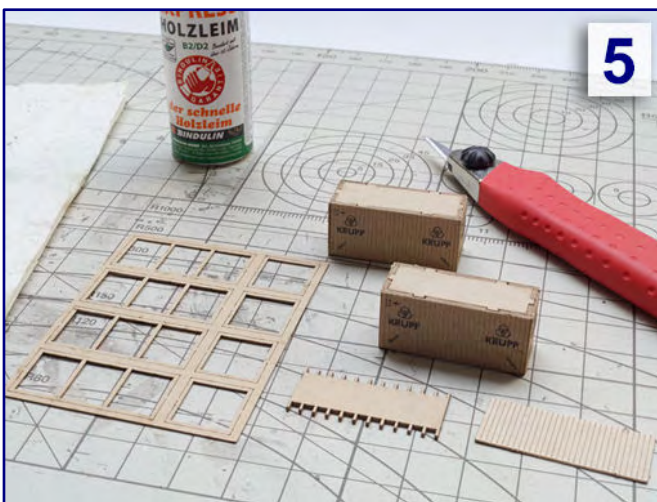
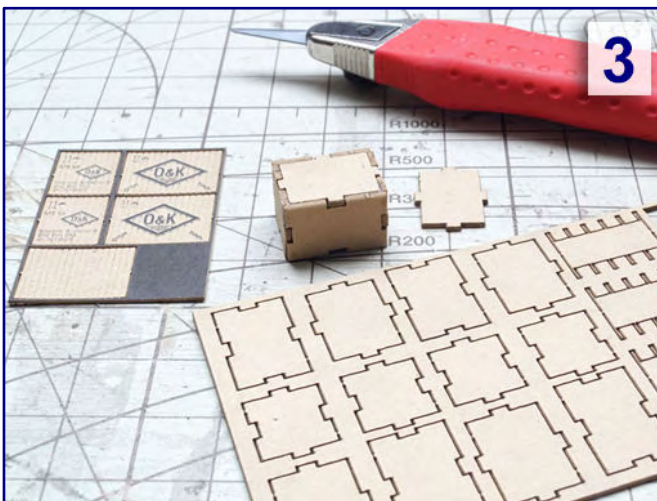
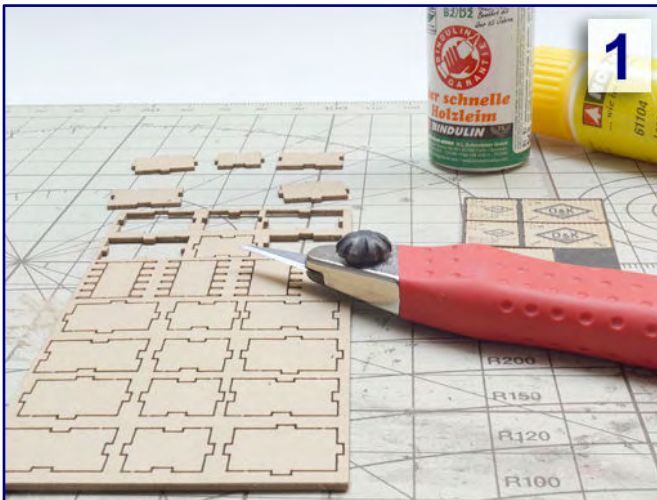
The level of difficulty of the kit remains manageable. Nevertheless, the assembly also should not be taken lightly. The well-illustrated instructions with large photos are quite useful, and break down the process into four or six steps, depending upon the type of crate.

The devil, however, is in the details, as we all know. The parts are not numbered, which is certainly unnecessary with such straightforward kits. But this also makes it all the more important to familiarise yourself with all pieces before starting to glue them together. Step by step, we detach the necessary parts, and assemble them according to the instructions.

As with a puzzle, they do not fit together at random, which prevents mistakes. Nevertheless, laying out the outer walls helps to understand which parts will later form the top, bottom and sides, and whether edges will abut or overlap.

Dry-fitting parts together before all parts should actually be standard procedure, as it does facilitate assembly, and helps to avoid ugly mistakes. After putting together the first crate, however, one will already have gotten the hang of it, and things will proceed much faster. Dry-fitting the parts also helps to identify where the core pieces of the crates connect with each other, and where glue should be applied.

This is piece of advice is usually not found in the instructions, but is nevertheless extremely important for an pleasing result. We prefer to use Bindan-RS glue, which can also be applied to the inner joints before



Five steps for a successful assembly:
Cut out the parts required for the current step with a sharp craft knife. Use a cutting mat (step 1).

Familiarise yourself with the parts and their gluing edges. Dry-fit everything together before applying any glue (step 2).

As with other kits, the first step is to assemble a stable core. The top facing side in this photo is actually one of the side walls of the crate (step 3), with another one lying next to it waiting to be used.

Next come the side panels made of much thinner cardboard. Here, too, we recommended to first dry fit all pieces. This helped us to determine that it is best to start with the end walls (step 4).

Finally, it is time to attach the top and the bottom (with pallet braces). For those crates with frames, the frames follow last (step 5).

the last part is put on. At the gluing edges we use a toothpick to help us apply the glue sparingly and precisely.

The Noch glue, which is set to be thinner, is the best choice for the outer walls made of much thinner cardboard. This prevents a thicker blob of glue from causing bulging walls later on. Be careful when cutting out the components, because, if the blade is not sharp enough, or the cut is not made carefully, the material could tear out.

First, the four side panels are attached. They must be aligned quickly and correctly before the glue sets: They must not protrude too far on the outside and they must be flush at the top and bottom. We can only achieve this by placing them on the flat surface of a table, carefully positioning them, pushing them back, if necessary, and then pressing them together.



Almost everyone will probably have seen these types of overseas shipping crates in old photos. And, now their scale models are ready to add interest to the Z scale world.

This is another small, but helpful trick that is not usually found in kits' manufacturers' instructions. Once the side walls are attached to the core, the top part follows in the same way. Finally, the pallet replica is attached to the bottom of the crate.

In the case of the two Krupp crates, there are still four reinforcing frame parts that need to be attached to the outside, in order to finish the model. They also need to be placed accurately, so we do recommend to test their fit, before applying any glue. This helps to prevent the front parts from being unintentionally twisted by 90°.

Again, it is particularly important to use the glue sparingly, in order to prevent any of it oozing out from between edges and causing ugly surface traces. We again use the Noch glue for such small tasks.

Once all four outer frame elements are attached, the assembly work is finished. The very attractive crates are now tempting to be used at last. In our small scale, this can only mean use on a flat or heavy-duty wagon.

Wooden crates as an alternative load:

<http://www.joswood-gmbh.de>
<https://laser-cut-shop.de>

Materials and tools:

<https://www.bindulin.de>
<https://www.noch.de>
<http://www.peter-post-werkzeuge.de>
<https://www.tamiya.de>



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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Wissen teilen im analogen Zeitalter

Historische Informationsquelle

Als das Dampflokalzeitalter zu Ende ging, reisten viele den betagten Maschinen in nostalgischer Erinnerung hinterher. Fotografien, die damals gefertigt wurden, sind heute historisch wertvolle Dokumente. Kaum noch vorstellbar ist es heute, wie schwierig es für Dampflokaljäger war, stichhaltige Informationen zum Einsatz zu erhalten. Die wichtigste Quelle wurde jüngst 50 Jahre alt und mit einer „Jubiläumsausgabe“ vom EK-Verlag gewürdigt.



Ronald Krug
Dampfgeführte Reisezüge der DB
Winterfahrplan 1966/67

EK-Verlag GmbH
Freiburg 2020

Gebundenes Buch
Format 21,0 x 29,7 cm
160 Seiten mit 222 teilweise farbigen Abbildungen

ISBN 978-3-8446-6230-6
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Preis 39,90 EUR (Deutschland)

Erhältlich direkt ab Verlag
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Einstigen Dampflokaljägern waren die Broschüren „Dampfgeführte Reisezüge der DB“ sicher mehr wert, als die offiziell verlangten 2,00 DM beim ersten Erscheinen dieses DIN-A5-Hefts. Preiswert, handlich und einmalige Informationsquelle – wer seine Leidenschaft den Dampfrössern verschrieben hatte, kam an dieser Broschüre nicht vorbei.

Wo damals planmäßig Dampfzüge zum Einsatz kamen, ließ sich anderweitig kaum ermitteln: Das Mobiltelefon war ebenso wenig erfunden wie das Internet, Fax und Ferngespräch auf dem Festnetz waren teurer Luxus. Als das Heft für Eisenbahnfreunde im Januar 1968 mit den Daten des Winterfahrplans 1967/68 erstmals erschien, war es folglich unverzichtbar.

Schließlich druckte der EK-Verlag eine Ergänzung zum Sommerfahrplan 1968 ins EK-Heft und entschloss sich, die Broschüre ab dem folgenden Winterfahrplan zu jedem Fahrplanwechsel neu aufzulegen. 50 Jahre später lohnt es sich, auf dieses Jubiläum kurz zurückzublicken und es zu würdigen: Bis 1976 erschienen in dieser Heftreihe insgesamt 15 Ausgaben.

Sie wurden beständig ausgebaut und angereichert, denn die planmäßigen Züge der Bundesbahn wurde ja immer weniger. Streckenaufnahmen ergänzten bald den Inhalt, später kamen noch ausgewählte Güterzugleistungen hinzu, danach ÖBB-Dampfzüge und schließlich Museumsbahnen in- und außerhalb Deutschlands.

Dann fanden auch noch Einsätze von Ellok-Veteranen Eingang, während die Qualität und Aussagekraft durch eingehende Sichtungsmeldungen von Eisenbahnfreunden und amtlichen Unterlagen der DB

gesteigert werden konnte. Autor Ronald Krug hat eine „Jubiläumsausgabe“ anlässlich des 50-jährigen Ersterscheinens in Buchform zusammengestellt. Ausgewählt hat er dafür den Winterfahrplan 1966/1967, was nicht zufällig geschah.

Zum einen ist es der Winterfahrplan vor der Erstauflage, der Autor geht also noch ein Jahr weiter zurück als seine Vorgänger. Weiter zurückliegende Fahrplanjahre waren nicht mehr hinreichend recherchierbar, weil die Diskrepanz zwischen Quellen und der tatsächlichen Lage zu groß schien. Zum anderen handelt es sich auch um einen Fahrplanabschnitt, der den Strukturwandel besonders spürbar werden ließ.

Die noch junge Bundesrepublik entwickelte sich von ihrer ersten Rezession in die Hochkonjunktur, was die Beförderungsleistungen stark ansteigen ließ. Was konnte, das musste auch fahren. So erlebte die Dampflok eine letzte Blüte, in der sie zu den modernen Diesel- und Elektrofahrzeugen einen Kontrast wie nie zuvor schafft.

Über mehrere Jahrzehnte hat der Autor Fahrpläne, Umlaufpläne und Lokführeraufzeichnungen gesammelt und ausgewertet. Bereits das zeigt, dass dieses Buch kein Zufallsprodukt ist oder mit „heißer Nadel gestrickt“ wurde. Gegliedert wurde es, wie die späteren Ausgaben der einstigen Broschüren, alphabetisch nach den einsetzenden Betriebswerken.

In gleicher Weise gibt es hier auch ausgewählte Informationen zu ausgewählten Güterzugeinsätzen oder Altbaufahrzeugen der Diesel- und Elektrotraktion, auch finden wir Informationen zu dampfgeführten Zügen mit Lokomotiven der DR und CSD auf bundesdeutschen Gleisen oder eben die Museumsbahnen.

Über 2.200 dampfgeführte DB-Reisezüge im Winter 1966/1967 sind so zusammengekommen, die detailliert mit Fahrzeiten, Fahrstrecken, Reisegeschwindigkeiten und meistens auch der Zugbildung beschrieben werden. Zum Vergleich sei wieder die historische Vorlage herangezogen: In der Spitze konnte sie etwa 950 Dampfzüge bieten!

Doch das Buch ist auch an anderer Stelle mehr als nur eine Erinnerung an alte Zeiten. Gewandelt hat sich natürlich das Erscheinungsbild, das nicht mehr aus Schreibmaschinenzeilen, Fotos und handgezeichneten Tabellen wie ein Puzzle zusammengefügt wurde. Und es wurde auch noch mit 222 historischen Fotografien kräftig angereichert.

Diese sind bunt und treffend ausgewählt worden, dürfen nicht als schon allgemein bekannt gelten und stammen fast alle aus dem behandelten Fahrplanabschnitt. Nur wo das nicht exakt zu gewährleisten war, sind Aufnahmen aus den angrenzenden Fahrplänen genutzt worden, sofern sich die Zugbildung nicht verändert hatte – geringfügige Abweichungen werden in den Bildunterschriften behandelt.

Ebenso hervorragend ist die Reproduktion dieser alten Aufnahmen, die fast durchgehend schwarz-weiß gedruckt sind, ein kleiner Farbteil ist am Ende des Werkes zu finden. Es ist immer wieder erstaunlich, was der Eisenbahn-Kurier aus alten Negativen herauszuholen vermag, denn das Originalmaterial unterliegt ja auch einem Alterungsprozess.

Nach vielen Zeilen berechtigten Lobes möchten wir nun schlussfolgern, für wen dieses Buch geeignet und empfehlenswert ist: Das sind natürlich alle Freunde der klassischen Bundesbahn, doch darüber hinaus auch fast alle Modellbahner.

Sie erhalten hier, wie nirgends sonst, Anhaltspunkte in Wort und Bild für die Zugbildung auf ihrer Anlage. Das müssen nicht immer lange und schwere Züge sein, wie hier bewiesen wird. Das können auch Füllleistungen einer Schnellzugdampflok vor dreiachsigen Umbauwagen sein oder die Baureihe 23 vor kurzen Güterzügen. Wer nah am Vorbild bleiben will, bekommt hier also die Argumentationsgrundlage!

.....
: **Publishing pages with reference possibility:** :
: <http://www.eisenbahn-kurier.de> :
: <http://www.ekshop.de> :
:.....

Typenkompass für den Luftverkehr Flug durchs Turboprop-Zeitalter

Viele Bücher der Typenkompass-Reihe haben wir an dieser Stelle schon vorgestellt. Zu Wasser, zu Lande und nun auch in der Luft, dürfen wir jetzt behaupten. Heute möchten wir nämlich zum ersten Mal einen kleinen Ratgeber für Propellerflugzeuge vorstellen. Da sie eher klein bleiben und als Modelle gut mit der Modellbahn harmonieren, dürften sie durchaus den einen oder anderen Leser ansprechen.



Marc Volland
Propellerflugzeuge – Verkehrsmaschinen seit 1945
aus der Reihe Typenkompass

Motorbuch Verlag
Stuttgart 2015

Taschenbuch mit Klebebindung
Format 14,0 x 20,5 cm
128 Seiten mit 8 S/W & 133 Farbbildern

ISBN 978-3- 613-03814-1
Titel-Nr. 03814
Preis 12,00 EUR (Deutschland)

Erhältlich direkt ab Verlag
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Wenn wir rechts und links über den Tellerrand blicken, dann ist ab und an auch mal ein passendes Buch zu den behandelten Themen finden. So haben wir uns umgesehen, was auch luftfahrtbegeisterten Modellbahnern einen ersten Überblick verschaffen kann.

Fündig wurden wir in der sehr umfangreichen Typenkompass-Reihe, die bei Transpress / Motorbuch-Verlag erscheint. Der Titel ist zwar nicht neu erschienen, aber immer noch erhältlich und gewiss ein guter Wegbegleiter.

Als Autor zeichnet Marc Volland für diesen hier besprochenen Band verantwortlich. Er verbringt nach Verlagsdarstellung als Hobbyfotograf viele Stunden auf Flugplätzen und Flugschauen in aller Welt. Seine Aufnahmen veröffentlicht er zusammen mit Artikeln in internationalen Fachzeitschriften.

Wir dürfen also davon ausgehen, dass er mit der behandelten Materie bestens vertraut ist. Und diese Erwartung sehen wir im Buch auch bestätigt. Allerdings haben wir beim ersten Durchblättern und in der Aufstellung der Buchdeckelrückseite viele uns bekannten Flugzeugmuster vermisst.

Das erschien uns merkwürdig, denn den Hinweis „seit 1945“ hatten wir schon zur Kenntnis genommen. Doch was ist mit so bekannten Propellerflugzeugen wie der „Super-Connie“ von Lockheed oder der Convair CV-340? Auch die Douglas DC-6 war nach unserer Erinnerung erst nach dem Zweiten Weltkrieg angeboten worden.

Festhalten müssen wir also, dass der Titel etwas irritiert, nicht aber irreführt. Die Auflösung auf unsere Fragen gibt nämlich das Vorwort des Autors: Er verweist auf die inhaltliche Freiheit, die er genießt, aber den beschränkten Platz, den ein Verlag nun mal vorgibt.

So konnte er nicht alle Typen, die nach 1945 gebaut wurden, ohne eine Vorauswahl behandeln. Nicht zu finden sind hier Sport- und kleine Geschäftsflugzeuge, was aus dem Titel allerdings auch schon ableitbar ist.

Bis auf zwei Ausnahmen blieben auch Kolbenmotorflugzeuge außen vor – dazu gleich noch mehr. Ebenso keine Rücksicht konnte er auf erfolglose Muster nehmen, die keine wirtschaftliche Bedeutung erlangt haben, darunter beispielsweise die Vickers Vanguard oder Bristol Britannia.

Dem Umfang des Buches geschuldet ist es auch, dass unterschiedliche Versionen eines Flugzeugs meist in einem Beitrag zusammengefasst sind. Das ist etwa bei der Vickers Viscount der Fall, deren Artikel sich sowohl auf die Versionen V700 als auch V800 bezieht.

Zu den selbstauferlegten Einschränkungen möchten wir Folgendes kurz erläutern: Um 1945 war die Entwicklung der Kolbenmotoren auf ihrem absoluten Höhepunkt angelangt. Diese Motoren trieben leistungsfähige Passagiermaschinen an, die wir teilweise zuvor erwähnt haben und die bis heute als Legenden gelten.

Allerdings waren sie auch störanfällig und aufwändig, weshalb sie schon bald von Turboprop-Motoren verdrängt wurden. Das erste Verkehrsflugzeug mit solch moderner Ausstattung war die Vickers Viscount und ihr sollten viele weitere bis heute folgen. Auch heute kommen sie noch auf Kurz- und sogar Mittelstrecken zum Einsatz.

So war es gar nicht möglich, nur nach dem Propellerantrieb zu unterscheiden und sie alle in diesem Werk unterzubringen, ohne dem Leser wichtige Informationen vorenthalten zu müssen. So schreibt der Autor, dass ein weiterer Band zu den Kolbenmotor-Flugzeugen zeitnah folgen solle. Im Verlagsprogramm ist er derzeit allerdings nicht.

In seiner Struktur folgt das vorliegende Werk im Wesentlichen anderen Titeln der Typenkompass-Reihe. Ein einleitendes Kapitel mit grundsätzlichen Ausführungen gibt es hier allerdings nicht, dafür sind aber mehrere Fotos eingeklinkt, die eine Doppelseite voll beanspruchen. Damit lässt sich das Erscheinungsbild erheblich auflockern.

Die einzelnen Flugzeuge, die hier behandelt werden, erhalten meist ebenfalls eine Doppelseite, die ihre Geschichte kurz zusammenfasst und wichtige Informationen zum Entstehen, technischen Merkmalen oder Meilensteinen, den gebauten Versionen und wichtigen Fluggesellschaften gibt. Eine kleine Tabelle nennt die wichtigsten Daten und verbauten Antriebsmotoren.

Jeder dieser Beiträge erhält im Schnitt zwei begleitende Fotos zur Dokumentation, viele sogar deutlich mehr. Das ist ein entscheidender Unterschied zu vielen anderen Bänden dieser Reihe. Nicht vergessen werden darf deshalb auch der Hinweis, dass die Aufnahmen gut und repräsentativ ausgewählt worden sind. Auch die Umsetzung in den Druck ist hier gut gelungen.

Dem Sinn des Buches und seiner Zielgruppe entsprechend sind die Informationen eher knapp gehalten, reichen aber völlig für einen ersten und allgemeinen Überblick. Mehr wird ein Modellbahner gewiss nicht verlangen, für den die Luftfahrt eher ein Nebeninteresse darstellt.

Und doch ist es ein netter Ratgeber, der auch gut in die Jackentasche passt und unterwegs oder auf der Besucherterrasse eines Regionalflughafens hilft, die gesichteten Maschinen zu bestimmen und einzuordnen. So ist der verlangte, moderate Buchpreis sicher gut angelegt. Freuen würden uns aber, wenn auch der angekündigte Zweitband erschiene, der dieses Werk erst vollständig macht.

Staying in the conversation (Part 2)

NoBa-Modelle unlimited

If the United States is the country of unlimited possibilities, then Aichtal in Baden-Württemberg must be a place of equally unlimited options. NoBa-Modelle may be one of the younger small-series manufacturers of Z gauge, but, with a rapid pace of new offerings, the duo behind this brand have quickly made themselves known. Today, they introduce themselves and their company to all Trainini® readers.

By Norbert and Barbara Heller. It all started with a love story between No (Norbert) and Ba (Barbara) - their name abbreviations provided the company name -- in 2011. Norbert was already a convinced Zettie and Barbara, who always liked to tinker, paint, and carve, was excited about a model railway layout in Z gauge.

So, together we built our model railway layout "Heller Berg" on a scale of 1:220. In the process, we quickly realised that we had to design and build a lot ourselves, as there were hardly any accessories.

In December 2016, No finally had his last working day as a mechanical designer, and the first 3D printer was ordered straight away. At Christmas 2016, we started with NoBa-Modelle: The first creation was a dumpster on a scale of 1:220.



A look at the six 3D printers that work for NoBa-Modelle today: Depending on the demands of the project, printing is done in PLA and SLA processes.

Placed on a large internet auction site, ten units were ordered by one customer, at the same time! And, he has remained faithful to us ever since. Soon we added construction cranes, vehicles of all categories, ships, buildings, all kinds of accessories and our locomotives, train cars, and trams.

In the meantime, we have produced over 600 models on six 3D printers, three of which are PLA (for filament) and three SLA (for UV light-cured resin) printers. For the models we print from PLA, we use the Anycubic I3 Mega model. The PLA we use is made of high-quality material, so we can achieve good results with it, already.

The resin models are made on three different machines: an Anycubic Photon S, an Anycubic Photon Mono SE and an Anycubic Mono X. Here, too, we use high-quality resin to achieve models with a high level of detail.



The printing is finished (photo on the left), the models reveal themselves after removal from the printing plate (photo on the right) and await further processing.

What is important with both technologies is that Norbert knows what these printers can do. After all, he specifically focuses the construction of his models on this. He often pushes the limits of what is possible, for example, the handlebars and spokes of the new motorbikes.

However, there are still two devices missing that complete the resin print: The first is the so-called "washing machine", a Wash & Cure from Anycubic. It rinses excessive resin off the models with Isopropanol.



In the isopropanol bath, uncured resin is rinsed off, so that it cannot hinder the further processing of the model.

Then it goes into the “oven:” this is where resin models are hardened with UV light. Between the rinsing and hardening process, however, the support structure is first detached from the model.

Creativity has no limits

Most of the time, the ideas come straight from us, particularly, when we are building a small layout ourselves, like the wooden ball run, our quarry pond, or even the tram layout.

During the construction of the last-mentioned layout alone, the following ideas have already emerged: the cemetery equipment, the little ice-cream cart, the shelter, the market place, the playground equipment, the skate park, the diving tower at the swimming pool, the ticket booths, the miniature golf course and the tourist restaurant with a lookout tower. We now offer all these models for sale.

For all three layouts, almost only models from our own programme were used. However, many ideas are also submitted to us by customers, and the wish list is still long, but we are definitely happy about such a vivid participation!

A current and perfect example at this point is Peter Sturm's case layout, which has been given the name “Elefantentreffen” (Elephant meeting). His ideas and wishes resulted in the motorbikes in various designs,

the pigling on the grill, various tents from one-man tents to tepees, straw bales, campfires and much more.



The quarry pond (photo above) and the tramway layout (photo below) are two demonstration layouts, during the construction of which many ideas germinated in the first place and which, as a result, now present many NoBa model products in practical use.

The motorcycle combination on display is only about 8 x 10 x 5.5 mm in size and was painted by Peter Sturm himself. As proud as he is of the delivered models, we are also, because of his confidence in our skills. There has simply been a very productive collaboration!



A prime example of customer suggestions is Peter Sturm's "Elefantentreffen" (Elephant meeting) (pictured above), where many new NoBa models are now built that were based on his ideas. One of them is the tiny Royal Enfield motorcycle combination (photo below), which he painted himself. Both photos: Peter Sturm

And since No can literally design in his sleep, these ideas are usually also implemented very quickly and offered very promptly in our sales network.



Norbert "No" Heller designs the models for the small-batch series using CAD. His many years of experience as a mechanical engineer help him with this.

How is a model created?

It always starts with an idea. This is usually followed by days of research into drawings or planning documents with dimensions, and sometimes we also buy drawings. Norbert then constructs in 3D-CAD on a scale of 1:220, whereby he puts a lot of emphasis on the highest level of detail.

Downloading files from the internet does not make sense for us, because the models offered there are usually offered in a scale that is much too large and the details are therefore lost when scaling; often the files are also messed up and, therefore, useless.

Next, the first test prints are made, and the progress of the output is checked again and again. If everything finally matches, the model is photographed, and placed in our online shop as a new item.

If the model is (also) offered as a finished product, it is Barbara's turn. It is first degreased, then primed and sprayed with the base colour.



In addition to the airbrush and brush, the drawing pen is also used for fine details. After all, NoBa-Modelle has been offering its products as finished models for a long time.

We use the premium airbrush paints from Vallejo and apply them with an airbrush. In the end, we work out the smallest details of the models either with a drawing pen or a fine brush.

In the last phase, the decals are applied to the surfaces, and finally the finished product receives a coating of sealant, again with the airbrush.

The decals we just mentioned come from our colleague and friend Björn Plutka (Z-Doktor Modellbau), who produces them very precisely, and accurately, after elaborate research.

This cooperation works excellently and as Björn is also very reliable, sometimes mistakes on models, that we have missed, are sorted out.

He receives a 3D design template from us and can thus match his decals. His support in



A finished model is completed with decals. These are often manufactured and supplied by Z-Doktor Modellbau (Björn Plutka).



Barbara "Ba" Heller removes the support structures from models that have just come out of the alcohol bath and are subsequently post-cured.

restructuring our website was also of great value to us – thank you, Björn.

Just as valuable for us are the exchange and cooperation with editor-in-chief Holger Späing, who also brings many ideas and suggestions to us.

Quo vadis, NoBa-Modelle?

Since our models, as you can certainly see here, are often created very spontaneously, we cannot predict what is to be expected as a new product in the near future. But one thing is certain: there are still many, many ideas for the future - we are not running out of material! We let ourselves be surprised and would like to surprise you too...

Our models have long since found fans all over the world: Japan, Taiwan, New Zealand, Australia, the United States, Canada, and all European countries. We are in regular contact and exchange with many of our customers, which is very motivating for us. And it is certainly also very inspiring.

Contact with this manufacturer:
<https://www.noba-modelle.de>

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Easter greetings from Bremen:

(I) look forward to and for the publication of each issue of your magazine. I became aware of it through the regular publication notices on the online platform Drehscheibe ("turntable"), in the model railway forum.



Foto: Michael Böttcher

Now, I am so fascinated by your publications that after 20 years of Z-abstinence, I actually started to build in this scale again, at first on the surface of a Din A4 sheet... Soon it will be a small layout in a circle...

Happy Easter to you and your team!

Michael Böttcher, Bremen

Questions about the hydraulic drive:

In connection with the presentation of the diesel locomotive V 80, a "hydrodynamic drive with fluid transmission" or "fluid transmission" was mentioned.



Frankly, I can imagine little or nothing about it. Could you explain in one of your next episodes how these drives work? There may be other types of drives. Would you also present them? I would be very grateful for a brief reply and thank you in advance.

Günter Finke, Leichlingen

Editor's answer: The question of suitable power transmission for large diesel locomotives was the crucial point that made engineers despair for decades. An internal combustion engine cannot start under load, so the engine output and wheel drives must initially remain separate.

Special solutions are, therefore, needed because the couplings and transmissions, familiar from small cars, are not suitable for transmitting the power of a mainline diesel locomotive. This gave rise to ideas such as the diesel drive of a compressor, whose compressed air was to work in cylinders as in a steam locomotive (V 120 / V 32).

An alternative seemed to be the diesel-electric drive. Here, the diesel engine drives a generator and produces electricity for electric traction motors. Such a machine is virtually an el-locomotive in which the traction current is generated in a stationary manner. This method has the disadvantage of high

locomotive weights and axle pressures because both the combustion engine and the electric drives have to be installed in the same locomotive.

As a result, this technology initially remained limited to the flying trains (the Fliegender Hamburger) in Germany, while it became established in the United States from the 1940s until today. German engineers, on the other hand, pushed ahead with the development of hydrodynamic power transmission, even for high performance. The breakthrough came in 1935 with the V 140 preserved in the Deutsches Museum in Munich.

As the name suggests, a fluid (oil) is set in motion in such a transmission, which is done via paddle wheels as in a turbine (flow). The inertia of masses, the relationship between displacement and force and the physical property that liquids cannot be compressed are exploited. The way it works is explained in detail and illustrated in the German-language Wikipedia (<https://de.wikipedia.org/wiki/Str%C3%B6mungsgetriebe>).

Unfortunately, we have not been able to find an author for the desired article; in the opinion of the editors, it would also leave the railway subject too much and delve deeply into mechanical engineering and engineering technology.

Mozart precision knives also inspire professionals:

The modeller's knives (presented by us; editor's note) are very good, I will recommend them to others.

Stefan Laffont (Modellbau Laffont), Aachen

We are happy to help with beginner questions:

I turn to you full of hope. I am a complete newcomer to Z gauge and am looking for someone who can give me advice and support. I have subscribed to your Facebook group "Trainini Mini-Club Z-gauge," and read it diligently.

(...) On the internet (...) people are talking and writing about the advantages of "digitalisation." (...) I know neither out nor in – analogue or digital? (...) Maybe you can help me and tell me who to contact in the Wien (Vienna) area.

First of all, I would like to thank you very, very much for your detailed answer. In the meantime, my "fleet" has expanded to include 2 diesel locomotives with bell-shaped armature engines: (...) Driving/shunting is really fun with both of them!

They react much more sensitively and the response via the Märklin controller (from the starter package) is much better than with the V 60 (...). The idea of digitalisation has been put on the back burner for the moment.

(...) So I will try to glean the necessary expertise from your digital "Trainini;" by the way, the only digital medium that doesn't beat around the bush. You write what's what, bluntly, clearly, understandably and very helpfully! Keep it up, Trainini!

Karl Odersky, Wien

Editor's reply: Unfortunately, this letter to the editor can only be reproduced in abbreviated form, but, at least, it makes sense. We have already answered our reader's questions directly and also mentioned the Z-Stammtisch-Gruppe Wien as an active and accomplished association in the catchment area. We hope and wish that both will now come into direct contact.

At this point, however, we would like to publicly explain once again an important criterion for the choice of analogue or digital operation in Z gauge: Often the impression is given in presentations that only the digitalisation of a model is decisive for good running characteristics. Our reader also understood this to be the case, but, unfortunately, it is not correct. A model should already run well in analogue before it is digitised. Therefore, its mechanics, contact reliability and a good motor as well as the possible control with the help of electronics are decisive.

If these prerequisites are met, it is well worth considering a conversion. Digital technology does not "make the lame walk!" It can improve the characteristics of a model even further, give it new functions (such as light and operating sounds) and open up additional possibilities that analogue operation does not offer, such as multi-train operation on the same track without current isolation.

Consequently, before a choice is made for or against a mode of operation, for the type and extent of components required, a system and thus manufacturer, consideration should focus on the following.

AZL- Models delivered:

This month, AZL again has a new mould that will be delivered for the first time. We are talking here about the EMD E7 diesel locomotive as an A-unit, which has been produced by plastic injection moulding.

Technically, it appears with a bell motor, including two flywheels, many connecting parts, LED lighting changing with the direction of travel, and enclosed front panels as known features of this implementation.

Prepared for easy DCC conversion, versions of the attractively painted B&O (Item No. 64610-1 / -2) and the Chicago & North Western (64613-1 / -2) in bright yellow colours, with discreet green trim in the roof and ventilator area, will be available at the start.



The new EMD E7A moulds will initially be offered in B&O (art. no. 64610-1; photo above) and Chicago & North Western (64613-1; photo below) versions. Photos: AZL / Ztrack

The recently introduced tank cars now appear in black for Virginian Gasoline individually (915001-1) and as a set of two (915031-1) and four (905001-1). AZL has also chosen the same combinations for Burlington Northern's orange R-70-20 refrigerator cars for the Western Fruit Express (914810-1 / 914840-1 / 904810-1).

Distribution partners in Germany include Case-Hobbies and the 1zu220-Shop, manufacturer photos of the current deliveries can be found on the following website: <https://www.americanzline.com>.

New **Trainini®** webpages are about to be launched:

The launch of our new website with expanded content is now planned for Saturday, 15 May 2021. Work is currently still underway on fine-tuning the pages and their content, as well as on building the new link list, which will probably also still have gaps, when it is put online.

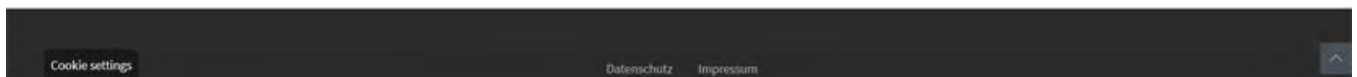


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Der Frühling steht vor der Tür, doch unsere Bastelsaison wird so schnell noch nicht zu Ende gehen. Neulich hörte ich: „So viel Zeit zum Basteln werden wir so schnell nicht wieder bekommen!“ Machen wir also das Beste aus den Beschränkungen - lesen und basteln!

[Weiterlesen](#)

<p>17.04.2021 Erstellt von Holger Späing</p> <p>Hilfreiches Zubehör von Joswood</p> <p>Bereits in unserer "Messe-Ausgabe" 2/2020 haben wir auf Holzkisten von Joswood hingewiesen. Nun sind weitere Versionen diese Überseekisten erhältlich, die dank vielfältiger Einsetzbarkeit auch Zettles gute Dienste tun werden.</p>	<p>09.04.2021 Erstellt von Holger Späing</p> <p>Aktuelle Märklin-Auslieferungen</p> <p>Märklin hat Anfang April 2021 mehrere Neuheiten ausgeliefert und damit seinen Lieferrückstand um einiges abbauen können. Für jeden Geschmack dürfte etwas dabei sein.</p>	<p>05.04.2021 Erstellt von Holger Späing</p> <p>Modellbau Laffont: Bausatzneuheiten 2021</p> <p>Zwei der vier neuen Bausätze von Modellbau Laffont sind ab sofort verfügbar. Wir stellen sie hier kurz vor.</p>
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Finally, it remains to be determined where and how the licensed **Trainini** archives should be reflected in the new format. We are also asking manufacturers and providers for permission to embed their logos in the link list and news items.

However, our readers can already look forward to the new website: even if it will not be fully developed right from the start, they can expect an attractive design, high information value, and practical benefits.

With its launch in mid-May, **Trainini®** will be considerably expanded into an information and news portal for Z gauge, that will keep you up to date even outside the publication dates of our magazine. We are also looking into a subscription function for a newsletter, as a further stage of expansion.

Märklin deliveries in April 2021:

At the beginning of April 2021 Märklin delivered several new items and was thus able to reduce its delivery backlog somewhat. The deliveries range from Era II to VI and include several companies and railway administrations from the Deutsche Reichsbahn to the Bundesbahn to the DSB.

A special heavyweight in the truest sense of the word is the NOHAB diesel locomotive series MV of the DSB (item no. 88637): Thanks to its housing made of metal plastic alloy, it has gained enormously in weight, and, thus, also, in tractive power.



Märklin's NOHAB has become a heavyweight, and has now been delivered in DSB design, as series MV (item no. 88637).

It is powered by a bell-shaped armature motor, illuminated with warm white LEDs depending on the direction of travel and is also up to date with the (still quite) newly constructed bogie screens. In May, we want to deal with this model in a short article.

For the friends of the Deutsche Reichsbahn-Gesellschaft a freight car pack (82327) consisting of two open cars "O Halle" and a Tempo tricycle, as well as another one (86010), with three hinged cover cars "K Wuppertal" have been released.

Product colours are worn by the also freshly delivered IC compartment coach Avmz 111.2 (1st class) of the Deutsche Bundesbahn (87241), which already belongs to era V. On the other hand, the "Donnerbüchsen" (Thunder Boxes) of the four-part passenger car set (87513) for the Deutsche Reichsbahn (Era IV) are bottle green. It consists of a Daai baggage car and three Baai corridor cars (2nd class).

The DB coaches for Era III corresponding to them are offered individually, but temporarily still have to do without a baggage coach, whose delivery date has been postponed. Available now are the BCi (2nd/3rd class; 87501) and Ci (3rd class; 87511 & 87512) Donnerbüchsen (Thunder Boxes). They are also painted in RAL 6007 bottle green and finely printed. They correctly do not yet have a DB biscuit according to their early period of use.



The three “Donnerbüchsen” (Thunder Boxes) of the early Bundesbahn era were also delivered, still without DB emblems (87501 / 87511 / 87512; photo above) as well as the heavy tank steam locomotive class 85 of the same era (88931; photo below), where, especially, the prototypically larger counterweights on the driving axle are appealing.

Two more locomotive models are now also available. With the modern diesel locomotive ER 20 D of the MRCE (88883) for Era VI, a revised model is waiting for its customers, which comes with a bell-shaped armature motor and warm white-red changing LED lighting.

The heavy class 85 tank steam locomotive has also undergone a product update. With warm white LEDs that light up at both ends depending on the direction of travel, rail clearers, brake replicas, and detail controls, it is sure to appeal to many customers in its early DB version (88931).

Its lettering shows pointed numbers as relicts of the Reichsbahn era, the model with bell-shaped armature motor is still reproduced without wind deflectors. The only disturbing feature of this model is that the light-emitting diodes do not emit any light at very slow speeds, contrary to many other recent implementations.

No International Toy Fair 2021:

With or without model railway participation was a question for a long time when it came to the replacement date in summer for the Spielwarenmesse. The event planned as the "Spielwarenmesse Summer Edition" was also cancelled on 30 March 2021.

The reason cited is the uncertain course of the pandemic, which is even more explosive here because of the exhibitors and visitors travelling internationally, and possible quarantine regulations.

Thus, the organiser states the following in its press release: "Above all, the lack of perspective around the trade fair industry on the part of politicians has a considerable impact on the planning of the trade fair organisation, and the travel arrangements of international manufacturers and buyers."

For this reason, he said, despite the measures already initiated, it would not be possible to hold the fair successfully, without a necessary and assured lead time. From now on, the focus will therefore be on the regular Spielwarenmesse from 2 to 6 February 2022, in conjunction with Spielwarenmesse Digital.

No April Fool's joke:

Like us, Spur Z Ladegut Josephine Küpper from Aachen (<https://spur-z-ladegut.de>) chose this headline when informing customers about current new products. Basalt chippings are currently offered for Märklin wagons 8622 (Omm 52 in old version) and 8650 (Eaos 106). As usual here, the material is reproduced with original stone pieces, and, therefore, looks better and closer to the prototype than any plastic reproductions do.



The Lake Constance ferry "Friedrichshafen" (art. no. 10484RF1) is the largest model among the current new products. Photo: NoBa-Modelle

And the latest from NoBa models:

A highlight among the current new products from NoBa-Modelle (<https://www.noba-modelle.de>) is certainly the Bodensee (Lake Constance) Ferry Friedrichshafen, which is offered as a pre-printed blank (item no. 10484R) and as a finished model (10484RF1). The latter requires about four weeks delivery time, and the effort required can be guessed from the photo.

But, even smaller things can make a lot of sense on the model railway: Three modern 240-litre rubbish bins (10261R), the barrier (5503R), and half barrier (5502R), each with an Andreaskreuz (St. Andrew's cross), or also the column-mounted slewing crane (3201R) are worth mentioning here.



A versatile accessory on modern-style layouts is the new column-mounted slewing crane (item no. 3201R; photo above), which here has taken up position next to a track. One of the larger buildings in the range is the tram depot (4124; photo below) with two stands, from which the DoT4 articulated railcar of the Stuttgart tramway emerges here as a finished model (5704RF). Photos: NoBa-Modelle

Further new products are dedicated to the topic of trams. The articulated railcar DoT4 of the Stuttgart tramway can be purchased as a rollable (5705R), motorised (5704R), or in both versions as a finished model (5705RF / 5704RF). It can be stored in the PLA printed tram depot (4124).

The creative designers must have liked their advertising car, because it is now also available with a NoBa chassis, motorised for self-design (5317R), or as a ready-made model (5317RF).



The new painting stands (20100), which can be ordered in both short (front) and long (rear) versions, are proving to be a valuable tool.

We have already come to appreciate the new paint stands (20100) as a great help for colour work. When ordering, you can choose between a short and a long version. These useful tools are made using the PLA printing process.

Cancellation of the Märklintage (Märklin Days) 2021:

It was with a heavy heart that Märklin announced on 30 March 2021 that Märklintage (Märklin Days) 2021, which was to be held in September together with IMA in Göppingen, would have to be cancelled. The still uncertain infection situation does not allow such large events to be planned at all, because their organization cannot be estimated at all.

So here, too, a decision had to be made with health in mind and safety for all involved. The next edition will therefore only take place in Göppingen from 15 to 17 September 2023.

Soldering stations from Conrad Electronic:

Soldering is usually a must when it comes to model railways: whether it's wiring your own layout, attaching supply lines to track profiles, repairing a locomotive model, or building your own from a kit or self-made parts.

Conrad Electronic (<https://www.conrad.de>) has analysed its product range, defined four target groups and recommends a suitable model from its own programme for each of them. Target group 1 are do-it-yourselfers, model makers, hobby electronics technicians and makers; target group 2, schools and training; target group 3, crafts and workshops; as well as industry and manufacturing in target group 4.

The following Toolcraft soldering stations were selected in order of suitability for the task: ST-80A "Analogue soldering station" (order no.: 1561692), ST-100 HF "Digital high-frequency soldering station" (1626065), LSL-951 "Digital soldering station" (2269448) and ST-200 "Pro soldering station" (2248468).

They are available on the so-called "Conrad Sourcing Platform." Other models are, of course, also available in the range.

Further Icarus variants at EtchIT:

The variant presented in the last issue is now followed by the Ikarus 55 as a coach with panoramic windows (item no. XD065_Z). A set of lettering and side windows (XD065dec_Z) is also offered separately for this kit. The frames of the windows are already printed on.



The new Ikarus 55 coach shows itself to the photographer with the optionally available lettering and side window prints. Photo: EtchIT-Modellbau

The eight double parking benches on concrete frames (ET025d_Z) are a nice accessory, as they were set up in many places as a resting place along hiking trails or excursion destinations in the spirit of the times.

The manufacturer's pages with ordering options can be reached as follows: <http://etchit.de>.

New at Micro-Trains:

The tank car from the last issue was now followed by a black example (item no. 530 00 500) as "Sweet Liquid #2" of NATX. With addresses of the Frisco (531 00 341 / 342), Penn Central (531 00 351 / 352) and Milwaukee Road (531 00 361 / 362) now also open bulk freight cars in light grey livery have been delivered.

Micro-Trains products are distributed by Case-Hobbies (<http://case-hobbies.de>), among others.

New world record at Wunderland:

Miniatur Wunderland in Hamburg has made extensive use of the period of forced closure since October 2020 to carry out long-planned work that would not have been possible with visitors. But, as the list of activities became shorter and shorter, and hopes of opening at Easter were dashed, new creative ideas were required.

Among them was one for a highly complex world record: the impetus was a video from the year before last. An Airbus A380 took off in Wonderland, with a Father Christmas on the wing, and a mallet in his hand, hitting water-filled bottles to the tune of "Jingle Bells".

This has now become the new campaign, in which a train travels along differently filled water glasses from Scandinavia to Italy through the entire wonderland, playing the world's best-known classical pieces. On 17 March 2021, the first attempt was successful, and the world record for the longest melody played by a model train went to Hamburg.

If you wish, you can watch the video of the record run here: <https://youtu.be/aBNHmUT3GPg>.

Supply problem at WDW Full Throttle:

According to information from its German sales partner Case-Hobbies (<http://case-hobbies.de>), WDW Full Throttle is currently not being supplied with axles due to a delivery stop. Bowser usually has these parts manufactured in China. According to this information, the search for an alternative is underway, but this will result in a price increase.

The 33-foot bulk goods wagons with two discharge hoppers were delivered as new products. They are painted in CSX's blue livery and are packaged as a set of two (item no. FT-3044-1 / -2). One wagon each is loaded with sand and gravel from the Hay Brothers production.



The blue CSX bulk goods wagons (art. no. FT-3044-1) look attractive thanks to their load inserts, which create an excellent contrast. Photo: WDW Full Throttle

Newly assembled is a collector's configuration (FT-COL49-1) of two coal wagons of the PRR (FT-2026) and PRSL (FT-3037), which were offered in 2009 and 2015 respectively under the given item numbers in packs of two. The two cylindrical bulk freight wagons "Southern (Big John) A&B" (FT-1065-1 / -2) in silver and black basic colour are shown as new products for March 2021.

Making the best of the crisis:

At Modellbahndecals Andreas Nothaft (<https://www.modellbahndecals.de>) there are now tank car inscriptions for all gauges to match the Corona crisis (art. no. 6659). At the suggestion of a customer, a decal set was created for such wagons, which show a transport of vaccines. Even if a wagon labelled in this way has no real model, it is an option to face this situation on the model railway with a little humour.

Märklin optimises its logistics:

Märklin has reacted to increased demands on its own warehouse, which in the past had repeatedly led to delays in the delivery process. Logistical capacities are currently being significantly expanded.

In the coming months, the previous Märklin warehouse in Ebersbach is to move to the parent company's central warehouse in Sonneberg (Thuringia). There is already a highly modern, automated and well-equipped high-bay warehouse there, which has been further expanded in recent months.

13,200 pallet storage spaces and also an automatic small parts warehouse, the capacity of which was expanded threefold in 2020, to a capacity of 32,000 containers, will speed up the aforementioned processes in the future.

A total of 180 employees have been looking after more than 4,000 products on an area of more than 50,000 m² there so far, if necessary, also, in several shifts, Märklin let its dealers and the press know. Thanks to a high degree of automation, the throughput times of all products are to improve with the completion of the move.

Herpa new products for midsummer:

Herpa has announced new 1:200 scale aircraft models planned for July and August 2021. We have selected those which, with a length of less than 30 cm, still remain suitable for layouts and fit European motifs.

In the series of the more sophisticated, because more detailed Wings models are now announced:

Aeroflot Yakovlev Yak-40 (Art.-Nr. 571456),
British World Airlines Vickers Viscount 800 (571463),
Airbaltic Airbus A320-300 – new colours (571487),
Zeppelin-Reederei Zeppelin NT “ZF – Next Generation Mobility” (571494),
Aeroflot Ilyushin IL-62M – Le Bourget 1971 (571524) and
Malév Hungarian Airlines Ilyushin IL-18 (571531)

The Swiss International Air Lines Airbus A220-300 with registration HB-JCL and christening name “Winterthur” (558952-001) is a modified edition of the model that carried the same part number without the -001 extension.

A similar model is also issued with the registration HB-JCQ without the baptismal name as a Snapfit model (613323) with retracted undercarriage. The following new products will also appear in this series of simplified models:

Sky Up Airlines Boeing 737-800 (613187) and
Condor Airbus A321 (613194).

A new model is the Airbus A330-700 Beluga XL (613286), which we therefore also mention. We also want to mention another model for friends of the music group Iron Maiden, which is produced in 1:250 scale: Boeing 747-400 “Ed Force One” for “The Book of Souls Tour 2016” from the stock of Air Atlanta Icelandic (613293).

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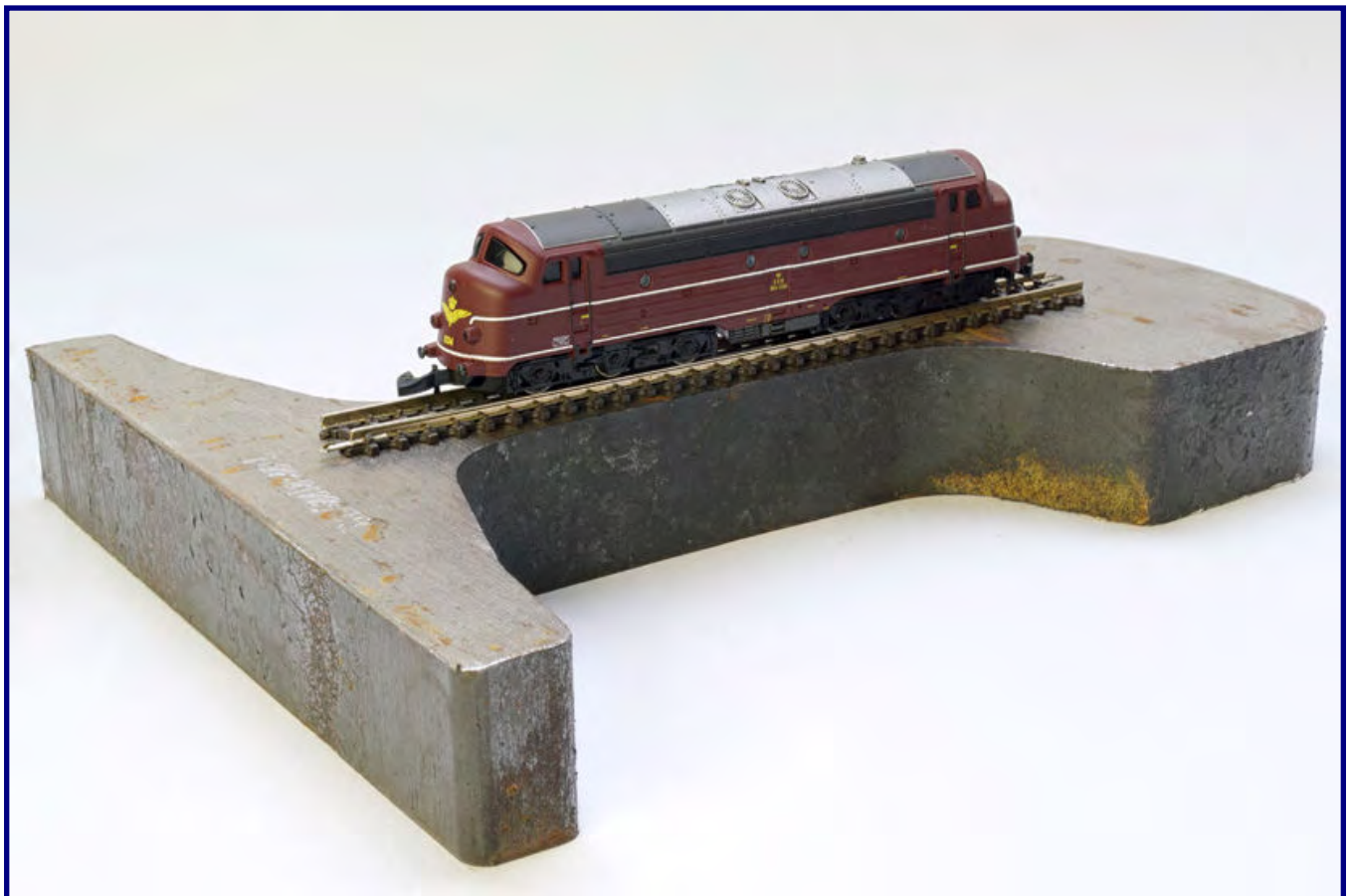
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German Magazine for Z Gauge



A Heavyweight Enters the Race

Model Railway Photography for Beginners
A Portrait of HOS Modellbahntechnik

Introduction

Dear Readers,

I am looking forward, hopefully with you, to a new edition of our magazine. Unfortunately, we were not quite able to meet the deadline we had set ourselves for our new website.

But, we trust that you agree with us: Quality comes first. A team of seven, consisting of editors, translators, and technical site supervisors, worked hard on it for about half a year along with the magazine.

A lot of time, which we all had to squeeze in somehow, besides work, family and other commitments, has gone into it in the course of more than six months. From the first idea to the definition of the design and structure, the technical set-up to the implementation, and translation of content, it demanded a lot from us.

We are proud of the result and very much hope that you like it too – of course it is nothing static and will be further developed, adapted, and hopefully supplemented, and constantly improved in the future.

From now on **Trainini®** is more than a magazine, it is to become an information and news portal for as many matters of Z gauge, as possible, on the highest journalistic level. Hopefully, it will be worthwhile to visit it more often than just once a month!

And, by the way, this new edition also came into being: Here we took a look at DSB's MV 1134, Märklin's latest NOHAB model with a metal resin plastic body. Our reader Hartmut Schnittjer has discovered the MAN 630 trucks from Ratimo-Z, which are also a perfect match for the SSy 45 heavy-duty wagons we presented in April.

Dirk Kuhlmann gives photo tips so that even beginners can successfully showcase their creations and hopefully present them in this magazine. Heinz O. Schramm introduces himself and his brand HOS Modellbahntechnik. This is like a short curriculum vitae and at the same time a look behind the scenes of his work.

We have also “procured” plenty of reading material: The German Locomotive Archive has no less than four volumes, which will make even rainy days entertaining. Those, who are waiting for Märklin's Vectron and are passionate about films, will find increased anticipation in watching a new DVD from EK-Verlag.

And, in the broader environment, the world of Z-gauge does not stand still either. In May there was and still is a lot to report. These are new products and news, but also announcements outside of the box.

The aspects of Z-gauge are varied and colourful, and that brings me back to our website: The site motto “My world on a scale of 1:220” also has at least two aspects. The personal pronoun should fit me as the father of **Trainini®** as well as each of our readers. Enjoy with us and above all, stay loyal to us!

Trainini® as well as each of our readers. Rejoice with us and above all, stay loyal to us!

Sin-Z-erely,

Holger Späing



Holger Späing
Editor-in-chief

Revised NOHAB from Märklin

A new Heavyweight

The round noses built by NOHAB have a true cult status for many railway enthusiasts, just like the V 200 in Germany, for example. It is therefore no wonder that there are models almost across all gauges. Märklin has given its models more weight, reworked some details and now sent them into the race anew. We look at the latest offshoot and also take a look at its prototype.

In **Trainini®** 5/2019 we already presented the prototype history of the European offshoot from the US F7 series of General Motors EMD. In it, we explained the differences in appearance and which state railways have procured and used machines of this type.



1995 in Strib during transfer, it showed its original design: two windscreen wipers, but no road number on the fronts, and, in addition, the roof and three-part fan band in light grey. The museum-preserved locomotive had previously brought a special train to the former ferry station, the track of which has long since been replaced by a cycle path. Photo: Ulf Häger

The occasion for the report was the first appearance of the pioneering small series model by FR Freudenreich Feinwerktechnik. In the meantime, Märklin has also revised its design as part of a product update and offered various versions for the first time or again. The latest offshoot of the round noses is a class MV locomotive from the DSB stock.

Following on from this, we would first like to take a look at the great prototypes and their use. The DSB was the first state railway to use this type of locomotive, which was known only briefly as “NOHAB” after its licensed manufacturer.

After Nydqvist och Holm AB in Trollhättan (Sweden) had acquired a European licence for EMD products from General Motors in 1949, the Danish State Railways ordered the first four examples in 1952. They were delivered in the spring of 1954 and entered service as MY 1101 (7 February 1954) to 1104.

Despite later export successes to Norway, Belgium, Luxembourg, and Hungary, the use of a diesel locomotive from the United States was not without controversy. At the same time, however, General Motors could be considered the only diesel locomotive manufacturer with presentable experience at the time.



On 28 August 1981, the first machine delivered to DSB as MV 1101 is already shown in Fredericia in its last livery and with only one wiper left on the front windows. Photo: Ulrich Budde (Bundesbahnzeit)

This was obviously decisive for DSB, but the licensed construction still took place outside the country and disadvantaged its own locomotive industry. In order to do justice to their own economic interests, it was agreed that Frichs from Aarhus would manufacture and supply the locomotive bodies. From the second machine onwards, the bogies also came from the Frichs factory.

The DSB's courageous decision, which was certainly not easy to implement, was later vindicated, as it ordered a total of 59 MY series locomotives, in addition to lighter offshoots for branch lines:

- MY 1101 to 1104 (ordered 1952),
- MY 1105 to 1124 (1954),
- MY 1125 to 1144 (1956), and
- MY 1145 to 1159 (from 1964).

There were variations between these series, which also led to a division of the designations from 1968 onwards: MY 1101 to 1105 had a 16-cylinder GM 16-567 B engine (1,700 hp), MY 1106 to 1144 the GM 16-567 C type (1,950 hp) and MY 1145 to 1159 the once again improved GM 16-567 D1 (also 1,950 hp) as well as a modified silencer ("grooved exhaust"). The weight of the machines increased from 98.6 to 101.6 tonnes.

From 1962 onwards, there was an extensive exchange of engines in the course of repairs. When the 1,750 hp units of the first four examples also found their way into other locomotives of the series, DSB decided to mark such machines by assigning them the designation MV, while retaining their order number, from 1968 onwards. This remained the case for MV 1144 until it was withdrawn from service in 1987.



MV 1134, template for Märklin's model, is already in the red and black livery with a goods train near Bredstrup on 28 August 1981. Photo: Ulrich Budde (Bundesbahnzeit)

The lower power was now immediately recognisable because of its operational relevance. On the road with the changed series designation were the 1101, 1102, 1104, 1109, 1134 and 1144 – a total of six examples. MV 1101, 1104 and 1134 received a 1,950 hp engine in 1984, 1973 and 1981 (again) and were again designated MY.

A major difference between the round noses and the American F7s is their six-axle instead of only four-axle bogies. The reason for this was the weaker superstructure in Europe, which required lower axle loads. Thus, a non-driven carrying axle was placed centrally between the outer driving axles.

While this was sufficient for the MY and MV for their use in the lowlands of Denmark, all other NOHAB and AFB models were driven on all six axles. It is remarkable that the DSB version, despite only four traction motors, still had an axle load of 17 tonnes, and thus could not operate on all branch lines, which later made a lighter version necessary.

Use and appearance

All machines delivered to DSB wore a brown-red colour scheme ex-works, corresponding to the colour shade defined in Germany as RAL 3011. Their inscriptions and the crown, on the side with the lettering DSB, on the front with wing wheel, were applied in yellow, the roof and fan belt were light grey, the bogies were black.

On each of the long sides there were two white lines running around the nose. While the lower one ran completely around the edge of the locomotive body, the upper one led over one of the two lanterns at the front in each case and ended behind it in a strong curve to it.

At the beginning of their service life, they did not yet have service numbers on the ends. Allegedly, the Danish King Frederick IX criticised this during a train journey because the number of the locomotive was not visible from the first carriage. From 1957 onwards, this was changed on the existing machines, the examples from MY 1125 onwards already had them ex-works.



MY 1101 of the DSB is on the move with a single 2nd class passenger coach in Rønbjerg station on 5 September 1976. Its design is, except for the already dismantled second windscreen wiper, identical to the Märklin model presented today. Photo: Kurt Rasmussen

At the beginning of the seventies, here in 1972, some railway administrations began to search for new and contemporary colour concepts. At the DSB, several designs envisaged a black-red colour scheme. The decision was made in favour of black locomotive bodies with red fronts, which NOHAB liked to call round noses.

The colour change for these locomotives began in 1973, the first machine in the new livery was MY 1147. In 1985 the repainting, which was carried out as part of repairs, was completed. Other changes included

the addition of women's names and shunter's platforms above the buffers in the nineties, which were to remain in place until the final withdrawal from service.



On 30 August 1981, MY 1141 is a border crosser: with a long passenger train, she could be captured on celluloid on this day in Flensburg, Germany. Photo: Ulrich Budde (Bundesbahnzeit)

It is certainly interesting to take a look at international operations, which make NOHAB locomotives interesting, not only for the recent past on German layouts: SNCB specimens pulled international trains via Aachen to Cologne, but also DSB machines reached German soil.

If German V 2000s ran for a time as rental locomotives in Denmark, the round noses also came across the border. DSB's MV and MY series were to be found in Flensburg, but have also been photographed in Hamburg.

A look at the Märklin model

After Märklin had already offered a revised SNCB version before, a Danish representative in original colouring with operating number on the fronts now also followed (item no. 88637). This feature, its road number 1134 and the designation as MV make it possible to narrow down the operating status somewhat.

From 1968 onwards, the class designation MV was used, and the locomotives already had a locomotive number on the front. MV 1134 was in service until 1981 with the engine that gave it this designation, before it was dismantled again. The model is therefore to be classified in epoch IV, when the passenger coaches were also already provided with UIC (The Worldwide Railway Organisation) lettering ("computer numbers").



After an extensive product update, we are once again presented with MV 1134 from Märklin, a DSB version of the NOHAB. The only missing feature that spontaneously stands out is the front skirt, which does not want to fit a system coupling.

In this time window, the Bundesbahn initially still had green and blue passenger coaches on the road, a limited number of pop colour coaches and then, from 1974 onwards, also those in the new ivory/ocean blue colour concept.

The now delivered new product 2020 is therefore good for exciting contrasts on the model railway, as long as it is used as a border crosser. It is painted RAL 3011 brown-red and carries the two white trim lines and yellow inscriptions that we have already described in the prototype section.

The only deviations are the colours of the fan belt and the roof area. The roof area around the exhaust and the fan rotors is painted in white aluminium, the areas in front of and behind it are painted in a dark grey. This was also used for the fan belts.

This does not correspond to the described and documented original state, but that is not what the model represents. We have not yet been able to clarify whether a change in the paint scheme is the reason for the difference or whether it is just due to the usual traces of use. In any case, a photo of MY 1101 from 1976 shows such a dark fan band and roof, in which lighter areas and partly also a lighter edge can be seen.

So Märklin does not seem to have made a mistake here. Either historical operating photographs were meticulously evaluated in Göppingen, or there were even relevant painting instructions that were then followed exactly.

The fan blades deserve another mention as a detail that is difficult to reproduce. On the NOHAB locomotives it was easily visible from many perspectives, making it appear very transparent. This is difficult to reproduce in all scales, and simply impossible for a Z gauge model in injection moulding.



The roof and fan blade were painted (mostly) in a darker grey, bordered near the driver's cabs with a line in the lighter aluminium colour (photo above). A proof of this design can be found in the prototype picture on page 7. The bogies with new panels according to the correct pattern, and the revised cast block of the running gear have benefited from the product maintenance (photo below).

An alternative would be a very fine etched part, which could be produced precisely and flawlessly, and which could be permanently mounted on the model without any gaps. The effort for such a revision would not be justifiable and would not bring any real gain: What should the viewer's gaze then fall on?

The chosen dark reproduction of the fan blade is exactly the way to save costs and to convey the prototype impression in the best possible way. It gives the impression of dark paint or dirt and suggests a dark engine room – problem solved.

Paintwork and printing are on the usual high Märklin level and give no cause for criticism. The rather matt finish of the paint is pleasant, which makes the shiny plastic parts for the round engine room windows and those of the driver's cab look better.

Engraved on the front windows are even two top-hinged windscreen wipers. This is a nice, if hardly noticeable detail, but it raises questions: Many photographs of DSB machines show them with only one in front of the right front window. Since they were present on both sides on delivery, they were probably removed later, at least in part. We have also found the 1134 with only one windscreen wiper in photos.

Before we go into the innovations of the latest edition after the product update has been carried out, we must briefly address the “birth defects” of this model, which it cannot discard and which must be accepted

as compromises. The Märklin model is to be regarded here as a low-priced and quite acceptable alternative to the considerably more expensive small-series model from FR Freudenreich Feinwerktechnik.



As with all other NOHAB models from Märklin, windscreen wipers can be found on both cab end windows. For the prototype, however, this equipment remains questionable. While the black printed lamp rings and the front door reproduction by means of pad printing look successful, the bare buffer plank without any detail remains one of the “birth defects” that can no longer be rectified without greater effort for shape changes.

While the large-series manufacturer weighed the costs against the expected number of units and took the price sensitivity of the customers as a yardstick, the small-series manufacturer set out to create a perfect model for purists that was as coherent as possible. So, both serve very different target groups who are equally happy.

In the case of Märklin, however, this also meant that no special running gear was developed for this type of locomotive. Instead, the chassis of the 103 and 151 series, which was considered most suitable, was taken as the technical platform. The bogie covers of the donors, which were completely unsuitable in effect and details, and which were still taken over without changes in 2005, immediately betrayed this.

This choice also led to the fact that the body of the NOHAB had to be constructed around the bogie. So, the prototype dimensions and proportions were not the leading criterion here either. Finally, the new housing had to be adapted to the chosen undercarriage and its appearance had to be adjusted in such a way that this would not be noticed immediately or by everyone.

Resulting and unavoidable flaws, for example in the side window area or a slightly too great length, can no longer be rectified. This also applies to the naked buffer beam without hints of brake couplings and draw hooks, at least enlarged buffer plates would still be an option.

Results of the product update

But there is also one feature that, according to our impression, Märklin has even achieved somewhat better than FR Freudenreich Feinwerktechnik. These are the driver's cab front windows, which were meticulously scaled in the small series to 1:220 scale and have also met the laterally pulled-down shape absolutely credibly – this remains at best a vague suggestion with Märklin.



The driver's cab front windows do not have the completely correct shape even with Märklin (photo on the left), but in the overall ratio of height and width they still appear more coherent from some perspectives than on the small series model. The new bogie covers contribute considerably to the now successful overall impression (photo on the right). The magnifying NOHAB factory plate and many other inscriptions underline the high print quality.

However, on the more expensive model they seem a little too high, which from some perspectives looks like wide open eyes. Märklin seems to have achieved a better balance between height and overall width, which saves a lot in the front view. The missing detail of the front door, including the handle, has been reproduced by pad printing, this time.

But artistic skills in pad printing do not make for product care. What is decisive is which features have ultimately been revised. The first thing that stands out is the new bogie frames, which finally fit this locomotive in terms of the details reproduced.

Gone is the smooth bar that hid the middle distributor gear and at the same time exposed the view of the outside wheels, because the housings originally belonging to this bogie were drawn much deeper in the bogie area.

At the same time, the interior has also changed in the meantime, as with other six-axle bogie locomotives. The middle axle of both bogies now gets by without a cogwheel and remains without drive – irrelevant for traction because this axle was always mounted somewhat higher and had no fixed rail contact.

Something has also visibly changed between the bogies. Even though the bogie is still the platform for various models, we now no longer have to make do with the features of the contributing electric locomotive. New sliders now allow to correctly reproduce battery box and ladder to the middle engine room doors.

The most serious errors of the predecessor editions could, thus be eliminated, and bring this model much closer to the prototype. This becomes all too clear in a direct comparison with the MY 1103 in almost identical design from 2011 (88633).



Märklin has dispensed with a drive for the middle axle in three-axle bogies, in the meantime, as is the case with the NOHAB (photo above). However, the centre axle is still mounted a little higher, for better cornering and has no direct rail contact via its running surfaces (photo below).

However, there is still some downside: although the driving characteristics have improved considerably with the new bell-shaped armature motor, which now also makes gentle creep speeds possible, we are not completely convinced by the lighting, which has been changed to warm white LEDs.

The locomotive starts moving so easily that its top light cannot even be guessed at. A light emission is only noticeable when the controller position is increased further. It should be the other way round and Märklin has done a much better job with many other models in the recent past!

A look at the locomotive's weight is a pleasure: While the earlier models weighed in at just 30 grams, it now weighs a whopping 44 grams thanks to metal plastic alloy. Almost 15 grams of this are due to the new compound material for the housing, where it once weighed only 4 grams.

As explained in our report on this technology in **Trainini®** 5/2019, Märklin was able to make use of the properties of metal for good traction without having to completely forego the advantages of higher flexibility of plastic. And so the new total weight, which already comes very close to a zinc die-cast model, benefits, above all, the tractive effort.



All in all, Märklin's NOHAB leaves a thoroughly good photo after the fundamental overhaul. The tractive power benefits noticeably from the metal plastic alloy body and the operating behaviour from the new bell-shaped armature motor. Only the coordination with the direction-dependent top light is not convincingly successful.

The latest DSB version pulls a lot more carriages and thus lives up to its powerful prototype. We have already mentioned that it also runs much smoother. The question remains, what can we attach to this locomotive?

This is already difficult because Märklin does not offer Danish passenger coaches. Suitable DSB freight cars can also only be found at the small-series manufacturer FR Freudenreich Feinwerktechnik. Here, however, it should be easier to find foreign examples from the large and small series to go with internationally operating DB types.

The prototypes were used in both freight and passenger service. In the case of freight wagons, we have almost free choice when we think about which goods were exchanged between Germany and Denmark or passed through Germany in transit. A colourful and varied train is then quickly put together.



Due to the lack of Danish passenger coaches in the Z-gauge programme of all manufacturers, we couple DB coaches to the locomotive, which can also be supplemented, on a case-by-case basis, by an SJ passenger coach from Sweden (model by FR Feudenreich Feinwerktechnik).

With passenger trains, on the other hand, it becomes a challenge: almost only German passenger coaches that ran in international trains come into question. These can be TEE wagons that are continued into the Kingdom of Denmark or D trains that were formed exclusively from DB wagons.

Here, chrome oxide green and steel blue UIC-X coaches (87400) can be mixed with sibling coaches already painted ocean blue and ivory. This also provides some variety and welcome contrasts.



An international express train carrying blue CIWL steel wagons (Item Nos. 8777 / 8778) is certainly not out of place behind MV 1134 either. This composition certainly offers a nice contrast.

If you want to reproduce a long-running express train in Europe, you cannot avoid the small series (FR Freudenreich Feinwerktechnik (express train coach AB8K of the SJ, item no. 46.299.00) or self-build (such as Az and ABz of the FS, presented in this magazine).

It would then also be conceivable to build another of the UIC-Y coaches on a Märklin basis, which are widespread in Denmark. They would have to be shortened and fitted with a different roof to get as close as possible to their prototype.

But that is another story. For the time being, it remains decisive that the reworked NOHAB is much too good for the showcase. There should be a possible use and explanation for the individual train formation.

Manufacturer of the basic model:

<http://www.maerklin.de>

Information and photos used for the article:

<https://bundesbahnzeit.de/seite.php?id=363>

<https://nohab-gm.de>

https://de.wikipedia.org/wiki/NoHAB_AA16

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MAN 630 from Ratimo-Z

The perfect Complement

In the last issue we presented the new SSy 45 heavy-duty wagons from Märklin. Supplemented by models of the six-axle type SSys 46 and Leopard 2 tank models from Artitec, the result is a sizable train. The transport is secured by two vehicles of the medical battalion, which are new products in the Ratimo-Z programme. Our reader Hartmut Schnittjer reports.

By Hartmut Schnittjer. As I had already read in **Trainini**® 3/2021 that a report on the new Märklin heavy goods wagons (item no. 82229) was to appear in issue 4/2021, I took this as an opportunity to create an additional freight load for them.

Since this pack is about tank transport, I thought about expanding it with ambulance wagons. But, first, I would like to mention that I am not glorifying war. My aim was to create an additional eye-catcher with manageable financial means and a little skill. The columns of the German Armed Forces are still in my memory and therefore a part of my childhood.



With the help of two ambulances based on the MAN 630 from Ratimo-Z, our reader has been able to appropriately expand his armoured transport train. Photo: Hartmut Schnittjer

But now to the project: The all-terrain MAN 630 truck had been on my wish list for a long time. Rainer Tielke has such a vehicle in his programme, and two examples were ordered. They were to become Bundeswehr ambulances.

There are photos of these trucks on the Internet that can serve as a model. At the same time I wanted to have my first experiences with Micro Kristal Klear (window reproduction), and wet decals (red crosses).

I ordered the adhesive paint mentioned above from Modellbahnunion and the red crosses from Modellbahndecals Andreas Nothaft. The colours were already available. Now that everything was in stock, I started painting.

First, I painted the wheels matt black and then the vehicle yellow-olive (RAL 6014). If I have researched correctly, this colour was used in epoch III, which I am in. Then, I made the indicator on the bumper.

As a base, I had the choice between a no longer used metal guitar string or pins. I chose the guitar string because its diameter is a bit thinner at 0.4 mm.

I shortened it to a length of about 6 cm, primed it and then cut it 4 times to a length of about 4 mm using side cutters. I then filed the cut surface smooth and glued it to the ends of the bumper with Uhu-Kraft-Alleskleber. A pair of needle-nose pliers or tweezers is a valuable aid here.

After the glue is dry, the bars can also be painted yellow-olive. Finally, a dab of paint is applied to the upper end of the bumper. I used RAL 1014 ivory.

Next, it was on to the red crosses. After a short bath in water they come off the backing material. With patience, tweezers, a small slotted screwdriver (from a pocket knife) and glasses I placed them.



Micro Kristal Klear is an adhesive varnish for fixing clear parts or for reproducing entire window panes. Illustration: Micro-scale / Modellbahnunion



Rainer Tielke offers three different versions of the MAN 630: Cargo bed and twin tyres at the rear (left), box body (centre) and cargo bed with winch between bumper and bonnet (right). Our reader chose the middle model with NATO box in unpainted finish. Photo: Ratimo-Z

continues on page 20



Unpainted blanks like these formed the working basis for our reader's project, with which he wanted to gain new experience. Photo: Ratimo-Z



The all-terrain ambulances for field use are created in three phases.

Step 1:
The colourless blanks are first primed in RAL 6014 yellow olive. Later, the headlights and the sphere on each indicator is painted.

Step 2:
The Red Cross symbols for hospital vehicles not taking part in combat operations, as agreed internationally via the Geneva Convention, are applied as decals.

Step 3:
Replicas of the window panes are created with Micro Kristal Klear from Microscale. The adhesive varnish, which is white in its liquid state, is applied with a brush to one corner of the window and drawn diagonally across the entire surface. After drying, it becomes a transparent pane. Photos (3): Hartmut Schnittjer

After drying overnight, the vehicles received a silk matt clear coat for protection. After another night of drying, the last step followed: the reproduction of the windows. With a brush (Rotmarder size 0) they could be "inserted" into the openings.



Rainer Tielke had already designed the MAN 630 as a milled model (models in the middle), but due to a lack of building instructions and better feasibility in 3D printing, he redesigned it (models on the outside). Clearly visible are the improved details in the area of the radiator grille and the roof of the NATO box. Photo: Ratimo-Z

The procedure is very simple: Take a little of the Micro Kristal Klear with the brush and spread it into the window frame. A white film forms, which is transparent after drying.

Now the vices are ready. An additional eye-catcher now enhances my layout. I might even consider making them rollable by means of brass tubes and steel wire, which then serves as an axle.

Manufacturer of the basic model:

<https://www.maerklin.de>

Materials and accessories used:

<https://www.modellbahndecals.de>

<https://www.modellbahnunion.com>

<https://www.rainer-tielke-modellbau.com>

<https://www.uhu.de>

Guide to Model Railway Photography (Part 1)

Successfully photographing Model Railways

How many of us are taking pictures of their models, but are not happy with the results and would like to improve on their photographic skills? From snapshot to photography: Our editor Dirk Kuhlmann has prepared a tutorial for staging our layouts more effectively, and to take photos that impress, without too much extra effort or cost.



By Dirk Kuhlmann. Many model railroaders have contacted me in recently for help with improving the photography of their modelling projects. After all, the limitations of smartphone cameras and compact cameras quickly show when taking photos of scale models, in particular, for very small scales such as Z.

The unfortunate and frequent habit of taking macro shots only adds to the frustration. Often, only a small area is in focus and the rest of the image literally drowns in an undefinable blur. But, just a little more distance from the subject will reward the amateur photographer with a sharp photo, at least, on the small screen of their device.

However, if the resolution of the camera is too low, this will immediately become obvious when zooming into the image on the large screen of a computer. Smooth lines will look jagged or “pixelated” on the large monitor of the computer. This effect is also related to the size of the image sensor of the camera that has been used.



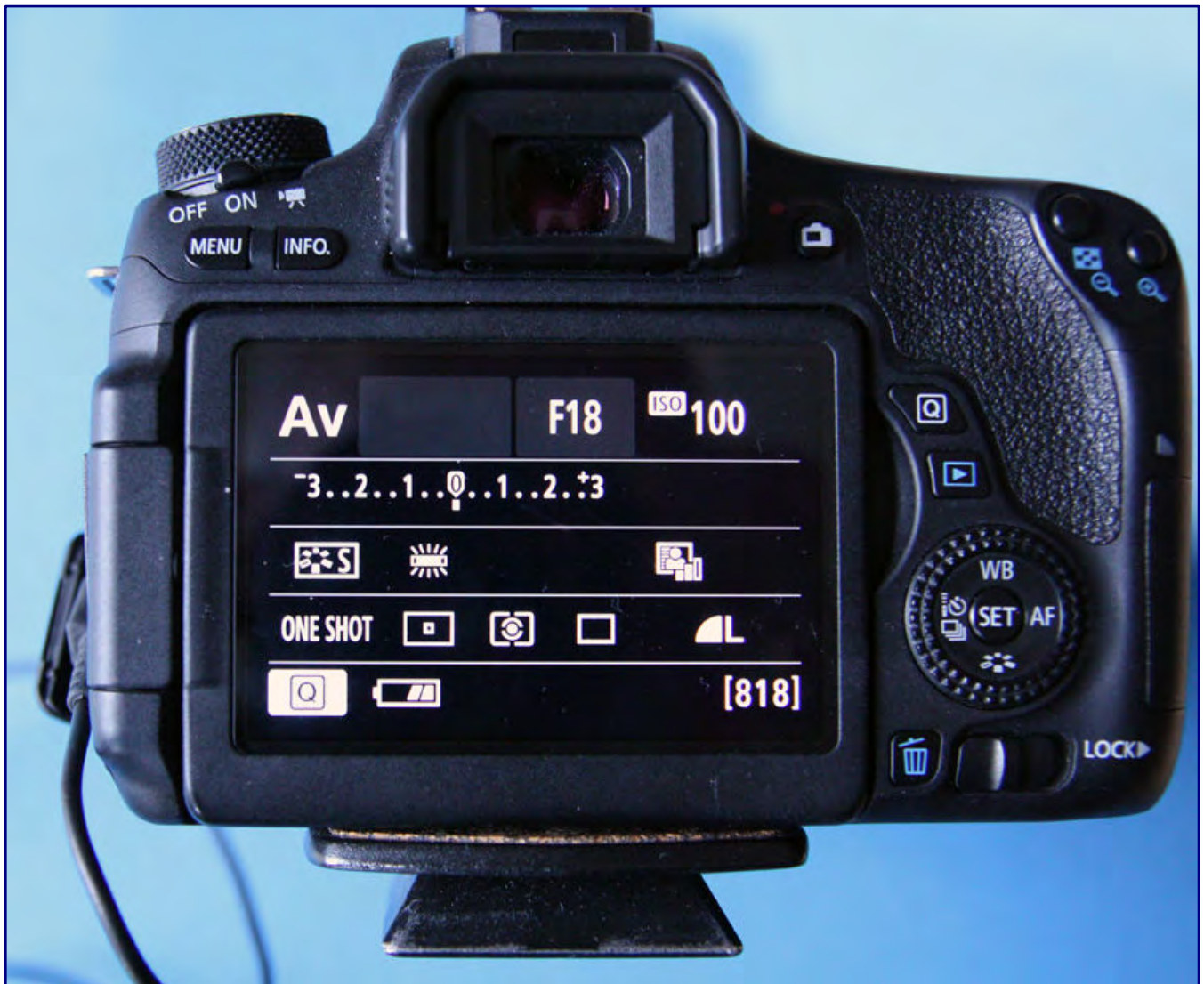
A typical SLR camera offers the setting options that are important for our type of photography: 24 mm focal length for the optimum initial setting (1), ISO (ASA) setting for the sensitivity, usually 100 ASA (2), and AV setting so that the camera automatically selects the correct exposure time for the manually set aperture (3).

For quick documentation purposes on internet forums, such a photo can certainly still be of some use. However, higher standards are required for presenting images on one’s own website, other public media, or even model railway magazines!

I have often seen beautiful 1:220 scale layouts with my own eyes. But when later looking at photos of those layout, mostly taken by their owners, they no longer reflected the charm and atmosphere of the original layout. It catapulted itself into a certain meaninglessness.

Forgive me for these harsh words! But, it is a real pity to have so many hours of intensive work on a gorgeous layout wasted by bad photos.

After some preparation and research on the internet, I now have a fairly complete overview. There is no lack of guides for model railway photography, ranging from highly professional to very poor. Readers that really want to immerse themselves in the subject will find all the information they need. However, most people's heads will be smoking and their brain will switch off after one page of concentrated and expert explanations.



The most important settings can be seen again on the display, here, for artificial light shots (cf., explanations on page 26).

Surly, there must be easier ways, or is photography just a highly scientific activity that amateurs should keep their hands off? Or even worse, we Germans tend to be absolute perfectionists, even if it's just a hobby. So, what are the possibilities for taking your own beautiful photos?

Let us assume that there are the following three typical archetypes of model railway photographers:

1. The imitator

He looks closely at his model railroad magazines and imitates the photos he sees there. That means practising taking photos over and over again. Asking a lot also helps, and at some point, he may get the hang of it.

2. The smart one

As an exhibitor, he waits until an author / editor stops by his stand and wants to take photos for a magazine. This saves a lot of learning time and work. But, what if the hoped-for person doesn't show up at all?

3. The long-distance runner

There are a lot of model railway enthusiasts with a second hobby, namely photography! Most of them have been associated with very specific themes for many years. Whether architectural, landscape or people photography, you can definitely bring this experience into model railway photography.

Looking back on my own photographic journey

The best example is always one's own photographic life. I have more than 35 years of landscape photography under my belt. In the past I strictly followed the typical "rules," but, since 1995, I have consciously broken with many photographic conventions.



An LED work light, the modern version to the once so often used halogen flood lights from the DIY centre (see also info box on page 25), is sufficient for the most important of our uses.

And, I did this despite studies in which photography was of a high priority. Why? "Taking photos" is a rather subjective act, and rules do sometimes or often disturb or destroy the intended outcome.

Practical tip: Studio light with amateur equipment

As implied by the Greek roots of the word photography – “drawing with light” - photographs cannot be taken without light. But there is not always enough natural light available for successful photographs, in which case, we need an artificial light source.

When in need of artificial light, it is almost always better to avoid using a camera's built-in flash or one that is mounted on its top. The reason for that is that these flashlights cast a harsh light with strong shadows and make for highly saturated colours and unnatural looking images.

A better and simpler studio light can often be found quickly: This can sometimes be a torch (flashlight) or a small table lamp, but also your 500-watt halogen spotlight from the DIY store that you may have last used when renovating your house.

If the light is not mounted on a support, you will need the assistance of another person who can hold and control it so that the scene is well illuminated, preferably from one side. It is worth experimenting with different light sources, directions, and intensities to get a feel for it.

Light not only serves to make image elements visible, but is also a stylistic tool, and a means for expressing moods. And it is this which makes for lively scenes!

In 2009 I tried to adapt my style of landscape photography for model railway photography, as well.

Only in 2014 the magazine “Eisenbahn-Kurier” was the first trade journal that entered into a deal with me.

You can already see how long things can take until one's own handwriting has developed to be perceived as beautiful and to be seen as fascinating by another observer.

It was quite an uphill battle for me until I had reached that point because, as is so often the case, established photo

graphers could not, or would not understand my way of working.

The following years have proven the opposite and with that I can only call on everyone to give it a try and be creative. Always remember, the camera is only the tool for you and your impressions. The way you see and perceive your world is the way you want to capture it in the photo, isn't it?

To get you started somehow, I will show you my “small” equipment and the necessary settings. Experienced photographers may overlook the fact that technical jargon and product recommendations are completely ignored here.

1. a Canon EOS 760D digital SLR camera with a 17-70 mm Sigma lens.
2. a stable, but also light tripod, which is also really good for travelling
3. an LED floodlight with 50 watts (e.g., Conrad 1182143) and a suitable light or microphone stand.

My SLR camera has a sensor that is about half the size of the slide or negative we know from analogue 35 mm photography, the so-called “half format.”

This sensor is, therefore, much larger, and can store more image information than the tiny versions of many mobile phones and compact cameras.

This is a great way to document the smaller model railway gauges, and the photos are really sharp.

But, please, always leave the highest level of image resolution set! We will not use any built-in flash, at all.

The seven important settings for daylight photography are, as follows:

1. Automatic shutter speed (AV setting, i.e., “aperture priority”)
2. Light sensitivity ISO 100
3. Lens set at 24 mm focal length
4. Set aperture to 18
5. Any scene modes to standard
6. White balance to automatic
7. Autofocus to spot metering

Just have a look at the instruction manual of your camera and try these settings, if you have an SLR or similar camera with larger sensors.

Owners of mobile phones and compact cameras may try a similar approach and set the maximum aperture to a fixed value, as long as this can be influenced. Especially because of their much smaller sensor, there are additional limitations with these types of cameras.



A typical example of a photo taken with a small camera sensor. It appears unbalanced and lacks sharpness when enlarged.

The recommended settings for artificial light photography are as follows:

1. Automatic shutter speed (AV setting)
2. Light sensitivity ASA (ISO) 100
3. Lens at 24 mm focal length
4. Set aperture to 18
5. Set "scene modes" to "standard"
6. Set white balance to the symbol "fluorescent tube" (if your LED spotlight or model railway lighting has a colour temperature of 4000 K)
7. Autofocus set to spot metering

Some cameras have a function to reduce flickering caused by fluorescent or AC-powered LED lights. In this case, please activate this function manually!

Now, it's time to focus on the object of desire

I always work with a tripod to avoid camera shake during long exposures. In addition, I use a wired remote shutter release, but the self-timer integrated in the camera can, of course, also be used.



Bruno Kaiser from Miba Magazine, a true model railway photography professional, in action: Here, more than 3,000 watts of light are needed, and a wickedly expensive camera is in use. But apart from that, we amateurs work in a very similar way.

I usually take the first photos at a distance of 30 - 60 cm. Of course, this means that there are more objects in the photo, than desired. But the larger sensor leaves room to later selectively crop the image, which I can usually check immediately, with the view and magnification functions of the camera display.

Besides, this slightly larger distance to the object has another advantage: more depth of field. The high f-number (small aperture) together with that certain distance produce exactly this effect.

However, the most important prerequisite for a successful photo is always the quality of the available light. It is not for nothing that professional photographers light their backdrops so elaborately. However, professional lighting can also ruin any atmosphere and lead to a "sterile" documentary photo, without highlights or excitement.

Therefore, if your layout is still of a reasonably transportable size, simply move the layout out into the daylight. If you have sunlight without clouds, then almost nothing can go wrong. Even the natural shadows cast can have an excellent effect, because they look just as natural on your model as in real life, and not like those from studio lighting.

With stationary layouts or other conditions, however, you have to use an artificial light source. I often use the light floodlight mentioned above. Usually, I first test different positions to see how the shadows work out, and to find the best possible illumination of the object. Just give it a try.

And, there is one more thing of utmost importance: get down on your knees with your camera, in other words, adopt the point of view of one of the little Preiser figures on your layout. These photos are often exactly the mood setters in model railway magazines and they allow the viewer to immerse themselves into the action on the layout.

My generation still comes from the time of analogue photography. Back then a maximum of 36 photos per roll film was a tough constraint. I even learned the chemical process of developing negatives and slides. Back then, no one could predict how a photo would actually turn out. Everybody therefore considered very carefully beforehand whether the settings and results were correct... or could be correct!



Sometimes our kids are the ones who produce the better photos! Why? The young man simply took the shot, simply being guided by instinct and the sheer joy of doing so. His successful photo was also published in **Trainini®** at the time. At the same time, it shows that success can often be achieved with simple means, if the available tools are used skilfully.

With today's digital technology, results can be checked immediately, and countless series of photos will end up on the hard disk of our computers. So, just take a step back and consciously take the extra time for preparing one beautiful shot, or maybe two with alternative settings.

That should be enough for not losing track and helping you to enjoy that single particular scene. Ideally, these photos will later tell real little stories about your model railway.

continues on page 30



With this initial information, we just want to whet your appetite for more. Perhaps it is time to take another look at the operating manual for your own camera and prepare the stage for the stars of your model railway layout. Our editorial team is always very interested in your mood-setting photographs.



Our last photo is a reminder of how important a suitable backscene is for model railway photography: it creates depth and a realistic finish at the same time.

Photos on page 29:

While small details are perceptible in the photo taken with an SLR camera (photo above), the result with a compact camera is rather sobering due to the lack of manual adjustment possibilities (photo below).

Of course, we know that not every prospective buyer wants to bother with a large and richly equipped SLR camera. In this case, compact system cameras might make for a sensible alternative. As for mobile phones, we are planning to put them to a photo test soon.

In another article for late summer, we will have a look at the “Photoshop Elements” image editing software, again with the aim to provide you with simple but effective instructions. Similar functions are available through many photo editing programmes found on the Internet, some of which are even available for free.

Author's photo portfolio:
<http://helenensiel.com>



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A00206 - Workshop

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Vierbändiges Deutsches Lok-Archiv Klassiker der Dampflokomotiv-Literatur

Die vier bei Transpress jüngst wiederaufgelegten Baureihenverzeichnisse für Dampflokomotiven samt Beschreibungen und Zeichnungen haben eine fast fünfzigjährige Geschichte vorzuweisen, die über inzwischen drei Verlage reicht. Schon diese Eckdaten legen nahe, wie wichtig diese Werke für Eisenbahnfreunde sind. Sie gehören zu einer Reihe, die über viele Jahre letztendlich alle Fahrzeugkategorien behandelt hatte.



Manfred Weisbrod / Hans Müller / Wolfgang Petznick
Dampflokomotiven I - Baureihe 01 bis 39
aus der Reihe „Deutsches Lok-Archiv“

Transpress Verlag
Stuttgart 2021

Taschenbuch mit Klebebindung
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Stuttgart 2021

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ISBN 978-3-613-71654-4
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Erhältlich direkt ab Verlag
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Die beiden Hauptautoren Manfred Weisbrod und Wolfgang Petznik sind Experten für die Geschichte des deutschen Dampflokomotivbaus. Bekannt wurden sie durch verschiedene Veröffentlichungen, allen voran das Dampflokomotiv-Archiv, das unter verändertem Namen auch hier nun wieder Thema ist.

Die Geschichte der Autoren und ihrer Buchreihe begann in der früheren DDR bei Transpress VEB Verlag für Verkehrswesen in Berlin. Dort erschienen zwischen 1976 und 1981 die vier Ursprungsbände, deren Nachfahren wir heute hier vorstellen dürfen. Sie reihten sich zu Archiv-Bänden, die auch Diesel- und Elektrolokomotiven, Triebzüge und Wagenbauarten behandelten.

Schon beim Ursprungsverlag erlebten die Bücher mehrere Auflagen, wurden inhaltlich und in der Gestaltung überarbeitet, um dann im Westen auch eine lizenzierte Auflage beim Alba-Verlag zu erfahren, die den Fokus noch auf ausländische Lokomotiven erweiterte, einige Bücher weiter fortschrieb und schließlich auch noch um Stationierungsdaten erweiterte.

Nach der Wende gelangte der frühere volkseigene Betrieb Transpress aus Berlin dann unter seinem Handelsnamen bei den Paul-Pietsch-Verlagen, die heute in Stuttgart ansässig sind. Das fasst wie ein Zeitraffer kurz das lange Zeitfenster und die einzelnen Stationen nach.

So ist das Erscheinungsbild all dieser Bücher heute sehr vielfältig, denn es wurden im Laufe der Zeit die Buchdeckelgestaltungen mehrfach geändert, neue Aufteilungen vorgenommen sowie Nummerierungen durch alle Bände der nun als Deutsches Lok-Archiv bezeichneten Reihe eingeführt und auch wieder verändert.

Betrachten wir nun den Teil der Dampflokomotiv-Archivbücher, hat sich so viel im Laufe der Jahre gar nicht getan. Und das liegt schlicht daran, dass deren Geschichte nun mal weitgehend abgeschlossen war, als diese Werke erarbeitet wurden.

Uns liegen zum Vergleich die Ursprungsbände der Jahre 1976 bis 1981 und mehrere ausgewählte, strukturell vergleichbare Titel aus dem Alba- und heutigen Transpress-Verlag vor. Die vier vorliegenden und zu besprechenden Bücher sind als unveränderte Nachdrucke der Auflagen von 1993 bis 1995 gekennzeichnet. Teilweise hatten sie bis dahin Überarbeitungen und Erweiterungen erfahren.

Ihr Aufbau und Struktur folgen also unverändert der bewährten und etablierten Gestaltung, die sie bereits in den Achtzigern mit Beginn der Lizenzausgaben bei der Alba Publikation erhalten haben. Auch das durchweg historische Bildmaterial, dem sein Alter natürlich anzusehen ist, das aber gut und ausführlich dokumentiert und ansehnlich reproduziert wurde, ist unverändert weiterbenutzt worden.

Wertvoll sind die vielen Übersichtszeichnungen mit Maßangaben zu den Fahrzeugen, aus denen vor allem Umbauwillige oder Modellbahner mit Schieblehre in der Hand viele zusätzliche Informationen gewinnen. Die wichtigsten Fahrzeugdaten sind auch jeweils am Ende aller Bücher in Tabellenform aufgeführt.

Einleitend beschrieben ist das von der DRG eingeführte Nummernschema. Inhaltlich umfassen die Aufstellungen die Dampflokomotiven, die von der DRG und nach dem Krieg von DR und DB eingesetzt wurden. Dies schließt also nicht nur Einheitsbauarten, sondern auch Neu- und Umbauten sowie Rekonstruktionen nach 1945 ein. Ebenso werden auch übernommene Länderbahnveteranen und Versuchslokomotiven behandelt, die es teilweise nur als Einzelstücke gab.

In dieser Hinsicht sind die Bücher also sehr vollkommen, doch es bleibt mindestens eine Lücke. Sie fiel uns gleich auf, weil sie schon aus den geänderten Buchbezeichnungen zu erkennen war: Ursprünglich umfasste der Band 3 auch die Baureihe 60, später begann die Zählung erst bei der Baureihe 61.

Entfallen ist damit mindestens die von der Lübeck-Büchener Eisenbahn (LBE) im Jahre 1937 übernommene Stromlinien-Tenderdampflokomotive, die in Deutschland einmalig blieb und eben als Baureihe 60

von der Reichsbahn erfasst wurde. Dieses Manko hatten aber tatsächlich auch schon die Auflagen aus den Neunzigern, wie unsere Recherchen ergaben.

Wen das nicht schmerzt und nicht die alten Klassiker aus sozialistischen Zeiten sein eigen nennt, der wird froh und dankbar sein, dass Transpress nach knapp zwanzig Jahren endlich wieder eine Auflage gewagt hat.

Und so hegen wir die Hoffnung, dass nicht nur diese wertvollen Klassiker der Dampflokomotiv-Literatur wieder verfügbar geworden sind, sondern die gesamte Reihe auch durch die anderen Traktionsarten sowie Reisezug- und Güterwagen in Taschenbuchform alten Formats noch folgen wird.

Unterstützt durch technische Daten sowie einer stark unterschiedlichen Anzahl Fotografien und Skizzen je nach Baureihe, werden die Dampflokomotiven hier schließlich bezüglich Entwicklungsgeschichte, Betriebsbewährung und konstruktiven Merkmalen in einem Umfang vorgestellt, wie es die kompaktere Typenkompass-Reihe aus gleichem Verlag nun mal nicht leisten kann.

Werfen wir abschließend noch kurz einen Blick auf die Aufteilung der Baureihen auf die vier Bände, wie sie unverändert gegenüber der letzten Auflage beibehalten wurde:

- Band 1 beschreibt alle Schnellzug- und Personenzuglokomotiven mit Schlepptender (Baureihen 01 bis 39).
- Band 2 behandelt alle Güterzuglokomotiven mit Schlepptender (Baureihen 41 bis 59).
- Band 3 widmet sich den normalspurigen Tenderlokomotiven mit Ausnahme der Baureihe 60 (Baureihen 61 bis 98).
- Band 4 ergänzt die Reihe um die vielen Schmalspurlokomotiv-Bauarten, die in einer einzigen Reihe zusammengefasst wurden (Baureihe 99).

Da alle vier Bände eng zusammengehören, sich ergänzen und nur gemeinsam Sinn machen, haben wir uns letztendlich auch entschlossen, sie gemeinsam in einer einzigen Rezension zu behandeln. Die Bücher sind ein Muss, quasi ein Standardwerk, und wir können uns kaum vorstellen, dass jemand nur zu einem oder zwei Titel dieser Reihe greifen wird.

Ganz im Gegenteil müssen wir attestieren, dass sie Lust auf mehr machen und den Wunsch wecken, gleich einer Enzyklopädie auch die noch vermissten, weiteren Werke hinzugesellen zu können!

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: **Publishing pages with reference possibility:** :
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Von ersten Versuchen zum technischen Standard 100 Jahre Entwicklung zum Drehstrom

Heute fährt jede moderne Ellok mit Drehstrom-Asynchronmotoren, zieht ihre Energie aber aus einer Fahrleitung, deren Versorgung sich seit hundert Jahren nicht mehr nennenswert geändert hat. 1979 war es eine Sensation, als die DB-Baureihe 120 als erste Drehstromlok bei einer Staatsbahn in Betrieb ging. Doch die Geschichte reichte viel weiter zurück, erlebte viele Versuche und Entwicklungen, aber auch schon Serienmaschinen im täglichen Einsatz, bevor ein neuer technischer Standard erreicht war.



CFT Video Berlin
Die Geschichte des Drehstromantriebs
Mit der Drehstromtechnik in die Zukunft

EK-Verlag GmbH
Freiburg 2021

DVD-Video
Bildformat 16:9
Tonformat Dolby-Digital 2.0
Sprache deutsch
Laufzeit ca. 58 Min.

Best.-Nr. 8492
Preis 19,80 EUR (Deutschland)

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Einen spannenden Film hat der EK-Verlag hier auf den Markt gebracht. 120 Jahre Eisenbahngeschichte weiß er einzufangen und anschaulich zum Zuschauer zu transportieren. Sein Thema ist der Drehstromantrieb in elektrischen Lokomotiven.

Was heute Stand der Technik und so selbstverständlich ist, hat die Elektroingenieure mehr als ein Jahrhundert lang beschäftigt. Die Vorteile dieser Technik, deutlich kleinere Motoren und spürbare Gewichtseinsparungen gegenüber Ein-Phasen-Wechselstrommotoren gleicher Leistungsfähigkeit, war auch damals längst bekannt.

Doch die Entwickler jener Zeit standen vor der für lange Zeit noch unlösbaren Aufgabe, wie dieser Drei-Phasen-Wechselstrom überhaupt zu den Motoren gelangt. Ab 1901 erfolgten bei Berlin auf der Militärbahn Zossen – Marienfelde Versuche, die die Vorteile dieser Technik klar bestätigten und ermutigten, weiter in diese Richtung zu forschen.

Waren die Ergebnisse vielversprechend, bereitete die seitlich zu montierende, dreipolige Oberleitung den Entwicklern Sorgenfalten: Zwar konnte sie den Strom den Triebwagen sicher zuführen, aber mit ihr ließen sich keine Weichen und komplexe Fahrwege überspannen. So steckte diese Technologie bei der Eisenbahn lange Zeit in einer Sackgasse.

Die Produzenten des Films wissen auch diese erste Episode mit interessanten Bildern zu dokumentieren und zu veranschaulichen. Sie bedienen sich einiger Interviewsequenzen, die sie mit dem leider viel zu früh gestorbenen Dr. Alfred Gottwaldt, damals Leiter der Abteilung Schienenverkehr im Deutschen Technik-Museum Berlin, gedreht haben.

Er erläuterte die damaligen Versuche und Entwicklungen anhand von Gedenktafeln an der früheren Strecke, historischen Aufnahmen und einem ungewöhnlichen Lok-Exponat seines Museums. Dieser Weg für das älteste Kapitel, das es zu behandeln galt, war klug gewählt, denn es handelt sich um eine Ära, in der der Film erst noch das Laufen lernen sollte.

Aber auch der weitere Verlauf ist spannend, denn keineswegs folgt danach ein größerer Zeitsprung um sechzig Jahre. Der Blick geht über die Grenzen Deutschlands hinaus und schwenkt zum italienischen Drehstromnetz, das mit zwei Oberleitungskabeln auskam, über der Lok aufgehängt war und eine Lösung für Weichen und auch die Stromabnehmer fand.

Deutlich wird spätestens jetzt, dass diese Technologie kein rein deutsches Forschungsgebiet war, sondern auch Techniker im Ausland gleichermaßen herausforderte. Seinen Durchbruch feierte es aber in der Tat in deutschen Landen.

Und damit kommen wir der Gegenwart schon näher und mit ihr auch dem Anteil bewegter Farbbilder. Die drei Henschel-Lokomotiven DE 2500 sind zu nennen, die als Diesellokomotiven per Generator ihren Drehstrom erzeugten und damit elektrische Fahrmotoren speisten. Sie sind den Eisenbahnfreunden auch heute noch bestens bekannt.

Eine dieser Maschinen, die ebenfalls mit Messwagen mehrfach im Film zu sehen ist, blieb ebenfalls im Technikmuseum erhalten und wird daher auch ausführlich vorgestellt und als Meilenstein der Entwicklungen ins rechte Licht gerückt. Es handelt sich um die 202 003-0.

Gegenüber ihrer Schwester 202 002-2, die in Kassel im Freien ausgestellt, nur kurz gestreift wird, hatte sie auch entscheidenden Anteil an der Entwicklung der ICE-Drehgestelle mit umkoppelbarer Antriebsmasse („UmAn“). Ihr umgebauter Stromlinienführerstand ist ein „Charakterkopf“ und deshalb in lebhafter Erinnerung geblieben.

Wir sind nun in einem Zeitfenster angelangt, das besonders auch die Freunde der modernen Bahn ansprechen wird: Hier wird die Entwicklung und Bedeutung der Baureihe 120 gewürdigt, die mit fünf Vorserien- und 60 Serienmaschinen auf die Schienen kam oder auch der Bau des ICE als erstes greifbares Ergebnis der überaus schnellen Halbleitertechnikentwicklung.

Nicht vergessen werden durfte auch die erste in Serie gebaute Drehstromlok, die in Diensten der RAG noch vor der Baureihe 120 das Licht der Welt erblickte. Der Weg bis zu den heutigen Fahrzeugen wie etwa dem Vectron von Siemens scheint kurz, bietet aber auch noch spannende Wege.

Da sind Prototypen wie der Eurosprinter (Baureihe 127) oder die 12X (Baureihe 128), deren X ein Platzhalter für eine dritte Ziffer war. Deutlich wird daran, dass sie als mögliche Nachfolgerin der Baureihe 120 ins Rennen gehen sollte, als absehbar war, dass es bei den 60 Exemplaren bleiben würde, weil ihre Technik schon wieder überholt war.

Doch es kam die Bahnreform, der Traum von einer Mehrzwecklok war mit der Spartenaufteilung schon wieder vorüber. Wenigstens die Grundzüge ihres Kastens und der Kopfform gab sie jedoch an die TRAXX-Familie weiter. Der Siemens Eurosprinter führte seinerseits zu den Taurus-Lokomotiven, die ihre Spuren schließlich im Vectron hinterließen.

In Summe ist dies ein äußerst spannender Geschichtsunterricht. An der Qualität von Bild und Ton gibt es ebenfalls nichts zu kritisieren. Und so kann dieser Film sicher vielen Modellbahnfreunden die Wartezeit bis zum Ausliefern des genannten Siemens-Ellokmodells erheblich verkürzen und versüßen. Bis das Modell dann eintrifft, weiß sein Käufer schon ums Vorbild gut Bescheid!

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: <http://www.eisenbahn-kurier.de> :
: <http://www.ekshop.de> :
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Staying in the conversation (part 3)

HOS Modellbahntechnik: To live for Model Making

Since 2013, Heinz O. Schramm has been active in Z gauge under his company name HOS Modellbahntechnik. With his fine etched constructions and UV-printed signs, he has since made a name for himself (not only) among the Zetties. Today he introduces himself and his work and offers a look behind the scenes.

By Heinz O. Schramm. How it all started: I was infected with the model railway virus more than 60 years ago. My father built a small H0 gauge layout for his son, i.e., for me. At the tender age of five, I was already allowed to hook up little houses, and other soldering work under the layout was also the order of the day, with a huge "roaster" with a copper hammer head. Well, there was nothing else...

After my father passed away at a very early age, there was no more railway modelling for a long time. In the seventies, I started ship modelling, or more precisely, the functional modelling of model ships. At that time, only simple analogue controls were offered, often with a lot of malfunctions.



Small series manufacturer Heinz O. Schramm evidently enjoys working in his small workshop. Photo: HOS Modellbahntechnik

I was able to win numerous regattas and championships until I had to bid farewell to this hobby: A move to the Swabian Alps deprived me of all the lakes that were available in the Rhein (Rhine) valley. There are practically no standing waters on the Alb, all water immediately goes into the karst, and flowing waters are also scarce.

continues on page 39



On the company's fifth anniversary, Heinz O. Schramm "treated" himself to an ultimately successful world record attempt at the Z-gauge weekend in Altenbeken in 2018: He built the world's smallest functioning brake shoe – the world's best performance thus moved from the large 1 gauge (scale 1:32) directly to Z gauge (scale 1:220). Photo: HOS Modellbahntechnik

So, there were only two options remaining: Model aircraft or model railways. And here fate played along once again: A colleague at work gave me a “broken” crocodile in Z gauge. After a few hours of tinkering, it was running again, and, all of a sudden, there were two happy Z gauge railroaders. I recognised the subtlety and diminutiveness of this gauge, because it fitted exactly into my small living environment!

The end of my working life announced itself around 2010; I have neither grandchildren nor a dog. So what is such a pensioner to do? Having been a builder myself, I found that the range of accessories was very, very manageable. And the quality only...



In the beginning is a drawing: Heinz O. Schramm sits at his new workplace after retiring. Photo: HOS Modellbahntechnik

The decision to found a small company therefore quickly matured: HOS Modellbahntechnik saw the light of day. As an “old metalworker”, metals of all kinds were to be my materials, of choice.

I quickly recognised the advantages of nickel silver and brass in various alloys. My first work was a small water crane on a scale of 1:220, from which many other parts for the railway depot developed.

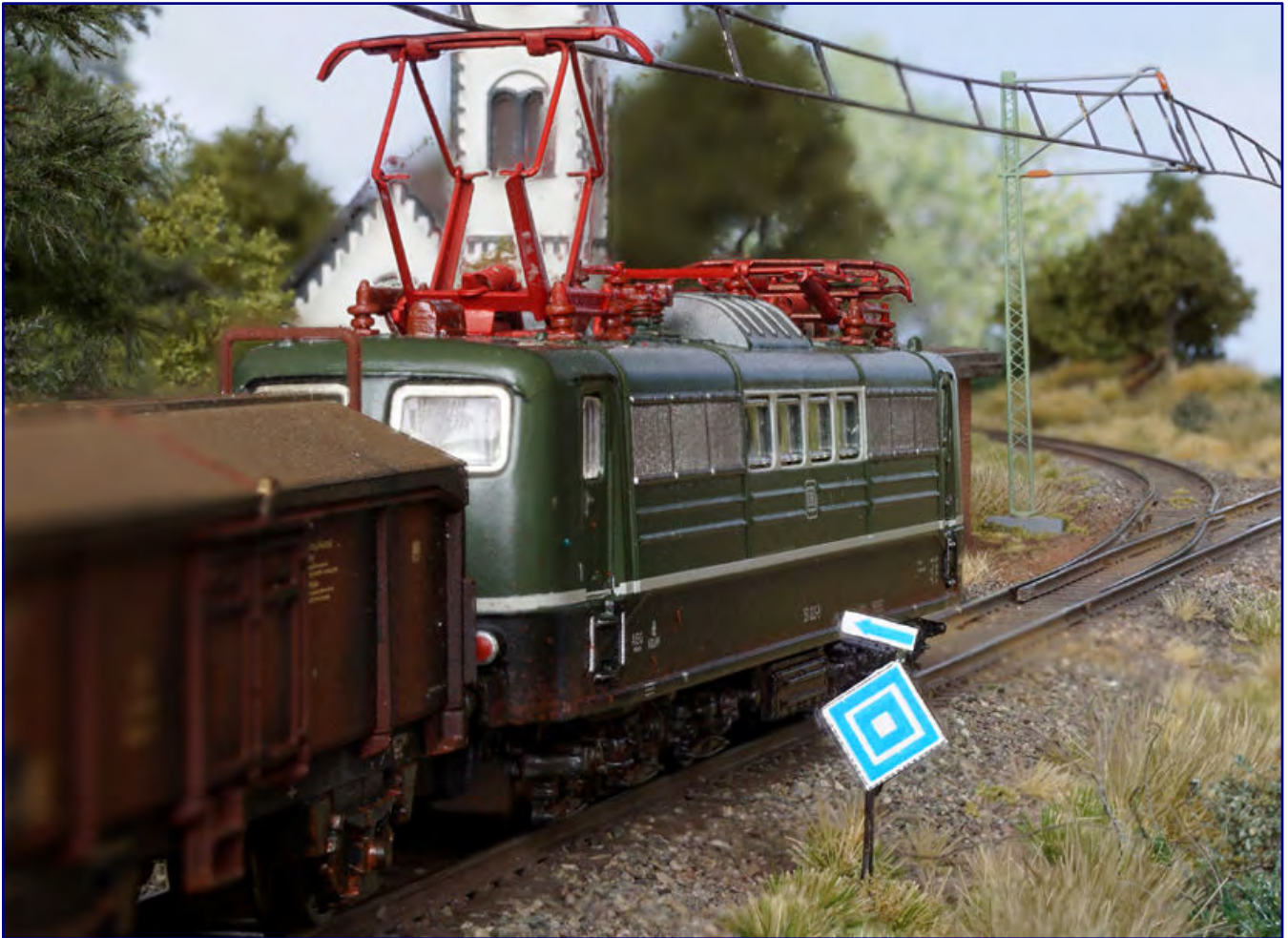
There were enough models, after all I had access to the old train yard Dieringhausen near Gummersbach: As a member of the Stammtisch (model railway club) Untereschbach e.V. many visits followed with pad, pencil, yardstick, and camera. It doesn't get more authentic than that.

My little workshop

Fortunately, I'm in Z-gauge, so a small 20m² workshop is enough for all jobs. Besides several lathes and milling machines, I am particularly proud of my camera soldering station (photo below).



Heinz O. Schramm at milling work for his models (picture above), to his left you can also see his milling station. He is particularly proud of his camera soldering station (photo below) for the smallest parts, which allows 48x magnification. Photos: HOS Modellbahntechnik



The product range, which is produced in the company's own workshop, also includes an overhead catenary system and UV prints. In addition to the code charts shown to indicate the end of the overhead contact line on the diverging branch of the line, there are also information and traffic signs, which are then mounted on thin nickel-silver panels.

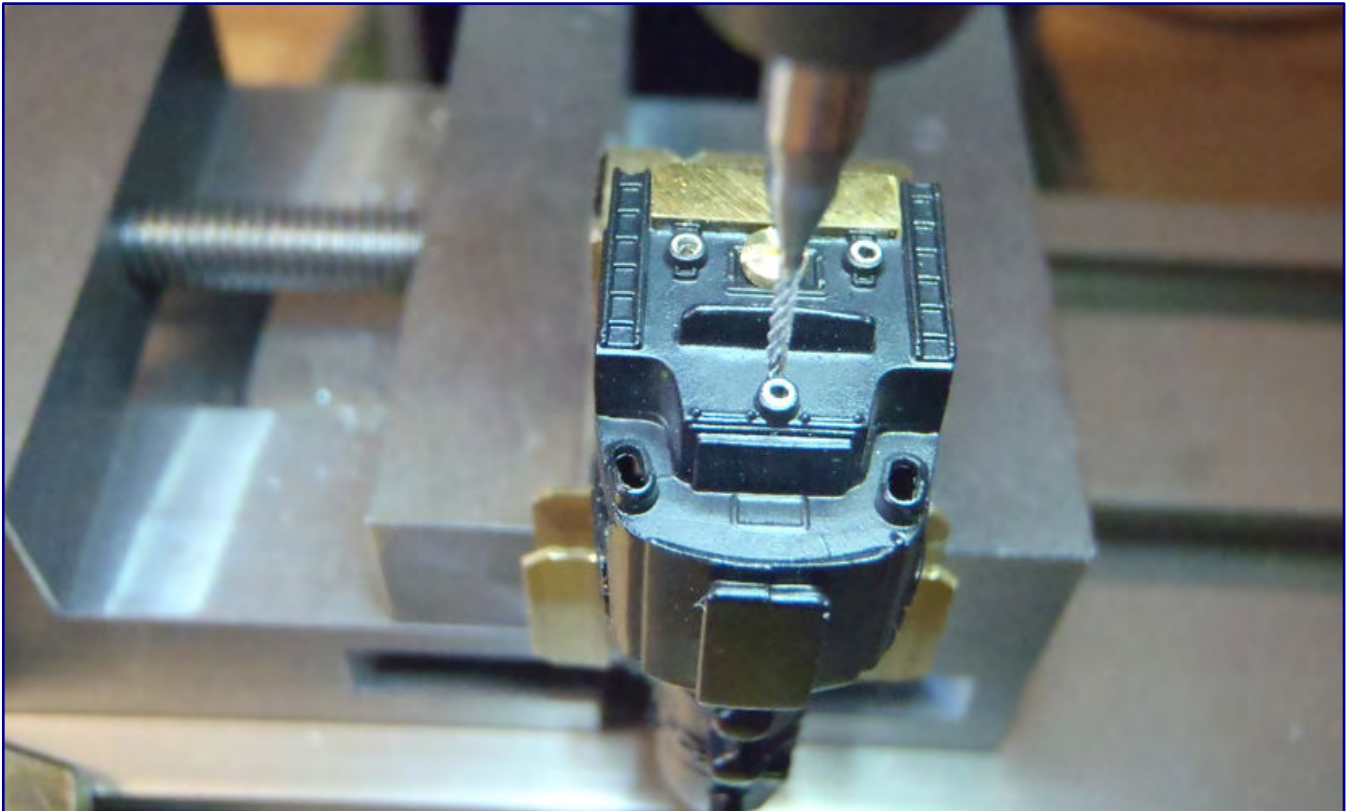
After all, my initial experiments with a microscope were all unsatisfactory. This is because only a depth of focus of a few tenths of a millimetre is available beneath the optics. With a camera and screen, on the other hand, a magnification of up to 48 times the operating range can be achieved – enough for the tiniest components, such as SMD LED 0201, and matching stranded wires with a diameter of 0.05 mm.

Such small components are difficult to solder with conventional soldering equipment, so I made all the nickel silver soldering tips for the Ersal Icon soldering device myself. The camera system is also useful for working with complicated nickel silver parts, thanks to sufficient depth of focus.

There was only one challenge I had to overcome. The hand is working somewhere and the eye sees the result somewhere else, and the poor brain has to bring the two together. Not so easy! From that moment on, I have had even more respect for surgeons!

My product range

My product range expanded steadily, always keeping an eye on what my colleagues in small series production were offering. It makes no sense to cannibalise ourselves in this small market. I specialised



Heinz O. Schramm's range of services also includes individual customer orders, such as here the installation of a three-light tip signal using LED technology in a Märklin class 89. Photo: HOS Modellbahntechnik

more and more in etching work in nickel silver, and more and more at the request of customers. The finest structures I can work on measure just 1/100 mm! That is fine enough for the Z gauge.

In the model railway world, signal boards and road signs in high quality are rather rare, but unfortunately also in the larger gauges. Following from a standard assortment, I offer all kinds of individual boards and signs.

Always printed with UV printing technology, then mounted on the finest nickel silver plates. Unfortunately, trials with laser printers did not provide satisfactory results, the resolutions are simply not fine enough for our small gauge. Due to the techniques, I chose, very realistic signs are possible – with all possible (or impossible) inscriptions.

But even prior to the small etched part, there is a drawing. I create all the drawings myself on my home computer. The graphics programme Corel Draw has the right tools for this. The finished drawing file is sent electronically to the specialist who exposes the films for sheet metal production. The films are then used to make film pockets that show the front and back of the sheet metal.

The photo positively coated metal sheets are exposed in these film pockets and etched in an acid bath after development and fixing. After a few weeks, I receive the finished plates from the etching workshop for further processing.

These are now used to create the fine objects for the train yard, e.g., sanding towers, slag lifts, examination pits, or pipe-blowing frames. Other parts “around the train yard” also originate here, for example, barbecue and drinks trolleys, apron light poles, coal and slag trolleys, luggage trailers for the platform, etc.

continues on page 44

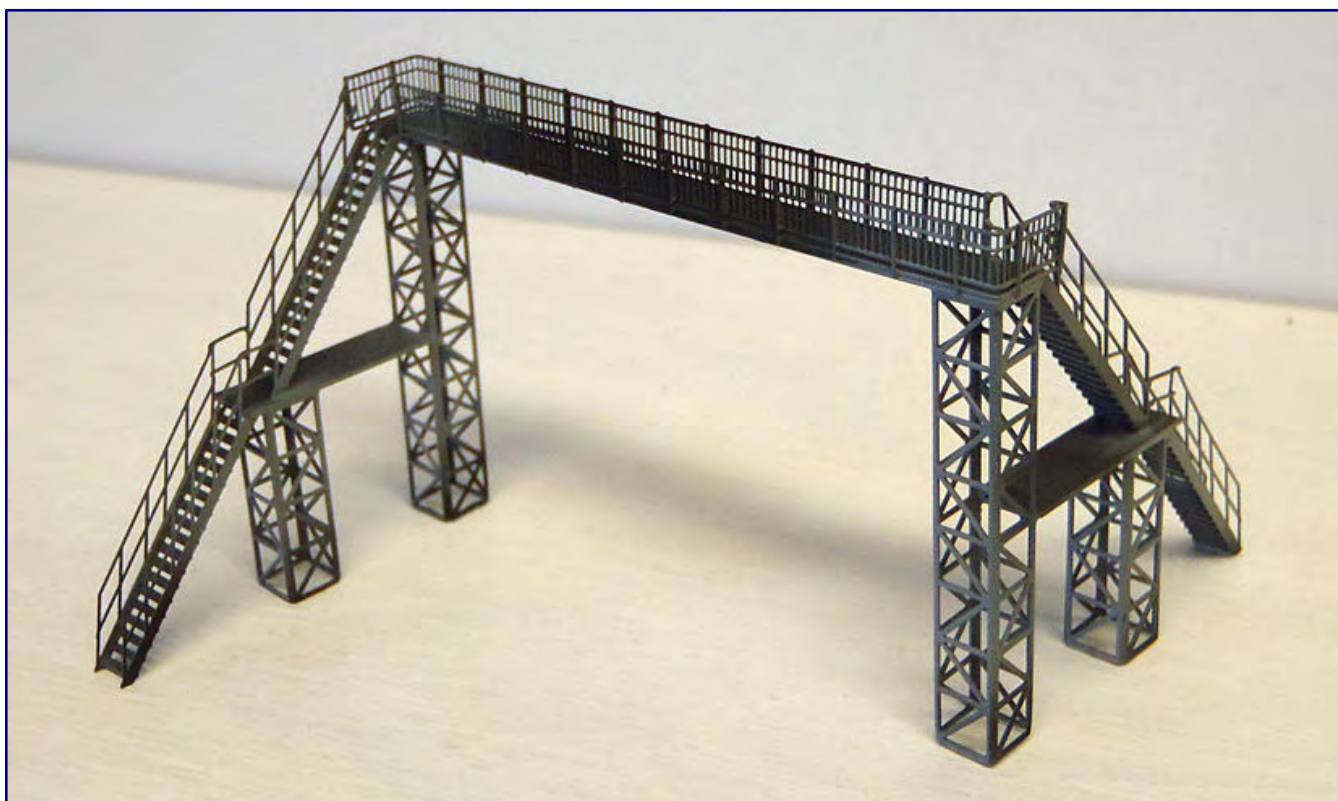


Photo above:
A special eye-catcher on Klaus Sieber's layout is the covered train station platform with LED lighting constructed by HOS Modellbahntechnik, which spans six tracks, and offers full lettering inside. Photo: Klaus Sieber

Photo below:
Also made to customer specifications, this elevated track crossing is made of nickel silver and even features chequered (ribbed) plate on the stairs. Photo: HOS Modellbahntechnik



Prominent showpiece: The signals, the light poles, the light bridge in front of the tunnel and the wire mesh fences on the AZL demonstration segment "Moffat Tunnel" also come from the workshop of HOS Modellbahntechnik. Photo: Karl Sinn (Spur-Z-Atelier)

My latest creation is a Bunker C (really thick, heavy oil) refuelling system for the oil-fired steam locomotives of the late fifties.

New projects

Last year I developed my own product line of catenary wires. Finest catenary wires in different lengths for the HOS catenary masts, but, also suitable types for the masts from Göppingen – as a replacement for the coarse and no longer up-to-date stamped parts. In addition, I offer the catenary wires also in a dark stained finish. **Trainini®** already demonstrated last year with its Himmelreich project, how they can be used effectively.

Matching the catenary components, there is also the complete assortment of catenary code charts: Pantograph up, pantograph down, and so on. They are a small but fine detail on the model railway layouts, that will not fail to make an impact.

A particularly difficult project is currently ahead of me: Functional semaphores for Z gauge. A variant, as a non-functional version, has long been ready for series production. The use of the smallest LEDs will result in a functional version, as well as a version of pre-signals. They will be operated by micro-servo motors.



The fine and contemporary catenary wires for the Märklin system geometry (photo above) belong to the more recent designs of the small series manufacturer. A preview of the ongoing developments is offered by the signal cantilever with two colour light exit signals and wing signals marked as invalid (photo below). Photo (below): Henry Christophori

It is often the wishes of my customers and their joy in the small details that motivate me again and again to try out new things, and make the seemingly impossible possible. I would like to thank everyone for this and look forward to further motivation!

Contact this manufacturer:
<http://www.hos-modellbahntechnik.de>



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Art. 702181

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Pleasant and gratifying feedback:

I was looking through your new issue and thought to myself: "Oops, you know that photo..." And then I landed in your journal with a letter to the editor and a photo – how nice!

Great, thank you very much, I am very happy! I also take it as motivation for future projects. I will be happy to report back to you once things have reached "presentable" dimensions.

In the meantime, I have registered in the forum "Z-Freunde international," what a source of knowledge, exchange and inspiration, I am reading my way through it. Speaking of reading, there is a lot to "absorb" in **Trainini®** 4/2021.

For me, the article about the work of the Heller couple in 3D printing is particularly interesting. A technology that also inspires me, even if for me as a layman most of it remains on the surface.

In my opinion, it is one of the technologies of the future (also for the broad hobby user community). I especially like the article about the V 23, the split pin solution for coupling seems to be very practical.

I still have a lot to read in your magazine, thank you very much for another very successful issue.

Michael Böttcher, Bremen

Editor's reply: Thank you very much for this confirmation and helpful message. Comments like yours always help us to consistently align the magazine with the interests and expectations of our readers. And that is our incentive!

Spelling of Russian names:

(You) have made a great effort again. But you still have to learn something about the spelling of Russian names!



The aircraft named after Russian designers are the Yak (Yakovlev) and IL (Ilyushin). I know that both names are spelled with a “Y” in the Anglo-American language. Not so in German-speaking countries!

That hurts somehow! By the way, this can also be researched very well on the internet.

Gunnar Häberer, Potsdam

Editor's reply: We have followed the spelling that Herpa has chosen in its product texts in order not to make it difficult to find them in case of interest. In fact, several spellings are possible and also common, which is why we also confronted the manufacturer with the content of this letter to the editor. They told us that they too had to decide on a spelling, which then had to be applied consistently. We would also like to add that Herpa also exports to the important Anglo-American language area, which certainly had a significant influence on the company's decision.

Making heavy-duty trucks more presentable:

Many thanks for the detailed report on the SSy 45 from Märklin. I was also very happy about the delivery of these long-lost wagons.

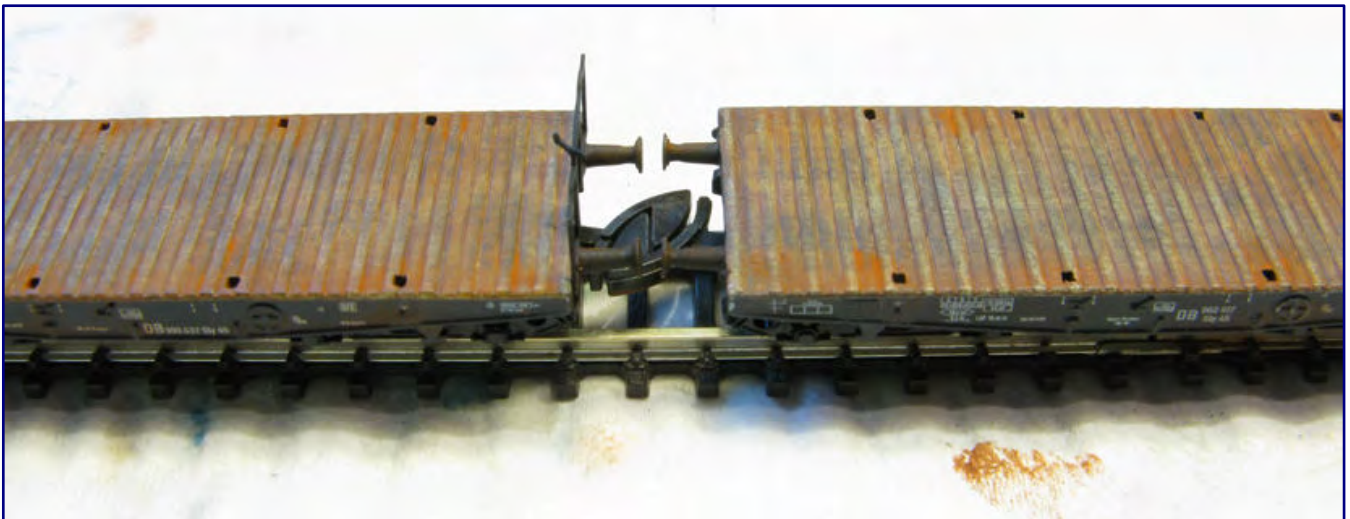
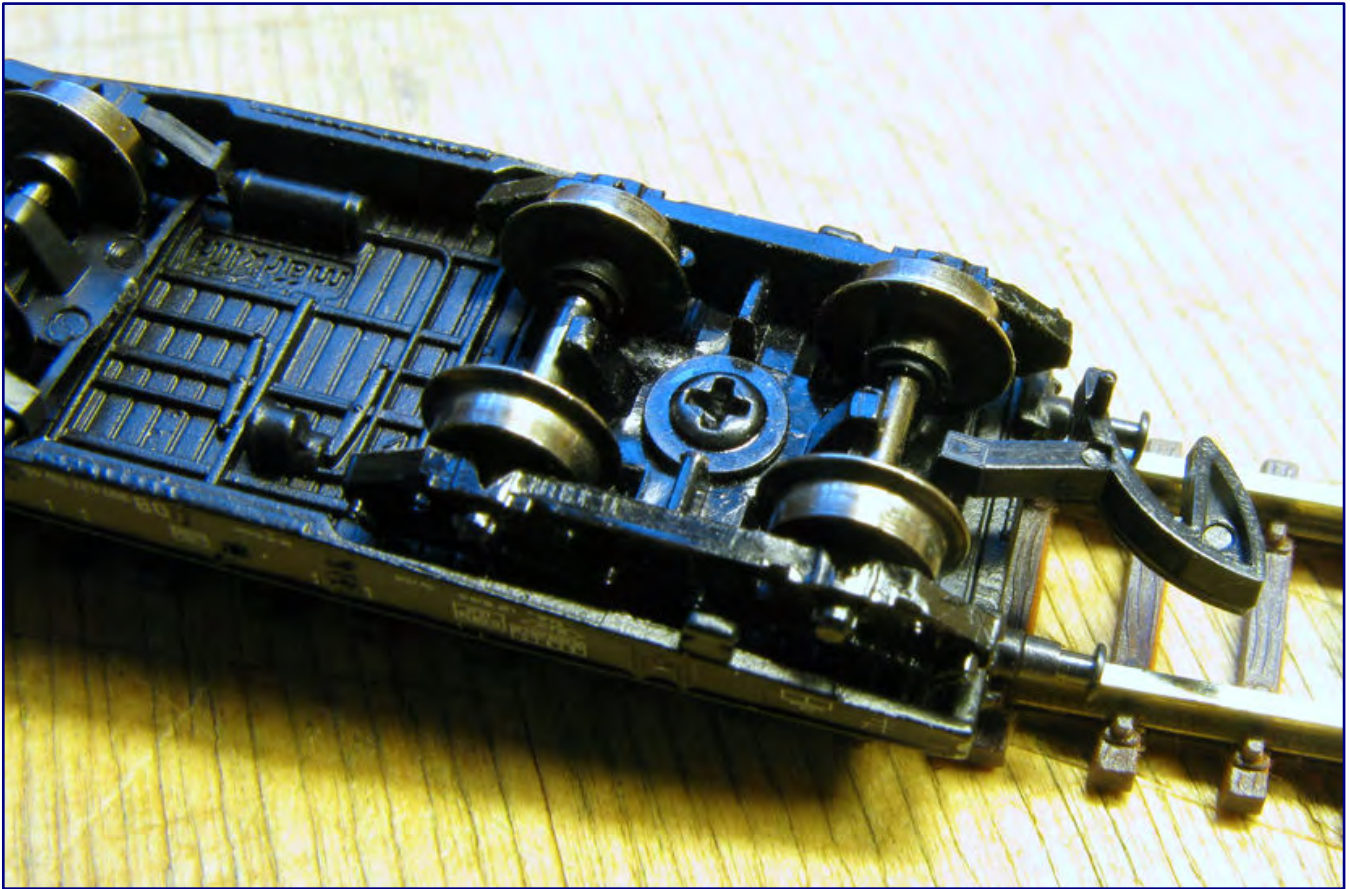
Unfortunately, with the wagons came disillusionment when they are coupled together. This “huge gap” is no longer in keeping with the times and greatly diminishes the joy of these wagons. After looking at the construction, I thought about how this could be improved. As an idea, I took a MTL coupling 907.

For the conversion, however, it must be taken into account that a return to the original is no longer possible.



After the dome spacing was felt to be unsatisfactory and not up to date, a hand had to be applied. The three heavy goods wagons from the Märklin pack 82229 now show themselves modified and also slightly aged. You can see more photos of the modification on the next page. Photo: Dirk Rohwerder

The mounting of the new MTL 907 coupling is too high, so it must be cut flatter. In the bogie everything up to the middle of the axle must be cut away so that the coupling has sufficient free space.



With the help of a MTL coupler 907, whose receptacle had to be cut flatter to fit under the also modified bogie (photo above), a closer coupler spacing was achieved. The result (photo below) justifies the effort. Photos: Dirk Rohwerder

The buffers were also replaced with Heckl buffers. The wagons were then aged. Attached are a few photos of the reworked SSy 45.

Dirk Rohwerder, Sprockhövel

Later start for our new internet pages:

In the last issue we provided more detailed information on the status of the new pages and their content for the first time. Unfortunately, we were not able to keep to the planned date for the changeover. Although

the pages of individuals and clubs have been integrated in the meantime and, above all, the licensed **Trainini Archives**, there were still some open questions to be clarified.

This concerned above all the handling of the imprint and privacy policy for our English-language pages, which under German law belong to the mandatory information and must therefore be targeted accurately, and, exclusively, to our laws and legal standards.

Now that these questions have been clarified, we are facing a new problem exactly at the time of publication of this issue: the change of direction already initiated the day before has so far not worked, and we are waiting for technical help from our service provider. We sincerely hope that you will soon be able to enjoy our “refreshed” appearance, both visually and in terms of content, because it will also change the paths to the respective issues.

The editors and translators would like to take this opportunity to thank Stephan Bauer, who is responsible for the technical part of the website, has set it up and will continue to look after it for us. We would like to take this opportunity to say “Welcome to the **Trainini®** team!”

Only a single Märklin delivery:

At the last turn of the month Märklin delivered the container car pack (item no. 82663) from the autumn new products 2020. Contrary to the product description, this is a set of three freight cars from the late German Federal Railroad (Era V).

Part of the three-part wagon composition are a four-axle container wagon Sgs 693, loaded with three 20-foot containers “Hamburg Süd,” and two two-axle Lgjs 598, each carrying two tank containers of the company Bertschi AG.



With the container car pack now delivered (item no. 82663), more modern Bundesbahn trains can be usefully extended and supplemented.

With this new product Märklin enhances modern freight traffic on Z gauge layouts, as it has developed more and more from the seventies to today's dominant form of transporting freight between seaports and inland.

New self-publishing house by Stefan Carstens:

Stefan Carstens, best known as a profound expert on freight wagons through his specialist book series, has announced that he will in future publish his books himself again. For this purpose, he will operate under the name Eisenbahn-Dokumentation (<https://www.stefancarstens.de>), with parts of words merging into “Bahndoku” in his logo.

Already in 1996, after the loss of the first publishing house he had worked with, he had self-published volume 3 of the freight car series. This was followed by a 20-year collaboration with Miba or Verlagsgruppe Bahn (VGB), during which he was able to concentrate on research, making drawings and publishing books.

EISENBAHN
DOKUMENTATION
STEFAN CARSTENS

“Unfortunately, due to the change of ownership, the fun factor fell by the wayside”, Stefan Carstens now writes on his new pages. This probably led to the decision that has now been announced. The next upcoming titles affected by this will be “Freight Wagons Volume 9.1” (planned publication in July 2021) and “Freight Wagons Volume 9.2” (autumn 2021).

In addition to two other partners, he was able to win the Lokomotive-Fachbuchhandlung (<https://www.lokomotive-fachbuchhandlung.de>) and the Modellbahnunion (<https://www.modellbahnunion.com>) as distribution partners. The latter has bought the largest part of the edition and is therefore also mentioned on the cover of the book.

Plans for the near future can also be seen in the new appearance: For example, the out-of-print volume 2 is to be revised and divided into two follow-up volumes 2.1 (refrigerated and beer wagons) and 2.2 (wagons with openable roofs and sliding wall wagons). The announced book on low-loader wagons will also be pursued.



In a new series of books, railway service vehicles and service freight wagons are to be presented in a well-founded manner by the author Wolfgang Henn.

Artitec new product delivered:

The mobile cattle ramp (art. no. 322.037), which is finely designed as usual and highly topical, was delivered by Artitec, and also completes the recently released small livestock loading wagons in scenes at loading areas.



The mobile livestock ramp (art. no. 322.037) from Artitec has just been delivered, over which livestock begin their final journey to the abattoir (slaughter house).

The Krupp-Dolberg excavator (322.025), which we awarded last year as the best new release of the year 2021 in the accessories category, has in the meantime become available again, but is already sold out ex-works. However, interested customers can still purchase one from many dealers at the moment.



The Krupp-Dolberg excavator (322.025), which is still in great demand, is not only a particularly successful model, but also a typical representative of the earlier cable excavators.

Decoder development with D&H:

Case-Hobbies (<http://case-hobbies.de>) announced that they are currently developing new decoders together with D&H. These are intended, for example, for AZL models Alco RS / RSD and the announced SW1500 shunting locomotives. These are intended, for example, for the AZL models of the Alco RS / RSD, and for the announced SW1500 shunting locomotives. But other decoder developments are also already in the works.

The latest new products from AZL:

The EMD E7, which was recently delivered for the first time, is a colourful package of A and B units. It now wears the colour scheme of the Seaboard Air Line (art. no. 64611-1 / -2). The version of the PRR (64604-1 / -2) in the reddish-brown shade known as "Tuscan red" also stands out with its roof antennas.



EMD E7 A-B (Item No. 64611-1; photo left) in Seaboard Airline colours and reissued A-B-B-A unit of Santa Fe's EMD F7 (63001-1; photo right). Photos: AZL / Ztrack

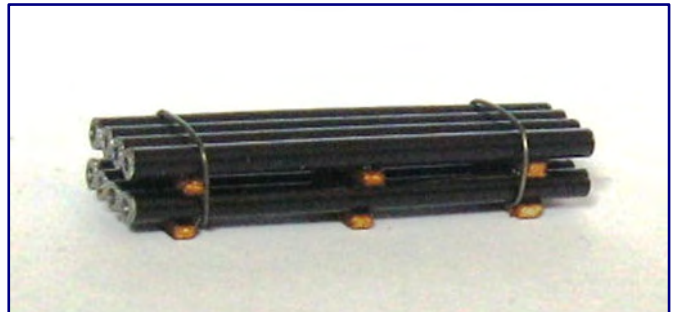
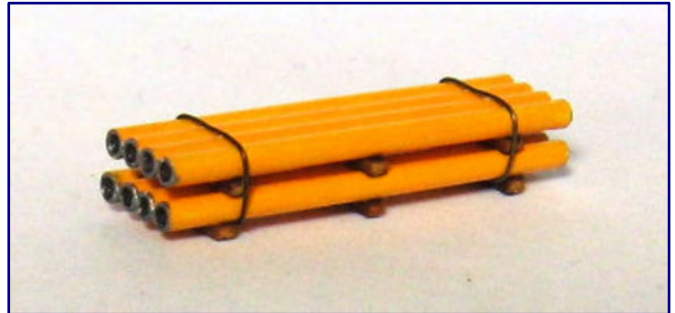
Due to the great demand, new A-B-B-A assemblies of the EMD F7 in the design of the Santa Fe (Warbonnet livery) will be added. The new service numbers also match the Super Chief train (63001-5 / -7 / -9), or are small follow-up editions of the models already delivered (63001-1 / -3).

The 8,000-gallon tank cars of 1917 now come in the "Warren" design individually (915002-1), in pairs, (915032-1), and, in fours, (905002-1).

Manufacturer photos of the current deliveries can be found at <https://www.americanzline.com>.

Load idea from Küpper:

Basalt chippings made of original material are currently offered by Spur Z Ladegut Josephine Küpper for Märklin models 8622 (item no. Z-1-4) and 8650 (Z-11-4). For the 8610 low side car or also the comparably long, but more realistically designed Märklin models of the R 10 and Rmms 33, there are new pipe loads.



Four new pipe loads are now available from Josephine Küpper: Yellow pipes are available in bundles of six (item no. Z-301; photo, above left) as well as bundles of eight (Z-302; photo, above right). Eight bundles each are tied down and secured with timbers. Blue (Z-303; photo, below left) and black bundles (Z-304; photo, below right) are also available. Photo: Spur Z Ladegut J. Küpper

These are available with yellow pipes as a bundle of six (Z-301), as well as a bundle of eight in the colours yellow (Z-302), blue (Z-303) and black (Z-304). These new products are made extremely precisely by hand. They do not put a heavy load on the wagons as they only weigh around 0.8 grams each.

All the above-mentioned loads and many more can be obtained from <https://spur-z-ladegut.de>.

Generation change at Simba-Dickie:

The group of companies, which was founded in 1982 as “Simba Toys” and expanded into the international Simba Dickie Group, is now entering its third generation. On 1 May 2021, Michael Sieber (65 years old) handed over responsibility for 2,950 employees and a turnover of EUR 715 million to his son Florian Sieber (34 years old).

Following the acquisition of Märklin, Florian Sieber has been its managing partner since 2013, and in 2018, also joined the management of the family business as co-chief executive alongside his father. At the end of the financial year, his father retired and handed over responsibility in full.

The baton was handed over with the following words from the outgoing Chairman: “Together with a committed team in Göppingen, my son Florian has put Märklin back on track – his, I am very proud of him. (...) I trust that with his knowledge and the perspectives masterpiece of a new generation he will successfully lead our family business into the future.”



Passing the baton: Michael Sieber (right) has now passed the management of the family business Simba-Dickie completely into the hands of his son Florian Sieber (left). Photo: Simba-Dickie / Lisa Gimpel

An excellently trained Florian Sieber, well prepared for his tasks, returned this vote of confidence with a fitting statement: "I have great respect for the new responsibility I am assuming for our family business, but I am also looking forward to the new challenges and tasks."

Whether and which changes or additions might result from this for the future management at Märklin, we did not find out yet.

New items from NoBa-Modelle:

New in the NoBa-Modelle range (<https://www.noba-modelle.de>) is the heavy Claas Tucano 560 combine harvester as a resin blank (art. no. 6619R), or as a painted finished model (6619RF).

The miniature model of the vehicle, which is intended for harvesting large fields, also consists of the combine harvester, a removable cutter bar, and a trailer on which the cutter bar is placed during transport across the road because, otherwise, the vehicle would be too wide.



The Claas Tucano 560 combine harvester is also available as a ready-made model (art. no. 6619RF). Photo: NoBa-Modelle.

Archistories- Windmill delivered:

Some have been waiting for this, as we know: Archistories has delivered its windmill 'Am Geestenvveen' (art. no. 702 181), a gallery Dutchman that was also once widely used in Germany. We described the kit in detail in the February issue.

It consists of solid-coloured hard cardboard parts and does not need any further colour treatment.

The manufacturer has also attached great importance to finding a suitable drive for the mill, which fits into the canopy and moves the wings (blades) at a speed that is true to the model.

A big challenge was the reproduction of the cap with its typical shape, which also shows curves and cannot be reproduced correctly with laser-cut parts. This problem was solved with a 3D printed part supplied by Z-Doktor Modellbau.

The excellent kit, which we would like to present in more detail in a craft article, is available directly from the manufacturer (<https://www.archistories-shop.com>), or from its distribution partners.



The new windmill 'Am Geestenvveen' (art. no. 702181) by Archistories is a jewel. Photo: Archistories

Own-brand new products of the Modellbahnunion:

Modellbahnunion informed us about a small new product for Z gauge. The small steel girder bridge with abutment (item no. MU-Z-L00133) as a laser-cut hard cardboard kit from their own production effectively spans smaller sections of landscape.

Small masterpieces are the new, also in-house trees of a Finescale series, as their specimens are handmade in the EU with a love for detail. They meet the highest demands of model railway enthusiasts. Due to the production method, the height can vary slightly in each case.

Since model trees in larger gauges are always reproduced too small, the N-gauge specimens that can be ordered from DM-Toys (<https://www.dm-toys.de>) can be used as an in scale conversion for Z scale.



The oak (art. no. MU_N-L00236; left) and spruce (MU_N-L00233; right) declared for the nominal size N are shown here as examples of visually successful trees from the Modellbahnunion that fit well with Z scale.

The pine (MU_N-L00232), spruce (MU_N-L00233), larch (MU_N-L00234), maple (MU_N-L00235) and oak (MU_N-L00236), each specified with 8 - 12 cm, come into question, if they do not completely exhaust the tolerance dimension.

If not, all tree species are displayed in a personal search, this may be because individual models are temporarily out of stock: After all, these are not industrially manufactured mass-produced goods.



Tank car "Flo-Sweet" (Item no. 530 00 510). Photo: Micro-Trains

The current deliveries at Micro-Trains:

Available now is the third Sweet Liquid tank car in light grey livery with black inscriptions and additional Flo-Sweet advertising notice (Item No. 530 00 510). Also, to be delivered are bulk freight cars of the Great Northern (533 00 161 / 162), Frisco (533 00 171 / 172) and Pennsylvania Railroad (534 00 111 / 112).

Micro-Trains products are distributed by Case-Hobbies (<http://case-hobbies.de>), among others.

The 2021 summer new releases from Märklin:

Märklin knew how to surprise the Zetties with the 2021 summer new products, but let's save the best for last for this announcement. The Märklin magazine annual wagon 2021 in blue livery (item no. 80831) was to be expected. This time it is a self-unloading wagon with hinged covers of the type Tad-u 961.

The new starter set "Museum Train" (81874) will fill the gap currently left by unavailable starter sets. The composition of track oval, running gear and three Donnerbüchsen (Thunder boxes) is known, but new is the steam locomotive 24 009, which has been added. This is a simple version without brake replication or detailed control.

The rail bus with trailer (88168) appears in the new, refined version now for the first time as a train class 796 / 996 of the Deutsche Bahn AG converted to single-man operation, which also carries the distinctive Dürr biscuit on the front-ends in the old familiar paint, in which the vehicles were on the road since their first year of service. The templates here were the last examples from Tübingen, which were only retired at the turn of the millennium.



A railbus set appears for the first time as Deutsche Bahn AG's class 796/996 (item no. 88168). Photo: Märklin

Also never offered as a model before was the class 218 diesel locomotive in purple livery with the simplified Ege biscuits it wore on delivery (Era IV). As 218 151-9 in revised form with bell-shaped armature motor (88792), it now appears in a one-off edition for MHI.

Since Märklin always wants to think in terms of trains, the matching coaches are still missing, which are also offered as an MHI special series and include a shape new product. The three-part set "local traffic" (87189) consists of a Bnrzb 719 (2nd class), and ABnb 703 (1st / 2nd class), as well as, a BDnf 738 driving trailer.

Behind this generic designation hides the "bunny box" that has been hoped for and wished for again and again for years, which got its nickname from the narrow control compartment next to a transition door.



A great train is also on the way as an MHI special series: The class 218 in purple livery (88972; photo above) has never been in the range before with the simplified DB logo of its first ten years of service. A typical local train hauled by it is shown together with a three-car pack (88972 / 87189; photo in the middle). This one also contains a long-awaited shape novelty with the “bunny box” BDnF 738 (photo below). Photos: Märklin

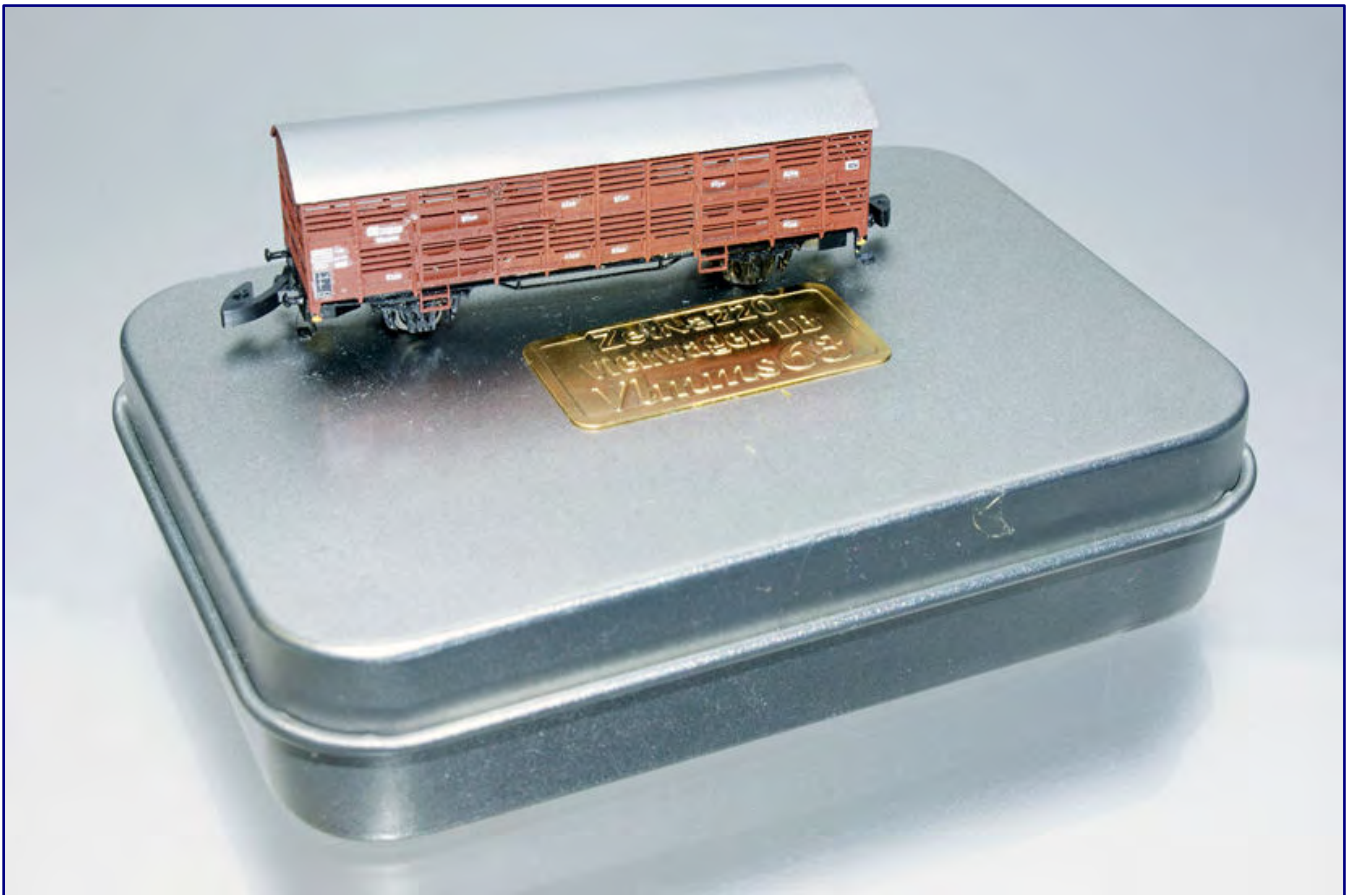
In keeping with early Era IV, all of the cars have computer numbers, black lower edges and presumably different versions of the Ege biscuit (positive and negative representation). Of course, the new car also shows warm white headlights or red tail lights depending on the direction of travel, implemented by means of maintenance-free LEDs.

A coupling hook at the end of the steering compartment is apparently not provided, but according to the product photos, it still has rubber bulges on this side as well.

First delivery of the Vlmms 63:

As a new model from ZetNa 220, the newly built German Federal Railroad hogging car Vlmms 63 (art. no. B002) is now beginning delivery. We reported about the current project and an actual sample in the February issue.

In the meantime, the wagon has been revised and now has the correct scale length; the incorrect hand sample can still be seen on our own side photos. The now delivered final version makes a good impression, and we will present it in more detail, especially since we can also show its prototype, in use.



The DB new-build wagon Vlmms 63, which began to replace the old small cattle wagons from the mid-1960s, also looks successful.

The interest in the models of the Sicilian small series manufacturer has strongly increased worldwide after our trade fair issue (February 2021) and the report in the last issue, as was reported back to us. The total number of copies ordered had exceeded all expectations, we were told.

The duo behind this brand was also amazed by the many countries, far overseas, from which orders were received. However, this also means that longer delivery times have to be accepted from time to time.

The small-series manufacturer can be found at <https://sites.google.com/view/zetna220>.

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Trainini

German Magazine for Z Gauge



A V 188 for Everybody

**Freight Car Metamorphosis
As a Beginner on his first Layout**

Introduction

Dear Readers,

Do you still remember the beginnings of your own model railway hobby?

By that I mean the aficionados who have been pursuing the hobby in 1:220 scale for decades.

How we sometimes cursed in the seventies that the selection of Z scale products was so modest. I myself (as a fourteen-year-old) discovered “kitbashing”, because there were not enough buildings for Z scale. Without further ado, 1:160 kits were trimmed down a bit. What didn't fit was made to fit.

And, today? A veritable “wonderland of goodies” has emerged, and the Internet also really helps with searching and finding things. But one thing has remained the same: Motivated newcomers to our scale are often faced with thousands of questions before setting up their first track oval.

Here in **Trainini®** and through a new series of articles we want to make it at least a bit easier to get started, especially since readers have already approached us with many questions throughout this Covid-19 period.

But we also have another tip, just pay a visit to your local model railway club; they can certainly help you if you need it, regardless of the gauge and... In my club alone there are now three new Z scale enthusiasts.

Fittingly, our reader and book author Heribert Tönnies explains, in an advance excerpt from his forthcoming third book (e-book), a relatively simple method for constructing track ramps.

Long before Märklin announcing their class V188 model, NoBa-Modelle had listened to various customer requests and explored how a V 188 model could be produced in an affordable way through 3D printing. In this issue, we show you the result of our build and adaptations of this model.

Märklin's Insider Club annual car 2019 (“Kuba-Imperial”) is unfortunately only a compromise when looking at its lettering and markings. With a few easy touches and small series parts, a de facto type G1 11 therefore becomes the G1 22 model it was supposed to be. Of course, we also add some suitable weathering. Other news from the model railway industry rounds off this issue.

Our regular appeal to readers still remains valid: Wherever in the world these lines are read, please stay safe!

In recent weeks and for reasons unrelated to the pandemic, parts of our editorial team have been reminded of how precious and unique it is to be in good health. The consequences cost time and often also nerves. That is why this issue has been released unusually late, and we cannot rule out further delays with upcoming issues over the next few months. We ask for your understanding and patience.

Sin-Z-erely,

Dirk Kuhlmann



Dirk Kuhlmann
Editor

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We thank Heribert Tönnies for his contribution and Eisenbahnstiftung for a historic photo.

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Cover photo:

V 188 002 a/b leaves a tunnel at the Spessart ramp with a heavy goods train, where it was found for years in goods train and pushing service. A special eye-catcher in the train is the colourful Kuba-Imperial car running in second position.

The V 188 from NoBa-Modelle The alternative “Bull”

The staff (of the Bundesbahn) called the powerful double diesel locomotives of the V 188 series, which the Bundesbahn had taken over from the legacy of the Wehrmacht and modernised, “bulls”. Long before a Märklin announcement, NoBa-Modelle had already listened to various customer requests and investigated how this class could be converted into a model in an affordable way using 3D printing. Today we would like to present the result in detail.

In February 2020, we presented the resin 3D-printed housings for replicating a V 188 (art. no. 5007R) offered by NoBa-Modelle as part of our Toy Fair coverage. The set consists of these two parts for the double diesel locomotive, and, also, includes a connecting drawbar for the two halves, together with two screws for permanent connection.



Our model is based on V 188 002, shown here as 288 002-9 on 22 July 1969 in front of Dg 5387 in Bergtheim (Schweinfurt - Würzburg line). Photo: Heinz Hangarter, Eisenbahnstiftung

In the meantime, the two „Hütchen” (“little hats”; shells/bodies) are also available in ready-painted and labelled form (5007L). Like many other model railway enthusiasts, however, we were attracted by the idea of finishing them ourselves, especially as we found a few challenges that we were happy to take on.

In our case, all the buyer needs, in addition, are two shorty chassis from Rokuhan (SA001-1 / Noch 7297903), suitable paints, and decals for the chosen road number. These can be made at relevant decal pattern manufacturers, so that the finished locomotive can finally stand in front of you for about EUR 100 cost.

This makes it a low-priced, but also compromised alternative or, in some cases, a supplement to the Märklin Club Model 2021. It will not meet the highest demands, but those who are more price-sensitive and who are willing to accept the deviations from the prototype to be discussed are well advised to go for the small-series model — the tinkering fun comes free of charge.



Test sitting on the chassis: To build up a V 188 from the two resin bodies from NoBa-Modelle, only two shorty chassis from Rokuhan are additionally required. The superstructures are simply attached.

First of all, we would like to point out that the locomotive was built as true to scale, as possible. This can also be seen in our data table in this article. Especially the bodies of the locomotive halves, but also the distance between the two vehicle parts, have been reproduced very precisely.

Nevertheless, we did not notice any problems when running on radii of 220 mm or more and also on opposite curves of the turnout, which makes the model very practical. The driving characteristics of the Shorty chassis are, however, not really slow runners, which of course cannot be blamed on NoBa-Modelle.

Data and dimensions of the double diesel locomotive V 188 (ex D 311 of the Wehrmacht):

	Prototype	1:220	Model
Length over Buffers	22.510 mm	102,3 mm	103,1 mm
Length of each locomotive half	11.080 mm	50,4 mm	50,2 mm
Width	2.910 mm	13,2 mm	15,0 mm
Height above SO* (rail head)	4.445 mm	20,2 mm	20,3 mm
Wheelbase (locomotive half))	6.000 mm	27,3 mm	26,5 mm
Wheelbase (each bogie)	2.000 mm	9,1 mm	9,4 mm**
Driving wheel diameter	1.250 mm	5,7 mm	3,8 mm
Service weight	147 t	---	25 g
Power	1.382 kW / 1.616 kW***		
Year of construction	1941		
Procured number of units	4 (2 transferred to the DB)		
Retirement	1969 / 1972		

* Before modification of the roof section by DB, does not correspond to the model
 ** Wheelbase in the bogie
 *** After conversion and motor replacement by the DB

All deviations from the prototype dimensions that cannot be attributed to measuring errors are due to the undercarriages used. NoBa-Modelle's aim was to create an inexpensive model and to dispense with a new undercarriage construction that could not be re-used elsewhere.

The Shorty chassis have altogether quite useful overall dimensions and can be hidden well in the body of the V 188. This made them the first choice for this project, because with this prototype a locomotive has

been found that does not have to be compressed or reamed to fit over this chassis.

The only thing that had to be accepted were a slightly too large width and the deviations in the running gear. Apart from the fact that the original has four rigid driving axles and the model has two bogies instead,



Before the 3D printed parts are painted, they first receive a five-minute bath in 100% isopropyl alcohol.

not been painted. The material of these parts does not take paint well, which is why you have to try it yourself.

A coat of paint is advisable to remove the plastic gloss, but the DB's prototype bogies were also painted RAL 9005 jet black. In an earlier project, the spray primers from Modelmates proved to be a stroke of luck, but, unfortunately, they are no longer available.

Parts to be protected, such as contacts and current-draining wheel surfaces, can be covered with masking tape (preferably Tamiya, Vallejo or Faller) and masking liquids (such as Humbrol, sold by Faller). Afterwards, it is only one stroke out of the spray can and the part to be worked on has the right colour, without completely losing its structure.



RAL 3004 purple red in a silk matt finish from Oesling-Modellbau gives the locomotive housings the already decisive closeness to the great prototype.

It is more complicated with the superstructures, because the resin parts should be degreased in an isopropanol bath beforehand. This simple alcohol is also used by the manufacturer to rinse out uncured resin residues. It does not harm the model, but guarantees a good painting result, and prevents adhesion problems caused by fingerprints or other grease deposits.

this also applies to deviations in the driving wheel diameters and axle positions. Those who want to take advantage of the price of this model will gladly accept this.

In the following we would like to describe how the unfinished resin bodies ejected by the 3D printer turned into presentable models, which also had a special feature built into them and which served us as practice and test pieces.

Reconditioning and painting the parts

The work on the bogies is done quickly. The bogies of the chassis are in a grey colour ex-works, and have

It takes two painting passes with the airbrush before the model takes on a recognisable shape. First, we choose a PUR primer from Vallejo in neutral grey before spraying on the RAL 3004 purple red top coat in a silk matt finish from Oesling-Modellbau.

The roof area is to be painted in RAL 9006 white aluminium, the paint is once again from Oesling-Modellbau. This is also done by spray painting, which is why the area must be masked off cleanly beforehand. The same can be done for the side area, where the same coloured trims are located. Practice is needed here, so that there is no paint infiltration and thus undesirable reworking.



After applying the roof colour, decorative stripes and black paint for buffer plank, lanterns and handrails, the painting is complete. Now it's time for some modifications and labelling.

We are very satisfied with the result so far. The remaining paint work is now limited to the deep black buffer beam including the lower locomotive lanterns and the driver's cab ascents. Here we use a brush and Aqua Color from Revell in a silk matt finish.

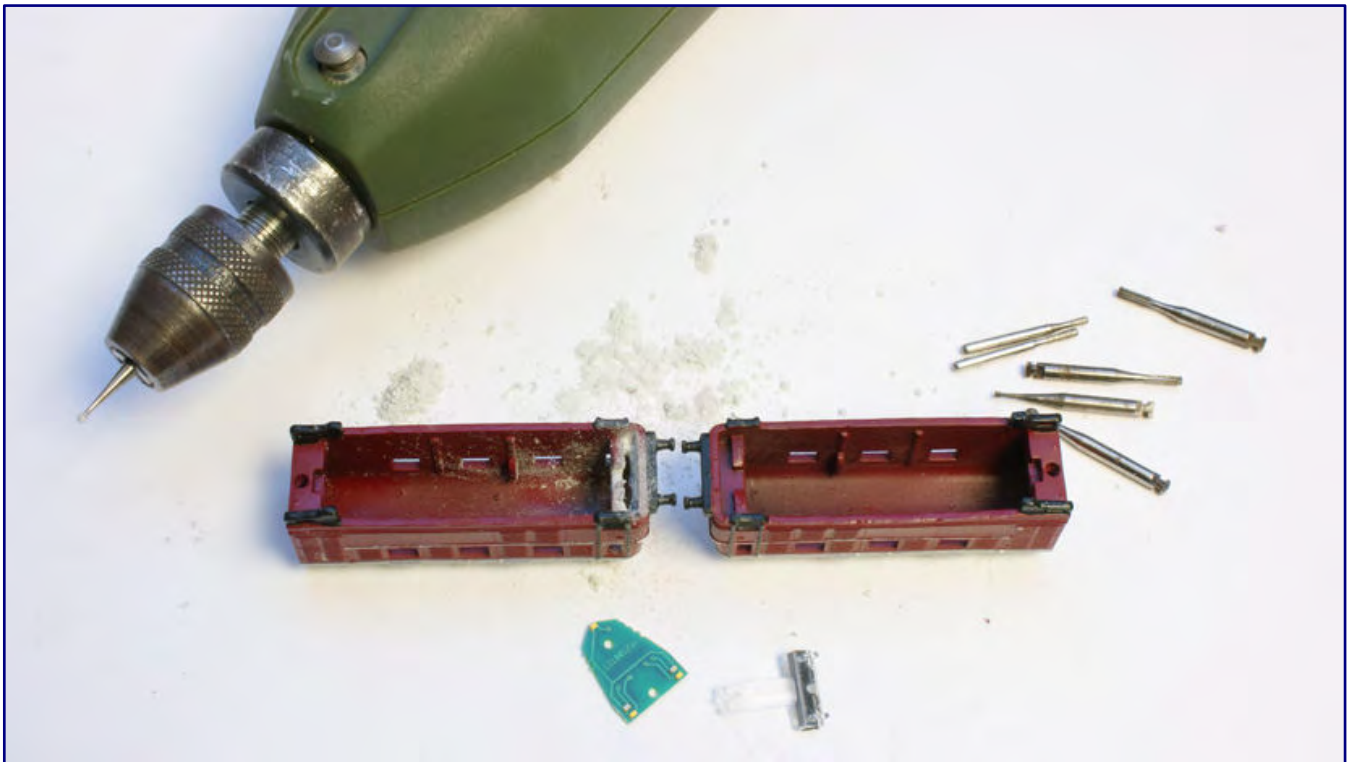
It should be mentioned at this point that we have chosen the V 188 002 a/b as a model, while Märklin wants to convert their sister locomotive. Differences are immediately recognisable: V 188 002 wore a red frame edge, which saved us a masking and painting pass, and the roof colours were not identical either. Even the inscriptions were arranged differently – it would not be possible to be more different with the dissimilar siblings.

Own adaptations

As soon as both halves of the locomotive have been painted, we can actually start labelling them. For us, it should remain at this point with a small test drive, which compensates us for the work done so far. The next step is the milling work in the front area of both body parts.

This is to be done carefully by hand with a micro drill grinder and disused dentist's cutters, and, is only necessary, because we want to provide the model with top and bottom lighting using LEDs.

However, we can save ourselves a larger self-build, because we have identified the lighting units of Märklin's V 200 (E161319) as a suitable circuit board. The side distances of the lower top lights and the



The floor area of the driver's cab must be milled out in order to insert a light board. However, the wide vertical walls (visible on the right housing) must remain to hold the chassis. The light guide board comes from Märklin's V 200 and has been stripped of its light guide.

height to the upper top light fit perfectly. Also, the red LEDs for the tail light are placed so close that they can illuminate the larger lanterns of the V 188 at the rear running end.

We only have to mill out the space between the outer casing and the partition wall and create a little more free space in the area of the upper tip light so that we can slide in the PCB without damaging it. The LEDs are particularly at risk of being torn off, because we have to pull off the attached light guide, which does not fit our model.



In order to achieve optimum adhesion of the decals and flush application of the films to the structures of the substrate, adhesive primer and softener are used as auxiliary agents.

We will explain later how to protect the sensitive parts. At this point we are only concerned with the preliminary work to be able to finish the exterior of the housing and not to risk any more damage to the

paint or inscriptions. The danger of the cutter slipping and going through the resin should not be underestimated and can lead to the total loss of a part. That's why we work carefully, and with as low a rotation speed, as possible.

If everything fits, we clean the inside of the housing by blowing it out and using a hair brush so that we can continue with the lettering. NoBa-Modelle obtains the white company inscriptions from Z-Doktor Modellbau as wet decals. Björn Plutka supported us here, and, also, created the previously missing labelling of the V 188 002 a/b and delivered them to us. For this, he deserves a special thank you.

They are applied in the usual way, whereby all inscriptions should always be given sufficient time to dry so that they are not accidentally moved or slip when the model is turned and worked on from the other side. The procedure is as follows:

1. Cut the respective decals as accurately as possible (leave little margin without damaging it),
2. Briefly place on the model to make sure that the cut fits (especially at corners or window openings),
3. Wet the target surface with an adhesive primer.
4. Immerse the decal in water for 5 to 10 seconds, depending on the size,
5. Allow the transfer pattern to "drag" on a non-absorbent surface, then wet with a little softener,
6. Place the decal on the model and push it onto the target surface with the help of a fine hair brush,
7. Carefully align there and smooth out with the brush, if necessary, drizzle on some more softener, if further shifting is required, and,
8. Finally, allow to dry well.



After the Micro Kristal Klear from Microscale has dried, all window surfaces are coated black on the inside with Revell lacquer to prevent unwanted light leaks or a view of the Shorty chassis block.

The inscriptions of the V 188 are quite extensive and some are not easy to apply, for example the circled vehicle half numbers near the buffer beam. When the steady hand gets lost in between, it is time for a break.

In this case, the model should be allowed an additional drying phase, and, if in doubt, work should only be continued the following day. Some of the decal patterns have also driven us to the brink of madness, at times.



You can see from the standing housing that the Revell Aqua Color dissolves the adhesive paint again, because the milky haze returns. This is not a problem and will disappear, but care must be taken when working.

Once everything is in place and dry, a semi-gloss clear acrylic coat (Bergswerk 83213) seals and protects the applied inscriptions from mechanical influences. At this point, the airbrush is used again.

The window panes are made with Micro Kristal Klear from Microscale, which is not available anywhere at the moment – all contacted dealers are waiting for a new delivery. That is why we do not give a current source of supply.

When this glue has also dried transparently, a step follows that has to be done carefully because it dissolves it again: To avoid unwanted light leaks, we paint the window replicas from the inside with deep black Revell Aqua Color, which was used before, using a hair brush. Black is also generously used to cover the entire front area where we had used the milling cutter.

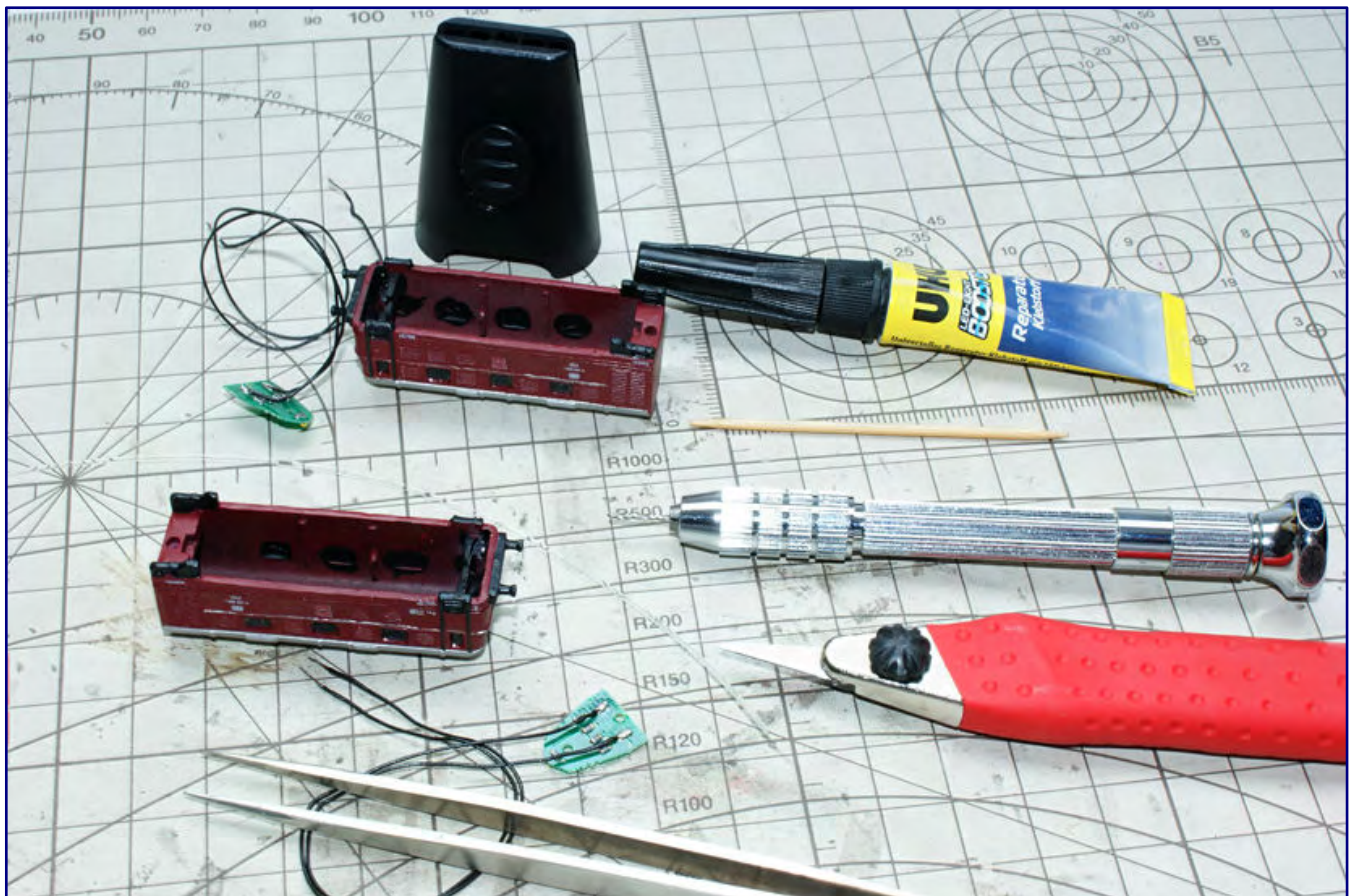
The locomotive body is finished at this point and is now waiting for its further use.

Electrical work and a critical look

In a perhaps unexpected waiting period, it is possible to work on the circuit boards. We have already taken off the light guides, but now we have to wire them, because the spring contact of the Märklin locomotive is not suitable here. The two poles are pre-tinned and then sufficiently long stranded wire is soldered on.

Since this has to be as flexible as possible and also very thin to be able to be placed inside between the running gear and the housing, we chose the extra-thin cable with a cross-section of 0.3 mm² in black colour from Viessmann (6890). The ring with 5 m will still be enough for several projects.

To prevent the LEDs from tearing off the PCB during insertion work – this happened to us with two of them, which we had to solder back on manually in the correct polarity – we came up with an effective protection: The light-sensitive adhesive Uhu Booster hardens crystal-clear and, in our experience, holds literally bombproof.



Fine, flexible stranded wire is to be soldered to the contacts of the light boards in order to be able to establish an electrical connection inside the locomotive housing. The Uhu booster prevents the SMD light diodes from being torn off and at the same time does not hinder the light emission.

We therefore cover all the light-emitting diodes with single drops of this adhesive and cure it with the UV-LED. The tear-off edges have now become sliding edges that will not swallow any light. And our repaired areas also receive an additional and effective fixation.

Then we lead the strands, with their loose ends pointing upwards, through the routed shaft and in the roof area into the larger opening of the housing, where the landing gear will take its place. We now pull

this in, loosen the top cover, which has two feed-through holes with the hand drill, and solder the strands fed through it to the current pick-ups on the chassis.

A short test now shows us whether we have chosen the correct polarity, which can be easily determined via the NEM definition by a brief thought. The test on the transformer also proves that all three lanterns are sufficiently and properly illuminated.



The cable routing needs to be well thought out (photo above): The strands are routed just under the roof into the area of the Shorty, where two bushings have to be drilled and soldered to the contact lugs on the driven bogie. Once this step is also completed and the housings are in place, both aids are permanently connected by means of a coupling drawbar and two screws (photo below).



Ready painted, lettered and with glued-in light guides, the model makes a good impression. The compromise in the undercarriage is not even noticeable from many perspectives.

There are no other unwanted light leaks, only at the bottom we have to provide a closure at the end of the project (with a suitably cut sheet of black construction paper fixed with a glue stick).



We can also successfully test the warm white-red light change. The project is now successfully completed, except for a few small things that still bother us.

What our model is still missing are light guides. These can be found on the internet in transparent versions in various diameters, but perhaps they can also be found – like ours – in the craft box. We push a suitable piece into the lantern openings until it meets the LED at the back.

Then we cut it to length, apply Uhu Booster into the lantern opening, push in the light guide, and cure the glue. The light guide may be cut quite short, and then the glue can also form a small lens on the outside. This may require another droplet, which is made to cure in the same way.

Once, this step has been carried out six times, the locomotive is ready. Now at the latest, the running gear and body are pressed firmly together and a critical review begins for us. The length of the strands must be sufficient to be able to remove the body without any problems and to work on the running gear, if necessary.



This photo illustrates, like no other, the still existing problem that the strands change the housing seat, and then make the double diesel locomotive bodies uneven.

At the same time, they should be as short as possible in order to take up little space inside. We have not yet succeeded in doing this perfectly, because after a few minutes the „Hütchen“ (“little hats”; shells / bodies) at both ends push up again a little in places. The locomotive then gives the impression of being “bent” in the middle of the two units.

We are also not yet satisfied with the handling characteristics of a chassis that has a noticeable tendency to derail. NoBa-Modelle has given us advice on reworking that can solve this problem, as this problem has also been observed by them, in some cases, with these very inexpensive parts.

However, this is also caused by the long coupling drawbars with Märklin-compatible couplers. The short alternative does not fit on this locomotive, while the long Rokuhan parts sag too much. A tighter guide that allows less downward play, therefore, also remains to be retrofitted.

There remains an in-house development by Z-Doktor Modellbau that can give these undercarriages better low-speed characteristics. It is not yet clear whether this electronic board will be offered in series production, because lawmakers have set high hurdles for the offering of electronic components.



The layout "Werdersheim" seems to be made for this model, on which V 188 002 is now happily making its rounds with a long goods train.

The only request we received was for a test fit to see if these parts could be used in NoBa-Modell's V 188. There is no doubt that they would fit well there and could be a mutual selling point. But this step is also still pending and can only take place when we have solved our "cable problem" inside.

- **Manufacturer of the basic model:**
• <https://www.noba-modelle.de>
- **Chassis used:**
• <https://www.rokuhan.com/english/>
• <https://www.rokuhan.de>
- **Lettering and materials:**
• <https://www.bergswerk.de>
• <https://www.faller.de>
• <https://www.oesling-modellbau.com>
- <https://www.tamiya.de>
• <https://viessmann-modell.com>
• <https://www.z-doktor.de>

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A GI 11 becomes a GI 22

Upgrading made easy

Märklin's Kuba-Imperial wagon is only a compromise, if we take its labelling as a yardstick. The Miele wagon of the same shape, on the other hand, has matching inscriptions, but only had a similar prototype. We want to change that with a few handholds and small series parts. Afterwards, the models will be given a patina suitable for operation.

Märklin's Insider Annual Car 2019 (Item No. 80329) was certainly on the wish list of many Zetties. So it should not have been a disappointment when a covered goods wagon in light grey with large Miele lettering was presented, instead of a new item in form.

The delivered model was labelled as a GI 11, a wagon of the association type with riveted design and labelled as a vehicle owned by the Deutsche Bundesbahn (German Federal Railways). This fits exactly to the shape of the chosen wagon, but, nevertheless, raises some doubts with regard to the prototype.



Unfortunately, we have to do without diagonal stiffeners on the right and left of the sliding door because of the design. Nevertheless, the two parts from Z-Doktor Modellbau for the sprinkler system on the undercarriage floor clearly show the decisive difference to the GI 11, which Märklin's mould construction has reproduced.

As is well known, DB's own goods wagons were painted reddish brown, so this example was somehow out of the ordinary. It would have been more logical to have a private wagon that was used on the state railway and had the company colours of its owner.

If we explain the background of this special design, it will immediately lead us to a quick and easy-to-implement project that we would like to present here. Nevertheless, some inconsistencies remain to be mentioned and explained, because they also influence the course of our adaptations.



At the time of our sample test, Z-Doktor Modellbau had designed two versions that differed in their spatial depth and, consequently, their stability. What they both have in common is that they have a long adhesive edge on the side facing the carriage floor for a perfect hold.

In the 1950s, the Deutsche Bundesbahn began to rent out GI cars belonging to splinter classes to customers on a long-term basis, or even to sell those cars to its customers. Therefore, the customers could also paint them in company colours and paint them with their advertising logos. They were recommended for the transport of large-volume, but relatively light goods, such as tin cans, but also high-quality appliances such as washing machines.

The companies Miele and Kuba-Imperial, among others, made use of them, which Märklin has done with the Insider Annual Cars 2019 and 2020. Miele rented at least two wagons over the years, while Kuba-Imperial bought them, if we can believe the labelling of the Märklin model (80330).

The Kuba-Imperial example bears the road number 566 425 [P]. But, what is more interesting, is that it is labelled as GI 22. This is one of the aforementioned splinter types, which the Bundesbahn apparently did not know what to do with in its regular service. In this case, however, the shape of the model does not quite match the original.

However, it is a good thing that the main dimensions, the running gear and the brakes of the "Verbandswagen," which was later classified as GI 11, were taken over for this type of the exchange type built from 1923 onwards. Thus, the basic dimensions are correct, and the external deviations are limited to a few points, some of which we want to correct.

What we don't like about the Miele wagon offered one year earlier, which is indeed labelled as GI 11, is that it was not a splinter type after the end of the war. So, we are left with great doubts, that an example of this type could also have been hired out.

If we take our cue from models that Märklin has produced in larger scales and are also inspired by implementations by other manufacturers, we come to the decision here to re-label and rebuild the 2019 annual coach. It, too, is to mutate into a GI 22, and, since we have also acquired a second example, they will both also receive new road numbers.

Quickly and easily implemented

The easily recognisable differences between the GI 11 and GI 22 models remain manageable: First, there is a diagonal brace to reinforce the bays next to the sliding doors, which the GI 11 did not have.

However, we will have to overlook this difference. In principle, it would be possible to attach such a strut, but, in view of the low profile thickness, this would be a greater challenge. Almost impossible, however, are the advertising markings on both models, which would have to be transferred without interruption, including the basic colour.



The two supporting struts are easy to fit in: they are placed centrally between the axle brackets and directly behind the ladder (see also enlargement on the top right). Glue them together with Uhu Booster.

We can and would, therefore, like to accept this manageable deviation. However, the situation is different for another characteristic feature of the Dresden further developments: Their prototypes were equipped with bogies that originated from or were derived from the stanchion wagons of the replacement types.

Therefore, behind the running boards of the loading doors there were supporting struts that gave the running gears additional stability. Even at a short distance, their profiles are easily recognisable, despite the black frame colour and are therefore quite “face-defining”.

From an early stage, the shapes of the supporting struts, including the gussets, seemed suitable for 3D printing. And so we got in touch with Z-Doktor Modellbau, because Björn Plutka understood and still understands how to make extremely filigree and clean structures with his printer.



On the Miele wagons, the no longer correct type designation and the operating numbers on the body and frame still have to be removed in order to be able to replace them later. We were able to do this with the unfortunately no longer available pad print remover from Prehm-Modellbahn, and a dental supply micro brush.

He was persuaded by our product idea and designed the parts we needed for this conversion (art. no. ZD-220-20002-1). In order to approach the optimal product, two versions were first implemented, which are identical in their length and height dimensions. The depth (thickness in the horizontal) of the supports to be glued under the wagon floor was varied, which is not visible when looking at the refilled wagon.

Attaching these two 3D printed resin parts is really easy. They are first placed dry with tweezers to determine the correct position directly behind the running boards and between the axle holders of the treated side.

The course should then be thinly painted with a suitable adhesive. Here we recommend Uhu-Alleskleber Super "stark & sicher" (formerly "strong & safe") or also Uhu Booster, which cures under the UV light of an LED, which we chose here.

Our choice is based on the fact that we only have to apply a light pressure for a short time. As soon as the enclosed LED unit has been in use for 20 seconds, the adhesive is cured and develops its holding power. We can then proceed directly to the colour design.

In this next step, we adapt the appearance of the attached supporting strut to the car floor. We therefore use deep black, matt Revell paint (Aqua Color 36108), and a fine hair brush and colour the part that has just been glued on until the wagon floor looks like it has been "cast in one piece" again.

After drying, we can go about changing the operating number and inscribed type on the two Miele wagons. Only the Kuba-Imperial wagon already has the correct designations, which we will, of course, leave in place.



The complete removal of the frame inscriptions that are no longer required (and have already been replaced here) is hardly possible. The arrowhead marks tiny remnants, including the complete "D" from the previous owner's inscription. These residues, barely visible to the naked eye, can only be carefully covered with matt black paint.

Before new labelling can be applied, however, the old ones must be removed, as gently as possible. The use of electric erasers is conceivable as well as chemical aids. We still have one in stock, but as far as we know it is no longer available: Tampon print remover from Prehm Modellbahn.

It is applied with a micro brush from dental supply, and after a short time, it dissolves the inscriptions to such an extent that they can be carefully wiped off or removed. In this case, the type designation and the operating number can be found on the left side of the car body as well as on the frame.

For the new labelling of the GI 22 we have chosen the service numbers 191 286 and 192 608. Both fit into the number range of this design, and, are, in many respects, conclusive for our project. However, we could neither prove nor disprove that they were indeed once used on wagons with Miele advertising. The company archives of the household appliance manufacturer from Gütersloh could not help either.

New decals for wet application were also made by Z-Doktor Modellbau, whose printing skills we are now well impressed with. Low font height, razor-sharp typeface and an almost invisible backing film that did not tear during application are evidence of the highest quality. Incidentally, not all suppliers on the market can print white.



Using a pointed hair brush in the smallest size available, we cover the imperfections shown in the previous Photo under triple magnification of a magnifying glass using Revell paint (Aqua Color).

The application to the model follows a procedure that has been described many times: cut as small as possible from the sheet, dissolve in a water bath for 10 seconds, then let it soak for a short time, and bathe the backing paper in a suitable softener ("Decal Softener"), while the substrate on the wagon is wet with an adhesive primer ("Decal Setter").

Then, the decal can be transferred to the object with the help of a fine hair brush (cat's tongue shape), moved into position, and smoothed out before it is allowed to dry in peace. Excess liquid can be soaked up at the edge with a cotton bud.



The before and after comparison shows the difference between the converted and re-labelled GI 22 (left) and the original model designed as GI 11 (right).

The softener is important so that the backing film fits tightly on the board structure of the wagon wall, but also on the frame support with fine rivet reproductions, and does not leave any bumps on the relief. Another drying break is then required.

Fine and ageing work

In a next step, we carry out a magnifying glass inspection. As expected, we do not find any faults on the body, because this area is easily accessible. On the frame, on the other hand, the unerring removal of the old labelling is not so easy, and so we find tiny remnants here that we did not cover.

Again, the deep black Revell lacquer is used, which now has to be applied accurately to the faulty areas with the finest hair brush. This can only be done with an extremely steady hand and under magnifying glass control. The result immediately compensates us for these moments of tension.



The Cuba-Imperial wagon has also already received supporting struts that match the type already inscribed at the factory, and has already been painted over in black. Now it is waiting for its patina, which will still give it operational traces.

Once this paint has also dried, we reach the final and also most exciting phase of the fine detailing. No freight wagon, not even one with such an advertising motif, was and is always in a freshly washed condition. Dust and dirt, to which it is exposed every day, leave traces that the models also need in order to look authentic and realistic.

It is important that we do not apply any colours indiscriminately, but think carefully about what kind of contamination will be found in various places, and how it got there.

We have identified:

- Rain run-off marks on the roof,
- Various types of dirt that get stuck especially in the board joints of the superstructure, and
- Flash rust as well as brake dust on axle holders, parts of the brake system and the frame.

The first two patinas can be applied and crafted with a brush. Again, the matt black from Revell serves us well. This time, however, we dilute it with a little water and a few drops of isopropyl alcohol.

So it flows well into the cracks and at the same time we reduce the opacity considerably towards a classic washing product. On the side walls, it flows into the joints and leaves clearly visible traces there, while only a light veil remains on the raised parts.



Using matt black Revell paint (Aqua Color 36108), water and a little isopropyl alcohol (96 %), we prepare a washing preparation with which, we can give the board joints and rivets on the sprinkler more plasticity, and at the same time recreate a stressed operating condition. Brushes and cotton swabs help to apply, smudge, and, if necessary, remove too much paint in one go.

The steps we are about to complete take a little time, which we are happy to take. As soon as the colour mixture has dried a little, we wipe the outer skin with a cotton bud. Every now and then, we even moisten it. We take off colour where the impression is too intense, if necessary, also with the help of simple alcohol.

Shortly afterwards, we can again use the brush. Only when the joints show the desired depth and the box struts and the door area with the latches show credible dirt adhesion, which also brings out the finest details such as rivets, are we done here.

After drying, we work on the roof. We apply the same paint thinly over the entire surface, let it set briefly and then paint from the line of the highest point of the barrel roof outwards to both sides of the wagon. This way we get, also after a few corrections, the typical traces that water leaves behind when it follows gravity.

The last weathering step aims at the undercarriage. For this we choose rust-brown and not too garish colours from the Vallejo range (distributed by Faller), which should be matt drying in any case.



The procedure is simple. For the even deepening of the board joints, our glaze is applied with a brush over the entire surface. As soon as it has dried a little, we wipe the surface with the cotton swab (photo above), and take some of the glaze off the walls. This step can be repeated until the joints are sufficiently dark and the walls show a slight haze of dirt. In the course of the work, we selectively touch up selected areas (photo below) in order to emphasise all textures and shapes in the desired way.

This time they are applied with the spray gun in a fine mist that emphasises the lower sections more than the adjacent frame parts at the transition to the coach body. This completes the colour work, and a matt clear coat preserves our results, but, above all, also protects the new inscriptions.

Tiefmatter Acryl-Klarlack (deep matt acrylic clear varnish) from Bergswerk (83211) convinces us at this point, because it really removes all the shine from the model and makes it look dull like the prototype. At the same time, it does not leave a grey haze when applied with spray.



Three at one go: While V 160 005 has both Miele wagons in tow in a through goods train, 55 2993 is shunting Cuba-Imperial's GI 22 on the lower track level, which was hired as a private wagon by DB.

In any case, we are very satisfied with the result and are happy about three new "individualists" in the wagon fleet. They will certainly attract attention in goods trains because of their colours, which differ from the usual photo, but, at the same time, they no longer outshine all the other representatives in the train with unnatural purity!

Manufacturers of the base model and the conversion parts:

<http://www.maerklin.de>
<https://www.z-doktor.de>

Sources for required accessories:

<https://www.bergswerk.de>
<https://www.faller.de>
<https://www.noch.de>

<https://www.prehmshop.de>
<https://www.revell.de>

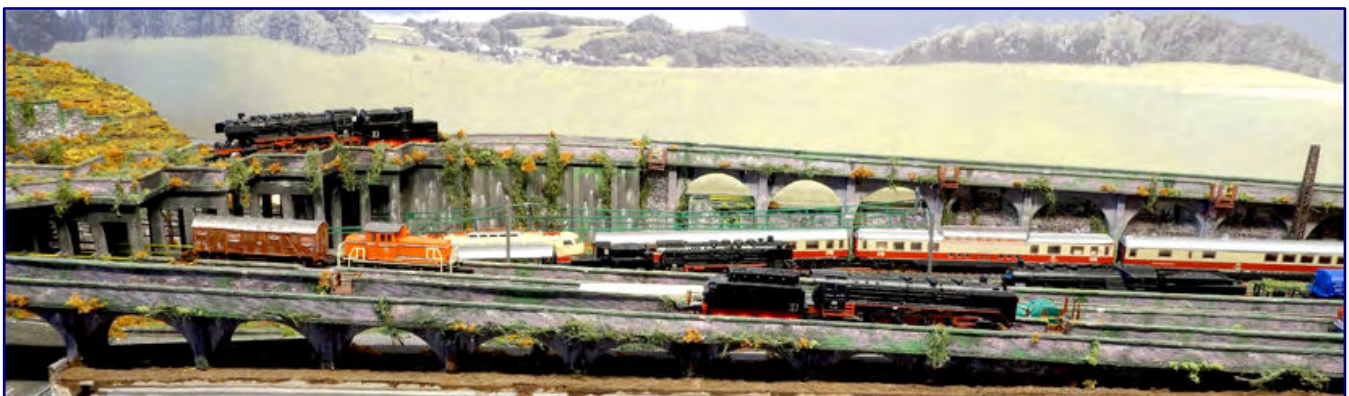
Self-built track ramps (Part 1)

Planning and finding Materials

Our reader and book author Heribert Tönnes has dedicated himself to model building that is as simple as possible, inexpensive and easy to master. He therefore also offers special options for newcomers who do not yet have access to a stash of kits and materials accumulated over years. In a two part series he will teach you to build arcade-lined ramps with simple means.

By Heribert Tönnes. Today, and in the next issue of **Trainini®** I would like to explain a method for the relatively simple construction of track ramps as an advance extract from my forthcoming third book (eBook).

For my large “Herilingen depot”, which is located on a slight slope, different heights between the double-track main line, the depot and the industry section of the layout had to be navigated.



Building ramps according to individual measurements in the simplest, most time effective, and inexpensive possible way, together with a lot of modelling fun, is the central aim of our author.

Due to the layout's topography and its theme which I already described in more detail in volume I of my practical guidelines, routing tracks through the sloping areas in the naturally given terrain was not possible.

The industries are located in the immediate vicinity of a city. In such a setting of inner-city and peri-urban locations, railway ramps were built to create pass-overs, often to cross over other tracks.

If such ramps were located not only on railway owned property, but directly on property boundaries or, in the case of crossings, on third-party property, they were built with arcades in the form of a stub or round arch.

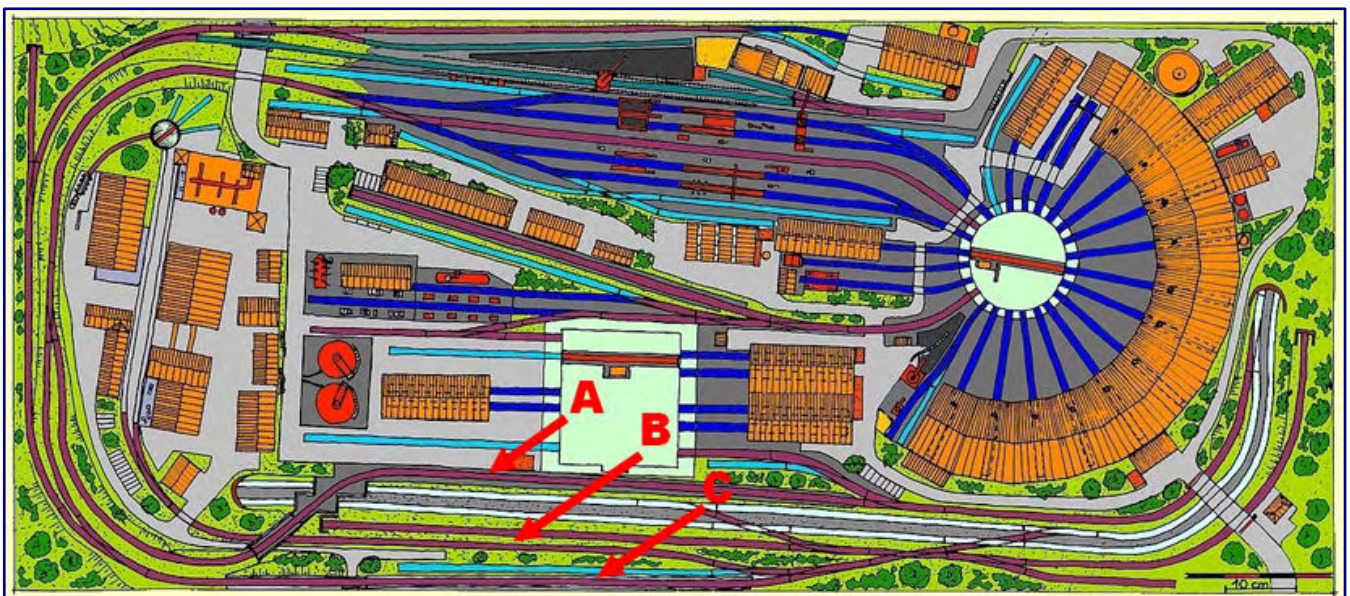
On the lower level, this made it possible to create a passage (for commercial traffic, roads, pedestrians, but also, with the appropriate height, as a superstructure to buildings or technical facilities). At the same time, this helped to avoid as far as possible cutting up cities with metre-high, wall-like railway structures, and, thus, made for transparency and visual lightness.



We often find ramps and overpasses in areas close to cities where various railway lines branch off. The example here shows a scene from Köln (Cologne).

To emphasise the location of my railway depot close to the city and to increase visual interest, I have, therefore, also decided to build track ramps. For my plan I need three versions of different lengths, which I implement with round arches as follows:

- For the track branching off from the double-track main line to the crossing bridge and further as a feeder track to the depot with an upstream branch to the locomotive depot (A on the track plan below),
- track routing under the crossing bridge, but across the access road, to the industries (B) and
- at the front edge of the layout for the outbound track from the depot to the main line (C).



The track plan of the depot layout shows the areas where ramps have to be constructed for crossing roads and tracks. The letters are explained in the article.

Unfortunately, there are no ready-made ramps available for Z scale which come close to the prototype I had in mind. I therefore had to build my own, also because of the desired layout-specific individuality. However, this also gave me the freedom to master almost any special situation typical for the layout.



After extensive research, the builder opted for a combination of concrete arcades in the form of round arches with reinforcing masonry pillars

For the planning of the ramps, some things had to be taken into account in advance in the design of the track plan. For example, it was necessary to determine the required clearance profiles (clearance heights) for track crossings and bridges, or, to plan the heights to be achieved with the resulting maximum gradients, from which the ramp lengths result.

I explained some of the basic principles in more detail in Volume I of my book series, which is why I will not go into more detail here and would like to refer interested parties to it. In this engineering report I will confine myself exclusively to the construction of the ramps.

After researching many ramps and elevated track solutions of the prototype, I decided on a combination of concrete arcades in round arch form (semicircle or, depending on the height, only circular segment) and brick reinforcement pillars placed in front of them.

This construction method also echoes the already completed brickwork of the coaling station of the depot, which helps to ensure visual continuity and uniformity with respect to the typical local designs on the layout.

For fall protection on the ramp, simple steel railings are usually used along the edge of the ramp. Because the three ramps on my layout are relatively long (without auxiliary structures approximately 220 cm in total), I chose a brick parapet with a concrete cover as a fall protection which can also be found in the prototype, and which simplified their construction, and helped to reduce cost.



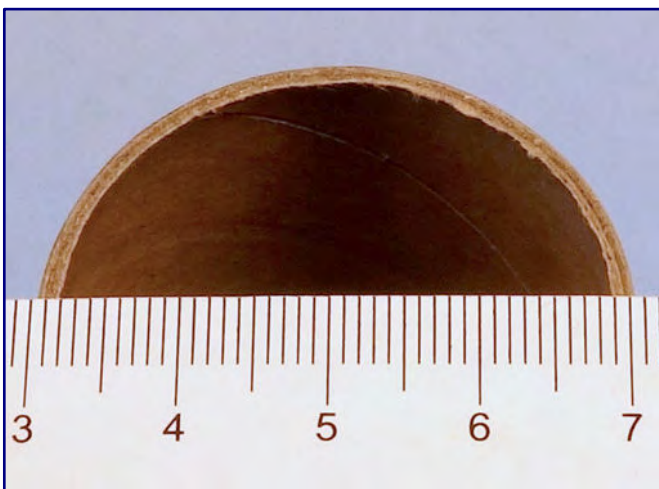
Three different types of arcades were chosen for the project: completely open design with recessed parapet walls (top left) closed arches with vertical (top right) and sloping brickwork (bottom right) in the same design.



In the area of the curved ramps on the left, however, steel railings were also used on smaller sections. I chose three variants for the arcades, which were also necessary due to the terrain topography of the layout:

- Completely open arcades, partly in the lower area with slightly recessed parapet walls, because of height differences in the terrain between the front and rear sides of the ramps,
- closed arcades with vertical walls (structurally necessary because the terrain is relatively high on the rear side of the ramp and it is a high wall), and,
- closed arcades with slightly sloping walls (because the terrain is high on the rear side of the ramp, but the wall height is relatively low).

In view of the transportability of the layout, which measures approx. 0.90 x 1.95 m, the main focus for me was on lightweight construction, which is why heavy wooden structures or plaster were out of the question. I therefore chose balsa wood again for this construction, which is very light, but nevertheless stable, if used correctly, and, above all, very easy to work with: no sawing, just cutting with a sharp craft knife.



A cardboard roll with an inner diameter of 3.5 cm was used as the basis for building arches (left). The precise use of a handsaw produced cleanly cut circular segments (photo right).

First of all, however, I was faced with the problem of how to quickly build uniform round arches, of which I needed a large number.

Tips for freehand sawing

If no saw gauge or mitre saw is available, the small roll strips can also be cut freehand.

However, there is a great danger that the saw blade will be tilted or skewed when sawing the relatively large diameter, so that the cut-off ring does not have a consistent width.

To avoid this to the extent possible, I mark the width of the ring to be cut off in advance with closely spaced marks. I then do not saw through from top to bottom in one go, but turn the roll a little further after the saw blade has cut through the wall thickness of the material.

And I only, then, continue the cut until the material thickness is cut through again. By slowly guiding the saw blade from mark to mark, I get a ring of relatively equal width.

Tips for eliminating width differences

If freehand sawing results in slightly different widths of the ring, I sand them off later, after the round arcs (usually half circles) are already glued to one side of the first balsa board.

This gives me a larger, more stable contact surface and allows me to guide the segment cleanly on sandpaper that is placed on a smooth surface.

The method of bending balsa wood used elsewhere for a wide under-tunneling is out of the question this time because on the one hand the round arches have a (too) narrow radius and on the other hand the effort is too high.

The method of bending balsa wood used elsewhere for a wide under-pass was not suitable for this purpose, one because the round arches have a too narrow radius, and, second, it would have meant too much of an effort.

But, as I also described in volume 1, an ambitious model railroader should always go through the shops with his eyes wide open!

in mind, but I discarded them because, first, the required number of rolls is too expensive, and, second, the precise processing (shortening vertically to the required width) is too time-consuming due to the plastic material.

At first, I had the plastic rolls of adhesive film

I finally found what I was looking for in the household department of a supermarket: replacement rolls for lint rollers.

These rolls have a cardboard core that is relatively sturdy (approx. 1.5 mm wall thickness) and an inner diameter of approx. 3.5 cm – this corresponds to about 7.70 m in Z gauge, i.e., an ideal dimension for the arcade arches.

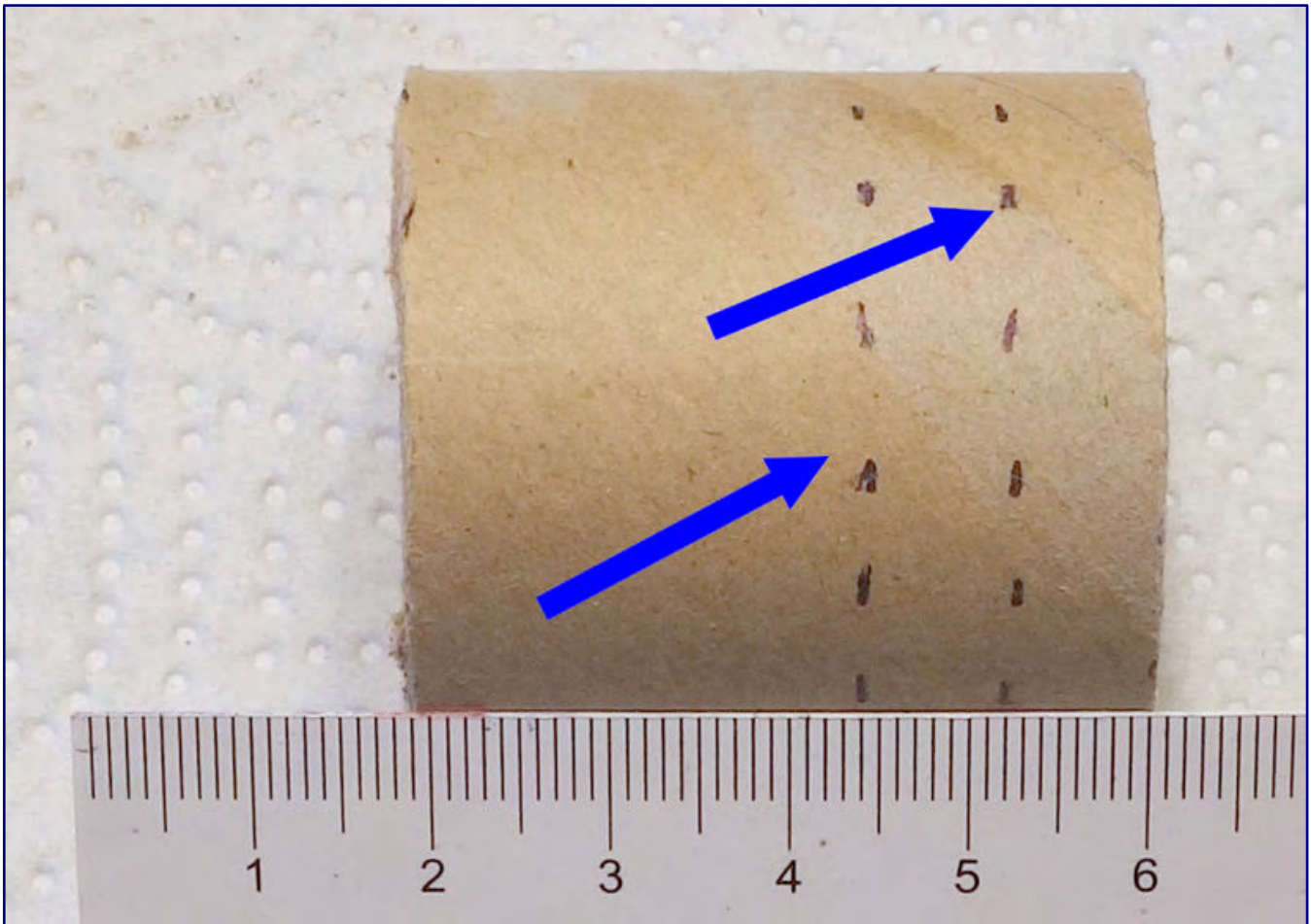
From the approx. 10 cm long roll, I could cut or saw 10 individual roll segments, each measuring 8 mm wide, which then makes for 20 round arches. Each package costs about Euro 1.80 and contains two rolls, which already yields 40 arches.

That should be enough and the price is unbeatable! It is easy to work with a small handsaw, a scalpel and sandpaper, so it was perfect for the intended purpose

Those who know my books know that I build my layouts in the simplest possible way, i.e., without elaborate technical equipment and tools. That's why I did not use the mitre



Rough edges that occur during sawing can be removed and smoothed with sandpaper.



Successive markings with a felt-tip pen provide orientation and are intended to prevent accidentally crooked cuts. After all, one is working freehand here!

saw here either (although I do own one), but a simple hand saw to cut the approx. 8 mm wide roll segments from the long roll.

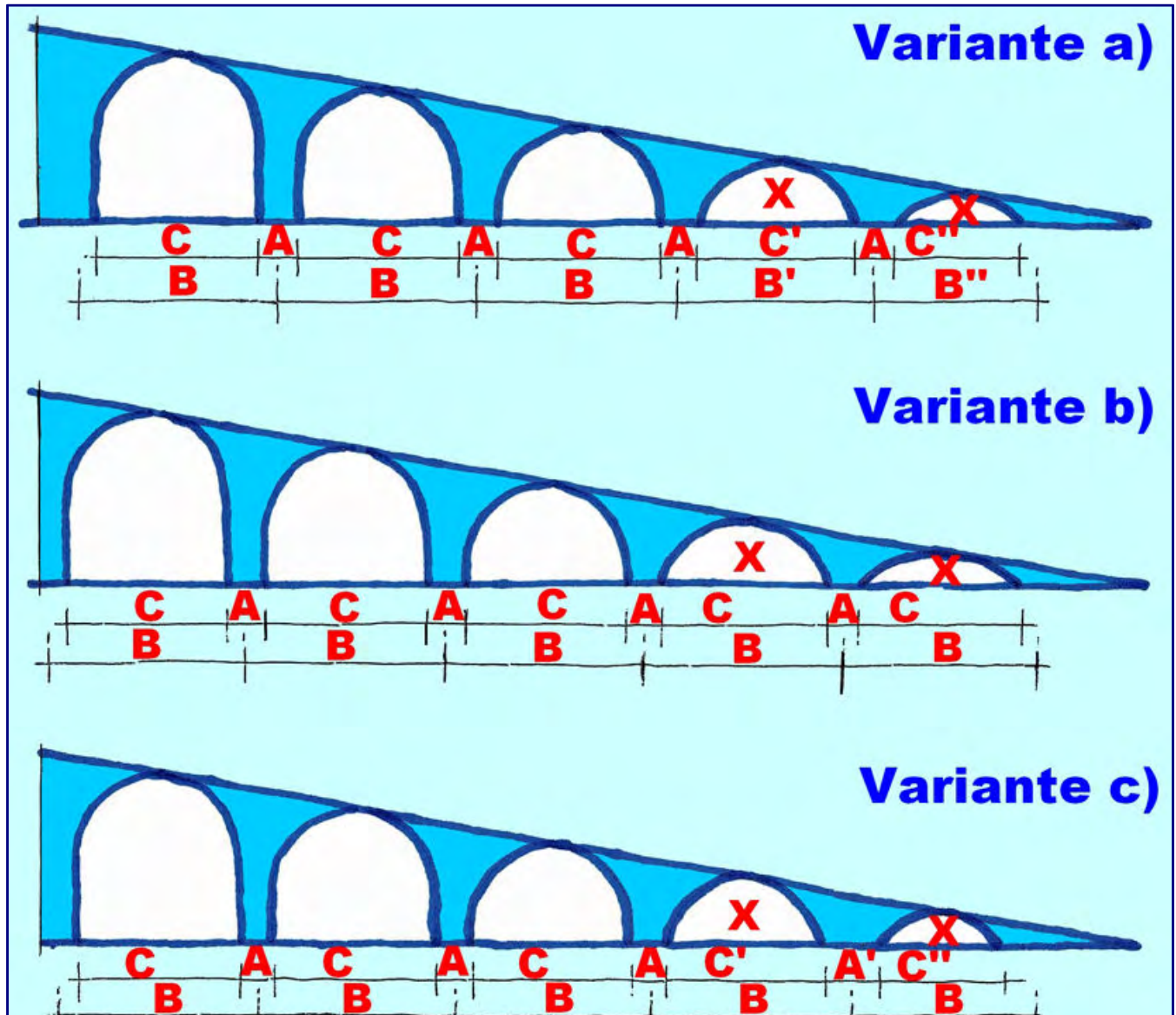
By the way, I did not use a wood saw blade for the cardboard roll, but a metal one. First, cuts are easier to control that way because the fine teeth allow for a smooth and gliding sawing motion, and, second, the cardboard does not tear as quickly because of the finer saw blade structure, which still results in a clean cut.

After cutting the segments, I ran the cut edges over a piece of 400 or 600 grit sandpaper placed on a smooth surface. This eliminates any minor inaccuracies. I also break the edges that may have become slightly frayed during sawing with the sandpaper. This can also be done later after gluing.

After having prepared the cardboard rings, I had to determine the division and spacing of the round arches. There is a special aspect to consider here: Where the round arches do not have a full radius in height due to the ramp becoming lower at the beginning of the ramp, three variants are possible:

- a) If the pillars between the arcade arches all have the same width, then the distance between the pillars decreases from arcade to arcade in the direction of the start of the ramp (while maintaining the same radius for all arches, because the height of the partial arch decreases more and more towards the direction of the start of the ramp, and, thus, the outer dimension of the arch on the base edge becomes smaller and smaller).

- b) The pillars between the arcade arches all have the same width, the distance between the pillars always remains the same, then in the direction of the start of the ramp the radius of the (full) round arch increases from arcade to arcade. The arch strips that do not have a full height radius must all be bent up to different extents; the outer arch dimensions on the base plate remain the same.
- c) The radius of all arcade arches and the distances between the arcade arches remain the same, then the width of the pillars between the arches increases from arcade to arcade in the direction of the start of the ramp. This does not look nice visually and the reason for the different widths of the pillars cannot be understood by the lay observer.



I therefore opted for variant a), because, first, it looks symmetrical, and, second, the (slight) reduction of the distance between the arcade axes in the low ramp area is hardly noticeable, with bushes placed in front of it later on, it is no longer noticeable at all.

All photos and illustrations in this article: Heribert Tönnes

Webages of the author:
<http://bestagernet.de>

N VI

81874 "Museum Passenger Train" Starter Set

Starter Set

Just the right start for all beginners and switchers. In addition to a four-piece set consisting of locomotive and cars you can expect a starter set with an enlarged track oval and power supply.

€ 269,- *



1x



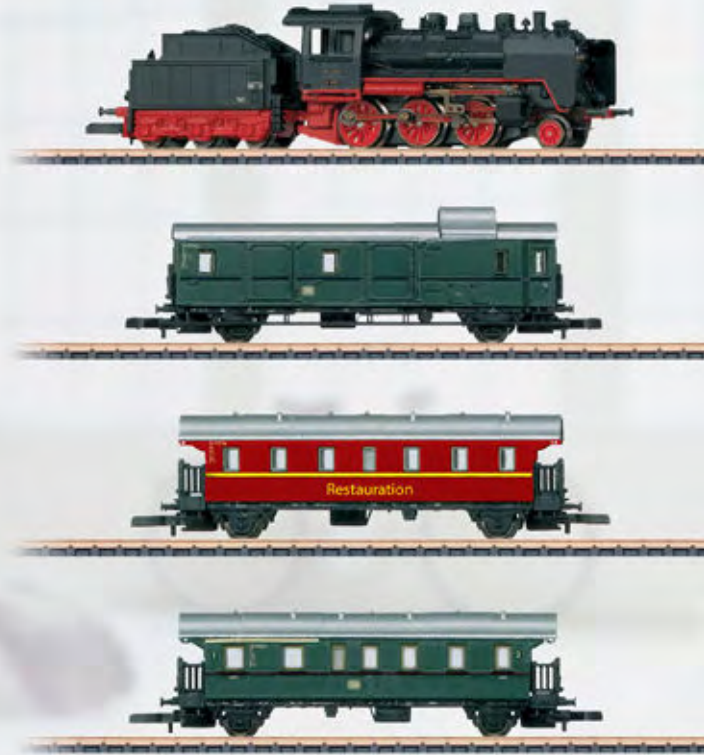
5x



4x



6x



You can get different track extensions with the sets 8190, 8191, 8192, 8193, and 8194.

Uerdingen Rail Bus

N

88168 Rail Bus with Trailer Car

German Railroad, Inc. (DB AG) class 796 (former VT 98) motor car and class 996 (former VS 98) control car. Crimson paint scheme. The units look as they did in Era V around 1997.

- Interior details modelled
- LED interior lighting in the control and motor cars
- Motor with bell-shaped armature

€ 209,- *



The first driving tests (Part 1)

Track Geometry and Basics

The passion for the hobby has been awakened, the dream of having your own layout is alive. This moment can mean a crossroads: Euphoria, many topics, big plans and in the end a frustrated retreat or practical learning on manageable works and later starting out big as well as successful. In a new series, Dirk Kuhlmann points out typical beginner's mistakes, teaches the basics and sets initial work tasks that have a key character.

By Dirk Kuhlmann. Welcome to our new series of reports, which revolves around the most important and basic topics concerning the hobby of model railways in Z gauge. In particular, the newcomer or the returning model railroader is looking for tips or tricks to put his little "treasures", effectively, in the limelight.



Am Lietwerk – a small piece of a perfect world; at the same time, it is also a good object for practising and rehearsing one's own skills.

In recent months, the members of the **Trainini®** editorial team have been increasingly confronted with questions about the basic structure. We would like to respond to the thirst for knowledge of our readers, who have discovered the most beautiful hobby in the world for the first time or have rediscovered it, with a series of our own.

However, we will not insist on general validity here, but rather act in a supportive manner, with suggestions that have proven to be practicable over the years. The pleasure of model railroading usually starts with a starter kit. Why not?

After all, all the components such as tracks, rolling stock, speed controller with plug-in power supply, and detailed descriptions are there, which is why the fun of playing can quickly begin.

While the train is making its rounds on the oval, thoughts naturally wander directly towards a model railway layout with all of its equipment and possibilities. The look into literature and catalogues now runs almost in parallel. But what would be another sensible purchase?



Treat yourself to a four-axle locomotive (e.g. V 100, V 160 or others) at an early stage and enjoy the driving culture of these modern products.

Extremely slow speeds are no longer witchcraft in this scale: We have tested vehicles that can creep along the tracks at less than the converted walking speed!

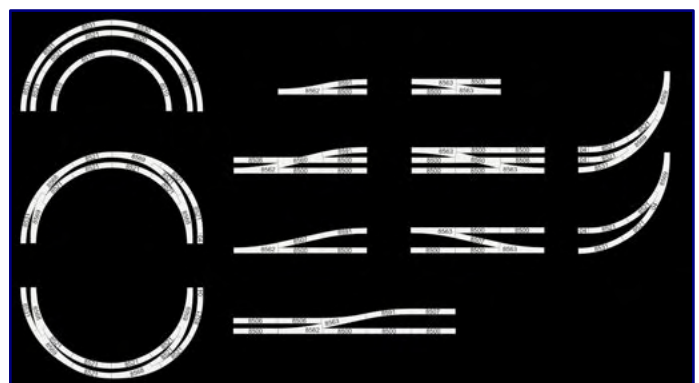
And, we would like to deliberately and explicitly add here: We are not talking about digital possibilities. Clean rails and as many power pick-up points as possible on the wheels of a locomotive make this possible, in addition to a good motor with a balanced and smooth-running gearbox.

A knowledge of the track geometry of each track provider is a basic requirement for initial plans. Illustration: Märklin

Track geometry

We can now extend our simple oval from the starter pack with some useful rails and points. To do this, however, we recommend taking a close look at the track geometry of the mass-production manufacturers in advance, e.g., on the internet or in track plan books.

The following PDF file on the Märklin pages provides a fairly quick overview:



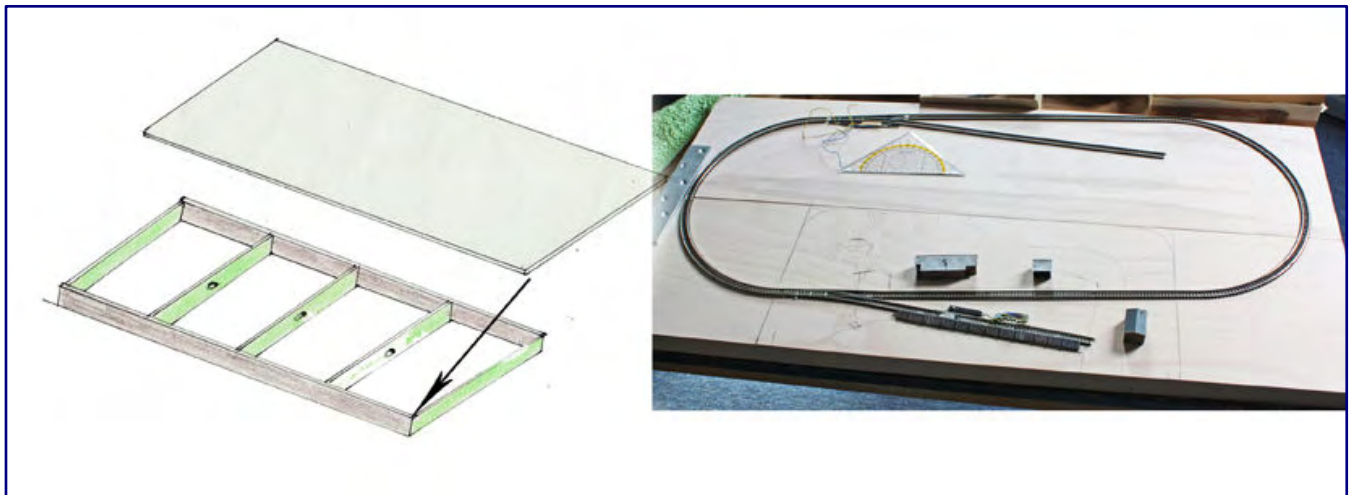
www.maerklin.de/fileadmin/media/produkte/SpurZ/0296_Gleisanlagen-Buch_Spur_Z_komplett.pdf

From Rokuhan there is this information service:

www.1zu220-shop.de/media/content/rokuhan/Flyer_Rokuhan_2017_12Seiter_A4.pdf

Märklin and Rokuhan have structured their track material very sensibly. For many track figures, examples are shown in their instruction documents.

For a first test track, not a simple “railway board” should serve as a base, but directly a nowadays usual box with a cavity for the necessary underground cabling. I will now give you a sketch for this.



These were the first steps towards the later "Strandgatan" layout. A box like this also hides all of the wiring beneath the layout.

This “box with rails” without a landscape could also serve as a test track for your rolling stock in the future. To ensure that our little “darlings” hum along quietly, we recommend a base made of 2 mm cork. You can get this as a roll in most DIY stores. Cork and wood can be glued together well with Uhu Kraft.

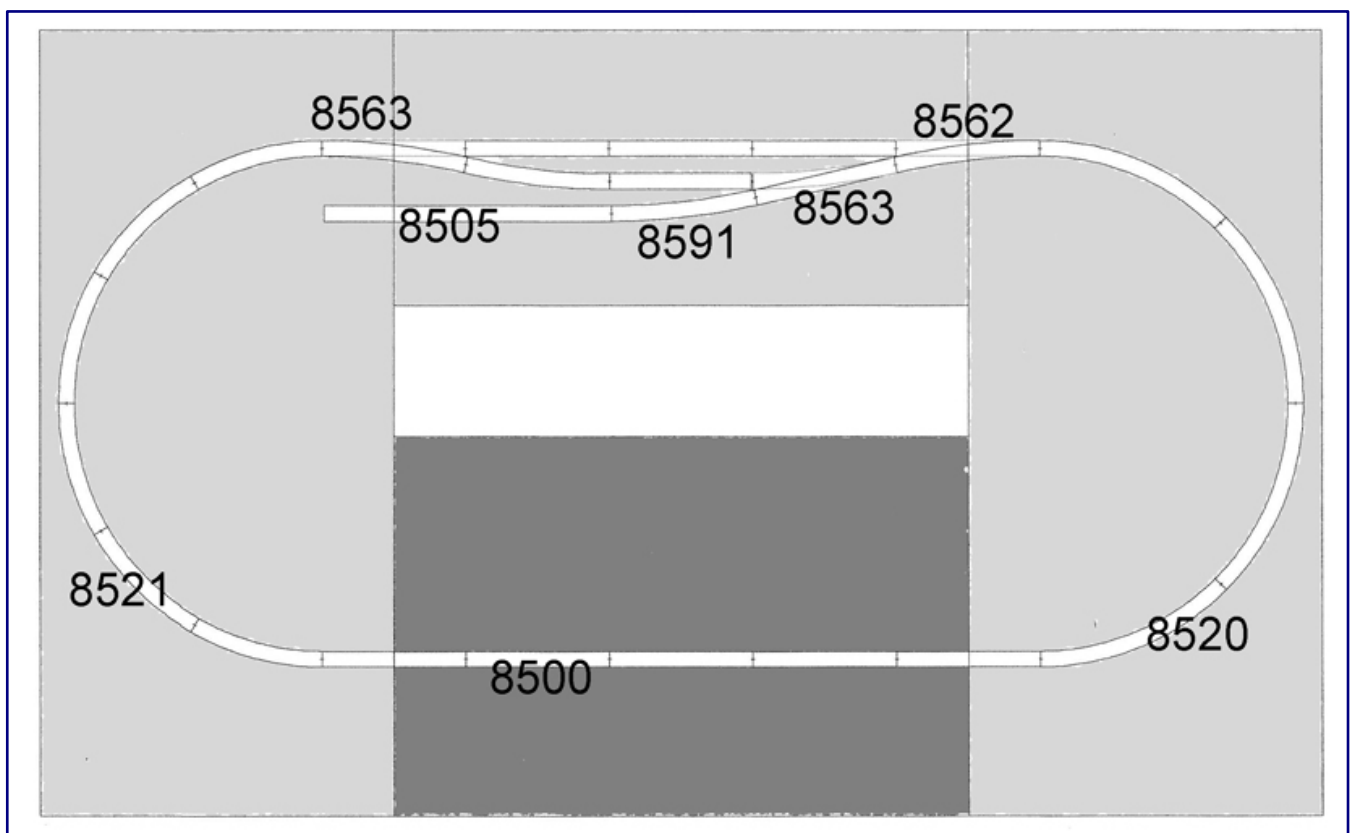


Just for comparison, even the later layout “Bad Rothenzell” (200 x 70 cm) was built following this box system.

The art of self-restraint

Important: Plan your first showpiece in a manageable size. This is the only way to learn and/or intensify all the necessary handicraft work in a timely manner. If you plan (too) large right at the beginning, frustration often sets in: Beginner's mistakes happen and permeate the entire work, a long time window for all construction steps and hardly visible construction progress at the beginning make the initial euphoria fade away quickly.

A beautiful model railway layout needs time and experience. It starts with the woodwork and ends with the placement of small figures or other details. In between, there are many tinkering sessions of various kinds to be completed. Model railroading is and remains the most extensive hobby of our time.



This is the small plan for the showpiece "Am Lietwerk". The bypass boxes are placed according to the perimeter.

If you start small at the beginning, you can acquire many different skills in a manageable time, gain experience (also from your own mistakes), ensure the important sense of achievement more quickly and lay a foundation for larger projects in the future.

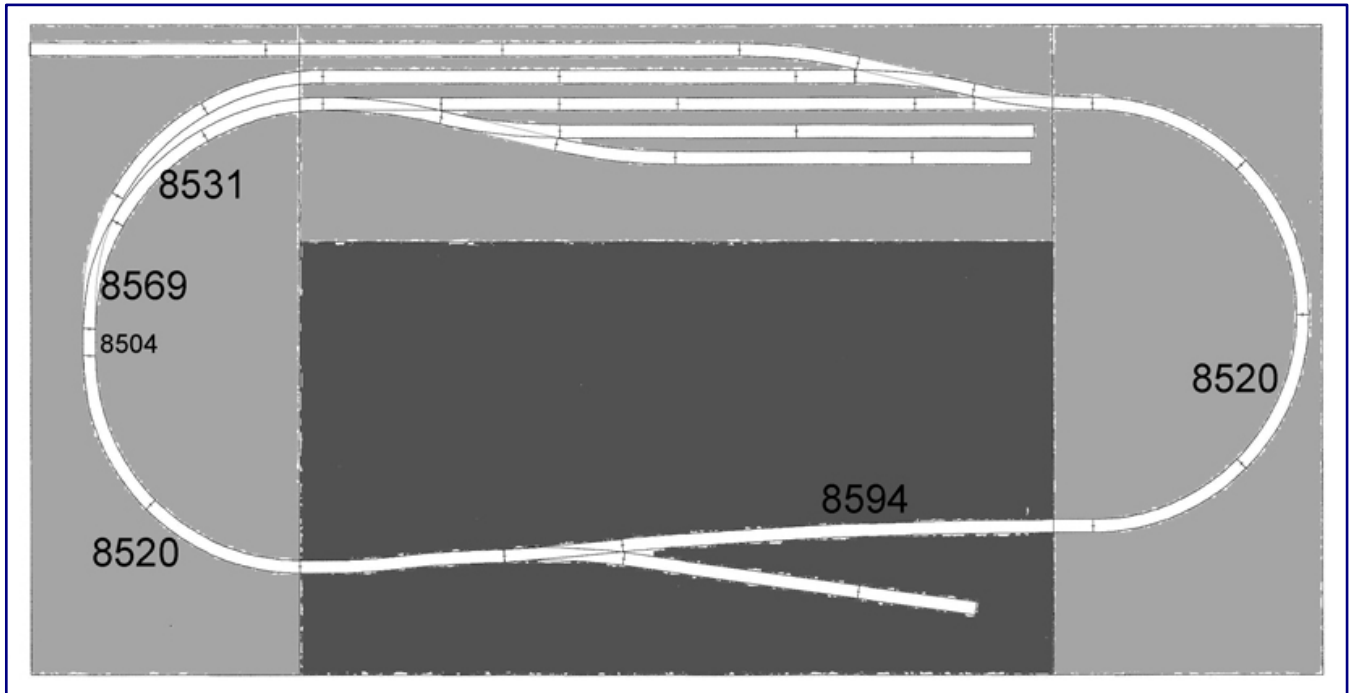
So that the whole procedure does not end in a purely theoretical instruction, we will show you everyday procedures on the basis of selected showpieces.

For a few months now, I have been building two smaller layouts that are excellently suited for documentation. We will go into more detail in the second episode. Exactly these two track plans serve as a template for all further work.

The first version is still a simple oval with an additional passing siding for a second train. With set 1 (art. no. 8191) from Märklin, everything would even be available in one package. The second version is

somewhat larger and has three additional sidings. Here, the other rails and points were bought separately.

Now, I will give you a few tips in advance to make the rail body look good especially in the later designed environment.



In the exhibition layout "Upland," a system for changing possibilities comes into play. Further below, we will illustrate this with a quadruple image.

Märklin offers, for example, two effective components with the siding (8590) and the separating track piece (8588), but even the beginner is bothered after a short time by the rather clumsy attachments for the cable feeds.

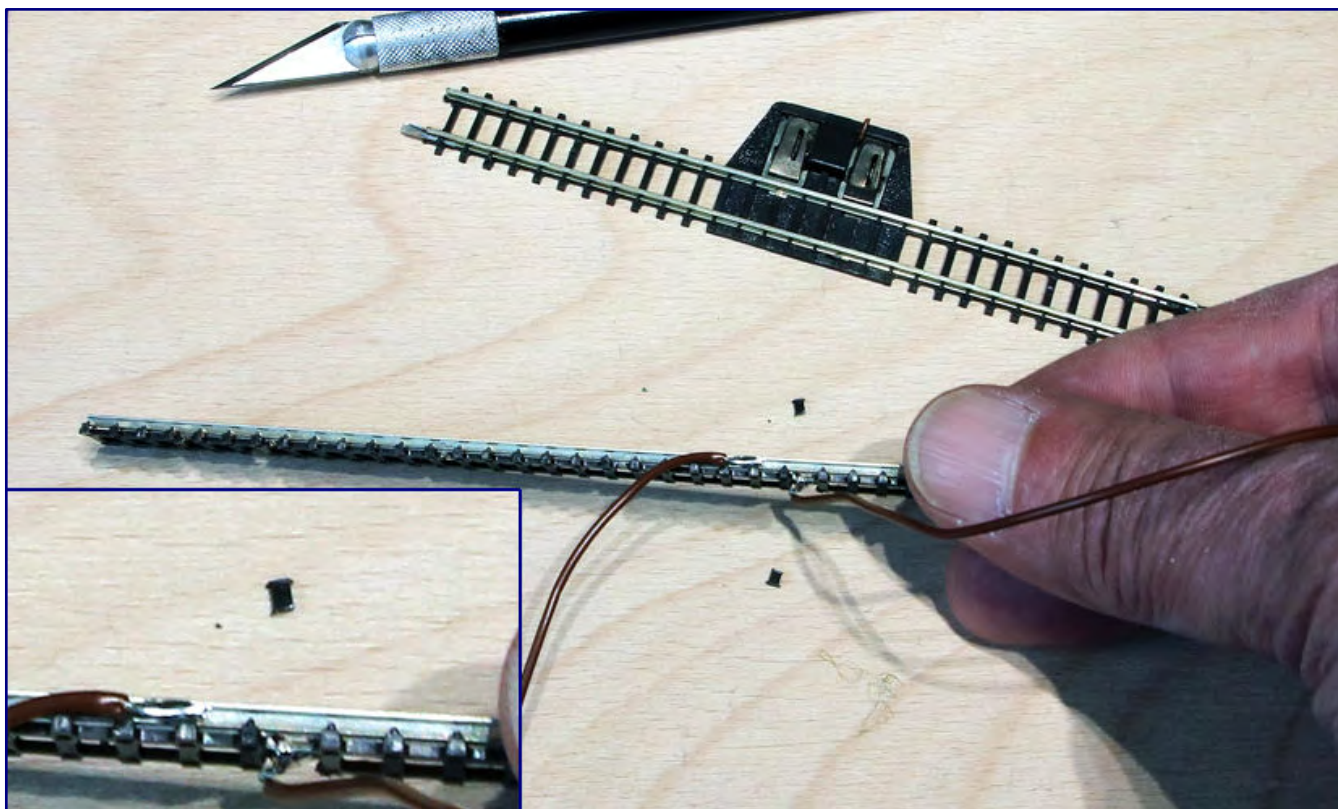
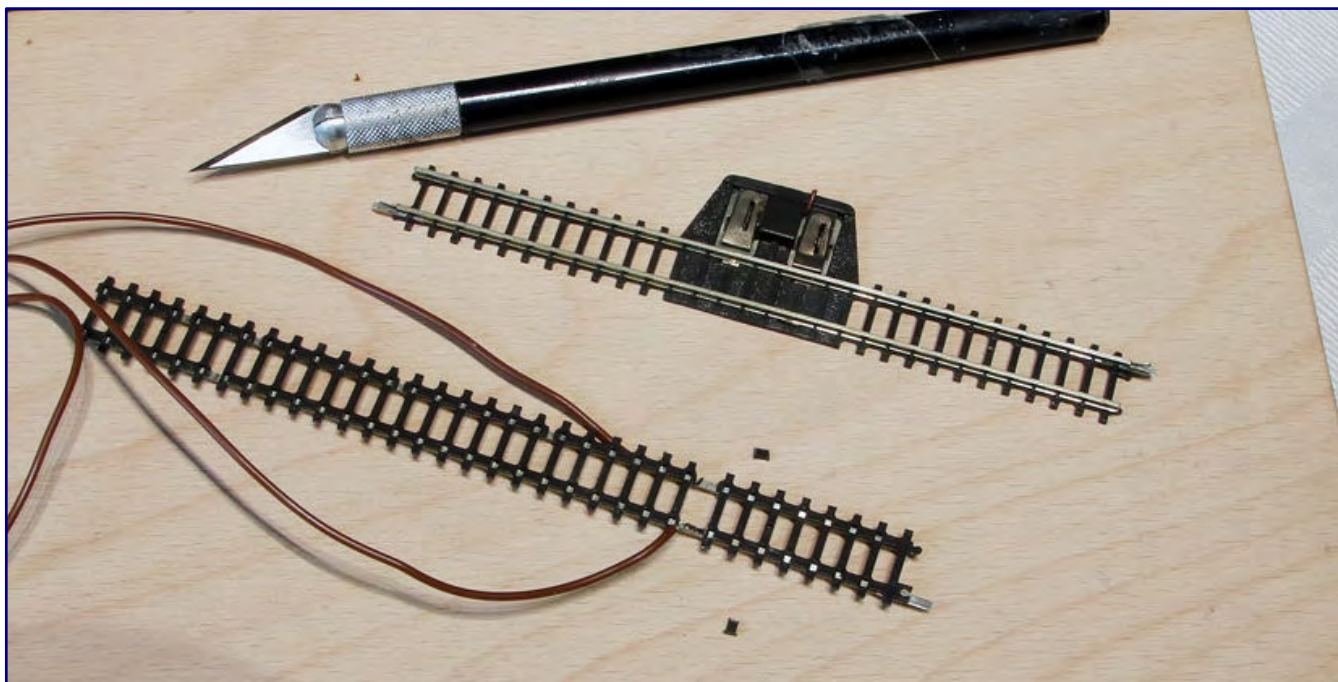
In both cases, the experienced model railroader soldered the necessary cables (mostly) to the track himself. For purely stationary operation, we remove a piece of the connector at the sleeper band and solder the stranded wire to the rail from below. On an exhibition layout, I usually place the soldering points on the outer flank of a rail (see photos on the next page).

This way, in case of a technical defect, the search for the cause would be much quicker. Who wants to spend half the time of a model railway exhibition lying under the layout?

In order to isolate a section of track (e.g., a passing siding) from the whole railway, we use the separating lugs of the respective track suppliers. These are quite ordinary connecting lugs in form, but made of plastic instead of conductive metal.

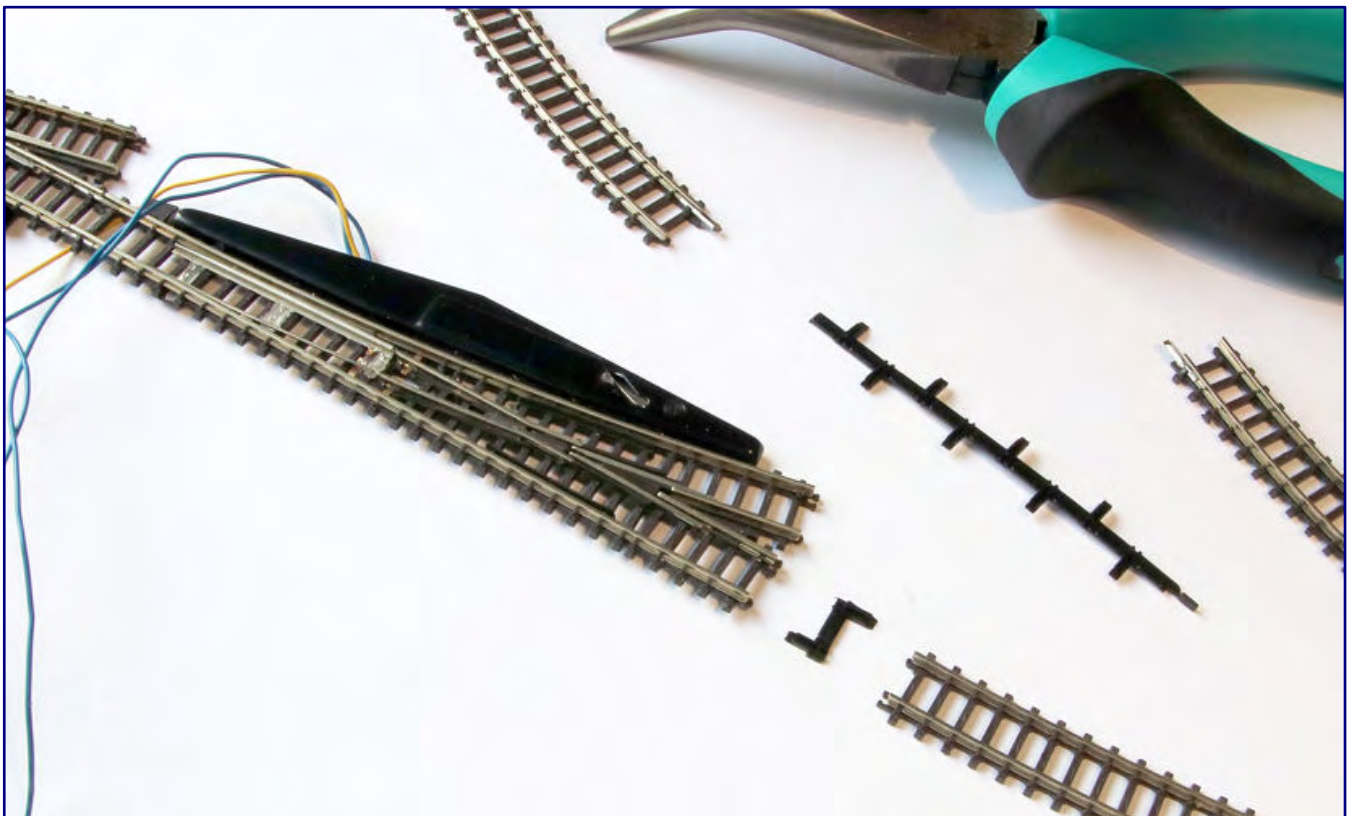
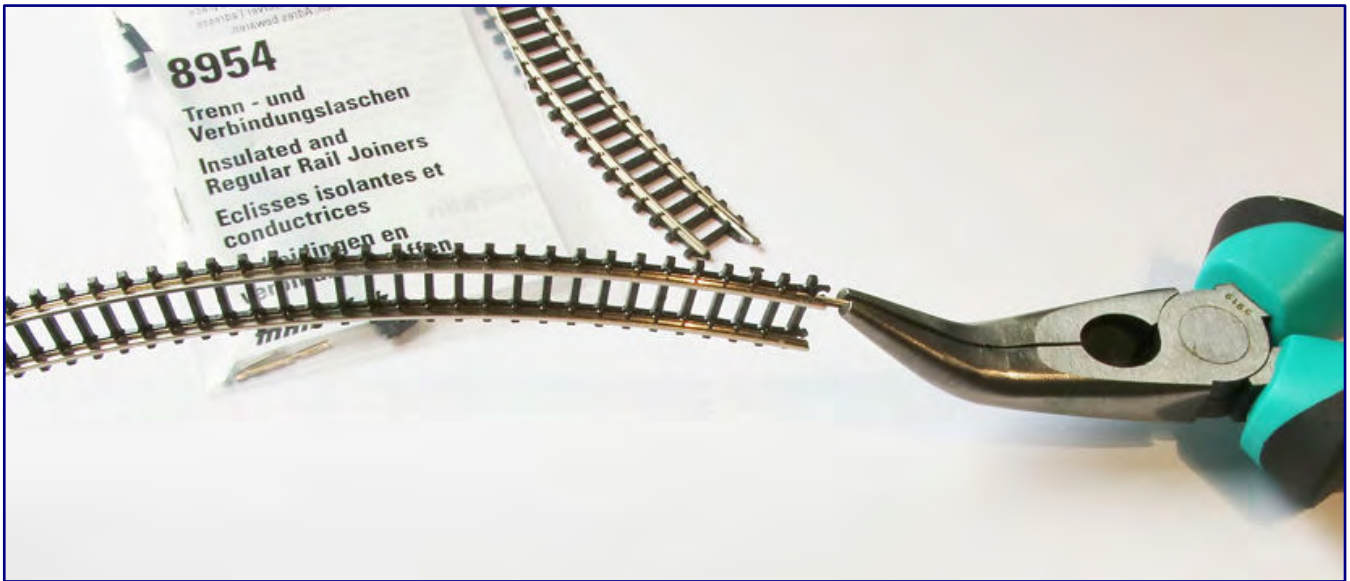
To do this, the sleeper strip is loosened a little from the rail profile and the metal tab is pulled off with a pair of precision needle-nose pliers.

Of course, we now also have to supply this insulated rail section with power. This is done in the same way as the instructions just given. Deviating from this, we now only add a small toggle switch (e.g., Donau-Elektronik, art.no. KS501C or KS134; end customer purchase via Mercur-Versand), then we can simply switch off the current to our train.



The photos show the usual soldering versions of the traction current wires on many model railways, as mentioned on the previous page. The lower one is preferred for exhibition layouts.

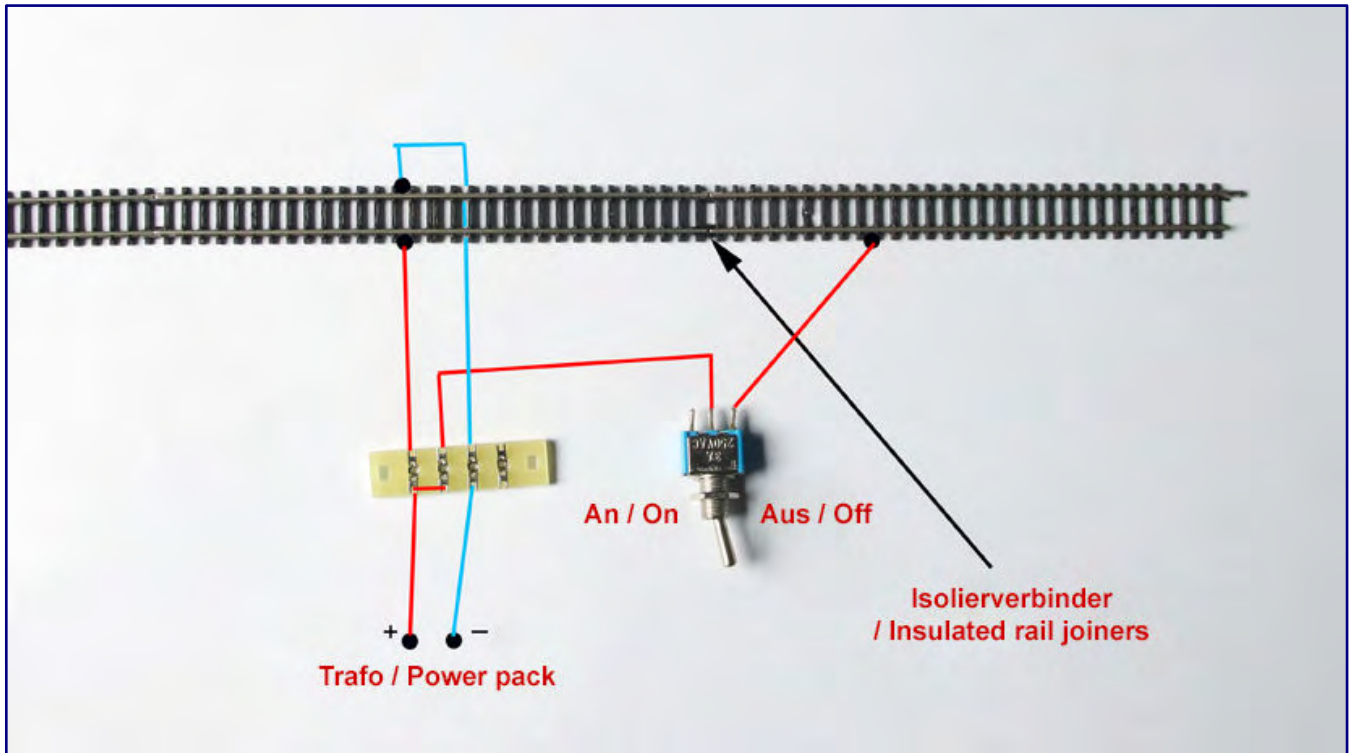
When installing an electromagnetic separator, simply follow the connection diagram, which is provided in every package. It is really simple. You can now fix your track with the track nails from Märklin, I only recommend gluing for final track structures.



With a little skill and a pair of needle-nose pliers, the rail connectors can be removed quite quickly (photo above). Separating tabs later provide an insulated piece of track (photo below).

With these simple possibilities, you can already do a little operation and now, at the latest, everyone's imagination should run away with them: A station is needed, accompanied by a larger workshop for locomotives, ... oh yes, ... a goods dispatch, ...stop!

To begin with, limit yourself to one or two themes that can be implemented on our proposals due to lack of space. Don't worry, even the greatest model railway professionals sometimes have to struggle with this – over and over again.



This is a simple plan for an isolated section of track. With this, a locomotive can be de-energised at any time while another train makes its rounds.

Back to our two examples: “Am Lietwerk” (approx. 40 x 25 cm) is set in the surroundings of the town of Warstein and mainly shows a single-track line at a block signal box. The slightly elevated position on a railway embankment shows off our rolling stock very well. The scenery is only rounded off by a lot of landscaping. The work is limited to a single theme.

“Upland” is 66 x 36 cm in area, more than twice the size of our first example, and thus offers two themed areas. On the one hand this is a simple stopping point for local trains and on the other hand a coal yard, which has a siding. Here, local goods trains could deliver fuel and pull off empty wagons. “Upland,” by the way, refers to an area near Willingen and Diemelsee in the Hochsauerland.

Have you noticed anything? Only the examples mentioned are given a landscape here, but not the so-called bypass. And it gets worse!

The designed “scene boxes” are to be removable in the future, and a new scene of the same size could fill the gap. A highly modern concept, and there is a possibility that it might even be approved later as an eye-catcher in the living space by the spouse. Even the professionals are picking up on this design again and again.

On the photo, which is divided into four parts, you can see the implementation at “Upland.” “Am Lietwerk,” on the other hand, has open segment boxes, an enlargement of the track is possible at any time.

You may have noticed that I have not addressed the issue of a digital railway at all. I am not an opponent of this technology. However, a slow and controlled introduction to it would be highly advisable.

Don't let experienced model railway enthusiasts “impose” a system on you. Everyone is convinced of their own technology and goes their own missionary way, especially in the clubs and forums.



A transportable diorama is a suitable and manageable first work for learning and practising various techniques. Cleverly designed, it can be expanded into a small layout at any time, as illustrated here in four photos:

- Photo 1 shows the box for the later Upland motif.
- Photo 2 shows “Upland” with matching and removable background box.
- Photo 3 shows the insertion of “Upland” into the large bypass box.
- Photo 4 shows the complete raw state of the new layout.

Upgrading your track is possible at any time and the degree of satisfaction is more likely to increase, if you give yourself the chance to learn in meaningful and self-contained individual steps – quasi stages.

From all these explanations, a mnemonic sentence emerges that should not leave your thoughts: Give the important topics of design the time they need, and a result is achievable.

In the next episode I will show you the construction of a saw plan for our two layouts and other small track work.



This final photo montage shows the “Lietwerk” finished (left) and in shell (right).

For those who would like to “try out” a little landscaping until then, the **Trainini®** issues, from May to December 2017, are recommended. Another tip for planning a larger layout at a later date would be to take a look at the 2020 editions of **Trainini®**. The construction of “Diemelta” is documented there.

Webpages of the author:

<http://www.helenensiel.com>

Sources of supply for presented material:

<https://shopware.donau-elektronik.de>

<https://www.maerklin.de>

<https://www.mercur-versand.com>

<https://www.rokuhan.de>

<https://www.uhu.de>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Modellgebäuden Wirkung verschaffen **Ausflug in die Vergangenheit**

Wenn ein Buchtitel den Begriff „Perfektion“ benutzt, dann fühlen sich besonders erfahrene Leser angesprochen und wittern eine spannende Lektüre. In diesem Fall lenkt das Wort jedoch von der Zielgruppe ab und weckt das Risiko, den Buchkäufer zu enttäuschen. Woran der Titel krankt und für wen er dennoch sinnvoll und hilfreich ist, das arbeiten wir in unserer Besprechung heraus.

Thomas Mauer
Modellgebäude in Perfektion
Vom Bausatz zum Supermodell: Farbe, Details, Inneneinrichtung, Licht

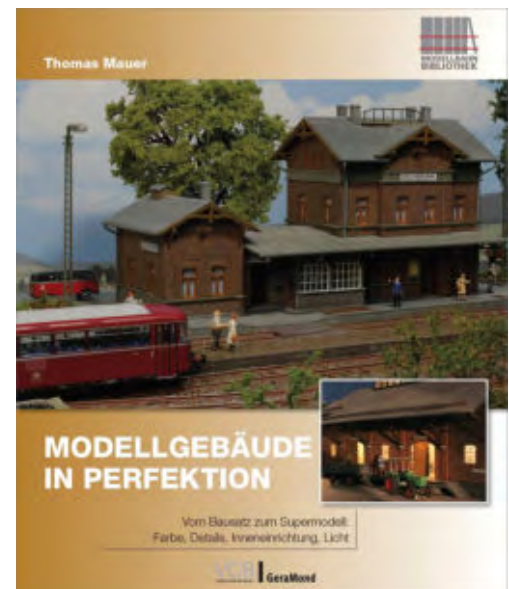
VGB Verlagsgruppe Bahn GmbH
München 2021

Gebundenes Buch
Format 22,2 x 26,5 cm
112 Seiten mit ca. 300 farbigen Abbildungen

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Preis 24,99 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

„Endlich Leben in der Bude!“, verspricht Autor Thomas Mauer in seinem Vorwort zum vorliegenden Titel. Das trifft völlig unseren Nerv und so sind wir gespannt auf die Lektüre. Am Ende können wir ein positives Gesamtfazit ziehen, doch dieses Buch lässt leider auch viele – zu viele - Möglichkeiten aus.



Das Themengebiet, das der Titel verspricht, scheint schier unerschöpflich. Es würde jeden Beitrag in einem Modellbahnmagazin sprengen: Gebäude lassen sich schon mit Farben verändern und aufwerten, mit eigenen Details individualisieren oder näher an ihr Vorbild rücken, mit einer Inneneinrichtung zum Blickfang des Betrachters machen und effektiv beleuchten.

Blicken wir auf den Umfang von gerade mal 112 Seiten, stellt sich schon vor dem Lesen die Frage, wie viele Fortsetzungen und Vertiefungen es wohl geben mag. Und genau hier liegt der Kern des Problems: Es werden viele Themen angerissen und vorgestellt, aber irgendwie bleiben sie alle an der Oberfläche. Das Buch bleibt eher mit vertanen Chancen als mit seinen Inhalten in Erinnerung.

Wie ein Synonym dafür stehen schon die Bilder im Vor- und Nachsatz: Sie zeigen eine Laderampe und eine Gleiswaage, die allenfalls als Randerscheinung mit Gebäuden zu tun haben. Und so fährt der Autor zwar ganze 18 Kapitel auf, aber wenn wir inhaltliche Überschneidungen und „Randnotizen“ streichen, dann wird die Luft schön dünn.

Der Verlag schreibt in seinem Kurzportrait: „Das Angebot an Gebäudemodellen für die Anlagenausstattung ist schier unüberschaubar.“ Dem stimmen wir zu, nur vermittelt das Buch eben das überhaupt nicht.

Die moderne Laserschnitt-Technik, die sich immer größerer Beliebtheit freut, ist gerade ein einziges Mal in diesem Werk vertreten.

Die sich mindestens ebenso rasant verbreitende 3D-Druck-Technik kommt in diesem Buch überhaupt nicht vor, auch den Kleinserienklassiker des Resingusses, lange Zeit nicht nur von Artitec praktiziert, suchen wir vergebens. Allenfalls ein vollständiger Eigenbau oder auch der Papiermodellbau wären in diesem Werk wohl wirklich fehl am Platze gewesen.

Und so spielt der verbleibende Plastikmodellbau mit Polystyrol hier nicht nur eine Hauptrolle, sondern fast den Alleinunterhalter. Einer Technik aus dem letzten Jahrhundert, die langjährige Modellbahner bestens beherrschen sollten, bietet Thomas Mauer hier eine Bühne für ihre Abschiedsvorstellung.

Nur ein einziger Hersteller hält mit seinen Marken bis heute verbissen an ihr fest und ignoriert beharrlich die moderneren Alternativen. Genau diesem widmet der Autor übrigens fast das gesamte Buch und macht Markenklassiker wie Faller oder auch Auhagen zu Statisten am Rande.

Gerade in der ersten Hälfte der Lektüre haben wir mehrfach das Buch zugeschlagen, um den Titel noch einmal zu überprüfen. Wir waren unsicher, ob wir hier nicht ein bezahltes Firmenportrait vorliegen haben. Das gibt es nämlich auch, stammt vom gleichen Autor und ist im selben Verlag erschienen. Dieses Buch könnte daher eine Resteverwertung nicht veröffentlichter Kapitel sein.

Das klingt vielleicht böse, ist es aber nicht. Es zeugt nur nicht von journalistischer Sorgfalt und dem Ziel, breit zu informieren. Der Autor entwertet seine Arbeit selbst, indem er sie zu nah an einen einzigen Anbieter im Markt rückt. Und damit kommen wir nun zum Thema Licht, das hier auch ausführlich behandelt wird.

Die Zeiten, in denen ein Glühlampensockel mittig in ein Haus gestellt wird und alle Fenster be- und Wände durchleuchtet, sollten lange vorbei sein. Abschirmmaßnahmen für Mauern, dunkle Zimmer und ein Blick auf Möbel und Menschen in beleuchteten Räumen sind das, was modernen Modellbau ausmacht.

Das wurde richtig erkannt und wird durchaus mit guten Ideen präsentiert. Unglücklich erscheint aber der Hinweis, dass es beim lang zurückliegenden Bau vieler Häuser noch keine Alternative zu (Mikro-) Glühlampen gab – also doch alter Wein in neuen Schläuchen? Auch bei den neuen Angeboten scheint die Zeit stehen geblieben zu sein, denn wieder beschränkt er sich unnötig auf den einen einzigen Anbieter, dessen Häuser er hier schon fast aufdringlich bewirbt.

Was die jüngere Vergangenheit an Effektbeleuchtungen, darunter startende Leuchtstoffröhren oder Fernsehsimulationen, hervorgebracht hat, werden Sie in diesem Buch nicht finden. Dieser Platz wurde für Laderampen und Gleiswaagen verschenkt.

„Alte Hasen“ und marktkundige wie versierte Modellbahner können auf dieses Buch daher getrost verzichten, weil es ihm an Vielfalt fehlt. Neu- und Wiedereinsteigern bietet es hingegen eine gute Plattform, denn es ist ein Anfang und ein guter Start ins neue Hobby.

Die Schritt-für-Schritt-Anleitungen und auch ein durchdachter Umgang mit verschiedenen Farben liefern für diese Zielgruppe wertvolle Informationen. Sie erleichtern den Einstieg ins Hobby, ohne die Themen zu überfrachten und ihren Leser zu überfordern. Und damit passt dieser Titel sehr gut zum Start unserer kleinen Artikelreihe für Anfänger.

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: Publishing pages with reference possibility:
: <https://www.vgbahn.shop>
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Legendärer Mercedes-Benz /8 Das Erfolgsauto mit dem Stern

Autos waren das Sinnbild für den Wirtschaftsaufschwung nach 1949 und bezeugten den Status ihrer Besitzer. Einen Stern auf der Haube konnte sich wahrlich nicht jeder leisten, als Taxi ließ sich aber durchaus in solchen Genuss kommen. Und doch wurde der Strich-Acht zum ersten millionenfach verkauften Mercedes-Benz. Damit gehört er auch auf die Modellbahn und hat sich ein eigenes Portrait wahrlich verdient.

Alexander F. Storz
Mercedes-Benz/8
Der Millionen-Seller

Motorbuch Verlag
Stuttgart 2020

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Alexander Franc Storz, Jahrgang 1965, hat Geschichte, Deutsche Literaturwissenschaft und Philosophie studiert. Er arbeitet als Journalist im Bereich Automobilhistorie für verschiedene Fachmagazine. Auch als Buchautor ist er uns aus dem Motorbuch-Verlag durch einige Titel bekannt.



Der Name verspricht uns eine gute Besetzung für das Portrait eines Autos, das eine Legende geworden ist und einen Meilenstein in der Firmengeschichte von Mercedes-Benz darstellt. Leider klafft hier im Maßstab 1:220 immer noch eine Programmlücke.

Gerade mal das Coupé hat es als Modell ins Märklin-Programm geschafft. Die viel wichtigere Limousine, die als einziger Mercedes in Deutschland sowohl die Taxi-Farben Schwarz als auch Hellelfenbein getragen hat, lässt bis heute weiter auf sich warten.

Das vorliegende Buch kann die Lust auf ein solches Modell bei den Modellbahnern wecken und vielleicht auch einschlägige Kleinserienhersteller auf den Plan rufen: Dieses Auto fehlt und das zu besprechende Typenportrait führt es auch dem letzten Autofreund vor Augen.

Dieses vollmundige Fazit gleich zu Anfang wagen wir, weil der fachlich versierte Autor keinen der wichtigen Aspekte auslässt, die diesen Fahrzeugtyp zum Erfolgsmodell (und heute beliebten Klassiker) gemacht haben.

Für viele unserer Leser wird er damals ein Traum gewesen sein, der im Gegensatz zu manch anderen noch das Potenzial hatte, Wirklichkeit zu werden. Wer dienstlich reisen musste und ein Geschäftsauto bekam, der wird sicher Bekanntschaft mit ihm gemacht haben. Alle anderen haben zumindest mal als Taxi in ihm gesessen.

Der Strich-Acht war und ist sprichwörtlich bekannt wie ein bunter Hund: Als er 1968 erschien, kannte ihn schon bald jeder. Diese Mittelklasse-Limousine war damals eben auch der Mercedes für Markeneinsteiger.

Mit knapp zwei Millionen gebauten Exemplaren brachten es die Vier- und Sechszylinder-Baureihen zusammen auf die Gesamtzahl aller vorangegangenen Mercedes-Nachkriegsbaureihen. Und heute treffen wir historische, bestens gepflegte Fahrzeuge auf Ausstellungen, wo sie stolz vorgeführt werden.

Alexander F. Storz versteht es wieder einmal, diese besondere Geschichte zu strukturieren und zusammenzufassen. Er beschreibt die Generationen der Mittelklasse-Fahrzeuge, die die Marke mit dem Stern nach 1945 angeboten hat und ordnet diesen Erfolgstyp in den geschilderten Kontext ein.

Gefolgt wird dieses Kapitel von seiner eigenen Entwicklungsgeschichte sowie den ausführlichen Vorstellungen der einzelnen Baureihen und der folgenden Modellpflege. „Die nüchternste Form der Perfektion“ beschreibt dieses Auto kurz, aber absolut treffsicher. Nicht vergessen wurde auch das bereits erwähnte Coupé – das erste überhaupt auf Basis der Mittelklasse bei Mercedes-Benz.

Doch was wäre eine solche Abhandlung ohne den Blick auf all die Kuriositäten oder auch Alltagsbegegnungen? Taxis im In- und Ausland, Kilometer-Millionäre, Umbauten oder auch Sonderfahrzeuge wie Bestattungswagen und Ambulanzen belegen, wie vielseitig das Gesicht des Strich-Acht in aller Welt einmal gewesen ist.

Unterlegt mit technischen Daten und einem weiteren Kapitel, in dem er sich als Versuchsträger zeigt, wird das Portrait rund und schlägt sogar eine Brücke in die Zukunft zu seinem Nachfolger, ebenso schon lange Geschichte ist (und auch noch eine Buchwürdigung in diesem Verlag findet).

Eine gute Mischung aus Werks-, Werbe- und Alltagsfotos unterstreicht die vielen Facetten, die sich in diesem Buch wiederfinden und zu einem runden Ganzen verschmelzen. Den meisten ist ihr Alter anzusehen und besonders Alltagsaufnahmen mit Auto aus dem Familienalbum sind handwerklich häufig nicht perfekt.

Aber – und das ist entscheidend – sie sind und wirken authentisch. Schließlich liegt keine historische Werbebroschüre der Daimler-Benz AG vor uns. Die Begeisterung für dieses Auto, die immer zu spüren ist, wirkt ehrlich und packend. Und so sind auch der Lesefluss und das Lesevergnügen von der ersten bis zur letzten Zeile gegeben.

Bei Freunden historischer Automobile und besonders dieser Marke sollte dieses Buch im Regal stehen. Modellbahner mit automobiler Bezug und Wunsch nach zeitgenössisch korrekter Anlagenausstattung sollte es ebenso wenig fehlen.

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: Publishing pages with reference possibility: :
: <https://www.motorbuch.de> :
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Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Praise for the May issue and our website:

Thank you for another very interesting issue! And the new homepage doesn't look bad either.

(...) And another question: Couldn't the button "Download" be labelled in the correct German "Herunterladen"?

Kyrill Makoski, by E-Mail

Response from the editors: We were very pleased to receive this praise and a lot of other enthusiastic feedback, including suggestions for improvement, about our new website. We did not receive all of them as letters to the editor intended for publication. The response has overwhelmed us, and gives us fresh impetus. We have immediately implemented the re-labelling of the button, as well as other suggestions for refinements to the design from other readers.

Our new pages inspire:

Congratulations on the new website. From my point of view, you have succeeded very well in implementing the "design language" that we are used to from the magazine.

The website not only looks modern, it is also clearly structured. Apart from the short presentation of the individual issues of your magazine (Magazine > Issues > [Year]); I also like the news section (Current News > News Articles).

For this, a very big praise – besides the already obligatory thank you for your tireless work around Z gauge.

Thomas Heß, by E-Mail




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Trainini 05/2021
Issue 190

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Last change: 06/28/2021
File size: 6.2 MB
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Topics:

Model
Revised NOHAB from Märklin
A new Heavyweight
MAN 630 from Ratimo-Z
The perfect Complement

Technology
Guide to Model Railway Photography (Part 1)
Successfully photographing Model Railways

News
Staying in the conversation (part 3)
HOS Modellbahntechnik: To live for Model Making

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Feedback and wishes for the new webpages:

Congratulations on the redesign of the Trainini website. You have put a lot of work into it, well done!

Overall, the site now looks much more modern. I especially like the eye-catching photos in black and white. I'm a big fan of b/w, the photos have much more flair. Of course, the search function via the issues is also great.

I think it's a shame that your previous craft projects haven't found a place. You put so much work and effort into them and made interesting things. That should not be left behind.

It is also a pity that the page does not scale appropriately for normal PC screens. For example, the providers' page (<https://www.trainini.de/spur-z-welt/anbieter>) is limited to 3 columns, 6 would probably fit on all standard 16:9 screens, 8 would fit on my screen. It's clear that laptops and mobile phones have small screens. And if everything were already perfect, you wouldn't have anything left to improve. :)

I wish you all the best for the future, it's unbelievable what you do in your spare time. Many a paid magazine can take a page out of your book.

Kay Anbuhl, Berlin

Editor's reply: We have reviewed the link lists under the page heading "Spur-Z-Welt" in response to this tip and have reworked them. Instead of three entries per line, we have now placed four entries per line, which turned out to be optimal with the tested monitor and browser settings. The page layout otherwise adapts to the output device and changes the display for 4:3 screens or devices with a portrait display such as mobile phones.

Handicraft projects or model ideas are not to be permanently omitted. Together with gallery pictures from readers and also film contributions, they are planned as a further expansion stage, which we will add to at a time yet to be determined.

Prototype detail explained on Märklin tank models:

Unfortunately, I only now had the chance to read your exciting magazine. I noticed the following small details in your article: The additional device on the Leopard 1A1's visor is not a thermal imaging device, but a classic aiming spotlight with white or infrared light. It had a range of up to 800 metres.

The device was also used on the other variants up to and including the 1A4 and also on older types such as M47, M48 or Kanonenjagdpanzer (cannon fighter tank). In all types there were infrared night vision devices, which were then installed at night in place of the glass angle mirrors. I was still shooting with the headlamp on the Leopard 1 until 1982. "Real" thermal imaging devices were not available until the 1980s with the 1A5 and the Leopard 2.

The Leopard 1 main battle tank was developed in the 1950s. The first batch (0-series) had already been in service on military training areas since 1963. The first series models were handed over to the then Minister of Defence (Kai-Uwe von Hassel) in 1965.

From this point of view, there could well have been SSy 45s that had not yet been redesignated in 1965, or they were transporting the 0-series from the factory to the tank troop school in Münster in 1963. So, all that remains is the inconsistent running gear of the Sym45.



In 1988, the Bundeswehr's M48 battle tanks are loaded in Stetten am kalten Markt. The precision with which the extra-wide vehicles are moved onto the heavy-duty trucks can be guessed. At the back of the turret the headlight box can be seen almost in the middle, which is on the front of the Leopard 1A1 (Märklin model). Photo: Christian Liebau

Nevertheless, I am highly satisfied with both models, especially with the level of detail of the “Leos,” you can even see the tools attached to the outside and small maintenance openings, as well as the protruding exhaust trims, super!

Attached is a photo of the loading of the M48 over a head ramp onto such wagons, 1988 in Stetten am kalten Markt (...). The big square box on the turret of the main battle tank holds the mentioned headlight, it was also used on the Leopard 1 as you can see on the models. (...) From the driver's point of view, loading was always a great challenge, because the extra-wide tanks had to be kept on track with centimetre precision in order not to exceed the loading gauge.

Instead of using modelling clay, we then fixed the tanks (...) with wedges and crosswise tensioned chains on floor rings that were manually tensioned with spindles. I think there was even a model chain in 1:220 (...).

The ambulance squad depicted by Mr. Schnittjer in the May 2021 issue was obligatory during loading, you could also use an M113 from Z-Panzer, for example. Normally, the escort vehicles drove on the road, but “on the set” there was always the VW-181 bucket truck or the MB off-road vehicle of the company commander, a 2-ton Unimog for supply (with food and for the chains and wedges), and a recovery tank, in the eighties also on Leopard 1 basis (...).

The soldiers were accommodated in a “nicely aged” 2nd class car, e.g., a green 8721, just like Mr Schnittjer did. Harnessed was the BR 216 or 218 in old red (strangely enough, I have seen the beige-blue ones very rarely in southern Germany).

Christian Liebau, by E-Mail

Editor's reply: Our reader did some more research in further correspondence and confirmed that the tank was not equipped with the aiming light until the 1970s, which is why a wagon marking before 1964 does not fit this version. A three-part wagon pack is also not “accurate.” It would have made more sense to have at least four wagons and tanks, which would have been a tank platoon (Gruppe --> Zug --> Kompanie; Group --> Platoon --> Company) in the structure of the time. A full company would have consisted of twelve tanks plus a chief tank, which would have resulted in as many as 13 tanks loaded onto 13 heavy-duty vehicles.

Reaction to the photo course in the May issue:

I tried to implement Dirks Kuhlmann's photo tips, and put the photos in my cloud. I uploaded the original photos as well as 1 - 2 details per photo.



This is one of the results of photographing the model railway after tips from Dirk Kuhlmann. Photo: Jörg Endreß

The cars are Märklin's infamous "erasers," which I spiced up with some paint. I treated many parts as well as the insides of the 4 Eaos with rust paste and rust fluid (mixed), which I had found in a craft store.

Jörg Endreß, Bremen

We had to answer the following reader question:

As I have been an avid reader for years and have also taken some tips from the magazine, and am currently preparing a layout, I would like to ask today:

Is there an easier way to get rid of the print track grid on 3D printed models, e.g., car models, without a lot of filling and sanding, as this is actually impossible with the moulds of, e.g., vintage car models without losing the basic shape.

But maybe one of you has already had this experience and experimented a bit and found a suitable solution. I am grateful for any tips.

Peter Lambert, München (Munich)

Editor's answer: The print mark grid cannot be prevented in 3D printing. It is only visible to a greater or lesser extent depending on the printer and its resolution, the printing process and the print orientation in the output device. We do not see any alternatives to sanding, but sometimes it is possible to do without filler, which is applied by hand. It is conceivable to use fine spray filler with the spraying apparatus, a method that is also used in car construction for the prototype - there this layer is called "filler" for smoothing the finest unevenness.

Of course, this requires preliminary tests, suitable material and a little practice. With the following suppliers of car models (in alphabetical order) we have so far found – thanks to resin as a printing material – no, or hardly any need for reworking: A2 Models (Netherlands; only for sale now (no new models)), EtchIT-Modellbau, NoBa-Modelle, Zcustomizer and Z-Doktor Modellbau.

Novelty in preparation at Ratimo-Z:

Rainer Tielke (<https://www.rainer-tielke-modellbau.com>) is currently working on a new product that he showed us when we corresponded about the article on his MAN 630 in the last issue. This is another vehicle from military service, the Willys Jeep.



Framed by MAN 630s in the foreground are two Willys Jeep models that will soon enrich the Ratimo Z range. They can be seen with the windscreen upright (left) and folded forward (right). Photo: Rainer Tielke

It will be offered in different versions, both as a recreational vehicle in different colours and in the widely known military version. Both will be available with the windscreen up and the front folded down. The delivery date is not yet known to us.

Märklineum finally opened:

Without celebration and big announcements, the exhibition in the Märklineum was opened on 18 June 2021. Previously, it had been unclear for a long time when Märklin's company museum could be opened to the public to its full extent. At last, the latest infection figures allowed the long-awaited step to be taken. Nevertheless, a large opening ceremony had to be dispensed with. Märklin announced that it would make up for this as soon as it could be reliably planned and carried out.



Managing Director Wolfrad Bächle cuts the red ribbon, symbolically opening the way to the company exhibition and the Märklineum's large display. Photo: Märklin

The exhibition can be visited from Tuesday to Sunday between 10:00 and 18:00. Due to the necessary limitation of the number of visitors to the permitted extent, tickets can only be purchased electronically in advance. For this purpose, there are two time slots daily for admission, in the morning from 10:00 to 13:30 and at noon from 14:30 to 18:00. The link for ticket sales is: <https://www.maerklineum.de/de/besucherinformation/ticketshop/>.

A local film report on the opening and the exhibition is available on Filstalwelle at the following link: <https://filstalwelle.de/video/2021-06-18-bahn-frei-fuers-maerklineum>.

Redutex films now available through 1zu220-Shop:

With the 1zu220-Shop (<https://www.1zu220-shop.de>), a German distributor with a wide reach is now also active in the distribution of Redutex texture foils for model making. These products, which originate from Spain, became popular in larger gauges through the accessory manufacturer Noch, who, however, only offered a selection of Redutex products.

For the articles that can be used in 1:220 scale, we were previously unaware of any supplier in Germany, and importing them ourselves from Spain was usually not in a sensible ratio between material value and shipping costs.

The specialist dealer who is certainly best positioned for the Z scale has now sensibly expanded its range, and offers a range of brands for this on no less than five Internet pages. Houses, roofs, walls and even roads can be designed with the printed and texture foils. There are hardly any limits to one's own creativity.

The direct link to the offer is: <https://www.1zu220-shop.de/Redutex:::1100252.html>.

New from American Z Line:

In English, interested readers can follow a conversation between Robert J. Kluz (distributor) and Hans Riddervold (co-owner of AZL) about the on-going projects of this manufacturer. The video for May 2021 can be found at the following link: <https://youtu.be/-jDGTUCWyVc>.

Of course, there are also new deliveries to report: The EMD E7 diesel locomotive has now reached the dealers as an A-unit in the almost black-looking Brunswick green livery for the Pennsylvania with five yellow decorative stripes (item no. 64605-1 / -2).



Another version, namely a red and silver painted version of the Southern Pacific A and B unit (64601-1_SET to -3_SET) belongs to the Golden State passenger train that travelled between Chicago and Los Angeles for 66 years.



EMD E7A in Brunswick green (Item no. 64605-1; Photo above) and Golden State train set (64601-2_SET; Photo below). Photos: AZL / Ztrack

The prototype can also be purchased as a ten-car train set (64601-1 / -2) in slightly different designs. Nine different cars to complete the train sets with different car numbers are also available for purchase individually (73048-5 to 73948-1).

The 8,000 gallon tank car from 1917 is now offered in black paint with Koppers lettering. Available are a single car (915003-1) and sets of two (915033-1) and four (905033-1), each.

Manufacturer photos of the current deliveries can be found under <https://www.americanzline.com>.

Convincing Artitec model delivered:

Artitec recently delivered a vehicle designated as "BRD MAN AE Cargo" (item no. 322.032). This truck belonged to the early equipment of the German Federal Armed Forces and is finely detailed, matt and colour-coordinated.

All in all, the model makes a very realistic and well-done impression. It reproduces the MAN 630L2 AE 5t mil gl/glw truck with two-piece 5 m unit sprayer (from year of manufacture 1960/61), which was once most frequently used by the Bundeswehr (German Armed Forces). Characteristic features are the rear single tyres, and a (front) cable winch as additional equipment.



The new Artitec model of the MAN truck for the Bundeswehr (art. no. 322.032) looks successful in every respect: Detailing, paintwork and lettering.

The prototype belongs to the same vehicle family as the Ratimo-Z models presented in the last issue. Nevertheless, they are a supplement and not a competing product, because different versions of the MAN-630 series were reproduced by both suppliers.

Suggestions for use on the home layout, based on Christian Liebau's letter to the editor, were planned for this issue, but had to be postponed at short notice due to unplanned events. They will appear at the next opportunity.

New DDR model at EtchIT-Modellbau:

EtchIT-Modellbau has introduced a new vehicle as a novelty based on a prototype from the former GDR. The RS01/40 "Pionier" wheel tractor (art. no. XD183_Z) is an important milestone in socialist tractor construction in the 1950s.

The kit produced in the DotHIRES process, which contains all the necessary 3D printed parts in unpainted form, was implemented as an electric starter. This is recognisable by the battery boxes on the left and right in front of the (removable) driver's cab.

The new heavy-duty low-loader trailer (XD130_Z), which offers plenty of space for heavy vehicles and other goods, is also produced in the same way. The model includes drive-on ramps and trestle supports. A suitable tractor unit (XD016_Z) has also been in the range for some time.

These and all other new products for all gauges can be viewed and ordered on the following page: <http://etchit.bplaced.net/data/etchIThome/index.html>.

Wagon exclusive pack available at the 1to220 shop:

A freight car set produced by FR Freudenreich Feinwerktechnik exclusively for the 1zu220 shop (<https://www.1zu220-shop.de>) is now available for delivery. Ordered about a year ago as a "Corona demand boost", Jörg Erkel was now able to receive the delivery and supply his pre-orderers.



The new wagon set "Era III Part 1" (item no. 49.019.04) is available in four parts from the 1zu220-Shop, produced by FR Freudenreich Feinwerktechnik. Photo: Jörg Erkel / 1zu220-Shop

The "Wagon Set Era III Part 1" (Item No. 49.019.04) consists of the following four models: a chrome oxide green service freight car of the type "Oppeln" with brakeman's platform of the DB, an open freight car Omm 32 (ex "Linz") of the DB, an aluminium-coloured pointed roof car series F of the FS with EUROP lettering, and a covered freight car Gms 54 of the DB.

The composition is intended as (first) part of a transfer train for main and branch line traffic. Only the service freight car should then be exhibited and used separately – for example in an auxiliary train – if the model railroader wants to stay close to the prototype: Service cars were not intended for public transport, and, therefore, did not run in the train reproduced here.



If there is sufficient interest, a second set of wagons of similar size but with different types of wagons is planned. With it, the wagon group should then grow into a stately unit.

The current deliveries from Micro-Trains:

The fourth tank wagon of the "Sweet Liquid" series with inscriptions from Miranda Sugar (art. no. 530 00 520) was added by MTL. The prototype had a length of 39 feet, the basic colour of this wagon is black, on which the inscriptions are printed in white.



The tank wagon "Miranda Sugar" (Item No. 530 00 520; Photo above) is wagon number 4 of a series. This example of the ICG (533 00 51; photo below) also belongs to the recently delivered bulk freight wagons.

Open bulk freight wagons with 33 feet prototype length in black (Southern) and brown basic colours (Southern Pacific) are now available for and in delivery (533 00 151 / 152 & 534 00 121 / 122). Particularly striking are the Illinois Central units (533 00 051 / 052), which in addition to white service markings also have striking orange adjuster markings and logos.

Micro-Trains products are distributed by Case-Hobbies (<http://case-hobbies.de>), among others.

There is something going on at Busch:

In the middle of the month, the accessories manufacturer Busch from Viernheim announced that it had found a new majority shareholder in MEG Modelleisenbahn Gesellschaft mbH in Weiden. The Vallen and Schumann families sold their shares to the new owner with retroactive effect from 1 January 2021.

This young group of companies in the model railway sector had already taken over Silhouette Modellbau GmbH (with the additional Mininatur brand) in recent years, and Lenz Elektronik GmbH in 2020. Busch, the latest acquisition, is now the third in the group.

According to company information, the idea of integrating Busch into a group of companies and, thus, maintaining it permanently arose in the context of the search for a successor for Jörg Vallen as the previous majority shareholder. Accordingly, it was not possible to find a successor within the family. The external image of Busch GmbH & Co. KG and the management by Jörg Vallen will, therefore, not change.

Märklin deliveries for June:

With the local train set (Item no. 81356) Märklin has finally delivered a long-awaited new item. With it, the class 141 electric locomotive now comes into the normal catalogue programme via an MHI special series.

In addition to the chrome oxide green painted locomotive for Era IV, the new products delivered include a three-car local train made of silver pieces. The driving trailer with Karlsruhe head without orange warning stripes seems strange at first, but it is based on the first prototypes of this new head shape. We will discuss the design, weaknesses and strengths of the locomotive and wagons in more detail in the next issue.



The recently delivered 212 067-3 in its original livery (item no. 88214) is intended for a construction train whose coaches have not yet been delivered.

At the same time, another novelty, the crimson class 212 diesel locomotive (88214), which was announced this spring, is coming into the trade. The well-done and well-running mainline locomotive is intended for a wagon pack (construction train), which is still waiting. For the first time, this model makes the extremely popular locomotive available individually in its original livery, but with the markings for Era IV.

At the same time the class 37 (88032), the former class 24 steam locomotive, was in service in the east of the present-day Federal Republic of Germany. With warm-white LED lighting, functioning linkage, and brake replicas as well as a bell-shaped armature motor, it also shows itself to be technically up-to-date. Behind its EDP number 37 1009-2 is the later, and still preserved, museum locomotive 24 009, which was later sold to the West and kept operational there.

At home in Era III is the E 18 of the DB in steel blue livery (88088) with (only) a silver-coloured separating stripe between frame and body. This is the standard livery of the express locomotive, which was missing as a model until today. Despite always having good driving characteristics, Märklin was able to improve on this thanks to product update, because the new model now also has a bell-shaped armature motor, in addition to warm white LED lighting.



With E 18 32 (88088) Märklin has now delivered an important, and, so far, not previously offered version of the shapely express train locomotive. With it, another running gear was converted to a bell-shaped armature motor.

More modern is the unloaded car transport wagon DDm 915 (87094), which also comes from the DB stock. With its chrome oxide green colouring, it corresponds to the operating condition of Era IV, and is perfect for supplementing passenger trains transporting (passengers') cars.

The two-axle old construction tank car with brakeman's cab (82325) has also arrived at the dealers. Its ultramarine blue paint scheme and lettering identify it as the property of BV-ARAL AG Bochum. Its prototype was employed by the Bundesbahn.



The ultramarine blue Aral tank car (82325; photo above) complements the previously released model. The enclosed Büssing truck was apparently newly designed, because it has some functional and shape differences to the previous model. Intended for modern freight traffic is the three-part pack of sliding wall cars (82418: photo below) from different owners.

It was already announced with the 2020 spring new products, after its grey predecessor with brakeman's platform, which was also designed to match Era IIIb, was surprisingly well received by our readers and other customers.

Like the predecessor model, the now delivered wagon also has a separately applied Aral company logo, the so-called diamond. This time, the delivered new products also include a Büssing truck with an Aral tank loaded on its flatbed. What is new about it is that it has rotating rubber wheels and a glass insert in the driver's cab.

The last new product delivered is a sliding wall car set (82418) for Era VI. It consists of three large capacity sliding wall cars in current operating condition. These are one each of Habbillnss (SBB Cargo), Habbiiins 17 of Transwaggon Zug/Switzerland and Habbillnns of Rail Cargo Austria.

The construction of the three models is still a very new form, which was previously only offered as wagons of the Swiss Post and differs significantly from the previous offers in the shapes of the outer wall panels.

Bad news from Z-Modellbau:

On the pages of the small series manufacturer Z-Modellbau (<http://z-modellbau.de/de/>) from Chemnitz, we have now found sad certainty on a subject that we had already feared after an order rejection. The

manufacturer writes there: "Due to my poor health, I regret to inform you that I can no longer accept orders."

One of the most exciting chapters in the small series history of our gauge could thus come to an unexpected end. We would like to remain hopeful and at this point wish Dr. Andreas Scheibe only the best for a full recovery of his health.

Noch delivers new products:

At Noch, the detail markers (art. no. 61155) are currently available, consisting of six paint pens in the colours black, white, silver, yellow, red and brown. Water-based acrylic paint from their 2 mm wide tip gives coloured accents to tyres, window and door frames, insulators on telegraph poles, metal parts or goods wagons.



The five detail markers (art. no. 61155) will help in future with some colour work on the layout or also on models. Photo: Noch

They adhere to many substrates such as metal, plastic, wood, paper, glass, ceramics and dry matt. They are preferably used where it is not worth opening a paint can and having to mix and clean the tools after small jobs.

Interesting for Z gauge can also be the new pine trees (21992) thanks to their height of 8.5 and 11.5 cm. Although they are stated as for H0, TT and N gauges, Zetties can benefit here from the fact that trees are clearly reproduced too small almost through all scales. So, if you want to recreate Mediterranean landscapes to recall your last holiday, you don't have to give this product a wide berth even in small scale.

Possible problems with wood procurement:

Readers from many regions report a shortage of wood at carpenters and DIY stores. We have also had to make such experiences in one of our projects. Although it is primarily construction timber that is affected, there are also delivery and supply problems with plywood, MDF and even chipboard.

Unfortunately, this also has an impact on our hobby. The “Landtag intern”, parliamentary magazine of the state parliament of North Rhine-Westphalia, also recently reported these shortages for the most populous federal state. Industry trade journals are also currently dealing with wood shortages and their causes (example: Verkehrsrundschau (Transport Review) / <https://www.verkehrsrundschau.de>).

Various sources mentioned damaged wood from the last two summers, which were too dry and could not be taken out of the forests in time, that was not (no longer) usable. A strong increase in demand could be observed, especially in the United States of America and China.

The situation is aggravated by the fact that national forestry enterprises have entered into long-term supply commitments in favour of these two countries, which make it even more difficult to satisfy domestic demand, which has also risen again in the meantime.

Model railway enthusiasts of all gauges are certainly well advised not to plan too short term, and too tightly at the moment, to examine possible alternatives, and to remain able to work in their own hobby room, thanks to a forward planning.

Useful new products from Schrax:

New in the range of this small series manufacturer (<https://www.schrax.com>) is a 200-litre steel barrel. The models are made of synthetic resin and are produced in various colours. They are supplied in the regular colour assortment. However, please contact the supplier if you have any other colour requirements.

The basic form of the roller beaded drum shown here was invented shortly before 1900, which makes the new models flexible in use. Mostly fuels, lubricants and chemicals are stored or transported in them. To make this visible in the miniature, a closed version, and an empty barrel without a lid are offered.



200-litre steel drums in various scales, designs and colours are now available from Schrax. The H0 gauge examples are shown here, as an example. Photo: Schrax

The walls have been reproduced as filigree as possible. However, the peeping through of the material can be easily and effectively remedied with a little paint. This makes it the better alternative to visibly out-of-scale wall thicknesses.

Current new products from NoBa-Modelle:

Shortly after the May issue was published, we received information about the following new NoBa products (<https://www.noba-modelle.de>). We would like to mention in more detail the Büssing President 14 with Emmelmann body, which goes back to a customer suggestion. Thanks to good contacts, we were able to assist in finding original documents.

The model, which is very successful and whose only problem is the flush reproduction of the windscreen in order to preserve the characteristic shapes, is available as a resin blank (art. no. 6378R) and as a finished model (6378RF). We are planning a short superstructure proposal for this model in a later issue.



The Büssing President 14 service bus from NoBa-Modelle (item no. 6378RF) has an Emmelmann body. Photo: NoBa-Modelle

The MAN 6 x 2 refuse collection vehicle (6377R / 6377RF) for modern model railways also looks appealing. Motorbike enthusiasts will be delighted with the addition to the range of the Royal Enfield Intercepto (6959) and the Continental (6960R).



The refuse collection vehicle (6377RF; photo left) has a current prototype. The concert stage 10411R; photo right), next to which the WC wagon (6152R) can be seen cut in, has a somewhat broader temporal application. Photos: NoBa-Modelle

Agriculture and market gardening are newly served with the following products: three fruit crates (10319R), small harvest tractor (6616R), a matching trailer with fruit crate (6617R) and small harvest tractor with trailer and fruit crate (6618R).

The new concert stage (10411R) will be used for music events, and the toilet trolley (6152R) that is to be installed is a result of official requirements. The scene can be framed with four deciduous trees without foliage (10320R), which allow a great deal of freedom in the hanging of leaves. Other new products are a field shed (4125), "Food Truck" (6206R) and the auto transporter (6062R).

Further new products available from the end of June:

From the end of this month, the two new products from Modellbau Laffont, which we presented in February, will be available. The boat repair building (art. no. Z3201) for epochs III to VI features finest engravings in solid-coloured architectural hardboard and has (without roof overhang) basic dimensions of 101 x 60 x 35 mm.

The modern commercial hall with rolling gates (Z3301) is manufactured in the same way as a kit and can be represented with open or half-open gates. Usable from Era IV, the basic dimensions here (again without roof overhang) are 109 x 65 x 35 mm.



The boat repair building (art. no. Z3201; photo above) and the modern commercial hall with roller doors (Z3301; photo below) are valuable commercial buildings for many layouts and are now available. Photo: Modellbau Laffont

The lasercut kits can be found and ordered on the following page: <https://modellbau-laffont.com/c/bausaetze-spur-z-massstab-1-220>.

Useful tools for DIY projects:

Bending tools are useful aids for self-builds and for assembling kits from etched parts. These consist of two-dimensional metal sheets that are cut, bent or folded to give them their final shape. They are joined and reinforced by gluing or soldering.

If something has to be bent or folded, the grip should be right, because nickel silver in particular does not allow any corrections or reworking – the material could quickly break. Precision and feel are, therefore, required.



The mini needle-nose pliers from Revell (art. no. 39079; left), the moulding pliers for photo-etched parts (300074067; centre) and the needle-nose pliers with scratch protection (300074065; right), the latter two from Tamiya, are presented to you as valuable tools for model building.

This is where special and often expensive bending aids come into play. In many cases, bending pliers can be a good alternative or a useful addition. We would like to briefly introduce three different examples at this point:

- Mini Needle-nose Pliers from Revell (Art.-Nr. 39079)
The smallest and cheapest model is well suited for gripping and shaping the smallest parts such as steps on Z-gauge models. Where it is also too big, only tweezers or tweezer pliers will then help.
- Moulding pliers for photo-etched parts from Tamiya (300074067)
Thanks to its narrow gripping surface, it offers good accessibility in many applications. The angular shape of the outside of the pliers also allows very precise bending at a 90-degree angle.
- Needle nose pliers with scratch protection from Tamiya (300074065)
At first glance, they look like ordinary pliers, which is basically what they are. What sets them apart are the replaceable Teflon gripping surfaces in the pliers' jaws. They protect sensitive parts from being scratched when they are gripped with them.

It, therefore, not only provides valuable help when working on many fine etched parts, but is also recommended when electronic parts or other components are involved that need to be handled carefully.

Herpa new products for autumn 2021:

Herpa has now announced the two Boeing 757-200s as new items for the months of September and October 2021, which we had already announced (with item numbers) in the February issue. They are now joined by the Boeing 747-400 as "Ed Force One" from "The Book of Souls World Tour 2016".

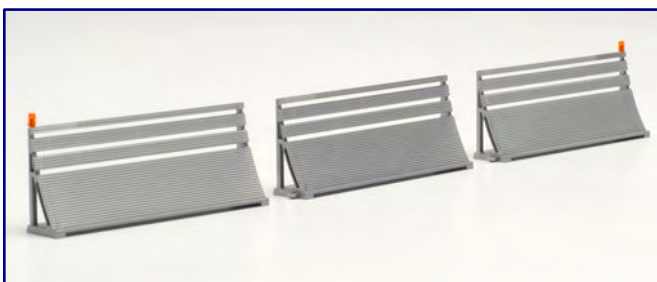
Hard to fit on standard model railway layouts, it will appeal all the more to friends of this music group. It is offered as a Wings model (art. no. 571609; with attachable chassis parts) and as a Snapfit model (613293; different scale 1:250).

Other Snapfit models, but this time in the usual 1:200 scale here, are:

Alitalia Airbus A330-200 (Art.-Nr. 610933-001),
TAP Air Portugal Airbus A321neo "Retro colors" (613316) and
Lufthansa Cargo Boeing 777F "Cargo Human Care" (613354).

In the area of Wings new products in 1:200 scale, the following new products are to be listed for European motifs:

Lufthansa Boeing 727-200 zu "50th Anniversary of 727-200 introduction at Lufthansa" (571326),
German Airforce Transall C-160 LTG 63 „Retro Brummel" (571562),
Russische Luftwaffe Tupolev TU-95MS (571579) and
KLM Lockheed L-1049G Super Constellation (571616).



A Boeing 727-200 (art. no. 571326; photo above) commemorates the 50th anniversary of the introduction of this type at Lufthansa, while the new-shaped air jet deflectors (571586; photo below left) and the Ziegler fire-fighting vehicle in the design of Hamburg Airport (571548; photo below right) expand the range of accessories.

Worth mentioning is the colourful model of the Coulson Aviation Boeing CH-47D Chinook (571517), which carries a US registration. In the accessories section, the Ziegler airfield fire engine in the colours of Hamburg Airport (571548) is offered.

In February, new moulds for the accessories range were announced. One such will soon appear in the form of air jet deflectors (571586), which are probably indispensable when designing airfields. The pack contains ten elements in three different lengths or designs.

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A Firecracker for Everyone

**Vimms 63 from ZetNa 220
First Steps for your Layout**

Introduction

Dear Readers,

We are still experiencing turbulent times: The longing for holidays in faraway countries is great, the infection situation at home is possibly relaxed, while countries, not too far away, are already being hit by a new wave.

Usually, our editions in midsummer are dominated by model railway themes that are intended to trigger a desire to travel and fit in well with the general wave of vacationers. Things are different this year, although our topics are no less exciting.

This choice of topics is not only because of the cadence of new releases. We have also deliberately chosen tinkering projects for the summer of 2021: This way, rainy days can be successfully spent in the cellar at the model railway layout.

However, it seems much more important to us not to suddenly become careless and negligent after one and a half years of austerity, to exchange virus variants across contacts of the most diverse regions and countries at large gatherings of people, and thus to prolong these miserable times even further.

How important, unique and worthy of protection our health is, we have had to experience at first hand, and this does not even require Covid-19. If several people in the private sphere have to spend almost the same time in hospital, visit doctors repeatedly, and often have to worry about their recovery, then this not only exhausts time and nerves for care, journeys and visits.

Above all, it clearly demonstrates that many things have no value without health and well-being. Here, too, we would like to set an example, an example of respect for the health of others.

Those who do not roam far, limit contacts and enjoy the most beautiful hobby in the world not only have fun, but also make a contribution to the community. And, that is exactly where this edition comes in.

With ZetNa 220 as the manufacturer, it is a little bit international, but with its livestock car VImms 63, it is still local to Germany. Märklin's push-pull (commuter) train fills a hurting gap in the programme and all together it will enhance the ride on the highly appreciated layout.

Building projects for experienced model railroaders and help for those new to the hobby ensure that the fire that burns within us when we build and tinker is not diminished. And for the evenings after a hopefully long and beautiful day of vacation, we have again found suitable reading.

In short: Actually, it's just like always! If everything feels familiar and routine, then we shouldn't really miss anything! In this sense, take care and stay healthy!

Sin-Z-erely,

Holger Späing



Holger Späing
Editor-in-chief

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We thank Eisenbahnstiftung and Gerhard Tüllmann (Brilon) for historical photographs.

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Cover photo:

141 258-4 makes one of the numerous intermediate stops with its local train. At the beginning of the seventies, it leaves a well-maintained impression, which also applies to its cab control car, which cannot be seen in the picture, but which has only recently been converted to a Karlsruhe head.

The 141 series in green

Attention, Firecracker!

their wishes were to come true. In early summer 2021, the most important prototype version followed in chrome oxide green paint, which also gave the locomotive its nickname "firecracker." For us, this is reason enough to take another look at this conversion as well.

In the summer of 2019, Märklin announced a "train set for local traffic" (art. no. 81356). Behind this MHI product, which sounded so unspectacular, there was supposed to be a real firecracker: The class 141, the only missing unit locomotive from the first DB new construction programme, was thus within reach of all customers.

At the end of 2017, a steel-blue sister locomotive in its original design had been announced as a 2018 Insider Club model, and had been delivered just two months at the time of the announcement of the MHI special series.



The new Märklin local transport pack (art. no. 81356) is presented here in full length, which has some special features in store.

The chrome oxide green paint job as a "firecracker", which was to follow surprisingly quickly, is certainly very familiar to the majority of readers and remembered from their own experience. But those who rubbed their eyes in disbelief in April 2019 and asked why customers without club membership would also be able to enjoy this design, which we praise so much, and which grants a certain exclusivity for premium customers, so quickly, had to be taught better.

A full two years passed before this new product arrived at the dealers. So, it will not be until the summer of 2021 that we will be able to critically examine and appreciate the Silberling (Silver Coins) that accompany it, which have not yet been offered in this form, and the green all-round locomotive of days gone by.

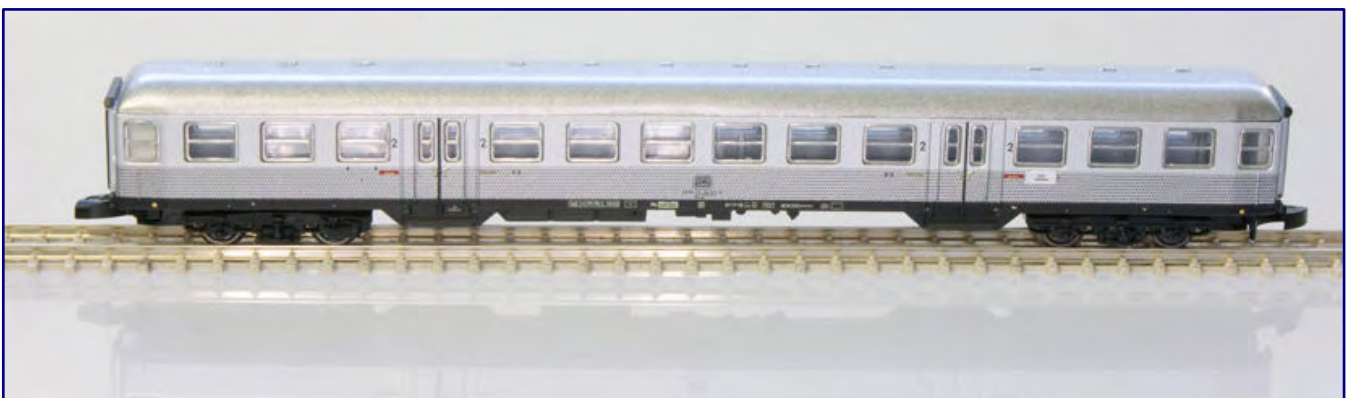


The locomotive and its three carriages have their upsides and downsides, which will be discussed in detail below.

Because, it is so important for model railways and is to establish a whole series of new standards for Z gauge, this first form variant also deserves a more detailed report. And, so, in the following, we work out what makes the MHI train set so special, and also state what we don't like about it.

Undiscovered Silberling (Silver Coins) Versions

The three local coaches that are given to the locomotive to be discussed are printing variants that have not existed before in this design. This is particularly evident on the BDnrzf 740 cab control car with a Karlsruhe head. This design was procured from 1971 onwards for S-Bahn-like service.



A black frame paint and the lettering typical for the beginning of the seventies look familiar. However, the Ege biscuit in negative design in the middle of the car is different from the Silberling of the first design.

For this reason, its front was also redesigned and later served as a template for converting the "Hasenkästen" (bunny boxes), which were unpopular with the prototype, to the same head shape.

The image of this driving trailer with two orange warning bars above and below the driver's cab front windows is familiar, before the appearance was to change with changing colour concepts. In the form now presented by Märklin, however, the model does not look familiar, at all.



Unchanged, the ABnrzb 704 has an incorrect roof, because the six cuckoo vents above the only five 1st class compartments belong to its sibling coach (photo above). The peacock eye pattern is not printed around the edges (photo below), which, fortunately, is hardly noticeable in the train set. The tail lights were also not designed in red.

It seems as if Märklin has forgotten the two stripe imprints. But this is far from being the case! The design of the first examples, that did not actually have the warning stripes, was reproduced here.

Whether it was more for aesthetic reasons or formal safety concerns that led to the later addition of the warning colour bars, we cannot answer with certainty. In any case, however, this model adds an interesting version to the existing programme.

It is lettered for Era IV, which also has a special value in combination with other features: The frame edge of the coach and its two siblings is black, which had not been offered in combination with Era IV inscriptions since the first appearance of the first Silberlings (construction period 1976/78 - 1983).

The supplementary print features also correctly include old class numbers (still) directly next to the entrance doors and written-out smoker and non-smoker signs in place of the later pictograms.

Rather standard on Märklin, but much rarer on the prototype according to our recollection, are the DB biscuits in negative design, i.e., the light and dark areas have been reversed in comparison to the fixed standard. This characterises all three coaches and is incidentally another difference to the original 8716 / 8717 / 8718 series.

What has not changed since 1978 (appearance of the cab control car) until today, and we do not like it is the lighting of the cab control car. Although it always had two red tail lights or three white head lights and an illuminated directional sign box depending on the direction of travel, its illumination was always poor.

This was due to the “flickering” incandescent lamps, which only provided a clearly visible emission at higher voltage. And Märklin actually managed to stick to this, although this technology has been outdated for more than twenty years and warm white LEDs have been the standard in Z gauge for well over ten years.



The Karlsruhe head of the driving trailer seems strange, but Märklin is right with this design: The first examples of these new control wagons did not have orange warning bars at the beginning.

At this point we can only shake our heads, because this shortcoming becomes all the more apparent in combination with the model of the ultra-modern class 141, because neither the motor nor the LEDs in the locomotive need a high voltage: the train runs at prototypical speed and the driving trailer remains virtually dark.

After the delivery of the train set was two years in coming and the “bunny box” with modern LED lighting was recently announced as a new design, hopes rose not only among us that this wagon could also benefit from it after all. Did some old stocks have to be reduced here?

The look at the other two wagons will be brief, as most of the features can be found there as well. It should also be mentioned that the printing on the models is up to date and that the line widths are correspondingly fine. The peacock's eye pattern, however, does not go around the corners, which is hardly noticeable in the train set.



This time, the Silberling are delivered without interior fittings. On the cab control car, the window frames in the folding doors of the luggage compartment are embossed black for the first time and reproduce rubber seals.

The window strips, whose frames are finely embossed in silver, look more precisely fitting than before. By the way, the loading space doors of the driving trailer have been embossed in black (rubber seals) for the first time, thus deviating from the original.

Train route signs are printed on the wagons, but also empty holders have been reproduced using pad printing – a bit of variety, that is definitely good. The “Silberling” second class is labelled Bnrzb 725, while its counterpart with 1st/2nd class is labelled ABnrzb 704.

Unchanged, there is only one roof shape for these two coaches, which belongs to the 2nd class coach and does not match the 1st class with the number of fans above the middle passenger compartment. But this is a minor detail that we have been able to get over and accept for over 40 years, especially since many Zetties probably had not previously noticed.

Immediately a shape variant

What we wrote in our test report (**Trainini®** 3/2019) essentially applies to the enclosed locomotive model. As expected, we find the basic chassis conception with pole division in longitudinal direction, adapted current guides, current pick-ups, and modified gearboxes compared to earlier designs again this time.

But, since the firecracker now has the modern DB lamps with separate front and rear lights, i.e., five instead of only three lanterns per front side, it was also necessary to change the light board. Märklin has

understood well here to change the face of the locomotive already in the second edition, and to provide for variety through a first shape variant.

Both models would look great next to each other if they did not belong to different preservation phases of the prototype. Those who don't care about this on the model railway will be happy. All others look forward to further variations of the varied prototype.

The ventilation grilles have also been changed. The outer shape still corresponds to the Schweiger ventilators, but with now vertical lamellas the model reproduces the multiple nozzle ventilators, which especially knew how to better prevent rain and snow from entering the engine room.

The all-round guttering, the detailing in the area of the buffer plank, and the individually attached handle and handrail bars remained unchanged. Märklin also retained the attractive roof design with individual cables inserted in the insulators.



The fine details of the 141 258-4 correspond to the previous version. However, the mounting of pantograph drives, compressed air whistles and striking bells could be improved before painting, so that they also take on the roof colour and shed their plastic shine.

The striking bell and the current collector drives are plastic parts that were applied after painting and show colour deviations from the roof, which are more or less noticeable depending on the incidence of light. The bell-shaped armature motor with low current consumption and best driving characteristics, on the other hand, is also impressive in this edition without any limitations.

We did not like the fact that the pantographs of the first model were taken from the existing parts stock. It should be known by now that they are not exactly to scale. But with such a delicate model as the E 41 / 141, this is all the more noticeable. So, it is a pity that Märklin has not yet abandoned this approach.

Note on the digitisation of the class 141

The digital supplier Velmo has a conversion solution for the E 41 from Märklin in its programme. However, this does not fit the new model that has now been delivered in its current form.

There, too, the light board has to be changed in such a way that in future it can not only display the light change, but also supply all five lanterns with their light precisely. Digital railway enthusiasts will, therefore, have to wait a little longer.

With such a delicate model as the E 41 / 141, however, this is all the more noticeable. So it is a pity that Märklin has still not said goodbye to this.

To make matters worse, this time larger parts of the series have received faulty copies that do not lock when ironed off, which is why after a few minutes at the latest, both pantographs are again in contact with the contact wire or cause the locomotive to get stuck at tunnel portals, if an overhead line is not used.

Our research did not uncover any defect-free model, which upset us, and prompted us to contact Märklin regarding this matter. The quality assurance was able to reproduce the defect on a number of models and has initiated a revised follow-up production, which, however, could not yet be scheduled.



For the first moulded version of the E 41 / 141, not only the lanterns but also the light board and light guides inside the housing were changed. Annoyingly, many of the delivered models do not have pantographs that lock in the lowered position, or lock only for a short time - these out-of-scale parts are no longer in keeping with the times, anyway.

Affected customers can contact the Märklin service directly or via their dealer in order to be able to exchange faulty pantographs under warranty or guarantee, if available.

We consider this step to be correct and consistent, which is why we may also praise it here. So there is actually nothing standing in the way of the new push-pull (commuter) train set to repeat the great success of the class 141, also in 1:220 scale!



The final view falls from the roof of the village church onto the passing local train: The class 141 once belonged, in its chrome oxide green colour dress as a "firecracker", to the everyday picture. It should still look familiar to many model railway enthusiasts.

We are looking forward to seeing the smallest of the standard locomotives from DB's new construction programme on layouts in the future as numerous as it once was in prototype. If the term "like bread and butter" applies to an electric locomotive, then, it does to this much-loved machine in its most beautiful livery.

With this in mind: have a good trip, little firecracker!

Manufacturer of the models:
<http://www.maerklin.de>

Vlms 63 of ZetNa 220

Only a short Heyday

The Vlms 63 design finally closed the chapter on small cattle wagons at DB. Today, no live cattle are driven to the slaughterhouses in freight wagons. At the end of the 1950s, things looked quite different and the Bundesbahn saw a need to replace its many older wagons. Thus, the wagon whose model by ZetNa 220 we would like to present today was created from the numerous predecessors in their conversion programme.

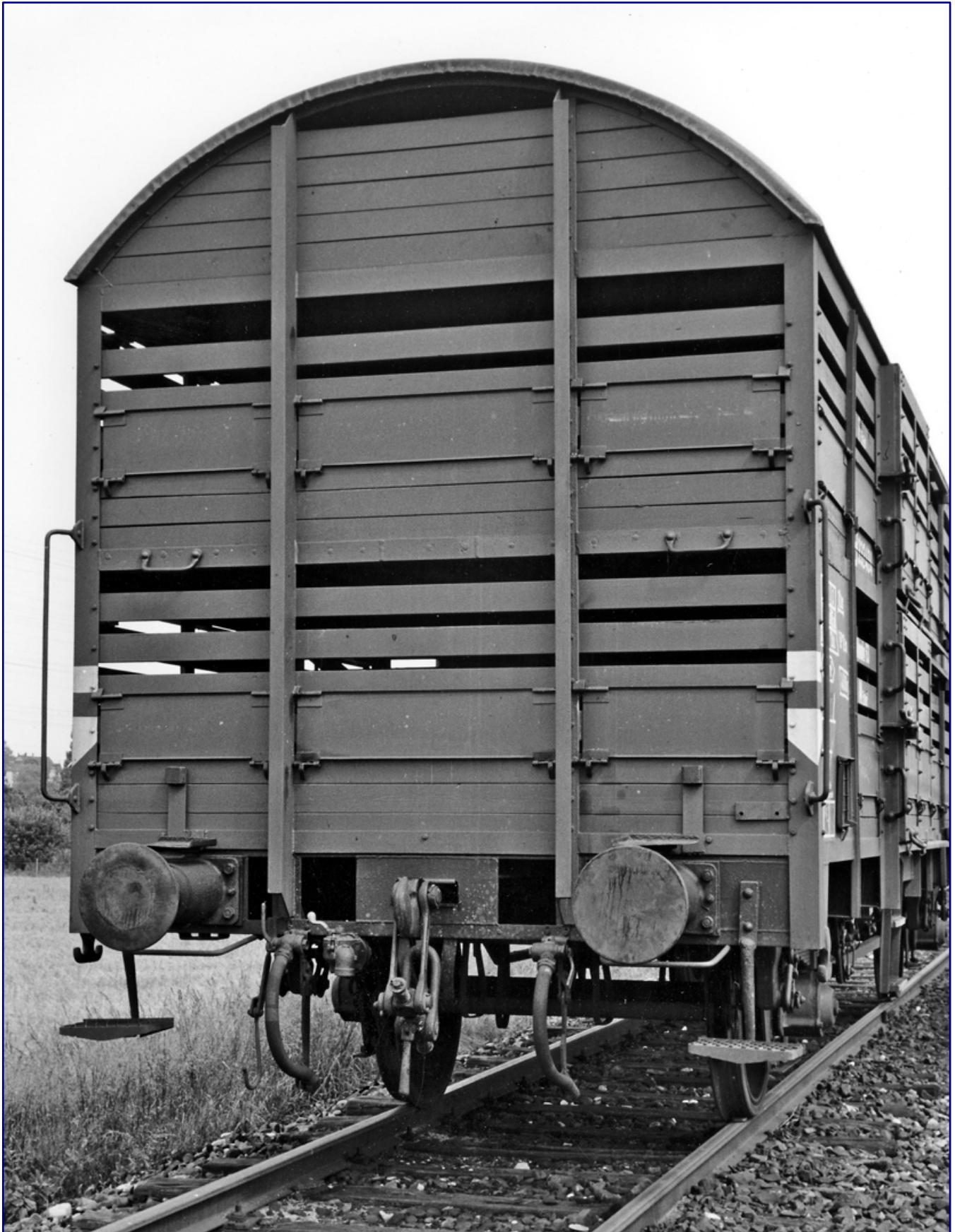
In **Trainini®** 4/2021, we have summarised the history of the German railroad cars from about 1890 onwards in order to be able to present and classify the model of the V 23 by ZetNa 220. In the meantime we also have the Vlms 63 of the Deutsche Bundesbahn, the last built representative of the main type V, whose history we would like to classify in the same way.

Our prototype section of the above-mentioned article concluded with the transport figures for 1953. In this financial year, the DB had almost 1,600 older types of wagons at its disposal, well over 90 % of which were still from the First World War period, or had even been built before that.

continues on page 14



In the sixties, cattle transport is still a lucrative business for the Bundesbahn. In Brilon, Sauerland, small cattle are loaded from the Hanomag lorry into the upper deck of a still almost new Vlms 63 using a ramp. Photo: Gerhard Tüllmann, Brilon



The front side of this Vlmms 63 reveals some deviations from the model of ZetNa 220 with regard to the two front wall struts and the termination of the boarding under the roof. Photo: Bustorff, Eisenbahnstiftung

Modernising the wagon fleet was, therefore, an obvious choice at the time of this last high point in the transport of live cattle. However, the decision to do so was not made until the end of the 1950s. The DB finally extended its conversion programme to include the main class V.

A test car of the new type Vmms 63 with the short service number 240, which identified it, accordingly, was ready for testing at the beginning of 1959. In contrast to the later production cars, it did not yet have a sprinkler system, but, otherwise, showed only a few differences with them.



Although the time of the cattle transports had already expired, the DB still had a total of 650 cars of the type "Vmms 63" built from 1960 onwards. The prototype, equipped with a heating cable, was designated Vmmhs 63, bore the conspicuously short road number 240 and, in contrast to the series-production wagons, did not have an trusses on the wagon floor. Photo: Bustorff, Railway Foundation

The main dimensions and underframe of this type of wagon corresponded to the Gllmehs 50, the impression of which is given by the Märklin model 8605, which has been in the programme since the start of Z gauge. In contrast to most of the covered wagons from the new construction programme, however, the side and end walls of the new construction shed wagon were made of wooden slats that left 40 and 60 mm. wide air gaps for the cattle.

Just like the old livestock wagons, the newly developed examples had two floors, but due to their greater length they had four instead of only two sliding doors on each side and a total of 58 ventilation flaps. Inside they had ten pairs of revolving doors to divide the loading area into compartments. Thanks to double-hook running gear and KE-GP brakes, they could be classified as high-speed trains (secondary class s).

In 1960/61, the construction of the new VImm(h)s 63 from old and reconditioned parts was underway, for which, most of the old cars were dismantled. The Bundesbahn created 650 copies in this way, which eventually almost completely replaced their predecessors; 270 of the new cars had a steam heating line and could be used in passenger trains; 190 had a hand brake that could be operated from a brakeman's platform.

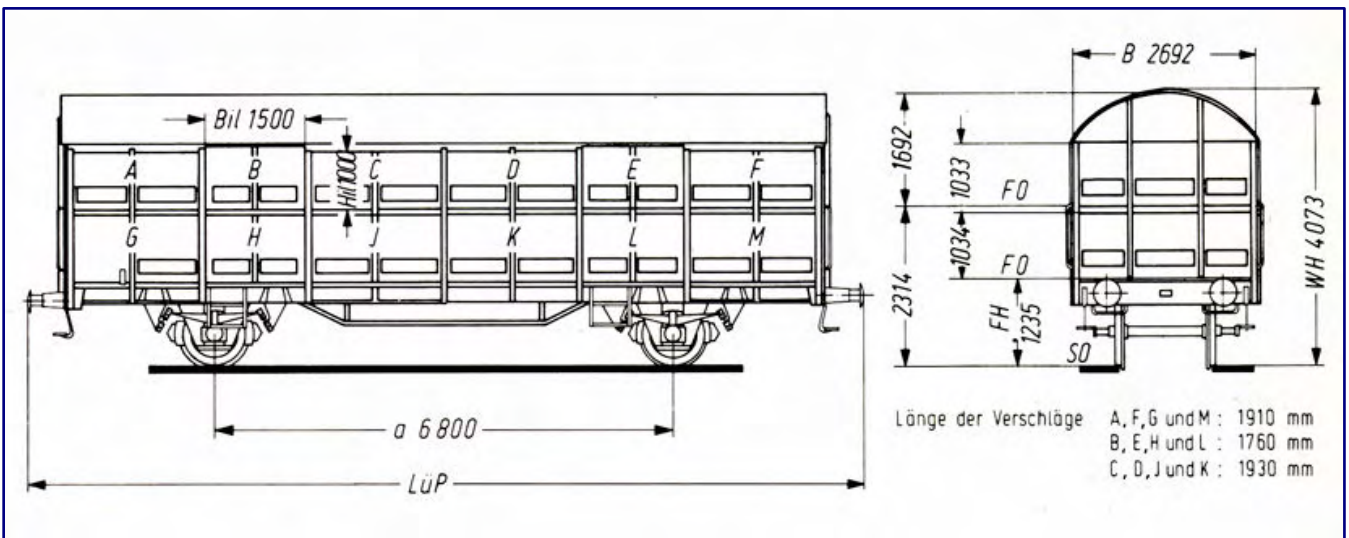


Photo above:

Between 1913 and 1927, 2,133 units of the A8 combination type livestock wagon were built. Designated Vh 14 by the DB, it also served as a parts donor for the Vimms 63 as part of the conversion programme. Photo: Bustorff, Eisenbahnstiftung

Photo below:

DB's drawing provides its customers with information about the most important dimensions, the height of the two loading levels and the length of the individual crates of the Vimms 63 / Hbes 358. Figure: Werbeamt der Deutschen Bundesbahn, Slg. Trainini

Only about 190 examples of the old types were still available after the completion of the conversion programme, and they were subsequently used for the construction of new G-cars. By 1966, all the old types had been removed from service.

But even the newcomers were often not given too long a life. In 1968, they became Hbe(r)s 358 (r from now on for steam heating line) with new service numbers, and, thus, at the same time a special type of covered wagon.

The stocks were already beginning to thin out, as the need for small livestock wagons was continually decreasing. Some of the wagons were converted to the related type Gbs / Gos 245, which we have already touched on with the Märklin model 8605.



From 1968 onwards, the remaining livestock wagons that had not been converted into covered wagons of the standard design were designated as Hbes 358. The photo clearly shows the revolving doors for separating the individual crates. Photo: Werbeamt der Deutschen Bundesbahn, Slg. Trainini

In 1980, the type designation was changed again to Hes 358 with new road numbers. In the meantime, none of the 40 remaining examples had steam heating. At the turn of the millennium, the stock had shrunk to 13 coaches, which are based in Buchloe – at least they had made it into the era of the Deutsche Bahn AG. In the meantime, this type has long since been taken out of service.

The implementation of ZetNa 220

In contrast to the previously delivered model of the V 23, the new DB car was already available in 1:220 scale. It could be found in the programme of Krüger Modellbau, as with the prototype on the chassis basis of the covered wagon related to it. The donor model for this was the Märklin model with item number 8605, on which a new superstructure was placed, and a sprinkler was mounted at the bottom.

ZetNa 220 went a different way constructively and designed for the new products presented here (item no. B002) not only the superstructure with the slatted walls and the smooth roof itself, but also the complete running gear. The production method is identical to the V 23: brass plates form the body and also the basis of the undercarriage.

The axle holders are again made of several layers of brass and carry Märklin wheelsets with pointed bearings, which means that the metal structure cannot be guaranteed to be potential-free. However, the catching brackets for the wheel sets used on the V 23 have now been dispensed with.



Nobly packaged and attractively designed, the Vlmms 63 from ZetNa 220 in Sicily arrives to be put to the test.

As far as the wheelset guidance is concerned, it should be noted that, due to the way the wheelsets are moved, they tend to inhibit the smooth running of the wheels. Lubrication with grease or candle wax can contribute positively to good running characteristics.

Data and dimensions of the crating wagon Vlmms 63:

	<u>Prototype</u>	<u>1:220</u>	<u>Model</u>
Length over buffers	12.500 mm	56,8 mm	54,6 mm
Greatest width	2.772 mm	12,6 mm	13,6 mm
Height over rail head	4.076 mm	18,5 mm	18,3 mm
Body length	11.260 mm	51,2 mm	51,7 mm
Wheelbase	6.800 mm	30,9 mm	30,6 mm
Net weight	14.500 kg	---	8 g
Years of construction	1960/61		
Number of units built	650		

More than two specimens will certainly not be found in a train set, otherwise the relatively high dead weight of 8 grams together with high resistance during cornering due to the large axle base would become a problem at some point. Buyers should be aware of this and choose their combinations carefully.

The coupling guide in the self-built coupling shaft is again a plugged hollow rivet, which can also be pulled and then allows the

exchange of the couplings or springs. Parts from Märklin were used here ex-works, which we also found on the predecessor.

One of the 650 series wagons was chosen as a model, which was obvious. Of course, the model also has the typical explosive system, which only the test car did not have. The superstructure, with the air gaps, sliding doors, and inspection flaps looks successful, and above all "airy", which reflects the prototype impression well.

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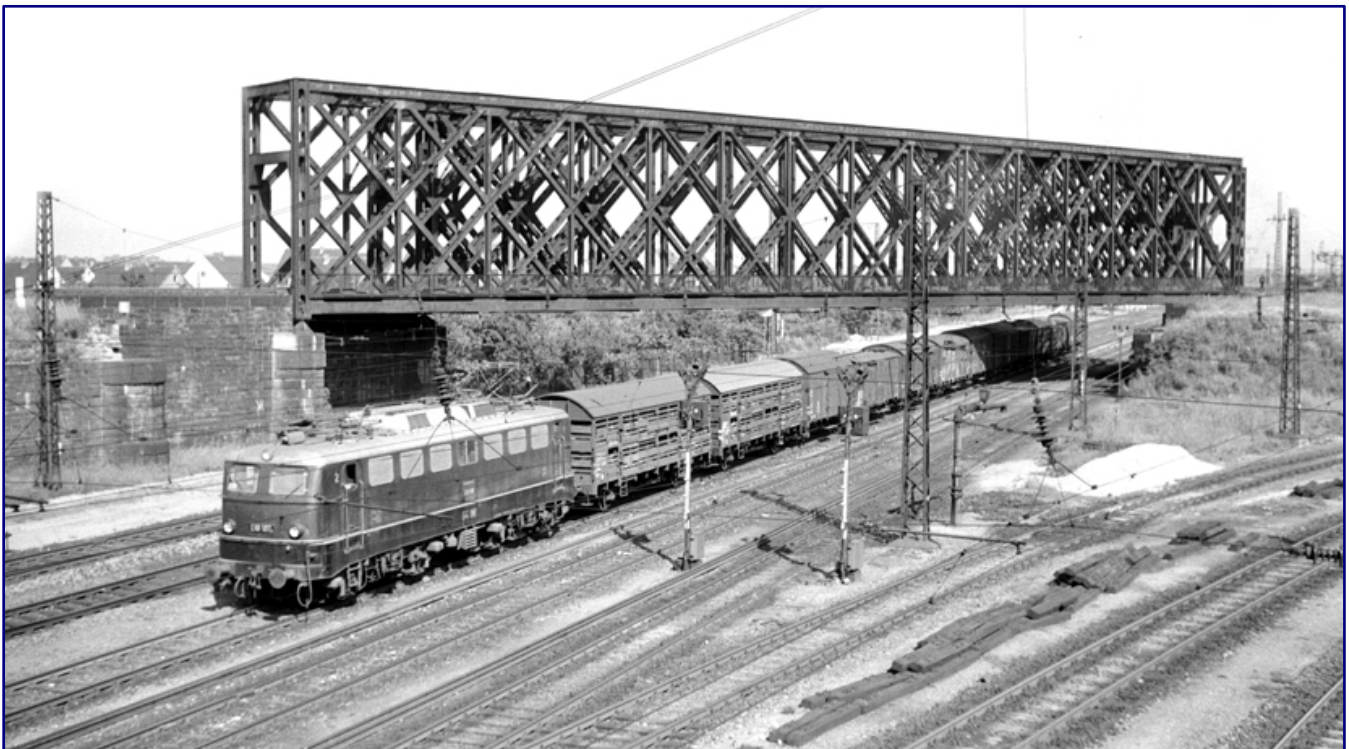


The Vimms 63 from ZetNa 220 is certainly not perfect, but it makes a good impression from all perspectives. And, it also stands out for its reasonable price.

The UIC rope hooks are also reproduced, only the handle bars at both ends of the car body have not been reproduced. The paint of the undercarriage and the roof correspond to the prototype colours, the side walls are a little lighter in comparison to the colour edge, which is not disturbing, because it corresponds to the rather weathered condition after a few years of operation.



The first model of a new-build freight car for Z gauge came from Krüger Modellbau (item no. 63555.1; rear) and was based on the Märklin 8605 running gear. The model from ZetNa 220 (B002; front) has been completely redesigned.



E 10 107 of the Bw Heidelberg pulls a Sg (express goods train) with two livestock cars Vlmms 63 at the head of the train through Mannheim Rbf on 24 June 1961. The photo provides good ideas for the use on the model railway layout. Photo: Helmut Röth, Eisenbahnstiftung

Legible and again quite cleanly applied are the decals with the white service inscriptions, which assign the model to Era III (Vlms 63) with the service number 332 232. The black chalk field is also visible, but a labelling box is missing. We were a bit irritated by the fact that the RIV inscription is again written in black on a white background. On German wagons it is otherwise also written in white on the brown background.

The adherence to the most important operating dimensions is well done, as our table also shows. The justifiable width deviation is not unusual, in order to be able to maintain the overall proportions and accommodate the undercarriage.



As if made for each other, the new products from ZetNa 220 and the mobile loading ramp from Artitec (322.037) allow for an appealing scene at the loading siding. The last animal has just been loaded and the sliding doors are closed. As soon as the ramp has also been removed, the group of wagons can be pulled off the loading track.

The noticeable deviation in the length over buffers is actually not attributable to the model itself, as the comparison with the almost perfectly adhered to scale car body length shows: Here, as with the V 23, the shortened and too small buffers are responsible, which were designed this way for reasons of operational safety.

If this bothers you, it can easily be remedied by replacing them with other small-series parts. All in all, the overall result is a good one, which is also a sensible enrichment of the Z-gauge range.

If you want to show some variety in your trains and on your layout, you will surely find a suitable model for your request at ZetNa 220, which is beautifully and appealingly designed and also remains affordable. By the way, the mobile loading ramp (322.037) from the Artitec programme is also a suitable addition, which is made for exciting loading road scenes with small cattle loading wagons.

Supplier of the model:
<https://sites.google.com/view/zetna220/home>

Loading ramp as an addition:
<https://www.artitecshop.com>

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Self-built track ramps (Part 2)

Construction of the Ramps with Arcades

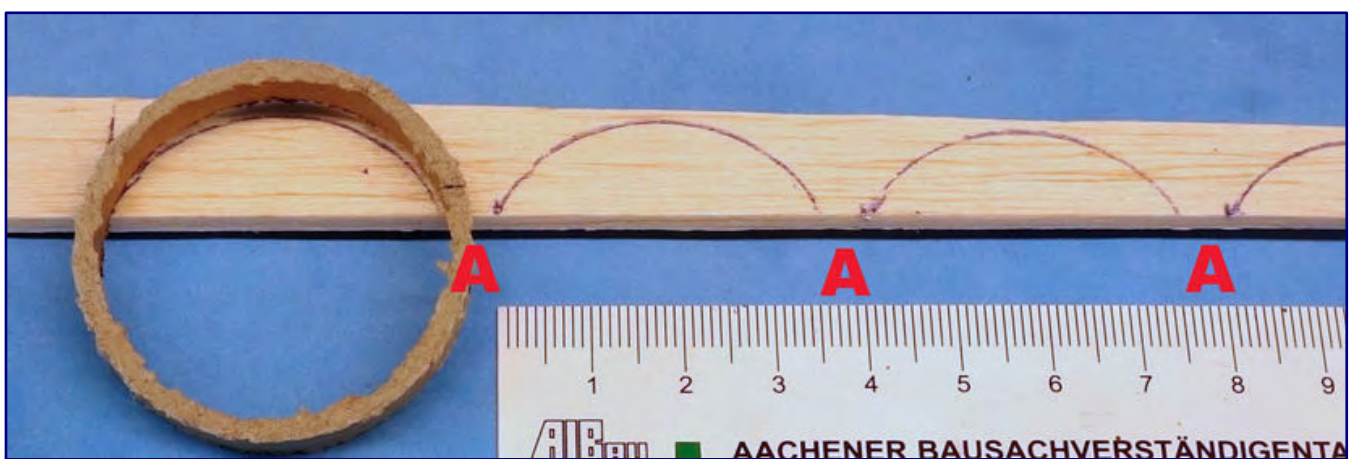
After our reader Heribert Tönnies presented his preliminary considerations and plans in the first part, and looked for suitable building materials, we can now get to work. A ramp with completely open arcade inserts is now created in an inexpensive way. Another version will also be presented later on.

From Heribert Tönnies. First of all, I will deal with the ramps that take up the exit track from the depot to the main line, as well as, the track to the industrial park. Because these ramps are located directly at the front of the layout, I want to have the greatest possible visibility here: The arcade arches remain completely open, so that especially children, but not only them, can look through here. In addition, the access road to the industrial park is led under the ramp through one of the high arches on the left.

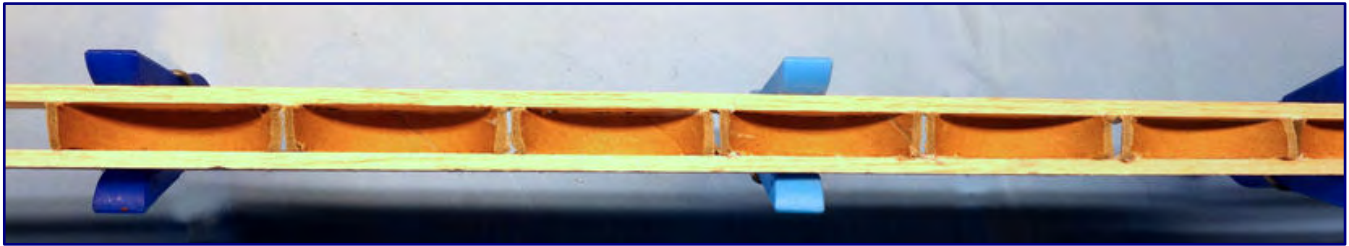


First, I cut the two side walls of the ramp from 3 mm. thick balsa wood in the required length and height. To save wood, I first make a rectangle: length = ramp length, height = ramp height. Then, I only need to make a diagonal cut by joining the two opposite corners and thus I already have the two side walls (see photo above).

I place the still complete strip rings on one of the two sides of the ramp, the upper edge of the circle flush with the top of the ramp, and trace the inner arc with a felt pen. Keeping the same, previously determined distance between the arcs at the lower edge according to variant a), in my case 7 mm., I draw all further arcs.



The inner arcs are marked on the wooden strip with a felt pen (photo above), then the circle segments can be shortened to fit and glued on (photo below).



After gluing on the second side of the ramp, clamps fix the component until the glue has set. Care should be taken so that no glue should come out on the inside shown here.

Now, I can shorten the arch strips to the required height and glue the circle segments to the side wall with wood glue. To prevent the balsa wood from warping due to the one-sided absorption of liquid from the wood glue, I weigh down the component (e.g., with books).

Tip "Component marking"

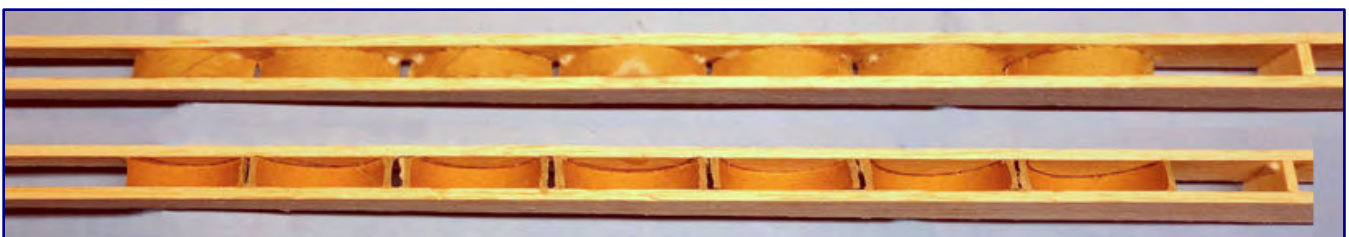
As already described elsewhere, I recommend marking the components with "bottom" (b), "top" (t) etc., in a place that will later be invisible.

Especially with the flat slopes of the ramps it is possible to confuse the top and bottom. An incorrectly glued component would be nothing but scrap.

After tightening the glue, I apply the wood glue to the free edges of the circle segment arches and glue the second ramp side on top. To set, I fix the parts either with clamps or (screw) clamps.

Wood glue that comes out in the area of the later open arches should always be scraped off immediately with a small piece of wood or similar. Because the arch cut-out will have to be cut out here later, there must not be any tough glue bead there, which would otherwise be very difficult to work on with the craft knife.

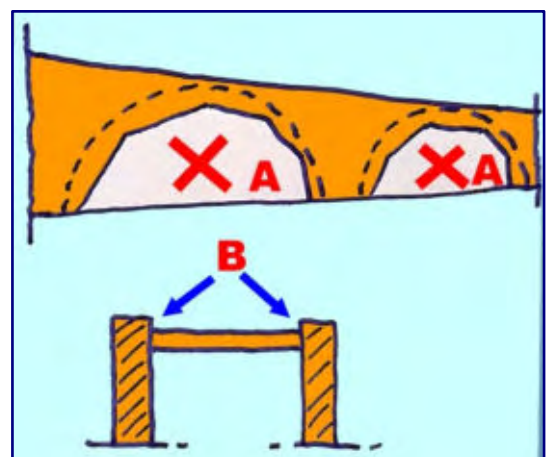
On the other hand, wood glue that oozes out of the top of the arches is desirable because it creates a larger gluing surface that gives the construction more stability. The first two ramp blanks are now already glued, and the following photo shows a view from above and below.



Now, all that remains is to cut the open sheets cleanly out of the side walls. To do this, I carefully run the knife along the inside of the cardboard sheets.

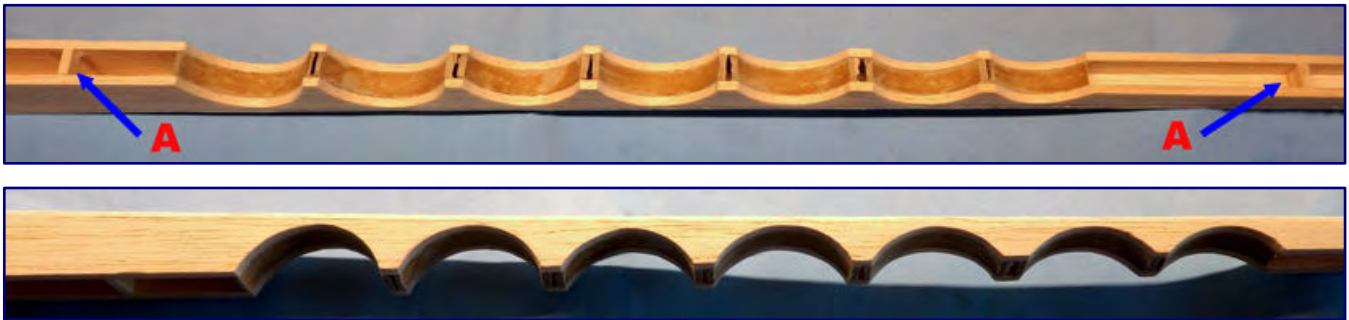
To make it easier, I cut out the side panels to be removed in advance with a generous margin to the circular arcs (mark A in the photo on the right). For the final fine cut, the side wall can also be left slightly beyond the circular arc (mark B in the photo on the right).

The balsa wood, which is easy to work with, is very easy to sand with sandpaper so that the edge to the board can be easily adjusted and smoothed. This procedure has the advantage of avoiding a bow that is accidentally cut out too large and time-consuming spatula work.



I decided to use this method for stability reasons: The ramp is very resilient due to its box shape. However, it is also possible to open the arches in the first step, which follows the gluing of the first side. I advise against this, however, because a side wall that is only glued on one side poses a risk of breakage when working with the quite large lengths.

The result of my blanks already looks promising. At the ends and beginnings of the ramps, where there are no round arches, I glued in small crossbars made of balsa wood to increase the stability.



This is what the intermediate result looks like after the steps described so far. The markings A indicate the crossbar connections made of balsa wood to stabilise the ramp.

In the next construction step, the top of the ramp is closed. This surface also serves as a track support. Different variants are possible here, both in terms of construction and width.

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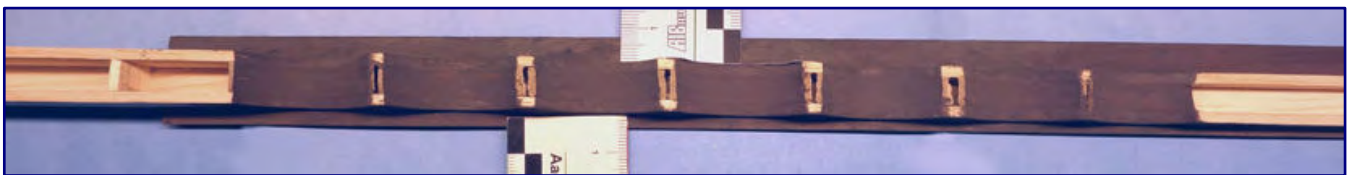
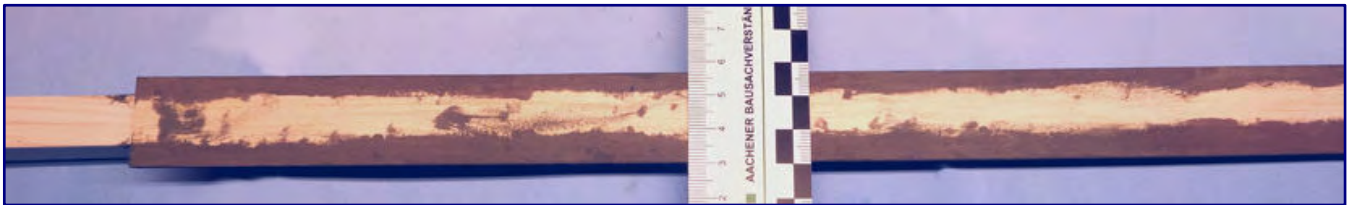
The large overhang on the ramp is placed in the direction of the viewer's main field of vision, i.e., towards the front of the layout. Safety exits in confined spaces, as found on the Moselle line (see photo on page 25) are also worth replicating.



Because I have to build as space-saving as possible, my little prize winners are not offered a separate and raised walking area next to the track. In the prototype, there would therefore be an absolute ban on walking on the track when it is not closed. Instead, there are safety exits at regular, shorter intervals, as can be found on the Moselle line, for example. In addition, I decided on an asymmetrical design for the ramps.

Usually, the walking areas next to the tracks on bridges or ramps protrude laterally, i.e., project beyond the vertical ramp walls. Although I don't design walking areas, I resort to this trick to increase the three-dimensionality. I place the large projection in the direction of the viewer's main field of vision (front of the layout).

For the top of the ramp, I call it the top plate, I cut a 26 mm. wide board from a 1.5 mm. thick balsa wood board. In reality, this corresponds to a concrete slab approx. 30 - 35 cm. thick, which is realistic for carrying the heavy load, especially with cantilever arm.



Along the edge, the top and bottom are coloured with a dirt shade to effectively conceal possible inaccuracies during further work (photo above). Then the cover plate is glued to the top of the ramp with wood glue (photo below).

If this is too thick for you, you can use the “beveling” trick already described in my book. The edges should not be sanded absolutely smooth later, because this is old Bw. You can see in the photos that this slightly rough edge looks like old, brittle and already slightly flaking concrete – and thus absolutely realistic.

In order not to have to paint along the prints to be glued on later (photo-realistic concrete reproduction), I colour a strip along the edge on both sides of the lower and upper side with a dirty grey-brown shade. This way I only have to do minor cosmetic work later without running the risk of irreparably soiling other components.

I glue the prepared top plate to the ramps with wood glue. It is important to keep the different protrusions (7 mm. at the front and 4 mm. at the back) and to keep the edges of the top plate parallel to the side walls of the ramp.

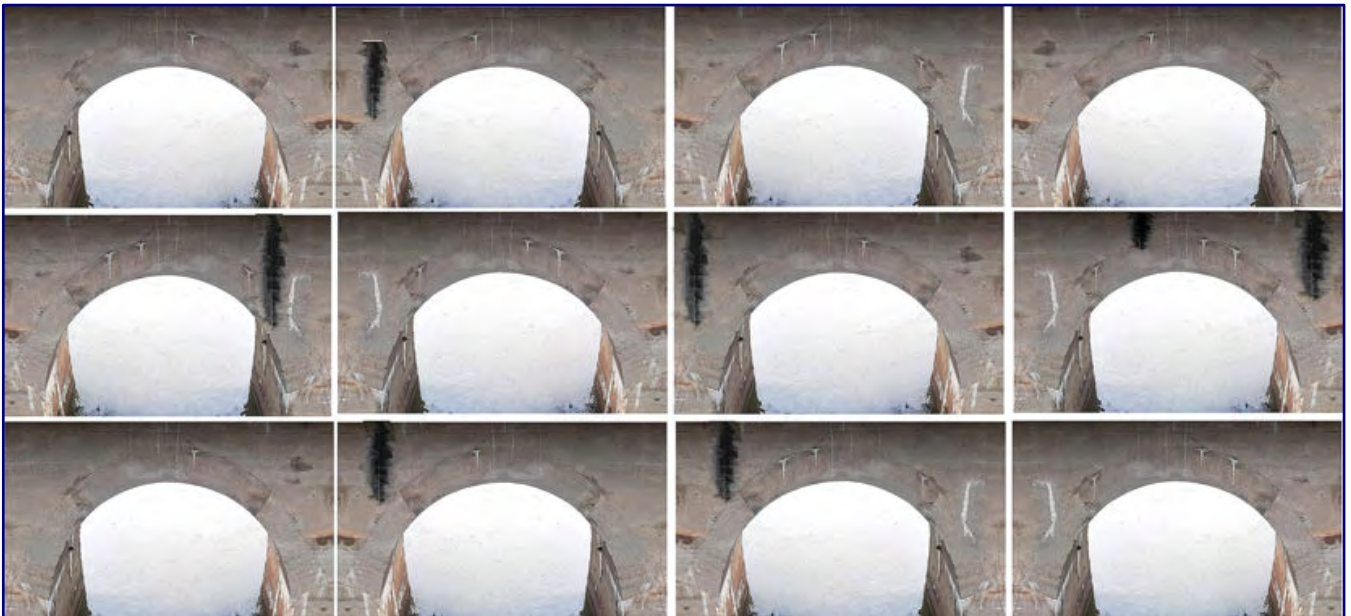
To avoid the cover plate bending up due to the wet wood glue, I either fix the cover plate to the ramp walls with pins or weigh down the construction.

For the design of the ramp walls, I decided to use a photo-realistic reproductions, as already mentioned. I found a suitable photo motif at a former jetty with an arcade construction for the unloading cranes. Its weathered concrete is ideal and the shape of the arches can also be adapted well for my purposes.



The arcade construction on a former jetty provided the photo template for a print motif that was used.

In the image editing programme, I “cut out” the individual arches and distort them in such a way that their dimensions match my prepared arcade arches as far as possible. In different variations, I combine several of these concrete arches to form a structural arch – I offer this structural arch for free download on my website.



Extract from the printed sheet with the sheets for the building project adjusted by distorting and changing the dimensions – also offered on the author's website.

Printed on 180g/m² matt photo cardboard at the appropriate enlargement level, I get a prototypical look. I glue one element cut out in the required width with all-purpose glue to the side wall, with the upper edge close to the cantilevered top plate.



In the centre of the pier, the butt edges are not to be aligned exactly: they will still be covered by a brick masonry support later on.

Tip "Cut off paper lamination cleanly".
Always leave the paper overlay (lamination) protruding slightly over the component (e.g., along the round arch). After the glue has dried, use a scalpel with a new (sharp) blade to carefully cut off the protruding paper edges along the wooden construction.
When doing so, guide the blade from the outside to the inside and do not return it in the saw cut (i.e., do not cut back and forth), but always in one direction. This prevents the paper from tearing and fraying. Then, sand the edge cleanly with 600-grit sandpaper, again from the outside inwards.

In order to be able to position the element axially accurately, I cut out the arch in advance, but leave the cut-out 1 - 2 mm. smaller so that I can later cut along the edge of the building with an exact fit.

The butt edges of the individual elements do not have to be worked absolutely precisely in the centre of the pier, because a brick

masonry support in front is still glued on here. The finished side wall can be seen in the photos.

For the pillars I need thin strips of balsa wood about 3 mm. wide, which I cut from a 1.5 mm. thick balsa board. I achieve the brickwork replica again by photorealistic lamination.

As a model I chose the façade of an older industrial building, which already shows some spotting. Using a drawing program, I again add individual segments offset and optimised, so that a complete DIN A4 sheet is filled with brick masonry structure. This structure can also be downloaded from my website, but here 3D foils from Redutex are also available as an alternative.

In contrast to the concrete arcade arches, however, I only print out the brickwork on 80 g/m² paper so that the fine edges remain sharp and are easier to crease.

I cut out approximately 8 mm. wide strips with a horizontal structure from the printout and glue them with all-purpose glue in the middle of the 3 mm. wide pillars. Then I fold the lateral edges backwards at the edge and glue them to the sides. I fold the overhangs at the back, which are still approximately 1 mm wide, to the back, and glue them there too.

The advantage of laminating around the back is that the pillars can be glued neatly to the substrate with no gaps. One disadvantage, however, is that the slight curvature can cause a hairline gap to the background despite the thin paper.



The pillars made according to the instructions in the text are presented to the viewer here before their further processing.

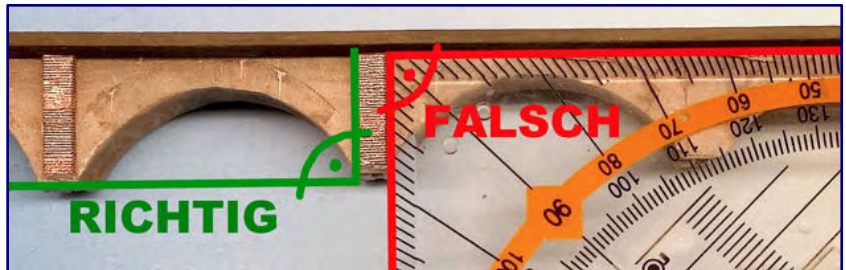
If this must be avoided, the paper lamination may only be passed around the sides and must be cut with the reverse side absolutely flat. However, there is then a risk that inaccuracies will occur. In this respect, everyone should try out and use the most suitable method for themselves.

Now, I just shorten the pillars to the required length and glue them exactly in the middle between the concrete arcades using all-purpose glue or wood glue. So that nothing slips, I fix the pillars at the top and bottom with pins.

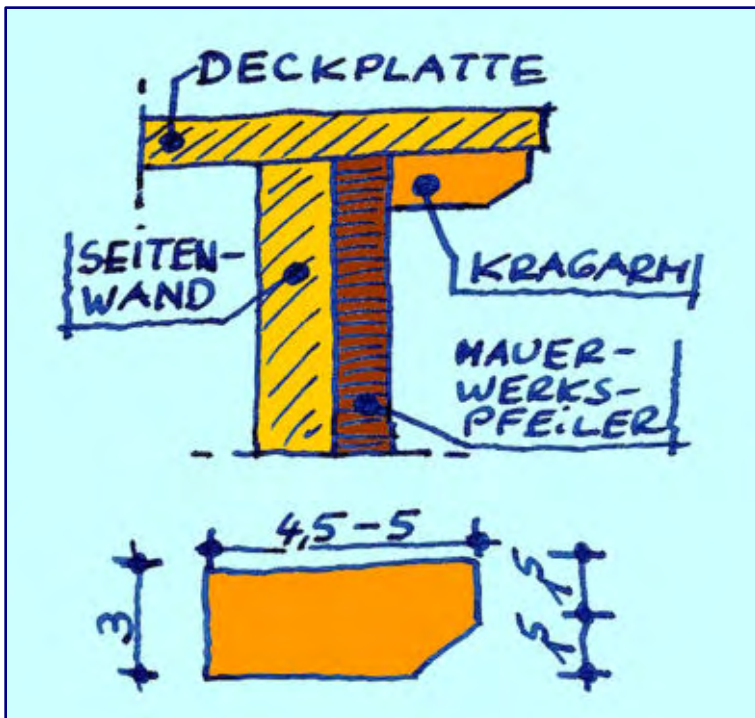


Until the adhesive sets, the pillars are fixed to the arcade base with the help of pins.

Caution: It is easy to align the pillars at right angles to the upper edge of the ramp. However, this leads to them being crooked by the angle of the ramp slope when the model is installed. Therefore, they must always be aligned perpendicular to the lower edge.!



In order to give the “show side” of the ramp some additional structure, I make small cantilever arms from balsa wood that support the widely projecting top plate in the area of the masonry pillars. This proves the need for the pillars and gives the top plate a nice structure on the underside. By the way, this design is statically also prototypical. I cut these parts from a 2 mm. thick balsa wood board in the dimensions according to the sketch (details in millimetres).



The cantilevers can be painted concrete grey before installation, but with a steady hand and a fine brush it is also possible after installation. I use wood glue to glue them centrally in front of the supports and on the arcade-free surfaces on the side of the wide cantilevered top plate.

Due to the short projection of the cover plate on the rear side of the ramp, cantilever arms are not necessary here, neither for prototypical nor for hobbyist reasons.



The cantilevers are glued on as shown here, but only on the side that will later face the viewer. The other side remains without these cantilever arms.

As already described at the beginning, I choose parapet walls on both sides as fall protection. A natural stone wall would fit in well with the theme of my installation. Due to the construction, however, such walls are relatively thick, unless it is a facing construction: "thin" natural stone slabs on a masonry or concrete core.



The brick look is also used for the parapet walls. In this way, the structure of the pillars is repeated to a certain degree.

However, such elaborate constructions were generally not used for the purely functional buildings of the railway. So that the structure of the pillars is repeated, I also use brick for the parapet walls.

This fits into the overall picture of the layout anyway, because at various points I also use retaining walls with double T-beams and brick masonry panels in between (theme: uniform construction methods and no "architectural exhibition").

Because many parapet walls are needed, I make them rationally in a larger stock. To do this, I print out several sheets on paper (80 g/m²) from the brick masonry template already described and cut them along the masonry structure into strips about 10.5 cm. wide.

I glue these strips on both sides with Uhu glue to a 10 cm. wide, 1 mm. thick and 60 cm. long balsa wood board (3 DIN-A-4 long printouts in landscape format fit on the 60 cm.). A longer length makes no sense, at least on a scale of 1:220, because the fineness makes further handling impractical.

Tip "Avoid bending up with larger components".
As already described above, the soft and open-pored balsa wood very easily absorbs moisture (e.g., from the glue). Because the wood expands on the damp side, this causes the balsa wood to bend upwards, which can hardly be returned to its original flat shape after drying.
Gluing the masonry structure on both sides at the same time already reduces this tendency considerably, but cannot completely prevent it from bending up.
However, because the adhesive diffuses somewhat through the glued paper due to the large surface area, no large-scale weighting can be done during drying (e.g., with books). The thin paper lamination then sticks to the weighting and is destroyed when lifted off.
I therefore take a thicker, stable (balsa) wooden board and fix the balsa wood plate glued on both sides to it with pins. In my experience, the paper does not stick to the underside with this method, because the lack of weight creates a minimal air gap that prevents this and allows the underside to dry quickly.

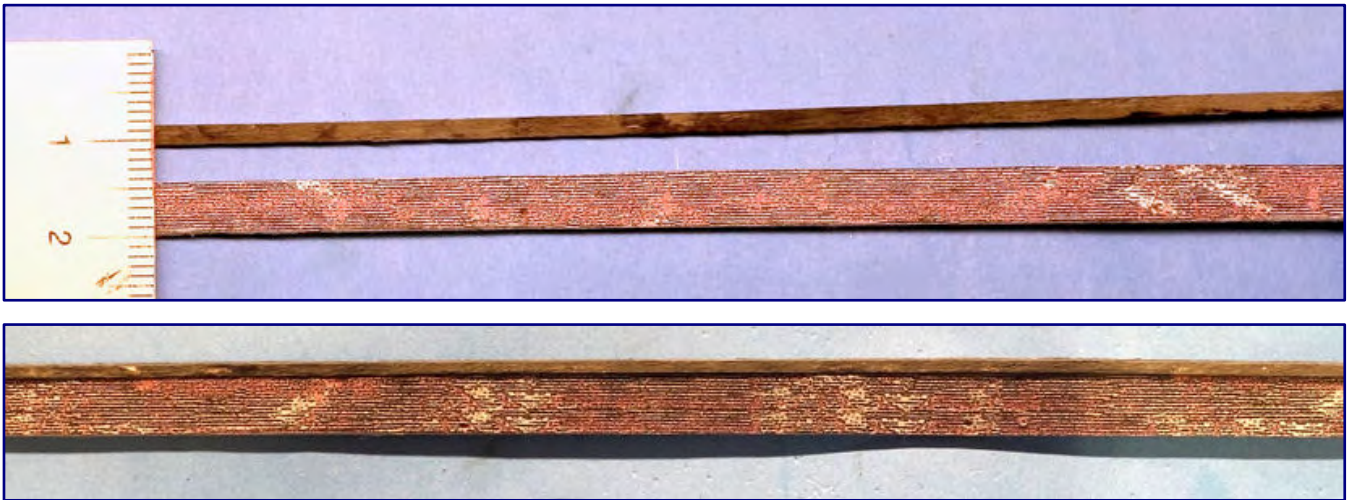
The Uhu all-purpose glue has the advantage that it "sets" relatively quickly and thus counteracts any curling of the paper due to excessive moisture absorption. However, a suitable spray adhesive can also be used. But because I am working at my desk (for the time being) and want to save myself the work of covering up against spray mist, I reach for the tube.

After the balsa wood laminated on both sides has dried well, I cut strips approx. 5 mm. wide parallel to the masonry structure with the craft knife. In Z gauge this corresponds to a real height of

approximately 1.10 metres. The thickness of 1 mm. represents a scale wall thickness of approx. 24 cm.

A thinner wall (e.g., 0.5 mm.) makes no sense and can no longer be processed cleanly. If you want it even finer, you would have to resort to plastic boards profiled with a brick look.

For the concrete covering of the wall, I take a 0.8 or a 1 mm. thick balsa wood board and paint it in an approximately 3 cm. wide strip on both sides in concrete grey, whereby I tend towards grey-brown due to the degree of soiling. The colour structure may become “streaky”, which emphasises the different levels of dirt. In addition, there will be a colour after-treatment later anyway.



After drying, cut off strips approx. 1.5 to max. 2 mm wide. The cut edges that are still rough from the cut are painted concrete grey to brown (photo above). They can then be glued to the centre of the prepared masonry strips using wood glue (photo below).

After drying, I cut off strips approximately 1.5 to max. 2 mm. wide. I also paint the cut edges that are still rough from the cut concrete grey to brown. Then, I glue the narrow strip in the middle of the prepared masonry strips with wood glue. The wall is now finished and can be glued onto the ramp.

In order to achieve a greater three-dimensionality with the wall disc, I do not glue the wall flush with the edge of the ramp directly at the front, but about 0.5 to a maximum of 1 mm. back (see arrow in the photo on the right).

In this way I create a small protrusion that gives me the opportunity to compensate for minor inaccuracies later and, if necessary, to cover them up with wildly growing greenery (photo on the right).

Because of the great length and the fineness, they cannot always be avoided. By the way, I use express wood glue (Uhu Holzleim D2 Express) to be able to make minor corrections during gluing.

If it comes out on the outside, it should be removed immediately. On the inside, on the





Arcades make no sense in the low ramp area. Therefore, the brick masonry structure is used there again.

other hand, a small groove can be created, because this creates greater stability and will later be covered by ballast or the like anyway.

Small inaccuracies cannot be avoided here and there over the great length with the thin wall strip. Most of the time there is a small gap in some places. I close this gap with wood glue applied with a toothpick. The glue often pulls back quite far after it has set. These areas can then be covered very well with green landscaping or with moss made of green paint of a thicker consistency.

In the low ramp area (beginning of the ramp), arcades make no sense. Here, I cover the ramp walls with the brick masonry structure, printed either on 180 g/m² or 80 g/m².

Because I apply the structure only after gluing the brackets, I have to cut it out of paper. It is easier to glue on the masonry structure before mounting the brackets. With this, the two front ramps are essentially finished, except for colour finishing and landscaping.

Ramp with partially open and closed arcades

I build the rear ramp in the same way. Only the open arcades have:

- A) Closed parapets in the lower area (due to the rising terrain behind),



- B) in the middle area, vertical wall panels in the arches (due to high terrain behind them), and
- C) sloping walls on the right (because the height of the arches is lower here and the terrain behind the ramp is almost at and later even above the top of the ramp).

A retaining wall to the locomotive shed will then be built here at the rear along the ramp.

Although the terrain rises at the back, I leave the three left arcades open in the upper round arch area to emphasise the openness and the detail. Similar designs also exist in prototype.



Retaining wall constructions can also be found on the prototype (photo left). The author took this up and built a passage with stairs to the locomotive depot (photo right).

Here, I will realise a pedestrian passage with stairs between the electric locomotive depot at the rear and the siding in front of the ramp (e.g., for an electric locomotive).

The remaining arcades are given a slightly recessed wall. For this purpose, a vertical wall is built from the left in the 4th to 6th arcade, the remaining arcades to the right of it receive a wall that rises obliquely from the front below to the back above, which particularly beautifully underlines the three-dimensionality of the then free-standing round arches. This construction was also common in the past.

The rear ramp wall does not need to be cut out in the shape of a round arch when the arcades are to be filled, because the substructure of the terrain is against it here anyway. For the infill, I draw the round arches on a 3 mm. thick balsa wood board and cut out the components to fit exactly.



Where the arcades are filled with a wall, the rear ramp wall does not have to be cut out (see right in photo).

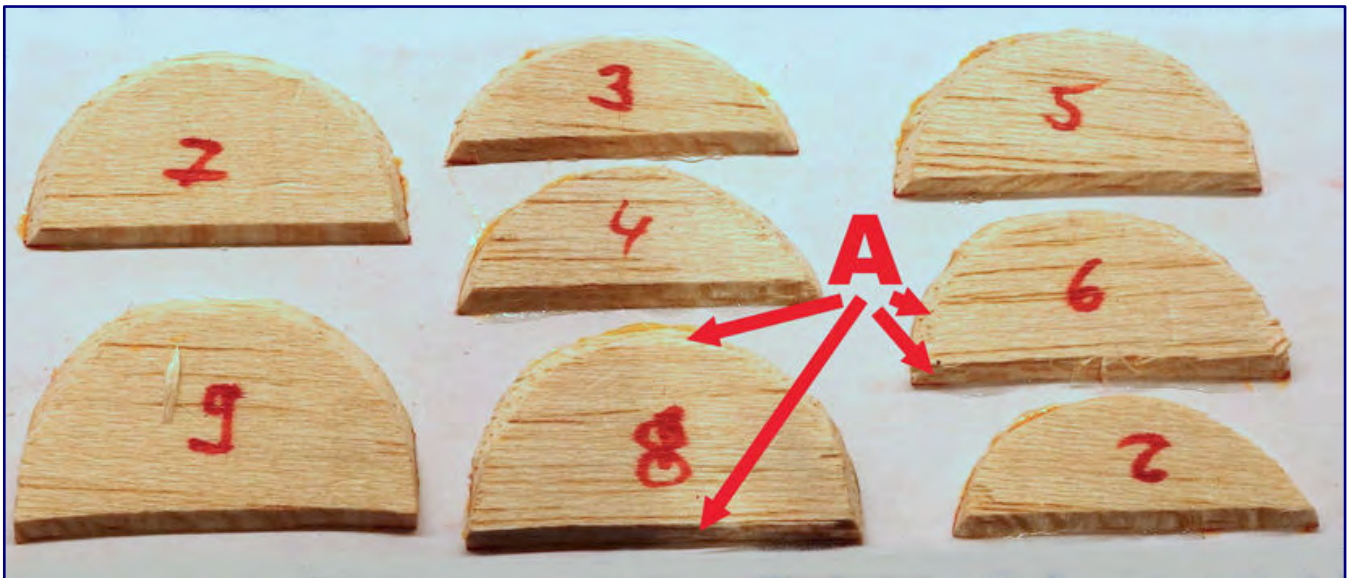
Tip "Template"

Because several arch segments have to be prepared as infill, it makes sense to cut out a template of an arcade arch and trace the outlines of the infill from here.

An already completed ramp with arcade arch (without mounted cantilever plate) can also be used as a template, the guidance of the pin must then be carried out somewhat more meticulously due to the thickness of the ramp.

The sloping fillings must, however, be bevelled at the back on the upper and lower edges so that they reach into the top of the round arch and fit tightly there with the front side.

And don't forget to number the parts, as it saves a lot of trial and error when gluing.



The slanted fillings are bevelled at the back on the upper and lower edges so that they reach into the top of the round arch and fit tightly there with the front side. Numbering the parts helps later in installing them correctly.

In contrast to the piers and the fall protection made of brickwork, I choose a walling of natural stone for the closed arcades. This matches the supporting walls of the stained slate rocks and thus creates the connection to the left side of the layout.



For the natural stone masonry, a photograph of the Zons Fortress served as a template, which was scaled as visibly as possible.

With the three materials (concrete of the arcade arches, brick masonry of the pillars and parapet walls, and natural stone of the wall panels as arcade filling) there is enough “visual” variety, that more is not necessary.

For the natural stone masonry, I am again compiling a DIN A4 page structure of a masonry taken at the Zons Fortress on the computer in an image processing programme. I also offer this structure for free download on my internet pages.

Tip "Paper margin"
There will always be smallest inaccuracies between the arcade round arch and the filling element to be inserted. To avoid gaps between the round arch and the brickwork, I leave the structural lamination minimally over the wood on all sides.
When inserting the filling element into the round arch, I then press the paper, which is soft from the glue, into the slot or against the arcade arch. If there are any small steps due to the thickness of the paper, they are either not visible or will be covered up later with the colour finishing.

Printed on either 80 g/m² or 180 g/m² paper, I glue the structure onto the cut-out filling elements with all-purpose glue. Due to the masonry captured in grazing light and in extremely high resolution, a high optical three-dimensionality is still achieved even with two-dimensional printing in Z.

The segment shown on page 34 has a stone height of 25 - 30 cm. when converted to original size, which is fine for a rubble stone wall, but still leaves the structure visible in Z gauge. Here, too, the use of structural foils is possible, as an alternative.



From this perspective, the versatility of the ramps and arcade arches can be clearly seen. Vegetation overgrowth and ageing contribute their part to the effect.

The rear arcade then presents itself with the left-side connection to the bridgehead of the crossing bridge structure designed in concrete.

As already mentioned at the beginning, I will lay the tracks as centrally as possible on the ramps. In order to meet the safety needs of the figures and the occupational health and safety regulations, I will install safety exits at intervals of approx. 13 cm.

This makes the ramp look more relaxed and is certainly a rather rare feature on layouts. In order not to overload this construction report, I will refrain from describing the rather simple reconstruction of this steel construction with only approx. 7 mm. high exits.

Finally, I weather the whole ramp construction. This includes the ageing of the concrete, dark green water drainage traces, white sintering, mossing and careful greening with hanging and creeping plants. I have already described the procedure in my “Volume I of Practical Instructions” and the subsequent titles.



You will also find further tips, e.g., on rock and path construction in the papier-mâché version and background information on the historic Bw with many old photos and reproduction instructions in the books published so far, which I offer via my website. Now I wish you a lot of fun with the layout to be built according to your own creative ideas.

All photos and drawings: Heribert Tönnies

Pages of the author:
<http://bestagernet.de>

Source of supply for Redutex:
<https://www.1zu220-shop.de>

Adhesive used:
<https://www.uhu.de>

The first driving tests (Part 2)

Curved Tracks on a good Foundation

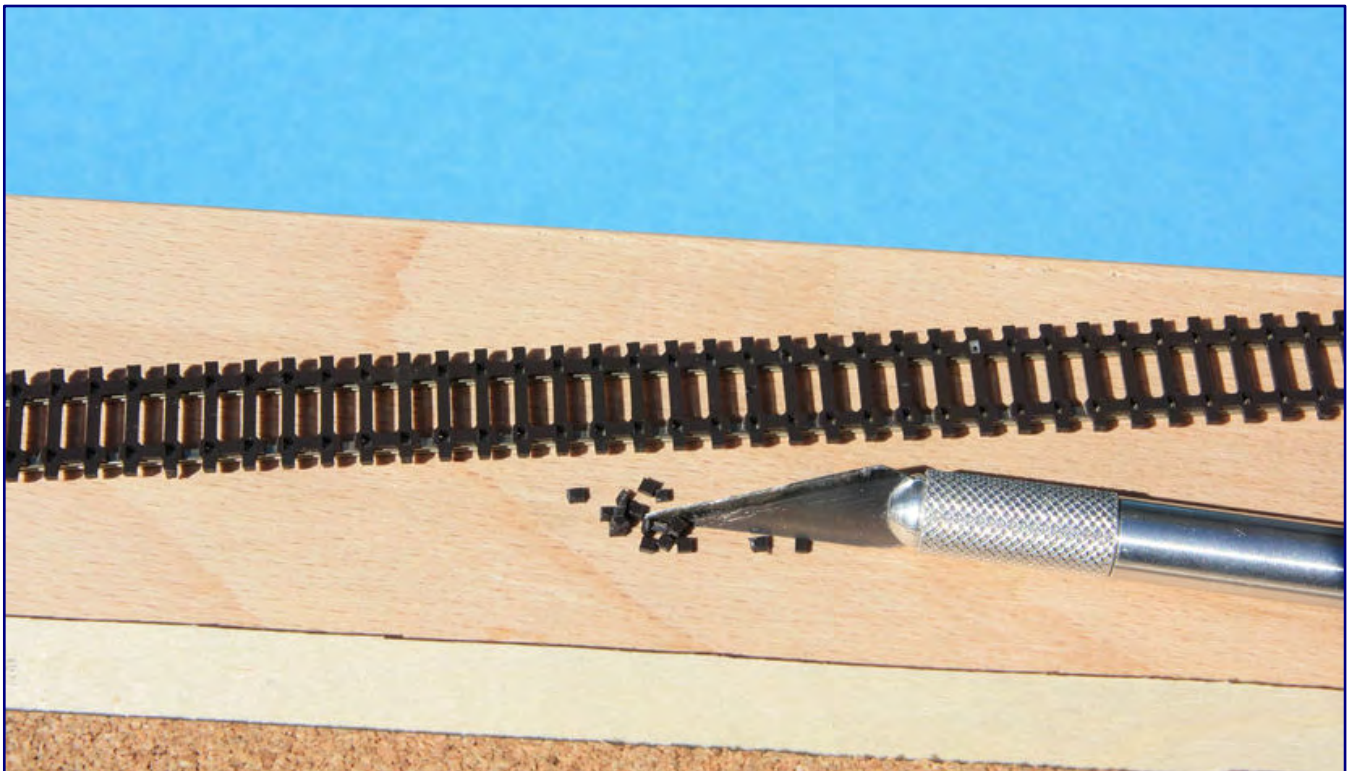
After basic considerations and introductions to track geometry, today we get down to work. To ensure that the first layout is a success right from the start, Dirk Kuhlmann gives hints and advice for an attractive and successful track layout, as well as successful woodwork that makes a convincing landscape possible in the first place. "Done right from the start" is the goal and result of his instructions in this sequel.

From Dirk Kuhlmann. Welcome to the second part of our fundamentals for a successful specimen of model railway. This time we want to show you a little bit more about working with flex-tracks and the necessary woodwork.

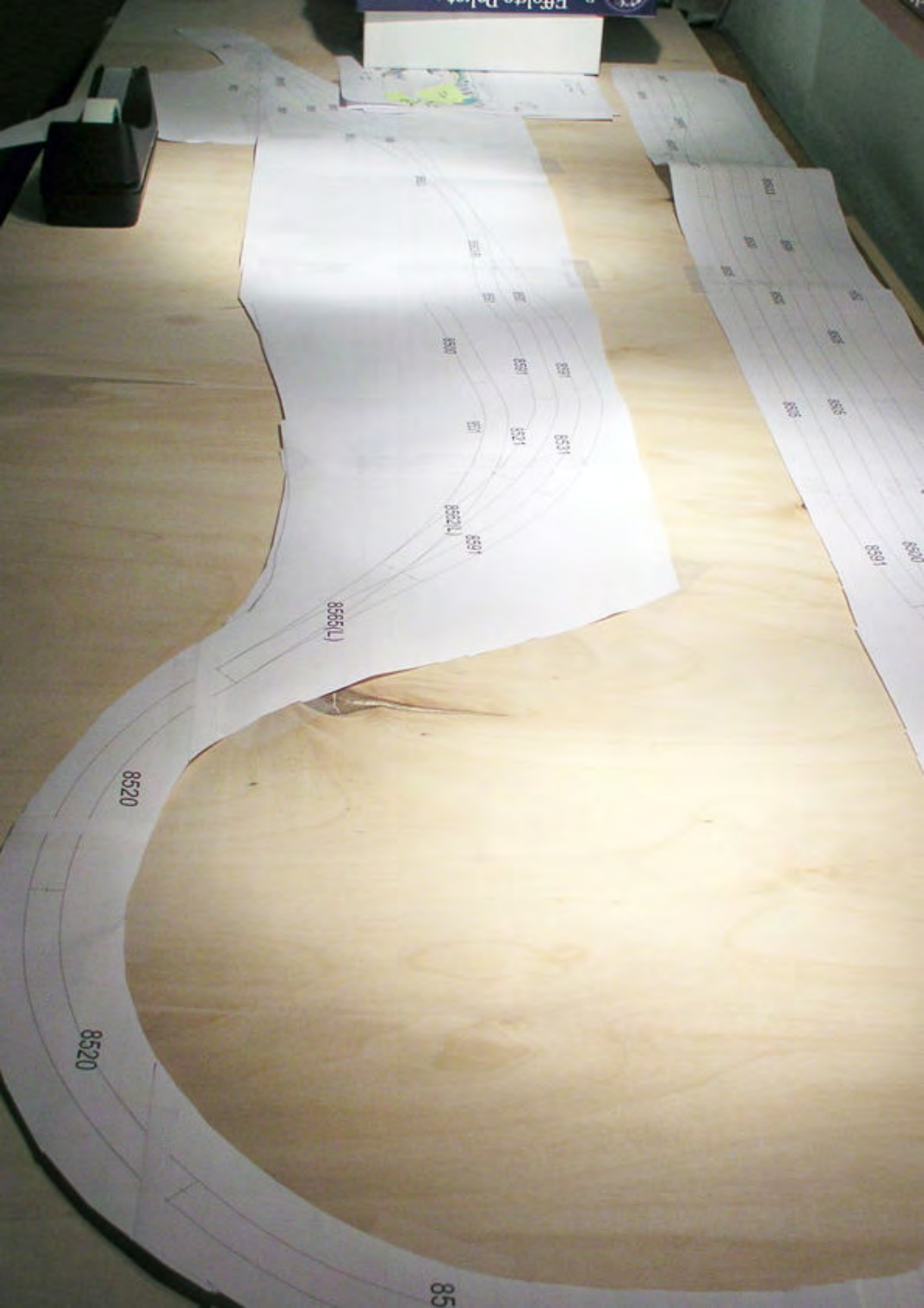
We already noticed in the first episode that the track systems from Märklin and Rokuhan are mature in themselves and leave only a few wishes unfulfilled. However, when it comes to highly sophisticated curves, the finished track bodies reach their aesthetic limits. Here, as with other scales, a flexible and longer track provides a welcome remedy.

Märklin flex track is 660 mm. long and thus combines six straight standard 110 mm. tracks in a single product. Before its use as a generously sweeping curve, however, it is necessary to remove the underside connections of the sleepers reciprocally with a sharp knife so that it can be bent without kinks.

continues on page 39



The flex track is perfect for elegant track designs, but handling it needs to be learned: The sleeper band can be opened alternately on the underside.



8565(L)

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8521(L)

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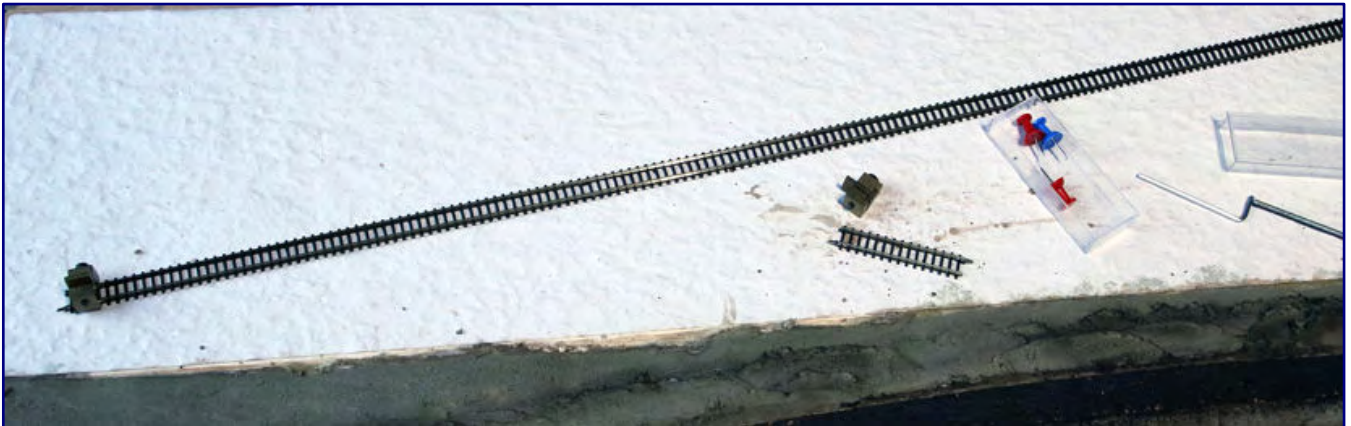
8520

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8520

The beginner is now recommended to use a 1:1 printout of his track plan as a useful and feasible template (see photo on page 38). This way, even a (perceived) rather complicated flex track can find its position on the layout.

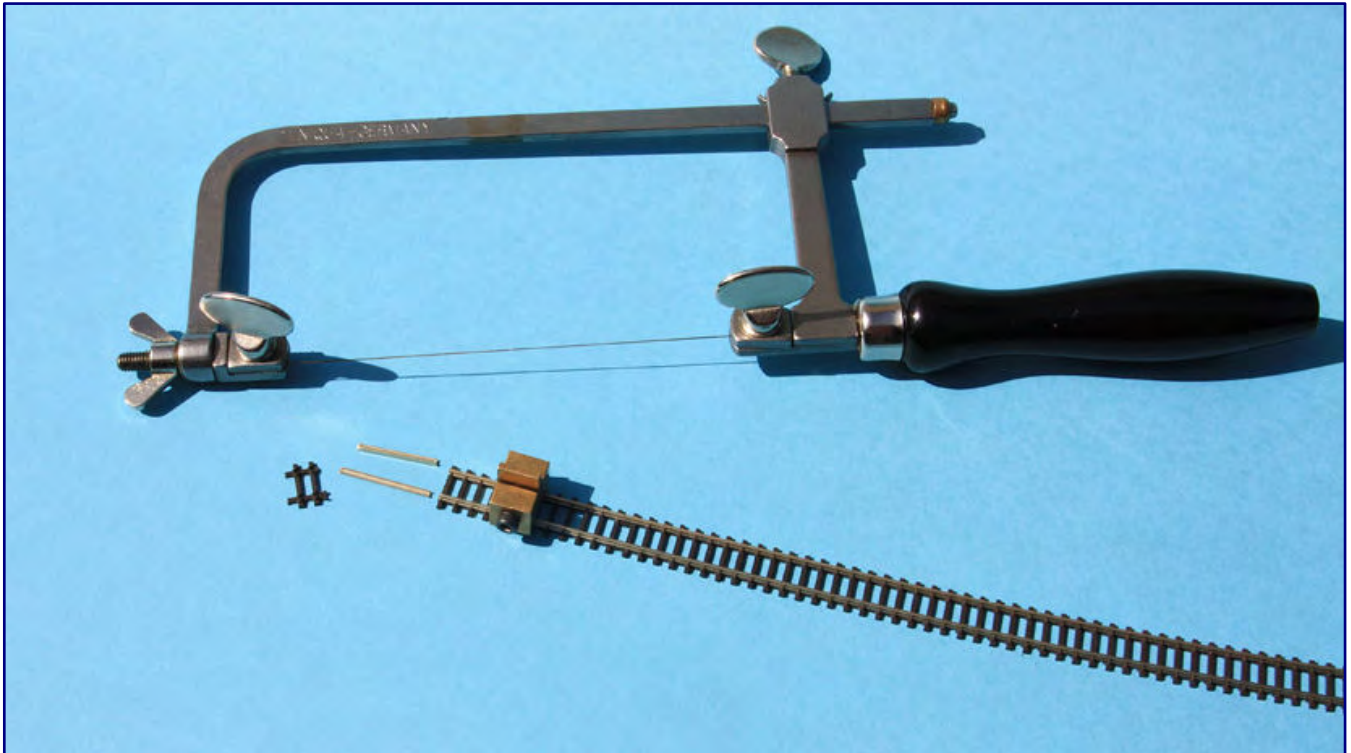
Since my beginnings, I have been working out a first curve with the help of the tried and tested spacer sticks, steel rulers and pins. The well-known Krause clamps (Fohrmann; art. no. 01 425) retain the track in the desired shape. I see this purchase as an absolute “must have” for this work! They represent a one-time investment that pays for itself quickly and permanently.



When working with the flex track, there is no way around Krause track clamps (in both photos) in the long run – a good and useful investment.

The subsequent and necessary trimming is done with the help of a jeweller's saw bow and a fine saw blade (32 teeth/cm.), afterwards track fixing nails (Märklin 8999) help to fix the sleeper strip to the base.

A facet-free side cutter also serves its purpose or the rather well-known track saw from Roco helps to shorten the outer rail to the dimension of the inner sibling. Only after the correct installation I remove the clamps. But one hint is important for me: Do some test work with used track, so that the different steps can be learned correctly, before you get down to real work. After the work is done, the big reward is a true-to-scale track layout.

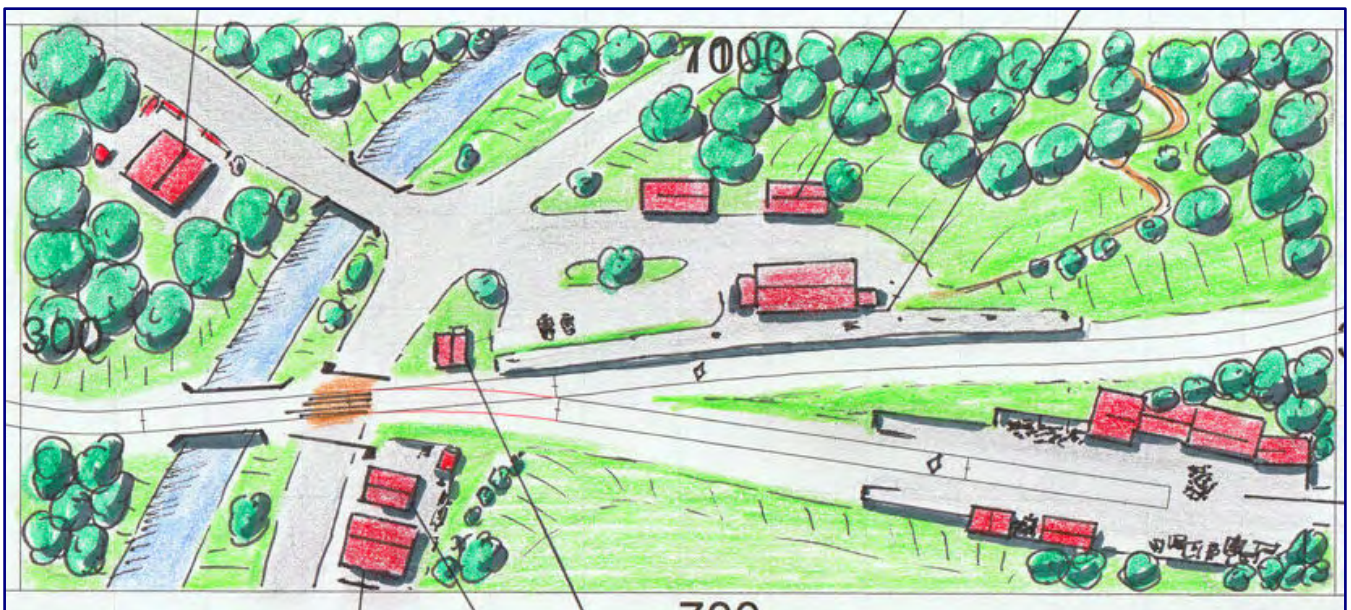


It is easy to see how the flex track was twisted into a gentle curve and fixed with the Krause clamp. The rail ends are now to be cut absolutely parallel and bevel free.

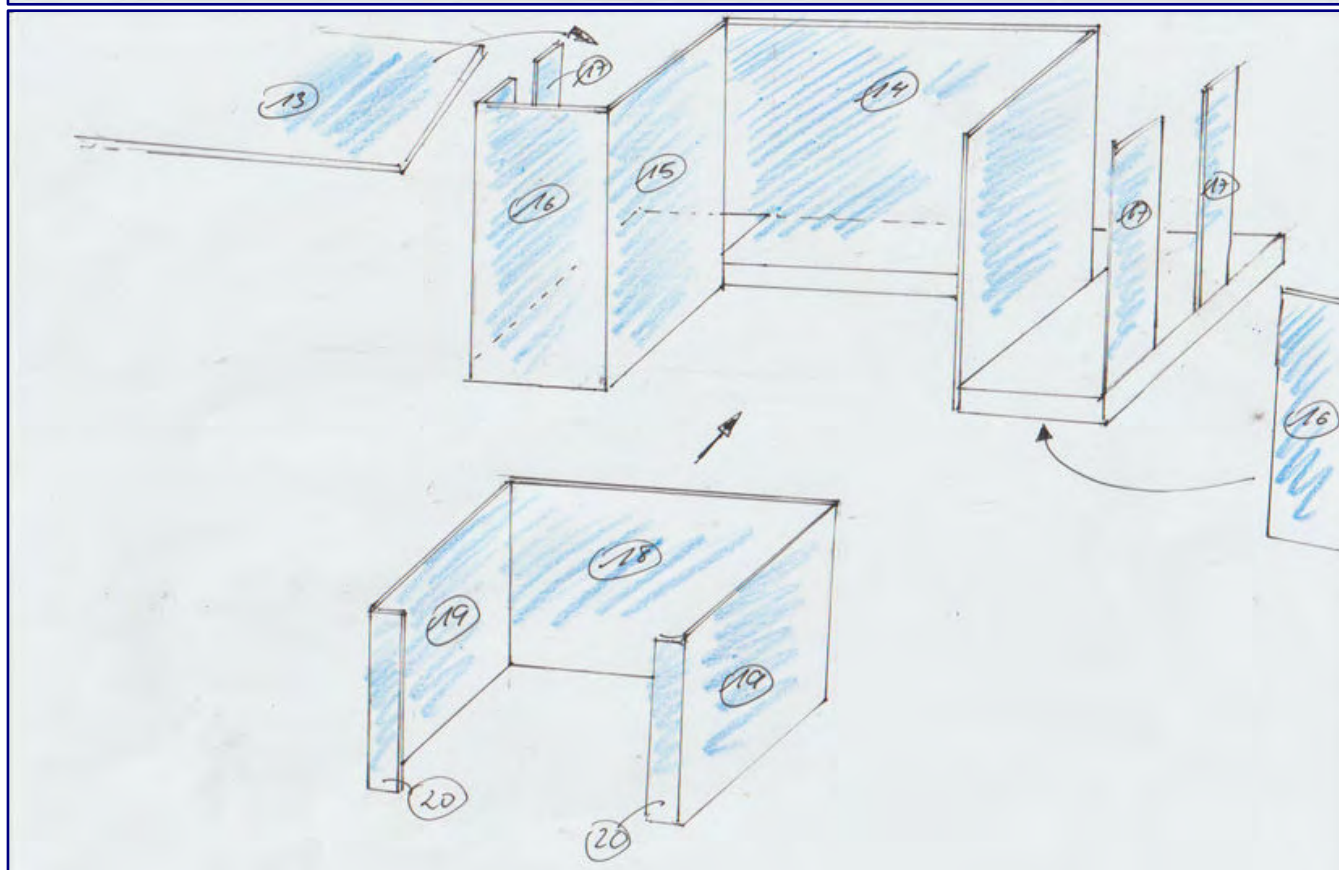
Woodwork with care

Let's now move on to a topic that causes model railroaders much more headaches – regardless of whether they are beginners or “old hands”: The wooden construction for their own small world. It is often simply called the “railway board”. And, all too often this expression is also implemented in this way.

continues on page 42



Sketch of the “Upland” exhibition layout.

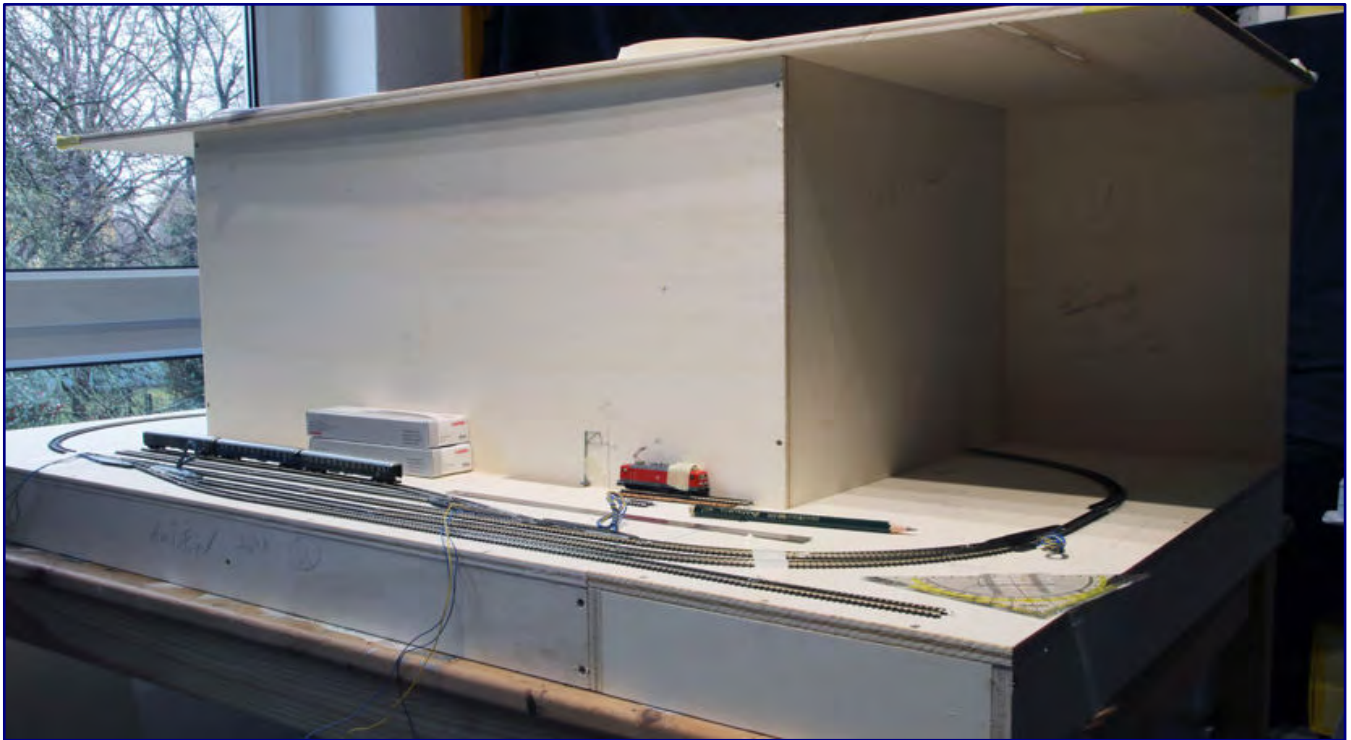


Many results often do not fulfil the final tastes of their builders. Consequently, beginner's mistakes can also be avoided at this point and more appealing results can be achieved with a little thought and planning.

In Part 1 I already recommended a smaller layout for beginners and returners. Now we also want to show the corresponding "railway layout" in its development.

Many readers asked again and again about the current state of an effective presentation in front of spectators. Here, you will find a variation that is very much in vogue. But please, everyone should decide this for themselves!

The basis in this case is of course also the present track plan, which provides the first dimensions. Our showpiece "Upland" will be 66 x 36 cm. in size, taking into account all the desired landscape features. A 6 cm. high box forms its basis.

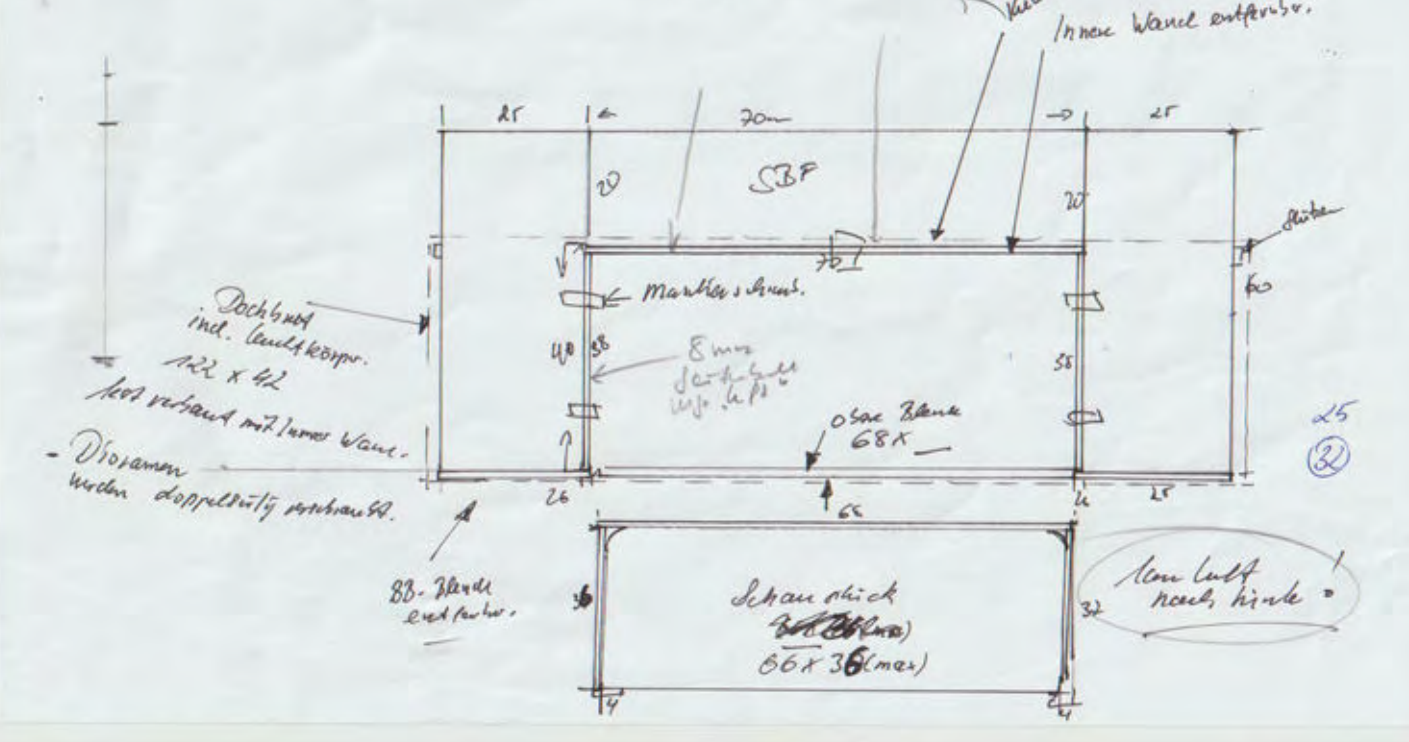
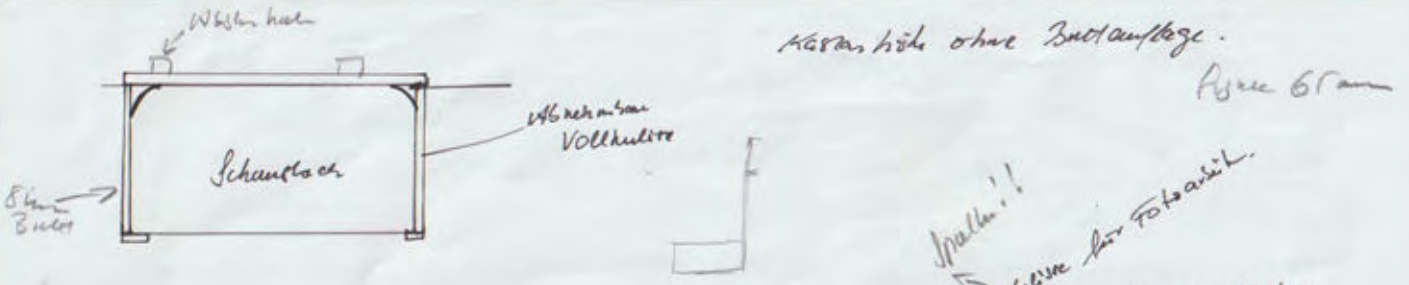


The unfinished box is in place, the first tracks are being laid on a trial basis and even the dimensions of the catenary for later passages are being determined.

With these key data I can now (as always) document the "remainder" in first sketches. Since it is to be a highly modern showpiece, only a "housing system" comes into question. Here, the electrical wiring, luminaries and background scenery are combined in one housing. A clean but scale-independent drawing provides the data for the individual wooden objects (see drawings on page 41).

If the result is satisfactory, a numbered list is used to assemble all the necessary individual parts. In order to save some costs, I create a sawing plan for my carpenter (first sketch and sawing plan see page 43).

continues on page 44



10mm Poppen 2500mm x 1700mm
Blatt ①

52x100		52x36		52x36		25x60			
						25x60			
36x66		36x66		36x32		36x32		34x6	
								34x6	
38x38		38x38		38x26		38x26		38x70	
								38x3	
								38x3	
								38x3	
								38x3	
								38x2	
								38x2	
								38x2	
								38x2	
								38x2	
6x60		6x60		6x60		6x60			
6x70		6x70		6x23		6x23		6x23	
6x23		6x23		6x66		6x66		6x18	
10x28		10x28		10x28		10x28		10x100	
5x40		5x40		5x68		5x100			
6x100		6x100		6x100		6x20		6x20	

A complete board of 10 mm. poplar plywood measures 2,500 mm. x 1,700 mm. as a rule. Please note that the dimensions on my sketches sometimes differ, and should, therefore, only be considered as examples. Thus, even during assembly, deviations from the plan may occur due to new ideas. In this case, a small mitre saw is required as an available tool.

In the case of different types of wood (e.g. beech), I ask my carpenter in advance for the dimensions of a complete board and draw the sawing plan according to the information received. I always take into account the cutting width of the saw blade, because there should be a few centimetres to spare!

I can really recommend that you go to a carpenter's workshop, because the cutting is simply better than in a DIY store. By the way, for insurance reasons, the DIY store (translator: in Germany) is not allowed to cut the wood to a width of less than 10 cm, whereas the carpenter in the specialist workshop is.

After a certain amount of time, the ordered blank cuts are ready and stored in the basement for processing, and sorting is now the order of the day. This takes a few hours.



This is what the box looked like in the summer of 2021, with only landscaping and the electrical installation to follow.

I also usually have leftover wood after cutting, as the carpenter basically sells and saws an entire board. In the further work, all of it is recycled for the landscape work. Larger leftover pieces always serve for the next projects, so nothing is wasted.

I also don't want to hide an important step before assembly. With a 25-mm. Forstner drill, the future cable passages are drilled into the struts. Then, it's finally assembly time.

With various metal angles, wood glue, Spax screws 2.5 x 16 mm. and a cordless screwdriver, it's time to get to work. Allow plenty of time for the wood to find its place at the right angle. The finished construction looks quite simple at first, almost boring.

But the further construction, carried out with sensible steps, comes closer and closer to the objective, and also provides much-needed motivation!

After initially laying out the tracks in the area of the future staging yard, including estimating the dimensions, the next step is to paint them. A 2 mm. thick cork mat serves as an insulating base for my track figures, then the rails are fixed with track nails. Finally, the box is already given its future lighting.



Cable passages and other openings are in place, now the lights will be installed for effective illumination. In the next part, we will get down to the basics of landscaping.

Make sure that the mains voltage connections are always kept separate from the future traction current. If all this has been taken into account and accomplished, you have already achieved a lot. You can look forward to the interim status achieved and part 3 of this series for beginners.

In the September edition we will show you the basic landscape construction and first attempts with the vegetation. From then on, your first work will also have a face and the progress will quickly become visible to outsiders. Until then, please remember: "Good things take time!"

Suppliers for tools and materials:

- <https://www.fohrmann.com>
- <https://www.maerklin.de>
- <https://www.ponal.de>
- <https://www.roco.cc>
- <https://www.uhu.de>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Die Geschichte der Baureihe 01

Portrait einer Berühmtheit

Was immer an der Baureihe 01 an Schwächen und Unzulänglichkeiten zu finden wäre, sie ist zweifelslos die Nummer 1 unter den deutschen Schnellzugdampflokomotiven gewesen. Sie war populär wie keine andere und schien lange Zeit überaus präsent, denn ihre Betriebsgeschichte endete erst 1973 bei der DB und 1985 bei der DR. Der EK-Verlag setzt dieser Baureihe nun das noch fehlende Denkmal in würdiger Weise.

Frank Lüdecke / Horst Troche
Die Baureihe 01 – Band 1
Der Star unter den Schnellzug-Dampflokomotiven der Deutschen Reichsbahn-Gesellschaft

EK-Verlag GmbH
Freiburg 2020

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344 Seiten mit 498 teilweise farbigen Abbildungen

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oder im Fach- und Buchhandel

Null-Eins – zwei Zahlen, mit denen jeder Eisenbahnfreund auf Anhieb etwas anfangen kann! In der Vorstellung taucht sofort die berühmte Schnellzugdampflok mit zwei Meter großen Kuppelrädern auf, die so viele Knaben und junge Männer zu begeistern wusste. Ihre Faszination ist bis heute ungebrochen.

Sie ist die Vorzeigelok der Deutschen Reichsbahn-Gesellschaft gewesen, obwohl sie weder die leistungsstärkste, schnellste oder zahlenmäßig am Stärksten vertretene Lok in ihrem Bestand war. Wie konnte sie sich so dominant und anhaltend im Bewusstsein der Menschen einprägen?

Wir halten also gleich zu Beginn dieser Besprechung erst mal fest, dass ein Baureihenportrait dieser Dampflok mehr als überfällig war, denn sie ist und bleibt für Eisenbahnfreude etwas Besonderes. Dass es so lange gedauert hat, hat aber auch seine Gründe.

Dieses Buch sollte das letzte große Werk von Dipl.-Ing. Horst Troche werden, der sich große Verdienste um die deutsche Eisenbahn und besonders die Dampflok erworben hat. Nach seinem hervorragenden Buch über die leichtere Baureihe 03 wollte er auch ihre große Schwester noch in gleicher Weise würdigen.

Doch daraus wurde nichts. Bevor sein großes Werk fertig wurde, verstarb er. Frank Lüdecke konnte sein Werk nun zu Ende führen und auf seine Manuskripte und Unterlagen zurückgreifen. Er würdigt seinen Vorgänger entsprechend in angemessener Weise als Mitautor.



Auch dieses Buch ist wieder, so lautet das Gesamtfazit, in nahezu perfekter Weise gelungen: umfassend aufgearbeitet, interessant präsentiert mit vielen passenden Fotos bester Wiedergabequalität und dazu auch einige überraschende Informationen. Das tolle Werk wird der historischen Vorlage mehr als gerecht!

Fast 100 Jahre ist es her, als die Baureihe 01 auf die Schienen kam und zusammen mit der 02 die Geschichte der Einheitsdampflok einleitete. Und auch, wenn sie nun fast schon vierzig Jahre abgeschlossen ist, bringen heute zugängliche Quellen noch Neues ans Tageslicht. Frank Lüdecke entwirft daraus im geschichtlichen Kontext gekonnt ein detailliertes Panorama dieser bemerkenswerten Lokomotive.

Werfen wir einen kurzen Blick darauf: Die Vorgaben für die Einheitslokomotiven waren eine deutliche Leistungssteigerung gegenüber Länderbahnmaschinen, ein Vereinheitlichen von Teilen als Beitrag für Rationalisierung und höchste Wirtschaftlichkeit. Anders als bei europäischen Nachbarn ging es nicht um eine reine Leistungsmaximierung.

Untrennbar mit der Einheitslok verbunden bleibt der Name Richard Paul Wagner als deren „Vater“. Viel zu lange hielt er jedoch an den zu Beginn der Zwanziger aufgestellten Grundsätzen fest, auch als sie erkennbar und nachweisbar überholt waren. Dieser Persönlichkeit, weiteren Beteiligten sowie auch dem Nachfolger Friedrich Witte wird ebenfalls ein Abschnitt des Buches gewidmet.

Das hilft beim Einordnen vieler Details und Merkmale, die im Buch beschrieben und hervorgehoben werden. Überraschend ist jedoch zu erfahren, dass die Deutsche Reichsbahn sich Mitte der dreißiger Jahre intensiv mit dem französischen Dampflokbaubau beschäftigte und sogar noch mehrere Maschinen mit Verbrennungskammerkessel und Vierzylinder-Verbundtriebwerk bauen wollte – verhindert allerdings durch den Krieg.

Das sind Facetten, die bislang nicht nachgewiesen werden konnten und anderswo folglich auch nicht zu lesen waren. Sie machen dieses Werk umso wertvoller, weil dies nicht nur die Geschichte der Baureihe 01 betrifft, deren Höhepunkte in den fünfziger Jahren in Ost und West ebenso wenig vergessen werden.

Natürlich fanden auch Bauartänderungen, Modernisierungen und Umbauten wie das Neubekesseln in Ost und West Eingang in diese Fachlektüre. Interessant ist auch ein Kapitel über das Bespannen des Rheingold kurz vor dem Zweiten Weltkrieg, um diesen Zug zu beschleunigen.

Statistiken, Daten über Laufleistungen oder Kohleverbrauch, Bilder von AW-Aufenthalten und die Betriebsdaten der 241 zwischen 1925 und 1938 gebauten oder umgebauten Lokomotiven runden auch dieses hervorragende Buch wieder ab. Auch den 14 erhaltenen Exemplaren (gezählt ohne die 5 Maschinen der Baureihe 01⁵) oder Anekdoten aus dem Betrieb widmet der Autor in angemessener Weise Raum.

Bei all den guten Worten über diesen Band, fragen Sie sich vielleicht, was vielleicht hätte besser gelöst werden können? Und ganz ehrlich, auf diese Frage können wir keine Antwort geben. In Summe ist dieses Portrait nahe an unserer Idealvorstellung dessen, was wir erwarten möchten.

Zu ergänzen bleibt noch, dass der vorliegende Band 1 vor allem die Entstehungsgeschichte mit der Konkurrenz von Vierzylinder-Verbund- und Zweizylinder-Triebwerk behandelt. Versuchsergebnisse und Vergleichsversuche mit Länderbahnlokomotiven werden deshalb ausführlich beleuchtet, ebenso natürlich die Technik dieser Baureihe.

Inzwischen ist auch Band 2 erschienen, den wir ebenfalls rezensieren werden. Dieser widmet sich dem Einsatz bei den Bahnbetriebswerken und weiteren Daten zu den Maschinen, die nicht in den Band 1 gehören. Zusammen ergeben sie das neue Standardwerk über die Baureihe 01, was nach den vielen lobenden Worten sicher kein Geheimnis mehr ist.

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Die schönsten Bahnepochen Wir wünschen allzeit gute Fahrt!

Eine neue Fachzeitschrift für Eisenbahnen zu gründen, darf sicher als ein mutiges Unterfangen bezeichnet werden. Umso mehr hat uns die neue Zeitschrift Eisenbahnklassik neugierig gemacht. Für unsere Leser haben wir einen Blick in die erste Ausgabe geworfen. Nun halten wir fest, was sie bieten kann und will, ebenso aber auch, welche Wünsche an künftige Ausgaben gestellt werden dürfen.

Robin Garn (Hrsg.)
Eisenbahnklassik
Geschichte • Kultur • Fotografie

Nord Süd Express GmbH
Gröbenzell ab 2021

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Während wir uns über viele Jahre stets wachsenden Zuspruchs erfreuen durften, kämpften viele Zeitschriften der Printpresse mit ständig zurückgehenden Auflagen. Als Hauptursache wurde ein schrumpfender Markt ausgemacht, in dem der Altersdurchschnitt der Konsumenten immer weiter steigt.

Und auch angesichts eines offenbar eingetretenen Wandels, wie das Hobby Eisen- oder Modellbahn öffentlich wahrgenommen wird, und einem Beleben als Folge der Freiheitsbeschränkungen in Folge der Pandemie bleibt heute noch unklar, wie nachhaltig zuletzt positive Entwicklungen sein werden.

Mitten in der Phase, die wir hier skizzieren, übernahm Geramond im letzten Jahr die Verlagsgruppe Bahn (VGB). Das gesamte Programm kam auf den Prüfstand, wurde auf Wirtschaftlichkeit und Überschneidungen mit Titeln aus dem bereits vorhandenen Portfolio überprüft.

Als Folge wurden einige Zeitschriften eingestellt und einige ihrer bewährten Kernelemente in andere Titel überführt. Ein Opfer dieser Entwicklung war das Magazin Bahnepoche. Geramond-Chefredakteur Michael Hofbauer schrieb dazu jüngst: „(...) es bestand angesichts der Ausrichtung (...) auf eine hochspezialisierte Zielgruppe leider auch keine realistische Chance, es jemals aus den roten Zahlen zu bekommen.“

Mit Robin Garn als Herausgeber hätte sich Geramond jedoch eine weitere Zusammenarbeit gewünscht, doch es sollte anders kommen. Und genau das führt uns zu dieser Titelbesprechung. Wir gehen unbefangen an diese Aufgabe, denn die eingestellte Zeitschrift gehörte nicht zu denen, die wir regelmäßig konsumierten.

Die von Karlheinz Werner und Thomas Hilge – früherer VGB-Verlagsleiter - neu gegründete Nord Süd Express GmbH möchte an eine Tradition anknüpfen und die Lücke im Zeitschriftenmarkt durch einen



eigenen Nachfolger schließen. Dauerhaft gelingen kann dies freilich nur, wenn sich mit den günstigeren Kostenstrukturen eines kleineren Verlags wirtschaftlicher arbeiten lässt und möglichst auch neue und breitere Leserschichten angesprochen werden.

Der Neuankömmling am Fachzeitschriftenmarkt heißt Eisenbahnklassik, bietet dem Leser einhundert Seiten Fachinformationen und soll künftig vierteljährlich erscheinen. Die hier zu besprechende Erstausgabe erschien Ende Juni und soll vertraut wirken, wenn die Vorgängerzeitschrift bekannt ist.

Das neue Magazin soll die Geschichte, Kultur und Fotografie der klassischen Eisenbahn mit Anspruch und Herzblut vereinen. Sachkundige Berichten, Reportagen und Geschichten von Zeitzeugen wollen Autoren und Redaktion ihren Lesern nahebringen.

Als Zeitreise mit unwiederbringlichen Momenten und neuen Erkenntnissen, epochalen Fotos und seltenen Dokumenten metaphorisch umschrieben, waren wir gespannt, was uns erwarten würde. Und der erste Eindruck passt: Die beiden größten Themen des Hefts, die zusammen fast die Hälfte aller Seite füllen, halten das Versprechen.

Aufschlussreiche Rechercheergebnisse, ansprechende und nicht schon hinreichend bekannte Fotografien sowie bislang kaum bekannte Fakten fesseln den Lesern. So war es bislang kaum bekannt, was die Sowjetunion mit den elektrischen Beutelokomotiven aus ihrer Besatzungszone vor hatte und tatsächlich angestellt hat.

Auch das – für den Rezensenten wegen Heimatverkehrsgeschichte besonders interessante – Titelthema über die von Dortmund aus eingesetzte Schnellzugdampflok der Baureihe 03¹⁰ im Zeitraum 1950 – 1958 war bislang eher ein Stiefkind der Eisenbahnliteratur. Über Fahrleistungsrekorde lässt sich nur staunen und endlich bekommen wir einen Eindruck, wie der stahlblaue Lack diesen Maschinen wirklich stand.

Aber das neue Heft hat auch Schwächen, die sich abstellen lassen, schließlich muss es seine Identität erst finden, wobei wir hoffentlich auch helfen können. Schwachpunkte sind auffallend viele Grammatik- und Bezugsfehler, die den Lesefluss stellenweise behindern und den Eindruck erwecken, dass hier mit massivem Zeitdruck gearbeitet werden musste.

Und Abkürzungen kennen im Deutschen per definitionem keinen Plural, weder mit noch ohne ein durch Apostroph abgesetztes s, das es im Übrigen auch in der englischen Sprache nicht gibt und deshalb auch als „Deppenapostroph“ stigmatisiert ist. Überflüssig in einem Eisenbahnmagazin sind auch politische Seitenhiebe („König:innen der Laufleistungen“) zur Gender-Ideologie.

Viele weitere Themen werden in der vorliegenden Ausgabe noch gestreift, die deutlich zeigen, wie breit die Themen und bunt die Facetten der historischen Eisenbahn sind, die eben nicht mehr bei der Dampflok aufhört! Leider kommen viele nicht über maximal eine Doppelseite hinaus und bleiben dadurch sehr an der Oberfläche. Hier sehen wir die Punkte, wo aktuell noch nach der idealen Ausrichtung und eigenen Identität gesucht wird.

Passende Literaturempfehlungen gehören auf jeden Fall hierher, auch die Rubrik „Fahren und Bewahren“ schlägt eine sinnvolle und für uns wünschenswerte Brücke zum heutigen Museumsbahnbetrieb. Und der Blick ins Ausland über ein ReisetHEMA kann ebenfalls bereichern wirken, wenn der Umfang nicht übertrieben wird, was hier übrigens gut gelungen ist.

Sehr gefallen haben uns übrigens historische Aufnahmen vom Bahnbetrieb in Basel (1932 – 1962), der rollende Legenden und Alltagsbetrieb als Fotostrecke präsentiert. Fazit: lesenswert!

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Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Praise and understanding around the new pages:

First of all, praise for the new Trainini look! You have to get used to everything new, but it occurs very quickly. Also, a big thumbs up for your magazine, which is steadily heading for No. 200, always great, informative, critical, realistic reports and features! Thank you very much!!!

Since you do this, as we do our hobby, free of charge, I wish all the sick people a speedy recovery and everyone else stay healthy and don't get stressed out, we are used to very different delivery times from our main manufacturer.

So first come family, health, career and then the hobby!

Andreas Schärf, by Mail

Good words from another reader:

It is with great regret that I have learned of the serious problems that afflict you all in the context of your great new appearance of Trainini. Whatever the case may be, I hope not all fears come true, and you can soon get back to your dreams and plans with confidence and health in body and soul.

I'm sure many are now waiting for the next issue of Trainini, but for this dedicated team there are probably more important things to do now. I / we are happy to wait! Stay safe!

Dr. Ingo Patschke, by E-Mail

Approval of the page message of 25 June 2021:

I write to express my sincere concerns and best wishes for your current situation and to let you know all of us here in America are quietly awaiting your return with a fantastic monthly publication.

Thank you and return when you are able. We understand. Take good care.

Dwight Landen, by E-Mail



Great understanding from our readership:

When a large profit-oriented toy manufacturer comes up with “fantastic” delivery date lists, it can get on your nerves.

However, if a small organisation like “Trainini” has to postpone the publication dates of its free magazine for personal reasons, then that is acceptable. We can only hope that good wishes for recovery of all concerned will suffice, and that there are no more tragic reasons.

Roland Graesser, Pincourt QC (Canada)

Editor's reply: We were very pleased to receive a wide variety of letters such as the ones mentioned above, which gave us feedback on the understanding and encouragement of our readers. Unfortunately, the situation is and will remain tense, because these are events and strokes of fate that in many respects will continue to have an impact for some time to come in terms of number, severity and timing. The recent flood disaster has also affected parts of our team at least somewhat and made their work more difficult.

Perfectly successful Leig unit:

A long time ago, the report: “Easy (made) express goods train” was presented in issue 5/2010. Using two of the then museum car 2009, a single unit was made from it.

As a concluding remark it was written: “Our Leig unit was created with quite simple means so that the proposal can be replicated by many model railway enthusiasts. As possible refinements for the skilled craftsman remain the addition of a toilet downpipe, the cutting of windows and the reproduction of window crosses as well as doors at the front sides. However, we deliberately leave this to the imagination of our readers.”

Since I was still missing these wagons and four museum wagons were in the showcase, I set about converting this interesting type of wagon into Z. With the help of the Miba freight car volume 1, I had drawings and dimensions at hand, so I could start.

I chose the unit with 8 m axle distance and formed a true-to-scale underframe by courageous cuts. That was the basis. Then, a total of 20 holes were drilled in the car bodies, which then led to the window/door cut-outs using a key file with the patience of a saint.



The toilet window was not forgotten either. The window/door panels/rungs were then created with PS profiles, the rubber beads were created from MS pipe. (There were both bellows and rubber beading!).



Two Leig units of different designs have been produced by our reader after a contribution suggestion from May 2010 on the basis of the museum wagons 2009. The left of the two has additionally received a roof with beading. Both photos: Dirk Rohwerder

On one unit, the roof beading was also reproduced as a variant and covered with a wafer-thin layer. The distinctive lettering was printed on a decal using an inkjet printer.

After this work was finished, the cars got a patina, the windows were glazed, and one car got end windows. After that the double units were firmly coupled. This tinkering project was really fun, now the express goods train can do its rounds.

Dirk Rohwerder, Sprockhövel

Former dealer and small series manufacturer deceased:

Already on 19 April 2021 Dipl.-Ing. Wilfried Schmidt from Hameln passed away at the age of 79. Under his former company name "Spur-Z-Partner Schmidt," he also took part in the International Spur-Z meetings in Geseke and Altenbeken, as well as in many trade fairs until his retirement.

Long-time model railway enthusiasts will still know him as a model railway dealer, small series distributor, and also small series manufacturer. Michael Bahls, who is well known today, also began his professional career there, before he also started his own business as a small series manufacturer.

With Dipl.-Ing. Wilfried Schmidt, the Z gauge community loses a deserving and committed member of its early days, who was able to give much input and also contributed to the fact that its scene could develop in a colourful and versatile way. Our condolences go to his wife Herta and all other relatives.

New magazine for role model friends:

"Where do railway enthusiasts feel at home? Certainly not between barren noise barriers and overgrown tracks of a railway that stops operating because there are leaves on the track or a switch is frozen."



Cover image: Nord Süd Express

This is how the young publishing house Nord Süd Express (<https://www.nordsuedexpress.de>) from Gröbenzell introduces a short presentation of its new magazine Eisenbahnklassik (Railway Classics), which we also present in a review in this issue.

So it turns back the clock according to its own understanding and wants to make a big promise to its readers: more classic railways, more information from the great times of rail transport than in any other magazine.

Anyone who is interested in the great era of the railway, when different types of traction were still on the road side by side, and the railway became an engine of the economic miracle, is in the right place. According to the publisher, the magazine sees itself as innovative.

The content focuses on the history, photography and culture of 20th-century German railways, but also includes information on today's operators of nostalgic and museum railways as well as other "preservers" of historical relics of rail transport. In future, expert reports, major features and short stories will alternate on 100 pages each.

Change of Management Board at the Spielwarenmesse:

On 1 July 2021, following the retirement of board members

Ernst Kick (chair) and Dr Hans-Juergen Richter, a new management trio took over with Florian Hess, Jens Pflüger, and Christian Ulrich (spokesperson).

All three are very familiar with their areas of operation. In addition to the joint management of the company and the supervision of subsidiaries and participations, the tasks are clearly structured: Florian Hess acts as Board Member for Fair Management, Jens Pflüger is responsible for finance, human resources and information technology, and Christian Ulrich, in addition to his role as spokesperson, is in charge of marketing and public relations.

Märklin new deliveries in July:

Six new items arrived at the dealers in the last few weeks. Among them is the six-piece container wagon set (item no. 82665), a very interesting set for the early Era V, which combines the familiar photo from Bundesbahn times with the beginning age of privatised railways.

It consists of two Sgs 693 carrying wagons and four two-axle Lgjs 598 for combined freight traffic. They transport various 20- and 40-foot shipping containers as well as tank containers. Striking and well-known designs of TFG (Bundesbahn), Hapag-Lloyd or Maersk, still combined with the addition "Sealand," clearly point to the period of operation.

The two double-decker car transport wagons DDm of the ÖBB with patch in pure orange (87095) belong to epoch IV and were also found across borders. They always ran in passenger trains and were not to be found in goods trains, as their classification as luggage wagons already makes clear. They can also be combined with Bundesbahn rolling stock without any problems.



ÖBB's DDm car transport wagons in pure orange also travelled across borders and can thus also be lined up with German passenger coaches without any problems, for example.

The blue-grey paint scheme of the Deutsche Reichsbahn is worn by the express train locomotive E 18 053 (88083), which many friends of Era II will have long awaited. Converted to bell armature drive and illuminated with warm white LEDs depending on the direction of travel, it has become a technically contemporary model.

A Bundesbahn classic in a modern guise is SGL's grey V 270.09 (88205) for Era VI. The former V 2001 of the Bundesbahn is in this design the Nuremberg Fair special model 2021, which unfortunately had to do without its usual exhibition frame.

Further new product deliveries are the annual wagon 2021 for the Märklin magazine (80831), which takes the four-axle self-unloading wagon with hinged cover Tad-u 961 of the DB as a model and the sliding wall wagon Hbbins of the SBB in advertising design of Tela-Kimberly (82385), which is on the road in epoch VI.

Flood Donations via Joswood:

The Bergisch accessories supplier Joswood (<https://lasercut-shop.de>), itself located in the middle of one of the severely affected flood areas, has started a fundraising campaign to benefit institutions that have suffered flood damage or are helping flood victims.

The handicraft business is grateful to have come through this catastrophe without any damage itself and now wants to show solidarity with those who were not so lucky. The recipients are still being selected and will be announced later.

For ten days from 18 July 2021, Joswood intends to donate 50% of its turnover generated during this period to the aforementioned causes. Owner Jörg Schmidt announces this in a video at the following address: <https://youtu.be/TkQ4Ynsa-c4>.

Zetties who want to participate here with a purchase are reminded of our report in **Trainini®** 4/2021, in which we presented wooden overseas boxes of this manufacturer and effectively staged them on Märklin SSy 45 heavy-duty wagons. The good response from our readers strengthens us in the view that friends of our scale can also provide good assistance here.

Loading scene of the Bundeswehr:

Inspired by the latest new products we have presented here, as well as reports and hints from our readers Hartmut Schnittjer (**Trainini®** 5/2021) and Christian Liebau (**Trainini®** 6/2021), we have produced a loading scene of Bundeswehr tanks at a loading ramp, as close to the prototype, as possible.

The heavy goods vehicles SSy 45 (art. no. 82229) and SSym 46 (82352) from Märklin were used, as well as a Leopard 1A1 tank from Z-Panzer, soldiers from a "Accessories Air Force personell" figure pack from Herpa (551663) and the new MAN AE Cargo Bundeswehr (MAN 630 AE; 322.032) from Artitec.



With the help of Märklin heavy-duty wagons, as well as accessories from Artitec, Herpa, and Z-Panzer, a Bundeswehr loading scene in late Era III was created, as further inspiration for our readers.

This scene could be supplemented by other tank and Unimog models from the aforementioned manufacturer, olive green painted vehicles such as the VW Transporter or also the MAN 630 from Ratimo-Z in the three design variations offered.

Shorty-Models of USA classics:

In Japan, which is plagued by a lack of space, so-called shorty models in all common scales are a big hit. This often trivialised and always shortened rolling stock allows many model railway enthusiasts to present their hobby in the home environment. Furthermore, children often feel very attracted to it.

Some years ago, Rokuhan expanded its product line in Z gauge accordingly. After several Japanese models, the US EMD F7 is now the inspiration for the entry into the North American, but also the European market as an A and B unit. Initially, versions of the Santa Fa (Item No. ST012-1) and New York Central (ST012-2) are available.

At Noch, they will certainly be added to the sales programme in the near future. Ex-works, they are painted and printed bodies on a unpowered chassis. To set the models in motion, powered and motorless chassis are already available separately in the Rokuhan range.

We have been told that, if the market is successful enough, matching carriages and conversions based on European models are also planned.

Faller new products available:

Faller is celebrating its 75th anniversary this year, because Edwin and Hermann Faller began in 1946 with the creation of the first construction kits and finished models of the accessories range. The most important new products for Z gauge can, therefore, also be regarded as an anniversary model, because



The recently delivered Oberneulander Mühle (item no. 282789), a gallery Dutchman, is more or less Faller's anniversary model for Z gauge to mark its 75th anniversary, as it is completely in the founding tradition of appealing functional models. Photo: Faller

it is something special in terms of size and function and is very much in the tradition of the company founders.

The Dutch windmill with an octagonal ground plan (art. no. 282789) consisting of 213 solid-coloured hardboard parts in seven colours has now arrived in the shops. Like the prototype, it has a surrounding gallery with railings, which distinguishes it from the so-called smaller earthen windmills.

The functional model is based on the listed "Oberneulander Mühle" in the Bremen district of the same name. With the separately available Faller gear motor (180722), the blades can be set in motion.

Since the design of the mill bonnet (cap), which deviates from the original, caused criticism, we are still planning a construction report including motorisation. In this context we would like to look for solutions on how to bring this part closer to the prototype, without losing its movement functions.

In view of the situation that has already been communicated, however, we assume that we will not be able to fulfil this plan until the 2022 vintage. Since, it is questionable how long the kits will be available, interested parties should stock up early in order to be able to build our proposal in good time.

MTL continues wagon series:

Micro-Trains continues the delivery of the Sweet Liquid series with wagon number 5. The latest model in this series is the light grey example of a 39-foot tank car discontinued by GATX with inscriptions from Domino Sugar (Item No. 530 00 530).

The company to which it is dedicated is now the largest sugar manufacturer in the United States of America.

Micro-Trains products are distributed, among others, by Case-Hobbies (<http://case-hobbies.de>).

Tank and car models, also for Z gauge:

The Dutch supplier Panzer-Shop.nl specialises in tank models and army vehicles in scales of 1:160 and 1:120. However, they also offer interesting vehicles for Z gauge, ranging from the time before the Second World War to the present day, all of which are 3D printed conversions.



The latest offshoot of the Sweet Liquid series is this "Domino Sugar" tank car (Item No. 530 00 530). Photo: Micro-Trains



Ford V3000 (left) and Mercedes-Benz L4500 (right) are two of the vehicles offered by Panzer-Shop.nl for Z gauge, in this case for Era II, and, as war surviving examples, for early Era III. Photos: Panzer-Shop.nl

Sorted by country, individual vehicles can of course appear twice in the range if they are in service with different NATO partners. Construction site vehicles, such as a Caterpillar track excavator or a wheeled loader (earth mover), are particularly interesting.

Civilian commercial vehicles can also be found in the assortment at <https://www.panzer-shop.nl>. The site is available in four languages, in order to be able to obtain information and order easily.

Four new products from AZL in July:

New at American Z Line in July is the EMD F7 as a combination of A and B unit of the Erie Lackawanna (item no. 63012-1). It is complemented by a single A-unit (63012-2). The mighty-looking EMD E7 also rolls in as an A and B unit and now wears the colours of the Missouri Pacific (64615-1 / -2).

The R-70-20 refrigerator cars now pull up in a bright orange Milwaukee Road livery and can be purchased as singles (914811-1), twos (914841-1) and fours (904811-1).



EMD F7A and -B (part no. 63012-1; photo left) of the Erie Lackawanna and EMD E7A and -B (64615-1; photo right) of the Missouri Pacific. Photos: AZL / Ztrack

Also available in the same configurations are the 1917 8,000-gallon boiler cars that belonged to California Dispatch (915008-1 / 915038-1 / 905008-1).

Manufacturer photos of the current deliveries can be found at <https://www.americanzline.com>.



Showcase in the Faller-Miniaturwelten with a selection of different product lines from its 75-year history.

Faller-Miniaturwelten is open again:

What today are the miniature worlds of Faller, as a company museum has a long and special tradition in Gütenbach: everything goes back to a model room, that for a long time was reserved only for business partners.

At times, it was also used to give presents to the children of employees at Christmas time - an event that was certainly very popular with the youngest and gave rise to career aspirations. Today, this room has become a modern company museum that vividly illustrates the 75-year history of the company.

Since the week of 28 June 2021, the Faller-Miniaturwelten (Miniature World) and the factory shop have been reopened. As a little surprise, the garden railway has been redesigned. Admission is free.

Opening hours are Wednesday to Friday from 11:00 – 16:00 and on Saturdays from 11:00 - 15:00. However, as the building is not barrier-free, visitors with limited mobility are requested to call (0 77 23) 6 51-114, in advance.

A new project from Ukraine:

Our reader Alexey Mark introduced us to a new project that he was able to complete after more than a year of work. He felt there was a lack of buildings and facilities based on industrial models, and this led him to the future theme of a wind turbine.

Sustainability should obviously also be expressed in the model world, and, so, he set to work. The first model was Faller's wind turbine for N gauge, but the drive motor was not placed in the base, but where the generator is located in the prototype.

The search for a suitable motor with sufficiently small installation dimensions and slow but equally smooth running proved difficult.

In terms of the template, the choice fell on the widely used E-82 model from Enercon. Its pylons have a height of 59 to 138 metres, depending on the location, and the rotor diameter is 82 metres. It thus generates an electrical output of between 2,000 and 3,000 kW.



A view of the generator with the self-printed sliding pictures for labelling and the attached element for the functioning beacons. Photo: Zmodell / Alexey Mark

The model height of 59 m should prove to be ideal for the model and so it was time to construct and print the necessary parts using grey coloured resin. It was difficult to find suitable templates for all the necessary views, especially the view from above. The following model dimensions resulted:

- Pylon height 24 cm.
- Rotor diameter 19 cm.
- Total height with rotor blade vertically upwards 33,5 cm.

continues on page 62



Additionally, a flashing electronic of the Hungarian manufacturer Trainmodules (<http://www.trainmodules.com>), and a down converter to control two beacons were used to make the wind turbine visible for aircraft.

Their parts were placed inside the column, the red LEDs (SMD type 0402) are placed in prototypical holders. In the future, however, the use of an in-house developed holder is planned.

Refined with applied etched parts, the assembled object was finally ready for painting with Tamiya colours in various mixing grades. Extensive and repeated masking was part of the necessary preparations. Base colour was LP-35 "Insignia white".



Several masking operations, colour tinting and painting were necessary to bring the mast base in colour to the original. The access door to the mast, which is necessary for maintenance and repair work, was not forgotten. On page 61 the completed and electrically driven wind turbine can be seen in full. Photo (also page 61): Zmodell / Alexey Mark

The rings on the mast were tinted from various shades of green from the Tamiya range. Other colours such as aluminium and red were also used. Alexey also made the necessary decals for self labelling on a Mimaki UV printer.

Finally, the completed wind turbine was screwed onto a circular base plate of 10 cm. diameter, connected, and the small plate was landscaped to present it effectively. Its rotation speed is set to twelve per minute, which is the average of the prototype rotation (6 - 18 revolutions/minute).

If our readers are interested in this development, they can contact us by e-mail ([zmodell\[at\]ukr.net](mailto:zmodell[at]ukr.net)) or through social networks (<https://www.facebook.com/Zmodelltrains/>).

Quietly continues with NoBa models:

After three weeks of relaxing holidays on the boat, things are a little quieter at NoBa-Modelle (<https://www.noba-modelle.de>) than we have been used to in recent months. Nevertheless, the creative duo has presented some extremely interesting new products.



Two versions of the bus Büssing BS 110 V (item no. 6379RF; photo above) and the tram T2 (5706R; photo below) as a still unpainted model. Photos: NoBa-Modelle

With the Büssing BS 110 V bus, another important model for Era III is now available. It is available as a moving model (item no. 6379R / 6379RF), and with open doors at the stop (6380R / 6380RF), each as a resin print blank and as a finished model.

For fans of street car themes, a catenary measuring car (5707R) and a rail grinding car (5708R) of the SBB are now in the programme. Meanwhile, the programme has been extended by the two-axle railcar T2 (5706R), and a motorised finished model of the already known articulated railcar GT4 (5703RF).

As a preview for the next few weeks, NoBa-Modelle has named a maize chopper that will be seen again in the fields of Europe in about two months.

New model at WDW Full Throttle:

At FullThrottle (<http://www.wdwfullthrottle.com>) a new model of the bulk goods wagons with cylindrical container is on offer. Painted bright yellow is the example posted under ACFX with advertising slogan "Old Dutch Cleanser" (item no. FT-1042-3).



Cylindrical Hopper "Old Dutch Cleanser" (art. no. FT-1042-3; photo left) and of the C&O/CSX (FTPZ-8022-1; photo right). Photos: WDW Full Throttle

Also released in June was the black 100-ton bulk freight hopper wagon with three discharge chutes and outside box struts (FTPZ-8022-1) of the CSX or C&O. Both of the above-mentioned wagons are delivered in double packs with different service numbers.

DDR roll-off container new at Schrax:

A roll-off container for the W50, a typical truck of the former GDR, has been available for H0 scale for some time. Now, it is also available from Schrax (<https://www.schrax.com>) in Z gauge.

With a cable winch, its prototype was pulled onto or off the transport vehicle via lowered telescopic rails. After the fall of the Wall, both these containers and their transport vehicles quickly disappeared from the traffic scene in the east of the Republic.

Voting for the EM diorama competition is underway:

It's the turn of the readers' jury, the editorial team of the railway magazine announces. Among the 39 exhibition layouts submitted for the adult category, there are also three works for Z gauge. In the youth category (ten participants), unfortunately no one took part this time in the smallest scale.

We are thrilled by the 15 x 30 cm. dioramas shown in a magazine insert and we are sure to recognise the handwriting of at least two of the three participants. Of course, we will not reveal them so as not to call into question the neutrality of the votes.

However, we hope that the members of our scale community will also participate actively in the voting and help to put our representatives in a good position. The following works are competing for the 1:220 scale:

- E03 "Timber loading" (removal of storm-damaged timber on the Ilztal railway after the winter of 2018).
- E17 "Loading ramp in Z" (lovingly designed loading road and ramp with locksmith's shop and forge)
- E20 "Raiffeisen Warehouse" (scenically designed agricultural warehouse with truck ramp and track).

Many attractive prizes will be raffled among the participants of the voting: three shopping vouchers of different traders worth between 250 and 500 EUR, wagon models, accessories, books and calendars.

Votes can be cast in writing using the voting coupon in the supplement of the 8/2021 railway magazine or electronically at <https://eisenbahnmagazin.de>. The deadline is 31 August 2021. Each reader may cast three votes in the adult category and one in the youth category.

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Trainini

German Magazine for Z Gauge



70 Years of Long-Distance Express Trains

Effective Model Photography
Digital to Sound Wonder

Introduction

Dear Readers,

When the 2021 novelties got known and we started planning another year's editions in February in the editorial office, I got aware of a special topic.

With the 50th anniversary of the Intercity, my attention also fell on its predecessor - the long-distance express train ("blue F-train"), exactly twenty years older. In 2011 we had dedicated an article to it, reviewing its history and presenting model proposals.

Until that June, none of our editions had been more successful and we were pleased with a strong increase in readers. In the sixth year of **Trainini®**, this was no longer an everyday occurrence. While things had once gone steadily and quickly uphill, we had long since built up a loyal reader base and at the same time achieved a remarkable market penetration.

Since then, we have not forgotten how much we had struck a chord with probably all railway modellers. And so we could not and must not miss this anniversary.

The result of our ideas is now in front of you: With no less than three articles and two books matching the theme, the blue F-trains are now our distinct thematic focus. Thanks to Oliver Strüber, we are following the history of a popular representative that had its origins in an express train, rose to international fame as an F-train, and then became the TEE (Trans Europe Express) and later the IC (Intercity train) and EC (Eurocity).

We have developed new train composition proposals and rebuilt a suitable locomotive model for them. According to the motto "Make good things even better", we chose Märklin's V 200 for this purpose. A multiple unit of the Hamburg type had to endure much less and still became a super model. How did that work out? Just let yourself be surprised!

A two-volume locomotive portrait of the class 103 forms the already mentioned, first literature recommendation. Also developed for the F-train, it also builds a bridge to the Intercity. Similarly successful on the roads was the Mercedes-Benz W123, for which we also review a book. Perhaps contemporary models in 1:220 scale will follow soon?

And we even managed to include the aeroplane as another competitor of the long-distance trains in this issue. But we are especially proud that we can continue our annual focus. Today we present Trafofuchs, the most important manufacturer of Z gauge miniature figures, which was badly affected by the floods a few weeks ago.

What else have I forgotten? Oh yes, we conclude a small series on model photography with tips and instructions on image processing, plus letters to the editor and current news in abundance.

All that now remains for me, on behalf of the entire editorial team, is to wish you lots of reading fun and, of course, good health. Take care of yourself!

Sin-Z-erely,

Holger Späing



Holger Späing
Editor-in-chief

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We thank Birgit Foken-Brock for her contributions and Eisenbahnstiftung for historic photos.

Date of publication of the German language version of this issue: 3 September 2021

Cover photo:

The FT 43 “Roland” with VT 08 514 at the head has just reached Werdersheim station on its way from Basel SBB to Bremen. Particularly worth seeing on this model, are the upgrades and rework on the Scharfenberg coupler.

Fokker 100 of the HLX

Flying for the Price of a Taxi

An advertising slogan that will be remembered for a long time and a particularly striking brand image on the aircraft used made Hapag-Lloyd Express immortal. Low-cost airlines such as HLX once gave the railways a hard time and raised the question of why environmentally friendly travel is so much more expensive. In such a context, the aircraft now presented certainly has its justification and a deserved place in model history.

If we are presenting an aircraft model today for the third time within a vintage year, then this does not follow our original editorial plans for 2021. At least, we now have a model that has not slipped into this vintage due to delivery date postponements.

We found it particularly exciting because of its colour scheme. Many readers will still be very familiar with the airline Hapag-Lloyd Express (HLX) and will have their own memories.

Therefore, we are sure that this realisation will appeal to many interested parties, whether they are planning to use it on or above the layout, want to build a separate diorama, or whether the model just arouses collectors' passion. In any case, it belongs to the period of transition between epochs V and VI.



The Fokker 100 with the registration D-AGPN is Herpa's prototype and was photographed here in Stuttgart on 6 May 2006 in the service of Hapag-Lloyd Express. Photo: Julian Herzog (CC-BY-SA-3.0-migrated)

Let us first take a brief look back at the history of HLX: Founded in 2002 in Hannover, the headquarters of the holiday group TUI, Hapag-Lloyd Express was the only non-holiday carrier in the TUI Airlines alliance.

It entered the market just two months after its direct competitor Germanwings, the then low-cost subsidiary of Eurowings (and later Lufthansa), also using Cologne/Bonn Airport as its base. Other bases besides Hannover were Leipzig and Stuttgart.

But, it also operated scheduled flights from other German airports, with a total of 40 domestic and European destinations on its flight schedule. And although there were initially fewer than its direct competitor, it achieved higher brand awareness and popularity within just a few months.

The reason for this was the distinctive yellow livery with black and white checks in the style of New York taxis. This design was accompanied by the catchy advertising slogan "Fly at taxi prices." For this, the airline immediately received the "Effie" efficiency award in Germany and Europe. The seat occupancy rate of HLX, for the longest time the abbreviation was displayed in large letters on the aircraft, was always very high, and, most recently in the summer of 2006, was about 79.5 %.

However, as early as 15 January 2007 Hapag-Lloyd Express was merged with its sister brand Hapagfly (formerly Hapag-Lloyd Flug) to form the joint umbrella brand TUifly, which marked the end of the airline's own operations and this brand.



This photo was taken from a similar perspective as the prototype on page 4. It proves the correct realisation by Herpa and only reveals a difference in the nose colour.

The fleet policy

Under its own brand, Hapag-Lloyd Express used aircraft of Group sister Hapag-Lloyd Flug and charter airline Germania. Mostly Boeing 737s were used, but also two Fokker 100s.

The model taken up by Herpa is an example that was originally ordered by Wardair Canada with construction number 11333, but was not accepted (maiden flight on 11 March 1991). Instead, it was delivered to US Air under the registration N881US on 2 April 1991. This company changed its name to



The view from the rear allows a look at the fine parts of the undercarriage and the clear view part inserted in the T-tail. All details have also been correctly reproduced in colour.

US Airways in 1997, from which it was then taken out of service after the terrorist attacks of 11 September 2001 and parked in the Mojave.

On 30 April 2003, Pembroke Capital Aircraft (Shannon) Ltd. acquired the aircraft and leased it to the airline Germania on 30 July 2003 with the German registration D-AGPN. There, it was used by the short-lived subsidiary Germania Express (Gexx) before being subleased to Hapag-Lloyd Express in March 2005 and operated out of Stuttgart.



The side view reveals the best view of the striking appearance in the style of a New York taxi, which made HLX unforgettable together with the matching advertising logo.

But, already in November 2006, this aircraft was returned. Presumably, the responsible persons at the lessee were not satisfied with the contract conditions, but, apparently, had no chance to have them adjusted in their favour.

After a four-year stopover in Mexico, the prototype aircraft then made its way to Australia in 2010, where the F100 is still very much appreciated today. There it still flies with Virgin Australia Regional Airlines under the registration VH-FZI.

The Herpa Model

As the comparison with prototype photographs also proves, the aircraft has been well captured in terms of proportions and appearance. In some photographs, including the one we showed, the model with the registration D-AGPN has a completely white nose.

However, this was only the case at times, as the evidence for the yellow and white design of the model seems to predominate. In any case, Herpa has not made a mistake here. The distinctive livery with the taxi check pattern has been reproduced just as well and also the strong yellow.



In the view from the front, the eye falls on the nose set off by imprints and on the blade wheels of the turbines at the rear. Fine details such as the windscreen wipers on the windows of the pilot's cockpit have not been omitted, either.

It is certainly also one of the powerful arguments for choosing this implementation when an airport section is to be occupied on one's own layout: Hardly any other design attracts as much attention as this one. It gives the model airport the attention it deserves on a layout designed according to contemporary motifs.

The small size of the Fokker 100 also speaks for itself, which is still smaller than the model of the Boeing 737, which would be just as conceivable in the same colour scheme, but to our knowledge has not yet been realised, at least, not by Herpa. The dimensions and conversions for the model can be found in the information box.

Measurements and details to the Fokker 100

	<u>Prototype</u>	<u>1:200</u>	<u>1:220</u>	<u>Model</u>
Length	35.310 mm	176,6 mm	160,5 mm	173,6 mm
Wingspan	28.080 mm	140,4 mm	127,6 mm	140,0 mm
Height	8.600 mm	43,0 mm	39,1 mm	45,2 mm
Take-off weight	44,45 t	---	---	223 g
Cruising speed	755 km/h			
Range	4.300 km			
Passenger capacity	109 - 122			
First flight	30 November 1986			
Engines	2 x Rolls-Royce Tay Mk. 650-15			
Thrust	2 x 68 kN			

We are also satisfied with the detailing. The fuselage and the wings are made of heavy metal casting, which makes the miniature very stable.

At the rear end of the rudder a transparent part has been inserted, also antennas on and below the cabin have been made as separate attachments.

The undercarriage and its open flaps are finely executed, well detailed and equipped with rotating wheels. These were even equipped with real rubber tyres. Even the

view into the two engines mounted on the side of the fuselage allows, despite the small size, the view of turbine blade wheels and at the rear of the exhausts.

For model railway enthusiasts without experience in aircraft modelling, the window reproductions by pad printing in grey are rather unusual. This is probably due to the fact that it would hardly be possible to see through them and the effort required for a hollow body with additional glass inserts would not be economically feasible. It should also be borne in mind that only this method of implementation allows easy adaptation to different cabin versions.



The D-AGPN also cuts a fine figure from the left side. The many small inscriptions that have been pad-printed on the nose, the access doors, the nose wheel flap and above the emergency exits also contribute to the overall appearance.

Many other details are also reproduced by pad printing. These include, for example, the entrance door, emergency exits, cargo hatches and even the tiny windscreen wipers on the cockpit windows. This proves how precisely and comprehensively Herpa designs 1:200 scale models today.

The logos have also been printed with the same fineness and precision. We could not find any missing features. We have already commented on the correct yellow and white design of the nose, which was also highlighted with the finest printed stripes.



Deployed on an airport section at the edge of a layout, no viewer would miss the Fokker 100 of Hapag-Lloyd Express. The striking yellow colour and the attractive design are always a guarantee of this.

All in all, this is another successful model for all those who want to design an airport section or just want to treat themselves to a small souvenir of a city trip that matches their model railway. In order to be able to depict the once typical competitive scenes of two rival airlines, the only thing missing from the Herpa range is an Airbus A319 from Germanwings (in the old design).

Model manufacturer:
<https://www.herpa.de>

The IC precursors of the DB 70 Years of Blue F-trains

Seventy years ago, the Bundesbahn launched its network of blue long-distance express trains. For twenty years after that, they shaped high-quality long-distance transport and faced competition from aeroplanes and automobiles. In 1971, 50 years ago, they were replaced by the new Intercity. We take advantage of this anniversary to present some new model ideas and a market overview.

Model railway enthusiasts seem to have a particularly close connection to the long-distance express trains of the Deutsche Bundesbahn. This is not surprising because they add a little splash of colour to any Era III layout. In addition, they were already short enough for model railways as prototypes. With them, high-quality long-distance traffic can be brought onto home tracks across all gauges.

So, the F-trains were and are also a great success for us: almost exactly ten years ago, in **Trainini®** 6/2011, we looked in great detail at the history of this train system and the rolling stock used and provided train formation suggestions. This story led to a leap in readership and may go down in **Trainini®** history as the most successful issue by far up to that time.



For 20 years from 1951, the light F trains shaped the high-quality passenger traffic of the Bundesbahn. This is reason enough to give them an appropriate role on the model railway as well.

While the prototype history has long since been completed and is probably also told in all the magazines, a lot has happened for the Z gauge in the area of models in the last ten years. With the anniversary of the Intercity coming in September, we thought it was time to take a closer look at F train models.

We look back at the new products that appeared during this time and how they can be used as part of an F train. We would like to start with the locomotive that stands like no other for the economic miracle, and, with it, for the great era of long-distance express trains: the V 2000.

In order to do justice to its popularity and significance as a prototype, we did not want to simply fall back on the Märklin model and looked for additional superstructure possibilities. Moreover, we did not simply work on a brand-new model, but instead made use of “leftovers”. The result compensates for all of the effort.

V 200 007 chosen

Our conversion was inspired by a suggestion from Jens Wimmel (Zettzeit) from 2015. At that time, he had designed his own engine room replica, which appears considerably more realistic than the too light factory print.

In addition, he had, at least to some extent, also made an additional optical improvement to his model, which we had already tested in 2007 on a model of the 221 series (V 2001). So, we also found help and fruitful exchange with and through Jens in the early phase of the implementation, for which he deserves a heartfelt thank you at this point.



The choice of prototype fell on V 200 007, which was maintained in working order for many years by the “Historische Eisenbahnfahrzeuge Lübeck” (HEL) association and is shown here leaving Berlin-Zoologischer Garten station on 15 September 1990 with a special train. Photo: Norman Künemund, Eisenbahnstiftung

Now all that was left was to find a suitable base model. In the leftover box there was still the damaged bogie of a class 221 (item no. 8820), which had already donated its housing for an earlier project. Due to a fall, it had damage to a bogie frame and could no longer accommodate a system coupling on one side.

By the way, at the start of the project a few years ago, there was no crimson V 2000 available for sale individually, which had made our plan even more difficult. Those who want to rebuild our proposal are better “blessed” in the meantime and can save themselves some of the necessary work steps out.

At the time, the housing required for our conversion could only be procured on the second-hand market if we had purchased an Insider train set “Local traffic” (81175). However, we had no need for the passenger cars contained in it.



Because of the original ornamental lines, which the basic model also has, we have oriented ourselves to the delivery condition of V 200 007, which it also shows here in 1966 in the ferry station Puttgarden. Photo: Fritz Wilke, Slg. Stefan Carstens (Eisenbahnstiftung)

Fortunately, however, we were then able to fall back on the housing of the V 200 020 that Märklin produced for the “Blauer Enzian” (81176) train set, also an Insider Club product. Just at the time of a visit to the Győr factory, the first trial spraying, painting and printing had taken place to test the tools and their settings before the series starts there.

These first samples are subsequently disposed of and do not enter circulation, especially as they usually still have faults that are supposed to be discovered in this way and then eliminated. Often the ink adhesion on them is also limited, presumably because not all surface treatments are carried out on these pre-series samples.

With the permission of the management, we were allowed to take such a sprayed sample, including engine room windows, as an exception, in order to be able to tackle this project. So, the resulting locomotive is at the same time a nice souvenir of this factory visit, and we would like to express our thanks for this as well. The paint seemed perfect, and, in this case, showed good adhesion.

Now the project could finally get underway and all that remained was to find a new road number for our “new” locomotive. After a short consideration, the choice fell on the V 200 007 for two reasons: The basic

model depicts a machine from the first construction lot of the MaK, which were characterised by the lettering "DEUTSCHE BUNDESBahn" on the long sides and a broader "V" on the front sides.

Our original also belongs to the same construction lot, which, following the road number, represents the second series locomotive of the class V 2000. A second argument was that the prototype of this locomotive was preserved as a museum piece and also in working order for a long time. Although it did not always wear the original decorative lines in the years of its second career, the model can be used true to the prototype well into the present day.

For the re-labelling, a quadruple copy of the new road number in silver-coloured print was required, which not all address suppliers can provide. In our case, Jens Wimmel was able to help out with his printer of the time, especially since he was also able to take the correct address height from the model itself.



Jens Wimmel helped us with the new addresses, which are still here on the backing paper in front of the cabinet, waiting to be used.

The first step was to complete the housing, because the driver's cab windows were still missing. They are identical to those of the class 221 models (V 2001) and can be found quite easily second-hand. The page Ersatzteile-1zu220 by Axel Reimann is always a real treasure trove for such things.

Another missing part, which was needed in duplicate, is the lighting unit (part number E161319) consisting of LED circuit board and plugged-in light guide, which was available from Märklin at the start of our project. It was also used in our V 188 construction (see **Trainini®** 6/2021), the V 200 007 even served as a temporarily previously approached test object.

Because, in this case, the installation would not be quite so simple. After all, we combined a current housing with warm-white-red LED light change and an old chassis with a three-pole motor and incandescent lighting. We will go into the idea for the solution in a moment, as soon as we arrive chronologically at this construction step.

Face change of the locomotive

The first thing to be done was to improve the appearance of the body. The brawny and powerful expression of the V 200 in both construction versions was very much the result of the front apron reaching far down. Märklin could only suggest this on the side, because it would be in the way of the system couplings and their freedom of movement at the front.

Our aim was to close it on one side and thus give the model a fixed direction of travel in the train set. Therefore, it did not bother us that one bogie had lost its coupling guide due to a fall. This mishap only determined the side that would be closed - the driver's cab side 2.

The cut-out at the front was closed with a piece of white square profile, which was permanently fixed with suitable glue from the Uhu range. After hardening, the transition between the insert and the apron edge was sanded and left no more gaps.



The standard buffers were exchanged for turned parts from FR Freudenreich Feinwerktechnik, the apron on driver's cab side 2 was furthermore closed with a polystyrene profile.

On the other hand, the milling cutter was used on the inside of the bogie. Here, the material was tapered until the damaged bogie could safely perform its rotational movements without bumping. The replacement of the Märklin buffers, which were too small, by correctly sized ones with curved (right side) and flat (left side) buffers was done with compatible parts from FR Freudenreich Feinwerktechnik.

The glue used was again Uhu All Purpose Glue Super Strong & Safe (currently "Strong & Safe"), which as a combination of an all-purpose glue and cyanoacrylate (superglue) always performs excellently and, above all, shows lasting success. It will also be the first choice for the attachment parts that are still to be mounted.

But before that, the skirt had to be primed and painted. The target colour was RAL 7021 black-grey, which was supplied by Oesling Modellbau. The first step, however, was a deep black priming of the apron insert and the new buffers with PUR primer from Badger.



With the application of the pad printing remover, the old labelling quickly made room for the new V 200 007 road number, which could then be easily wiped off.

If the adhesion to the brass is good, the buffers do not need any further paint treatment. However, the apron had to be painted in this step, because it will be fitted afterwards and is then no longer completely accessible.

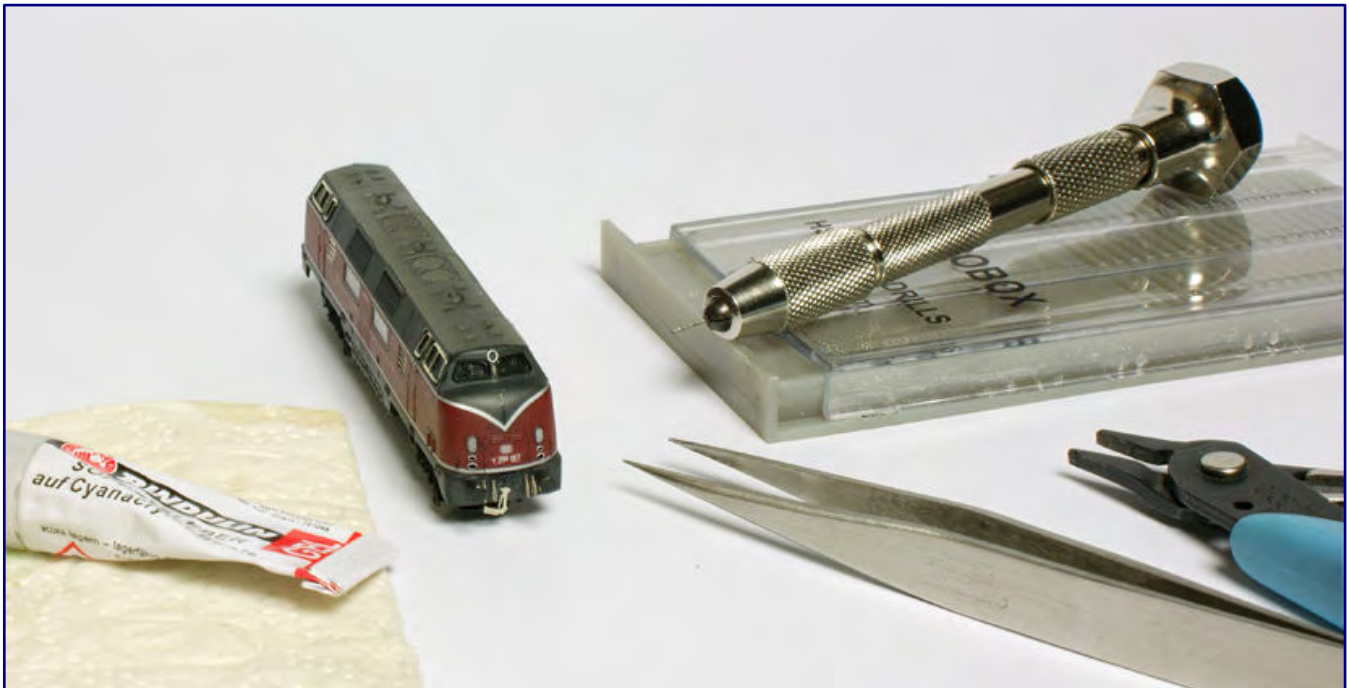
When the paint was dry, we preferred another step, namely the replacement of the serial number. We removed the old number with the (unfortunately no longer available) tampon print remover from Prehm-Modellbau, whereby a fine micro brush from the dental supply helped us.

We had to be careful not to accidentally remove or damage neighbouring labelling! After this material had evaporated, the new silver-coloured number was cut to size and applied to all four sides. Drying times should be observed here, as well, otherwise sliding labels suddenly slip and lie at the bottom or vertically and do not yet fully adhere.

The last step was to seal the dried labelling. This was important not only to protect them, but also to match the gloss level of the finished areas to the rest of the paint. Bergswerk offers finely graded clear lacquers, among which the right one is guaranteed to be found; otherwise, they could also be mixed together and individually adjusted.

After the protective lacquer had dried, the following parts from the KoMi Miniaturen range were used: a heating hose for steam heating (ZW 7) below the indicated hook coupling and four brake air hoses in the "through buffer beam" version (ZW 10) between the coupling and buffers.

There were four of them because the V 200⁰ was reversible, which opens up additional use options for the model. They are glued with instant glue gel from Bindulin into tiny holes by hand, which were made with the pin vice (170522) and the finely graded drill set (170523) from Faller.



The buffer plank on the closed side of the apron could be completed according to the reversible prototype using parts from KoMi Miniaturen.

Now we have reached the point where we would deal with the technology. In the old chassis, a plug-in lamp was still connected between the chassis and the circuit board, whereas today's versions conduct current via a spring contact in the roof to the light circuit board connected to the housing instead. These spring contacts were not mounted in the housing either, but would not have been of any use to us.

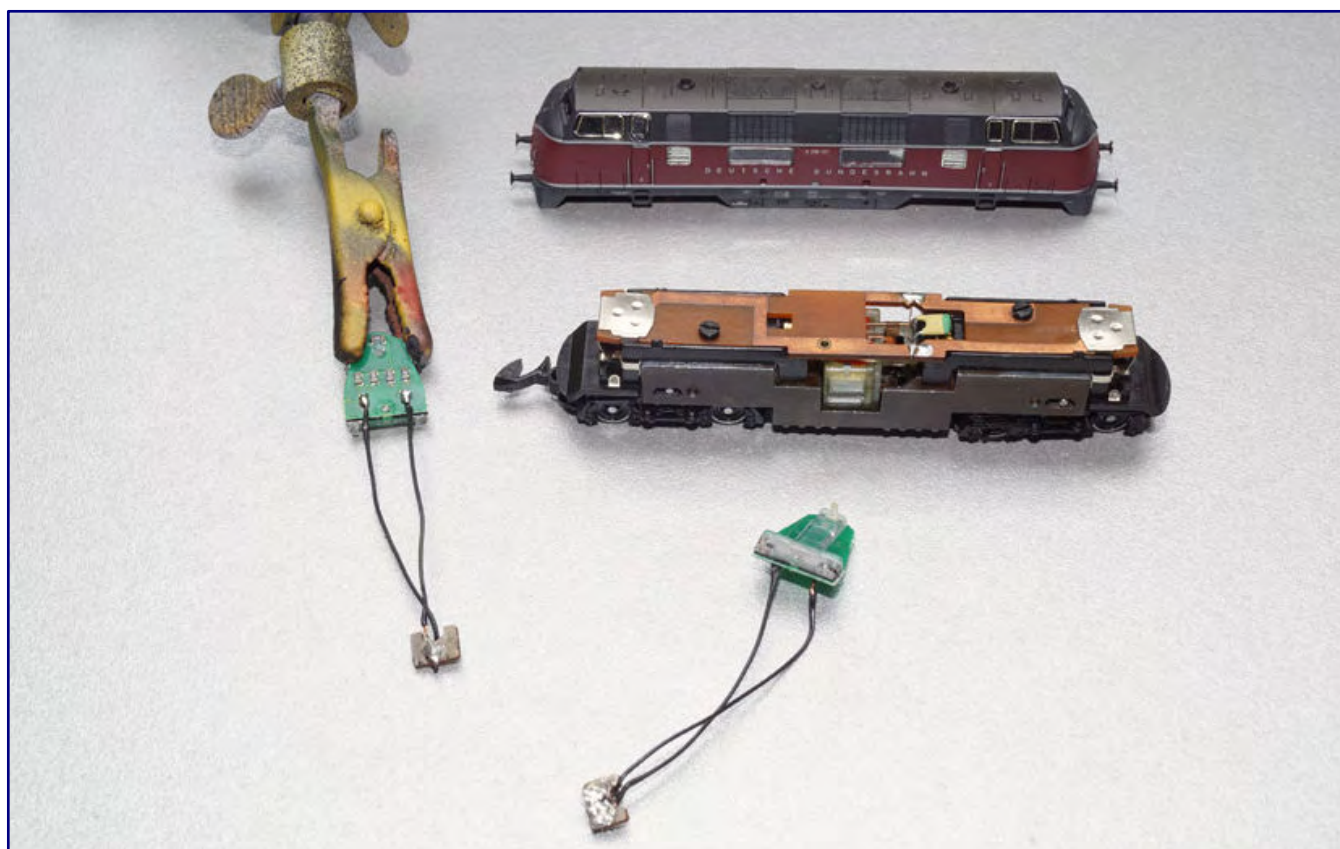
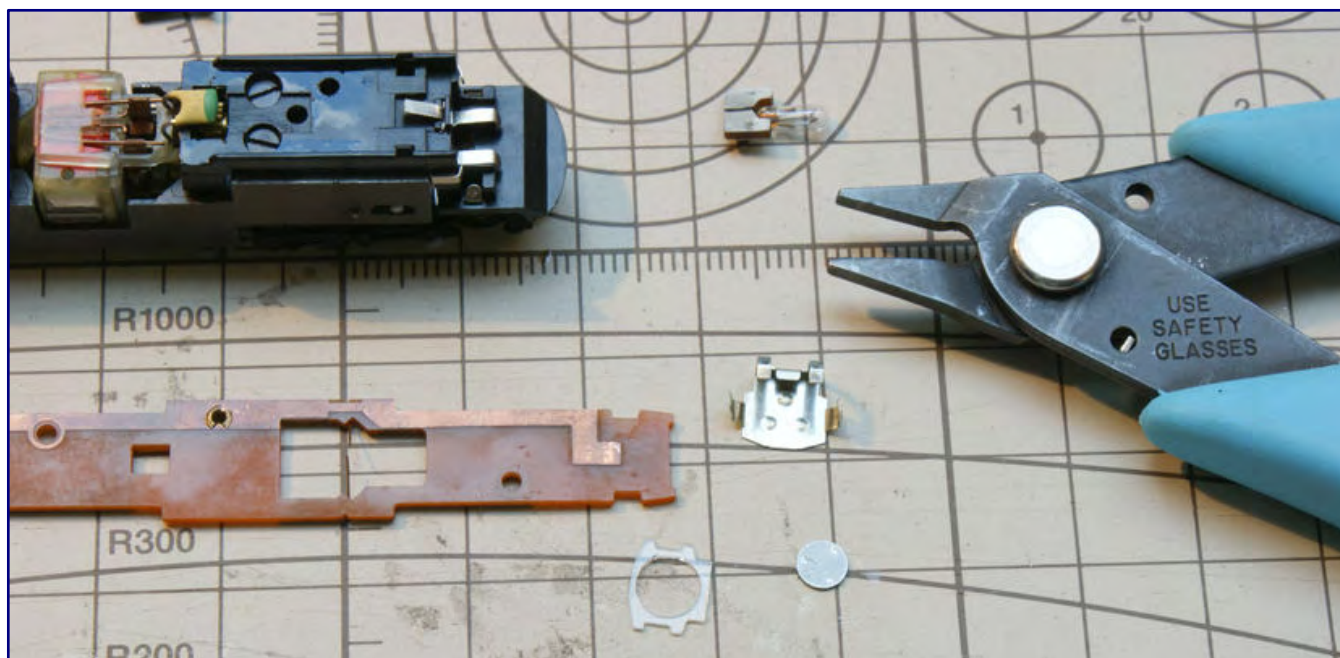
On the underside of the printed circuit board in the chassis, the clamp attachments each concealed a small silicon plate that acted as a diode and controlled the light change of the bulbs. They were no longer necessary and were removed.

In the simplest case, we could now unsolder the bulb from the socket and attach flexible wire instead. We soldered the other ends to the two connection points of the lighting units.

Now current would always be transmitted and the unit would distribute it to the warm white or red light-emitting diodes when the base was correctly plugged in: an ingenious idea, it seemed to us. The housing would remain removable, the plug-in bases could be pulled out and access for maintenance would be easy.

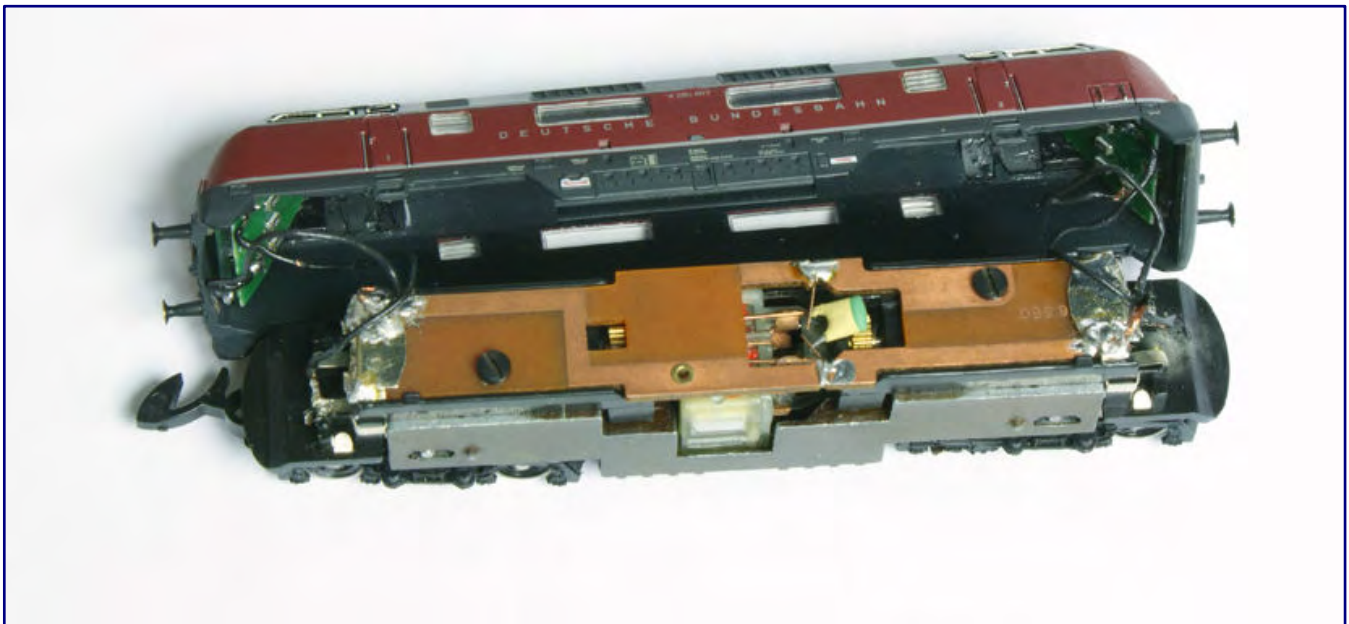
You guessed it... It sounded too simple to work. Although the light test was successful, the housing could no longer be placed on the chassis. The lighting unit, inserted at an angle in the "little hat," occupied in parts exactly the space where the end of the old circuit board with the upper bracket sat.

All attempts to shorten the circuit board and to continue using the clamps failed without success. Here we had to resort to a radical solution that forced us to solder the Viessmann stranded wire (which had to be long enough) to the extremely shortened circuit board and at the bottom to the still existing clamp.



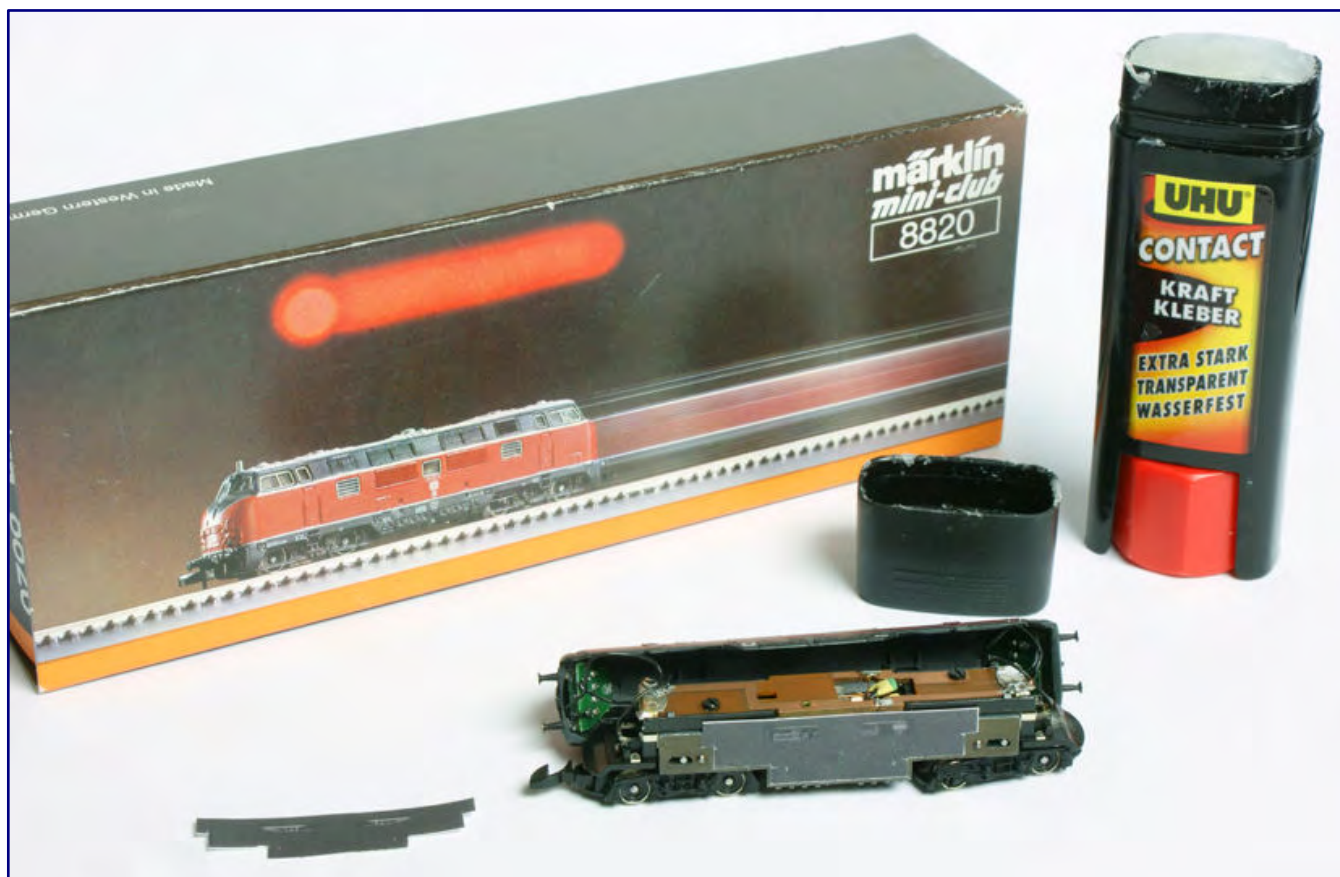
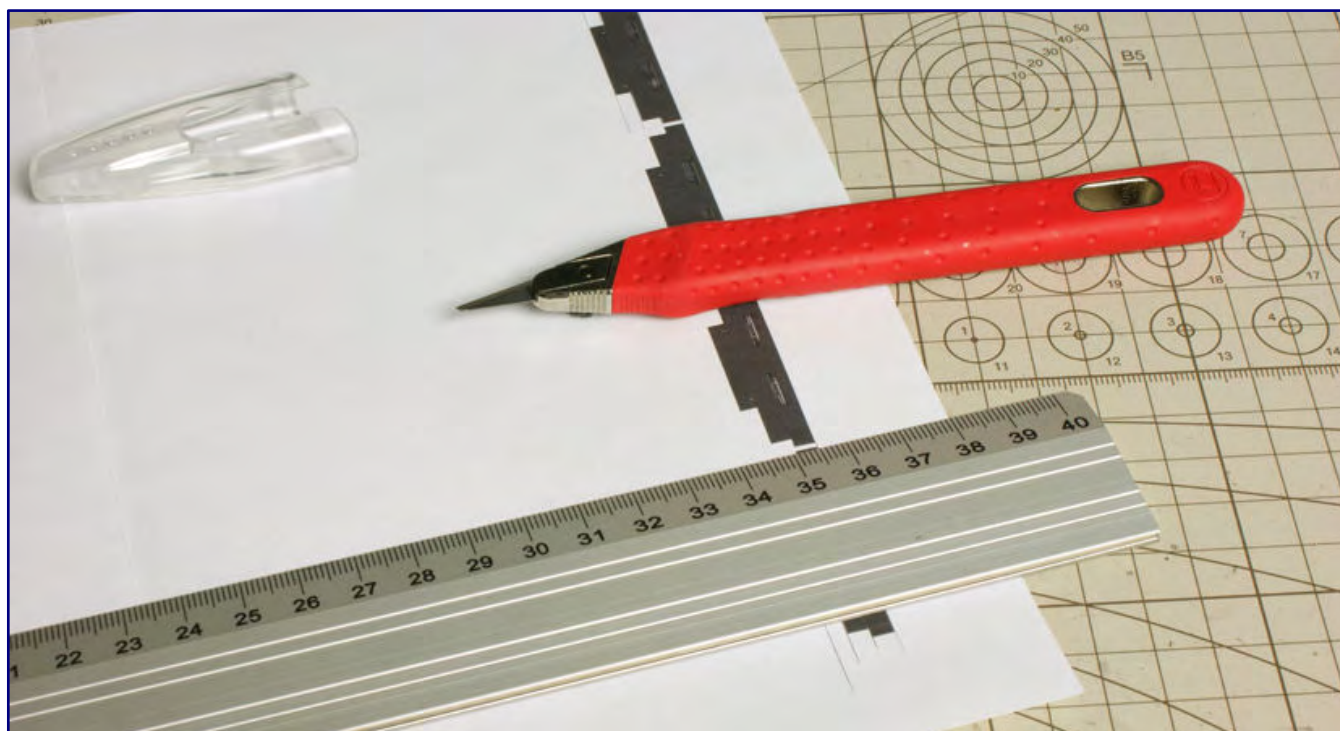
Under the metal clips of the circuit board are small selenium plates that were necessary for the light change with incandescent lamps. These are to be removed as part of the conversion to LED lighting with tail light (photo above). The idea was to use the plug-in sockets of the previous lamps for the current consumption of the small light boards, which find their place in the housing (photo below).

But finally, the housing could be put on. The light and running tests were also successful, V 200 007 seemed ready for use on the layout. But one small thing was still missing, for which we were allowed to fall back on Jens Wimmel's preliminary work, which we are also allowed to offer for download on our pages (see info box): the new engine room replica.



After the original plan for the lighting solution proved to be unfeasible, a radical solution had to be found: the PCB had to be cut flush with the upper edge of the chassis (photo above). Then the strands already connected to the light boards were soldered to the main board (photo below). It is important to ensure that the polarity for the light change is correct.

A laser printer is recommended for printing the PDF document in unscaled size, the printout itself only has to be cut out and glued to the side of the (cleaned) chassis block. We used an Uhu contact glue pen for this, which unfortunately has been taken out of the range.



The engine room replicas by Jens Wimmel are neatly cut out with a sharp scraper along a steel ruler (photo above) and, following their shape, glued to the locomotive's chassis with a suitable adhesive (photo below).

At most, it should be considered whether a foil coating would be useful as protection against lubricant stains – but the print could otherwise be replaced at any time. Markings on the replica indicate which side



Since this locomotive belonged to the most modern that the Bundesbahn had to offer, we have attached V 200 007 passenger coaches from the DB's new construction programme. With these representatives of the UIC-X type, it can appear in F-train service from about 1961.

is facing driver's cab 1 or 2, while the shape following the Märklin template makes it undoubtedly clear where exactly the replica is to be placed.

Finally, V 200 007 was now ready and could also fulfil the role it was given on the model railway, in contrast to the later replicated, more powerful V 2001: It was the popular draught horse of the blue F-trains, figurehead of the modern Bundesbahn and a backbone of this long-distance traffic concept.

We, therefore, now take a look at the trains that it and other series have hauled and which can now also be modelled.

The F-trains as Models

Ten years have passed since our report and these have brought some new possibilities to present long-distance express trains in an interesting and varied way. So, you will certainly be curious to see which wagons our V 200 007 will now pull.

Its lettering and design follow the first years of service, which are attributed to epoch III in model railways – the heyday of this F-train concept. It is perfectly matched by passenger coaches, which were already announced the year after our article: The A4üm-61 express train coach (87101) also bears lettering from the time before the UIC numbers and its steel blue paintwork immediately reveals what it was intended for.

Two to three copies already make a long-distance express train, which is only missing a rolling restaurant. The purple express train dining car WR4üm-64 (87131), operated by DSG, was also announced as a new product at the same time.

It has a slightly different design from the later coaches without decorative lines, which, by the way, is also documented in prototype photos. Thanks to its unusual appearance, it (and thus the entire train) is sure to attract the attention of onlookers.

Already mentioned in the first article, but left without a photo, we had a special vehicle, which was also designed for long-distance express train traffic, but remained luckless with the prototype: the VT 10 501 "Senator" (88100). Its nickname refers to the FT 41/42, which it was supposed to cover. As a single unit, however, it was not able to serve both directions on the same day.

Even as a model, it does not seem to have been a great success. Announced in 2008 as an Insider Annual Model in its original livery, the production models only arrived in 2010 for the 175th anniversary year of the German railways, and almost fell into oblivion, again.



The VT 10 501 "Senator" also belongs to the history of the F-trains. Although its prototype was only in service for a short time, its model is a special eye-catcher.

The modified and somewhat more modern-looking paint scheme variant from the technical conversion of the prototype has not been added by Märklin to date, but after a technical revision with the bell-shaped armature motor, which has now become standard, it would certainly, also, be an appealing option for the ordinary catalogue programme.

The same applies to the class 601, the former VT 115, which came to honour as an F-train (and from 1971 also as an Intercity) with the beginning migration from the TEE traffic and already showed the way to its successor in the transition period with attached front plates "Intercity". Formally, it was still a long-distance express train that bore the term Intercity as a name supplement.

A special model for the first generation of F-trains was also the "Blauer Enzian" (81176), to which our V 200 007 would also fit as the leading locomotive. The set of the former Henschel-Wegmann train supplemented by a wagon, conveys a basically familiar photo, but remains instantly recognisable thanks to the shape of the wagons and because of the cockpits at the ends.

continues on page 23



The “Blaue Enzian” (photo above) hauled by V 200 020 is one of the particularly popular F trains that Märklin has taken on since our last report ten years ago. At the time of that set, class 01 (photo below) was also frequently used in long-distance express service. We have attached here a mixed set of former Rheingold and Schürzen (skirted) coaches.

In 2013 Märklin also only offered this set with the V 200 020 to Insider Club members. Still possible and open here is also another design option with simple DB letters instead of the biscuits common after 1955. A class 01 express steam locomotive would then be ideally suited for such a train.

This leads us to another model that should not be missing in this era of German long-distance traffic. Two years after the club model for the 40th anniversary of the Mini-Club, this most famous of all German express steam locomotives also followed in the 2014 catalogue programme. The model with the item number 88011, depicted in Era III, could be freely purchased by anyone interested.

We have hooked a train to the engine with old building boiler, which is made up of Schürzen (skirted) and the former Rheingold coaches and in this way offers some variety. Such sets were common for a long time, when the highest domestic train category was still a steam service in many places.

However, the plans of the Bundesbahn once looked different. In high-quality high-speed traffic, mainly diesel multiple units were planned, everything else was to be more of a temporary solution. But provisional solutions are known to last the longest, and so locomotive-hauled trains prevailed in the end.

The ideal image of the DB was embodied in the VT 085, put into service precisely for this train category. Märklin announced it as a four-part unit with two motor coaches (88721; one power car motorised) in 2010 as FT 43 "Roland" (Basel SBB – Bremen).

We mention this model, which was already known at the time of the last report, for two reasons: It was not until later that Ratimo-Z published milled parts that improve the appearance by creating closed-looking transitions. And Dirk Rohwerder recently showed us how the Scharfenberg coupling ("Schaku") can be further enhanced with just a few brush strokes and tinkering. The result can also be seen on the cover and may serve as an inspiration for tinkering.



Four-unit VT 085 is on its way from Basel SBB to Bremen as FT 43 "Roland." Dirk Rohwerder has upgraded his Märklin model on the fronts and made it even more appealing.

We are now approaching the latest and final phase of the blue trains. Electric traction became increasingly important during this period. The steam locomotive no longer corresponded to the photo of the modern railway, and the diesel locomotive was now also less common in these services because the overhead wire now hung over many of the main lines travelled.

The starting point for the advance of the electric network was also Bavaria, where the electric locomotive had already gained greater importance before the Second World War. At home there were, among others, the four lone locomotives of the class E 19. As the fastest German representative at the time, this class was also used in F-train service from Munich, as photographic evidence shows.

Therefore, we have also included the model of the bottle-green E 19 11 (88086), which was released in 2013, in our list of suggestions and put it in front of the apron car pack (87357), which is one of the new products for 2020. It reproduces the standard design of these cars with the written abbreviation DB (instead of the written name) and is suitable for many F-train compositions.



In front of the latest set of Schürzen cars for F-train service, we have harnessed the bottle-green E 19 11, which was also used in such services from Munich for a period. Of course, a cobalt blue E 18 would have done just as well, especially since Märklin has only recently delivered such a model in the standard livery.

Märklin itself had planned and recommended the model of a steel blue painted class 0310 express steam locomotive (88856) at the same time. We mention this only for the sake of completeness, because the reproduced locomotive is only an ordinary, two-cylinder class 03, which literally “sails under a false flag.” It lacks important features such as the closed driver's cab and the tender adapted to it. The prototype did not have a front skirt like the model.

The class 01 in steel blue livery (88012), which Märklin offered as a special model, is also without a prototype. However, the design suits the steam locomotive well and is also based on the BR 0310 representatives once based in Dortmund.

Those who do not like the proposed E 19 11 because of its different colour from the train set, but who value the closest possible resemblance to the prototype, need not despair. The somewhat older sister E

18 (88088) was announced and delivered in 2021. It also wears the steel blue and impresses with the familiar livery in which most of the locomotives were on the road.

The model of the class 103 is to mark the end of the F-train history. It first came into operation in 1965 with four test locomotives as the E 03 and was to shape the future of long-distance express trains in addition to TEE traffic with similarly short and light trains.



E 18 32 represents the latest edition of its series as a Bundesbahn machine. Not only in terms of colour does it fit perfectly with blue F trains, because the operational profile was also tailored to this. It harmonises particularly well with couchette wagons.

When the first examples were put into service in 1970/71, the plans for the IC were already ready in the drawer. So, the F-trains remained only a short episode in their operational history, which, however, can be depicted well in the model.

What is important is an example with a low road number and scissor pantographs. This is exactly what Märklin offered in 2018 with the number 103 107-9 (88544) in a current, technical version that clearly stands out from earlier editions.

We have assembled her train from a blue compartment car Aüm 203 as it was included in the train pack 87400 (announced in 2011), a dining car WRümh 132 from a late edition (8713) and two reworked examples Aüm 203 (8710). The individually offered cars were in the range from 1972 to 2009 and are easy to obtain used and tend to be cheap.

So we draw a pleasing conclusion: The mostly short long-distance express trains are among the most popular themes on model railways and offer an incredible wealth of variation and design possibilities. So, there should be something for almost every taste and budget.

And, also, the hobbyist's heart is always addressed and challenged. So, we will certainly not run out of material here in the future, because Märklin seems to be just as aware of this and serves the customer wishes again and again with appropriate models.



The final point and at the same time the transition to the Intercitys is the class 103: With 103 107-9, one of the first series locomotive motifs from around 1970/71 is used here in front of an F train. The attached car set is correspondingly modern.

We are pleased about this diversity, which we otherwise miss all too often, and are now looking forward to September, when the Intercity will also celebrate its 50th anniversary and Märklin is expected to announce its autumn novelties.

Manufacturer of the basic models:

<http://www.maerklin.de>

Sources for required fittings and materials:

<https://www.bergswerk.de>

<https://www.bindulin.de>

<https://www.ersatzteile-1zu220.de>

<https://www.faller.de>

<http://www.fr-model.homepage.t-online.de>

<http://www.komi-miniaturen.de>

<https://www.oesling-modellbau.com>

<http://www.peter-post-werkzeuge.de>

<https://www.uhu.de>

<https://viessmann-modell.com>

Location of the engine room illustrations:

<https://www.trainini.de/magazin/downloads>

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From long-distance express train to intercity **Helvetia – Switzerland in the Name**

For around half a century, one train name decisively shaped the photo of DB's international long-distance travel: the "Helvetia". Started as an F-train, it always remained part of the highest train categories of the Bundesbahn – as TEE, IC, EC, and finally ICE. And the name said it all, as the trains always had a Swiss departure or destination station.

By Oliver Strüber. Shortly after the Second World War, international long-distance travel on German tracks did not play a major role. It was mainly important business trips, mostly in the context of Allied interests, that gradually brought such train connections back onto the rails.

After the currency reform and the founding of the German Federal Railways, however, the basic conditions for international travel soon changed. Initially, the focus was still on business travellers, but suitable offers were also to be created for long-distance holiday travel.



This photo of an F train hauled by an unknown V 200, heading for the main station against the imposing backdrop of Hamburg's Speicherstadt (harbour warehouse city) on 20 June 1959, has shaped the perception of long-distance express trains to this day. Photo: Walter Hollnagel, Eisenbahnstiftung.

Switzerland, in Latin, "Helvetia," played an important role as an international banking and trading centre. A direct train connection from the German financial centre of Frankfurt (Main) to the Swiss banking metropolis of Zurich promised not only good capacity utilisation but also wealthy customers. The inclusion of the train route in the F-train network of the DB, introduced in the summer timetable of 1951, was an obvious choice.

Planning progressed well, and the connection was scheduled to be introduced as a high-speed railcar service in the summer timetable of 1952. However, the prevailing shortage of railcars put a spanner in the Bundesbahn's works: the delivery of the new VT 085 was delayed and the originally planned new routes were cancelled.

This also affected the pair of express railcars from Frankfurt (Main) to Zurich. DB was unable to offer an equivalent replacement for the time being. The desired direct connection to Switzerland was offered by the long-distance train pair F 77/78, converted from the D 77/78 and consisting of only three coaches, which ran via Mannheim, Karlsruhe and Freiburg to Basel SBB and were hauled by Offenburger 01 and 01¹⁰.



01 170 (Bw Hamm) stops at the head of an F train on 1 July 1955 in Münster (Westf.) Hbf. The poor state of maintenance of the locomotive also contributes to the bleak scenery ten years after the end of the war. We have to imagine the beginnings of the "Helvetia" as D 77/78, and, a little later locomotive-hauled, F 77/78, in a similar way. Photo: Reinhard Todt, Eisenbahnstiftung.

With its timetable position, it mainly found a Swiss audience: F 77 left Basel at 6:56 in the morning and reached Frankfurt (Main) at 11:51, while the return train started there at 18:55 and reached its destination ten minutes before midnight. Connection to Zurich was provided by SBB train 599 (arriving at 1:18 am), which left a quarter of an hour later.

The Helvetia Express

On 17 May 1953, the pipe dream finally became reality. In the meantime, DB had enough newly delivered VT 08⁵ sets at its disposal, which is why a direct express railcar connection between Frankfurt and Zurich could be started. And, after the successful introduction of sonorous names for the most important long-distance trains of the DB, the FT 77/78 was also allowed to have one.

The "Eisenbahn-Fachmann" proudly announced in its 11/1953 issue: "The new 'Helvetia-Express' Frankfurt - Zurich covers the 430 km route in just under five and a half hours. With this new FT connection and the 'Montan-Express' between Frankfurt and Luxembourg (...) which has already been running for some time, a start has been made on the creation of continuous high-speed connections with neighbouring countries."

And the "Bundesbahn-Mitteilungen" also praised the Helvetia Express, which rushed from the Main to the Limmat in only 330 minutes, in the highest terms on the occasion of the press trip for journalists from the Federal Republic of Germany and Switzerland, which was carried out in advance on 12 May.



Compared to the pre-war state, this simpler arrangement of an F-train coach with six seats per compartment formed the "new 1st class" from 1956 (class reform). Until then, the photograph dates from 1955, such carriages were marked as 2nd class. The former 1st class with only four seats per compartment and more comfort, as this carriage once had, was formally offered by the DB only on international trains. In the F 77/78 "Helvetia" it might have looked comparable. Photo: Bustorff, Eisenbahnstiftung

Compared to its locomotive-hauled predecessor, the journey times had changed only slightly: FT 77 left Zurich HB at 7:41 in the morning, crossed the border to the Federal Republic of Germany after about 20 minutes of border control in Basel Bad Bf at about 9:15, to then make its way via Mannheim and Darmstadt to Frankfurt (at 13:07). In the opposite direction, the FT 78 left Frankfurt at 18:25 and arrived at its destination station at 23:51.

The Helvetia Express was also of special significance for the Deutsche Schlafwagen- und Speisewagen-Gesellschaft (DSG), which operated the dining room of VT 085 during the journey. For the first time after the war, the DB subsidiary, which had been founded two years earlier, was allowed to cater for passengers travelling across borders.

The great popularity led to the extension of the route to the north. As early as the 1953/54 winter timetable, the Helvetia Express was extended in both directions to/from Hamburg-Altona. The three sets VT 08 505, 506 and 507, which had been relocated from Frankfurt-Griesheim to Hamburg-Altona and for which a new vehicle hall had been built there, were now used.

In order to meet the surprisingly high and continuing demand, VT 085 had to be enlarged on Fridays (FT 78) and Saturdays (FT 77) by a second middle car to form a four-car unit.



VT 08 506 (Bw Frankfurt-Griesheim) leaves Frankfurt main station as FT 77 "Helvetia" in 1955 to travel towards its destination Hamburg-Altona. Worthy of note are the plain letters "DB" on the front, applied after removal of the wing wheels and not yet adorned by the "biscuit", a hitherto undiscovered variant of this train by Märklin. Photo: Reinhold Palm, Eisenbahnstiftung

The upcoming timetable change brought a noticeable acceleration, because the train's previous route via Kassel (with a head-end stop there) was abandoned in favour of the shorter connection via Fulda and Bebra. This also eliminated the previous stop in Offenburg, which shortened the journey time by about three quarters of an hour.

With eleven hours and 19 minutes, the Helvetia Express was the fastest train of the DB in the summer timetable of 1954. It was able to further expand this position in the summer of 1955, because, by using the faster Riedbahn between Frankfurt and Mannheim with the abandonment of the intermediate stop in Darmstadt, further minutes could be saved.

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HAMBURG-ALTONA

AUSGANG
ZU DEN GLEISEN
5 UND 6 1 UND 2



The demand for seats on the FT 78, which was usually fully booked, remained high, so that DB decided to couple the two-car pre-war express railcar VT 04 501 (Hamburg type) as an extension unit instead of an additional middle car at the end of the train on high-demand Fridays. The VT 08⁵ unit could, therefore, run in three parts again, which was maintained in the following timetable sections.



North of the Main-Neckar-Bridge in Frankfurt (Main) VT 08 512 and VT 08 508 (both Bw Frankfurt-Griesheim) are running as FT 78 "Helvetia-Express" (Hamburg - Zürich) in May 1957. Photo: Kurt Eckert, Eisenbahnstiftung

Photo on page32:

A VT 08 arrives as FT 77 "Helvetia-Express" on its way to Hamburg-Altona at the main station of the fair and state capital Hannover. (14 June 1958). Photo: Walter Hollnagel, Eisenbahnstiftung

The class reform in 1956 finally made the Helvetia-Express a purely first-class train. In the 1956/57 winter timetable, the "Flying Hamburger" VT 04 000 was finally also used as an extension. However, there were always braking problems between it and VT 08⁵.

Second-hand book recommendation on the article topic:

Hans-Wolfgang Scharf / Friedhelm Ernst
Vom Fernschnellzug zum Intercity

Eisenbahn-Kurier Verlag
Freiburg 1983
ISBN 3-88255-751-6

Upgrade to TEE

For the 1957 summer timetable, the DB converted the train into one of the new Trans-Europ-Express trains (TEE) while retaining its route and train numbers. It now had the shortened name "Helvetia".

Until the purple/beige TEE trainset VT 11⁵ could start running, VT 085 and several of the pre-war express railcars had to help out. This also affected the TEE "Helvetia". It was not until 14 October 1957 that it could gradually be operated with the new VT 11⁵.

continues on page 36



Photo above:
The inaugural train of TEE 78 "Helvetia" on 2 June 1957 was formed by VT 04 501 and VT 08 503, captured by the photographer north of the Main-Neckar bridge in Frankfurt (Main). Photo: Kurt Eckert, Eisenbahnstiftung

Photo below:
Until November 1957, VT 08⁵ had to help out as a TEE. Since there was often not enough space, old VTs had to be attached regularly, here a VT 07⁵ arriving at Hannover Hbf at the Zugspitze in August 1957. In the background, a CIWL sleeping car can still be seen, which arrived in the state capital as a group of course cars from the F 11 "Nord-Expreß" under train number D 111. Photo: Carl Bellingrodt, Eisenbahnstiftung



Photo on page 35:

Also on 2 May 1958, a VT 085 with attached TEE sign expanded the scheduled TEE 78 "Helvetia" (Hamburg - Zurich). VT 11^s set, photographed in Hamburg Hbf. Photo: Walter Hollnagel, Eisenbahnstiftung

With the changeover to a TEE, the journey times were also shortened further, this time by around half an hour: the departure of TEE 78 in Hamburg-Altona was brought forward by ten minutes to 12:38, and it arrived in Basel SBB 35 minutes earlier than before, namely at 22:06. The Helvetia reached its destination Zurich at 23:14.

The northbound TEE 77 could also be accelerated by 17 minutes by shortening the border stop in Basel and moving the passport controls on to the moving train. It is therefore no wonder that the TEE "Helvetia" was also right up there with the fastest DB trains of the 1957 summer timetable. In some sections, the TEE diesel multiple units were able to reach their maximum speed of 140 km/h, as scheduled.

Even as a TEE, the utilisation of the train pair remained very high in both directions. And again, DB was initially unable to increase the seating capacity due to the lack of available middle cars. Thus, on the particularly popular section between Hamburg and Frankfurt, a second train unit in the form of a VT 085 with a TEE logo was often attached.



When TEE 77 "Helvetia" with VT 11 5013 stops at Heidelberg Hbf at the Zugspitze on 15 June 1958, it is travelling without an extension unit and without additional middle cars. Photo: Walter Hollnagel, Eisenbahnstiftung.

It was not until 1958 that retrofitted centre carriages VM were available, thanks to which the train was subsequently used with seven, to a maximum of eight, intermediate carriages, which eased the space problem, at least, temporarily.

From the same year, the Helvetia of both directions also stopped in Göttingen, which reduced the previously around four hours without a stop, still the longest journey without a stop on DB, between Frankfurt and Hannover to three hours.

But the capacity problem remained: All seat tickets were still frequently sold out. However, the use of additional middle cars was prohibited in view of the load capacity of the machinery, especially on long gradient routes. On special occasions, such as the Hannover Fair, a VT 08⁵ had to be coupled up again to increase capacity on some routes. In the early sixties, the DB therefore already thought of another solution.



As TEE 78 "Helvetia" waits for passengers on track 11 of Hamburg Hbf on 14 January 1960, additional middle cars in the TEE multiple unit have long been the order of the day. But even that did not always prove to be a sufficient solution. Photo: Walter Hollnagel, Eisenbahnstiftung.

The successful conversion of the two F-trains "Rheingold" and "Rheinpfil" to locomotive-hauled sets in 1962/63 finally revealed the solution suitable for the TEE "Helvetia". The newly built coaches, characterised by great luxury and state-of-the-art technology, formed the blueprint for the TEE sector.

As they could also run locomotive-hauled from 1965 onwards due to new international agreements, DB decided to convert both the TEE "Helvetia" and the "Blauer Enzian", which was converted into a TEE at the same time, accordingly.

The completion of the electrification between Hannover and Hamburg at the beginning of 1965 created the technical prerequisite for the continuous harnessing of the Helvetia with an electric locomotive. On 12 April 1965, the time had come. The train was now composed of two compartment coaches Av, one open-plan coach Ap, the new TEE dining car WR and one of the bar cars ARD.

On the German section to Basel, the trains were expanded by two Av and, in case of heavy traffic, by another Ap, thus offering a maximum of 324 seats. The actually predestined E 10¹² could only be used between Frankfurt (Main) and Mannheim for the time being. On the other sections from/to Basel SBB, blue E 10s were on the train, mostly already the nine "Bügelalten".

Unfortunately, the darkest chapter in Helvetia's history also occurred in 1965: On 12 August, the northbound TEE 77 ran at full speed into the rear of the through goods train Dg 6621 at Lampertheim station in Hesse, which had been "taken aside" for overtaking, but had not come to a stop without a border sign.



Two days after the terrible accident, on 14 August 1965, the four-month-old locomotive of the TEE 77, the Frankfurt E 10 364, presents itself to the photographer. Four people lost their lives in the collision, 45 others were injured, some seriously. It is the darkest chapter in the history of the Helvetia. Photo: Helmut Röth, Eisenbahnstiftung

Four dead and 45 injured were the tragic result of this accident. The E 10 364, which was only four months old, was severely damaged and two carriages, an Ap and an ARD, were almost completely destroyed.

Three important milestones in the history of the TEE should also be mentioned: From 1966 onwards, the TEE reached its destination even faster after the DB increased the maximum permitted speed from 150 to 160 km/h on several sections of the line. This led to a further half-hour reduction in journey time and an average speed for TEE 77 of 105.3 km/h.

After further E 1012 / 112 had been ordered and delivered, the Helvetia could be crewed with the DB flagships on all three crewed sections between Hamburg-Altona and Basel SBB from summer 1968.

This allowed the maximum speed to be increased to 160 km/h on the entire section, which added another 30 minutes or so to the journey time. In 1969, the train number of the southbound TEE changed from 78 to 76. In the summer of 1971, it changed to TEE 73/72 for both directions, and with it the timetable location changed slightly.

continues on page 41



The rescuers must have seen a picture of horror, as this photo from the press conference on the day of the accident (12 August 1965) proves. According to subsequent investigations, TEE 77 "Helvetia" was travelling at around 120 km/h on the Riedbahn. The locomotive driver recognised the danger of the goods train, which was not free of profiles, and initiated an emergency braking manoeuvre. However, this was no longer able to prevent the collision at 11:35 am. Photo: Werner Kortokraks, Eisenbahnstiftung

Comparison of departure times of the various Helvetia trains

	„Helvetia-Express“ Winter 1954/55		TEE „Helvetia“ Winter 1961/62		TEE „Helvetia“ Winter 1966/67	
	<u>FT 77</u>	<u>FT 78</u>	<u>TEE 77</u>	<u>TEE 78</u>	<u>TEE 77</u>	<u>TEE 78</u>
· Zürich HB	7:39	23:51	7:41	23:14	7:40	22:55
· Basel SBB	8:46	22:46	8:47	22:10	8:54	21:51
· Basel Bad Bf*	9:13*	22:36*	8:55	22:00	9:01	21:37
· Freiburg (Breisg) Hbf	9:53	21:39	9:32	21:14	9:38	20:58
· Baden-Oos	10:54	20:36	10:25	20:18	10:29	20:07
· Karlsruhe Hbf	11:16	20:16	10:43	20:00	10:47	19:50
· Mannheim Hbf	11:55	19:37	11:22	19:26	11:24	19:18
· Darmstadt Hbf	12:33	18:55	-	-	-	-
· Frankfurt (Main) Hbf	12:56	18:36	12:15	18:33	12:15	18:27
· Göttingen	-	-	14:57	15:56	14:37	15:58
· Hannover Hbf	16:52	14:38	16:03	14:56	15:39	14:57
· Hamburg Hbf	18:45	12:45	17:45	13:14	17:20	13:20
· Hamburg Dammtor	18:49	12:39	17:50	13:05	17:24	13:11
· Hamburg-Altona	18:58	12:30	17:58	12:55	17:32	13:04

* with customs control (21 or 15 min)

TEE 72 Helvetia

1.

Zürich HB (7.00)–Basel SBB–Mannheim–Frankfurt (M)–Hamburg
 (–Hamburg-Altona (16.36))

⚡ *** 211% 450 t

▲ **ab Basel und Frankfurt**

a)	Avm	18	Basel SBB–Hmb=Altona	73	73	Hmb	745	4362
				<u>730</u>				
	Avm	17	" "	"	"	"	"	"
	Avm	16	" "	73	"	"	741	"
	Avm	15	" "	"	"	"	"	"
	Apm	14	" "	"	"	"	"	"
	Apm	24	Zürich–	"	"	"	746	4361
	Avm	25	" "	"	"	"	"	"
1)	ARDm	26	" "	"	"	"	"	"
	WRmz		" "	"	"	"	"	"

▼ **ab Zürich und Mannheim**

a) Fr–So auch 21. XI., 21., 26. XII. 1. I., 19., 23. IV.,
 1., 30. V., nicht 24., 31. XII., 22. IV., sonst Bd

1) Abt 6 ◊ und ☒ ab Basel Bad Bf
 Abt 5 ☒ Basel SBB–Freiburg
 Ⓜ 34082 Do

TEE 73 Helvetia

1.

(Hamburg=Altona (13.32)–) **Hamburg–Frankfurt (M)–Mannheim–**
Basel SBB–Zürich HB (23.00)

⚡ *** 211% 450 t

▲ **ab Hamburg u Mannheim**

a)	Avm/Am	13+	Hmb=Altona–Frankfurt	172	177	Ffm	305	
	Apm	14	" –Basel SBB	72	72	Hmb	741	4362
	Avm	15	" "	"	"	"	"	"
	Avm	16	" "	"	"	"	"	"
b)	Avm	17	" "	"	72	"	745	"
				<u>720</u>				
	Avm	18	" "	"	"	"	"	"
	WRmz		" –Zürich	"	72	"	746	4361
1)	ARDm	26+	" "	"	"	"	"	"
	Avm	25k	" "	"	"	"	"	"
	Apm	24	" "	"	"	"	"	"

▼ **ab Frankfurt u Basel**

a) So, auch 26. XII., 23. IV., nicht
 24., 31. XII., 22. IV.

b) Fr–So auch 21. XI., 21., 26. XII., 1. I., 19., 23. IV.,
 1., 30. V., nicht 24., 31. XII., 22. IV., sonst Bd

1) Abt 5 ☒ Freiburg–Basel SBB,
 Abt 6 ◊ und ☒ bis Basel Bad Bf

Ⓜ 34091 So



Sequence plan for TEE 72 "Helvetia" from train formation plan A for express trains, valid from 1 October 1972. Illustration: Sammlung Oliver Strüber

From TEE to the IC

When the Intercity was introduced at the DB for the 1971/72 winter timetable under the motto “Germany at two-hour intervals”, the TEE 73/72 was integrated into this new IC line system. It became part of Line 3 Hamburg - Basel with an extension to Zurich.

As a result, it also made a new system stop in Fulda. And the series 103 locomotives that had just been delivered soon became DB's new showpieces in fast long-distance transport, even before the Helvetia.

The 1973 summer timetable then brought a further change: the southbound TEE 73, shortened to seven carriages, swapped its timetable position with the IC 177 “Diplomat” and now ran one cycle, i.e., two hours, earlier.

IC 179		Helvetia					
Hamburg-Altona (12.36) — Hannover — Frankfurt (M) — Mannheim(— Basel Bad Bf (20.39/41)) — Basel SBB — Zürich HB (21.57)							
)4(*** (bis HH Hg 200 km/h) 214% 500 t							
max 15, AA 14, FF 13, RM 14							
▲ ab Hamburg-Altona und Mannheim							
a)	Bm	263 +	Hmb-Altona — Basel SBB	584	572 ¹⁾	13371	2513
aa)	Bm	263 +	.	170	.	14075	2517
	Bm	264 +	.	178	178 ¹⁾	01157	2514
①	Bm	265	.	.	.	.	.
	Bpm	266	.	.	.	.	.
	Bpm	267	.	— Basel SBB	.	178 ¹⁾	01159 2516
						195	
b)	Bpm	268	.	.	.	.	.
	Bpm	269	.	— Zürich	.	195	01154 2515
②	Bm	270	.	.	.	.	.
③	WRmz		.	.	.	.	.
	Avm	272	.	.	.	.	.
	Avm	273	.	.	.	.	.
	Avm	274	.	.	.	.	.
	Apm	275	.	.	.	.	.
▼ ab Frankfurt (M)							
a)	Fr, auch 4. IV., 15. V., nicht 5. IV., 17. V.						
aa)	Sa, auch 5. IV., 16. V., nicht 29. XII., 6. IV., 18. V.						
b)	So, auch 11. VI., 26. XII., 1. I., 8. IV., 16., 27. V. bis Zürich, sonst bis Basel SBB						
	①	Abt 1 Frau u Kind, Abt 2 u 10					
	②	Abt 11  Freiburg — Basel SBB, Abt 12 Dst					
	③	bewirtschaftet durch SSG					
	1)	Basel Bad Bf 17588					
■ 12/13 ■							

Sequence plan for IC 179 “Helvetia” from train formation plan A for express trains, valid from 3 June 1984. Illustration: Sammlung Oliver Strüber

With the introduction of the two-class system IC '79 (advertising slogan “Every hour, every class”), DB then revised its long-distance travel offer particularly thoroughly. A large proportion of the long-distance express trains still running as TEEs were now converted into ICs, including the Helvetia.

It now ran under the new train numbers IC 178/179; both car classes were separated by the dining car between them - the so-called block train formation. The southbound train now even started in Westerland (Sylt), whereby it was classified as a D train (D 179) on its first section to Hamburg-Altona and was hauled by a 218.

At the head of the train, an ABm 225, which normally does not run in IC traffic, rolled to Frankfurt (Main), followed by a Bm as a course coach to Saarbrücken, and, following this, the IC coaches. In Niebüll, another coach was added to the D 179, which an NVAG locomotive had brought with train 6 from Dagebüll (connection to the ferry to Föhr).



On 23 July 1988, 103 160 in front of EC 78 "Helvetia" (Zurich HB - Basel SBB - Mannheim - Frankfurt / Main - Hannover - Hamburg-Altona) shows itself to the photographer next to the A 6 near Hockenheim. Photo: Helmut Röth, Eisenbahnstiftung

It and the ABm 225 were discarded in Hamburg-Altona, while the Saarbrücken coach remained loyal to the Helvetia, now running as IC 179, as far as Frankfurt (Main). However, this was only a short episode, because already in 1981, all course coaches were dropped, as well as the extended train run from Westerland, the Intercity continued to run between its previously known end points under the same number.

A small break in the summer of 1986 was the change to the two-digit train numbers IC 78/79, before the Helvetia became an EC (Eurocity) the following year. The changes brought about by the ICE service, which started in 1991, were more significant. With a view to the first two new lines (NBS), the Bundesbahn adjusted the Intercity network. DB temporarily abandoned the direct connection Hamburg – Zurich by the EC Helvetia and forced a change of trains in Mannheim.

However, persistent protests by customers led to the revival of the Helvetia as early as 1992, which now once again connected Alster and Limmat as EC 70/71, albeit in a timetable that had been changed once

again in order to save on train sets by optimising running schedules. This was due to the severe shortage of rolling stock that had occurred as a result of the fall of the Berlin Wall. The new timetable for both train directions did not cause enthusiasm and good frequency.

EC 79		Helvetia	
Hamburg-Altona(12.49)-Hannover-Frankfurt (M)-Mannheim (-Basel Bad Bf(20.35/38))- Basel SBB -Zürich HB(22.00)			
*** 212x 550t			
max 15, AA 14, FF 14			
⚡ ⚡ ⚡			
(bis HH, RM - RK Hg 200 km/h)			
↑ ab Hmb-Alt			
Wi	a	Avm 265	Hmb-Alt Zürich 78 78 01172 2476
		Apm 264	" " " "
		Avm 263	" " " "
		Avm 262	" " " "
	01	WRmz	" " " "
	02	Bm 260	" " " "
		Bpm 259	" " " "
	03	Bpm 258	" " " "
	04	Bpm 257	" " " "
	b	Bpm 256	" Zürich 01173 2477⓪
			BaselSBB
Sm		Bm 255	" " " "⓪
		Bm 254	" BaselSBB 01171 2478⓪
	c	Bm 253	" " " "⓪
Wi	cc	Bm 253	" " 70 576 14071 2467⓪
↓ ab Frankfurt			
a)(Sa) nicht 19.XII.-02.I.,01.-03.IV., 22.V.			
b)Fr+So nach Zürich HB, sonst nach Basel SBB			
c)Fr+So auch 08.,16.VI.,23.,30.XII., 31.III.,04.IV.,11.,23.V.,nicht 07.VI.,25.XII.,01.I.,01.,03.IV., 22.V.			
cc)Mo auch 05.IV.,24.V.,nicht 04.IV., 23.V.			
01)bewirtschaftet durch SSG			
02)Abt 11 DSG Mo-Sa Hannover - Frankfurt sowie Ⓜ Freiburg - Basel SBB, Abt 12 Dst			
03)rollstuhlgerecht			
04)Beschilderung Hmb-Altona - Basel SBB und Basel SBB - Zürich HB			
05)Abt 1 Frau u Kind, Abt 2 u 10 Ⓡ			
⓪)zu Spalte 6: Basel Bad Bf 24588			
Ⓜ 11/12 Ⓜ 10/12 Ⓜ			

From 27 September 1987, EC 79 "Helvetia" was in service according to Train Formation Plan A for Express Trains - Sequence Plan as per this document. Illustration: Sammlung Oliver Strüber

After several ICE 1 sets had been approved for Switzerland, things started to move again in the summer of 1993. Since 27 September 1992, the ICE "Panda" Hamburg- Altona — Zurich was the first international ICE to travel to Switzerland, with departure times close to the old IC times, at least in the southbound direction. The new ICE 70/71 "Helvetia" finally started on 23 May 1993. Two train sets were again used for it, but from summer 1995 the train journey already ended in Basel SBB instead of Zurich, the opposite direction was served unchanged.

In 2002, DB decided to abandon almost all train names. This also meant the end of the Helvetia as the name-leading train. Connection and train numbers remained for the time being, but this was soon followed by another change in the timetable. The ICE connection 70/71, in the meantime, with a train run extended in both directions to Chur, has however remained until today.

Information about the train run on the internet:
[https://de.wikipedia.org/wiki/Helvetia_\(Zug\)](https://de.wikipedia.org/wiki/Helvetia_(Zug))
<http://www.heinrich-hanke.de/eisenbahn/reisezuege/helvetia.htm>

VT 04 501 with Velmo upgrade

Cutting Edge Digital

The simple digitisation of models with the help of Velmo's products is one of our readers' repeatedly expressed requests for contributions. Fittingly for the round F-train anniversary, we have now found a Märklin model whose upgrading seems particularly sensible. In this way, in addition to the anniversary reporting and the desired topic, we can also create an understandable link between analogue limits and digital possibilities.

Anyone who owns a model of the SVT 137 "Hamburg type" (Deutsche Reichsbahn) or VT 04 501 (modernised version of the Bundesbahn) express railcar will certainly have often wondered what thoughts went through the designer's mind when he designed this vehicle.

Unfortunately, it looks to the owner like "wanted, but not able": On one side a heavy half with motor, inside just stuffed with chassis block, drive and circuit board and at the other end a lightweight with see-through possibility.



For the planned digital conversion, we only need our basic model of VT 04 501 (Märklin 88870), the Velmo conversion parts (item no. SDS219229) and a screwdriver. Immediately after checking that the delivery is complete, we can get started.

For full annoyance, an interior light only shines from this half, while the other remains completely dark. "All or nothing", we would like to say, but Märklin never wanted to get rid of the birth defect of these models. But what is the vehicle supposed to represent credibly in this layout?

Composition of the decoder type designation at Velmo

There are three to four important pieces of information encoded in the factory designation of Velmo digital boards that the customer needs to understand if they want to order a matching part number directly from the range. We explain this with our example of the SDS219229:

- S The first letter indicates the type of decoder (L = locomotive decoder, S = sound decoder, F = function decoder).
- DS The first letter indicates the type of decoder (L = locomotive decoder, S = sound decoder, F = function decoder).
- 21229 The number combination corresponds to that which Märklin also used for its circuit board. If the decoder is suitable for several variants with different markings, Velmo uses the identification of the first product delivered.

Deviating from this, the train for which it is intended is encoded as a reference for function decoders (e.g. "SBAHN" or "ICEXP"). In the absence of a number, this designation was also used for the locomotive decoder for the 1812 series from Rokuhan/Noch ("RN181").
- A Addition of a possible variant of the decoder (not applicable in our example)

The prototype had no sleeping compartments and, if half the multiple unit had had to be closed due to defects, would certainly not have gone on the route. The lack of capacity could not have been compensated for internally with average capacity utilisation.

The lack of consistency or the failure to make a clear decision in this construction has always been a source of irritation and concern. And we know that we are not alone in this, because there are many approaches to make this shortcoming invisible or to remedy it.

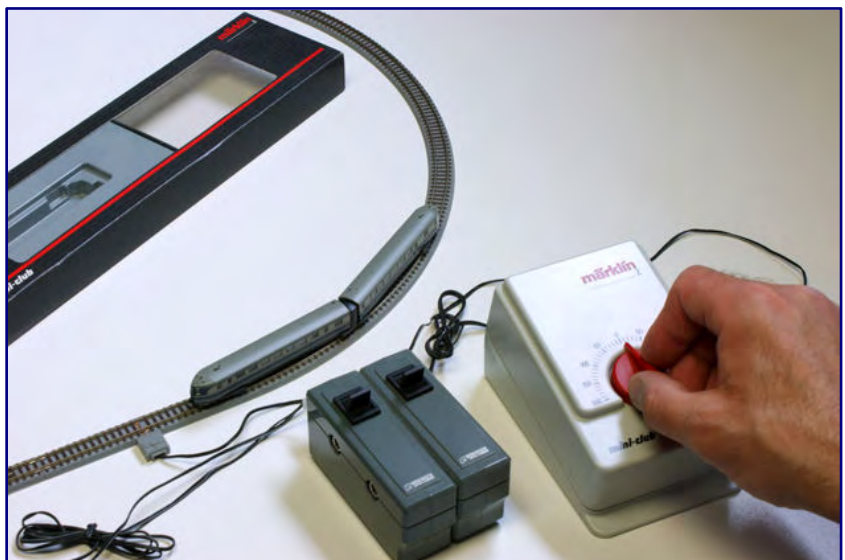
For us, the best option of all that would be conceivable here was and still is digitisation with Claudius Veit's exchange boards. Velmo has "upgraded" even more in the last

few years and has long since proven that even operating noises are no longer part of the digital fantasies of Z-gauge.

The standard protocol today is DCC, but the multi-protocol decoders can also be addressed in the SX1 and SX2 data formats if desired. In the case of VT 04 501, which we have as a model with item number 88870 in pigeon blue-grey livery, it also acquires operating sounds, but in addition even an interior lighting that also includes the motorised half of the vehicle.

So what was probably considered impossible in Göppingen, Velmo has not only questioned, but also solved. We have obtained the exchange parts confection and will now install them in our model.

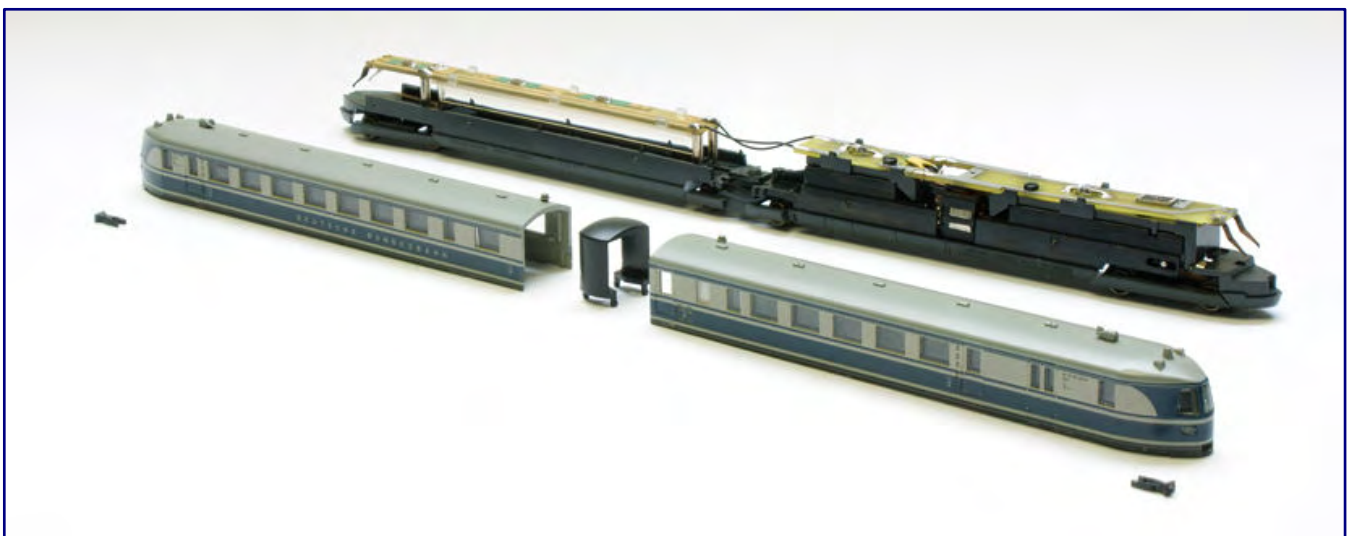
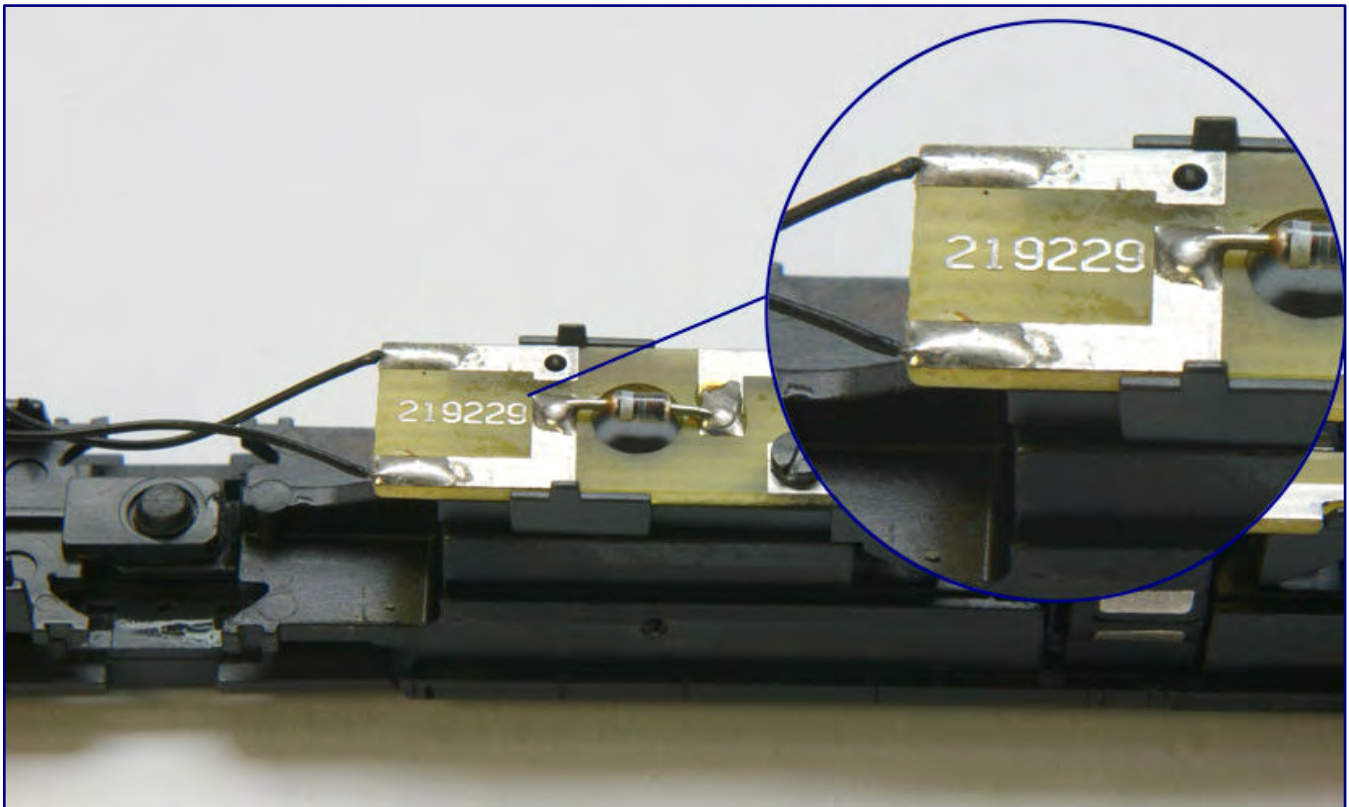
The multi-protocol sound locomotive decoder SDS219229 included in the set fits all Märklin multiple units of the Hamburg type that have a circuit board with the numbers 219229, SW141 210B or SW141111B. There it replaces both circuit boards for analogue operation in the multiple unit.



Before the conversion, there is still an analogue test run: the train should run perfectly, because digitisation does not cure mechanical problems!

Decoder installation made easy

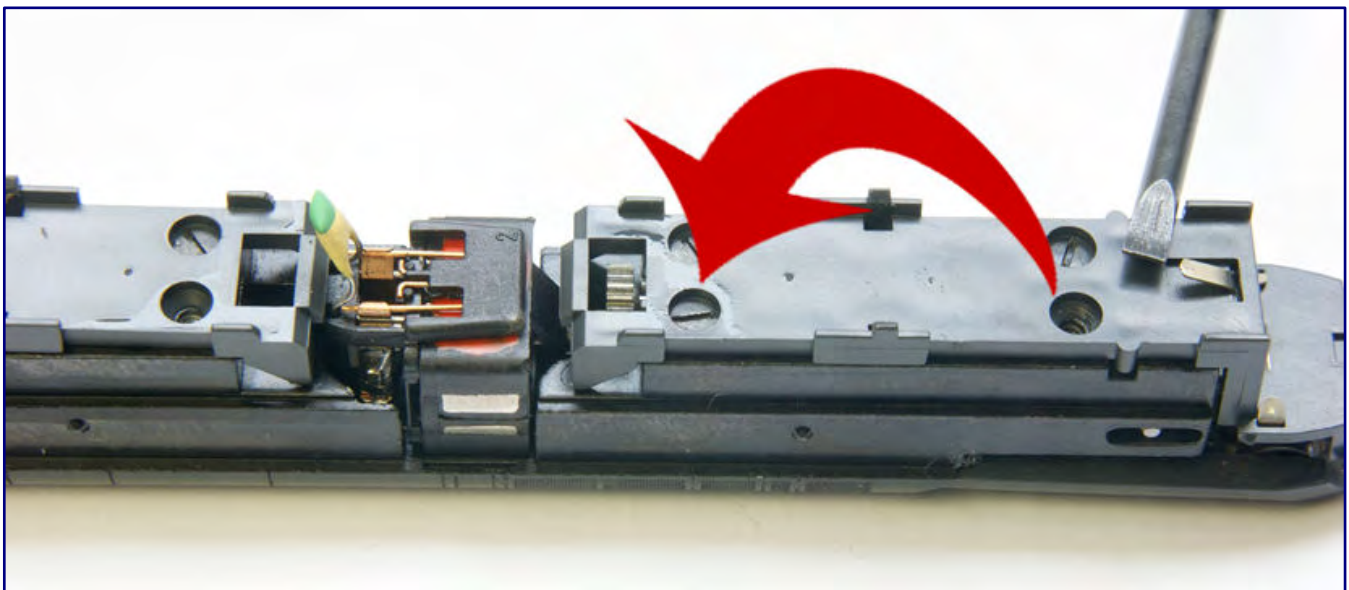
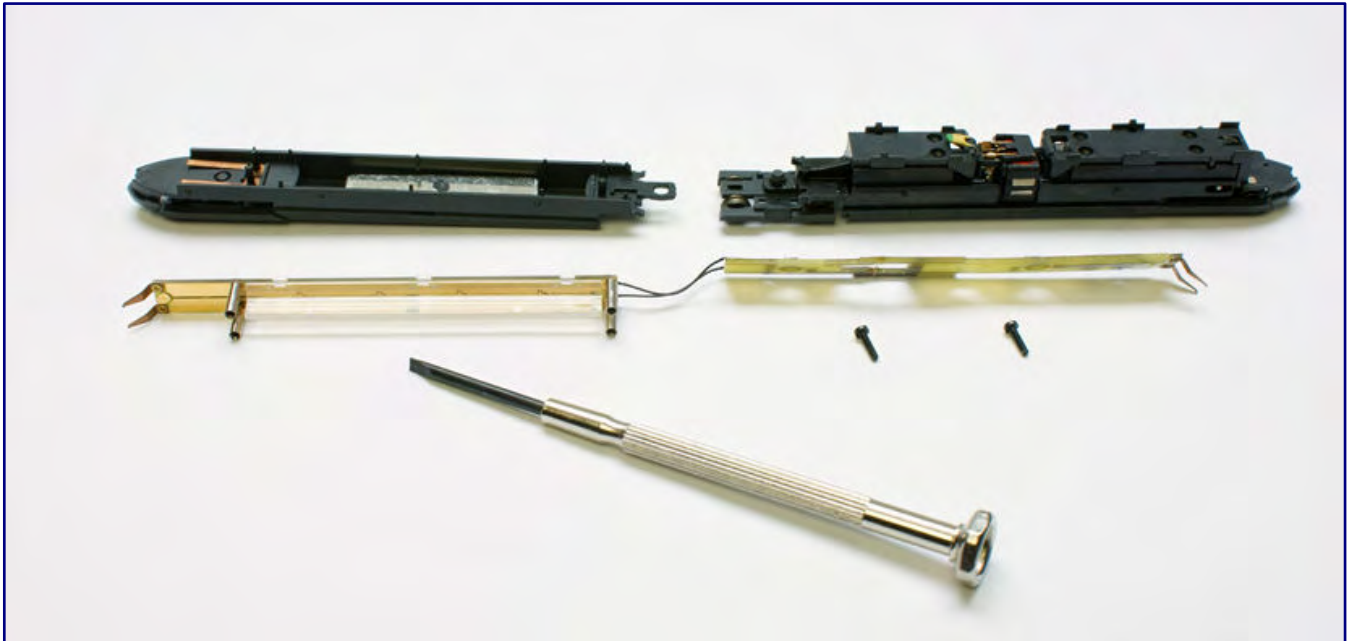
Our digital conversion begins with an (analogue) test drive, because conversion is not a panacea to help a poorly running model achieve good driving characteristics. So, we make sure of the technical condition and possible need for maintenance.



The number printed on the standard board (photo above) leads to the appropriate exchange part at Velmo for the planned digitisation. For the installation we have to get to the inside by pulling off the Scharfenberg coupling, spreading and removing the two housings and finally the transition that is plugged onto the Jakobs bogie (photo below).

It turns out that the selected model has good track contact, runs smoothly and without any noticeable disturbing noises, all previous light functions are given and the driven chassis does not heat up either. So we can start the conversion without any worries. If there were any abnormalities, cleaning with lubricant, and possibly a brush replacement would have been necessary.

Instead, we can now remove the two power car housings, after we have first pulled the plugged-in Schaku dummies at both ends. The operating instructions enclosed with the model provide information on where and how to spread and remove the plastic housings.



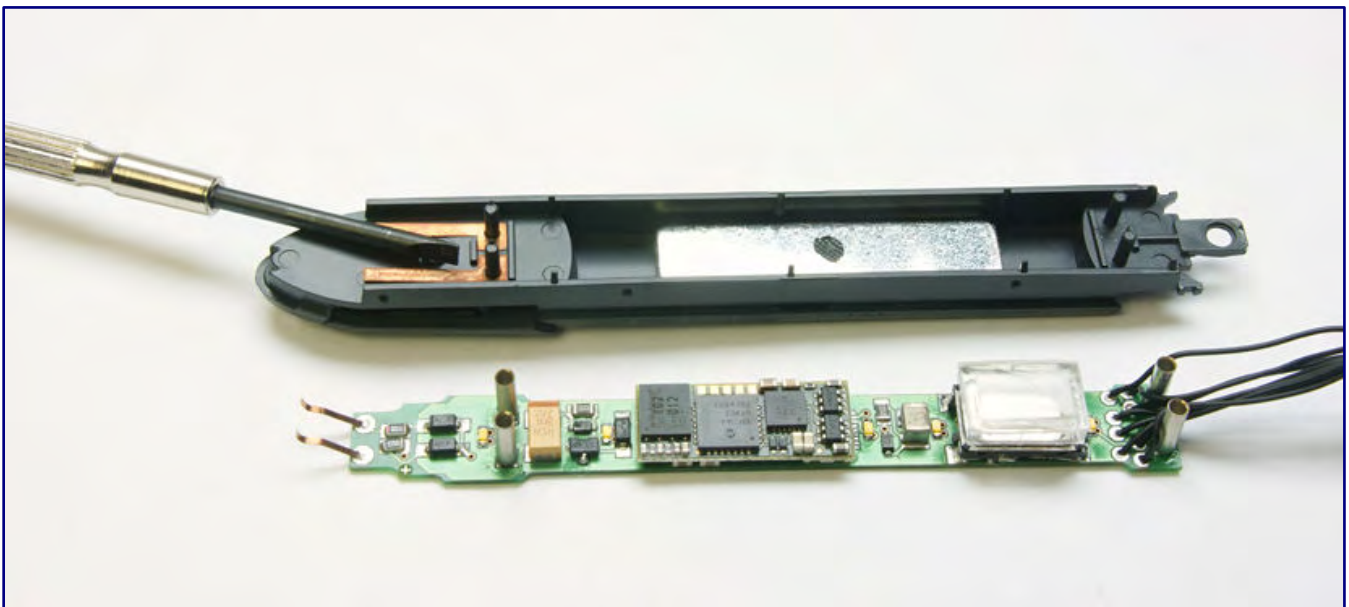
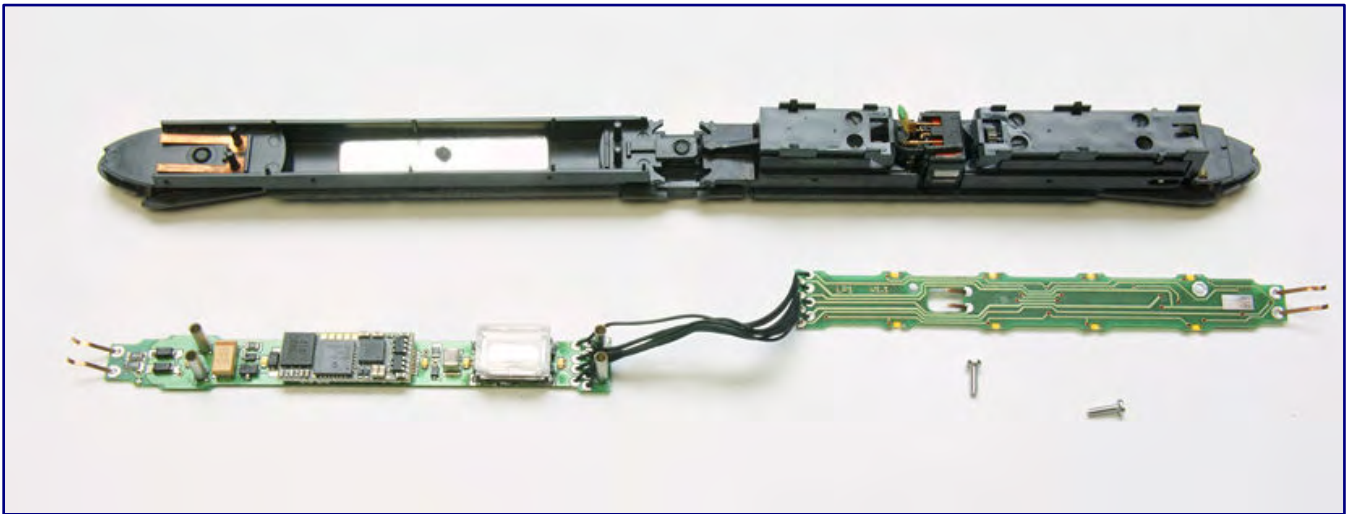
On the motorised carriage, the circuit board can be removed after loosening two screws; on the non-motorised part, the lighting circuit board is plugged onto plastic pins with four sleeves (photo above). In our case it sat there very tightly and had to be lifted off carefully after we had also loosened the holder from the bogie pin as a precaution. The next step is to move a short, countersunk screw from the front right to the rear right on the exposed gearbox block (see arrow marking in the photo below).

It is advisable to take souvenir photos of both halves of the train beforehand, which can later tell us which housing belongs on which chassis and how the Scharfenberg couplings are correctly re-inserted. In the age of mobile phones with cameras, this should not be a problem. As a last preparatory step, we also pull off the transition from the middle Jakobs bogie to the top.

Now we can loosen and remove the two screws of the circuit board on the motor carriage. After that, the circuit boards of both train halves can be removed. On the motorised part, the front, small, countersunk screw is then moved, as shown in our photo.

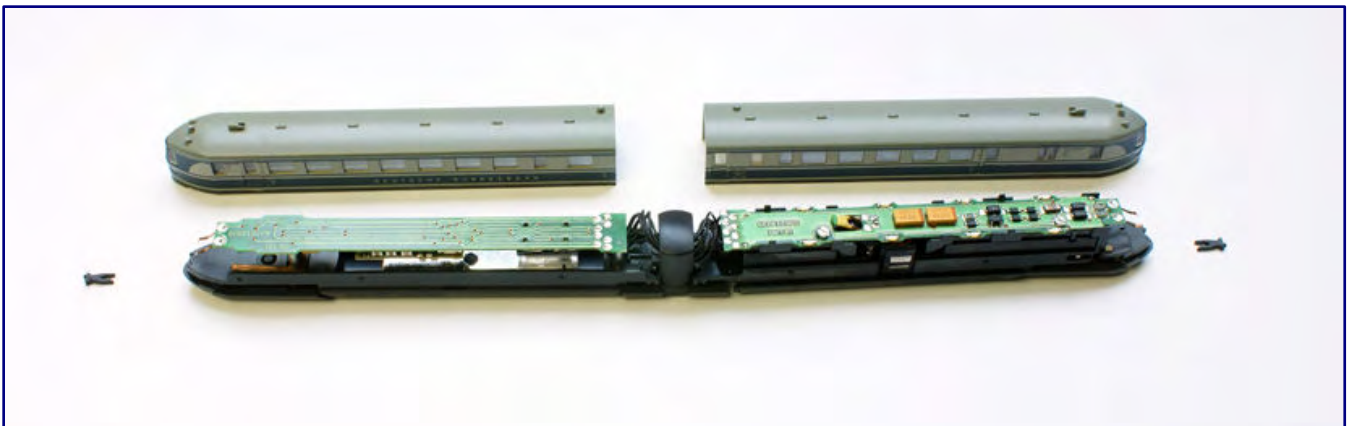
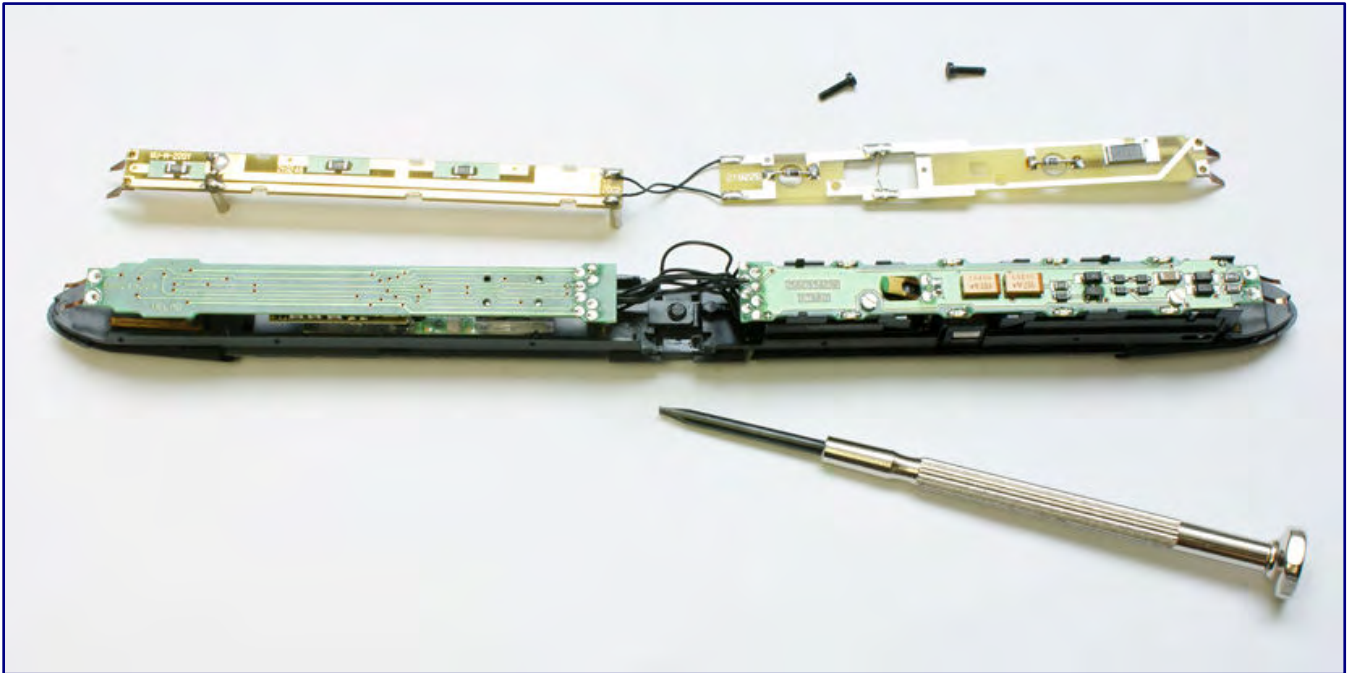
On the non-driven train section, the factory lighting board is plugged onto plastic pins with plug-in sleeves. Pulling off should be done carefully and without pressure to the sides so that these retaining pins do not break off. We keep the removed parts, because they would be necessary, in case we ever want to dismantle them.

When inserting the locomotive decoder, there are a few things to keep in mind. In order not to mix up the assignment of the two circuit boards, we first place them next to the train and look where the cut-out for the interference suppression capacitor of the motor set is to be found. This board is the one for the motorised part, the stranded wire connection will later disappear under the transitional replica.



Before installation, we place the digital exchange boards next to the model and use the future holding points to make sure that the two halves are correctly assigned and in the correct installation position (photo above). If the board is only to be plugged into the part without the drive, it is important to ensure that the sleeves are placed carefully and absolutely vertically and pressed down until they are securely in contact with the spring plate underneath (photo below; position indicated by the screwdriver).

With this, the correct alignment has been found, and the bottom and top sides should also be clear when we look at the installed SMD light-emitting diodes of the (new) interior lighting. The LEDs require special



As soon as the circuit board is fixed on the motorised part with the enclosed screws and securely plugged on at the other end, the conversion is finished. The circuit boards removed and longer factory screws go into the spare parts box (photo above). Now the reassembly begins in the reverse order of the disassembly. First, the carriage transition is put in place, the strands are passed through cleanly and with sufficient room to move, and the plastic part is then pressed into its holders (photo below).

care so that they are not accidentally torn off when installing the boards. This is especially true if we are about to snap the PCB onto the chassis.

But first we bend the aforementioned interference suppression capacitor on the motor vertically upwards to make the following work a little easier. Now we carefully place the circuit boards and first guide them into their lateral guides on the motor trolley. Once we have found the correct position, we press them lightly and snap them into place on the chassis.

On the non-driven part of the trolley, we make sure that the sleeves of the PCB are placed absolutely vertically and then carefully pressed all the way down until the contacts touch each other well. Afterwards we can push the suppression capacitor on the drive part down into the cut-out.

Directly afterwards, we check the position of the motor connection wires on the Velmo board. As with the analogue circuit board, they must rest securely on the two interference suppression capacitor connections, but must not short-circuit each other, i.e. they must not touch each other. Once this has been

successfully achieved, we screw the PCB onto the motorised part of the vehicle using the screws supplied by Velmo.

Under no circumstances should the original Märklin screws be used, because the new board is thinner than the dismantled analogue version and therefore has shorter screws as a fixed part of the delivery.



One after the other, the wagon housings are put back on and then the dummies of the Scharfenberg couplings reinserted. The VT 04 501 is now ready for a digital function and driving test.

The subsequent assembly is done in reverse order to the initial disassembly: fitting the transition on the centre bogie, then pressing on the two superstructure housings, and, finally, reinserting the Scharfenberg couplings.

Test and commissioning of the digital model

Before we put our model into operation, we would like to add an important note that is important for all readers who want to enjoy a digital model for the first time:

Not all digital command stations available on the market are factory set for the needs of Z gauge.

The digital voltage of the control unit must then be reduced to a level that is compatible with Z gauge in order not to risk damage to the decoder, model or operational infrastructure.



The test begins with reading out the decoder address on a programming track. We can ignore the fact that the factory DCC address 3 in the unit's memory is doubled for the time being.



In the digital function test, we check the reaction to driving commands, the light changing and switching functions and, subsequently, also the operating sounds. Since no faults or problems are detected, individual setting changes can follow.

This is especially true for the current flow, from which the control unit assumes a short circuit and switches off the track voltage with a short time delay. With selected control units, the time delay can even be preselected.

Our first test is done in programming mode with reading out the factory address (here: 3). Since the built-in D&H SD18A decoder is pre-configured ex works for the respective Märklin model, no special settings should be (and are) required here. It really can't be simpler than Velmo makes it for its customers.

Those who want to adjust individual parameters according to their own needs after the first check will find information on the possibilities, configuration variables (CV) and setting values as well as possible error patterns and causes in additional operating instructions. Velmo is also known for providing good customer service via e-mail contact for individual problems.

We would like to briefly name a few typical problem areas that the manufacturer itself mentions: The number of programmed decoder speed steps must match the entry of the address to be controlled stored in the digital central unit.

The same applies to the distinction between the historical, short addresses and the long addresses introduced later. Even if there are discrepancies between the settings of the decoder and the central unit, this will lead to malfunctions.

Otherwise, only the following parameters seem to us to be necessary and sensible for conceivable adjustments:

- Change the locomotive address (because “3” is the factory setting for almost all providers);
- Setting a maximum speed (for possible insertion into an electronically controlled system programme or aligned with the prototype maximum speed);
- Adjusting the starting and braking deceleration (for optimal operational flow and safe stopping before signals showing stop); as well as,
- Volume for the operating noise (for good perception without annoying third parties).

For example, we changed the address in CV 1 (short locomotive address) to the value 4 in order to exclude an overlap with the next new acquisition. At the same time, according to the programming instructions downloadable from the manufacturer's web3pages, we have changed the value under CV 29 (basic settings) so that our decoder listens to the long address (CV 17 / CV 18), which we have subsequently assigned to “604”.



We have changed the short DCC address from 3 to 4, but the vehicle is to be addressed via a long address (604), which we also store in the CV settings. As soon as the vehicle is configured, we save its settings on a locomotive card (Modellbahnunion). This saves us from having to create a new layout on other control centres, because we can have the data read in straight away.

For digital switchers, it should be briefly mentioned at this point that changed properties of locomotive decoders are stored and therefore also remain when the operating voltage is switched off.

This also applies, for example, to the last selected direction of travel. All settings can also be changed as often as desired and, if necessary, can be returned to the factory settings by means of a reset function.

continues on page 54

Illustrations on page 53: Assignment of various values to the 7 bits of CV 29 (illustration: Velmo operating instructions) and the calculations required to store the new value 46 instead of 14 so that the decoder listens to the long address.

Bedeutung	Wertebereich	Voreinstellg.
Bit 0 - Fahrtrichtung drehen	0 - Fahrtrichtung normal 1 - Fahrtrichtung gedreht	0
Bit 1 - Anzahl Fahrstufen	0 - 14 Fahrstufen 1 - 28, 128 Fahrstufen	1
Bit 2 - Analogbetrieb	0 - Analogbetrieb aus 1 - Analogbetrieb erlaubt	1
Bit 3 - Rückmeldung erlaubt	0 - Rückmeldung aus 1 - Rückmeldung erlaubt	1
Bit 4 - keine Funktion	0 - muss immer 0 sein	0
Bit 5 - Erweiterte Adresse	0 - verwende CV1 1 - verwende CV17,18	0
Bit 6 und 7 - keine Funktion	0 - muss immer 0 sein	0
Wert für Register CV29		14

CV 29 - Grundeinstellungen

Bit	0	1	2	3	4	5
Dez.wert	1	2	4	8	16	32
Werte	0	1 ↓ 2	1 ↓ 4	1 ↓ 8	0 16	0
						= 14
Soll erw. Adresse	0	1 ↓ 2	1 ↓ 4	1 ↓ 8	0	1 ↓ 32
						= 46

↳ Bit 5: Erweiterte Adresse

- 0 - verwende CV1 (kurze Adr.)
- 1 - verwende CV 17,18 (lange Adr.)

Each locomotive decoder offers (different numbers of) memory locations for the many control options, fine adjustments and functions. Depending on the data protocol in Selectrix, they are called parameters (par), characteristic values, or, common under DCC, configuration variables (CV).

If these are to be programmed subsequently, no physical access to the decoder is required. The model does not have to be opened. All the digital model railroader needs is his control unit and a programming track. The programming track, which is separated from the system current, protects against unintentional overwriting of the data of other decoders that are on the layout in their models at the same time.



VT 04 501 has been successfully converted and is ready for use on the layout. At the platform, it is waiting to depart.

In retrospect, all that remains to be said at this point is that the digital conversion with the Velmo solution is a matter of a few minutes and does not place any demands on the operator that would not already be present with the maintenance of their own vehicles.

Neither soldering skills nor knowledge of electronics are required, and no permanent changes need to be made to the initial model. At the same time, this also means that we can rest assured that we can restore the model to its original state at any time, as long as we keep the analogue parts that have been removed.

However, to ever seriously consider this thought seems inconceivable to us. With more individual functions, our model has become much closer to the prototype. The operating noises of the engine or even the horn, the compressor and the closing doors are already a blast.

Besides, it would still be possible to use it in analogue mode, if we were to accept a slightly impaired controllability. And then the point of criticism from the introduction would still pay off: All or nothing - our VT 04 501 now has interior lighting in both train sections, which we can also switch off. From now on we will certainly no longer do things by halves!

Basic model:
<https://www.maerklin.de>

Digitisation services:
<https://velmo.de>

Source of supply for locomotive cards:
<https://www.modellbahnunion.com>

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Guide to Model Railway Photography (Part 2)

Photo editing and optimising

In our June issue, Dirk Kuhlmann explained what minimum equipment is required for taking good model railway photographs. But first and foremost, successful images always also depend on the person behind the viewfinder. Consequently, there were also tips and hints for making the right choices on motifs, backdrops and composition. Today, the second and last part of our mini-series deals with the key aspects of photo editing on the computer.

By Dirk Kuhlmann. In the second part of our small photo tutorial, we want to take a closer look at photo editing software. In addition to the well-known Adobe Photoshop, there are many other good software solutions available from specialised shops.

In addition to these fee-based solutions, there are a number of free image editing programmes, of which, GIMP is probably the best. However, all providers have one thing in common: getting started with editing is usually complicated and discouraging for many amateur photographers.



Following up on the explanations given in part 1 of our series, we are now taking a photo which we will immediately edit with a simple software.

As a result, model railway enthusiasts often are not quite satisfied with their images. Although one may have gotten a nice shot of a beautiful scene, it frequently might lack that final touch of perfection, so to speak.

Types of graphic design software

Graphics software is a generic term for computer programmes that are used to create (design) or edit (change) digital photos, drawings and graphic elements on the computer under screen control. These include image editing programmes (image editor), painting and drawing programmes or vector graphics programmes (drawing editor).

A distinction is made between two types of computer graphics: Vector and pixel based. Vector graphics describe figures as line connections of different points, which also connect to form areas. Because of this, their enlargement does not result in any loss of quality. They are used, for example, to digitally save geometric figures. Well-known representatives of these programmes are Inkscape or Corel Draw.

Pixel-based graphics, on the other hand, are composed of many individual pixels with colour information. Because of this, the file size can be reduced in a compression that involves a loss of quality. This technique is mainly used to digitally save photographs. The programmes GIMP and Adobe Photoshop Elements mentioned in the article belong to this group.

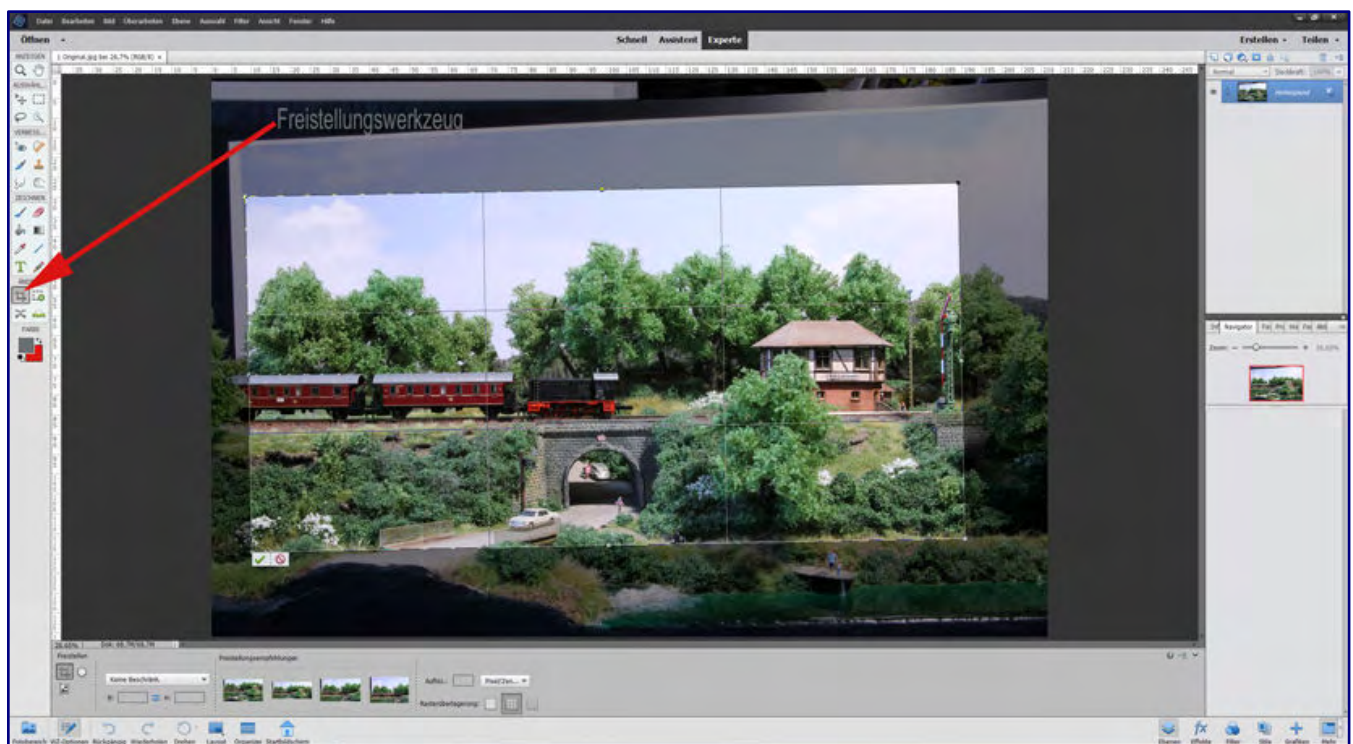
Unfortunately, even so-called and fast network solutions or automatic image enhancement options are not always satisfactory. What do we do now?

Unfortunately, even so-called and fast web-based solutions or automatic image enhancement options are not always satisfactory. So, what do we do now?

Using the example of a single image, I will guide you through the further editing process in relatively simple steps and only with the respective software tools for manual photo re-editing.

Step 1 -The Original Photo

I chose Adobe Photoshop Elements 2021 (abbreviated: PSE) for demonstrating my approach. It is important to note that there are actually no major functional differences between commercial and free software solutions. Adobe PSE sits right next on my computer to its professional big brother (Adobe Photoshop CS) and should be completely sufficient for what I am trying to show you here.



I now call up our example photo in the so-called expert mode. Under the tab "Revise" we find our applications. The function for selecting only a section of the picture is called "cropping tool" and is located on the left bar.

Step 2 - The Photo in PSE

With the cropping tool it is now possible to display only the desired section of the entire photo and to cut off the surrounding elements. You do not have to follow any rules, simply let yourself guide by your own taste.

By the way, the cropping tool has two very different modes: On the one hand, you can cut away parts of the borders at the top and bottom as well as on the right and left. This also allows you to change the format of the photo.

If, however, the original image format (for photos often a 4:3 width to height ratio) is to be retained in the selected section, the mouse pointer is pressed simultaneously with the shift key when cropping. The defined section can then also be moved on the original image with the mouse (grab the frame with the mouse pointer in the centre of the section) before the selection is confirmed by clicking on the green tick.



The cropped photo (see the image on the previous page for how to do this) is only the first, but crucial step for assessing the overall effect.

The cropped photo gives a first but decisive impression of its effect. And if the selected crop should not turn out as successful as expected, the undo function (under the "Edit" tab) can be used to return to the original image and the process can be repeated endlessly.

Step 3 - Colour correction and decolourisation

Most photos of model railway layouts are probably taken under artificial light. It is not surprising, therefore, that the colours sometimes look a bit strange. Especially with so-called mixed light, when different light sources with different light colours (measured in Kelvin) act on the scene at the same time, the automatic white balance camera setting quickly reaches its limits.



Via the menu selection “Adjust colour” in the tab “Revise” we reach the colour settings. In the example case we reduce the saturation from the basic value 0 to the new -14.

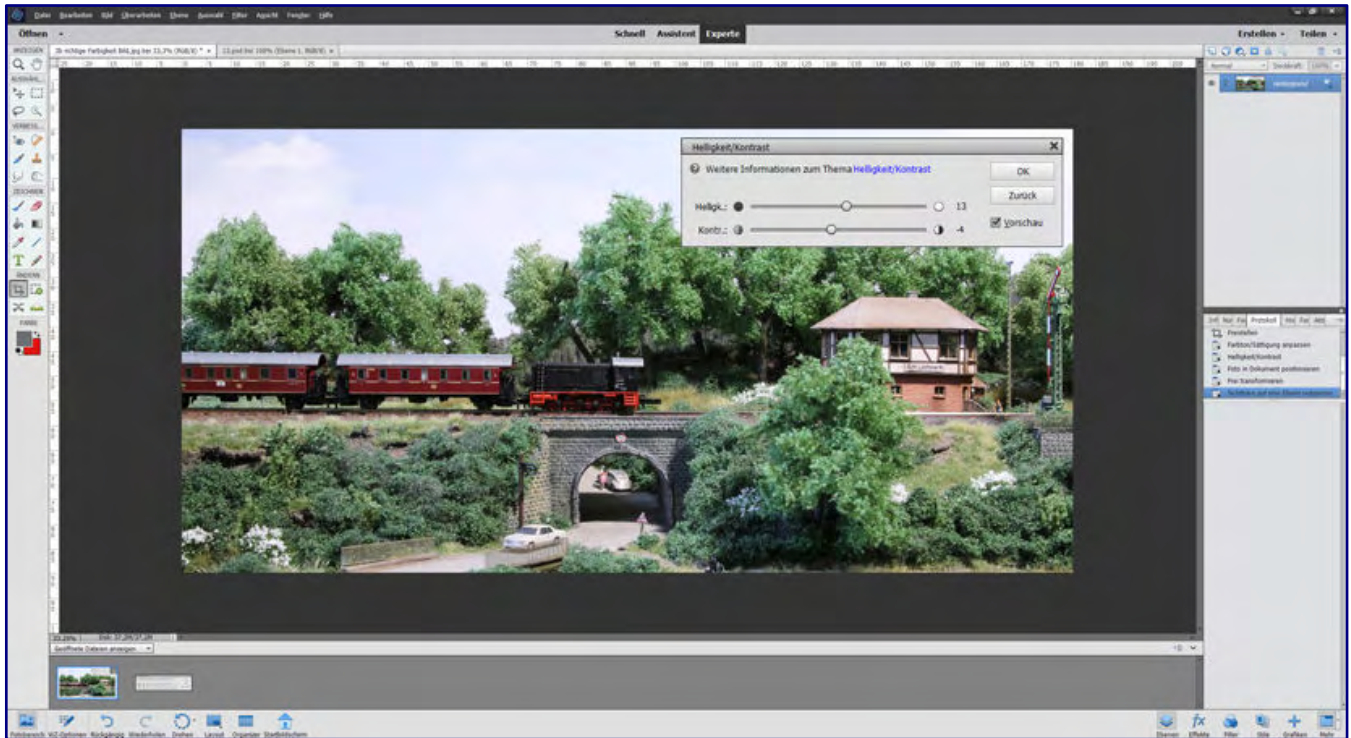
Here, all programmes have an automatic colour correction built in. A quick click and the process continues. However, many photos are still downright dull because of excessive saturation. We often see these results even in professional magazines. The only thing that helps here is a discreet decolourisation / desaturation. Under the selection “Adjust colour” in the tab “Revise”, the problem is solved quite quickly.



Oversaturated photos are a typical problem in model railway photography, often caused by the use of integrated or camera mounted flash units. The changes towards a more realistic impression after this correction step are clearly visible.

Step 4 - Lighting and contrast

Even with the best lighting, a beautiful photo can sometimes appear too dark or too bright. Here, too, corrections are possible, but easier with underexposed photographs. Extremely overexposed images, in which only white areas are visible where there should actually be fine details, are the only ones that can no longer be salvaged.



With “Adjust lighting”, also found under “Edit”, we correct minor exposure errors and ensure a balanced impression (photo above). Contrasts can also be softened or increased in this selection, if necessary. The result of this step is shown in the photo below.



Fortunately, such mishaps happen much less often today in the age of digital photography than they did in the analogue age: In the past, a completely wrong exposure metering became obvious only once the film was developed and it was too late for a second attempt. Today the screen on the back of a camera will immediately reveal major errors.

When reworking a minor exposure error, which usually occurs when areas of the picture are very differently illuminated, the function “Adjust lighting” in the same programme tab comes in very handy.

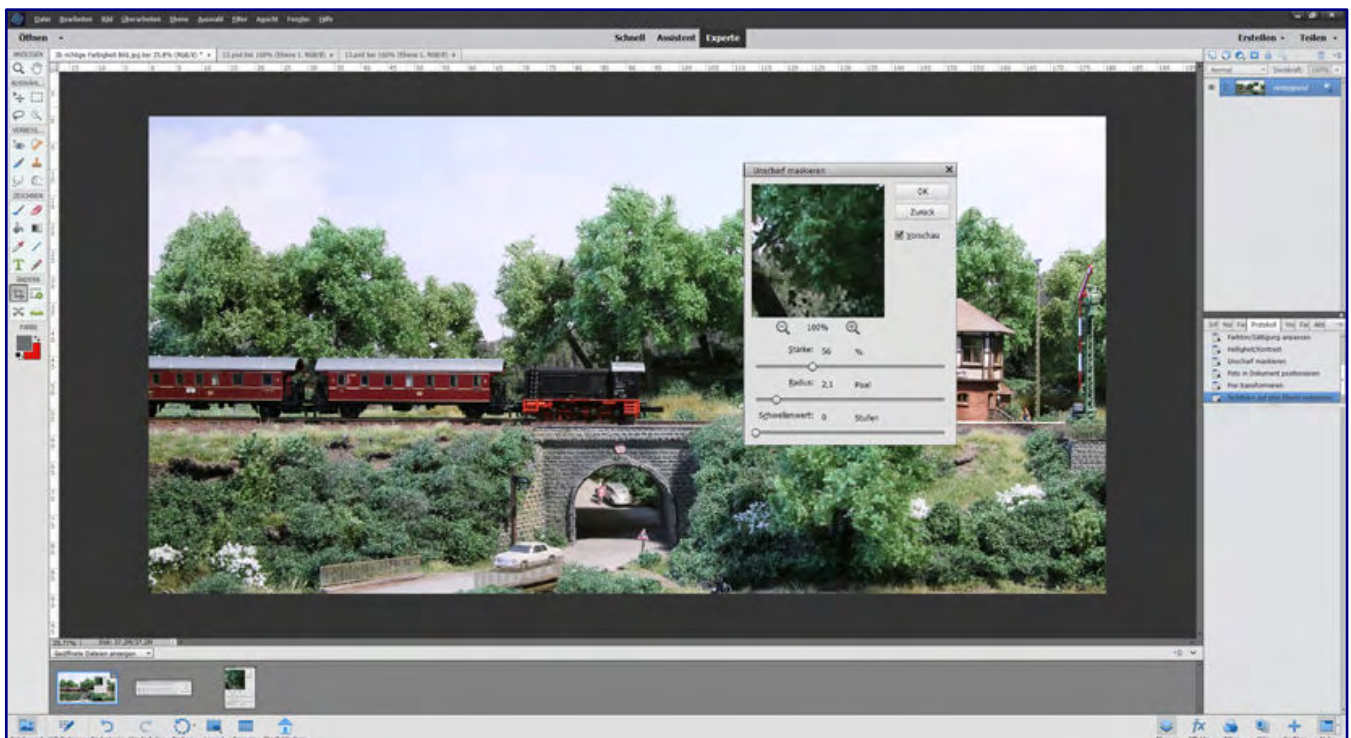
Even a contrast that is too hard can now be softened a little. Just try out different image effects here, because a shot without or with too little contrast will otherwise quickly look boring or like an optical “fabric softener” that would make any tension disappear in a foggy soup.

Step 5 - Sharpen the Photo

As a final point, we sharpen our photo a little. The aim here is not to make sure that the last and smallest inscription of a Z-gauge locomotive can still be read, but to create a pleasing overall impression of the photo. We remain in the same programme tab and now first select “Unsharp Mask.” The following basic values have proven to be a good starting point:

- Strength 56 %
- Radius 2.1
- Threshold value 0

By the way, don't be misled by the strange terminology.



We have reached the last step and are sharpening our shot a little more. After that, everything should fit for a perfect overall impression.

Step 6 - Save the result

Now we already have our final result. The final edited photo should now be saved in the highest level with a new file name. This way, the original photo is always preserved and would be available unaltered for future creations or interpretations.

As you can now easily see, it is only a few steps to a beautiful result. In the first part of this short series, I had already demonstrated the most important steps for taking good model railway photograph.



The edited photo meets our expectations and can thus be saved on the hard disk. To ensure that the original file is always retained for other projects, the edited photo is given a new, i.e., a different file name!

Thus, in this article, all the intermediate results look quite appealing. However, it remains important to emphasise that all this can only result from an already good photo. You should always take the time to carefully select the motif, the lighting, the perspective, etc. A series of pictures with different camera settings (exposure series) can also be helpful.



In the early days, this was also ideal for me as a creative starting point for further excursions into image processing. Of course, in the course of almost twenty years, various special programmes have been added, but for a “completely normal” reworking, I like to fall back on this procedure again and again.

The cover shot of the September 2020 issue of Miba magazine shows a section of the Z-gauge layout “Bad Rothenzell.” You may not believe it, but this photo was edited exactly according to the steps listed above.

The mood, the light and the colourfulness also convinced the editors. Why don't you try it yourself and surprise our editorial team with your contribution for a wonderful cover for a **Trainini®** issue or for our annual photo calendars.

Photo editing software:
<https://www.photoshop.com>
<https://www.gimp.org>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Mercedes-Benz W 123

Verkaufswunder aus Sindelfingen

Als noch nicht diskutiert wurde, ob Daimler ohne Benz auftritt und wie die Firmengründer im Unternehmens- und Markennamen vertreten sein sollen, da schlug die Geburtsstunde eines besonderen Fahrzeugs. Der Mittelklasse-Mercedes W 123 war eher bieder und geizig ausgestattet, aber grundsolide und für seine Qualität geschätzt. Evolution statt Revolution prägten seinen Ursprung und seine Geschichte, die im folgenden Titel zusammengefasst wird.

Alexander F. Storz
Mercedes-Benz W123
Das meistgebaute Modell 1975 - 1986

Motorbuch Verlag
Stuttgart 2021

Gebundenes Buch
Format 21,0 x 28,0 cm
144 Seiten mit 200 teilweise farbigen Abbildungen

ISBN 978-3-613-04370-1
Titel-Nr. 04370
Preis 19,95 EUR (Deutschland)

Erhältlich direkt ab Verlag
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Autor Alexander Franc Storz hat im Verlag zum vorliegenden Titel bereits einige Auto-Fachbücher veröffentlicht. Schon mit dem Band zum Vorgänger des hier behandelten Modells aus dem Hause Mercedes-Benz hatte er uns begeistert.

Als Journalist im Bereich Automobilhistorie, tätig auch für verschiedene Fachmagazine, ist er ohne Zweifel die richtige Person für dieses Buch. Im Fokus steht dieses Mal das größte Erfolgsmodell aus dem Hause Mercedes-Benz, denn kein Auto wurde dort so häufig gebaut wie der W 123.

Auch 25 Jahre nach dem Ende seiner Produktion kennt wohl jeder dieses Fahrzeug: Es ist im Ausland, vor allem auf dem afrikanischen Kontinent, bis heute präsent, war das typische Taxi, mit dem vermutlich jeder schon gefahren ist und hat sich auch als historisches Fahrzeug bis heute gehalten.

Der Verlag drückt dies in seiner Titelbeschreibung wie folgt aus: „W 123 - ein Buchstabe, drei Ziffern, mehr braucht es nicht, um eine Legende zu beschreiben.“ Dem ist nicht viel hinzuzufügen, außer dass Kenner ihn heute als wohl besten Mercedes verehren, der je gebaut wurde. Er galt und gilt bis heute als perfektes Beispiel für schwäbische Wertarbeit.

Dass das aber nicht ganz korrekt ist, kann der Leser diesem Titel schnell entnehmen: Erstmals wurde ein Mercedes-Mittelklassemodell auch in einer Kombiversion angeboten. Die durfte freilich nicht so heißen, weil sie eben kein Handwerkerauto sein sollte, sondern ein „T-Modell“ (T für Touring) für Kunden, die aufgrund Lebensumstände oder Sportgeräte mehr Ladefläche brauchten.



Jedenfalls kam diese Karosserieform nicht aus Sindelfingen, sondern wurde immer im früheren Borgward-Werk Bremen gebaut. Und auch diese Version hat nicht wenige Verehrer, die den Bremer Ausführungen eine noch höhere Qualität als den schwäbischen „Originalen“ zusprechen.

Sicher ist es spätestens jetzt zu bemerken, dass hier kein technischer Aufsatz vorliegt, der jede Komponente genauestens unter die Lupe nimmt, Entwicklungshintergründe und -ergebnisse vorstellt und Vergleiche zwischen diesem Auto und seinem Vorgänger anstellt.

Das große Thema des Buches ist vielmehr der Mercedes-Benz W 123 im Alltag statt auf Hochglanzprospekten. Genau das macht auch den Nutzen für Modellbahner aus. Die gut ausgewählten und bestens wiedergegebenen Aufnahmen kontrastieren zwar auch Daimlers Marktpositionierung und Kundenwahrnehmung, viel stärker erinnern sie aber an die Zeiten, als uns dieses Fahrzeug alltäglich begegnet ist.

Das liefert Anregungen, natürlich auch Erinnerungen und Ideen für das Gestalten auf der Modellbahnanlage und auf Dioramen. Schmerzlich bewusst wird uns an dieser Stelle, dass der W 123 bis heute eine echte Programmlücke im Maßstab 1:220 darstellt.

Es gibt entweder gar keine Modelle wichtiger Ausführungen, Uraltaufgaben sind längst nicht mehr zeitgemäß oder es reduziert sich auf das deutlich seltener anzutreffende Coupé (Märklin), das auch nur selten angeboten wurde. Vielleicht mag dieser Buchtipp daher auch Kleinserienhersteller anregen, hier Abhilfe zu schaffen?

Das wäre auch dringend erforderlich, denn immerhin war das Erfolgsmodell von 1975 bis 1986 in Produktion, heute fahren nicht wenige der jüngsten Exemplare mit einem H-Kennzeichen immer noch herum und erfreuen uns auf Ausstellungen.

Eine enorme Vielfalt wird uns bei der Lektüre präsentiert: Viertürer-Limousine, elegantes T-Modell, zweitüriges Hardtop-Coupé und sogar ein gestreckter Achtsitzer ab Werk, dazu Sonderaufbauten und -fahrzeuge für Polizei, Feuerwehr, Krankenfahrzeuge oder auch Leichenwagen.

Und obwohl die große Zeit des Tunings glücklicherweise an diesem Modell noch vorbeiging, hat der Autor auch deren Auswüchse zusammengestellt und dem Leser nicht vorenthalten – eine wahre Schocktherapie. Wenigstens einige der Cabrio-Umbauten und die AMG-Modifikationen nehmen sich da ansprechend aus.

Mit viel Sachverstand und Liebe zum Detail präsentiert der Autor alle diese Fahrzeuge im früheren Alltag, als Versuchsträger und Rennwagen, auch weit über Deutschlands Grenzen hinaus. Das unvermeidbare Maß an Technik ist in Tabellenform am Ende angefügt.

In Summe sind wohl alle Facetten unterhaltsam und laienverständlich zusammengeführt worden, womit der Band jeden Eisenbahnfreund ansprechen kann, dem es nur um Passion für alte Technik und „Beiwerk“ auf seiner Modellbahn geht.

Und bis wir diese Epoche der deutschen Automobilgeschichte auch im Maßstab 1:220 umfänglich darstellen können und solange wir eine wichtige Vorlage schmerzlich vermissen, vermag unser dieser Titel doch wirksam zu trösten. Er lädt halt zum Schwelgen und Erinnern ein.

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Baureihe 103 - stärkste Lok der DB Paradepferd der Bundesbahn

Für den Dienst vor TEE- und auch F-Zügen wurde sie entwickelt, 1965 in Form von vier Probelokomotiven in Dienst gestellt und ab 1970 dann in Serie beschafft: die Baureihe 103. Rund 30 Jahre bestimmte sie den schweren Fernzugverkehr der Deutschen Bundesbahn und ihrer Nachfolgerin Deutsche Bahn AG. Auch heute ist ihre Geschichte nicht auserzählt. Der EK-Verlag widmet ihr gleich zwei Bände, um sie umfassend vorzustellen.

Christian Wolf / Christian Ernst / Uwe Clasen
Die Baureihe 103 – Band 1
Vorgeschichte, Entwicklung & Technik, Einsatz der Prototypen

EK-Verlag GmbH
Freiburg 2017

Gebundenes Buch
Format 21,0 x 29,7 cm
240 Seiten mit 374 teilweise farbigen Abbildungen

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Christian Wolf / Christian Ernst / Uwe Clasen
Die Baureihe 103 – Band 2
Einsatz der Serienloks, Versuchsdienst und Verbleib

EK-Verlag GmbH
Freiburg 2017

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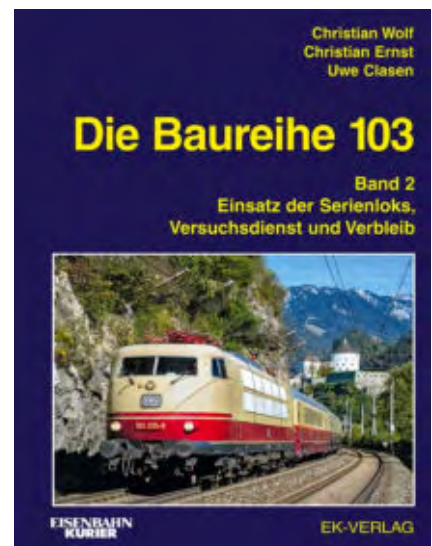
ISBN 978-3-8446-6031-9
Art.-Nr. 6031
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Einige Jahre ist es her, da haben wir Baureihenportraits für die ehemalige Paradelok der Bundesbahn vorgestellt. In ein neues Format gebracht und geringfügig angepasst, fanden sie damals eine Wiederauflage.

Auch in der EK-Baureihen-Bibliothek darf ein umfassendes Porträt über die legendäre Baureihe 103 natürlich nicht fehlen. Und hier richten sich die Autoren traditionell an einen anspruchsvolleren Leserkreis, der mehr Tiefgang bei technischen Beschreibungen, Beheimatung und Einsatzgeschichte erwartet.

Insofern sind die heute vorgestellten Titel kaum mit jenen zu vergleichen, die wir schon einmal rezensiert haben. EK-Bücher sind, dem Beschriebenen folgend, halt auch dicker und deshalb zwangsläufig auch teurer. Dass dieses Geld gut angelegt ist, bestätigen aber gerade diese zwei Bände wieder einmal. Es sind Standardwerke, die niemand wieder aus der Hand geben wird!



Steigt über die Jahre der Wissensdurst oder Informationsbedarf, lässt sich mit den blauen Büchern aus Freiburg nichts falsch machen. Deshalb stellen sie immer wieder eine klare Kaufempfehlung dar – so auch heute.

Bereits 2017 sind diese beiden Bände zu einer Lok erschienen, die für den internationalen TEE-Verkehr, aber eben auch die leichten F-Züge vorgesehen war und deshalb so gut zu den Themen dieser Ausgabe passt. Dass sich ihr Einsatzprofil so deutlich in Richtung schwerer Intercity-Züge verschieben würde, konnte niemand ahnen, als 1965 die Prototypen und ab 1970 die Serienmaschinen in Dienst gingen.

Schon Ende 2002 hatte der EK-Verlag eine einbändige Erstauflage präsentiert, als die letzten Vertreterinnen sich gerade von der Schiene verabschieden sollten. Erforderliche Aktualisierungen und auch ein konsequentes Fortschreiben bis in die Gegenwart konnte damals niemand ahnen. Sie haben sich zu einem Umfang von fast 500 Seiten summiert und dadurch das Aufteilen auf zwei Bände erzwungen, die wir kurz beschreiben wollen.

Band 1 beschreibt die Vorschichte, also das Umfeld des Entstehens einer so leistungsfähigen Lok. Dazu gehören auch die Erfahrungen im Probetrieb, die zur Technik sowohl der E 03 (103⁰) als auch der Serienausführung 103¹ (kurze und lange Ausführung mit 115 bzw. 30 Exemplaren) führen. Bauartunterschiede, Farbgebungen (inklusive Studien) und Instandhaltung werden im weiteren Verlauf ebenfalls abgehandelt.

Eine Überleitung zum Band 2 stellt dann die Einsatzgeschichte der vier Prototypen dar. Dieser knüpft nahtlos an und widmet sich vorrangig jener der 145 Serienmaschinen, fortgeschrieben bis in die heutige Zeit, denn die Geschichte ging zur Jahrtausendwende längst noch nicht zu Ende.

Versuchsdienst, Ausmusterung sowie Verbleib heute noch existierender oder sogar aktiver Exemplare ergänzen die vielen Informationen. Beispielsweise ist die für eine deutlich höhere Geschwindigkeit zugelassene 103 222-6 heute bei Rail Adventure und gehört längst noch nicht zum alten Eisen. Statistiken und wichtige Daten dürfen in EK-Baureihenportraits nicht fehlen und fanden daher auch Eingang in den Abschluss von Band 2.

Wie können wir diese vielen Eindrücke geeignet zusammenfassen? Lesen wird es sich wohl so, wie auch bei vielen anderen Büchern aus diesem Verlag: hervorragend zusammengestellte und perfekt reproduzierte Bilder, die alle Inhalte und Stationen der langen Geschichte umfänglich repräsentieren, dazu anspruchsvolle Texte und Informationen mit Tiefgang, ohne langweilig zu wirken.

Zweifelsfrei sind auch diese beiden Bücher die Standardwerke für die Baureihe 103 und werden es wohl auch auf lange Zeit bleiben. Und diese kleine Einschränkung gilt auch nur vor dem Hintergrund, dass die Geschichte der Baureihe 103 auch heute noch nicht endgültig beendet ist und in einigen Jahren sicher ein weiteres Fortschreiben erfordern wird.

Der Vergleich des aktuellen Doppelbands zum Vorgängerwerk zeigt eine in vielen Bereichen neue oder erweiterte Bebilderung, darüber hinaus auch eine neue oder überarbeitete Aufmachung der Inhalte. Und das führt zu einer kleinen Korrektur des zuvor Geschriebenen: Vielleicht gibt jemand doch ein solches Buch aus der Hand – dann aber sicher nur, um das Buch von 2002 gegen die fünf Jahre jüngere Doppelausgabe zu ersetzen.

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: <https://www.ekshop.de>
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Staying in the conversation (part 4)

Trafofuchs - News from the Fox's Den

Trafofuchs is one of the most important small series manufacturers for Z gauge and can already look back on fifteen years of history. Currently, the programme and the manufacturing process are undergoing a decisive change, as the duo of Volker Brock and Birgit Foken-Brock will explain in the following. But the question of where the company name actually comes from is always the big question.

By Birgit Foken-Brock. Customers and trade fair visitors often ask us where the company name "Trafofuchs" actually comes from. After all, we neither deal in transformers nor are we called Fuchs (translator: German for fox).



Behind Trafofuchs is a duo consisting of husband and wife team Volker Brock, who is mainly responsible for products with lighting, and Birgit Foken-Brock (in photo), who got famous in the Z gauge scene for her creative design of figures and scenes - including some celebrities. Photo: Trafofuchs

In 2003, when we needed an alias to register on an electronic auction platform to buy accessories for our first Z gauge layout, Volker's eye fell on the Märklin transformer. At the same time, our son had a toy fox hopping over the still bare tracks. Both impressions combined in his mind to form the term "Trafofuchs" - our later brand name was born.

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The last two celebrities to be transferred to 1:220 scale by Birgit Foken-Brock were TV presenter Carmen Nebel (picture above) and her guest Bernd Stelter (picture below). The presenter and entertainer, who was born in Unna, had the honour of presenting her with her own miniature as a gift during her ZDF Christmas show. As a memento, he received his 1:87 and 1:220 scale miniatures set up against the background of the studio set.

As newcomers to the Z gauge community, we didn't know where to find suitable decoration material at the time and we started to tinker ourselves, first with simple trees and small cottages. When Volker brought a pack of Preiser figures to paint one day, there was no stopping me. Reminded of my art studies, I picked up a paintbrush, while Volker used his knowledge as a PC technician to turn to lighting vehicles with LEDs.



Over the years, working life has provided many and also very popular inspirations for different figures, here of a miller and his journeyman.

In 2006 we officially founded the company Trafofuchs and over the years built up our extensive range of approximately 300 different Z gauge items. The spectrum ranges from illuminated decorative parts, such as St. Andrew's crosses, warning beacons and lights, to emergency vehicles with flashing warning lights and unlit cars.

For more life on the layout, I am constantly expanding our selection of figures: from the railway station to various situations in town and country, to various scenes in working life and the well-earned after-work hobby and sport.

In addition to traditional costume figures, milled carriages and figures for Era I, there are also many decorative parts for Wild West layouts. And of course, winter and Christmas layouts can also be enlivened and enriched with our products.

Although we discovered more and more of our colleagues' offerings over time, we have by and large succeeded in covering crucial gaps in the assortments and avoiding parallel developments. This was facilitated by the fact that for many years model railway modellers considered Preiser's assortment to be sufficient and, on the other hand, probably no one dared to tackle the tricky subject of figures.



Many developments were also based on models from the American market. Staff and guests on the US rail ferry are just one example. And Trafofuchs customers appreciate the quality offered even and especially in times of Far East counterfeits.

Today, with the oversupply of bad Far East counterfeits of Preiser figures, things look a little different. But our customers do seem to appreciate quality. Some of our articles, such as the speeding postman on his service bike, canoes or our ice cream vendor with a small ice cream van, have been in our assortment since 2006 and they are still frequently ordered.

Many of the items have been improved over time, depending on technical possibilities. New products are also constantly being added because customers specifically ask for them.

Request show for individualists

We try to respond to these individual requests and produce matching figures according to photos or descriptions. This is always a variation from the actual serial production, means our catalogue offers as announced on the internet pages.

Since we only offer ready-assembled and painted figures as well as models, each item has to be handled many times. But the customer can then take them straight out of the box and put them on his layout.

For many years I redesigned unpainted Preiser figures and adapted them to the corresponding situation. This often required major “surgery” with scalpel and superglue, whereby anatomical knowledge from my naturopathic training helped me a lot.



A large and important part of the programme is not the “off-the-shelf” products, but figures developed according to individual requests, such as these three Mexicans taking a siesta on the ship.

The close relationship with the Preiser company as a supplier, however, also brought problems. When no more Z gauge figures were available for five years, Trafofuchs was only able to survive because model railway modellers from all over the world sent us their remaining stocks of figures so that we could keep the business going. We are still grateful to them for that!

Three quarters of a year ago, I made my first experiments with 3D printing and very quickly realised the opportunity this technology offered to become independent of suppliers. However, I soon had to realise that models I found on corresponding platforms were mostly not suitable for Z scale.

These figures were also too static and “constructed” for me. Of course, if you complain, you have to do it yourself. Modelling figures with the help of computer programmes was initially unfamiliar and difficult, especially as the programmes were not necessarily self-explanatory.

Since I am more used to working with my fingers than with the computer mouse, it took some practice. It is a long way from a virtual ball to a printable figure with countless recursions and improvements.

The Future with 3D Printing

But my excitement for this young technique is huge. At last I can design figures from scratch the way I want them to be. For example, it makes a difference whether I glue a box to a figure's belly subsequently or whether the posture is designed from the beginning so that it looks natural when carrying the box.

I often have to stand myself in front of the mirror or ask the family to assume a certain posture so that I can then recreate it plausibly and authentically on the computer in the CAD programme.



With increasing business success and sustained delivery problems from Preiser, the need to become independent of this supplier grew. CAD design and 3D printing offered a solution, which led to the ongoing rearrangement of the delivery programme. This is demonstrated here by the example of the ice skater, which we see as many printed blanks and a finished piece. Photo: Trafofuchs

Step by step, as a result of these activities, many compositions and individual figures will be replaced by our own 3D designs in the future. For example, we have already completely changed the winter assortment!

For many years, we have also been offering products made of etched or milled parts. But since we can't do everything ourselves, we have found and involved suppliers for this. This additionally increases our popular assortment with attractive articles or parts.



Milled and etched parts required for figures and models come from suppliers. In this way, they too have played a decisive role in what Trafofuchs has been valued for over the past twenty years.

But, no matter what technique, whether cast, milled, etched, printed or as a reconstruction of polystyrene figures, in the end everything has to be painted. Besides good red sable brushes and paints, it is also important to have a suitable workplace. My worktop is so high that my forearms always rest on it. Freehand brushing doesn't work as well as working under time pressure.

In general, we take a lot of pleasure in making the many small details, which we also incorporate into our products. And Volker and I sincerely hope that all our customers will see this in the products we deliver.

Setback by a deluge

A few days after writing this article, the company premises in the basement of our house in Erfstadt were flooded by penetrating rain and seeping water. Fortunately, we managed for the most part to push back the gushing floodwaters over a period of hours, as well as to bring the movable inventory and equipment to safety or put them on pedestals.



Trafofuchs got off lightly during the flood, but the private rooms and workshop in Erfstadt were damaged. Necessary repairs now hinder ongoing production and require customers to wait longer. Photo: Trafofuchs

But even though the water was not too high, the damage is unfortunately substantial. Since no craftsmen are currently available in our region as a result of the massive and severe damage, we will have to repair most of it ourselves.



Birgit Foken-Brock paints new figures in her workshop. This picture was taken before the flood, because the workplace is temporarily located in the garden shed. Photo: Trafofuchs

In the meantime, I have set up a makeshift workshop in the garden shed, which was also flooded but has since dried out again. Unfortunately, our customers still have to expect somewhat longer delivery times, as we are more than well occupied with coarser work than Z gauge.

Please be a little more patient with us than we would otherwise demand or wish while we carry out the urgently needed repairs to preserve the fabric of the building. Because there is one thing you need not worry about: Despite flooding, Z gauge will not perish!

Webpages of this supplier:
<http://www.trafofuchs.de>



Spende und werde ein Teil von uns.
[seenotretter.de](https://www.seenotretter.de)

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Spendenfinanziert

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.



The following letters to the editor reached us on the basis of the first delay message from June, which we had communicated on our pages.

For the first time in the history of **Trainini®**, we have not been able to complete this edition within the month for which it was intended.

This will also have an impact on at least the September issue, which we will probably shorten and also postpone by a week.

This turbulence was triggered by a death in the family of our editor-in-chief, which has unfortunately subsequently occurred.

Understanding from the reader's side:

I also wish you and your editorial colleagues and your family's strength and confidence that you will eventually overcome the strokes of fate in your private environment, and I hope with you that the people affected can recover again.

Of course, everything connected with that has priority, then there are the professional obligations and the other demands of everyday life; the hobby just has to take a back seat. The fact that despite all the stresses and strains you still managed to deliver the two latest issues of Trainini on time before the end of each month (and you are not obliged to do so!), and with a considerable amount of content, deserves my respect!

In view of the circumstances described, I did not expect this; I am all the more pleased about it. Perhaps the editorial work will also help you to get your mind off things, at least in between, during this difficult time.

I hope that (as far as possible) all Trainini readers will continue to wait with patience and forbearance for the coming Trainini issues, even if there should now be major delays and/or cuts in content. (...) I wish you a quiet weekend, to take a deep breath!

Jochen Brüggemann, by E-Mail



Good wishes from the Netherlands:

Hello, my name is Robert Thalbruck, yes, from the Dutch Website op-zet.nl. But I'm also a true follower of your very inspiring Trainini! Such a Pity that we have to wait, but of course even more for you who try to make it month after month and normally with such a fantastic result!

I sincerely wish you strength with your health problems and be aware that you're not alone in this. Get well soon!

Robert Thalbruck (<https://op-zet.nl>), by E-Mail

First hobby repairs after the flood:

I'm sure you've heard about the storms here in the region, which have caused much more serious problems than defective model railways.

After a good two weeks of helping in the disaster area, today I sorted through a friend's items that were not to be disposed of and came across his Z-gauge railway. Since everything is dry again, and I didn't find much rust, I want to try to get the locomotives running again bit by bit, but as a half-novice I'm finding it a bit difficult. (...)

The article on three-axle bogies in locomotives provided very interesting detailed knowledge. (...) Thanks to your email, I found the article immediately and devoured the whole issue. It's nice to look beyond the gauge and to admire the work of your editors.

By the way, the first locomotive is alive again, of course, the old BR 89, a grateful test object because it didn't end up in the mud. So, it seems to be possible in principle. (...)

Axel Müller, by E-Mail

Praising feedback from the manufacturer:

I have been reading your magazine with respect for the last two evenings! As an H0-railroader, I am already enthusiastic about what you can do in Z-gauge! Many years ago, we were at an international meeting of Z gauge railroaders in Geseke. (...) We don't have much to offer for Z gauge, but it was a very nice weekend, organised in the best possible way.

I would like to thank you for including us in your current issue. I wish you and your team continued success and always good ideas. Model railways are the most beautiful hobby in the world! May the world and our hobby remain with us for a long time to come.

Roland Kühn, Fohrmann-Werkzeuge GmbH, Görlitz

20 Years Miniature Wonderland:

On 16 August 2021, the Miniatur Wunderland Hamburg looked back on its 20th anniversary. Unfortunately, no celebration took place due to the pandemic restrictions, but, nevertheless, the operators fortunately knew how to draw public attention to themselves.

After Miwula opened its doors to visitors in the summer of 2001, it has grown quickly and steadily to eventually even become Germany's most popular tourist attraction. Incidentally, the founders Frederik and Gerrit Braun have also managed to make model railways respectable for large audiences with their excellent show layout, where the railway plays a rather, albeit central and important, secondary role.



It is astonishing and admirable what the two Braun brothers (outside in the photo), here with VIPs at the opening of the Italy section, have put together in Hamburg.

It all started on 16 August 2001 with the three sections Knuffingen, Austria, and Central Germany, which were soon joined by other themed landscapes such as Hamburg, USA, Switzerland, Knuffingen Airport, Italy (since 2018 with Venice), Monaco, and soon South America. 36 million euros have been poured into the currently 1,499 m² facility in around 1 million working hours so far.

It has already attracted 20 million visitors, most recently around one million per year. Today, 360 employees look after the well-being of the guests, who also accept long waiting times for a visit, as well as the facility and its operation.

The anniversary celebration was cancelled with a heavy heart in consideration of the current restrictions. Instead, the creative team has put together a three-minute anniversary film that summarises the highs (and lows) of the last twenty years in an exciting way. It can be seen at the following link: <https://youtu.be/qjhVSB6GLKk>.

Exclusive single garages from Archistories:

Exclusively for Ztrack, Archistories has designed and produced new single garages based on US models. They were designed in the style of 19th century farmhouses, which have met with great enthusiasm. They can be built as a supplement to these, but can also be set up individually.



As an example, we show the sand-coloured garage (art. no. 426070-S) from all sides, which also makes the included tool hook-in bar visible. Photos: Archistories

The colour of the solid-coloured hard cardboard kits is matched to the houses (the Ztrack article numbers are in brackets): dark blue (426070-DB), grey (426070-G), light blue (426070-LB), yellow (426070-LY), green (426070-O), red (426070-R), sand-coloured (426070-S), and white (426070-W).

Joy and disappointment with Märklin deliveries:

In the last few weeks, many new products have arrived at the dealers, but some of them do not arrive in full numbers right away. One example is the class 41 oil multi-purpose steam locomotive (item no. 88275) for Era III, which is the 2020 insider model. A detailed test of this locomotive is planned for the October issue, when perhaps the Era IV version will already be available and will allow a comparison of the different details.



Despite small faults, the class 41 oil (item no. 88275) looks very successful and will undergo a detailed test in October. Behind the G 10 it has coupled a stake car pack Rmms 33 with car model (82132).

On the other hand, the steam locomotive class 85 from the bronze fine casting edition (88932) is completely delivered with transparent protective lacquer. Visually, it impresses with a reduced inscription range, which is intended to emphasise the colour contrasts between the metal and the parts laid out in black.



The side view with the round company sign under the DB biscuit identifies 220 085-5 (88206) without a doubt as a locomotive from the Krauss Maffei construction lots. But Märklin seems not to have noticed this, because only the side views of the model are correct.

The delivery of the class 220 in purple livery (88206) for Era IV has also begun. The 220 085-5 came, like the prototype for the 88203 model, from the Krauss-Maffei construction lot. Characteristic for this was



The comparison with the model of the V 200 150 (left), which was also built by KM, proves that the 220 085-5 (right), which has now been delivered, has the wrong trim line inlet: only MaK deliveries had the wider-looking version, the correct one would be the clearly more pointed-looking inlet of the sister locomotive, recognisable without a magnifying glass and particularly embarrassing for the manufacturer, because of the repetition of this mistake.

the more pointed design of the trim lines on the fronts, which continued after their dismantling also with the painted-on trim lines and also reached the MaK machines.

Märklin has unfortunately, despite explicit editorial advice in the announcement, even with reference to correctly executed H0 models in this respect, for the second time implemented the decorative line course recognisably incorrectly according to MaK delivery condition. We illustrate this with a comparison photo to the V 300 (same front shape), which was also built by KM.

In our view, this cannot be a question of cost and thus an economic compromise. In our opinion, customers may expect a prototype-correct model, especially in view of the demanded purchase price of more than 200 Euros, otherwise the prototype selection would simply have to be adjusted to a suitable MaK template. This repetition mistake is incomprehensible for us, the manufacturer disappoints us all the more here.

We were pleased with the bottle green painted standard luggage wagon Pwi-30 (87521), a "blunderbuss" without DB logo, and also the themed pack "coal loading" (82337) consisting of an open freight wagon of the O 10 formation type with real coal load. Especially the enclosed Fuchs excavator 300 with high driver's cab looks very appealing and is suitable for depicting an emergency coal loading at the depot.

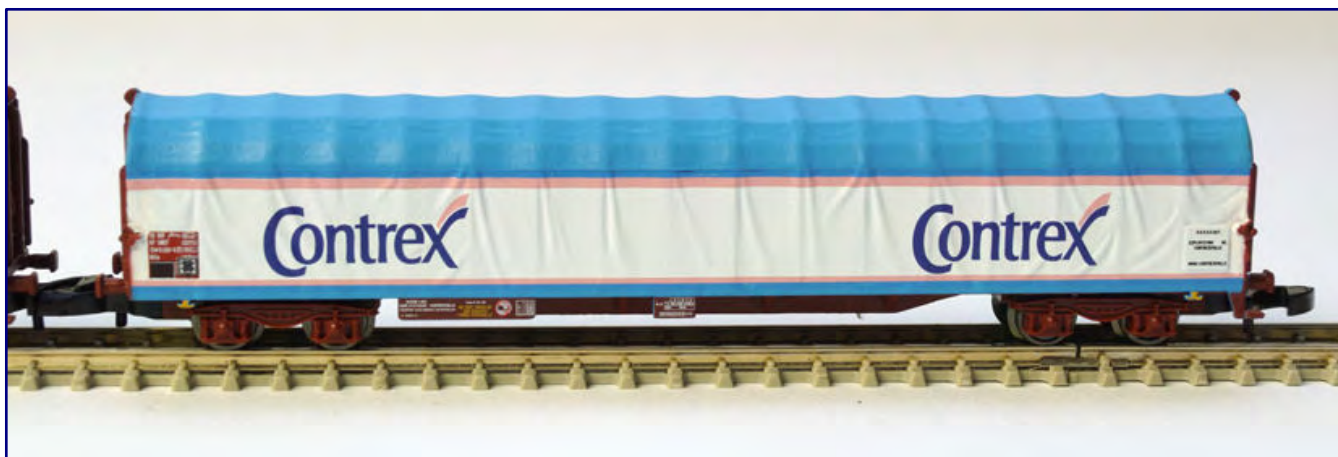
In the meantime, the friends of Era VI were treated to the sliding tarpaulin wagon pack (86356), consisting of two wagons Shimmns-tu 718, one of them with advertising logos and the slightly aged sliding tarpaulin wagon Shimmns 718 of DB Cargo (86357). All of the wagons are classified with DB Cargo and painted in traffic red.



Although it does not contain any new moulds, the themed pack "Coal Loading" (82337) looks appealing to enrich the supply tracks of a depot with it.

The three-part car set consisting of two DB Rmms 33 stake cars (82132) and a car model for Era III, which still comes from the 2019 spring novelties, has also arrived in the shops. One is without handbrake with wooden shelves and wooden stanchions, the other with brakeman's platform, steel shelves and pressed sheet metal stanchions.

According to the enclosed instructions, the end stanchions can now also be pulled down for the first time, which we succeeded in doing on one example, as the photo shows. The new product also includes a light blue Hanomag delivery vehicle with platform.



The bottle-green Pwi-30 baggage car (87521; photo above) and the Rils sliding tarpaulin car with Contrex advertising logo (from 82427; photo below) are also among the current deliveries.

Also arrived is the three-part DR freight car pack (82268) consisting of two Gbkl covered freight cars (ex "Dresden" type of association) and a Klms 3430 low side car. One of the covered cars for Era IV was intended for transporting tetraethyl lead and is therefore equipped with yellow sliding doors and corresponding warning signs.

Also available are the SNCF sliding tarpaulin wagon pack (82427) with three attractively designed Rils-type wagons and advertisements for the three mineral water brands Evian, Vittel and Contrex, as well as the hard cardboard kit "Freight wagon MCI-43 parked" (89012).

International Spielwarenmesse (International Toy Fair) Flood fundraising campaign:

In Nürnberg (Nuremberg), too, the dramatic photos from the German flood areas moved the staff. Spielwarenmesse eG therefore wanted to support the people affected in their reconstruction efforts as far as possible. That's why it called on its employees to take part in an internal fundraising campaign in which every euro was quintupled by the company.

Within a very short time, a sum of 9,000 euros was collected. The donation went to an alliance of several aid organisations, such as the Arbeiter-Samariter-Bund (ASB), CARE Germany or the Malteser Hilfsdienst, with the "Aktion Deutschland Hilft". The initiative significantly supports the emergency services on site during the rescue and clean-up operations.

An important detail from NoBa-Modelle:

Tiny, but important they are: We are talking about trashbins, which should not be missing on model railways. At the suggestion of our editorial team, NoBa-Modelle has now constructed typical US representatives. Of course, we would almost like to say that these bins, which are 3D printed from resin, are hollow on the inside and can therefore also be filled with a bit of rubbish.



Typical representatives of US-style bins are these NoBa model designs that will soon be available for purchase. The 3D printed models are hollow on the inside and designed with a removable lid, so that they can be filled, prototypically.

Soon, they will certainly appear on the pages of this supplier, and be activated for purchase.

Waterway design with Noch:

The range for watercourse design has been supplemented by Noch 2021 with two new products that have now arrived in the trade. One of them is the new water colour composition (art. no. 60878) with five coordinated, ready-to-use colours.



New set of water colours (art. no. 60878).
Photo: Noch

They can be precisely dosed and mixed with the help of dosing tips to create application examples such as a mountain lake, pond in the forest, beach or even a lagoon by the sea. After drying, they can also be painted over and doused with the hot melt water pearls from the same company.

The model water “colour” (60877) is easy to handle: It is simply poured into the prepared body of water in a layer of 2 to 3 mm and left to dry. After about 12 hours it has hardened and shrunk by half its volume. What is new about this water, which is already known in principle, is in this case the blue-green colouring ex works.

Micro Trains Deliveries:

MTL plans to deliver only one model this month: Part 6 of the “Sweet Liquid Series Molasses Gilbert (530 00 540) dedicated. This address is on a 39-foot tank car with a filling dome.

The wagon was built in 1926 and is insulated and heated for the H.J. Gilbert Company from Savannah (Georgia). Molasses, a raw material made from sugar for various foodstuffs, was transported in this wagon.

Micro-Trains products are distributed by Case-Hobbies (<http://case-hobbies.de>), among others.

Locomotive design for vehicle repair:

A newly designed class 218 locomotive now symbolises the history and future of vehicle maintenance in Cottbus (FZI) at Deutsche Bahn AG. 218 497-6 was chosen to build a bridge from steam locomotive refurbishment to ICE maintenance in the future with a livery supported by Märklin and Piko.



This is how the new class 218 in FZI special design, supported by Märklin and Piko, will look in Z gauge. Photo: Märklin

The highly different design on both halves of the locomotive is the result of a creative competition among FZI staff. Märklin and Piko will offer the models in different scales. For Z gauge, Märklin's implementation (item no. 88807) will have a bell armature drive and direction-dependent lighting with warm white and red LEDs.

Modellbau Laffont announces a new product:

Modellbau Laffont (<https://www.modellbau-laffont.de>) has announced yet another new product in 2021 with the Ludwigsau-Friedlos stopping point (art. no. Z3601).



The half-timbered stop south of Bebra (Hesse) dates from the 1920s and is located on the double-track, electrified main line between Bebra and Fulda.

The building, which is made of solid-coloured architectural hardboard, can therefore be used in epochs II to VI.

Finest roof tile engravings are a special feature worth mentioning of the small building with the dimensions 35 x 19 x 25 mm (L x W x H).

It will be available from the manufacturer as early as the end of September 2021.

The Ludwigsau-Friedlos stopping point (art. no. Z3601) is a quaint, new product that will be released by Modellbau Laffont. With a little creativity and imagination, there are plenty of possible uses in the most diverse eras. Photo: Modellbau Laffont

Expansion in the signal programme:

The Austrian supplier ZDC Z Dream Collection, which presented its trade

fair innovations in the February issue, has expanded its selection of ÖBB signals for Z gauge. As of now, the ÖBB main signals in the design set up from 1980 are available in a three-handle version, as well as a double signal.



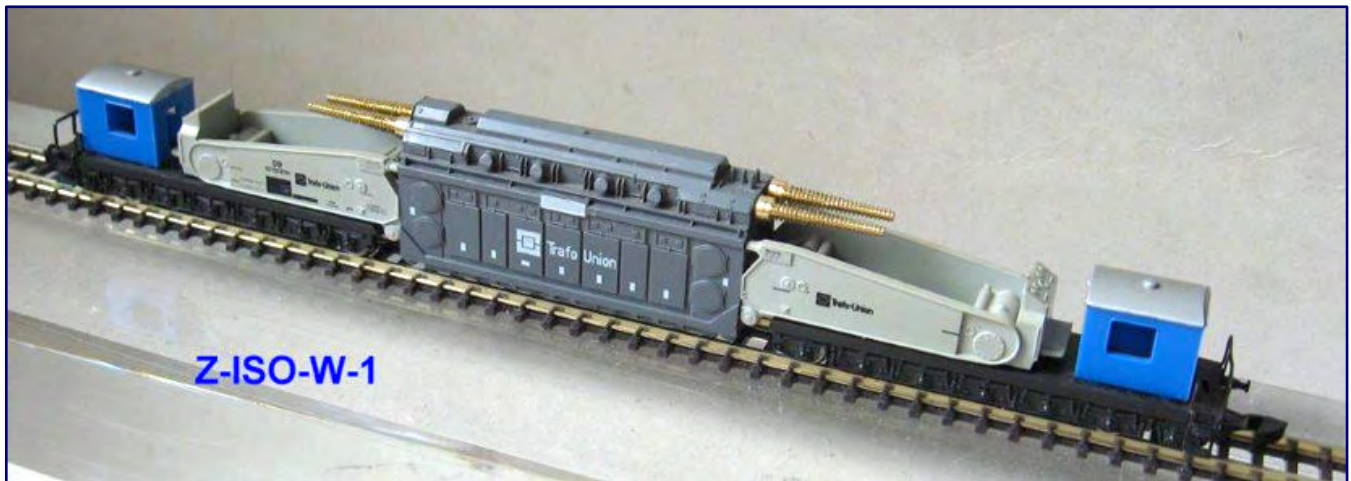
On the basis of two ÖBB signals (photos on the left and right) as well as a Ks multi-section exit signal of the DB (photo in the middle) we would like to show the programme extensions by illuminated and controllable finished models. Photos: ZDC Z Dream Collection

The production of the German Ks signals (combination signals), the modern design of the Deutsche Bahn AG, which can also be found on high-speed lines, has also begun. Until now, they have represented an absolute market gap in Z-gauge.

We therefore hope and are confident that this idea by Michael Koppensteiner will enjoy great popularity and can advance Era VI in Z gauge well. The manufacturer can be found on the internet at the following address: z-dream-collection.jimdosite.com.

Load upgrade from Aachen:

Gauge Z loader Josephine Küpper from Aachen (<https://spur-z-ladegut.de/>) is currently offering brass insulators (4 pieces per pack, item no. Z-ISO-W) to complement Märklin's 8620 transformer transport wagon. The use of these realistic looking parts is easy: cut off the plastic insulators with a sharp knife, drill a suitable hole in this place, glue it in - done!



The brass insulators by Z gauge loader Josephine Küpper are intended as exchange parts or replacements for the stock ones. Photo: Spur Z Ladegut Josephine Küpper

New products at AZL in August:

Offered by American Z Line in August are single A (63009-2) and combined A-B (63009-1) units of the EMD F7 in Wabash livery. These locomotives are now offered individually in the Santa Fe war bonnet livery as A (63001-4 to -10) and B unit (63001-X).

The longer EMD E7 is in the colours of the Illinois Central. An A-B set (64616-1) and a single A-unit (64616-2) are also available.



The EMD E7 in Illinois Central colours (item no. 64616-2; photo left) and the R-70-20 refrigerator car as "Cold Train" (914807-1; photo right) are two of the current AZL deliveries. Photos: AZL / Ztrack

The R-70-20 refrigerator cars are now available with NRDX adjuster labelling as a sky blue painted "Cold Train" and are offered individually (914807-1) and in packs of two (914837-1) and four (904807-1). The black 8,000-gallon tank cars for the Union Oil Company of California (915009-1 / 915039-1 / 905009-1) are also offered in the same configuration.

Manufacturer photos of the current deliveries can be found under <https://www.americanzline.com>.

Deliveries from FR Freudenreich Feinwerktechnik:

A total of 32 machines of the Ma series of elocomotives were built from 1953 onwards, and for a long time they were the workhorse of the Swedish railways SJ. FR Freudenreich Feinwerktechnik had announced this locomotive as a main novelty in 2021 and has now delivered it. No more models are available from the manufacturer.



The pantograph colour decides whether the Ma 828 of the SJ (item no. 46.136.01), produced by FR Freudenreich Feinwerktechnik, is in its last operating condition, or as a museum locomotive. This is the museum version. Photo: Thomas Tegtmeyer

It was available in analogue or equipped with a DH05C decoder, which is connected to six soldering points. Thanks to its weight of about 60 grams, the model also pulls quite a lot of wagons.

The brown SJ version Ma 828 (item no. 46.136.01) was produced as the current museum locomotive with grey painted pantographs and red scissor pantographs in the last operating condition of the SJ.

Fewer new products at Model Railway Decals Andreas Nothafft:

At the moment there are rather few new products to report at Modellbahndecals Andreas Nothafft (<https://www.modellbahndecals.de>), but again they have tried to serve all eras as far as possible. If you would like to order them for Z gauge, please contact the manufacturer and have them scaled to fit:

- detailed coat of arms of the K.Bay.Sts.B. (Art.-Nr. 8184),
- Complete lettering for a refrigerated van Berlin of the DRG (6621)
- Complete lettering for a sulphuric acid tank wagon of the Winkel chemical factory in the Rheingau region of Germany. (6622)
- Complete lettering for an oil tank car of the Hagen-Eck main depot for operating materials, used by DB (6623), and

- Lok-Komplettbeschriftung für eine der beiden Metropolitan-101 der DB-AG (6624).

We have left out of this list labelling that can no longer be reproduced decipherably on a scale of 1:220. Regarding the sulphuric acid tank car, we add that there are no suitable Z-gauge models of it, ex-works, but they could certainly be converted to Märklin running gear with the help of 3D printing.

Regarding the fictitious inscription of a Corona vaccine wagon, Andreas Nothhaft has added the information that from now on 1 euro per item from the sales price will go to the Railway Foundation, which we also like to see supported as an important partner of our activities.

New advertising car from the 1zu220-Shop:

The 1zu220-Shop (<https://www.1zu220-shop.de>) continues its Westheimer beer wagon series in prototypical design. Again, it is not only intended to appeal to collectors, which is why another wagon was chosen that could have run like this on the Bundesbahn. As we think it is definitely a success.



For its new advertising wagon (item no. 8600.176), the 1zu220 shop has resorted to a different wagon shape, as the beer brand does not seem to fit into the range of previous examples and this model has a historical template in the H0 size range.

Since the series on the current Westheimer products has been completed and he should therefore also consciously maintain some distance from them, the special model produced by Märklin pays tribute to a historical brand.

The choice fell on an ordinary refrigerator car, which was designed after a now 40-year-old H0 template and whose model anniversary is in focus. It bears the logo "Hirsch-Bräu Pilsener" (art. no. 8600.176), and is available now, while stocks last, exclusively from the 1zu220-Shop.

More new products from NoBa-Modelle:

As already announced, the Jaguar 860 maize choppers have now arrived in the range, available as a residual print model (art. no. 6620R) and as a finished model (6620RF). Other models from the agricultural sector have also been added to the range. These are the tractor trailer rear tipper (6602R / 6602RF) and the Unimog U 1450 (6210 R / 6210RF). Not far from the field you will find the 5 wild boars (13001R) at the edge of the forest.



Again, rows of new products at NoBa-Modelle: The Jaguar 860 maize chopper (item no. 6620RF; photo above) has now been delivered, to its right you can also see the five wild boars (13001R). Among the commercial vehicles, the Unimog U 1450 (6210RF; photo below left), and various versions of the Mercedes-Benz LP 608 (6382RF to 6385RF; photo below right) have been added. Photos: NoBa-Modelle

One of the main innovations is the cubic Mercedes-Benz LP 608 truck, which is available in a wide variety of versions: Flatbed with 4,200 mm (6381R) and 3,200 wheelbase (6383R / 6383RF), furniture van with 3,200 mm wheelbase (6382R / 6382RF), flatbed/tarp (6384R / 6384RF), flatbed with open tarp (6385R / 6385RF) and as a tow truck (6386R / 6386RF).

A Finn dinghy on a slip car (10485R) is also available for boating enthusiasts, while motorcyclists will be delighted with the Harley Davidson Captain America (6963R), Billy Bike (6962R) and Easy Rider (6961R). We would also like to inform readers who are not familiar with track gauges that a programme for the scale 1:160 is currently being developed.

This brings us to the area of construction, which will also be enriched with some new products: MAN tractor unit 6 x 4 (6016R / 6016RF), available also with semi-trailer (6923F) and semi-trailer and excavator (6924F). The three-axle drop deck semitrailer (6070R) is also available separately, as is the excavator



A complete package is the ready-made model consisting of MAN 6 x 4 tractor unit with semi-trailer and excavator (6924F). Photo: NoBa-Modelle

with crawler chassis (6509R / 6509RF). This section is completed by a rubble skip (2010R), the prototype of which has a volume of 7 m³.

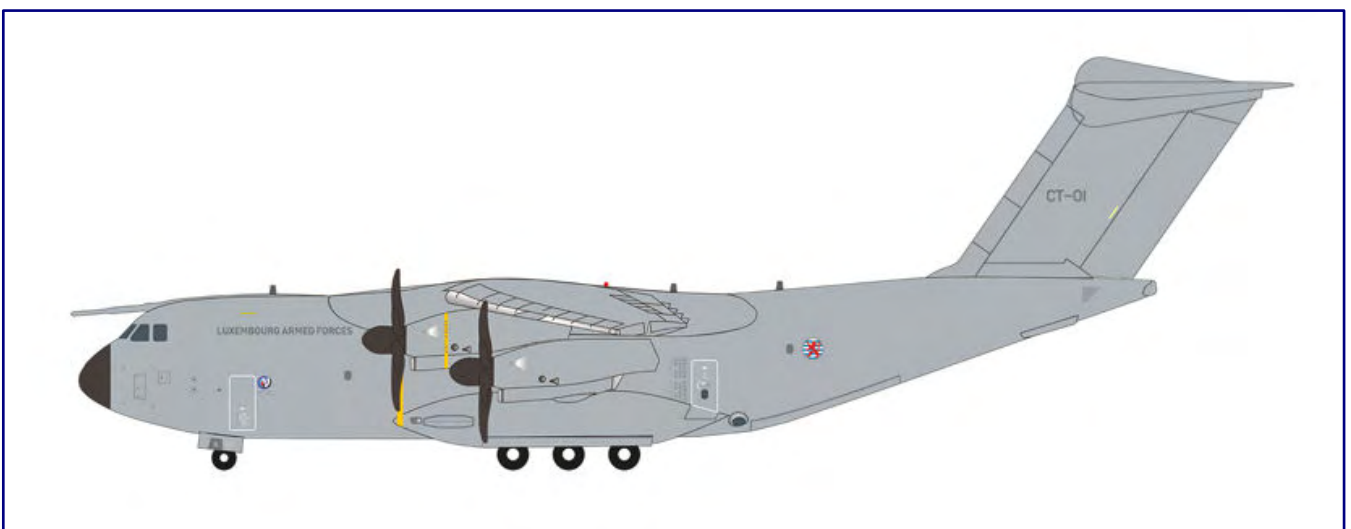
The GT4 articulated railcar is now also available as a rolling tram (5709R). Photos of all new products and ordering options can be found at <https://www.noba-modelle.de>.

New Christmas campaign follows:

Märklin has announced to its dealers that it will again launch a Christmas campaign in 2021. Details are not yet known, but it is to run under the name "Finally, be small again." As in previous years, the aim is to introduce newcomers and those returning to the world's most beautiful hobby during the Christmas season with a coordinated and clever campaign.

New aircraft models for the end of the year:

From the Herpa Wings and Snapfit new products in 1:200 scale, we have again made a selection of those models that are or were found at European airports and do not exceed a length of 30 cm. Further new products for the months of November and December 2021 can be found on the manufacturer's pages (<https://www.herpa.de>).



The Airbus A400M "Atlas" was recently in the media again because of evacuation flights. At Herpa it will soon appear as an aircraft of the Luxembourg Air Force (art. no. 571722). Photo: Herpa

The only military model we would like to mention is the Transall successor Airbus A400M “Atlas” in the version of the Luxembourg Air Force (art. no. 571722). The list of civilian aircraft, on the other hand, is somewhat longer:

- Condor Boeing 727-200 (571647),
- Olympic Air Bombardier Q400 (571661),
- Interflug Ilyushin IL-62M (571708),
- Rada Airlines Ilyushin IL-62MF (571715),
- Pan American World Airways Douglas DC-4 (571739), and
- KLM Lockheed L-1049G “Super Constellation” (571746).



Among the civil aircraft, this holiday plane is very interesting: the livery of the Boeing 727-200 (571647) from Condor's inventory followed the original livery proposal for the then parent company Lufthansa. Photo: Herpa

In the Snapfit segment, only the Hungarian Air Force Airbus A319 (613422) falls under the above criteria.

Märklin cooperation with collection insurer:

The model railway manufacturer Märklin and Artima, the art and collections insurance of Mannheimer Versicherung AG, have entered into a cooperation. Since 2021, model railways have also been included in the art and collections sector of this insurer.

Anyone who feels addressed by this may find a tailor-made solution here beyond the usually inadequate cover provided by household contents insurance. According to a press release, Artima developed its own set of conditions for the needs of the target group of model railway collectors and landscape builders in regular exchange with experts from Märklin.

It is an all-risk insurance for locomotives, wagons and individually built model landscapes and constructions. The few exclusions are explicitly listed in the conditions. Electrical and digital accessories are covered, as are the reconstruction costs for the model layout landscaping, after a claim. More favourable special conditions apply for members of the Märklin customer clubs.

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German Magazine for Z Gauge



50 Years of Intercity

White Shark from NoBa-Modelle
Archistories in Portrait

Introduction

Dear Readers,

We are still struggling to get back on schedule. After months of improvising, during which we had to put many projects on hold, this is no easy task.

And, so, I expect that we will still be busy until the end of 2021 getting back on schedule, even in the many areas that are not visible to the outside world.

One instance of what I am describing here is the class 403/404 express train from NoBa-Modelle: I had long hoped for a Märklin model after the one appeared in their H0 programme.

Honestly, I have to admit that with three different bodies and only two possible prototype variants, it has no chance of recouping its development and manufacturing costs.

All the more I was pleased about the small series model that uses chassis parts from Rokuhan-Shortys. As this train had been specially developed for use in Intercity traffic, it fitted better than any other model to the anniversary we would like to commemorate today: 50 years of Intercity in Germany.

Unfortunately, we were not able to complete the train in time for the publication of this edition. With a heavy heart, it was therefore only enough for part 1, and we will still catch up with the final result. Nevertheless, it will probably be able to convey our excitement.

But we have also showcased the anniversary celebrant in this way: We present the long history of the IC and its concept in more detail than hopefully any other professional journal. Of course, we cannot go into as much depth as a book, but we have covered all aspects of this long-distance concept and its pioneering work in sufficient detail.

It is also appreciated in the miniature, because we have looked at typical train arrangements and would like to show that the Intercity can be very versatile. If you are interested, you will also find useful ideas in our DVD tip. The book presented is dedicated to another anniversary: Faller is 75 years old this year.

But the variety of topics is far from exhausted: Markus Gaa reports on a private model railway meeting that is something very special after so many months. In an interview, Frank Drees wants to introduce us to the philosophy and aspiration of his Archistories brand.

Last, but not least, as you are used to, we have compiled the latest news from the world of model railways and Z gauge, including new products that have been delivered. The scope is a bit smaller than in the last months, but no less interesting. Have fun reading!

Sin-Z-erely,

Holger Späing



Holger Späing
Editor-in-chief

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We thank Henning Folz, Andreas Petkelis, Peter Rappold, Oliver Strüber and Eisenbahnstiftung for their photo contributions.

Date of publication of the German language version of this issue: 2 October 2021

Cover photo:

In the early days of the Intercity, 103 107-9 hauled one of the new trains that were to determine the future of the Bundesbahn. In the absence of sufficient TEE-standard carriages, we still find blue compartment carriages from the F-train era, which has just come to an end, in her train.

Series 403/404 from NoBa-Modelle (part 1)

Ancestor of German High-Speed Trains

When the manufacturer NoBa-Modelle came out with a model of the class 403/404 Intercity multiple unit, our hearts immediately beat faster. This vehicle with its distinctive and futuristic looking front design had been on our wish list for a long time. And since we know that we are not alone in this, it seems apt to mark the 50 year IC anniversary with an article about this model.

Few trains have caused as much controversy as the former class 403/404 express train known as the “Donald Duck” or “Duck’s Bill”. Despite being limited to three prototypes and never having entered regular production, the train has always remained well known.

Years ago, Märklin surprised its H0 gauge customers with a new model that appeared in both prototype versions. For the smaller Z scale market, on the other hand, it obviously seemed too risky to invest into developing this model.



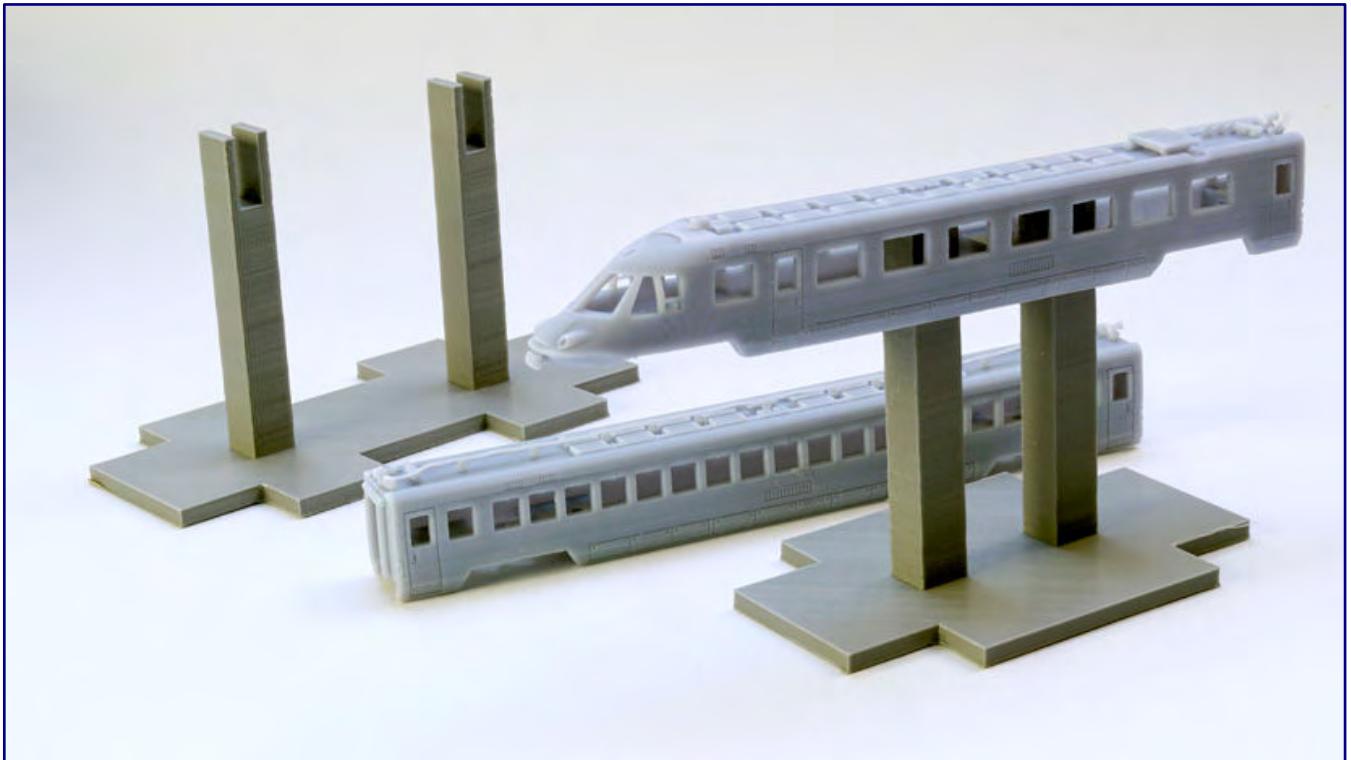
Class 403 / 404 multiple unit in intercity service, photographed in 1975 in the track area of Nuremberg main station. Photo: Jan Oosterhuis (public domain)

After all, it would have required three different car bodies, plus window inserts and many other parts. The prospect of amortising these costs was probably hopeless, in view of the small number of variants.

As a consequence, this multiple unit whose design concept divides opinions amongst railway enthusiasts, but which also has a loyal fan community, became a case for small series production. The company NoBa-Modelle discovered a way to bring it to life using a 3D printing process and powered chassis from Rokuhan’s Z Shorty range.

The models are available both in a ready-made version (art. no. 5207RF) and as a more affordable kit (5207R), with the latter consisting of body shells which still need to be painted, labelled and motorised. We have resorted to the kit option, because this train in its original IC design makes for a nice and exciting modelling challenge lasting several evenings.

At the same time, it should be noted that the “ET 403” is not a suitable object to start your first attempts at building your own rolling stock. Although it does not require different components to be adapted and joined together, it is quite demanding in terms of all the paint work to be done.



Two unfinished shells take a seat on NoBa-Modelle's painting stands for a trial run to find out which one is better placed on the short and long versions of this tool.

The reason for this is the slanted and curved surfaces of the end carriages, which make masking difficult. This challenge, which we mastered after thorough preparation and research, will be a focus of this first part.

Originally, it was planned to present the entire project in this article, and to add a second short report on the Lufthansa Airport Express version of the ET 403 at a later date. Instead, we will now divide this building report into two parts of similar length, but with still some uncertainty as to when the second part will be ready for publication.

While the sequel will be dedicated to applying the lettering, inserting glass panes and motorising the train and should also include the promised look at another version of the train, today's article will look at what additional parts are still needed and preparing the 3D printed shells. Special attention will be given to the preparations for painting the trains!

Off to work we go

NoBa-Modelle prints the multiple unit exclusively in resin. This makes sense because of the better surface quality and given that a PLA print might not yield a sufficiently appealing exterior. Nevertheless, we carefully inspect all four shells for any imperfections before we start, because corrections will be practically impossible later on.

At the same time, we procure the necessary additional parts that will be needed to make our multiple unit run. They are all available from Rokuhan via the Noch sales channel, the article numbers used by Rokuhan are indicated below (note that they are not identical to the article numbers used by Noch):

2 x	SA001-1	Z Shorty Motor Chassis (Normal Type)
3 x	SA006-1	Z Shorty Container Car (blue)
2 x	A048	Retracting pantographs (from the class 181 ² model)



In addition to the train kit, we need Rokuhan parts, available from Noch. A total of five Z Shorty chassis, including two motorised ones, provide the necessary bogies and drive.

All lettering, covers, coupling rods and ballast weights for the individual wagons are included in the kit, including the necessary fastening material. We also purchased Märklin-compatible type 907 couplings from Micro-Trains (002 04 010).

Right from the first inspection of the shells and components, we were unconvinced by the proposed fixed connection of the four train sections by screwed couplings, which cannot be separated during operation. This makes for stable connections, but would very much complicate the re-railing of the complete train.

If these couplings could be replaced by detachable connections, the vehicle would be more flexible in operation and could also be run as a three-car train or with only two end cars. In principle, nothing would stand in the way of adding a fifth carriage. So, it is worth a try!



NoBa-Modelle includes other parts in its kit that are needed to complete the train: Base plates with weight inserts (above), coupling rods (left), and the necessary fastening material in the form of different screws.

After checking for completeness, one would normally start with correcting small imperfections of the printed shells. In our case, however, we are very satisfied with all surfaces. The high resolution of the resin prints and certainly also the well, thought-out alignment in the printer, make all surfaces look surprisingly smooth. In fact, the individual print layers are hardly visible without a magnifying glass.

So, it's straight to the choice of a suitable primer. Since the basic colour of the train is RAL 7032 pebble grey, it has to be a light-coloured version such as white or light grey. We decide on "White Primer" (SNR-401), a water-based PU primer from the US manufacturer Badger.

Before bringing the air-brush into action, we clean the surfaces of the four shells. Even though they have already been treated to an isopropanol bath by NoBa models, we subject them to this procedure again ourselves.

The aim is to remove all fingerprints from surfaces in order to prevent paint adhesion problems later on. Ultimately, however, this means that the parts are no longer touched with unprotected fingers after the bath! They are ready for painting as soon as they are completely dry.

Painting challenge

We had already dropped a hint at the beginning that the paint job is a main challenge with this project. After applying the base coat, many more painting steps follow, later also with the help of a fine hair brush.

But, first, the window strips and the trim strips above and below them will have to be spray-painted. This will be followed by the lower skirts. On the end cars there is always the problem that the paint edges do not run over straight surfaces.



After cleaning the shells with alcohol, the paint process starts with a base coat of white PUR primer (Badger). Only then does the work of masking off the individual surfaces for the top coat begin.

Since they are curved, they are very difficult to mask off and are not always safely protected from paint mists. Whilst anticipating that some retouching might become necessary, things did go wrong in areas where we did not expect it: Nobody is immune to making mistakes, as will become clear.

Nevertheless, the process gives us great pleasure and is a welcome challenge. And as we all know: Facing new challenges helps us to improve our skills. At the beginning of the process, we scout the market to see which masking tapes are offered by different manufacturers.

In the past, we have had to pay a lot of dues here: sometimes paint would bleed underneath the masking tape, and on other occasions masking tapes would stick too strongly to the surface and tear off the layer of paint to which it was attached.

Eventually, we ended up using masking tapes from Tamiya, which we could always rely on. They are available in three widths, which unfortunately only help us with the larger surfaces in this case. However, we also needed narrower versions that are as flexible as possible, and can also be guided around curves without kinks.

So, we came across Vallejo products that seemed worth a try. Faller then unexpectedly pointed out to us that they now also have their own products in their range that offer exactly this flexibility. So, we gave them a try!

Faller's flexible masking tape (170533) contains two rolls of 18 metres each. The widths of the tapes are 2 and 3 mm. The material they are made of is similar to PVC insulating tape when applied and is probably related to it.

If we have to go around bends with it, it is easy to pull and adapt the tape to the desired shape. It behaves just as we want it to: it doesn't want to return to its original state, it doesn't wrinkle, and it sticks excellently, even to curved surfaces, without damaging the paint underneath.



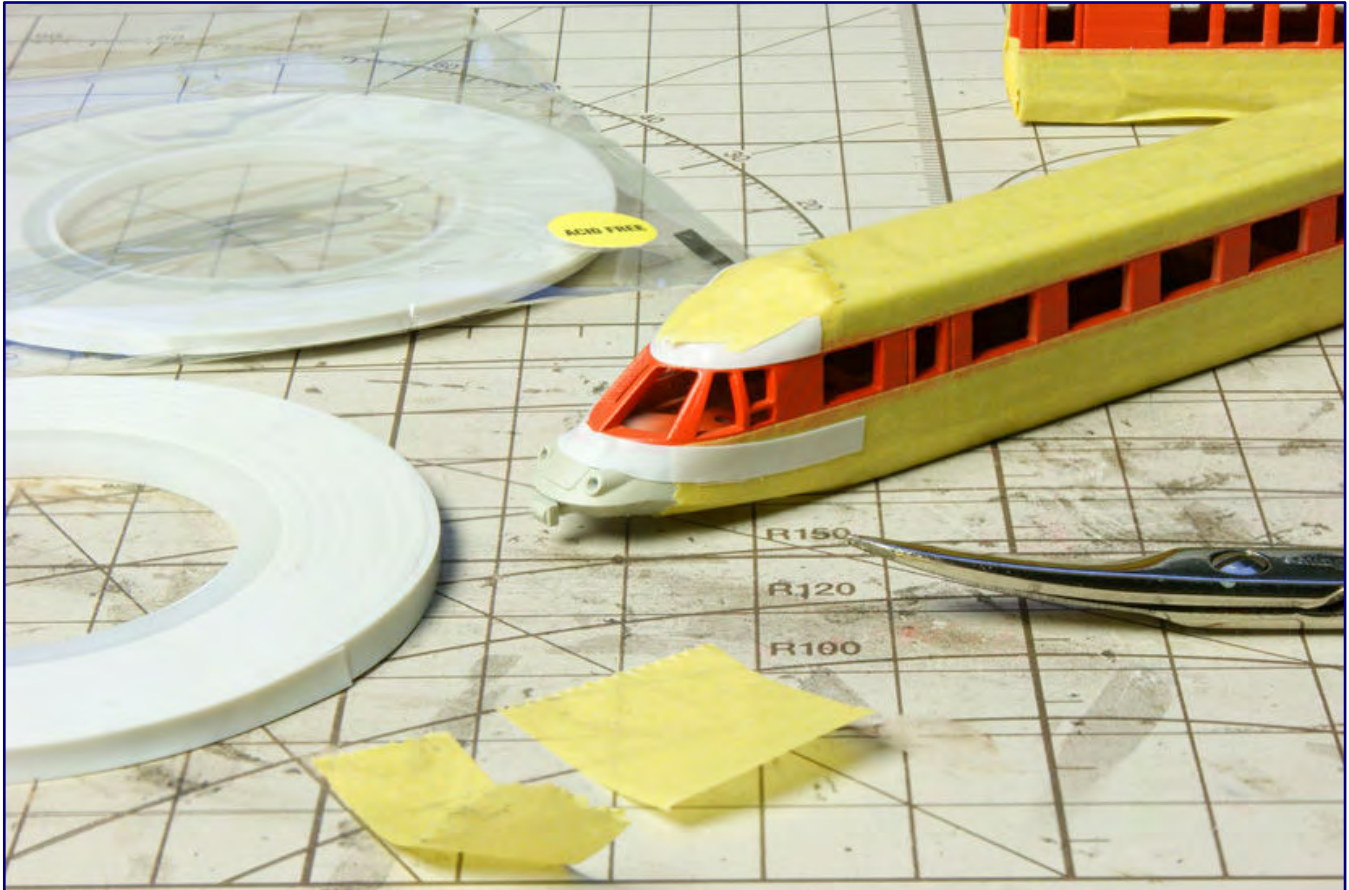
The base colour and the orange of the two decorative stripes have been applied. This has already required a first masking process. The next one is just about to start. The products from Tamiya (left) and Faller (far front and right) are used again.

Of course, caution is advised: steps against paint infiltration must be taken here, as well. Due to a lack of experience with this product, we initially assumed that this risk here was perhaps even higher than with the paper-based tapes from Vallejo and Tamiya due to the material.

We also use our favourite product from Tamiya, which has proven its worth over the years: available in a practical dispenser holding an 18 metre roll, we use the 10 mm (300087031) and 18 mm (300087032) versions to protect the adjacent surfaces from paint mist.

In areas without any curved surfaces, we only use the Tamiya masking tape. In transition areas, such as the lower part of the front skirts, a liquid masking product is needed, in addition. We use one that is distributed by Faller and available in many modelling and model railway shops: masking liquid by the English manufacturer Humbrol (489217), in a 28 ml jar.

Where needed, it is carefully applied with the fine hair brush. As it dries quickly, one has to quickly remove the brush from the object after application. Otherwise the masking liquid might stick to the brush and come off again. The milky-purple liquid turns transparent when dry. Once this has happened, it can be painted. Afterwards, the rubber film can be carefully lifted from the surface with a toothpick tip and peeled off.



This image illustrates how the white Faller masking tape follows the curves of the train's front and tightly sticks to the surface. The other protected surfaces are covered with yellow Tamiya masking tape.

We would also like to describe how surfaces are protected from paint bleeding underneath the masking tape. There are several approaches to this:

1. The spray pistol is guided in way that its jet does not hit the adhesive edge from above and press the aerosol underneath. Spray lightly from below over the edge or exactly at right angles to the object. However, it is not always possible to avoid infiltration, so here are two more approaches.
2. Apply a first thin coat of paint to the adhesive edge in exactly the same shade as the masked surface underneath. Although bleeding can now occur, it does not disturb the overall picture. The drying paint seals the adhesive edge and is then sprayed over in the target colour. However, if the paint is not sufficiently dry, an unintentional wet-on-wet mixing may occur in the following step!
3. The same procedure as in point 2 can also be carried out with a matt clear coat. It should be matt because then it dries considerably faster than paints with a higher gloss level. The risk of colour distortion due to unintentional wet mixing is much lower here. Later differences in the gloss levels of the model are evened out by the final protective varnish.

It is always important to bear in mind, especially with regard to point 3, that decals adhere less well to a matt base! The matt clear coat (we use the deep matt acrylic clear coat (83211) from Bergswerk) should not cover large areas, because otherwise they have to be treated again before applying inscriptions.

The all-important procedure of preparing the paint job for this train kit is now described in detail. If you want to rebuild our project, you should take this to heart to ensure success. There is a lot of masking to be done over several steps, given that window strips, decorative strips on top and bottom as well as the apron cannot be applied cleanly in one go and without masking.

We strongly recommend to refrain from painting with a handheld brush. It is simply not possible to achieve a sufficiently precise and even result with a brush and the expected sub-optimal outcome would not do justice to this beautiful train.

The right mixture makes the difference

Now it's finally time to apply the colours. Our topcoats are again from Oesling Modellbau and have a silk matt gloss level. What is special this time, however, is that with the exception of the base and apron colours, none of the available paints are directly available in the right RAL colour codes.

The Bundesbahn had put a lot of thought into the livery of its flagship train. Alternative possible paint schemes were assessed on models, and finally even a 1:1 model was built.

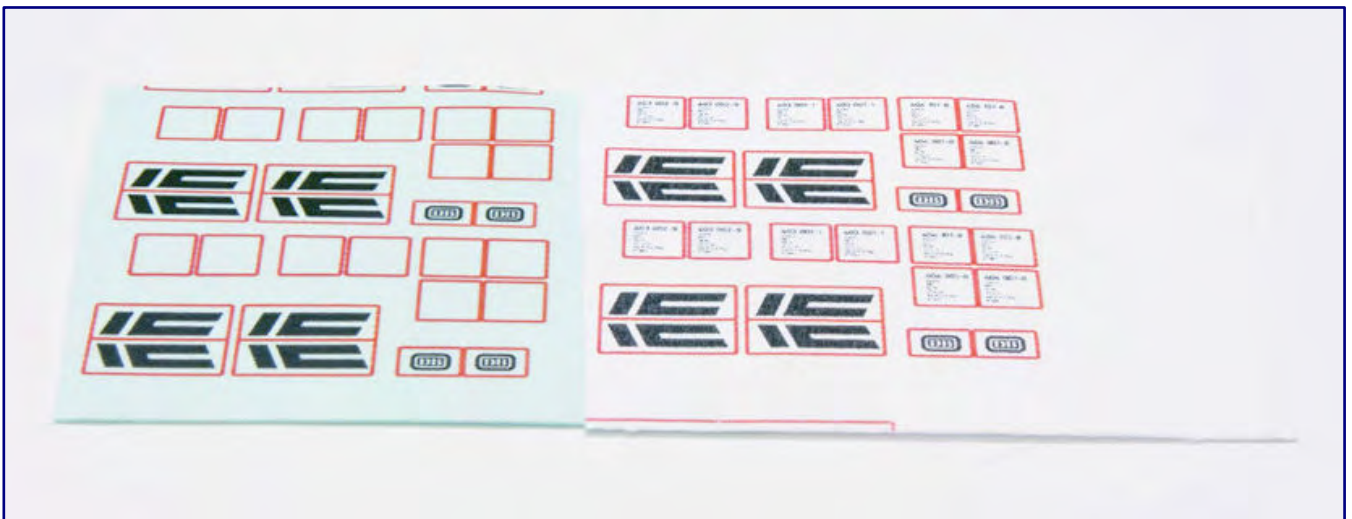


For the areas along the windows, a mix of three different RAL shades is required to achieve the desired result, as noted in the article.

The intention was that the paint to be chosen should use the 1970s pop culture colour spectrum, emphasise the unique lines and shapes of the train and also harmonise particularly well with the tinted and gold-tinted windows in the area of the window band. Extensive colour tests by the BZA Munich (the Bundesbahn Central Office), on a TEE wagon were the result.

For the window band, it was decided to use a mixture of 50 parts RAL 8022 black-brown and 1 part RAL 2002 blood orange. This harmonised particularly well with the colour reflections of the windows and made the many windows of different widths and with deviating distances merge visually into one long window band.

Once moving, this created the illusion of the train running at a higher speed than was actually the case and emphasised the character as a multiple unit. In addition, decorative stripes above and below the window band gave the train a train slimmer appearance.



Some errors still need to be corrected: For example, paint mist deposited on parts of an end car (above; left). NoBa-Modelle also includes the necessary decals with its kit (bottom). It is a good idea to mark the cutting edges in colour and also to include a paper printout (right in the photo) that makes the white prints traceable with black ink.

For the stripes, the BZA Munich decided on a mixture of 50 parts each of RAL 2004 pure orange and RAL 2002 blood orange. RAL 7032 pebble grey had already been chosen as the basic colour, which had also been used for the side walls of the Bundesbahn's pop colour passenger carriages. The apron was painted in RAL 7021 black grey.

All of the mixed or pure paints came from Oesling Modellbau. For the silk matt finishes as we use them, their article number is always composed of the colour type and gloss level code 8100 and the RAL number, so for example 81002002 for the blood orange from the pop colour spectrum.

The individual painting steps can now follow one after another, each with a few days of drying time in between. Start with the basic colour pebble grey, then spray on the decorative stripes, first as a wide band and finally masked off so that the window band can be darkened. The skirt follows at the end and requires the most masking fluid in the lower front area.

We are not immune to mistakes either, which is why some corrections were inevitable. On one powered car we did not mask correctly and noticed the mishap too late. A mist of paint from the window band got to the decorative strip and part of the roof of the coach.



Interim status as of now: All four wagons have received their basic colour scheme. Brush work on brake resistors, electrical cables and the rubber beads will follow.

Likewise, there are still a few areas where paint has bled under the masking tape and that need to be fixed. And the width of the decorative strips has not turned out to be a complete success everywhere. It is and remains an exciting project, which gives sufficient pleasure and motivates us to take these steps.

With this, we have already completed our description of what we have achieved at this stage of the build. In the continuation of this article, we will work with brushes in order to paint and highlight the brake resistors on the roofs, the roof insulators and cables as well as the rubber bulges. Markings and a final varnish of clear coat will finish the paint work as planned.

Modelling the windows will be an interesting challenge, in particular as we aim to imitate as closely as possible their original tint. And once that is done, the train will have to learn to run and get its drives installed.



At the end of the second part, our train will hopefully look like the one that the manufacturer NoBa-Modelle presents on its own web pages. Photo: NoBa-Modelle

This brings us to the point of providing more detailed information about the prototype, which will surely grab your attention as a reader and put this train kit on your wish list!

Manufacturer of the base model:

<http://www.moba-modelle.de>

Sources for the parts required:

<http://www.rokuhan.de>

<https://www.micro-trains.com>

Masking tapes, paints and primers:

<https://www.badgerairbrush.com>

<https://www.bergswerk.de>

<https://www.faller>

<http://www.oesling-modellbau.com>

<https://www.tamiya.de>

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Intercity trains suitable for the layout **Brevity is the Spice of Life**

The class 103 is one of the most popular German electric locomotives in all gauges. This is certainly no surprise, as it was very much on the spot for years as the flagship locomotive of the German Federal Railways, and with it the Intercity. We have had a look around to find some trains that are worth seeing and for the most part also short trains that cut a good figure on Z-gauge layouts.

An Intercity is a beautiful eye-catcher on the model railway and can so far appear in four liveries from three colour concepts: in the starting concept based on the TEE colour scheme with the basic colours purple and ocean blue, in product colours, and in the long-distance colour scheme of the Deutsche Bahn AG. The transitional variant with a completely traffic-red window ribbon, which was later only used for the car train, should not be forgotten here.



The InterCity, at the beginning of its era in red-ivory-coloured livery, is celebrating its 50th anniversary. We are therefore looking at suitable train compositions for the model.

During German Federal Railways times, we remember Intercity trains with long train lengths since the introduction of 2nd class, but there were also very short versions, albeit quite few in number. The train lengths and class ratios depended on the IC line and its basic capacity utilisation as well as the demand on individual weekdays and in the various frequencies.

The following 14 variants were common for 150 trains at DB in 1979/80 (source: Vom Fernschnellzug zum Intercity; Freiburg 1983):

<u>Variant</u>	<u>1st Class</u>	<u>Train restaurant</u>	<u>2nd Class</u>	<u>Total</u>	<u>Number of trains</u>
1	2 cars	1 car	2 cars	5 cars	4
2	2 cars	1 car	3 cars	6 cars	2

3	2 cars	1 car	4 cars	7 cars	6
4	2 cars	1 car	5 cars	8 cars	11
5	2 cars	1 car	6 cars	9 cars	14
6	2 cars	1 car	7 cars	10 cars	3
7	2 cars	1 car	8 cars	11 cars	5
8	3 cars	1 car	4 cars	8 cars	1
9	3 cars	1 car	5 cars	9 cars	13
10	3 cars	1 car	6 cars	10 cars	33
11	3 cars	1 car	7 cars	11 cars	29
12	4 cars	1 car	5 cars	10 cars	2
13	4 cars	1 car	6 cars	11 cars	3
14	4 cars	1 car	7 cars	12 cars	24

The advantages of Z gauge are obvious here, because scale-long trains without shortened wagons should not be possible in any other scale. But even in 1:220 scale, this requires a larger layout with a long parade track.

The locomotive should ideally be equipped with traction tyres or be weighted down a bit, the latest edition of the class 103 from Märklin has been moulded in metal-filled plastic, and has, thus, gained quite a bit in tare weight.

If Intercitys based on the IC 71 concept are chosen, they are more convenient for model railroaders, because due to the lack of 2nd class they were usually much shorter. The situation is similar with many ICs of the present day, which often run on former Interregio lines and frequently also comprise only six to eight passenger coaches including the bistro coach, which also accommodates part of the 1st class.



The appearance of an InterCity was almost always more diverse than many remember. It was not limited to the use of the class 103 alone. Photo: Noch

Hereafter, we would like to suggest some train compositions that are close to the prototype and can be represented with Z-gauge models. The focus is on short sets, but all model railway eras in which IC traffic can be represented are to be covered.

The locomotives used

Not all locomotives that were used in Intercity traffic were or are also available in Z gauge. Therefore, we cannot list individual combinations here, but the following article about prototype train compositions should provide suggestions, in case suitable models are added in the future.

The standard locomotive of the Intercitys for many years was the class 103, of which the DB had procured 145 series machines. Only in 1970/71 were they used in front of F-trains, for which they had actually been developed and ordered.

From 26 September 1971 onwards, the newly introduced Intercity was its most important area of operation, which no other electric locomotive series was to compete with. The class 103 can be used in front of ICs both with scissors pantographs, with which it was delivered, and with the single-arm pantographs, which it received later, and for which it is better remembered.



The class 103 became the main carrier of IC traffic, and this is the way it should be in the model. Individual locomotives, such as 103 245-7 here on 7 June 2009 in München (Munich) Central Station, remained in the fleet even after the turn of the millennium and were still allowed to help out in high-volume traffic.

In 1979, another series entered the service of the German Federal Railways: the first three-phase current electric locomotive in the high-performance range. The five machines listed as class 120⁰ were shown at exhibitions and extensively tested.

Above all the 120 005-4, which had a deeper front kink and does not exist as a Märklin model, was used early on in Intercity services. Her sisters followed later or replaced her on a case-by-case basis. This

means that Märklin models 8853 and 88527 can also be used well in front of such trains, particularly as they also look timelessly elegant in their original livery.

However, it should be considered that these locomotives are only suitable for two-class IC trains, because the IC-71 concept was before they entered service. Of course, this also applies to the series locomotives of class 120¹, which Märklin has issued several times in orient red, traffic red and various special designs on the same model basis.



Just one example of splashes of colour in front of IC trains is the elegant 120 159-9 in Märklin livery, which has become known as the “mourning locomotive”, also photographed on 7 June 2009 in Munich Central Station. It is also the penultimate series locomotive of its class. Of course, it was also offered as a model, as its markings already reveal.

Class 111 locomotives were regularly used in front of trains according to the IC-79 concept, sometimes even in double traction in front of heavy trains. This was due to the sudden lack of fast electric locomotives when the frequency was increased.

The ocean blue- and ivory-coloured locomotives were primarily used, but there is also evidence of urban railway livery in individual cases. Orient red and primarily traffic red examples are not appropriate, because at the time of the introduction of these colour schemes, the DB could already draw on the sixty series locomotives of class 120¹.

The 110 and 112 series were not in scheduled IC service. The 110, with a maximum permissible speed of 150 km/h, was too slow for this type of train. The 112, which was not inferior to the class 111 in this respect, was not scheduled for IC service either, although it even carried the matching colour scheme.



120 112-8, probably one of the most beautiful advertising locomotives of the class 120, also came across the country ahead of the Intercity. However, when it was waiting in front of the camera lens in the warm evening light at Munich Central Station on 24 April 2014, it was in fast local service in the direction of Nuremberg.

This brings us to 218 217-8, the only locomotive of its class with a purple/ivory livery. Its design was only a livery trial in 1974, which was also carried out in parallel on 218 218-6 with the ocean blue and ivory combination chosen a short time later.

218 217-8 was not at home in a depot that had to harness IC trains, which is why the use of this diesel locomotive in front of the type of train being discussed here is almost unlikely. Nevertheless, other class 218 locomotives were and still are in Intercity service.

Mostly, they operated in double traction, which was not necessary for traction reasons, but to supply the air conditioning systems. Only remarkably short ICs, of which there are more today than in the past, are hauled exclusively by one locomotive. Due to the long IC service life, all colour concepts were represented on this class.

Many people may find the former everyday locomotive of the local traffic in front of such high-quality trains surprising. In fact, however, it provided regular services, starting from Wiesbaden. When the DB tried to speed up traffic and removed the Hessian capital from the IC concept, a replacement had to be found due to political pressure.

These were the Wiesbaden-Citys consisting of only two coaches, one each 1st and 2nd class, in the direction of Frankfurt (Main) and later also to Mainz, for which they were sufficient in terms of performance and maximum speed. At least until now, a suitable version of this locomotive is missing in the Märklin programme.

However, it becomes more critical with the train cars, which as “exotics” are also not to be expected to be provided. We have nevertheless mentioned this train because in the early days it was still necessary to fall back on a Bpmz and an Apmz. In this version, the Wiesbaden-City was equipped with a locomotive at each end due to the lack of a control wagon, certainly a very interesting model variant.

We do not want to leave the field to Märklin alone though. There is another Intercity locomotive that is not in Göppingen's programme. Here, Rokuhan has filled a gap in cooperation with Noch.



The class 218 has always been a regular locomotive in InterCity traffic and, contrary to all planning, its era has still not come to an end. In order to supply the carriages' air-conditioning systems, it can regularly be seen in double traction in front of this type of train. Model railroaders should take this into account, if possible, as shown in this photo.

We are talking about the class 181² dual-system locomotive. Throughout its entire service life, it was mainly used in cross-border traffic, but also sometimes pulled trains in domestic traffic.

It could also be seen in front of Intercitys, of which there were even cross-border versions. The IC did not leave this field of activity to the TEE and later EC alone! The class 181² has seen four colour schemes in the course of its more than 40 years of service and in all of them it was harnessed to the IC. That is why we can call it a typical Intercity locomotive.

The last and most important Intercity locomotive was and still is the class 101. It was procured in 1996 in exactly the same number as its predecessor 103 (145 units), and was supposed to replace it completely, which it finally did.

Märklin was a long time in the coming with the most important version in traffic red, but this too has been a thing of the past for some years now. In addition to the class 120, it was suitable and continues to be chosen for advertising designs because of its plain exterior walls. As with hardly any other model, the Göppingen company has also made use of this for Z gauge.



The class 101 succeeded the class 103 in the prototype from 1996. It also received many special liveries. One of the most recent and certainly the most attractive is the one by Märklin for the company's 160th anniversary, with which it travelled throughout Germany for a year and will remain so in the model for even longer.

Short Trains 1971 - 1979

Due to the two-hour interval and the fact that there was no 2nd car class, the trains were even shorter at the beginning of the Intercity era. They were not suitable for model railways on all lines in terms of the number of carriages carried, but the proportion of ICs that could be displayed on an average-sized model railway layout was at least higher.

Of particular interest to model railway enthusiasts is certainly the acute shortage of wagons that the Bundesbahn had to contend with. It led to the fact that for a longer period of time blue passenger coaches from the new construction programme also appeared in the IC, which previously ran in the F trains.

Thus, Intercitys with only three or four coaches, including a dining car, can also be formed with two steel-blue Aüm 203s. The fourth day coach can then be an IC large-capacity coach Apmz 121, for example. In individual cases, single A(ü)m 203s still joined the train after the initial phase, which is why such a splash of colour can also be welcome on the model railway.



A typical Intercity from the time of the start of operation still had to get along with compartment coaches as a substitute and also promises colourful model delights.

For what appears to be a typical Intercity based on the IC 71 concept, we looked at the IC 113 / 116 "Glückauf". In the 1971 winter timetable, it regularly consisted of seven coaches, which is not unusual among all trains. In later timetable years it became longer and would then be oversized for most layouts.

This train was not chosen by chance: In its scheduled carriages Avm (Am) / Avm / WRmz / Apm / Avm (Am) / Am / Am, two ordinary compartment cars were already included, two more frequently appeared in addition in the train composition, as the bracketed additions show.

Another example, which however requires compromises for the model implementation, is the IC 184 "Südwind" München (Munich) - Bremen (counter-train IC 185 "Nordwind" driven with class 601) from the summer timetable of 1972, a service of class 103: Avm / Avm / ARmz / Apm / Avm (Am).



Suggested train composition for IC 184 "Südwind", all Märklin material: 8854, 8724, 8724, dining cars from 81593, 8725 and compartment cars from 87400. Even in the prototype it sometimes happened that a dining car had TEE inscriptions. Here, it replaces a half dining car ARmz, which does not exist as a model.

Here, too, an air-conditioned 1st class compartment coach was replaced by an ordinary, non-air-conditioned passenger coach, which also has its appeal with this layout-suitable Intercity. However, the half dining car from this train is not available as a model.

This requires either self-building or a compromise, for example by substituting the bar wagon ARDümh 105 from the wagon pack 87285, which, however, does not reproduce the correct prototype shape either, but is based on the body of the only dining car.



This InterCity largely corresponds to our proposal for the IC 106 "Rheinpfeil": Here, the humpback dining car has already been exchanged for a WRmz 135 (Märklin 8727) and the car order deviates from the train composition plan. The first car behind the locomotive (Apmz 121) would have to be moved to the end of the train to correspond exactly to the original composition plan.

Comparable train compositions with this compromise would be IC 182 "Hermes" (Munich - Bremen) of the lightly frequented line 4 from the winter timetable 1973/74 (Avm / Avm / ARmz / Apm) and IC 165 "President" (Frankfurt/M. - Munich) from the summer timetable 1975 (Apm / ARmz / Avm / Avm).

Particularly interesting are regular trains with the observation coaches ADm 101. Here, we find the IC 106 "Rheinfeil" (Munich - Hannover) from the summer timetable 1972. Its coach order changed several times on the way because of discontinued through coaches and exchange with the TEE "Rheingold" in Duisburg Hbf.

We, therefore, consider the Munich - Dortmund section, which is suitable for model railways and can be reproduced as follows: Adm / WRm Avm / Avm / Apm. If you want to extend the section in the direction of Hannover, you can add two more Avm at the end of the train. It is also interesting to note that humpback dining cars were still found on this train until 1972, which contributed to the choice of this timetable section.

We close the era of single class IC trains with the class 403/404 express train, which NoBa models can contribute. Among other things, it ran as IC 182t "Hermes", for which a four-part basic configuration was provided. If you can run it longer, you can add an additional high-capacity coach, which could certainly be obtained from the small-series manufacturer without any problems. Photo evidence for this train exists from July 1975.

Digression: the DC trains

Chronologically, we would like to include the short episode of the DC trains as feeder and connecting trains of the Intercitys for stops during transport. It is not easy to reproduce them and to make them recognisable as DC, as they were not made of special railway carriages and can therefore only be distinguished in the model from an ordinary D-train by the train destination sign.

Therefore, we have collected some DCs on the basis of known photos, which at least appeared colourful and thus appealing. Equally attractive are those City-D-trains that often ran exclusively or predominantly with pop-coloured carriages.

It should also be mentioned here that in many trains the 1st class coach (Am) was exchanged for a mixed class coach (ABm) as a result of poor utilisation. If this is to be incorporated in the model, a suitable model from the earlier Heckl small series programme is required.

The trains of lines 11 to 18 (see prototype article) ran, like the later two-class ICs, at least frequently in block train composition. This is a small distinctive mark for this type of train, which we also want to use.

The prime example for our proposal is the DC 913 "Münsterland" (Emden - Frankfurt/M.) from the winter timetable 1973/74, usually formed exclusively from pop colour coaches and led by class 216: Am / Bm / Bm / Bm.



Two train composition proposals for DC trains of line 11: DC 913 "Emsland" from the winter timetable 1973/74 (picture above) and DC 910 "Emsland" from the winter 1974 (picture below).

Explanations of the type abbreviations from the train composition plans:

Symbol	Description	Model example
Am	Compartment coach 1 st class	Märklin 8710 (or 87400)
ABm	Compartment coach 1 st / 2 nd class	Heckl Kleinserien
Abn	Silberling 1 st / 2 nd class	Märklin 8717
Adm	Observation coach 1 class	Märklin 8728
ARmz	Half dining coach	Similar to Märklin 87285
Apm	Air-conditioned open-plan coach 1 st class	Märklin 8725
Avm	Air-conditioned compartment coach 1 st class	Märklin 8724
Bm	Compartment coach 2 nd class	Märklin 8711 (for DC), 8721 (DC & IC), from 87282 (IC) & from 87404 (for DC)
Bpmz	Air-conditioned open-plan coach 2 nd class	Märklin 8759 & from 87282
Byl	Middle entry coach 2 nd class	Märklin 87335 (or from the train set 81175)
WRbumz	Quick-Pick-Dining coach	so far only Z-Club International on Märklin basis 8727
WRmh	Dining coach	Märklin 8726 (or in 87281 or train pack 81593)
WRmz	Dining coach	Märklin 8727

One 2nd class coach shorter, but otherwise identical in appearance, was the DC 910 "Emsland" (Frankfurt/M. - Emden) in the same timetable section).

In winter 1974 it continued to run beyond Emden to Norddeich, but had exchanged its Am for an ABm.

It was now hauled by class 220 on the Ems land line, and class 110 on the electrified sections.

The train pair DC 931 / 932 "Diemel-land" (Dortmund - Bebra / Bebra - Dortmund) was even shorter in the winter timetable 1974/75: ABm / Bm / Bm. It ran via Unna - Soest -

Altenbeken - Kassel, and, due to the lack of an overhead catenary wire, was still diesel on the entire section at that time.

Less attractive, because partly formed from local transport material, were the DC trains of lines 21 to 28, which emerged from existing train connections. Here, we have chosen DC 891 "Oldenburg-City" (Oldenburg - Hannover) in the winter timetable 1974/75: Bm / ABm (or ABn) / Bm, hauled by class 220.



Two proposals for DC that emerged from already existing trains: DC 891 "Oldenburg-City" from the 1974/75 winter timetable (pictured above) and DC 892 "Weser-City" from the summer of 1974 (pictured below). Only the upper one is not immediately mistaken for a normal local train, as it is made up exclusively of D-train carriages and the pop colour carriages make it stand out somewhat from the crowd.

DC 892 "Weser-City" (Hannover - Cuxhaven) in summer 1974 was no more attractive: Bm / ABm (or ABn) + Bm. Here, we still simulated the failure of a coach and replaced the first Bm with a centre-entry coach (Byl).

Two-Class Intercity

The trains following the IC-79 concept are exciting, as the Intercitys have now regularly become much longer. Many of them will blow up even generously sized model railway layouts. The locomotives also reach their limits of tractive power.

A train of the configuration variant 2 explained at the beginning is made up of Märklin's car sets 87281 and 87282 together with class 103 (88543). At least two IC ran in this short version. Freely extended by

two 2nd class coaches, it would be possible to recreate a composition with a total of eleven train compositions.

In some cases, it would also be attractive to add the entertainment car WGmh 824 (87210) to the end of the 2nd class car block. This was certainly not an everyday occurrence, but corresponds to train compositions that are photographically documented.



Detail from a train formation proposal: In the two-class intercity, a lounge car is lined up directly behind the locomotive. The larger block of 2nd class coaches is represented by the following Bm 235, which must also be joined by dining cars and 1st class.

IC 580 “Riemenschneider” (Munich - Hamburg-Altona) from the 1980 summer timetable also has a special appeal. We want to take advantage of its special features and have put together a combination from its running days that is still model railway friendly:

Dm / Bm / Bm / Bm / Bm / Bm / Bm / WRbumz / Avm / Apm



Adding colour is the use of an alternative locomotive in the colours of a different paint scheme, here represented by Rokuhan's 181 206-5. Photo: Noch.

As can be seen, this train was one of the few Intercity in which a baggage car (and actually also a mail car) was included. The dining car is a copy of the unpopular Quick Pick car, which was once released by Z-Club International and which could follow as a correct model from Märklin, as the basic form (in the tourist train) is now available.

Those who cannot accommodate this train on their layout simply shorten it by one or two 2nd class coaches. It should be noted here, however, that none of the air-conditioned large-capacity coaches were used yet, and the train consequently lacked the usual comforts. This is typical for the early, two-class trains and can also be transferred to other IC by omitting Bpmz 291s, or reducing them to a single car.

The class 120 is also to receive IC honours in our proposals. We have selected IC 686 "Albrecht Dürer" (Munich - Bremen) for a suggestion here, as it was on the move on 7 January 1981. Led by 120 005-4, it consisted of five Bm 235, one WRmz 135, one Avmz 111 and one Apmz 121.



The IC 686 "Albrecht Dürer" of 7 January 1981 can be reproduced from Märklin models as shown here. Only two compromises are necessary: The pre-production locomotive of the class 120 has to be reproduced by the model of the 120 004-7, and we have replaced the dining car WRmz 135 by a closely related WRmh 132. The number of the five Bm 235 (picture above; shown are three examples) may be reduced on the layout, if necessary.

Again, no Bpmz 291 was represented. The train can be easily reproduced in this consist, only the pre-production locomotive with the deeper kink must be exchanged for one of the usual appearance.

In order to be able to reproduce two-class trains suitable for model railways, a little trick can help. On special occasions, there were also special trains according to IC standard. Such a train with 120 004-7 is documented for 14th September 1986 and is very interesting.

With only minor compromises, this very short train without dining cars can be reproduced with Märklin models.



We have somewhat loosely reproduced the special IC of 14 September 1986 hauled by 120 004-7. This refers to the use of Märklin coach 8758 (Apmz 123), in the original there were two Apmz of a different design.

A final option is IC runs beyond the system terminus stations. For example, groups of coaches continued to run from Munich to Garmisch-Partenkirchen for tourist reasons. One air-conditioned 1st class coach and two 2nd class coaches are often sufficient for this. Sometimes these trains were also hauled by the class 103. A 111 or even a 110 should be just as credible here.



On the Baden Rheinbahn, parallel to the Mannheim - Stuttgart high-speed line behind the noise barrier, 120 004-7 pulled into the new Hockenheim station on 14 September 1986 in the morning. It had a special train on the occasion of the partial relocation of the Schwetzingen - Hockenheim - Graben Neudorf line. Photo: Archive Peter Rappold

Product colours and DB long-distance traffic

It becomes more difficult for us to describe concrete train compositions from the time of the product colours to the present. A mere repetition of the plans would probably become boring at this point. Therefore, we propose to follow the suggestions already made and only replace them with the contemporary train car stock.



Three different colour schemes can be seen in this InterCity of the transition period: The locomotive and fourth coach still wear the livery introduced in 1974, which closely followed the TEE scheme. The two 1st class coaches are painted in product colours, but the first of them still has the old window frames in ivory paint, while the dining car marks the transition from the product colours to the long-distance colours of the Deutsche Bahn AG.

In the last years of the German Federal Railways, little or nothing has changed in terms of train lengths. Air-conditioned 2nd class compartment coaches (Bvmz), which were newly added, do not yet exist as a Z-gauge model, which is why we cannot consider them here. So, we have to follow the established models.



An example of a model in product colours with old window frames is the WRmz 135 dining car with Märklin item number 8774.

Depending on the period in which the train is to run, the proportion of Bpmz in the train set increases. From 1986 onwards, individual coaches appear in product colours in the Intercity, which make it look very colourful, especially as the roof colours were also not uniformly designed.

During this transitional period, many of the repainted coaches still had the old window frames in ivory colour. This did not look nice, but was immediately noticeable and is therefore also a stylistic device to reflect this time of transformation in the model.

Märklin picked up on this phenomenon and, in addition to the “worked out to the last detail” product colour specimens, also offered those with deviating window frames. An example of this is the dining car with item number 8774 from the production period 1989 to 1994.

1994 also marks the transition to Deutsche Bahn AG, externally identifiable by the change from the so-called Ege-biscuit logo to the Dürre-biscuit logo, the nicknames of the two company logos. Märklin already had both versions in the assortment for the product colours.

Further transitional periods also existed on the IC, however, which can also be taken up on a scale of 1:220. First of all, push-pull trains were introduced in Interregio traffic, which soon also served as a template for the IC. Märklin took this into account with the 87750 and 87752 car sets.



The still typical InterCity can be formed from Märklin cars according to the long-distance colour concept and a red class 101 electric locomotive. Such trains are often used as push-pull trains, and therefore also offer the attractiveness of being pushed around on the model railway layout.

The first-mentioned train car set reproduces coaches that were created by dispensing with the set-off stripe. At the same time, the Orient red was replaced by traffic red. This modification and the idea-giving product colours also ran mixed in the same trains for a long time. But soon this colour concept was only used for night and car trains.

“The brighter, the faster” was now the slogan of the Deutsche Bahn AG. Local trains were and still are clearly dominated by the traffic red, slower long-distance trains showed a fairly balanced colour scheme between the red and white areas, while the IC adopted the “white sausage” appearance of the ICE.

This is still the case today, but the Intercity trains are often shorter than in the past and often run as push-pull trains. The class 101 is an ideal vehicle for this, which can at least provide variety with its many

advertising liveries. The classic dining cars have been phased out in the meantime, and the bistro cars from the Interregio have taken over their duties. This can also be easily reproduced with Märklin products.



At the time of publication of this issue, it was not yet possible to recreate an IC 2, here 146 570-7 in front of such a train in Düsseldorf Hbf on 18 November 2016.

The final model is the IC 2, which has been announced as a new autumn model in 2021 and will also find a place on the layouts in the future. It is painted in the ICE colour scheme, but does not embody the claim to high speed.

As a model, it is also a noticeable compromise, because neither the shape of the locomotive nor that of the coaches is correct for it. Nevertheless, more than a few are looking forward to this train. They will also be able to accept that they will receive it with only four double-decker coaches, while its original is permanently on the tracks with five units and is not even allowed to run in the replicated constellation.

Manufacturer of the models used:

<http://hecklkleinserien.de>
<https://www.maerklin.de>
<https://www.rokuhan.de>

Compiled train formation plans:

<http://www.welt-der-modelleisenbahn.com>

Photos of various train formations:

<https://hobby-eisenbahnfotografie.de>

50 Years of Intercity

Revolution in Long-Distance Transport

The ideas that led to the Intercity network, with which the Bundesbahn launched a top product on 26 September 1971, date back to 1967. Although critical voices were heard at the beginning, this well-thought-out system was to revolutionise long-distance transport and become a model for many similar networks in Germany's neighbouring countries. Even today, it is impossible to imagine long-distance rail transport without it.

As early as 1951, the Deutsche Bundesbahn operated a network of fast trains, which initially only carried 2nd class, which then became first class with the class reform of 1956. The service times, train schedules and the comfort offered were geared to the needs of business travellers.



For the first time, they represented a route system that followed the plans and proposals of the passenger train timetable consultant at the HVB headquarters, Ministerial Advisor Dipl.-Ing. Carl Fischer. The main feature was that train runs were linked at important transfer hubs so that the target group could reach all economically important cities in Germany on the same day.

One of the characteristics of the long-distance express train system was that some train pairs only ran once or a few times a day. They also did not have a fixed frequency, but were integrated into the timetables according to identified demand.

From 1957 onwards, they were joined by the Trans-Europ-Express trains, which began to connect the European economic centres in a similar way, but with even greater comfort. They now formed the top product not only in DB's long-distance transport. The top speed in the DB network was set at 135 km/h

with the new long-distance express railcars. In 1958, changes to the EBO (Eisenbahn-Bau- und -Betriebsordnung - Railway Construction and Operating Regulations) increased this to 140 km/h.

From the same time onwards, the well thought out network of long-distance transport was increasingly undermined by international desires and developments within Germany. With a few exceptions, only quite independent line systems remained.



A class 601 diesel multiple unit waits for departure to Munich as F 120 "Prinzregent" in Frankfurt (Main) Hbf in 1969. It is one of the six F train pairs that carry the additional designation "Intercity". Photo: Reinhold Palm, Eisenbahnstiftung.

Nevertheless, TEE's as well as F trains gained a considerable clientele in the fields of commerce, business, industry and politics. But they were also popular for private and holiday travel. As a result, the trains became longer and thus increasingly interesting for DB. After all, 300 to 400 passengers per train were no longer uncommon.

However, in the competition with air travel as an alternative and the increase in individual transport, DB also had to develop further and plan what the long-distance transport of the future could and should look like.

In the meantime, the automobile had achieved a traffic share of about 70 % for business trips, while the railway and the aeroplane accounted for about 14 % in roughly equal shares. The main competitor was thus clearly determined; the railway had to be able to offer at least the same level of comfort and be as fast as possible in order to win back shares here. This required equally well coordinated transfer possibilities.

The "big bang" for the new concept of Intercity transport that was to follow from these considerations is generally dated April 1967. At that time, Ministerialdirigent Dipl.-Ing. Wattenberg from the HVB made the

proposal to introduce a dense inner-German high-speed railcar network that was to connect all the important economic centres of the Federal Republic of Germany with each other.

The road to “Intercity 1971”

The increasing electrification in Europe and the need for higher space capacities had in the meantime led to agreements among the railway administrations participating in TEE traffic to also allow locomotive-hauled trains.

This process had begun with the conversion of the “Rheingold” and “Rheinpfel” F trains into TEEs in the summer of 1965. Conversely, this led to the release of the VT 115 diesel railcars and a suitable field of application had to be found for these comfortable vehicles.



Abbildung: Sammlung Oliver Strüber

In the 1968/69 winter timetable, the vehicles now operated as class 601 appeared in F-train service. This network had been supplemented by some connections in a first step and thus represented a first anticipation of what was to follow in 1971.

Six pairs of trains led by the TEE railcar were additionally given the designations “Intercity A” to “Intercity F,” which was intended to indicate travel time reductions as effectively as possible, and, at the same time, made it possible to test the later train designation for acceptance.

In the same year, passenger train timetable consultant Dr.-Ing. Hussong presented an operating programme on behalf of the Board of Management. After it had been approved, the Board of Management



In 1971, the DB presented its new product in an effective advertising campaign and chose the now unforgettable motto "Germany at two-hour intervals". Illustration: Sammlung Oliver Strüber

speed of 200 km/h on longer sections, for which the four high-speed locomotives of the E 03 series, which had been tested since 1965, were already designed. The cruising speed, i.e., the calculated average speed between two stops or the total distance travelled by a train, was to increase to 140 km/h. This was soon, and for some time, impossible.

However, this was soon and for some time not feasible: to put the brakes on the economy of the year, the superstructure budget was cut in 1970 and the tracks could not be upgraded for the higher loads to be expected.

At the same time, they were already under greater strain due to a strong increase in traffic, which is why it was not even possible to make full use of the great tractive power of the class 103, the series copies of which were delivered from 1970 onwards. In addition, there were problems with the line train control (LZB) that still had to be solved.

set up a working group at the end of 1968 consisting of specialists from all the departments involved, who were to work out the most appropriate solutions by May 1969.

The chairman of this committee, department president Dipl.-Ing. Friedrich Scheller, finally presented a comprehensive proposal for a new and rhythmic Intercity network system.

On 1 August 1969, it was approved by the Executive Board and incorporated into the existing timetable with minor deviations.

The target date for the implementation of the new long-distance concept was 26 September 1971, the start of the 1971/72 winter timetable, a rather unusual date, as such serious changes were usually implemented at the start of an annual timetable (start of the summer timetable).

The choice of date was a compromise, however, and could not be delayed any further for competitive reasons. It could not be moved forward either because of the long delivery times for new passenger coaches, and the procurement of multiple units was also ruled out for the time being because of the development time required.

In its ambitious project, the Bundesbahn was now aiming for a higher top

A series of accidents in the first half of 1971 put DB under additional pressure: it was now often accused of having “succumbed to a speeding frenzy”. In view of the technical progress and the state of electrification that had been achieved, which made higher train weights and longer runs possible, this was not objectively tenable. Nevertheless, it made the Bundesbahn abandon its plans for the time being.

The 1971/72 winter timetable nevertheless showed a slightly increased cruising speed of 108.2 km/h, but it could not be maintained permanently. The Intercity was not free of teething troubles when it was introduced to the market; delays of 20 minutes were not uncommon. In 1972, the DB had to make the first corrections and add special surcharges to the timetables, which meant that the average travel speed dropped to 102.5 km/h.

At the beginning, the new trains were not all that economically significant, because the relatively few Intercity trains were offset by around 600 two-class D trains and 2,000 express trains with feeder functions. But they were an important calling card and, as a top-quality product, a flagship that DB needed in order to survive in the competition.



An Intercity according to the original concept of 1971, i.e., consisting only of 1st class coaches, passes through Wuppertal-Zoologischer Garten on 3 May 1978. 103 103-3 can be seen at the head. Photo: Wolfgang Bügel, Eisenbahnstiftung

In terms of the concept, the state railway was oriented towards the comfort of the TEE and also committed itself exclusively to 1st class. Since 1968, a clear increase in demand could be felt here (1970: + 14 %). With the Intercity, it was able to react to this and at the same time make the two-class long-distance trains in 2nd car class more appealing at the expense of the IC.

The Bundesbahn also saw its plans confirmed by the studies “TEE market research” (October 1968), “TEE travel habits” by GfK (May 1968), and the UIC report “Optimal travel comfort” (March 1969).

It was new and hitherto unique in the world for a railway administration to set up a network of fast and comfortable trains covering wide areas of the main traffic flows. The trains ran, equally uniquely, in an approximated two-hour interval timetable.

The existing TEEs were included in the system and the frequency, which DB was largely able to push through in the International Timetable Conference against the opposition of neighbouring railways. And it was to be proved right, because even the initial opponents were later to take up the idea of the interval timetable and also copy the Intercity in many cases.

A network of four lines was planned for the Intercity, serving 33 cities.

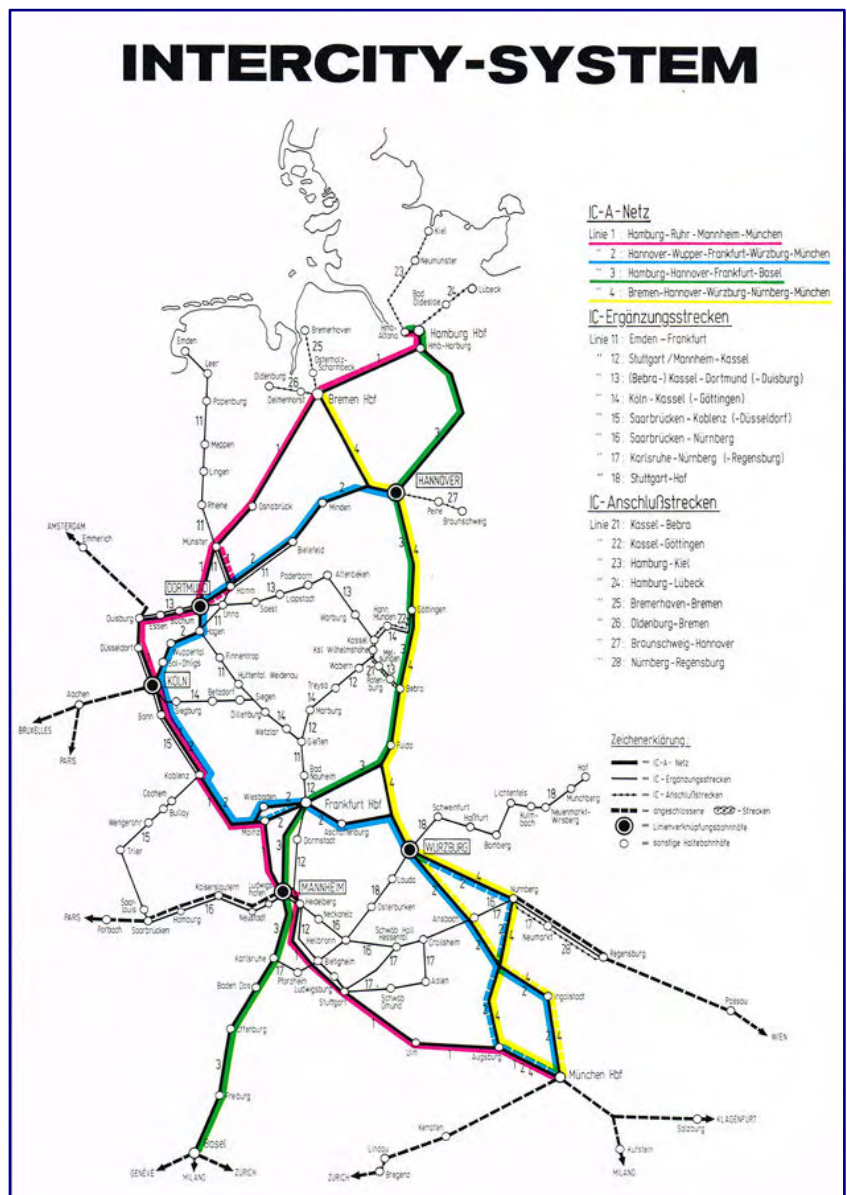
Dortmund, Hannover, Cologne, Mannheim, and Würzburg were chosen as the five connection points of the system, where two trains of different lines stopped at the same platform at the same time and enabled the desired, fast transfer.

This network was called IC main network or “Intercity A”, and was also implemented. “Intercity B” (Intercity supplementary network) was also planned from the beginning, but it was not a success, but we will come back to that in a moment in the following chapter.

First, let’s take a look at the four lines that did not necessarily correspond to the train routes. Some trains were run permanently or seasonally beyond the IC terminus stations for circulation or tourist reasons, others were shortened somewhat for service considerations.

Likewise, there were always “line-switchers”, i.e., trains that changed at one junction station to the running route of another line. Nevertheless, it was possible to maintain the two-hour rhythm with short and reliable transfer times:

Linie 1
Hamburg – Bremen – Münster – Dortmund – Essen – Cologne – Mainz – Mannheim – Stuttgart – Munich



In the 1973 Annual Report, the Bundesbahn presented its 1971 Intercity system together with the supplementary and connecting routes that had been introduced in the meantime. Illustration: Oliver Strüber Collection.

Linie 2

Hannover – Dortmund – Wuppertal – Cologne – Wiesbaden – Frankfurt (Main) – Würzburg – Munich

Linie 3

Hamburg – Hannover – Fulda – Frankfurt (Main) – Mannheim – Basel

Linie 4

Bremen – Hannover – Bebra – Würzburg – Nuremberg – Augsburg – Munich

This list shows that the Hannover and Munich junction stations were even served by three lines; in addition to the junction points, Augsburg, Bonn, Bremen, Frankfurt (Main), Göttingen, Hamburg, and Koblenz were also served by two lines.

The route network covered around 3,700 km with 528 connections between the 33 cities. Essentially, it covered the former F-train network, but offered significantly more direct connections, sophisticated transfer options, and significantly more train connections.



The former TEE multiple units of class 601 supplemented the locomotive-hauled trains. The TEE emblem on the front ends was covered by a new Intercity metal plate during these operations, as seen here on 7 September 1972 at the Bw München Hbf. Pho to: Dieter Junker, Eisenbahnstiftung

The concept and launch date were therefore fixed, so the Bundesbahn had to introduce its new concept in a promotionally effective way, “Deutschland im Zwei-Stunden-Takt” (Germany at two-hour intervals) memorably summarises the key points of the Intercity. With this motto, it began advertising its new top product in the 1971 summer timetable, and from July onwards it intensified advertising in all other areas as well.

On 26 September 1971, the new idea then went into practical everyday operation. The series 103 locomotives and the class 601 multiple units, which were no longer used in TEE service, were the top performers.

Only the newly ordered, air-conditioned 1st class passenger coaches, modelled on the Rheingold coaches, were not yet in service in sufficient numbers, which is why ordinary Aüm 203s, as they also ran in F train service, were required as a substitute. This was to remain a permanent problem, albeit with a decreasing tendency, because as soon as enough cars were delivered, a noticeably increasing demand caused new and constant shortages.

The White Shark is coming

No sooner had the Bundesbahn's success child been born, at least as a plan, than the question of what the means of propulsion for the fastest traffic should look like in future also came back into focus. Both a locomotive-hauled train and the multiple unit have advantages and disadvantages due to their specific characteristics.

On the part of DB, the following demands were made on a vehicle for high speeds:

- A modern appearance should symbolise the latest state of technology.
- Light weight was important to protect the superstructure, which is subject to high stresses at high speeds, and also promised cost reductions.
- The seating capacity should be easily adaptable to the then current demand.



403 006-0 in IC service waits for departure at Holzkirchen station (part of Munich Central Station) on 10 November 1974. Photo: Archiv Petkelis

The first two points clearly spoke in favour of a multiple unit, the desired flexibility rather for locomotive-hauled trains. However, it was not clear what the optimal vehicle would look like. The final outcome of these discussions can be seen in today's fast long-distance traffic.

A first answer that followed was the conversion of four class 601 power cars to gas turbine drive. The four class 602 units were for testing purposes, as a class 603 series multiple unit was also planned, but never built.



On four class 601 machine cars, the traction diesel engines were replaced by AVCO-Lycoming gas turbines (type TF35) with 2,200 hp output. The vehicles were easily recognisable by the significantly enlarged air intakes, as illustrated by the comparison of 602 002-8 with the regular version (right) on 30 November 1975. Photo: Prof. Dr. Willi Hager, Eisenbahnstiftung

Thanks to the AVCO-Lycoming type TF35 helicopter turbine, built under licence and supplied by KHD, which was installed in place of the diesel engine, the power head output doubled to 2,200 hp and made it possible to transport ten-car sets at 160 km/h. The train was used in Intercity service.

The four units of the 602 series were used in Intercity traffic from July 1974 onwards, initially exclusively in the same series. It was not until January 1975 that it was possible to use a 602 with a 601 at the other end, because this required a newly developed synchronisation control for the diesel engine and gas turbine to run together.

From then on, three power cars were in service, the fourth serving as a reserve for a possible breakdown. Their scheduled IC service ended between 22 February and 28 May 1978, partly due to expiry. In retrospect, it should be noted that the use of gas turbine multiple units in Intercity traffic did not appear to make sense at any time, because all lines were electrified.

However, the project "class 603" and the quite inexpensive testing of this drive by converting existing vehicles followed a euphoria that had broken out in Europe (France and Great Britain), as well as in America. The reason for this was the expected difficulties in drawing current from the overhead line when maximum speeds of up to 300 km/h were anticipated.

On 24 May 1970, the order was placed for the construction of three test multiple units of the class 403. Strictly following the IC-71 concept, they were intended as pure 1st class multiple units. All four coaches were powered and consistently followed the lightweight construction using aluminium.



The Deutsche Bundesbahn proudly presented its new class 403/404 parade train wherever it could. In internal competitions, it even searched for a catchy name to anchor in its customers' consciousness. According to their ideas, it would then probably have been remembered by us as "Hecht" (pike). Illustration: Sammlung Oliver Strüber

The two end carriages (class 403) offered compartments, the two intermediate carriages a large room (class 4040) and a half dining car with 24 seats (class 4041). As the first German high-speed train, it can certainly be seen as an early precursor of the ICE. In the seventies it really embodied the modern Bundesbahn and had a function as a parade train.

The DB took great pains to find a suitable paint scheme that corresponded to the zeitgeist of pop colours, emphasised the sleek shape of the multiple units and harmonised as well as possible with the gold-anodised windows. Models (also in 1:1 scale) were built and various colour mixes tested until the perfect appearance was found.

The Bundesbahn even wanted to establish a name that would convey the appearance and characteristics in a memorable way and would become naturalised among customers. The name “Hecht” (pike) was one of the favourites. However, the three trains were more commonly known as “white shark” and “Donald Duck.”

The first multiple-unit train was delivered to the customer on 2 March 1973. The trains went into scheduled Intercity service in the 1974/75 winter timetable, but were to be withdrawn from this high-quality service again in the 1978/79 winter timetable due to the “IC ‘79” concept. In these four years, especially the poorly utilised line 4 (Munich – Bremen) became the main line for the class 403/404.

This hardly allowed a profitable use of the new trains and completely missed their potential. Only relatively few passengers were thus able to enjoy the latest vehicles at all and were useful for representative surveys of how they were received by the public.

Initial plans for additional middle cars, which did not necessarily require their own drive because of the power reserves, were not implemented after the teething troubles had been remedied. Thus, their use was wrongly considered rather inflexible and costly; in the two-class Intercity concept of 1979, they then no longer had a chance and were eliminated from these services. They only found a future in special trade fair and regular service, then from 1982 in charter service for Lufthansa.



A class 602 power car leaves Cologne Central Station on 17 May 1977 as IC 147 “Dompfeil” bound for Hannover. Photo: Peter Schiffer, Eisenbahnstiftung

Successes and failures

Line 4 (Bremen – Hannover – Bebra – Würzburg – Nuremberg – Augsburg – Munich) remained a problem child for a long time, and in contrast to the other three, it was only poorly utilised. Here, it was to take years until the Bundesbahn found a suitable solution.

There was also a major failure to cope with, because part of the concept, which was already available in 1971, was implemented with a delay and then only half-heartedly. Officially it was called “Intercity B”, which meant a supplementary network to “Intercity A”, the main network. The DB had announced early on that it was to follow in the summer of 1973.

Introduced with the timetable change on 3 June 1973, it covered a route length of around 3,000 km and included a further 73 cities. The Intercity Network A and B now served 121 cities and included around 80 % of all express train traffic in its convenient services.



220 051-7 is in service in front of DC 991 “Tauberland” on 25 April 1975, when it is photographed near Eubigheim. The consist is indistinguishable from an ordinary D-train. Photo: Peter Schiffer, Eisenbahnstiftung

The trains of the B network were a combination of the familiar two-class express train and the comfortable Intercity. Since pure 1st class trains could not be operated economically on the routes to be served, they were also to carry both car classes like the D-train. Since they travelled as such to the “City”, the Bundesbahn specially created a new train type called DC (“City D-train”) with a trademark based on its IC.

They were to be used on eight lines and, like the ICs, were to have train names that picked up on the landscapes they passed through. The following lines went into service for the 1973 summer timetable:

Linie 11

Emden – Münster – Hamm (Westf.) – Hagen – Hüttental-Weidenau (Siegen) – Gießen – Frankfurt (Main)

Linie 12

Kassel – Frankfurt (Main) – Darmstadt – Mannheim / Stuttgart

Linie 13

(Bebra) – Kassel – Paderborn – Dortmund – Duisburg – (Mönchengladbach)

Linie 14
Cologne – Siegen – Gießen – Kassel – Göttingen

Linie 15
(Cologne – Bonn) – Koblenz – Trier – Saarbrücken

Linie 16
Saarbrücken – Mannheim – Heidelberg – Heilbronn – Crailsheim – Nuremberg

Linie 17
Karlsruhe – Stuttgart – Nuremberg – (Regensburg)

Linie 18
Stuttgart – Würzburg – Hof

These eight lines were connected to the IC-A network on one or both sides. Their function was to independently serve the en-route stops not served by the IC as well as a feeder function for the main network.



216 081-0 passes in May 1975 in front of a DC of line 11 on the left track the block station Bentlage, which belongs to Rheine. Photo: Will A. Reed, Eisenbahnstiftung

A second group of the supplementary system were IC connecting lines for which there was less passenger potential and which were only introduced as new services in individual cases. In most cases, DB used existing express and fast trains that had or arranged short connection times to IC. These trains had group names that characterised the route in terms of landscape or city and always ended at -City:

Linie 21 (Kurhessen-City)
Kassel – Bebra

Linie 22 (Kurhessen-City)
Kassel – Göttingen

Linie 23 (Förde-City)
Hamburg – Kiel

Linie 24 (Hansa-City)
Hamburg – Lübeck

Linie 25 (Weser-City)
Bremerhaven – Bremen

Linie 26 (Oldenburg-City)
Oldenburg – Bremen

Linie 27 (Welfen-City)
Braunschweig – Hannover

Linie 28 (Donau-City)
Nuremberg – Regensburg

The DC trains were also to be associated with a quality concept, which is why they were provided with their own logo and highlighted in advertising. Unfortunately, despite the prototype coaches ABvmz 227 and Bvmz 237, the DB had failed to procure high-quality coach material for the 2nd class. Therefore, the usual express train carriages were to be used, but these were to be of the latest design and in good general condition.



In 1972 Linke-Hofmann-Busch (LHB) in Salzgitter developed two prototypes of air-conditioned coaches, including the Bvmz 237 shown here. It had eleven compartments, and, instead of hinged folding doors, it was fitted with newly developed swivel-sliding doors of an experimental design with push-out steps, which was later adapted to the Eurofima coaches with folding steps. Photo: Grandt, Eisenbahnstiftung.

The concept read well in theory, but the subsequent implementation was rather half-hearted and largely ignored the needs of the customers. It was not a success and the number of trains was quickly reduced before this type of train disappeared completely in 1978.

The problems that resulted in a lack of customer acceptance were of different kinds: First, the trains were not very attractive for 2nd class customers, as their connections were exclusively coordinated with Intercitys that only carried 1st class.

And, certainly the difference in comfort between simple express coaches and air-conditioned TEE/IC coaches was not helpful in gaining more popularity with business travellers. Some may also have missed the dining car, which was replaced by a minibar service.



Whatever changes were to take place in the IC concept, the class 103 remained the pillar of this system for thirty years. When 103 164-0 was photographed in Würzburg Hbf, it still wore its front apron and also the DBS 54 scissor pantographs with Wanisch rocker. Photo: Archiv Petkelis

Instead of high-quality wagons, there were often wagons that were in poor condition inside, and sometimes also outside. This prevented any publicity from developing.

On lines 21 to 28, most of the trains consisted of middle entry carriages or Silberlingen (silver coloured commuter coaches) and certainly did not meet the standards of a long-distance train. If they had been running in rush-hour traffic shortly before, the cleanliness of them also left a lot to be desired.

Handelsblatt put it in a nutshell with the following sentence: "It is (...) a superfluous designation, because the 'DC' are simple D-trains without special comforts (...)." And this criticism seems flattering, because many of the trains used in 1973 were still far below this standard.

For the Bundesbahn, however, there was also a need for further action. First and foremost, line 4 was considered a problem child on which the capacity utilisation urgently needed to be increased. At the same time, however, a stagnation in the development of the Intercity became apparent from 1975 onwards, which seemed to jeopardise the long-term expansion plans.

The DB board of directors therefore pondered the idea of adding more passengers to the system in order to secure its future. The class 103 was strong enough to still run at 160 km/h even with a double train load.



In the 1978/79 intercity timetable, two-class trains were tested for the last time before the concept was changed - with far-reaching consequences also for the train material used. Illustration: Sammlung Oliver Strüber

So in the end it seemed the most sensible and perhaps the only option to open up the IC for 2nd class in perspective.

However, this had to be done in such a way that the well-paying 1st class customers did not leave or the capacity in this coach class was reduced in favour of the masses.

Accordingly, in the period from 1976 to 1978, trials were carried out with Intercity trains that also had 2nd class.

In order not to alienate the business travellers, both classes were sharply separated, which was given with the inclusion of the dining car. With this block train formation, it was

only possible for the different passengers to meet in the rolling restaurant.

Despite initial criticisms and warnings, this idea proved to be purposeful and successful, which was soon to lead to a change in the IC concept. Despite the need for more comfortable DB did not participate in the ordering of corresponding Eurofima coaches, however, 2nd class coaches with IC standards, which was recognised by then at the latest.

The Integrated Control System (IBS)

The year 1979 would finally bring the major changes that had been announced for three years. The Bundesbahn had worked out its "Integrated Service System", or IBS (Integriertes Bedienungs-System in German), for short, and consistently enforced the timetable times required for it at the International Timetable Conference. In fact, there was hardly any possibility of compromise, because then all the plans would have become wastepaper.

The IBS became generally known under the more common name "IC '79". DB massively advertised with the catchy slogan "Intercity. Every hour. Every class.", which effectively conveyed the key points. The addition of 2nd class partially caused many trains to be weakened by some of the 1st class carriages.

Illustration on page 48: Sammlung Oliver Strüber

continues on page 49

INTERCITY. JEDE STUNDE. JEDE KLASSE.



Verkehrssystem der Zukunft –
bei der Bahn schon heute.
3100 Kilometer Streckennetz.
47 Städte. Täglich 148 Züge.
Der Anschluß-IC steht meist
am gleichen Bahnsteig.

The only way to compensate for this was to halve the frequency of the service, which would also result in a time advantage for the existing customers. This inevitably led to what the railway administration now knew how to emphasise so effectively in its advertising.

In fact, with a few exceptions, the intervals were always fixed to the minute. This enabled frequent travellers to have a good memory and overview. What seems self-evident today was completely new in 1979, and led to great resistance from other railway administrations.



At Sollnhofen, this two-class intercity consisting of eleven coaches was photographed after variant 11 from page 17 of this issue on 12 April 1980. Photo: Archiv Petkelis

After all, D-trains and TEEs also ran across borders and also had to be accommodated in this time scheme, which was not always possible. Similarly, the single-class TEEs were now no longer to be integrated into the IC system, but were not to create competition for the Intercity at home.

The idea of changing from one Intercity to another at the same platform of a junction, i.e., the connection points of the four lines, was retained - and as far as possible without waiting times. Since the trains had become significantly longer with 2nd class, the Bundesbahn introduced carriage level indicators on the platforms and also divided them into specially marked sections A to F for easier orientation.

So, when the new concept was launched with the timetable change on 27 May 1979, important cornerstones remained as tried and tested pillars: The four lines continued to exist, the junctions were also not touched, course cars were to be dispensed with even more consistently and loading times were not envisaged in future either.

In fact, there were only very few ICs that carried a baggage car. The “line exchangers”, which ensured many direct connections, also remained an integral part of the concept.

Another focus was on increasing the cruising speed, as further sections had been upgraded for up to 200 km/h in the meantime. The DB's rolling stock became more of a problem. While air-conditioned coaches were standard in 1st class, for 2nd class they had to fall back on ordinary passenger coaches of type B(ü)m 234, which were technically upgraded with magnetic rail brakes.

Built in series, these coaches were designated Bm 235 and were then also allowed to travel at 200 km/h. The comfort that the Intercity was able to convey with them in 2nd class, however, was for years only that of an ordinary D-train. Now the misguided procurement policy took its revenge, especially since the DB had to give almost all of the newly commissioned coaches of higher comfort (Bpmz 291) to cross-border traffic due to international agreements.

This could not remain free of criticism, which, however, also reached the Bundesbahn elsewhere.

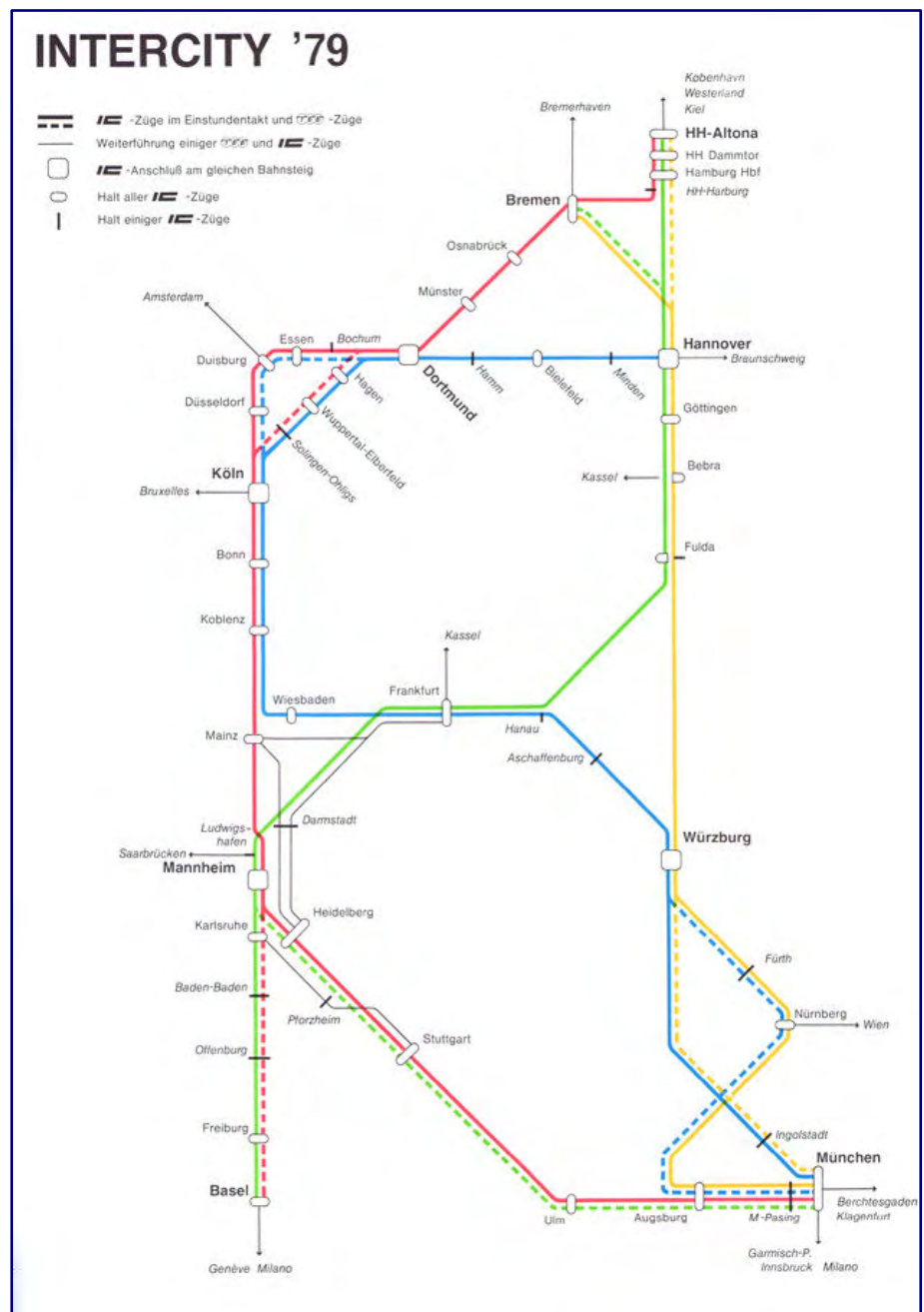
The only way to counter the decreasing desire to dine on the train was through self-service trolleys, where customers took a ready-made meal from the shelf, which was then heated up by far fewer staff in disposable dishes and handed back with plastic cutlery.

Although the Quick-Pick coaches were described as successful, well thought-out and sensible, they were not at all accepted by the customers and became the cause of many complaints.

Soon ÖBB also refused to accept such wagons when crossing the border.

Originally, they were not intended for this high-quality traffic, but for ordinary express trains, but the one-hour interval also resulted in a lack of dining cars, which DB did not know any other way to deal with.

All in all, however, the IC concept proved its worth and became a model for many railway administrations to copy, which was also expressed in the end of the TEE and its replacement by the Eurocity, the international offshoot of the IC.



In the 1979 annual report, DB graphically depicted the new Intercity concept and also showed where supplementary TEE trains were still in operation. Illustration: Sammlung Oliver Strüber

A brief look should be taken at the locomotives used in IC service from 1979 onwards. Due to the lack of 2nd class coaches and the fact that new locomotives or conversions no longer seemed worthwhile, the 403/404, 601 and 602 series were withdrawn from high-quality scheduled long-distance services with the "IC 79" concept.



This photo from Cologne Central Station, taken in 1980, illustrates the principle of trains stopping at the same time on the same platform for quick transfers. The newly introduced platform sections, in the photo you can see the signs with the marking E, also belong to the IC '79. The IC 625 "Meistersinger" stopping on the left is one of the few Intercity trains to carry a baggage car. Photo: KlausMiniwolf (CC-BY-SA-4.0)

However, the class 103 could not possibly provide all the services that the hourly service entailed. Although designed for top speeds of 200 km/h, the Bundesbahn had not procured any other suitable locomotives.

So 110s were also used on sections that were travelled at lower speeds. Likewise, it used the more advanced 111 (160 km/h top speed) in IC traffic instead of ending its construction in favour of 200 km/h fast machines. And where the performance of one locomotive was not sufficient, the 111 had to be used in double traction.

The five pre-series examples of the class 120 from 1979 were also used in Intercity traffic as part of their testing. Nevertheless, the 145 class 103 locomotives were required to achieve high performance and impressive maximum daily kilometres over the years.



In the summer of 1981, the class 120⁰ machines ran intercity services between Munich and Nuremberg. On 5 August 1981, the photographer catches 120 001-3 in front of IC 521 (Hannover Hbf – Munich Hbf) in the station entrance of Schwabach on the bridge over the river of the same name. Photo: Henning Folz

Intercity 1985 and the political turn

When the Deutsche Bundesbahn celebrated “150 years of railways in Germany”, it saw a suitable occasion to make further corrections to the IC system. The goal was now to further shorten travel times, serve additional stations, create more connections and also improve services. Two new lines now celebrated their premiere.

Line 2 no longer ran via Hagen and Wuppertal, but in an east-west direction through the Ruhr region and no longer touched Wiesbaden.

The missing connection from Hagen and Wuppertal was made up for by the new Line 5, which started in Dortmund and also connected Frankfurt Airport to the IC network to continue via Würzburg, Nuremberg, and Augsburg to Munich.

Line 4A, also new, now connected Hannover and Bremen as the shortest of all lines, because the previous Line 4 from Hannover was now a direct connection to Hamburg. Overall, the number of trains increased from 157 to 205 per day with these changes. With 440 line kilometres for 200 km/h, the cruising speed could also be increased to 108 km/h.

From 1987, the class 120 also entered IC service with 60 series locomotives and at least temporarily provided some relief for the class 103 fleet.

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The class 111 was regularly found in IC service on some sections. 111 002-2 leads an Intercity to Munich Hbf at Hechendorf on 5 September 1981 (photo above), while 111 125-1 waits for departure at Mainz Hbf. Photos: Archiv Petkelis



Locomotives of the classes 110 and 112 were not regularly seen in front of the Intercity, as here 112 494-0 on 7 March 1987 in front of IC 693 to Augsburg near Gabelbach. Photo: Archiv Petkelis

The last major changes in the IC concept began in 1991. On the one hand, after the fall of the Berlin Wall and German unification, it became necessary to connect Berlin and then all other major cities in the east of the republic to the network.

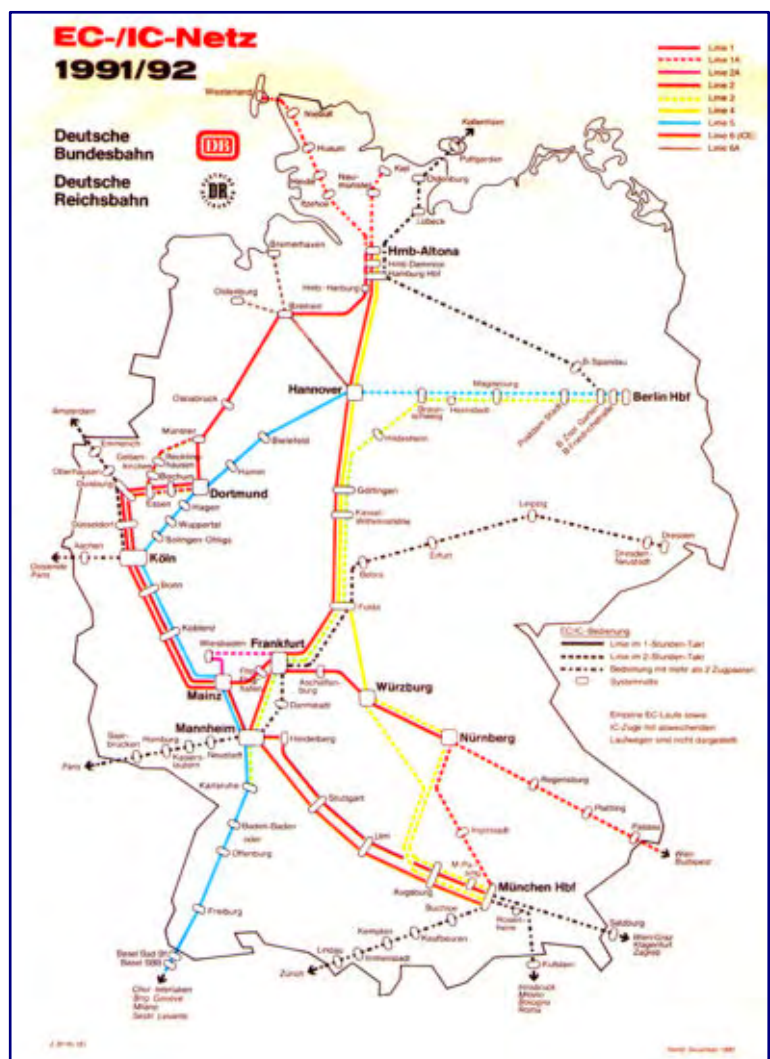
In the same year, however, the ICE service was launched, as a result of which the Intercity was no longer the Bundesbahn's top product. The new high-speed trains replaced previous IC runs and were integrated into the overall system.

The high-speed network benefited in total and with the growth of the ICE fleet, the expansion in the east could also be further advanced and completed.

However, while almost all the investments of the Deutsche Bahn AG, which had been founded in the meantime, were directed towards the ICE, the Intercity was at best trading water.

The material used was getting older and more and more approaching its economic limit of use.

Photo on page 55:
In the Intercity behind 103 163-2 on 24 April 1993 there is still a 1st class coach, which is not painted in product colours, as the train passes in front of the camera lens in Amstetten. Photo: Archiv Petkelis



In 1991/92, a major change took place in the IC network: Lines 4 and 5 connected Berlin, and Line 6 was the first ICE service. Illustration: Sammlung Oliver Strüber

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The IC "Max Liebermann" was the first Intercity of the Deutsche Reichsbahn in summer 1990 and connected Berlin with Hamburg in the footsteps of the "Flying Hamburger". This use of the leased class 601 was limited to the period from 1 August to 30 September 1990. Photo: Benno Wiesmüller, Eisenbahnstiftung

Only the 145 class 101 locomotives can be effectively booked here. However, they were to replace the class 103 one-to-one and initially freed them up for interregio services before their retirement became foreseeable at the turn of the millennium.



The class 120 was still used in front of an IC in the form of the series locomotives 120 101 to 120 160, here 120 151-6 with advertising design on 11 September 2005 in Hamm (Westf.).

While there were initially also projects and plans for replacements for the Intercity in the form of multiple units, these successors were formally put into service as ICEs with higher fare requirements (series 411, 415 and 605).

In fact, this step meant a first devaluation of the ICE in terms of comfort and speed.

This trend continued long after the turn of the millennium: The latest successor with the project name "ICx" finally appeared in the timetables as ICE 4.

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101 053-7 is seen in front of IC 2202 to Norddeich Mole on 12 July 2015, photographed near Salzbergen (photo above). IC 2012 arrives with 218 494-3 and 218 343-2 at Immenstadt on 21 August 2018, 218 476-0 waits for departure in front of IC 2084 (photo below). Photos: Roel Hemkes/CC-BY-2.0 (photo above) / Joachim Bügel, Eisenbahnstiftung (photo below)

The “x” of the project designation was originally to be replaced by the digit of the IC generation, so the train only received an upgrade to the highest comfort level on paper. Therefore, it was given a lot of folklore to disguise its true identity and distract from the often-criticised poor seating comfort.

However, because the wagons officially running as IC are now completely obsolete, and the class 101 is also approaching the end of its useful life, the need for replacement became more urgent. This also includes the retired Interregio coaches of the next lower comfort level, which have been running in new colours as part of IC for years.



An IC-2 train set waits for departure from Düsseldorf Hbf on 18 November 2016 as IC 2009 to Cologne Hbf. With comparable trains, it was also possible to travel to the cathedral city at the same time for a lower local transport price. Is this a future concept?

Perhaps in order not to completely blur the line between IC and ICE, DB AG reached into its bag of tricks one more time and lowered the comfort level of the Intercity even further. In order to save costs and quickly obtain suitable vehicles, it withdrew existing purchase options from Bombardier for double-decker local trains that were no longer needed in their actual intended purpose as a result of lost riots.

The role that was actually intended for the ICE 4 was now taken over by uncomfortable and rocking local trains, painted as “Weißwurst” (a white sausage), charged a significantly higher fare and summarily referred to as IC2.

The initiated and consistently continued devaluation of the once high-quality long-distance transport at Deutsche Bahn AG only allows for the pious wish that no ignorant passenger will have to undergo an ordeal on a route of 300 km or even more.

Wikipedia entry on the Intercity of the DB:
[https://de.wikipedia.org/wiki/Intercity_\(Deutschland\)](https://de.wikipedia.org/wiki/Intercity_(Deutschland))

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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

75 Jahre Faller gewürdigt

Geschichte eines Häuslebauers

Faller ist ein Synonym für Modellhäuser geworden, doch in der Geschichte des Herstellers aus dem Schwarzwald sind noch viele anderen Produkte und Linien zu nennen. Anlässlich des diesjährigen Firmenjubiläums geht ein Buch wieder der Geschichte nach. Wir möchten erklären, warum es toll und spannend, aber eigentlich kein Jubiläumsband ist.

Ulrich Biene
Faller - Die Welt von oben

Delius Klasing & Co. KG
Bielefeld 2021

Gebundenes Buch
Format 21,5 x 28,6 cm
184 Seiten mit 380 überwiegend farbigen Abbildungen

ISBN 978-3-667-12124-0
Preis 29,90 EUR (Deutschland)

Erhältlich direkt ab Verlag
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Die Brüder Hermann und Edwin Faller setzten vor 75 Jahren ihre Geschäftsidee um, den Menschen Modellbaukästen und Gebäudemodelle an die Hand zu geben. Für Ulrich Biene war das der Anlass, fünf Jahre nach seinem ersten Faller-Buch im selben Verlag einen neuen Titel zu veröffentlichen.



Die Geschichte des Häuslebauers, dessen Name zu einem Synonym für Modellbahnhäuser werden sollte, ist in groben Zügen sicher den meisten bekannt: Den Baukästen folgten Fertigmodelle von Häusern. Anfangs aus Holz, Pappe und Gipsputz gefertigt, hielt schließlich der Kunststoffspritzguss Einzug und sollte die Modellbahn revolutionieren.

Neben die Fertigmodelle traten Bausätze für Preisbewusste und setzten sich durch. Auch Bäume kamen früh ins Sortiment und blieben es bis heute. Selbst vor Figuren machten die Gütenbacher nicht Halt. Und mit ihrem Car-System haben sie ohne jeden Zweifel auch Maßstäbe gesetzt. Dies lenkt unseren Blick auch auf Faller AMS (Kürzel für Auto, Motor, Sport).

Keine Frage ist es, dass der Autor vieles abuarbeiten hatte, um 75 Jahre im Rückblick präsentieren zu können. Herausgekommen ist ein feines und lesenswertes Buch, das nahtlos ans erste Werk anknüpft, ohne sich spürbar zu wiederholen.

Neue und ansprechende Fotos, zu denen nun im Vorwort auch der Sammler genannt ist, der die Modelle dafür bereitstellte, krönen diese ansprechende und unterhaltende Lektüre. Aber sie ist eben nicht, was der Titel verspricht: „75 Jahre Faller“ steht auf dem Buchdeckel und verspricht einen Jubiläumsband.

Genau das ist dieses Buch aber nicht! Angesprochen werden vor allem Freunde und Sammler historischer Faller-Modelle. Es taucht wieder tief in die Geschichte ein und weiß die historischen Erfolgsmodelle zu nennen, zu beschreiben und einzuordnen.

Sehr gelungen wirkt auch, dass ins Faller-Stammwerk, frühere Außenbetriebe und zu den Heimarbeiterinnen geschaut wird. Ebenso rückt Ulrich Biene wichtige und verdiente Faller-Mitarbeiter der frühen Firmengeschichte in den Fokus und belässt es nicht dabei, die beiden sich bestens ergänzenden Brüder mit ihren individuellen Talenten zu portraituren. Der Ton wirkt dabei etwas sachlicher und weniger euphorisch, als wir ihn vom ersten Band in Erinnerung haben.

Neben Aspekten der Firmengeschichte werden auch einige Produktlinien betrachtet, darunter die großen Kassenschläger wie auch Fehlgriffe, die schnell wieder verschwanden. Kindheitserinnerungen werden wach, die schrill-bunten Siebziger haben viele sicher längst verdrängt, aber wohl nicht vergessen.

Der Aufbau der Kapitel und Texte ist bekannt, die Texte sind dieses Mal aber länger und wirken vollständiger. Der Autor vermittelt seinen Lesern viele Fakten, weshalb auch Käufer gut bedient sind, die das vorausgegangene Buch schon besitzen.

Aber warum kommen wir zum Fazit, es handele sich nicht um einen Jubiläumsband? In kurzen Worten erklärt, müssen wir erst mal festhalten, dass der beanspruchte Zeitraum 1946 beginnt und bis ins Jahr 2021 reicht. Betrachten wir die ausführlich vorgestellten und beschriebenen Produkte, scheint die Geschichte aber spätestens in den Neunzigern zu enden.

Das Jubiläumsmodell 2021 findet sich überhaupt nicht im Buch, immerhin reicht die vierseitige Chronik bis ins Jahr 2021, verweist dort aber auch nur auf die im Frühjahr erstmals ausgefallene Spielwarenmesse und ein Neuheitenfeuerwerk, ohne konkret zu werden. Die handelnden Personen nach dem Ausscheiden der Gründer spielen im Buch ebenfalls keine Rolle.

Und so fehlen dem Werk viele Stationen, die nach der „Detailoffensive“ in den Achtzigern bis heute folgten. Nur wenige sind exemplarisch hier wiederzufinden und haben eher nostalgischen Charakter wie die Gasolin-Großtankstelle Brandshof aus dem letzten Jahr.

Als perfektes Beispiel mag das Faller-Car-System dienen. Als würdiger und maßstäblicher Nachfolger von Faller AMS ist es selbstverständlich im Buch vertreten und auch erwähnt, dass es für die Spur N ebenfalls eingeführt wurde. Doch die enorme Aufwertung durch Digitalisierung und Ortung der Fahrzeuge fehlt leider auch.

Unterrepräsentiert ist auch die Baugröße N, die Spuren Z und IIm (über Pola) müssen mit kurzen Hinweisen auskommen. Leider vernachlässigt wurde auch die Zukunftstechnik der Lichtschneidetechnik, die den Werkstoff Hartkarton wieder salonfähig machte und den Modellbau zu seinen Wurzeln zurückführt.

Weitere Beispiele für wichtige Produkte, die Maßstäbe oder Ausrufezeichen setzten, eigentlich nur bei Faller erscheinen konnten und die wir deshalb vermissen: viele spektakuläre Kirmensmodelle, das große und in der Modellbauwelt einmalige Hallenbad, der nachhaltig ausgerichtete Bahnhof Horrem, Kloster Bebenhausen (Jubiläumsmodell zum 70. Firmenjubiläum) oder auch das noch sehr junge Container-Verladeterminale.

Vor uns liegt ein tolles Buch, das wir gern gelesen haben und nicht missen möchten. Es als Jubiläumsbuch herauszustellen, geht aber leider etwas an den tollen Inhalten vorbei. Sie sprechen auch ohne dieses sicher werbewirksame Attribut die meisten Modellbahner an, denn Faller ist schließlich nicht nur irgendein Häuslebauer!

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50 Jahre IC aus Sicht des Eisenbahn-Kuriers Ein deutscher Jubilar im Filmrückblick

Ein rundes Jubiläum feiert der Intercity, erfunden durch die Deutsche Bundesbahn. Er bestimmt dieses Jahr nicht nur Magazintitel und Neuheitenprogramme der Modellbahnhersteller, sondern fand auch Platz im DVD-Angebot des EK-Verlags. Und trotz kleiner Schwächen verbreitet er viel Freude beim Anschauen für den interessierten Eisenbahnfreund

CFT Video Berlin
50 Jahre Intercity in Deutschland
aus der Reihe „Berühmte Züge und Lokomotiven“

EK-Verlag GmbH
Freiburg (Breisgau) 2021

DVD-Video
Bildformat 16:9
Tonformat Dolby-Digital 2.0
Sprache deutsch
Laufzeit ca. 58 Min.

Best.-Nr. 8601
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Am 26. September 1971 ist der Intercity-Verkehr bei der Bundesbahn an den Start gegangen, wie wir im Vorbildbeitrag dieser Ausgabe bereits erläutert haben. Sein heutiger 50. Geburtstag ist nicht nur uns eine ausführliche Würdigung wert, sondern in Film-Form auch dem EK-Verlag.



Sein wegweisendes Konzept, das zum Vorbild für den Fernverkehr vieler Bahnverwaltungen wurde, und die lange, noch längst nicht zu Ende geschriebene Geschichte geben genug Stoff für knapp eine Stunde Video-Genuss.

Der Käufer sieht in einem Zeitraster das Erscheinungsbild des IC von den Anfängen bis heute. Im Fokus stehen dabei die bekannten Zugpferde wie die Baureihen 103, 120 oder 101. Aber auch die eher in Nebenrollen agierenden Fahrzeuge wie die Baureihen 218, 245, 403/404, 601 oder 612 fehlen nicht.

In Szene gesetzt wurde ebenso der IC2 als Doppelstock-Wendezug mit den Baureihen 146⁵ und 147⁵ sowie dem von der Westbahn gekauften Doppelstock-Triebzug. Wird der 612 klar als Mogelpackung entlarvt, wird auch mit dem Verweis auf die Nahverkehrsherkunft der IC2-Erscheinungsformen nicht an berechtigter Kritik gespart.

Doch einige Dinge fehlen diesem Film auch, um eines Jubiläums rundum würdig zu sein: Viele Aspekte der Geschichte kommen zu kurz oder fehlen ganz, darunter beispielsweise der Hinweis auf die Blockzugbildung oder die geschichtlichen Umstände, die zu diesem Konzept führten.

So hätten der Fahrzeugmangel der Bundesbahn in den Siebzigern und frühen Achtzigern Erwähnung verdient gehabt wie auch die so umfangreich erforderlich gewordenen Änderungen im Netz, als Deutschland sich plötzlich einer Vereinigung von Ost und West näherte. Der Netzgedanke und die damit umgesetzten Linien kommen ebenfalls viel zu kurz.

Erwähnt sind, eingebaut über eigene Werbefilmsequenzen der Bundesbahn, nur die vier Ausgangslinien des Systems IC 71. Die letzten größeren Änderungen aus dem Jahr 1985 und eben das Erschließen des Ostens fehlen hier. Lediglich der IC Max Liebermann und die Anbindung Berlins fanden Berücksichtigung.

Zu Gute halten müssen wir den Filmproduzenten, dass Fotos wenig für fast 60 Minuten Film taugen. Ein Video lebt von bewegten Bildern und so musste der Schwerpunkt zwangsläufig auf dem eingesetzten Rollmaterial liegen.

Und das ist dann sehr gut gelungen: gute Auswahl, weitgehend repräsentativer Querschnitt durch alle Fahrzeugtypen und Lackierungsvarianten. Auch die Aufnahme- und Wiedergabequalität ist tadellos. Wie üblich, fanden auch einige historische Aufnahmen Eingang in den Film, die allerdings ausnahmslos Eigendarstellungen der Bundesbahn sind.

So werden viele Aspekte erklärt und deutlich, die zur Idee der DB und ihrem Selbstverständnis gehören. Doch um auch an der letzten Stelle rund zu sein, hätten wir uns noch private Filmaufnahmen aus den Siebzigern gewünscht, die auch das zeigen, was der Bundesbahn nicht genehm war. Wir denken beispielsweise an blaue, nicht klimatisierte Aüm 203, weil es an geeigneten Reisezugwagen fehlte.

Abwechslungsreich und spannend sind Blicke in die Führerstände verschiedener Lokomotiven und Steuerwagen, in die Bahnbetriebswerke und das Treiben auf wichtigen Bahnhöfen. Streckenaufnahmen vermitteln die Dynamik, mit der wir den Intercity verbunden haben und immer noch verbinden sollten. Auch wenn der ICE längst in die Fußstapfen des Intercity getreten ist, gehört er noch nicht zum alten Eisen.

„Rückblick, Gegenwart und Zukunft des Fernverkehrs in Deutschland“ lautet der Anspruch dieses Films und mit nur leichten Schwächen bei der Vergangenheit ist es den Verantwortlichen gut gelungen, diese wichtigen Züge umfassend zu portraituren.

Wäre dieses Jahr nicht das 50. Jubiläum, auf den der Filmtitel sogar aktiv hinweist, gäbe es nichts zu kritisieren: Der IC ist umfassend und gelungen dargestellt und auch ein völlig Unwissender könnte ihn nach dem Anschauen der DVD wohl richtig einordnen.

Es sind halt Kleinigkeiten, die entscheiden, ob das Ergebnis eine Spitzennote erhält oder „nur“ in den oberen zehn Prozent einzuordnen ist. Deshalb ist uns wichtig, diese Bewertung noch mal explizit hervorzuheben.

Uns ist schließlich auch bewusst, wie schwierig unsere Erwartungen auf diesen letzten zehn Prozent umzusetzen wären: Als der IC „geboren“ wurde, waren Filmrollen noch unglaublich teuer und jeder Hobbyfilmer überlegte gut, wofür er sie einsetzte.

Und damals standen nicht die modernen Züge, die die Zukunft der Bundesbahn bestimmen sollten, im Mittelpunkt des Interesses. Schließlich würde es sie noch lange Zeit geben. Wer damals filmte, der richtige sein Objektiv auf die letzten Vertreter der Dampftraktion.

Und so müssen wir dem EK-Verlag auch attestieren, dass er mit einem Jubiläumsfilm über den Intercity im Markt wohl ein Alleinstellungsmerkmal innehat! Und das wird aus den dargelegten Überlegungen wohl seinen Grund haben. Wir freuen uns, den Jubilar auch immer und immer wieder auf der Mattscheibe anschauen zu können. Auf die nächsten 50 Jahre!

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Staying in the conversation (part 5)

Architecture and (Hi)story

Archistories has been active in Z gauge for over ten years, a decision made out of passion. Frank Drees sets high standards for himself and his products, with the clear goal of precision and perfection. The models embody architecture in its purest form and tell a story. Editor-in-chief Holger Späing asked the founder and owner what distinguishes him, his brand and his models.

Trainini: Hello Mr Drees, please tell us, since when has Archistories operated?

Frank Drees: Hello Mr Späing! I founded the Archistories brand in Hannover in 2009. And as it happens, in the beginning you often have different ideas about where exactly you want to go. In the beginning, I only did projects in 1:87 scale. At that time, it was mainly the Frye railwayman's house, the prototype of which is still standing in Lingen in Emsland opposite the former railway repair works.

Many people will certainly remember the Emsland line and the factory, as this was the last line in Germany at the time that was still operated with steam.

Long ore and oil trains still shuttled here between the Ruhr and the North Sea until 1977. That was really impressive and attracted countless photographers to Emsland back then.

Just for fun, I converted the Frye railway house to 1:220 to see how detailed you can build in the smallest common scale.

I myself was a bit unsure at first, as were many customers, because of the tiny size of some of the components; but, it went surprisingly well, and the result was a great success.

To this day, the railwayman's house is one of our bestsellers.

In the meantime, I am convinced that I can expect the customer, especially the Zetties, who are really good at modelling and who are certainly among the most skilled model railroaders in terms of fine motor skills, to make a few extra efforts, because the results are worth it.



Frank Drees talks to us about his company and brand Archistories, which specialises in hardboard laser-cut kits for Z gauge.

Trainini: Who or what can customers imagine behind the name Archistories?

Frank Drees: Archistories is, as I said, a brand that I founded twelve years ago, and I am solely responsible for it. I try to find the best partners in each field of work and to achieve the most convincing results together. This is not easy in this very specialised field of work, because many who do outstanding work do so exclusively privately, or as small entrepreneurs.

That means we have to be flexible, for example, in terms of publication dates and such, and it works.

We publish when a product is fully developed and also available at short notice. It doesn't matter if it's in the middle of summer or between the years. I don't put myself under excessive time pressure to present something half-baked as a hand sample at some trade fair or other.

So it's important to have the best people in the network in order to set special emphasis on the Z track market together. That is my personal goal for every new product.

For example, I consider Avantgarde model trees to be unrivalled when it comes to finding particularly beautiful solitary trees for decorating dioramas. This is important when it comes to perfect product photos. As a recent example, your readers can hopefully see this in the product photos of the gallery Dutchman Am Geestevenen.

Try to achieve such a realistic effect in Z scale with mass-produced foam flooring.

That is acceptable as a filler for large areas, but simply nothing for close-up and macro shots in product photography.

Then, there is the area of diorama construction, which we need for our product photos and presentations; here, your editorial colleague Dirk Kuhlmann has delivered unique results time and time again.

And, also for good product photography, we have our now experienced partners with whom we have been working for years. This also applies to graphic work in the field of building instructions.



Good partners are important for Archistories, an example of which is the choice of model trees for the windmill model presentation.

The field of work is highly specialised, and that's why it's important not to always have new partners with whom you would then have to start over again from Adam and Eve every time.

If we have a product for which it is better to add certain components, for example, 3D printed parts, then, this is done, as with the windmill bonnet, by "Z-Doktor" Björn Plutka, who brings together a lot of patience and commitment.

And, so for each special trade, I have to find the partner with whom it simply fits in terms of quality and also on a human level. Then the whole thing is fun, and I have a lot of fun!

Trainini: By the way, we want to introduce Avantgarde-Modellbau here next month. One question that now somehow suggests itself is the question of your own standards for a product from your company. How would you describe your philosophy?

Frank Drees: As I have just said, my aim is to deliver a mature product that is completely convincing in appearance and also presents no unsolvable puzzle for any customer in terms of design.



The search for a suitable partner also involved the mill itself: The bonnet was finally designed and manufactured by Z-Doktor Modellbau.

As I said, my goal is to deliver a mature product that is visually absolutely convincing and does not pose any unsolvable riddles for customers in terms of design.

If, on the other hand, fictitious buildings are involved that quote a certain architectural style, such as Prussian railway construction, then creativity is needed, a trained eye for proportions and effect, as well as a feeling for what a façade needs, for example, in order not to appear flat and boring.

When a product development is completed, the product has been built at least five times during the process in order to identify errors and optimise design details.

If, for example, a tenon joint is very wobbly and you have to assume that some customers might squeeze a little harder, then this detail has to be solved differently and the tenons perhaps lengthened or the joint optimised in some other way.

When the product is completely finished, it is built a few more times because the client gets a finished model for presentation purposes and photo samples are also needed. This way I make sure that a product is really fully developed.

Even though I have been doing this work for ages, I'm never immune to mistakes, that's just the way it is, and that's why I play it safe.

Our customers may already have noticed that after building the basic construction, they have a solid, stable structure in their hands on which the rest can then be well built.

This is very important, because it's no use having frustrated customers who say "once and never again", because they end up with a dented box on their craft table.

Trainini: You mentioned clients, i.e., private or commercial customers who do the distribution themselves. How many products are commissioned by Archistories?

Frank Drees: In the meantime, there are so many of them, and at times even all new products in very different scales. And it's getting more and more, so that I hardly get around to making my own products, some of which have been haunting my mind for years.



Even the basic construction provides a stable building on which the other layers of hardboard are applied.

The windmill I mentioned earlier is a matter of the heart and a product of my own. It took a long time to get there, because I didn't find it appealing to make a windmill without a drive, and then there was the problem of the bonnet.

So, I searched for a long time until I found a partner for the 3D printed part and a drive that was small enough, had a high gear ratio and was of high quality. I am very happy with the result.

As far as commissioned work in general is concerned, there are of course a lot of enquiries from private customers who have loved the so-and-so station since they were children and would like to have it; often together with the idea that such a thing is generously priced at 500 euros.

I have to say quite clearly that this is not the case at all. Product development is pure engineering work that often takes several weeks. That is already clear from my previous descriptions.

If, after development, a product is to be made into a kit, an alternative is a one-off model, and it becomes even more complex, and thus more expensive. A commissioned product

is therefore only something for people who can simply afford it or for entrepreneurs with a well-thought-out plan that pays off.

I can imagine that this information will not be to everyone's liking, but I would ask you to remember that the development of rolling stock also involves high development costs. This is no different for us.

In the meantime, we work with many international partners who have us develop and produce product series.

My brand is successful, and I can say that with some pride. And customers know what they can expect, and I make it a point not to disappoint them. This is my simple business model.



A lot of care also goes into the colour coordination. In this case, it was the choice of brick colour, the cladding of the cone and the wood tone of the gallery beams (in the photo, an attempt with dark brown).

Trainini: I'm glad you mentioned the windmill. It was one of my personal dream models. Now I would like to look in another direction. What does the look into the future promise?

Frank Drees: I have great partners everywhere and this network is constantly growing. This brings me into contact with completely different building styles and sometimes exotic customer wishes, and it also broadens my own horizons.

You get to know people you would never have met otherwise. That's a lot of fun for me. So, I would like to continue on this path, on which so many beautiful flowers have already grown, to put it figuratively.

As far as the technical implementation of the products is concerned: we will continue to choose combinations of the best technology for each component, simply because it has proven itself.

That is and will remain our wonderful hardboard and, for certain details, sometimes a different technique. I have no reservations whatsoever about other materials, but sometimes there are parts that are important for the overall effect that simply cannot be realised well in cardboard or would be difficult to build.

In such cases, 3D printed parts are often the proverbial cherry on the cake. Although the price is driven up by accessories, my point of view is: it is better to sell less than a cake without a cherry on top.



Finding and selecting a suitable high-quality drive became a major challenge. Patterns of the mill bonnet were used to check whether it would fit in without any problems.

About the balcony railing on the mill Am Geestevenen, for the construction, I followed the same tried and tested procedure: a stable sub-structure and the façade construction in the usual way. I think the mill is not that difficult to build, anyone can do it well.

A special feature here is the bearing of the drive. That was a bit tricky, but the way it is now implemented, the motor is nice and stable in position. It works great.

As far as the design is concerned, it is not at all easy to work out a coherent colour concept: Is the cladding of the cone better in grey or in dark brown?

Does the railing of the balcony look better in white or dark brown? And, do I make the brickwork brownish or more reddish?

In the end, I liked the red for the base best because it adds a bit of colour to the model. For the cone, I found the warm-looking brown best, and since the background was rather dark, I achieved a nice, filigree contrast with the light grey railing and the tracking rods of the same colour.

I then added an unobtrusive colour accent with the beautiful, rich dark green that appears in various places on the mill, such as the doors, discreetly on the wings and the very filigree reel at the back.

Trainini: You mentioned that the bonnet was a particular challenge. What were the difficulties?

Frank Drees: Yes, first was to find a shape that crowns the upper part of the mill, but does not look too brutal, but fits harmoniously into the overall picture. It is important how far the bonnet projects above the cone, how high it is, and so on.

In reality, underneath the thatched covering of such bonnets is a roof structure, which in this case is very similar to boat building. You have to think about that when the outer roof skin is being shaped. So, it should not become too organic either.

I always strive to work sensibly in scale, so it was a challenge to accommodate a reliable, high-quality gear motor that is small enough that the building does not have to be made unnecessarily larger because of it. That's exactly what I managed to do in the end, but it wasn't easy, that's true.

The upper part of the drive had to be fitted into the bonnet without compromising the stability of the pressure part. To check the penetration depth in reality, I cut open different versions of the bonnet and checked the space requirements. When the engine hits the bonnet, vibrations are transmitted, which leads to unnecessary noise. This was then adjusted.

Finally, the canopy was given a beautiful rough surface that represents a thatch covering.

I can justifiably say that Z-Doktor Björn Plutka printed my ideas exactly in resin, a truly outstanding result.

Trainini: What else is connected with the development of such new products?

Frank Drees: I always attach great importance to a sensible external image. When so much effort and time goes into a product, as with the windmill, then, I think it is only appropriate to invest a minimum of effort in the appearance as well.

This means lovingly building the diorama, effective photos, realistic lighting and the whole thing photographed by a professional.

Even if the Z scale model railway market is a niche, Archistories would still like to contribute to its further development. Blue photo wallpaper with white sheep clouds in the background with gaudy colours gives me heartburn.

I see the special attraction of Z gauge in building as close to reality as possible and that's why we try to ensure that all product photos are realistic, without any tricks, by the way. The products look exactly the same in the photos as they do in the customer's home.

I have the models photographed from all sides (i.e., in front of a white or black background) and then there are usually also a few photos in the diorama, which are often also taken in the "Atelier Kuhlmann". Fantastic results, sometimes even as a video!

Every now and then we also take very elaborate photos for trade fairs, social media or our websites. For me, that's another cherry on top of the cake. A particularly beautiful one!



As a result of many trials, studies and test constructions, a model has been brought onto the market that appears harmonious and balanced, and is, therefore, well received by customers.



This atmospheric photo proves how important it is to have a well thought-out, harmoniously coordinated product and the right environment in which to present it. Archistories does not want to leave anything to chance at this point.

Trainini: So, I think I have to stop now. Now, I've heard so many times about cherries that I'm getting hungry. Besides, what you probably don't know is that an Archistories kit is still waiting to be assembled. I'm looking forward to that now! Thank you very much for the interview.

Frank Drees: Many thanks from me too. It was fun!

All Photos: Archistories

Manufacturer's web pages:
<https://www.archistories.com>



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**ZEITGLEICH MIT
DEN KULT-EVENTS:**



Get out and meet friends

The Desire for Model Railways

Well over a year and a half of deprivation, during which we cannot or can hardly practise our hobby publicly, at least not in attendance events, is already behind us. When a manageable group is suddenly allowed to meet again in larger rooms to operate together, that is something very special. Our reader Markus Gaa therefore shares his joy and enthusiasm.

Von Markus Gaa. I think we all share a feeling. We, the US Z Lines, don't want to exclude ourselves. What drives us especially is to run long trains, to do shunting jobs, to build up a large modular layout, and, last, but not least, to meet with friends. Meeting, talking, making plans and tackling new challenges is fun for us and also a kind of holiday.



The chance to meet was used to set up a large US modular layout in the community space. Photo: Franz Vetter

So, it was Peter Dumler and Peter Simon who made it possible for us to feel and experience all this again after such a long time. Both organised the premises, the catering for the hungry and thirsty model railroaders and also all the other details around it. It was not a public event, but nevertheless prepared for all eventualities with a resilient Corona hygiene concept.

In the period from 13 to 21 August 2021, the Lutheran congregation of Wunsiedel made their rooms available to us for our meeting. The church council was also there in small groups and let themselves be infected by our euphoria. I would like to take the **Trainini®** readers with me today, to inspire them with photos of our hobby and, of course, to seduce them a little.

continues on page 75





Photo above:
The modules on display also have a lot to offer in terms of scenery. Lively life and typical American scenes are guaranteed there. Photo: Franz Vetter

Photo below:
Being able to run heavy goods trains in a model is the touch of something special for friends of US railways. Here, two "Challengers" are seen in heavy service. Photo: Markus Gaa

Photo on page 74:
The construction stretched over two halls. The installation was therefore simply built through the door. Photo: Franz Vetter

Across two halls, the Rock Forrest Line was set up with other modules from the High Franks Division, these were the modules and segments in Z gauge, and also Peter Dumler's Rätische-Bahn layout in H0m gauge, as well as the Kirmes segments (H0 size) by Andreas Dumler.

The US Z Lines is a group of enthusiastic US model railroaders of Z gauge. We come from different continental European countries. What we have in common, besides the joy of US model railroading, is the building of modules according to a uniform standard.

We run digital and use mainly products from Digitrax. However, DCC decoders from various manufacturers are also used. The vehicles are mainly from American Z Line and Micro-Trains Line.

On older modules, the tracks are still from Märklin, but since then almost all track is from Micro-Trains or Atlas, because they have a narrower, and, thus, more prototypical sleeper spacing.



Fairground segments by Andreas Dumler in H0 gauge rounded off what was on show in a larger scale as well. Photo: Markus Gaa

I would like to say thank you to all the participants who made the meeting a lot of fun. My special thanks go to Peter Dumler and family, and to Peter Simon, who invested a lot of time and effort to make wonderful moments and great experiences possible for me and us, the US Z Lines.

To the readers of **Trainini®**, I hope you enjoy looking at the photos here, watching the videos on YouTube and of course dreaming about model railroading. See you again – sometime for sure!

for two more photos and info box, see page 77



The Rätische Bahn of the Dumler family also followed the 1:87 scale and consequently presented itself in H0m gauge (photo above). An Allegra is just setting off from the station (photo below). Photos: Peter Dumler (photo above) / Markus Gaa (photo below)

Pages on the active members and their material:

- <http://www.uszlines.de>
- <https://www.americanzline.com>
- <https://www.micro-trains.com>
- <https://www.digitrax.com>

Videos on the internet about the event:

- <https://www.youtube.com/user/z220bahner>
- <https://www.youtube.com/watch?v=yhmqVfsD5iY&t=4s>
- <https://youtu.be/VU6OK8fIF-M>

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Call against vaccination fatigue:

Do we want to continue to read "appointment cancelled" because of rising Corona infection numbers? Wouldn't we rather discover the "new products" that our hobby has to offer again soon and exchange ideas in person, and then "with a good feeling"?

For model railway enthusiasts, the vaccination against the Corona virus should be an incentive, if one would like to soon participate again carefree in one's regulars' table, club meeting or exhibitions.

It is possible that not everyone can be vaccinated, e.g., because of illness or an allergy to ingredients of the Covid 19 vaccines. However, everyone who can be vaccinated should do so and the vaccination offers are becoming more and more straightforward. Talk to your family doctor!

Do it for yourself, for your family and for all of us together and especially for the children and grandchildren who are currently still excluded from the vaccination campaign. For our future, I would like to counteract the increasing vaccination fatigue here.

Sabine Helene Kuhlmann, Düsseldorf

Obituary by Jürgen Faulhaber and Holger Späing:

At the age of only 56 years, Thomas Zeeb, a prominent Spur Z friend, passed away in the night from 11 to 12 September 2021. We are surprised and saddened, we cannot believe it. Our thoughts are with his loved ones whom he left behind.

Thomas had a decisive influence on our gauge and our hobby for decades. Since the nineties, he was active in and for the Z Club 92, founded the model railway club under this name in Stuttgart, and must have been personally known to many Zetties from fairs and exhibitions.

Professionally employed in the body shop "at Daimler", where he was entrusted with the maintenance of robots, he was also a passionate Mercedes-Benz driver in his private life.

His passion for model railways was collecting models and also photographing them. For more than a decade he was responsible for the collectors' database "Collection Märklin Spur Z" at Modellplan.



Thomas Zeeb has sadly passed away. Photo: Dirk Rohwerder

From this context, he was a profound connoisseur of Z gauge model history, a valued contact person, a helpful hobby colleague and also a good friend. He was also able to set a milestone by co-founding the Z Gauge Convention South Germany, which takes place every two years alternating with the Altenbeken exhibitions at the Märklin site. We would like to keep him in honourable memory for his services.

Another top model from Artitec:

Artitec (<https://artitec.nl>) has delivered its ladder for overhead line maintenance (item no. 322.035) in 1:220 scale. Like other finished models of the manufacturer from Amsterdam, this one is also assembled by hand from resin and photo-etched parts.



The ladder for overhead line maintenance (art.no.322.035) also joins the list of top models from Artitec.

The striking yellow colour gives it impact, the application has been done so discreetly and skilfully, that the ladder does not look brand new, but like a well-maintained tool that has been in service for a long time.

We would like to equip a gang with this finely detailed ladder, and create typical work scenes for a construction train, even if this would also permanently eliminate a passage possibility on the chosen track.

Re-launched pipe loads:

With Spur Z Ladegut Josephine Küpper sewer pipes (art. no. Z-160) are transported again. The specialist from Aachen has reissued the popular load and also other raw loads (Z-305 to Z-307), but also points out that the range has even been expanded in this respect. A look at the webpages (<https://spur-z-ladegut.de>) is, therefore, worthwhile.

Current status on planned trade fairs:

After the number of cases throughout Europe dropped significantly in some cases in the summer, trade fairs seemed to be within reach again. Model railway exhibitions are currently planned in Germany, the Netherlands and Austria.

We are providing an up-to-date overview here, but recommend that our readers find out for themselves close to the time of the event whether the plans can be implemented unchanged and how tickets will be sold. In most cases, tickets can only be purchased electronically in advance, so that the permissible visitor numbers can be represented with certainty!

The following fairs with model railway participation are known to us for autumn 2021, chronologically in the order in which they will be held:

Modell, Hobby, Spiel in Leipzig
1 to 3 Oktober 2021
<https://www.modell-hobby-spiel.de>



Modellbau-Messe in Wien (Vienna)
23 to 26 Oktober 2021
<https://www.messeninfo.de/Modellbau-Messe-M12243/Wien.html>

Eurospoor in Utrecht
29 to 31 Oktober 2021
<http://eurospoor.nl>



Faszination Modellbau in Friedrichshafen
5 to 7 November 2021
<https://www.faszination-modellbau.de>

Intermodellbau Dortmund
17 to 20 November 2021
<https://www.intermodellbau.de>



The latest Märklin deliveries:

The Insider annual car for 2021 (item no. 80331) has arrived at the dealers. The model is based on a private car of the type Uc, used by the German Federal Railways, from the stock of the former chemical and pharmaceutical company Hoechst AG from Frankfurt (Main).

As in the last years, the manufacturer from Göppingen presents the members of the Insider Club a model based on a form that has existed for years. This wagon is to be located in the late epoch IV: Märklin indicates an operating condition around 1994, i.e., in the transition period to the Deutsche Bahn AG.

Also on sale now is the vehicle display (89024) with eight Tempo tricycles in the colours zinc yellow, azure blue, fire red and pale green (box body) as well as turquoise blue, pure orange, pebble grey and moss green (flatbed version as new form variant). All models are individually packaged, rollable with rubber tyres and have windscreen glazing.



Two of the three recently delivered Märklin new products are the Insider annual wagon for 2021 (item no.80331; photo above), and the class 042 multi-purpose steam locomotive (88276; photo below).

Shortly before the editorial deadline, models of the class 042 multi-purpose steam locomotive (88276) also arrived at the dealers, but this did not yet have an effect on the total edition. We will present both oil-fired steam locomotives in detail in the October issue, and then also show that they have not been realised absolutely correctly in every detail.

Railway strike in Wonderland:

While many customers were annoyed by the recent rail strike and the stubbornness of the railway board in demanding real wage cuts from its employees, Miniatur Wunderland Hamburg took this regularly recurring event with humour.



In Hamburg, the recent railway strike was also a theme on the show layout, garnished with a portion of humour. Photo Miniatur Wunderland

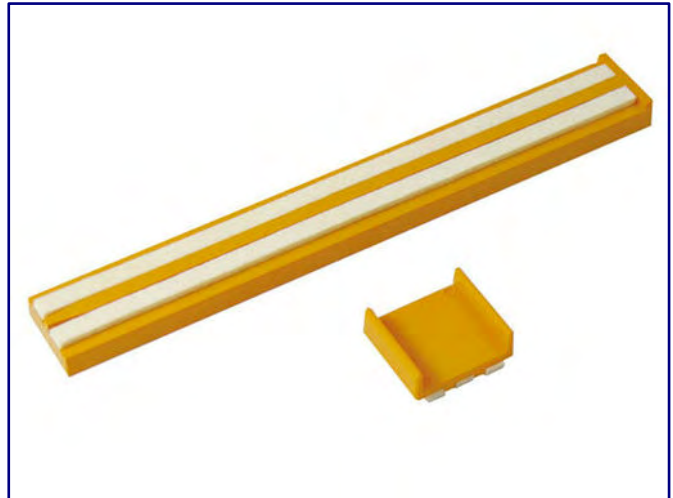
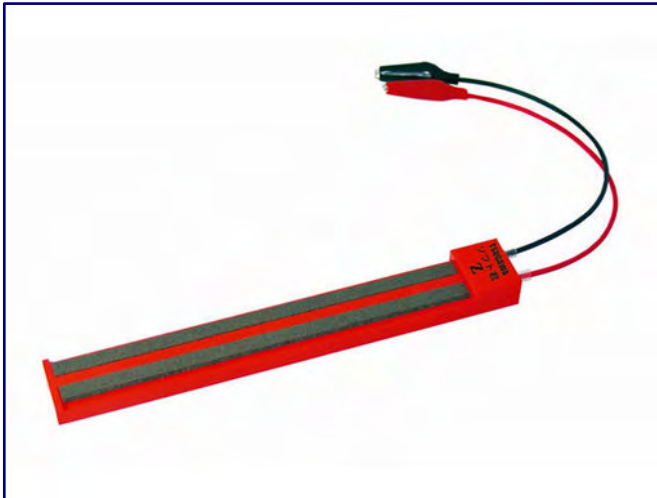
There it also took place in parallel and in small scenes. A group of train drivers demonstrated on a platform in Mesebach, Claus “Superman” Weselsky stopped an ICE in the Harz mountains and a group of train drivers blocked a track, bringing everything to a standstill. In addition, a new version of “Major Claus” was sung and authorised by Peter Schilling, however, the video can no longer be called up for licensing reasons.

Wheel cleaning system at the Model Railway Union:

Modellbahnunion from Kamen has introduced two wheelset cleaning systems for Z gauge in its programme. Their sponge conducts electric current and thus gently cleans the driven wheels of the mounted locomotives.

The tool is connected to the track or transformer with the enclosed cables of the electric version (art. no. MU-Z-A12508). With light pressure, the locomotive is held on the tool while its rotating wheels are cleaned by the sponge.

It is suitable for both analogue and digital models, and adhesive tyres are no reason not to use it. The usable length is about 180 mm. The non-electric version (MU-Z-A12509) is identical in length and has a



Two wheelset cleaning tools are now available from Modellbahnunion. They are suitable for motorised vehicles (item no. MU-Z-A12508; photo left) and non-driven models (MU-Z-A12509; photo right). Photos: Modellbahnunion

cleaning felt over which wagon models can be pushed, and whose wheels then also clean themselves when turned.

The provider's electronic shop can be reached at <https://www.modellbahnunion.com>.

AZL's latest deliveries:

At American Z Line, the EMD F7 diesel locomotive is now ready for delivery in the colours of the Milwaukee Road. Available here are a set of A and B units (item no. 63005-1) as well as a single A unit (63005-2).



These locomotives look much flashier and more eye-catching in the blue and yellow design of the Alaska. These models, also offered as A-B combination (63011-1) and single A-unit (63011-2), also include attached snow ploughs on the fronts of the A-units.



The R-70-20 refrigerated wagons will roll out in September in the eye-catching design, white paint with red sliding doors of the SOO Line. Available here are a single car (914808-1), as well as a pack of two (914838-1) and four (904808-1).



Manufacturer photos of the current deliveries can be found at <https://www.americanzline.com>.

New from C-M-K:

The zoo assortment at Creativ-Modellbau Klingenhöfer is experiencing further growth. A new addition is the long-tailed tree dweller known as the small spider monkey. Consequently, it is not a representative of the great apes, baboons or gibbons.

EMD F7s of the Milwaukee Road (photo above) and the Alaska (photo middle) as well as refrigerator cars of the SOO Line (photo below) are currently being delivered by American Z Line. Photos: AZL / Ztrack



The brown bear family (photo left) has been revised, the "spider monkey" (photo right) is completely new in the range. Photos: Creativ-Modellbau Klingenhöfer

The popular brown bear in the range is now available in a revised new edition, together with two cubs. The figures can be ordered at <https://www.klingenhoefer.com>.

Watering cans at Schrax:

Watering cans are part of everyday life, often just not on the model railway. Schrax (<https://www.schrax.com>) now wants to change that and has constructed two different models to serve all eras. There is the galvanised tin casting can of earlier days, which is made with grey moulding material.

For more modern plants, the plastic watering can that is common today is suitable. Different colours are offered, which are delivered in a colourful mix. Special colour requests can of course be made when ordering. The models are based on 10-litre cans, which are often available in cemeteries.



Pouring cans in two design presentations and several scales are now available from Schrax - on the far left of the photo is a Z-gauge example. Photo: Schrax

The parts are delivered to the customer with a small protective bracket so that the very delicate handles and the neck of the can survive transport undamaged. It is removed with a scalpel or sharp knife, and then the watering can is ready for use.



This year's Micro Mouse Halloween wagon, a box car in a free special design (art. no. 507 00 700), is scheduled to go on sale soon. From the Sweet Liquid series, wagon number 7 is now due for Imperial Sugar (530 00 550).



Two box cars, each in the same design will follow in red for the ATSF (505 00 431 / 432), in mint green for the New York Central (505 00 441 / 442) and in a strong green for the Reading (505 00 461 / 462).



Micro-Trains products are distributed by Case-Hobbies (<http://case-hobbies.de>) and others.

Start of autumn with new calendars:

Autumn is upon us and the various publishers are once again presenting their calendars for the coming year. The major publishers Geramond/VGB and EK-Verlag are again offering a wide variety of themed compilations on classic and modern model themes, model railways and individual designers.

For a short presentation, we have chosen another EK publication: the calendar "Model Railways 2022". Although all the monthly motifs appear summery, they are well put together, and varied.

Covered carriages in special Halloween design (photo above), the New York Central (photo middle) and Reading (photo below). Photos: Micro-Trains

This concerns different track gauges, urban and scenic themes as well as different countries. Likewise, all three types of traction have been considered in a balanced way.

Zetties will only miss a motif that is also dedicated to their favourite gauge. But even that won't curb their enthusiasm or prevent them from making a purchase.



New car supplements presented:

The 1zu220-Shop has introduced new truck models, which are exclusively manufactured for them by Wespe Models and are only to be given away as free additions to ordered Insider Club models. This time the manufacturer MAN has been considered with the corner truck 19.230 DFK (item no. WM-HRT10-01) and the later front steer 26.280 DFK (WM-HRT11-01).



The MAN 26.280 DFK (photo on the left) and the 19.230 DFK (photo on the right) were unveiled as new additions to future Insider models. Photos: 1zu220-Shop

The blue-red colour scheme of the previous editions is also retained for these two models. This should make it possible, if desired, to credibly reproduce the former vehicle fleet of a local haulage company in the vicinity of Westheim (town of Marsberg in the Hochsauerland district).

New models and collector's packs at Full Throttle:

WDW Full Throttle (<http://www.wdwfullthrottle.com>) has put together a new collector's pack (item no. FT-COL55-1) based on models from MKT. It consists of the models FT-1046A and FT-2059, a cylindrical and an open bulk wagon. Another set with factory-made Ford wagons (FT-COL37-2) consists of the open bulk freight wagons FT-2047 and FT-2048 of different colours.

New are the 100-tonne bulk wagons with side box struts and three discharge dashes in design for the Rio Grande (FTPZ-8025-1) as well as cylindrical bulk wagons in white livery and red inscriptions for the Southern Pacific (FT-1066-1).

WDW Full Throttle is available in Germany among others at Case-Hobbies (<http://case-hobbies.de>).

Exhibition at Giesinger Bahnhof in München (Munich):

25 and 26 September 2021 was a special day for the model railway enthusiasts of the Z-gauge regulars' tables (clubs) in Bavaria and Vienna.

Organised by Z-Freunde International e.V., a model railway exhibition open to the public took place for the first time in about one and a half years on this weekend, naturally under Corona regulations with a hygiene concept.

The event took place in the cultural centre Giesinger Bahnhof in München (Munich).

ZFI **GIESINGER BAHNHOF**

Modelleisenbahn-Ausstellung
in der Gepäckhalle des Kulturzentrums Giesinger Bahnhof
am 25. und 26. September 2021

Kein aber kein - Ausstellerverhalten im Maßstab 1:220 (Nur Z)
geöffnet am Samstag 25. September von 12:00 Uhr bis 20:00 Uhr
abends Anlagenspräsentationen auch im Nachtmodus
geöffnet am Sonntag am 26. September von 10:00 bis 17:00 Uhr

EINTRITT FREI

Vereinstellungsort:
Gepäckhalle im Kulturzentrum Giesinger Bahnhof Giesinger Bahnhofplatz 1, 81525 München

Vereinsteller:
Z-Freunde International e.V.
www.z-freunde-international.de **www.z-freunde-international.de**

mit Axel Hempelmann, 80987 München, Tel. 089 215 53 02 49

Beachten Sie die aktuellen Corona - Hygienevorschriften!

In its luggage hall, there were models, single, box and board layouts as well as digital demonstrations on display. Some visitors took the opportunity and even travelled some 600 kilometres to attend.

Plenty of autumn new products at Märklin:

The Zetties were well thought of on the occasion of the autumn novelties presented by Märklin in mid-September. Among them are models to be expected, but also real surprises. Foreseeable was the Christmas car 2021 (item no. 80631), which this year is dedicated to a blunderbuss in blue livery with Christmas motifs.

It is presented in a clear “delivery vehicle” with one half transparent blue with silver tinsel. With the enclosed ribbon, it can easily be hung from a Christmas tree.

The question of how Märklin would honour the anniversary “50 years of Intercity in Germany” remained exciting. Here, an IC 2 of the Deutsche Bahn is put on for the MHI, which falls out of familiar moulds and therefore requires some compromises.

On offer is a class 146^s electric locomotive (88485) and a four-car pack (87298) consisting of two DBpza 682.2 2nd class double-decker coaches, one DApza 687.2 1st class and the DBpbzfa 668.2 2nd class driving trailer. However, a fifth coach to add to a prototypically long unit does not seem to be planned, as we have the impression.



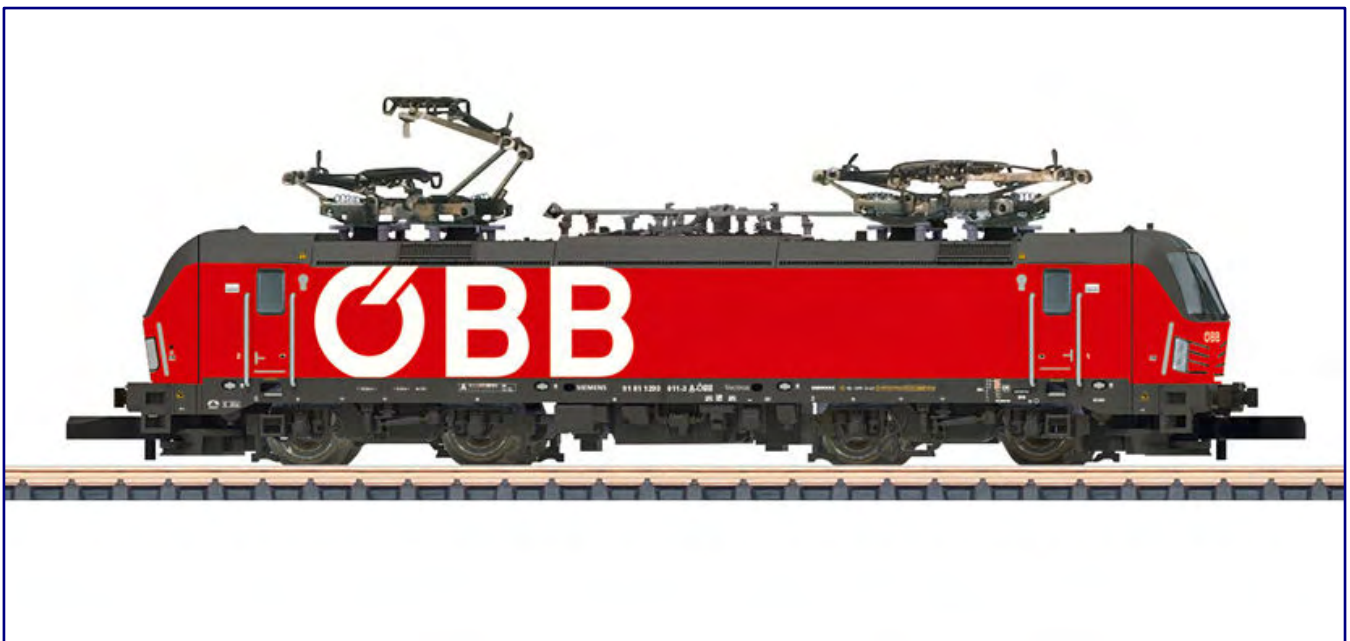
Unfortunately, the car pack for the IC 2 (Item No.87298) only includes four double-decker cars instead of the prototypical five. Photo: Märklin



The matching locomotive for long-distance traffic (88485) is offered separately and prototypically labelled as class 1465. Photo: Märklin

On 1 October 2021, the prototype for the new products of the class 101 electric locomotive in design for the DB Museum in Nuremberg (88678) is to be presented, which is to promote an exhibition starting at the same time that deals with the topic of design on and of rail vehicles.

Surprisingly, Märklin has added a third version of the Vectron long before the first two announced models are delivered: Following the example of the ÖBB, the class 1293 with four single-arm pantographs (88234).



The Siemens Vectron has not yet been delivered, which does not stop Märklin from announcing a third variant (88234) with the ÖBB version. Photo: Märklin

The Kittel steam railcar type CidT is now to roll in its probably most beautiful livery for the DRG (88146). The wine-red-light ivory livery really suits the vehicle and is underlined by a buffer plate warning coat.



The class 280 receives ivory-coloured trim strips as Epoch IV version (88804) instead of reproductions of the earlier trim strips in white aluminium version. Photo: Märklin

Diesel traction is covered with the class 280 multi-purpose locomotive for Era IV (88804). It then carries a three-light headlight and separate, red tail lights, but no exhaust muffler. The main difference to the Epoch III version for the Insider Club members are the ivory-coloured trim stripes, which were painted on the prototype after removing the white-aluminium-coloured trim.

Already announced and presented in the last issue was the diesel locomotive class 218 (88807) of the FIZ of the Deutsche Bahn AG.

Latest news from Panzer-Shop NL:

The Panzer-Shop NL (<https://www.panzer-shopnl.de>) says it has reason to celebrate. They are moving into a new building, which offers more space and a better working environment. At the same time, some new, also highly detailed vehicles have been added to the programme, which is why a visit to the manufacturer's pages is worthwhile.

And since an inauguration celebration with customers, especially from the international environment, is impossible, Kevin Boogard is making another offer to interested customers: Until 3 October 2021, there is a 10 % discount on orders of vehicles, figures and scene accessories, which will be calculated automatically.

All you have to do is enter the code "Umzug" in the shopping cart to activate it.

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Trainini

German Magazine for Z Gauge



New Oil Fired Steamer tested

**Layout Design for Beginners
Visit to the Märklineum**

Introduction

Dear Readers,

After two really “big ones” in August and September of this year, the edition in front of you is back to its usual size. And, so, it also allows us to get back into the usual rhythm of our publication dates.

In view of the many time-consuming events and the large number of topics with a close monthly connection, it was difficult to organise everything in such a way that nothing and no one fell by the wayside.

I would like to take this opportunity to explicitly praise all those who work with me on **Trainini®** month after month: In addition to further volunteers, these are regularly my editorial colleagues Dirk Kuhlmann and Joachim Ritter, as well as our translators Alexander Hock, Christoph Maier, and Martin Stercken.

Each of them is there when they are needed, and that is by no means just for the core task they have chosen in the team. It is simply fun to work in such a great team and I am always happy about the joint exchange in video conferences. That simply had to be said!

So, we're back on track and hopefully that largely applies to the world around us as well. The hobby season has started again, and Dirk Kuhlmann continues to give valuable advice to the newcomers and returners among our readers. Perhaps we will soon see the first results at the fairs and exhibitions that are planned again, in the meantime?

Two of our translators also had an outing, because they were among the first visitors to the new Märklin Museum, called Märklineum, and took a good look for us and for you. They received a friendly welcome from Märklin and were allowed to take a close look at the new layout. In addition, there were nice conversations, many exclusive explanations and also views behind the scenes. We would like to say thank you for that, too.

In order not to lose sight of anyone, the annual focus topic for 2021 is the series “Staying in touch.” This time, Philipp Meyer, a young small-series manufacturer with a “green thumb” for modelling, introduces himself. His striving for perfection is clearly noticeable in his portrait, and I hope you are as excited about his work as I am, after reading it. Perhaps you will find a new supplier as a result?

A lot has also happened at Märklin, now that we are talking about deliveries. We present some of them in the news of this edition, where you can also find the new products of many other manufacturers. We took a closer look at the steam locomotive models of the 41 Öl (Oil) and 042 series. The almost identical products appeared almost simultaneously for the Insider members and other customers. Of course, we found a few points of criticism, as you will surely expect – let us surprise you!

I don't want to give too much away at this point and simply hope you enjoy reading.

Sin-Z-erely,

Holger Späing



Holger Späing
Editor-in-chief

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We thank Philipp Meyer for his contribution, as well as Eisenbahnstiftung and Claudia Mühl for their photo support.

Date of publication of the German language version of this issue: 23 October 2021

Cover photo:

Shortly before 17:00, 042 096-8 passes one of the small stations along the Emsland line with a gas tank wagon train on a warm early autumn afternoon in 1976. The view is directed towards the forthcoming, last winter that the steam locomotive will be operated on the Bundesbahn.

Multi-purpose steam locomotive of the DB Success on the second Attempt?

In 2006, Märklin caused an uproar among the Zetties when the class 41 multi-purpose steam locomotive with an old-style boiler and oil tender was to be foisted on them as 042. 15 years later, the Göppingen-based manufacturer has taken a fresh look at this model and is taking its task seriously this time. And, so, we want to know how well he has done his homework, especially since insider club members and other customers will be presented with the new design almost at the same time.

In 1936 Schwartzkopff delivered two class 41 prototypes to the Deutsche Reichsbahn-Gesellschaft. Their running gear was a new development with an adjustable wheelset running mass of 18 or 20 tonnes for the first time, while the boiler was brought over from the class 03. Unlike the 03, however, the boiler overpressure was designed for 20 bar.

For the first time, the DRG had received a multi-purpose locomotive that could be used universally and which proved its worth. The two pre-series locomotives, both of which later remained with the DB, were followed by a total of 364 series-produced locomotives by 1941. However, after only a few years the St 47 K boiler steel used showed signs of fatigue, which led to the boiler pressure being reduced to 16 bar. Nevertheless, some replacement boilers had to be procured as early as 1943/44.

After the Second World War, a total of 220 units remained with the DB, 217 of which were returned to service. However, the boiler steel caused increasing problems that could not be solved with welded-in patches made of the usual St 34.

continues on page 6



On 27 August 1973, 042 166-9 waits at the Rheine depot for its next assignment. Photo: Wolfgang Bügel, Eisenbahnstiftung



41 254

Met. Dep.
18.9.67

The Bundesbahn could only buy time until a replacement boiler of a modern design was ready. In 1955, the boiler of 41 229 burst during the water pressure test at AW Braunschweig, which led to the prohibition of the patching by BZA Minden.

This was identical to that for the 03¹⁰ series: complete welding technology to save weight, missing feed dome, MV 57 mixed preheater system, superheated steam regulator, and a combustion chamber. A specific heating surface load of 75 kg/m² made it particularly evaporative. 41 331 was the first locomotive to receive this boiler in April 1957. Machines converted from 1958 onwards also received oil main firing, in some cases.

The exterior photo changed significantly with the conversion, the class 41 immediately looked much more modern: Instead of the sloping front skirt, a wide step could be seen under the smokebox, which ran around the corners to the side boards and formed a small step there. The DB reflex glass lamps were attached to it at the bottom, which kept the buffer beam free.



042 210-5, the former 41 210, passes through Salzbergen station on its way to the oil refinery in Lingen on 27 August 1973. Photo: Wolfgang Bügel, Eisenbahnstiftung

Photo on page 5:

41 254 from the Osnabrück Hbf depot arrived in Hamburg-Wilhelmsburg on 28 August 1967 with Dg 7103. After its restoration at the Bw Altona it will take over as Sg 5514 (Hamburg-Altona - Osnabrück Rbf). Photo: Wolfgang Bügel, Eisenbahnstiftung

The large smoke stack and the single dome on the centre of the boiler made the locomotive look slender, yet powerful and elegant at the same time. Although it came from the Reichsbahn's standardised locomotive programme, the class 41 now blended harmoniously into the photo of the new-build locomotives.

All in all, the newly boiled forty-one locomotives totalled 103, 40 of which were fitted with oil main firing. For a long time, they were to be the Bundesbahn's most heavily loaded freight locomotives; their monthly

kilometres were often between 10,000 and 14,000 km. The most important area of operation were the routes between the Ruhr area, Bremen, and Hamburg: the “famous runway”.

The 042 series at Trainini®
We also published a prototype article in issue 8/2006, to be found via the licensed archives or in the directory of the German National Library.
In the same issue, the earlier model 88272, which we criticised, was also presented, which was supposed to reproduce the class 042 for the first time.

Typical home depots were Kirchweyhe and Osnabrück, but they were also joined by Bremerhaven, Fulda, Göttingen, Hamburg, Hannover, Kassel, Uelzen, and Wanne-Eickel. The last home was to be the Rheine depot, where all of the 35 remaining oil-fired machines were brought together from the summer of 1971.

The re-designation on 1 January 1968 into the classes 041 (ex 41 coal) and 042 (ex 41 oil) was still experienced by 34 conversion locomotives with coal firing, which finally disappeared in 1971, and all 40 oil locomotives.

From the mid-sixties onwards, the tide had slowly turned for this series as electrification increased. Initially, the coal-fired locomotives had become dispensable and were now removed from the maintenance stock, while the oil-fired version continued to be fully maintained. By the time the 041 was retired, however, the stock of the sister series 042 had already been reduced by five units.



042 113-1 passes through Leschede station with Ng 17336 and a strong plume of smoke on 27 August 1973. Mineral oil and gas tank car trains, like this one in the photo, were typical services of class 042 on the Emsland line. Photo: Wolfgang Bügel, Eisenbahnstiftung

The final end of the steam locomotive came in Rheine in October 1977, and was still experienced by 042 096-8, Märklin's prototype for one of the two models presented, and for 042 113-1. From May 1976, the class was actually no longer in scheduled service, but was still often enough under steam to stand in for freight traffic or construction train service, when required.

The Rheine depot also liked to have passenger trains hauled by the fast 042, which until 31 May 1975 also had to stand in for the much faster 012, when one of the latter's very scarce stock was out of action. This versatility makes the shapely multi-purpose locomotive with typical DB silhouette so interesting as a model.

Märklin's new Model

After a faux pas in 2006, Märklin let fifteen years pass before the company dared returning to the market with a newly constructed locomotive of the oil-fired class 41. The Insider members, for whom the 41 356 (art. no. 88275) had been presented as the 2020 Insider Club model, were to enjoy the new model first.

Even before the model was delivered, the announcement of an Era IV version (88276) with the road number 042 096-8 also followed in the regular catalogue programme. Both versions were delivered shortly after each other in the summer of 2021, which is why they will be presented here together and in a comparative manner.



The models of the 41 356 (item no. 88275; rear) and the 042 096-8 (88276; front) appeared shortly after each other. Their essential differences are limited to the existing or dispensable buffer plate warning paint, and the Indusi magnets on one or both sides on the trailing frame.

Technically, both models are similarly closely related to the insider predecessor model of the class 03¹⁰, as the class 41 in old boiler design was and is with the two-cylinder class 03. This is due to the prototype and its character as a standard steam locomotive.

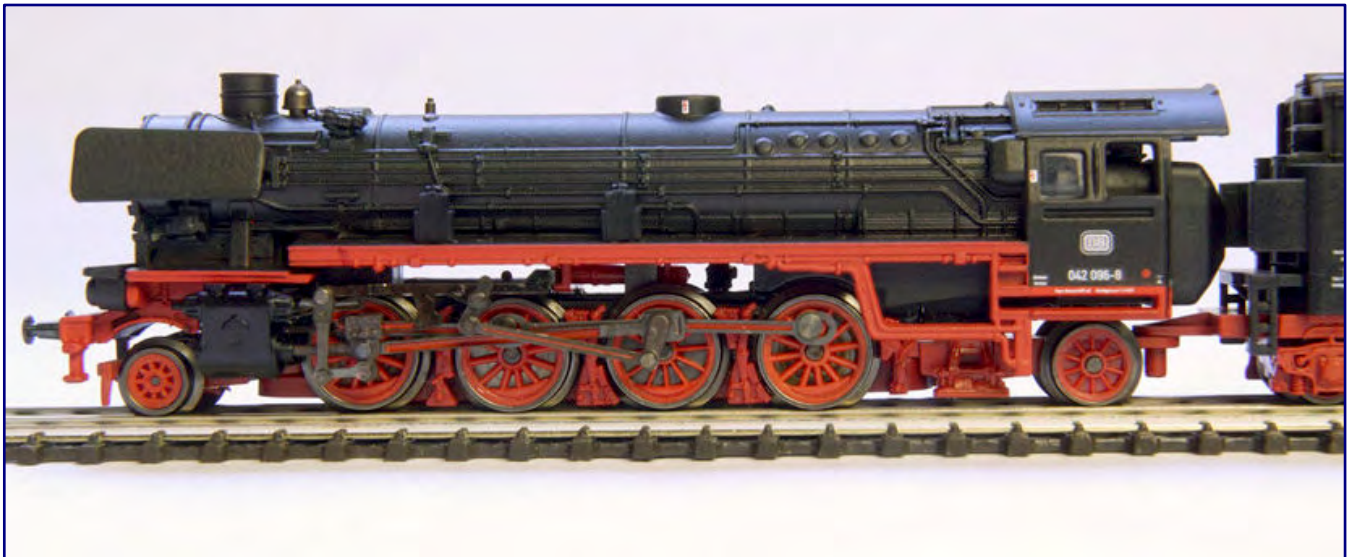
Both received the same new boiler in the prototype, which essentially reduces their differences to the different running gears. In addition, the tender has an oil tank instead of the coal box cover. But, also, the connecting and coupling rods show some differences, in this case.

The first impression when we take the models out of the box is good. The essential dimensions seem to be quite well taken, and also the proportions of the locomotive seem coherent. Only the oil tank protruding over the edge of the driver's cab roof does not want to match what prototype photos convey to us.

For the chassis, the familiar Mikado chassis was used, but it has undergone a fundamental product update: Converted to bell-shaped armature drive and LED lighting, it has now, also, received complete and movable detailed controls.

Adjustments to the wheels and cylinders also went hand in hand with this, and a new wheel gear cover was also due. This is now, as with all other revised models, made of plastic and reproduces the partly one-sided as well as scissor brakes prototypically. The forerunner, which is already available in the kit, has reproduced rail scrapers.

The rear bogie did not have to be redesigned either, but it is subject to minor differences in detail, depending on the model: In the case of the class 41 it is unbraked, carries no air reservoir, but has Indusi magnets. In the case of the Epoch III model this is one only on the right side, on 042 096-8 they are on both sides. This corresponds to the historical procedure of the German Federal Railways for locomotives that are mainly used in forward motion.



The distribution of the brakes on the individual wheels is shown correctly: Only the second coupled axle and the driving axle are supplied by scissor brakes on both sides. Only model 88276 shown here is equipped with a left-hand Indusi magnet.

Compared to the 03¹⁰ series, for which the new boiler forms were previously designed, the connecting and coupling rods was correctly adapted: At the height of the smokebox, it has a step under which the plate surrounding the front sits.

In the prototype, the rear part of the connecting and coupling rods system determined the maximum width of the locomotive. At this point the converted original dimensions have been well kept. Because of the depth of the detail control, the two steam cylinders protrude a little beyond this edge, as is the case with the other models with the new control.

However, we are now looking at the area that shows the only, but, unfortunately, quite noticeable deviations from the original: First of all, there are the sandboxes, which the DB had moved from the boiler sides to the round housings for new boilers.

They can also be found on the model of the class 41 ÖI / 042, but it doesn't take much knowledge of the prototype to realise that something can't be right here. On each side of the locomotive, we find two sandboxes for four coupling wheels – that doesn't work!

The correct number would be four, arranged in pairs above the first and third dome axes. In addition, they are also very eye-catching, because they are not sunk into the curve, but are reproduced standing on it. This was the case with the BR 03¹⁰ in the beginning, but not at any time with the class 41. It distorts the impression of the prototype in a disturbing way, which is confirmed by a comparison with our original photos.



The high sandboxes are disturbing, which are not correct in this respect as well as in number and arrangement on the connecting and coupling rods for the class 41 oil (compare prototype photos on page 4 and 6). The front also lacks a face-defining feature...



...which we have retrofitted in the form of the sloping boiler supports following the example on page 5 and painted red.

Since holes always have to be drilled subsequently on the casting and the attachments are made by plastic injection moulding, this conversion error is not comprehensible to outsiders.

Correct moulded parts could also have been used on other models of the 03¹⁰ series, the great importance of the 41 series should also justify the correct drilling templates.

Now, the express steam locomotive, which was so quickly taken out of service in the prototype, once again appears more correct, while the most universal of all standard steam locomotives has to live with a birth defect for a second time, which is definitely easier to recognise than just the one missing sand pipe of the old boiler version.

The attached front part, which reproduces the reflex glass lamps with light function suspended under the buffers, has also been adopted unchanged. Here, however, there is another difference between the 03¹⁰ and the 41, which could not be taken into account.

On the locomotive discussed here, two diagonally projecting support plates could be seen, which led behind the buffer beam in the direction of the lanterns.

Painted red, they would immediately catch the eye of any observer, but they are completely missing from the model. Self-help is required here, which can be achieved with suitably cut, then glued, and subsequently colour-contrasting, PS profile pieces.

The steam bell on 042 096-8 is also incorrect, because all prototype photos known to us from its time at the Rheine depot (1968 to 1977) show it without this feature. In fact, we have to add that we have not found a single photo of evidence that shows a steam locomotive of this class with this equipment at all. Here, too, the locomotives were obviously “copied” without a comparison to an original locomotive.



At least for 042 096-8 the equipment with a steam bell behind the smoke stack is not correct, a mistake that can be avoided without any problems because it is not costly.

For 41 356 we cannot refute or confirm the bell. Here, at least, it seems credible to us, because machines used on branch lines were also equipped in this way. Because of its adjustable axle pressure, the prototype was also quite suitable for this.

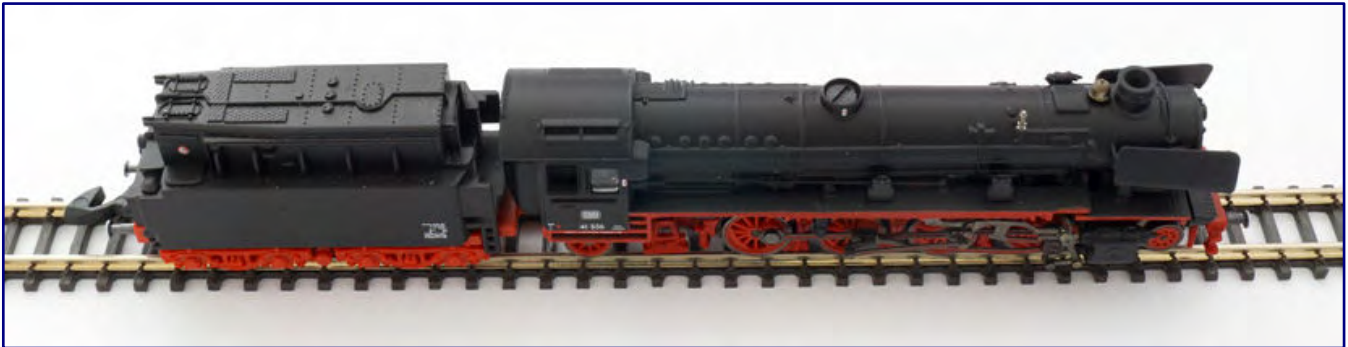
Taking measure, it says

When it comes to adhering to the dimensions, our measurements show what was to be expected. The essential data have been well adhered to, larger deviations only appear in the length over buffers and the overall axle position.

Checking the distance between the trailing axle and the first tender axle then confirms the suspected reason. Most of the oversize can be traced back to the too great of a locomotive-tender distance, which is due to running through small radii. A suggestion to Märklin here would be to provide the trailers with a second opening (for close coupling), which would also allow a narrower display case setting and incidentally eliminate the difference.

As with other Z gauge steam locomotives, the wheel diameters of the 042 series are somewhat too small, with the deviations being distributed differently among the individual wheels. The only quite clear outlier again concerns the trailing wheel (see table).

However, we do not consider diameters that are too small as measured here to be critical, because there is a plausible reason for this. In order to ensure operational safety, oversized flanges are required, and in Z gauge, this is 0.6 mm. In many steam locomotive conversions this creates space problems because coupling wheels are very close together, sometimes too close.



The very large locomotive-tender distance, which is the main reason for measurable dimensional deviations, can be shown particularly well from the bird's eye view.

The designer must then decide in favour of operational safety and accept an increase in length beyond the converted prototype dimension or prioritise appearance and risk a derailment-prone model.

A viable compromise lies somewhere in between and opens up if it is not based on factory-new, but largely worn wheel rims. This is exactly what Märklin's standard has been since 1972 and is recognised by us.

Data and dimensions for the series 41 ÖI / 042 of the DB:

	<u>Prototype</u>	<u>1:220</u>	<u>Model</u>
Length over buffers (LüP)	23.905 mm	108,7 mm	111,4 mm
Greatest width (diameter)	3.100 mm	14,1 mm	14,6 mm
Height above SE (steam dome)	4.280 mm	19,5 mm	19,2 mm
Total wheelbase	20.175 mm	91,7 mm	96,2 mm
Wheelbase of locomotive	12.050 mm	54,8 mm	56,0 mm
Wheelbase of loco/tender *	2.425 mm	11,0 mm	13,9 mm
Diameter of coupling wheels	1.600 mm	7,3 mm	7,0 mm
Diameter of leading wheels	1.000 mm	4,5 mm	4,2 mm
Diameter of trailing wheels	1.250 mm	5,7 mm	5,0 mm
Diameter of tender wheels	1.000 mm	4,5 mm	4,4 mm
Service weight**	175,7 t	---	39 g
Design	1'D1' h2		
V _{max}	90 / 50 km/h (forwards / backwards)		
Power	1.450 kW / 1.975 PS		
Conversion years (new boiler)	1957 - 1961		
Total number of units (DB)	40		
Retirement	1977		
* Distance from trailing axle to 1st tender axle			
** Fully loaded with fuel			

We now come to the Göppingen parade discipline, the painting and printing of the models. Both models are painted in the correct colours RAL 9005 deep black and RAL 3002 carmine red, the overall appearance is much more matt and pleasant than the early steam locomotive models showed.

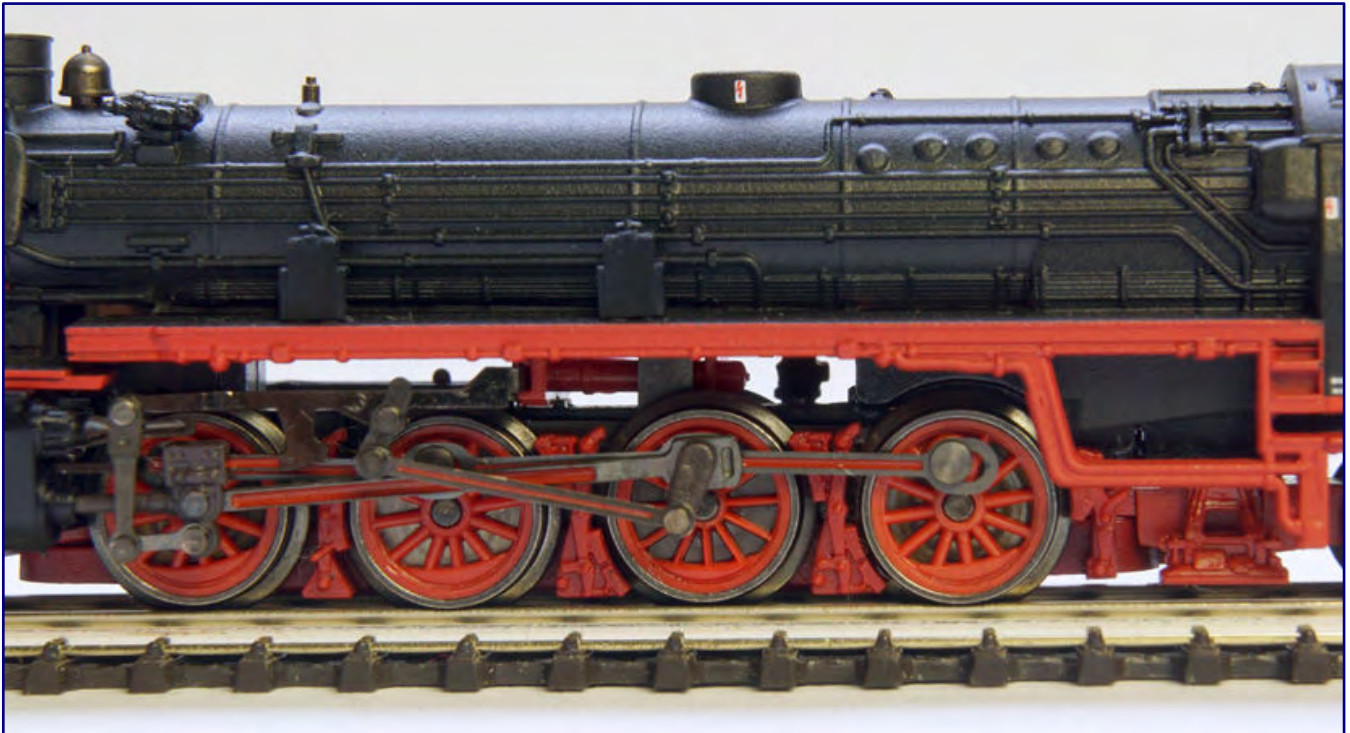
The plastic parts, such as the tender running gear, also show no perceptible deviations.

It should be mentioned in passing that the wheels of the driving axle do not have increased counterweights, but they do have much finer spokes, valid for both models, by the way.

Unfortunately, the copper wheels and the Indusi magnets show some plastic shine, but this is largely unnoticed in the overall impression.

More disturbing are the little things that make a big difference and yet are always left out: Once again, we are looking in vain for red buffer sleeves, and also the edge of the front attachment part was not highlighted in crimson. This is not good for the aesthetics, which is why we have to rework these parts ourselves.

The high bar of the class 01 from 2012, the 40th anniversary of the Z gauge, was not quite reached in the most recent steam locomotive conversions in another point – as is the case with the 41 ÖI / 042: The control carrier is not moulded to the body, and is painted in carmine red.



In the enlargement it is easier to see that the spokes on the coupling axle are considerably finer than on the three coupling axles. If this one wheel is a new construction, it remains an open question why the enlarged counterweight was not reproduced.

Instead, it is only indicated by a stamped part that reaches up to the slide track in front of the cylinder. In the area in question, only the black nickel plating comes into play; paint was also spared here. There are no further points of criticism here, wheels and control parts otherwise appear pleasant and unobtrusive thanks to the black nickel plating, because this corresponds most closely to the operating condition.

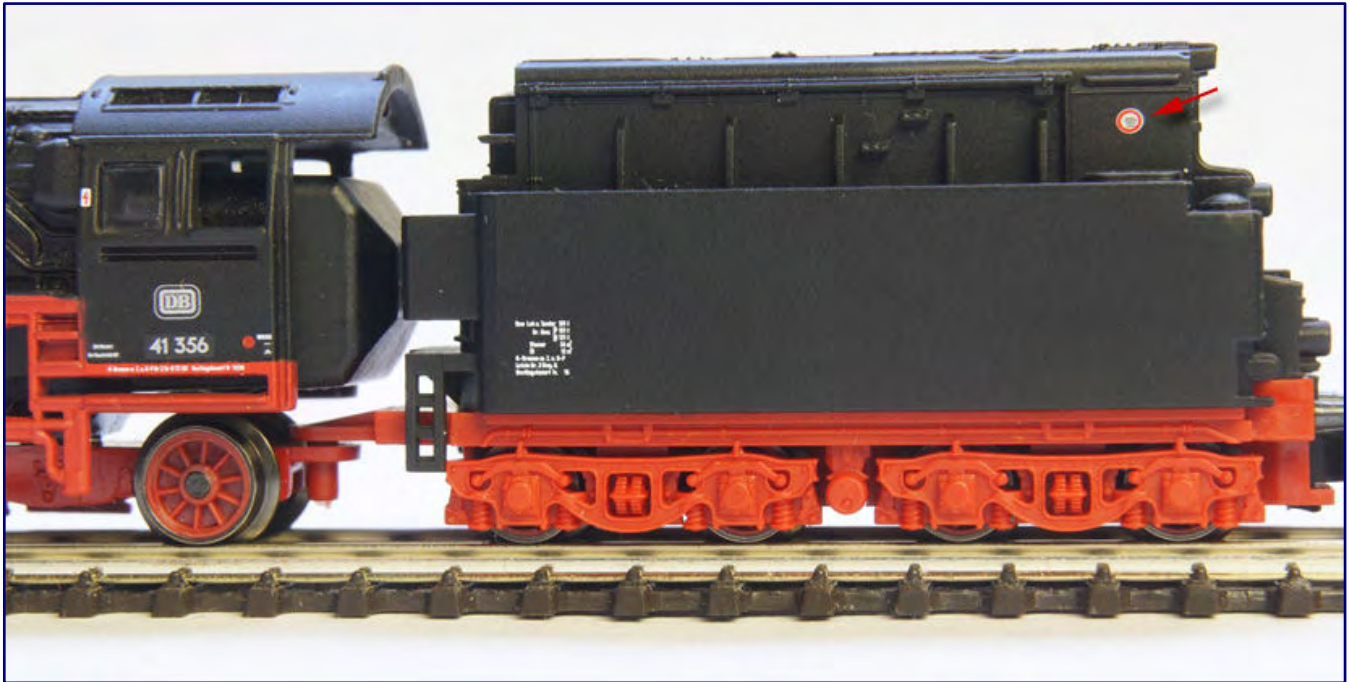
As expected, the printing on both models is impeccable. Here, individual deviations have also been taken into account. For example, the locomotive numbers on the 41 356 are printed in silver on a separate black underprint. In this way, the riveted locomotive plate of the time is taken into account.

On 042 096-8, the underprint has been omitted and the locomotive number printed in white, while the DB biscuit and home sign are also silver-coloured here. In this case Märklin follows the silk-screened signs that were issued for computer numbers from 1 January 1968.

41 356 bears an examination date "L2 Bwg 8.12.60" on the side and on the front buffer beam, which indicates the operating condition of about 1961. Correctly, this locomotive is, therefore, at home in the later dissolved BD Münster and assigned to one of the large strongholds of this class with the Bw Osnabrück Hbf. Its main area of operation was therefore the taxiway mentioned in the prototype section.

042 096-8 had only one home with this inscribed number, which can also be read correctly inscribed, because the prints are flawless: It is the Bw Rheine. The examination address "Bwg 26.5.71" also reveals its last, already started maintenance section.

The abbreviation "Bwg" stands in both cases for the former AW Braunschweig, which was the last one to examine steam locomotives. On 26 June 1974, 042 096-8 was there once again for an L2 examination.



As usual, all the inscriptions have been applied with great care. The warning sign on the oil tank (see arrow marking) is almost completely decipherable under the magnifying glass: "No smoking, bare lamps, or fires."

One last difference between the two models that should be mentioned is the warning paint printed on all four buffers on the Insider model 88275. The boiler misalignment known from the 03¹⁰ series at delivery due to a faulty shielding part of the lighting inside, seems to have been resolved, as we could not detect it in this case on either of the present models.

Technical evaluation

To record the electrical values, we first run in the models. A rather loud gear noise is clearly audible, which does not seem to disappear. However, it does not sound like badly working parts, so it is at least harmless.



The usual photo shows the tenders with finer running gear compared to the coal version. Apart from the running numbers, they differ from each other only by the buffer plate warning paint on 41 356 (rear).

The current consumption values also speak for good work. At transformer position 100, the models draw 16 mA, at 150 the value increases to 22 mA. This puts them within the usual measuring range of bell-shaped armature motors, as we had expected.

The lighting on the front side of the locomotive is by means of warm white LEDs and is active depending on the direction of travel. Märklin has dispensed with lantern lighting in the tender, as implemented on the 03¹⁰ series, in both cases. It is again pleasant that the top lighting is clearly visible and evenly distributed over all three lanterns immediately after starting up.

The starting characteristics themselves seem to be limited more by the 67011 transformer used, than by the drive. The models already start moving slowly at a track voltage of just under 0.4 V. We calculate 2.6 km/h prototype speed.

After that, the regulator range of the transformer makes a big jump to 2.2 volts, which let our model to run at 35 km/h. Tests show that the small steam locomotive can also safely drive through points at 1.0 volts on the track at the equivalent of 6 km/h. Both versions are, therefore, unrestricted. Both versions are, therefore, fully suitable for layouts, even if they race away at over 200 km/h at full track voltage, where only 90 km/h were permitted for their prototype.



As with some of the most recently delivered steam locomotives, on both models presented here the bell-shaped armature motor has been covered at the rear with a black cap in order to keep the connecting cables out of sight.

Finally, let's weave in some train formation suggestions, because here, too, the 39 grams of weight that this conversion brings to the scales do not seem to us to be an obstacle to prototypical use. By the way, 32 grams of this are allotted to the locomotive, which can influence the traction.

On the rolling road, the machines of the Bw Osnabrück Hbf and Kirchweyhe were often on the road with mixed goods trains, which can be put together as desired and varied from Märklin, and small series material. Tank wagons were also not uncommon there, which makes many ideas feasible for 41 356.

The same applies to 042 096-8 on the Emsland line, where in addition to many boxcars, a Pwghs 054 (FR Freudenreich Feinwerktechnik) goods train escort car could often be seen behind the locomotive. Mineral trains were also typical, because serving the Wintershall connection in Salzbergen was the job of this class.



For the Epoch III version of the steam locomotive, the Insider Annual Model 2020, we have selected a mixed goods train in which the wagons from the 86000 pack are used.

Another option are shorter trains of open wagons (main class sign E) and also car transport wagons (models by Märklin; announcement by Bahls Modelleisenbahnen), which determined the traffic to and from the VW factory in Emden. Sometimes there was only a single pair of cars behind the locomotive.

Until 1975, a few examples of class 042 could be seen on the Emsland line and branching off to Bad Bentheim (on to Hengelo) in passenger service in front of local and express trains. Typical sets were formed by four four-axle conversion coaches, Silberlinge (silver coloured passenger coaches), occasionally also combinations with centre entry coaches or single express coaches.

They underline the versatility of this locomotive type, which makes it so indispensable also for model railways, especially as it belonged to the very last of the Bundesbahn along with class 043. There is no way around this only multi-purpose steam locomotive from the standard programme, even in the model.

Concluding Summary

With its very free interpretation of the 042 series from 2006, which made us Zetties the laughing stock of other model railway enthusiasts, Märklin has set the bar for making amends very high itself. It seems very understandable that the manufacturer wanted to let the grass grow over this matter for so long.

It took fifteen years until we were allowed to hold a model in our hands on which the first impression also corresponds to the printed series. For historical reasons, we are now also very critical of this history, especially as we attach great and lasting importance to this model in the Z gauge programme.

All in all, Märklin has delivered a beautiful locomotive, but it cannot match the level of the class 01. There are mostly small things, but they add up to visible faults and should, therefore, be pointed out.



On the Emsland line, class 042 locomotives were often seen with tank cars. Since it does not always have to be a block train, we have added a matching example with the old Eva logo from pack 82326 behind the locomotive.

Märklin could improve this in future editions, but this would require another injection mould and changes to some tools. In order to match the overall character of the class 41 with new construction boiler, however, many things are actually indispensable. We remember that with the 44 series, for example, we had to wait many years for the reproduction of the typical middle cylinder.

Often, the customer could rework this with average craftsmanship, but in view of the asking price, which we certainly cannot call a special price, many would rightly consider this an imposition. At this point Märklin itself sets the bar high, only to break it.

We hope that our expectations will be shared in Göppingen and that a good locomotive will turn into a very good one. The sandboxes alone have a lot of influence on the overall impression! What will be remembered very positively are the good operating characteristics. Many will be pleased about this for exhibition operation, because the Mikado is one of the better pulling steam locomotives.



Despite individual weaknesses, we nominate the Insider Club model 2020, the 41 356 (88275), for the best new releases of the year 2021 in the category of locomotives.

Together with the successful illumination of the locomotive lanterns and once again excellent paintwork and printing, the joy clearly outweighs the joy. That is why the first delivered Insider Club model 88275 also receives a nomination for the best new releases of the year 2021 in the category locomotives. However, it will probably be difficult to assert itself there.

Manufacturer of the basic model:
<http://www.maerklin.de>

Other manufacturers for suitable freight wagons:
<http://www.bahls-modelleisenbahnen.de>
<http://www.fr-model.de>

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The first driving tests (part 3)

Green profiled is the Trend

The first step is always the hardest, as the old saying goes. To ensure that nothing is left to chance or can go wrong when starting out in landscape construction, our editor Dirk Kuhlmann provides important hints for our newcomers, and those returning to the hobby. But why reinvent the wheel? Valuable advice is accompanied by proven and still valid hints from our 2017 edition.

In the now third part of our small beginners' guide, we would like to introduce you to landscape construction and the first greens. Due to the diverse but highly interesting operations, we refer several times in this report to older editions of our magazine.

For many Z gauge railroaders, these instructions have become a veritable “bible” or even just a starting point for landscape construction.



A DSB local goods train (with empty E-wagons) is on its way back. Our small beginners' series is intended to enable anyone to create similarly appealing showpieces with deliberation and skill.

It should be said in advance that, if possible, such small showpieces should not have any gradients, as it would not be possible to install moderate ramps in a small space. Either way, it would not be particularly beneficial to this small gauge. With an appropriate terrain, however, you can very well simulate inclines and declines.

The basis of our landscape consists of 20 mm thick hard foam boards (XPS; extruded polystyrene), which you can buy in all DIY stores. Ordinary wood glue, several weights, a sharp bread knife, and a rasp are the basic materials for our work.

Right here, I would like to refer you to our edition 7/2017, where the construction and processing are explained in detail on the basis of the example “Rothenzeller Weg” from page 33 onwards. At the end of the article we have compiled this, and the following links to the file on our server. Of course, the editions can also be accessed via our licensed archives.



This is the framing of Schmiedehagen, here all the necessary work steps and materials are present in one picture. The timely inserted walls are also glued in place.

Take your time and start in advance with a few trials on a test board. Even with my experience, I go exactly this way with absolutely new and for me unknown subjects. This is the only way to practise new techniques and moves before things get “serious.”

We are now continuing to work on our small layout

On our showpiece “Upland” you can see exactly the steps of the “rough” landscape design in the small sequence of pictures. My experience has shown that our photos are usually sufficient as a construction suggestion for the interested model railway fan.

By the way, I always make stencils for the later streets, squares or properties out of normal paper. As soon as they fit, they are applied to thin plywood (from 1 mm). By instinct, the streets are shimmed with small pieces of “wood” and later covered with fine filler.

continues on page 23



Photo above:

This is the implementation of the plan from part 2 of our series. Hard foam boards form the basis for heights and depths of the landscape. The railway, on the other hand, will always run on a plain.

Photo below:

Here, the principle of stencil work for the streets can be observed very visibly. Again, all wall parts are already installed.



Photo above:
The first layer of filler has been completely applied and the first placement tests with the houses are now taking place. At the same time, test runs with rolling stock are taking place repeatedly.

Photo below:
Streets, squares and terrain are now completely installed on "Upland." Even the future background scenery is already in place in the framing.

Even on such a small piece of model railway layout, there are tiny but deliberate irregularities in the landscape. In any case, it is important to mention the now parallel work on the artificial structures. Even a bridge pier or a platform edge should be ready for installation, so that a homogeneous filler application is created during the later work.

With Molto's wood repair filler, this is achieved in an excellent way; it is a really good alternative to the old "plaster method". Moreover, the surface dries out sandy, which is very close to reality.

Of course, you can see some rather unknown buildings / model houses in the pictures now. This is what is known as "kit bashing": a construction principle in which completely new houses are developed from one or more kits.



Earth and sand now complete the substructure of the whole layout before we move to the "green area." All the houses are now in their final place.

With a little paint and ageing, it is usually no longer recognisable whether it is plastic, cardboard or metal. Don't worry, in the course of time you too will produce such unique pieces. This is a quite common way for many model railway fans, as soon as the experience increases and the desire grows for models that just not everyone has and are immediately recognisable.

Let's continue a little further, because for now we are collecting raw materials in the wild. I usually get my raw materials from many regions, so sometimes a wayside is examined more closely, for example, on vacation, or random samples are taken from a field. The "bagged" materials are then crushed and run through a sieve several times.

For our 1:220 scale, the tea strainer is the last station in the preparation process. Then the fine material is placed in an oven preheated to 200° C for 30 minutes. Finally, the material is then largely sterilised (germ-free), the body proteins of all life stages of insects and other soil organisms destroyed.

You can follow the rest of the process and even the design of a rock or the ballasting of the tracks in **Trainini®** 8/2017 from page 12. If you have now followed the design timetable exactly, then the complete “wet” work would be finished from this point on. The preparations for the landscaping can begin.



After repeated grass landscaping and the placement of small bushes, our model railway already looks quite natural. A photo taken against the light brings out the standing grass, particularly well.

The selection of grass fibres can possibly become a painstaking action, as in these days a large number of manufacturers offer these products. It is not only worth going to the dealer. A visit to the large trade fairs would also be worthwhile, because only there are most of the manufacturers on site. My choice years ago was the company “Mininatur” with its particularly natural green shades, but this is of course a matter of taste.

You also need an electrostatic landscaping device, and there is a wide range of options. A look in the forums or word of mouth is usually enough to decide on a suitable device. In our gauge, all makes are basically suitable, as grass fibres with a maximum length of 4 mm are laid.

The turf recipe

The manual is actually so comprehensive but highly informative that the **Trainini®** editorial team presented two extensive parts to its readers in autumn 2017. We had interviewed many Z gauge railway fans in the run-up and were amazed at the many responses we received.

I have now compared these two articles with the personal state of knowledge in 2021 and noticed that the instructions are still valid. So: let's get to work!

In our opinion, the exhibits created by many model railway modellers after 2017 were much more important, because a noticeably improved landscape design was the positive effect of our series of reports at that time.



That's nature: chaotic and unorganized. And that's how it should look on a model railway in order to have the best possible effect.

The results could already be seen in various specialised magazines and in the near future also at various exhibitions. The following **Trainini®** editions deal with the technique of perfect grass landscaping - good luck with your implementation:

- Part 1 in **Trainini®** 9/2017 starting on page 13
- Part 2 in **Trainini®** 10/2017 starting on page 24

Finally, I would like to give you a very important tip, even at the risk of repeating myself over and over again: Dare to go to a model railway club meeting in your region, usually, there is a lot of tinkering going on there, and almost all activities can be seen, regardless of gauge.

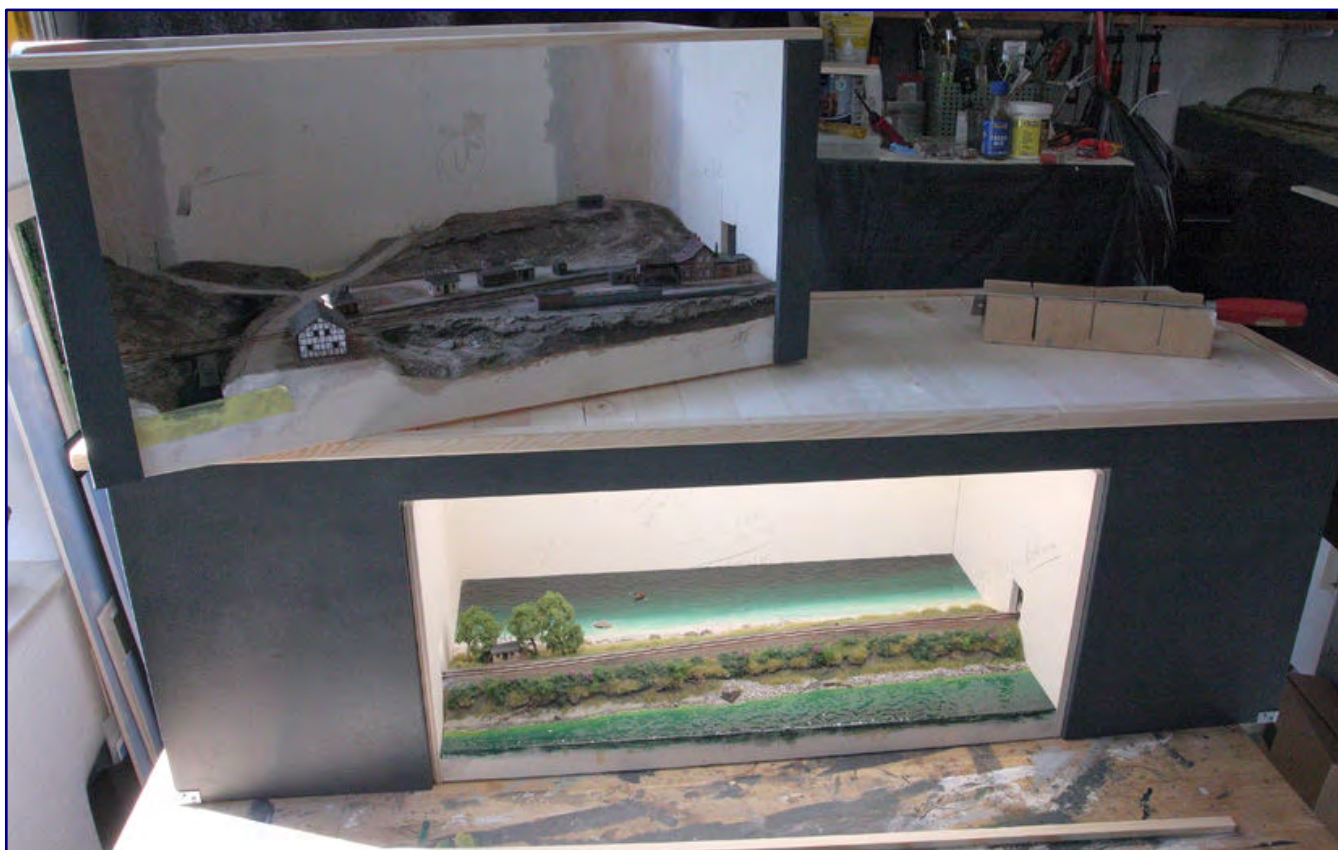
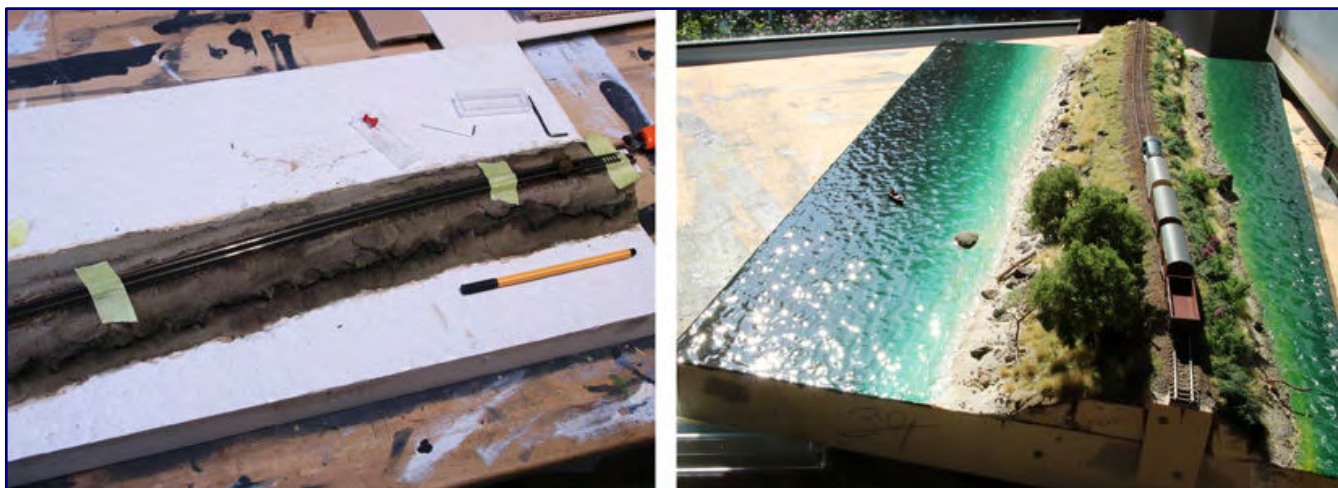


Photo above:
A second interchangeable module was built at the same time. This railway embankment is an excellent place to take pictures of various trains (see lead photo on page 20).

Photo below:
The box contains a bypass and allows continuous operation in the loop. Both track dioramas, our interchangeable modules, for the presented concept can be seen here in one photo.

I have been running the well-known model railway club FdE Burscheid e.V. in Leverkusen for some time, and here, for example, a small work series on the subject of water and landscape design has been running since late summer 2021, also in Z.

This exchange with professionals and “old hands” on the one hand, as well as beginners and restarters on the other, is already paying off for us. Besides very well-known H0-gauge layouts, there are also well-known exhibits of nominal size Z in our premises.

It is an indication of an old, yet also modern model railway club. And we are not alone! The 1:220 scale is simply an established model railway, today.



4:55 pm shows on the platform clock. It leads us to the many small details for a beautiful model railway. More about this in the next and last episode of this small series.

In the upcoming and final part, we will look at a little bit of the small stuff for the

- **Access to the issues referenced in the article**
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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Niedergang eines Traumberufs **Schwelgen in alten Zeiten**

Das waren noch Zeiten: Die Eisenbahn war das bestimmende Transportmittel im Alltag der Menschen. Hart arbeitende Maschinen und schmutzige Männer auf Führerständen prägten das Bild des deutschen Wirtschaftswunders. Die Bahn stand für ehrende Maloche, ein beherztes Anpacken und Schuften, wie es die Menschen im Ruhrgebiet nannten. Auch dahin zog es den Autor des neuen EK-Buches, der seinem Berufswunsch geradezu hinterhereilte.

Steffen Lüdecke
Traumberuf Dampflokheizer
Erinnerungen an Lehrte, Wedau und Rheine

EK-Verlag GmbH
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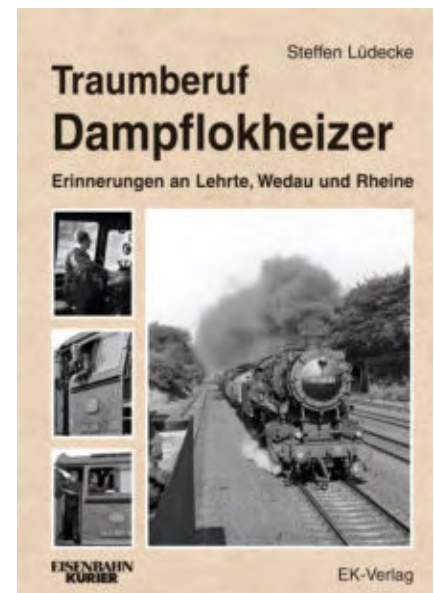
Das Brüderpaar Frank und Steffen Lüdecke gehört zu den alten Bekannten der deutschen Eisenbahnliteratur. Mit viel Sachverstand haben sie viele Titel und Aufnahmen beigetragen, die Wissen und Erinnerungen an die Zeit der Dampflok wachhalten.

Zu jener Zeit, in der ihre Beiträge spielen, waren Heizer oder Dampflokführer trotz der Schwere körperlicher Arbeit noch Sehnsuchtsberufe vieler Jungen. Der Autor des vorliegenden Buches, Steffen Lüdecke, träumt auch dann noch vom Dampflokführerstand, als die Idole der Jugend längst Rennfahrer, Astronauten oder Rockstar heißen.

1974 volljährig geworden und noch Pennäler an einem bayerischen Gymnasium, sollte sein Traum doch noch in Erfüllung gehen. Dank eines Tipps von Gleichgesinnten, hartnäckigen Bewerbens und Wohlwollens bei den DB-Verantwortlichen kann er in den Schulferien eine verkürzte Heizerausbildung in Lehrte absolvieren.

Doch Bedarf hat die DB schon lange nicht mehr an Heizern, denn der Traktionswandel ist fast abgeschlossen und die früheren Heizer werden zu Lokführern fortgebildet und sind künftig allein auf der Lok unterwegs. Eine Hilfsanstellung rückt in weite Ferne.

Als er fast aufgegeben hat, öffnet sich doch noch eine Tür und er kann in den Sommerferien 1974 ins Ruhrgebiet reisen, um im Bw Duisburg-Wedau als Heizer auszuhelfen. Hatte ihn zuvor die Erwartung an einen verdunkelten Himmel und qualmende Schlotte nicht einmal daran denken lassen, ins industrielle



Herz Deutschlands zu reisen, sollte es nun zeitweilig zur Heimat werden. Die Liebe zur Dampflok erforderte ihre Opfer.

Immer dabei war die Fotokamera, die Eindrücke und Arbeitstage im Bild festhielt, weshalb Steffen Lüdecke fast fünfzig Jahre später seine Erinnerungen mit seinen Lesern teilen kann. Wichtig für freie Tage war auch die Broschüre „Dampfgeführte Reisezüge der DB“, an die wir mit einem anderen EK-Verlagstitel erst vor wenigen Monaten erinnert haben.

Und so kommt es 1976 auch noch zu einer Reise nach Rheine, wo nur ein Jahr zuvor die letzten dampfgeführten Reisezüge der Bundesbahn einen Schlussstrich zogen. Ausnahmsweise gestand die Bundesbahndirektion Hannover dem Autor zu, eine kurze Zusatzausbildung auf Ölokomotiven zu erhalten und die erforderliche Prüfung abzulegen.

Wichtige Bilddokumente sind so entstanden, die Eingang in dieses Buch fanden und von Aufnahmen fremder Autoren ergänzt werden, wo es erforderlich ist. Allesamt sind sie handwerklich gut, perfekt reproduziert und mit Bedacht ausgewählt.

Ihnen kommt in diesem Werk eine besondere Bedeutung zu, wie schon am Verhältnis der Seitenzahl zu den Aufnahmen sehr deutlich wird. Und die Fotos beschränken sich nicht nur auf die Eindrücke aus dem Arbeitsalltag bei der DB. Wann immer es freie Tage gab oder ein außergewöhnliches Ziel angesteuert wurde, war der Fotoapparat dabei.

So konnte dieses Prachtstück der Literatur um viele weitere Motive angereichert werden, die beispielsweise einen Blick ins BZA Minden oder AW Braunschweig erlauben, aber auch Privatbahnmaschinen im Bild festhielten. All das soll aber nun nicht darüber hinwegtäuschen, wie gut uns auch die Texte gefallen haben.

In guter Erinnerung haben wir die Ausführungen des leider viel zu früh verstorbenen Wolfgang Staiger, mit dem unser Chefredakteur ein freundschaftliches Verhältnis pflegte. Auch er war als Student in Aushilfsheizerdiensten bei der DB, allerdings einige Jahre früher.

Und so gibt es hier etwas, dass diese Lektüre anders und einzigartig macht: Auch wenn nur wenige Jahre zwischen den Berichten beider Hilfsheizer liegen, scheint Steffen Lüdecke schon einer anderen Generation anzugehören. Seine Mitschüler reden vom Führer schein und Autokauf, träumen von der Freiheit, endlich tun und lassen zu wollen, was man will.

In diese Zeit fallen die kurze Heizerausbildung, die sechswöchige Hilfstätigkeit in Wedau während der Sommerferien und die abschließende Zusatzprüfung in Rheine, die den Autor auch dazu befähigt, später auf der Museumslok 41 018 tätig zu werden, um deren Erhalt gerade er sich persönlich auch verdient gemacht hat.

Diese Begeisterung ist zur Mitte der Siebziger, als die Dampflok ihre allerletzte Blüte erlebt, längst erklärungsbedürftig und so schildert der Verfasser zunächst eingehend, wie jemand seines Alters überhaupt Dampflokfreund werden konnte und auf Mitschüler mitunter „schräg“ gewirkt haben muss.

Solche Passagen werden vielleicht gerade auch jüngere Menschen ansprechen, die deutlich schwerer einen Zugang zu solch überalterter Technik voller Schmutz und körperlicher Anstrengung finden und eher Nostalgie mit der Dampflok verbinden.

Steffen Lüdecke nimmt uns alle mit auf eine spannende Reise vor allen Arten von Güterzügen, vor allem auf dem dichten Streckennetz des Ruhrgebietes mit einzigartiger Kulisse von Stahl- und Walzwerken, Zechen, Kokereien, Kraftwerken und Raffinerien. Und wir danken ihm seine Arbeit mit einer Nominierung für die Neuerscheinungen des Jahres 2021 in der Kategorie Literatur.

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• <http://www.eisenbahn-kurier.de>
• <http://www.ekshop.de>

Dampflokklexikon wiederaufgelegt Im Wesentlichen nichts Neues

Im Juni 2015 stellten wir ein Transpress-Buch gleichen Titels vom selben Autor vor. Der Buchnummer folgend, handelt es sich beim vorliegenden Buch um einen neuen Titel. Gespannt waren wir daher, was sich angesichts der abgeschlossenen Geschichte der Dampflokkentwicklung an neuen Erkenntnissen ergeben haben könnte.

Thomas Estler
Deutsche Dampfloks
Das Beste aus 180 Jahren Dampflokkbau

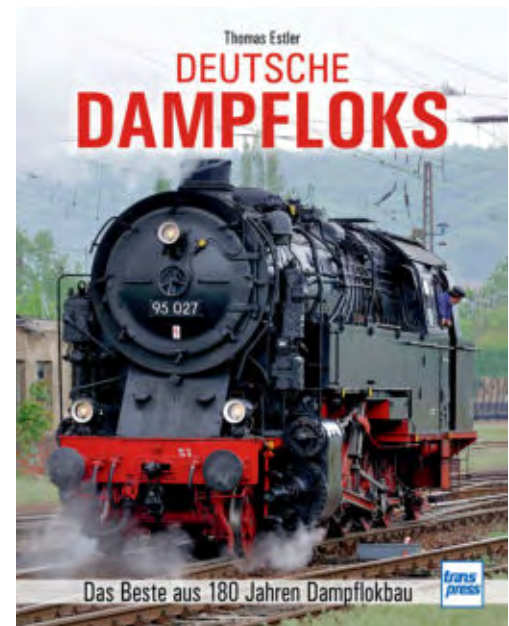
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Autor Thomas Estler ist durch und durch eisenbahnbegeistert, hat vermutlich deshalb auch beruflich viel mit Verkehrsplanung zu tun. Auch als Verfasser von Fachartikeln und Büchern ist er seit Jahren kein Unbekannter.



So erschien bei Transpress vor rund fünf Jahren auch schon ein Buch gleichen Titels, das er geschrieben hat. Da die uns nun vorliegende, neue Auflage nicht unter der alten internationalen Standardbuchnummer (ISBN) erscheint, sollte es nicht bei einem reinen Nachdruck ohne größere Änderungen geblieben sein.

Entsprechend vergleichen wir und stellen fest, dass die feststellbaren Änderungen einen derart geringen Umfang haben, dass ein vergleichbares Buch anderswo als geänderte Zweitaufgabe mit gleicher Nummer zu erwarten wäre.

Eine leicht feststellbare Änderung betrifft das Buchformat, denn Transpress hat die schmale Seite um exakt einen Zentimeter schrumpfen lassen. Dazu wurde das Bild auf dem vorderen Buchdeckel ausgetauscht und die Rückseite neu gestaltet. Um es nun eindeutig identifizieren zu können, bedarf es folglich einer neuen ISBN – der Verlag hat die einschlägigen Regularien also geschickt ausgenutzt.

Spannend blieb, ob sich auch im Inneren etwas getan hat. Mit der Auflage aus dem Jahr 2014/15 waren wir, gerade angesichts des erschwinglichen Preises, der sich auch dieses Mal nicht geändert hat, sehr zufrieden. Jede behandelte Dampflokk wird kurz, aber mit den wichtigsten Geschichtsdaten, technischen Merkmalen und Daten behandelt. Einzig die Reproduktion der Fotos war in Einzelfällen kritisch.

Und genau hier sind auch die beinahe einzigen Änderungen zu entdecken. Während sich die Gliederung und Gestaltung der Inhalte nicht verändert hat und auch die Seitenzahl konstant geblieben ist, gab es einen Austausch weniger Bilder durch bessere Aufnahmen. Zudem sind einzelne der zu kritisierenden Fotos etwas besser reproduziert worden als 2014.

Laut Verlagsangabe hat die neue Auflage rund 30 Fotos weniger als das alte Buch. Wir haben nicht nachgezählt, aber auch keines vermisst. Bei den Beschreibungen der Baureihen haben wir nur eine Änderung wahrgenommen: Bei den Schmalspurlokomotiven Baureihe 99²³⁻²⁴ der HSB ist ergänzt worden, welche der vorhandenen Maschinen heute noch im Dienst stehen – hier gab es zuvor keine Differenzierung.

Fraglich bleibt an dieser Stelle, für wen sich dieses Buch eignet? Wer den Vorgängertitel bereits im Regal stehen hat, kann auf einen Austausch gut verzichten, solange er nicht besonderen Wert auf den Bildertausch legt. Wer sich für Dampflokomotiven interessiert, ein solches Lexikon braucht und nicht besitzt, ist mit diesem Klassiker von Thomas Estler sicher gut beraten.

Es beschreibt sämtliche Dampflokomotiven, die in den Diensten der Deutschen Reichsbahn-Gesellschaft, der Deutschen Bundesbahn, der Deutschen Reichsbahn der DDR und der Deutschen Bahn AG standen. Erfasst sind Regelspur- und Schmalspurmaschinen. Nur Länderbahnkonstruktionen, die nicht mehr von der Reichsbahn übernommen wurden, fehlen ebenso wie wenige Typen, die nur auf Umwegen zur DRG fanden – auf unsere Besprechung aus dem Juni 2015 sie hier verwiesen.

Die einzelnen Baureihenbeschreibungen variieren in Länge und Umfang, was aber nicht störend ist. Darauf hatten wir auch beim letzten Mal hingewiesen. Eine erkennbare Systematik, nach der dieser Umfang festgelegt wurde, ist weiter nicht erkennbar. Für einen ersten Überblick reicht es aber auf jeden Fall.

Die Stärke dieses Titels ist ja nicht der fachliche Tiefgang der Informationen, sondern der möglichst umfassende Überblick über alle wichtigen Konstruktionen aus jener Zeit, als die Dampflok ihren technischen Höhepunkt erreicht hatte. Und dazu gehört dann mindestens ein Foto, um vom Bauzustand und Aussehen einen repräsentativen Eindruck zu vermitteln. Dem wird das Buch auf jeden Fall gerecht.

Nur die jüngeren Aufnahmen, die deutlich nach dem Zweiten Weltkrieg entstanden, lagen als Farbaufnahmen vor und konnten entsprechend wiedergegeben werden. Das ist gewissermaßen sogar eine Stärke, denn die alten Schwarz-Weiß-Fotos sind meist gut ausgeleuchtet und kontrastreich, Schwächen betreffen eher die Bilder aus der Zeit, als der Farbfilm noch in Kinderschuhen steckte.

Unveränderte Pluspunkte erhält auch das neue Buch für die Kurzvorstellungen der deutschen Lokfabriken am Anfang, das Erläutern des grundsätzlich noch gültigen DRG-Nummernschemas für Baureihen aus dem Jahr 1925 inklusive der Umzeichnungen durch DB (1968) und DR (1970). Es weiß also wieder Erinnerungen auch an viele längst vergessene Unternehmen zu wecken oder deren Bedeutung anhand der Produktionszahlen herauszuarbeiten.

Unser Fazit damals wie heute ist also identisch. Die Lektüre eignet sich gut als erster Einstieg in die Eisenbahn- und Dampflokgeschichte und wird sicher vor allem jüngeren Lesern eine große Hilfe sein, die mit der Dampflok nicht mehr aus eigenem Erleben vertraut sind.

Für Eigen- oder Umbauprojekte der Modellbahner eignet sich das Buch allerdings nicht, denn Zeichnungen und vollständiger Maßangaben wird der Leser hier vergebens suchen. Aber das ist auch wohl eher ein Folgeschritt nach dem ersten Überblick, der mit individuellen Baureihenportraits versüßt werden könnte.

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The world comes to Göppingen A visit to the Märklineum

The moment had finally arrived in mid-June: Märklin's new museum, the "Märklineum", opened its doors to the public. Directly on the grounds of its headquarters in Göppingen, the company takes its visitors on a journey through its more than 160 years of history. Our translators Alex Hock and Christoph Maier have already been there and report whether the visit is worth it.

By Alexander Hock and Christoph Maier. Märklin is showing some self-confidence: right at the beginning of the Märklineum exhibition area there is a large plaque with the sentence "From Göppingen into the world" with a reference to the fact that Märklin was already a "global player" from the end of the 19th century and exported its products to all parts of the world.

But the world also enjoys coming to Göppingen, as the visitor numbers at the biennial Märklin Days prove.

The former Märklin Museum, whose beginnings date back to around 1900, was also well visited with around 200,000 visitors a year.

But now the Märklin Museum has finally re-opened in a completely new building adjacent to Märklin's main factory.

On a total area of around 3,000 square metres, the company presents a cross-section of all epochs of its long history and a product range that once extended far beyond model railways.

In addition, the basement houses a large H0 scale exhibition layout, whose construction is already well advanced, but still far from complete.

Until further notice, visitors will not only be able to observe operations on the layout, but also gain an interesting insight into the techniques of layout construction.

A bistro and a well-stocked museum shop, which opened over two years ago, add to the experience.



Our translators Christoph Maier (left) and Alexander Hock (right) paid a visit to the new Märklineum at the end of July 2021, and were given an exclusive welcome.

continues on page 36



The Märklinmuseum welcomes its visitors in a modern and inviting building at Göppingen's Stuttgarter Straße (top). But the entrance via the inner courtyard of the factory also has a special charm: Immediately, one's gaze falls on the freshly restored 44 1315 locomotive, which is now a bit difficult to photograph because of its weather protection in the design of a Märklin train box (bottom).

But first things first: As passionate Zetties, we had of course heard that Märklin was working on a new museum building and that it was opened in June after some delay, partly due to the pandemic.

However, the fact that it would work out so quickly with a visit, and the two of us together at that, was a lucky coincidence. Although we are both originally from Germany's southwest, we have been living and working abroad for years, one in the United States, the other in Belgium.

During the coordination of the translation of the June issue of **Trainini®**, it turned out rather casually that we would both be in Swabia during the same week in July for family visits that were scheduled at short notice.



It is no coincidence that the interior architecture is reminiscent of a railway roundhouse. We are virtually on its covered turntable.

We quickly got down to business and booked tickets via the Märklineum website (see info box at the end of this article). A few days later the moment had come: a big hello and the first personal encounter, because until then we had only known each other virtually via **Trainini®** editorial video conferences.

We started our visit by taking a closer look at the impressive museum locomotive next to the museum's parking lot: A 1:1 scale class 44 with the operating number 1315. After an eventful service history from 1942 until its decommissioning in 1977, this locomotive was exhibited in Kornwestheim and Heilbronn for many years, before being acquired by Märklin in the summer of 2018, and transferred to Göppingen.

Externally restored and protected by a roof resembling the box of a Märklin railway model box, the locomotive now adorns the grounds of the Märklineum, creating a link between the real world outside and the miniature world inside.



View of the large H0-scale exhibition layout from the visitors' gallery. The concept of dividing it into different sections and structuring design and operation according to the different model railway eras is an interesting feature of this layout.

After that we proceeded to the entrance area of the Märklineum which follows the design of a railway roundhouse and also contains the large museum shop, which we refrained from visiting for the time being.

A journey through all eras

Our first stop after showing our tickets was the visitors' gallery, which is located above the large H0 scale exhibition layout. From there, one gets a complete view of the layout, which is housed in a 400 square metre hall in the basement.

A Märklin layout engineering team of about ten has been working on it since March 2019, unfortunately, with interruptions due to the pandemic. We were lucky and given a behind-the-scenes tour of the currently 160-square-metre layout by Christoph Rabel, who is in his dream job “Deputy Head of the Layout Construction Department.”

Many parts of the layout are still work-in-progress, but its dimensions, concept, and character are already clearly recognisable, and the landscaping is also well advanced. Train operations are also already taking place on a large part of a total of around 480 metres of track and in the five staging yards.

We particularly liked the basic concept of combining all six model railway eras on one layout. The sections of eras I and II are already almost finished, while work on eras III and IV still continues, especially on the detailing.

Sections covering eras V and VI will be added at a later date. The possibility of being able to present all eras on one layout was a key task given to the layout team as it will allow Märklin to showcase its latest models or prototypes across all eras.

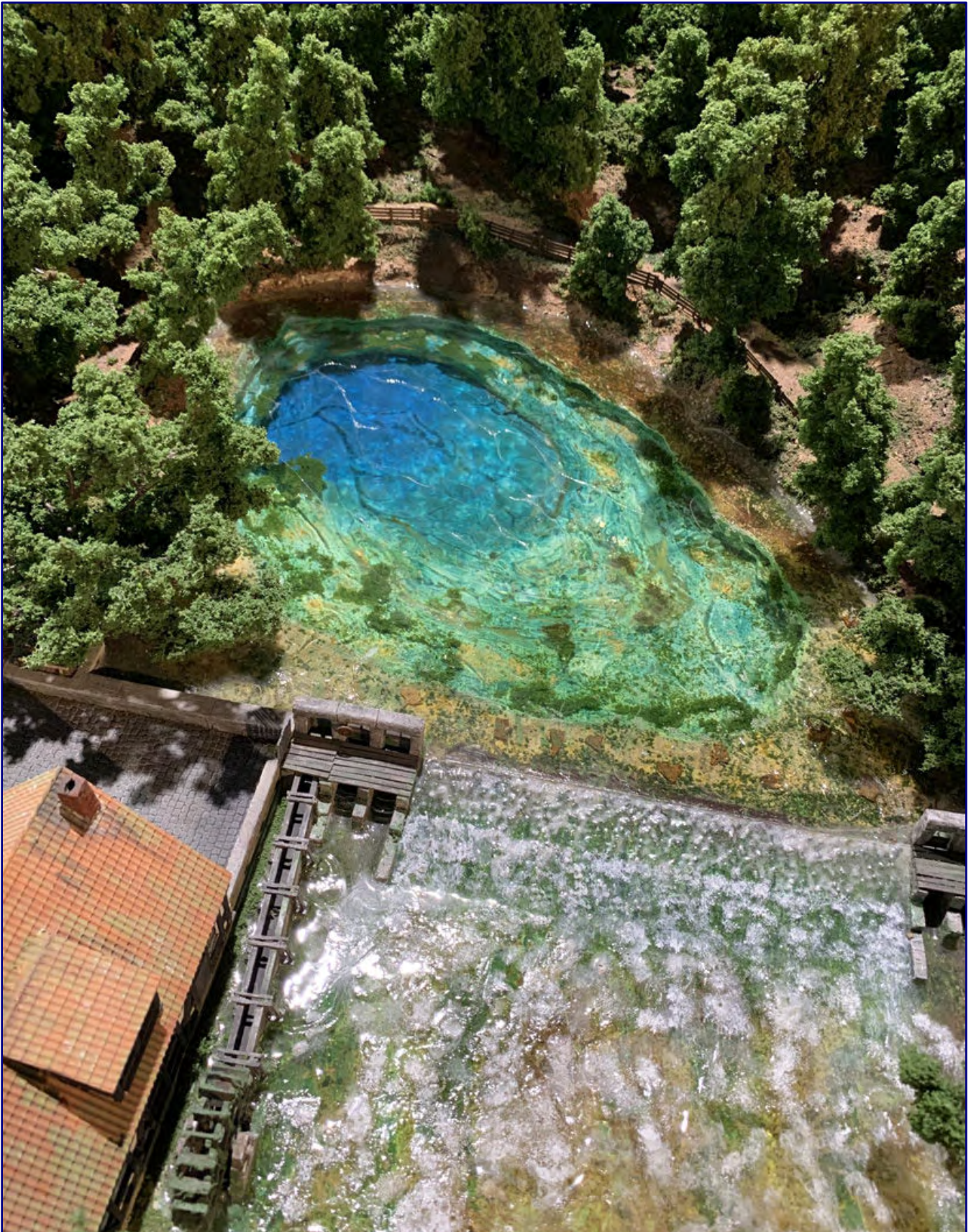


A man with a dream job: Christoph Rabel from Märklin's layout engineering team. He received our translators personally and gave them an exclusive look behind the scenes.

continues on page 40



While regular visitors have so far only been allowed to view the layout from the gallery in the background, Alexander Hock (in the photo), and Christoph Maier (behind the camera) were allowed up close and given detailed explanations about its concepts and construction.



The design began with the legendary “Blautopf” (“blue pot”), a karst spring in the south German municipality of Blaubeuren. The historic hammer mill can be seen in front on the left.



Many beautiful themes from southern Germany and around 185 years have been integrated in the exhibition layout (top). They also provide inspiration for one's own layout at home. Active train operations (bottom) are used to test the operational reliability of the layout and to identify possible weak spots.

This layout also convinces in visual terms: the colouring of the landscape motifs set in Germany's south-western state of Baden-Württemberg and their greenery are well chosen and make for a very harmonious overall look, as we can confirm as former residents of this region.

In addition, we liked the realistic, natural design of the landscape that surprises the viewer with many lovingly elaborated detail scenes, but without producing a visual overload. The long, harmonious parade routes built with flex track also contribute to this very convincing overall impression.

We also liked the honest admission that not all details of the layout were planned in advance, but that many ideas emerged "on the go". Some elements that did not turn out well during construction were discarded, scrapped and redesigned. Let's be honest: Many of us will have had similar experiences with their own model railway projects.



Already weathered buildings are waiting to be installed in the layout. Many kits from established accessory manufacturers are deliberately used here, because the exhibition layout is also intended to demonstrate and convey what a model railway enthusiast with average ambitions can create at home.

We also liked the fact that the landscape design, with all its ambition for realism and the use of modern techniques and materials, was deliberately kept at a level that a model railway enthusiast with an average level of ambition and skills could identify with and replicate.

The use of self-made materials such as the soil on the layout, which is ground and coloured from the bark of sycamore trees, also fits well into this picture. Scratch-built or kit-bashed buildings are also valuable sources of inspiration for the home layout. Apart from the satisfaction of creating something unique, this way of modelling also helps to save money and stay within budget.

The rear part of the layout rises sharply and provides space underneath for a large staging yard. Like the layout itself, it is built on several levels and allows for varied operations. Tidiness and organisation are essential aspects of wiring a layout; all cables are therefore neatly laid and labelled, as they should be on any layout: This is the only way to effectively trace faults or make changes.



Christoph Rabel explains the substructure of the large exhibition layout, the wiring and the multi-level staging yard. Alexander Hock listens closely.

Light and sound effects are also planned, by the way, but not yet ready for operation. They would also be a nuisance as long as the layout is still being worked on.

In our opinion, this is an attractive and successful facility that is up to date in every respect. It is all the more interesting to visit because it is still being built and visitors can follow the progress of the construction work directly.

The only downer for us Zetties, however, was that there was no Z gauge layout to be seen in the museum for far and wide. But more about that in a moment.

Journey through the company history

After we had forgotten to look at the clock while viewing the large display, we had to hurry a little in order to be able to devote ourselves adequately to the actual exhibition.

In a spacious hall on the upper floor, a journey through more than 160 years of company history was waiting for us. It is by no means limited to the topic of model railways, but also documents Märklin's development from a small factory for tin toys to a world-renowned producer of metal toys and model railways.

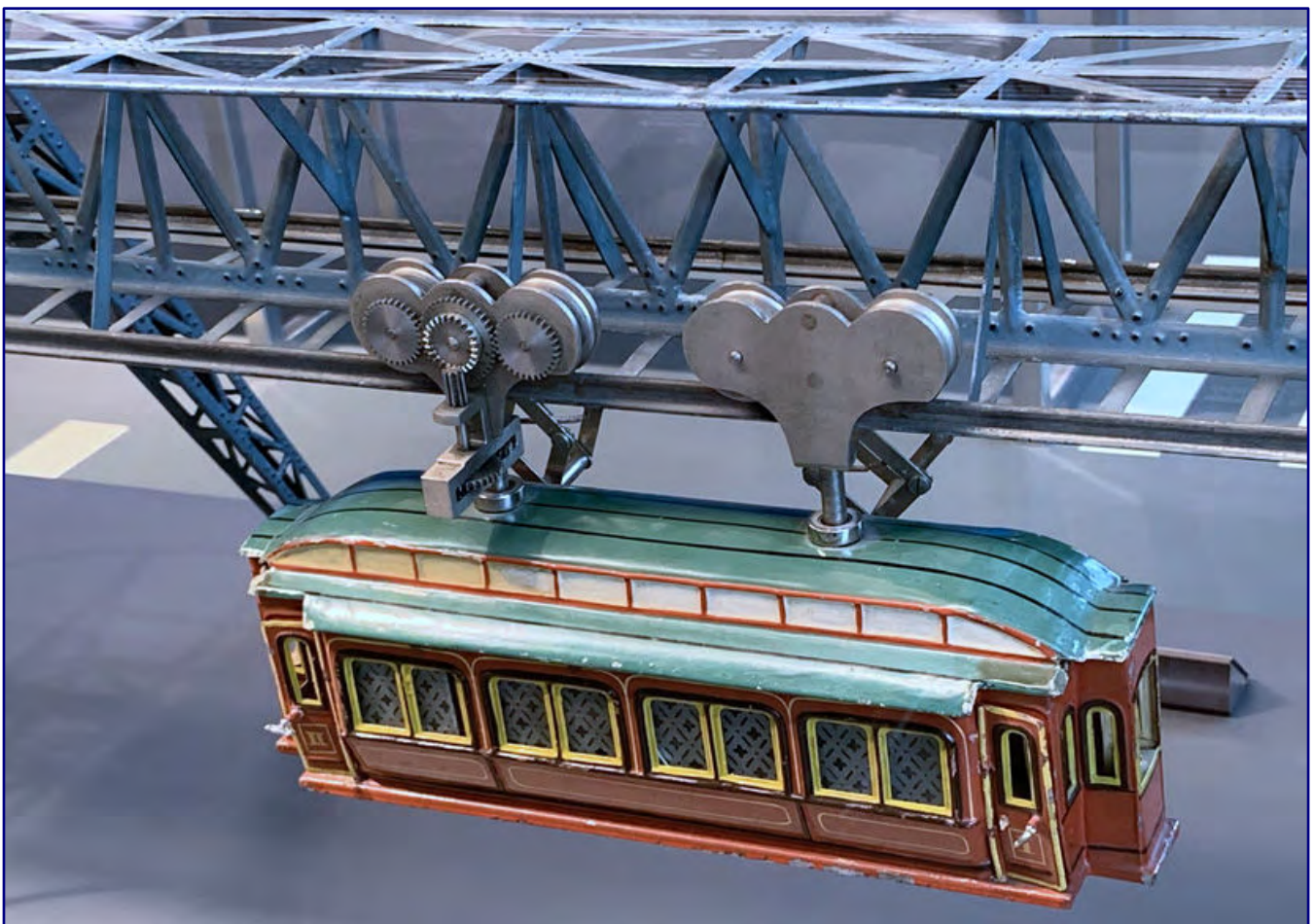
Numerous gems from the categories of tin toys, steam engines, metal construction kits and model railways, all in immaculate condition, are displayed in large glass vitrines. All pieces are in immaculate condition and one can only marvel and wonder at how they were able to survive the turmoil of time so unscathed. The exhibition is also enriched by numerous display boards and interactive technology.



Among the many exhibits of the Märklineum, the “Crocodiles” in all gauges ever produced must of course not be missing. After all, they are considered by enthusiasts to be Märklin’s “heraldic animal”.

A large part of the exhibition is devoted to model railways. From gauge 1 to 0 scale and H0 scale, the respective product range from the past to the present is presented in detail.

continues on page 47





A classic of Märklin history is the "Maxi" tin toy train for beginners in gauge 1. The "Rössle" ("horse"; top) embodies very well the idea of this long-discontinued programme, whose boundaries to the authentic looking railway models had previously become increasingly blurred. One strength of the Mini-Club scale is its potential for an almost uncompromising reproduction of prototypical scenes – we only have to think of the large and popular exhibition layouts at the beginning of the eighties. The Z scale model of Göppingen station (photo bottom) is a good example for that.

Photos on page 43:

Märklin Sprint as one of the legendary products from the time when slot-car racetracks had reached peak popularity must not be missing from the exhibition either (photo top). A model of the Wuppertal suspension railway embodies the era of tin toys (photo below).

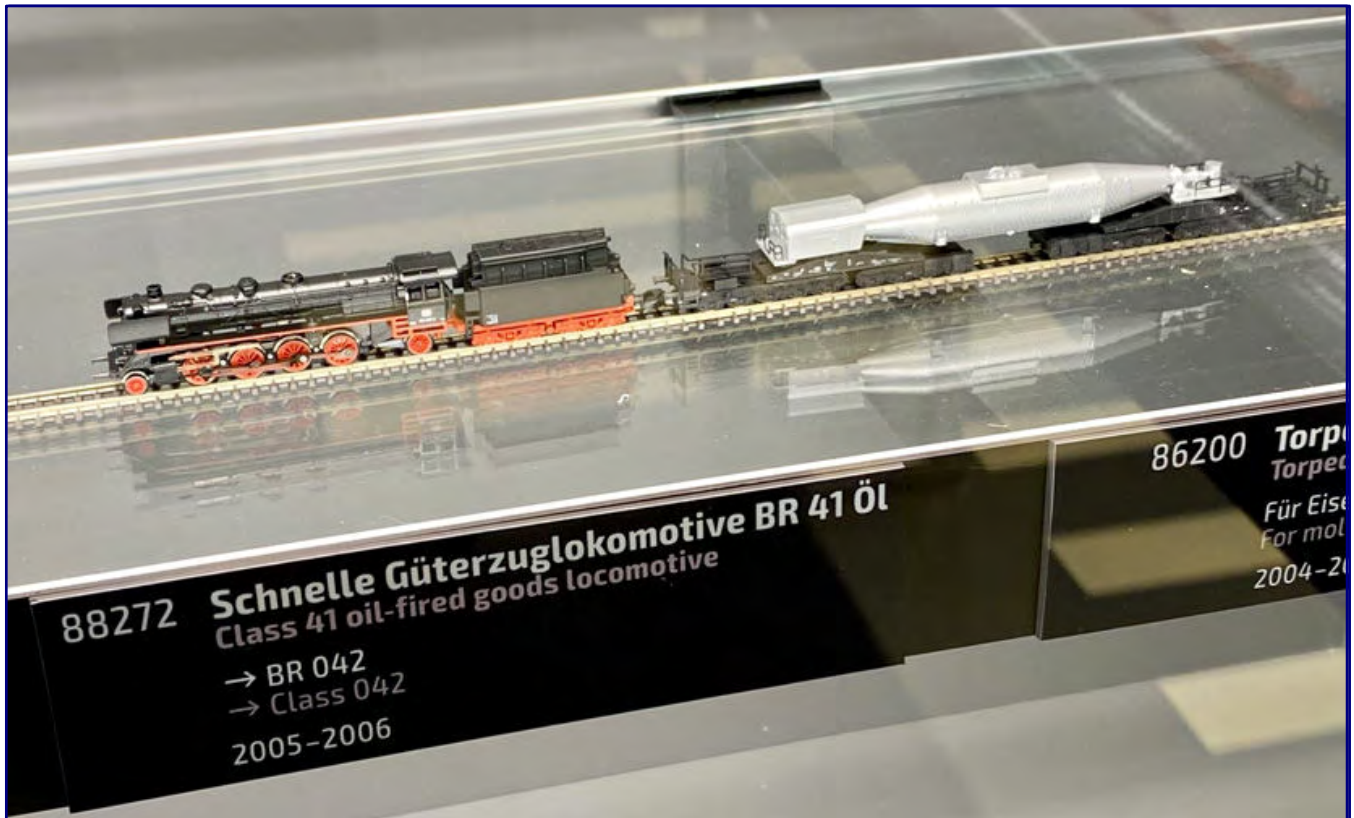


SPUR Z (MINI-CLUB) KREIERT EINEN SPEKTAKULÄREN MIKROKOSMOS
DIE KLEINSTE LIEBT GROSSE AUFTRITTE
Z GAUGE (MINI-CLUB) CREATES A SPECTACULAR MICROCOSM
THE SMALLEST LOVES A BIG ENTRANCE

One glaring omission from the exhibitions are Z gauge layouts. As the only exhibit in this category, Märklin is presenting a frying pan, of all things, in reference to historical competitions and as a result of a completely skewed public perception. Unfortunately, this perpetuates clichés that have long been outdated and carries the risk of exposing the most loyal of customers to ridicule in the model railway scene. We feel that this presents a distorted image of Z gauge.

Z gauge is also represented with an extensive cross-section of models that reflects its development from its market launch almost fifty years ago until today. Some people may become nostalgic when they discover one or the other locomotive that they may have had at the beginning of their own model railway career or that they would have liked to have but were never able to get.

However, many Z scale friends will leave this part of the exhibition with a somewhat stale aftertaste: Although Z gauge has long since outgrown its infancy, and the hobby is today pursued at a modern, often even the highest level, the museum presents this scale still in a somewhat clichéd manner.



There is also a large cross-section of the Mini-Club programme from almost fifty years on display. The largest wagon model of all times, the torpedo ladle wagon (art. no. 86200), should not be missing. Because of the current reference to the test in this issue, the first attempt at a model of the class 41 Öl (88272) should not be missing either.

In the context of the other photos at the beginning of the issue, it becomes clear why it was derided as a "very free interpretation of the class 042" and thus became part of the Mini-Club history. The completely prototype-free combination of an old building boiler locomotive with oil tender had led to massive customer complaints in 2005/06, after Märklin had previously still shown this caricature with a Pacific chassis of the class 03 in product photos!

Although Z scale (and we as practitioners) no longer need to shy away from comparison with the larger gauges, the curiosity and cuteness aspect of the 1:220 scale, which is actually believed to be outdated, is, in our view, overemphasized in the museum displays.

Thus, the only Z gauge exhibit on display in the museum that would remotely have the character of a layout is a frying pan layout. It is precisely this type of running trains that once made us the laughing stock of enthusiasts of the bigger scales. This is something Märklin should be aware of and which makes us wonder why it was not taken into account when setting up the exhibition.

In our opinion, this confirms the impression we also have of the Z gauge exhibition layouts that Märklin presents at trade fairs and which have long been outdated in terms of design and poorly maintained. To be frank: in this respect the Z scale inventor and market leader is running decades behind the state of art.

It would be nice, therefore, if Märklin would finally wake up, especially in view of the 50th anniversary of Z scale next year. The necessary layout engineering and design expertise undoubtedly exists, as the impressive H0 gauge exhibition layout described above proves.

Museum shop

We discovered that it is easy to spend a whole morning or afternoon in the Märklineum. At the end, we had just enough time to pay a brief visit to the museum shop on the ground floor.



The visit ends at the well-stocked museum shop. Our translators could not quite resist and left with some additions to their Z scale collections.

In addition to souvenirs and literature, enthusiasts will find a wide range of items from the current Märklin, Trix and LGB product ranges, as well as from well-known accessory manufacturers – admittedly, at recommended retail prices.

Bargains and second choice items can be found in the treasure trove, which has also made it into the new building. In the end, we couldn't quite resist and started our journey home with a few new additions to our personal Z gauge collections.

Visitor information:
<https://www.maerklineum.de>



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**ZEITGLEICH MIT
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Staying in the conversation (part 6)

The Avant-Garde of Model Making

Philipp Meyer knows only two goals: the highest possible realism and scenic perfection. These attributes are to be clearly expressed by his brand name Avantgarde Modellbau. Such ambitious goals can only be realised in small series. He describes to our readers his curriculum vitae as a model railway enthusiast, his passions and how he puts all this into practice in his product range.

By Philipp Meyer. My encounter with Z gauge began as a child on two tracks, so to speak, as I was able to take over mothballed Mini-Club railways from both my father and my uncle, both of whom were not very ambitious model railroaders.

What fascinated me most about Z gauge was the small scale. The nominal sizes H0 and N seemed too coarse, too ordinary, it just didn't suit me, and I had always been interested in the particularly small and fine things.



Philipp Meyer takes a very close look: a Japanese maple is critically observed in a Berlin park, a transposition into the model seems feasible to him. Small details such as the winged nutlets can significantly change the colour appearance of the tree, which would not be recognisable from a greater distance or with a purely photographic study.



The first larger diorama built entirely with self-developed vegetation was exhibited in Zell (Mosel) in 2009. The imitation of the grass is somewhat coarser and not quite to scale, which leads to a slightly grainy appearance.

It became clear to me very early on that the accessories market for the 1:220 scale was very expandable and that I was dealing with a thoroughly demanding size. The flip side was that I had to build in a gauge that was much less common.

And, so, I was busy with self-builds from a very early age, and, at the age of 11, I went to the Z gauge meetings in Bochum, looking for small series manufacturers who offered at least some of what had long been standard in the other gauges.

Even then I thought it was a nice idea to dedicate myself professionally to the production of model railway accessories later on, maybe to be big in a very small niche. At that time, however, the focus was still on technology, nature came a little later...

Formative, both positively and negatively for me, was the Silflor vegetation range that appeared around the turn of the millennium, which visually represented the gold standard of landscaping at the time. However, this only applied to the nominal size H0, for Z gauge the meadow mats with their distinctive, punched foliage leaves were not very useful.

Then the idea of small series production became a little more concrete and focused on the landscaping segment, which seemed to me to be particularly expandable at the time and also suited my steadily increasing interest in nature.

The first commercial developments appeared a few years later and were sold via internet auctions under the name "MoteWe Micro Flora", including toadstools, flowers, and various weeds.

Developing a complete, high-quality vegetation assortment, however, turned out to be much more difficult than expected. My aim was not to simply produce a few different green grasses, but to create a lively, highly detailed and fine miniature vegetation that was easy to work with mats and complete in itself.

Although I had a unique selling point with the reproduction of the spikes on tall grasses, for example, because this was not even implemented as a detail in H0 at the time, I had neither a significant demand for such detailed products nor the prospect of being able to produce them in series in a complete programme in the near future. So, this project petered out more and more.



Another test diorama was created for the last Spur Z weekend 2018 in Altenbeken. Here, you can already see the further developed meadows, plants and trees. In 2022, the construction of layouts and dioramas will continue. Photo: Dirk Kuhlmann

Interestingly, trees played only a minor role at that time, but now they came into focus in the search for products for which a noteworthy buyer market already existed: high-quality model trees on a scale of 1:87. The first products were spruces, which then actually sold well.

Spurred on by this success, the first spruces in 1:220 scale appeared, building on them: Z gauge had me back! I realised that there was more potential here than in the project of a comprehensive range of vegetation mats for Z gauge, which was overambitious for me at the time.

The birth of Avantgarde Modellbau

When the focus shifted to trees, the project name also changed to Avantgarde Modellbau, which was intended to show where the project was headed in terms of quality: to the top, ahead of the others!

In the meantime, the largest and most realistic tree assortment for the scale 1:220 has grown out of this. It includes more than 100 different types of trees in different heights, age classes and designs. Countless other tree types can be created at the customer's request in order to realistically recreate nature across the scope of the model.



The greatest amount of work on this old, gnarled oak is in the reproduction of the deadwood, the intergrowths, cavities and broken branches. As in the original, mosses and lichens grow on the trunk.

The beginnings with “bread-and-butter trees” such as spruce, oak, and beech were soon joined by exotics such as the extremely crooked Süntel beech or the loblolly pine, a North American pine species, to name just two examples. And every other excursion to a botanical garden or a park ended with the question: Can this or that plant also be represented in miniature?

Trees are, after all, complex structure, not only in nature, but also in the model. The problem is not to build something that looks like a tree even from two metres away. The challenge is rather to build a model that still looks particularly realistic, close-up.



Here, you can see a multi-stemmed small-leaved linden, a very old tree with lots of deadwood. Although the leaves are rather dark green, the tree appears yellow-green, which is due to the small yellow fruits and the associated bracts that change the colour appearance. This is also implemented in the model, as well as the typical trunk shoots, which, however, lack the fruits.

The shape and colour of the bark, the leaves, the location and environment of the tree as well as its branch structure determine how it looks in total and must also be recreated believably on a small scale.

In the beginning, the foliage was based on the (sometimes elaborately recoloured) foliage of the company Silhouette, formerly Silflor, but this was gradually replaced by various in-house developments.

The former material was still consistently abstract and differed mainly in colour, but in the meantime, I can draw on a wide range of different material that can credibly recreate the leaf shapes, colours and sizes of the original even in the small scale of 1:220.

Special mention should be made here of the textured foliage introduced at the beginning of the year, which once again significantly enhances the appearance of trees with large distinctive, lobed or slit leaves, i.e., especially maples, sycamores, and oaks.

continues on page 44



Photos above:

The leaves of the sycamore are up to 30 cm in size and, similar to the maple, pointedly lobed. On the left is a tree from early production with abstract foliage, on the right a plane tree with structured leaves, which is possible today. Both trees are about 9 cm high. Another detail that still stands out, even in miniature, is the variegated bark of this tree species.

Photo below:

The Virginia oak is a typical tree of the US southern states. It is distinctive because of the curtain-like hanging lichens that usually grow on it. It took several years before this tree could be converted to the nominal Z scale according to its own requirements.



The house parapet has also been further developed. A contemporary new edition is then also planned here for 2022.

Interestingly, these developments were ultimately “by-catch” in two projects that followed my ambition rather than business sense, because the sales market for French tamarisk in Z gauge is inevitably, very small. But sometimes things are worthwhile, that are not really worthwhile.

It is an ongoing process, because the entire range is in constant development, in which the above-mentioned representation of the foliage leaves is only one point of many. A next approach to be able to make the trees even more realistic and detailed is the structure of the leaves on the individual branches.

This approach may not be particularly efficient from a business point of view, especially when combined with a very extensive product range and the company's own desire for perfection, which pulls and pulls on delivery times.



This horse chestnut, too, is already bearing the current foliage, which knows how to reproduce the delicate leaves and their colour in the best possible way. But it does not yet bear fruit.

However, that's what I found so appealing about small-scale production; not putting the economy in the foreground, but doing what you like and what you enjoy. I am still researching and developing the meadow mats, but currently only for my own extended needs.

Another project on my list is professional layout and diorama construction. However, the focus is not on the railway, but on the landscape: railway traffic in a rather minimalist and reduced presentation, but then with colour plays and motifs off the beaten track, with the highest detailing of the landscape itself.



The diorama exhibited in Altenbeken in 2018 shows the highest art of design in every corner. Also in the photo here are the spruces, which are a factor in the success of Avantgarde Modellbau's range. Photo: **Trainini®**

In order to finally be able to put the vegetation that has been developed over the years away from the series productions into the ground appropriately, I need such things particularly urgently. Then everything will fit into a beautiful overall picture, realistic of course, just the way I like it, and it is important to me personally.

That will be tackled next year. I am looking forward to it.

All unmarked photos of this article: Philipp Meyer

.....
: **Manufacturer's pages and source of supply:** :
: <https://avantgarde-modellbau.beepworld.de> :
.....

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Series 120⁰ of the Intercity (Trainini® 9/2021):

Thank you very much for another very readable magazine. However, I would like to contradict the thesis on page 19 that the BR 112 was not used in IC service. In the summer of 1985, I used the IC "Oldenburg-City" or "Weser-City" between Hannover and Bremen at weekends.

The locomotive at that time was the BR 112, if you enter the mentioned IC in search engines, numerous photos appear that prove this: with one or two 1st class coaches as well as four 2nd class coaches (Bm and Bpmz), short, as befits a model railway (there were no dining cars, only a minibar).

Measured against the total passengers per km volume in IC traffic, the 112's share of the service was probably marginal, but it was still used as scheduled.



120 002-1 was used repeatedly in summer 1985 in front of Intercity trains between Hannover and Bremen. Photographed here in Bremen Hbf. Photo: Jörg Endreß

By the way, on Sundays I had the special pleasure of seeing a locomotive of the BR 1200 in service. The driver(s) made full use of the locomotive's acceleration capacity, so that one was literally pressed into the seats when starting up; the 112, on the other hand, accelerated like a lame duck.

Hannover was then reached several minutes ahead of schedule, which then had to be stalled because (presumably) the platform was still occupied. Once I took my camera with me and took a photo, which I attach (scan from the paper picture).

I often continued my journey with an IC (...) in which the ABvmz 227 mentioned on page 45 was used, which had aroused my curiosity because of its appearance and was much more comfortable than a local coach.

The locomotive here was a 111 (also in S-Bahn livery). The train came from NRW and unfortunately already ended in Hildesheim. I only realised that it was a prototype many years later. All the best and thank you for providing us Zetties with up-to-date and well-founded information.

Jörg Endreß, Bremen

Editor's reply: In the article about the fifty-year history of the Intercity in Germany, we made a badly abbreviated reference to the use of the class 112, and, therefore, referenced it in the wrong context. This class was not originally planned for the trains of the IBS (concept "IC '79"), but very much so with subsequent changes to the system from 1985. Here, it was then used, for example, on the new line 4A, which our reader rightly points out.

The Märklin deliveries for October:

At the beginning of the month, the tank car pack (item no. 82326) from the 2021 spring novelties was completely delivered to the trade. It contains three two-axle old building cars with brakeman's platforms in different paint schemes.



The three newly delivered tank cars (item no. 82326) will attractively enrich mixed goods trains as splashes of colour, and/or once everyday representatives.

The two blue examples are labelled "Hobum" and "F. Thörl," the third example in grey comes from the Eva stock. All three originals are employed by the Bundesbahn and belong to epoch III.

The paintwork and lettering look very appealing, which is why the trio is certainly a successful addition to goods trains. The only possible improvement would have been to also paint the attached ladders with the platforms instead of only spraying them in the wagon colour. Thus, from some perspectives a minimal plastic sheen can be perceived.

Also, arrived at the dealers is the supplementary track construction pack (86572), which contains a Krupp Ardelt crane together with protection wagon and supplementary low side wagon of the former class X 05. All three cars are painted RAL 6020 chrome oxide green, and are lettered according to Era IV.

The protection wagon based on the model 8610 from 1972 has no steps in the middle anymore, the short low side wagon has a load adapted in length, which long-time Zetties might know in a similar design from an earlier Railex edition.

Two diesel locomotive models have also become available: The "Ludmilla" of class 232 (88136), lettered for the Deutsche Bahn AG and still painted in the DR colour scheme, has received a metal-filled plastic housing. This has increased its weight to a total of 53 grams and it is ideally equipped for heavy traction tasks, especially as it is also powered by a bell-shaped armature motor.



The now available class 232 diesel locomotive (88136) has gained a lot of weight thanks to a housing made of metal-filled plastic. However, extreme caution is advised, when putting the model back into its box so that the buffer sleeves do not break!

The template for the model has already received some modernisations: The higher Siemens fans can be seen on the roof, square instead of round buffer plates are emblazoned on the buffer pads. Painting and printing are impeccable and extensive, only the front step is not coloured.

When putting the model back into the deep-drawing insert of the packaging, caution is called for: The model sits very tightly, in our opinion too tightly, in the insert, which is why there is a risk of the buffers breaking off if it is only inserted at a slight angle and then pressed on.

Very well received was the diesel locomotive 218 497-6 of the FZI Cottbus of the Deutsche Bahn AG (88807), painted in two colour schemes, which is already sold out at the factory, but to our knowledge is still being produced in two batches. One half is designed in the colours of a steam locomotive, the locomotive number is framed and resembles a screwed-on sign. The sponsor's address from Piko can be seen on this half.

The opposite side has the design of DB Fernverkehr, and the logo of the Märklin company. The separating edges of both paint finishes run diagonally over the sides and over the tank and roof. The mounted hoods



Due to the great demand, the 218 497-6 of the FZL Cottbus (88807) has to be issued a second time. Hopefully Märklin will take advantage of this to also correct the selection of bogies.

are reproduced in metal colour as on the prototype. Because of the system coupling, the mounted snow ploughs of the prototype locomotive could not be reproduced.

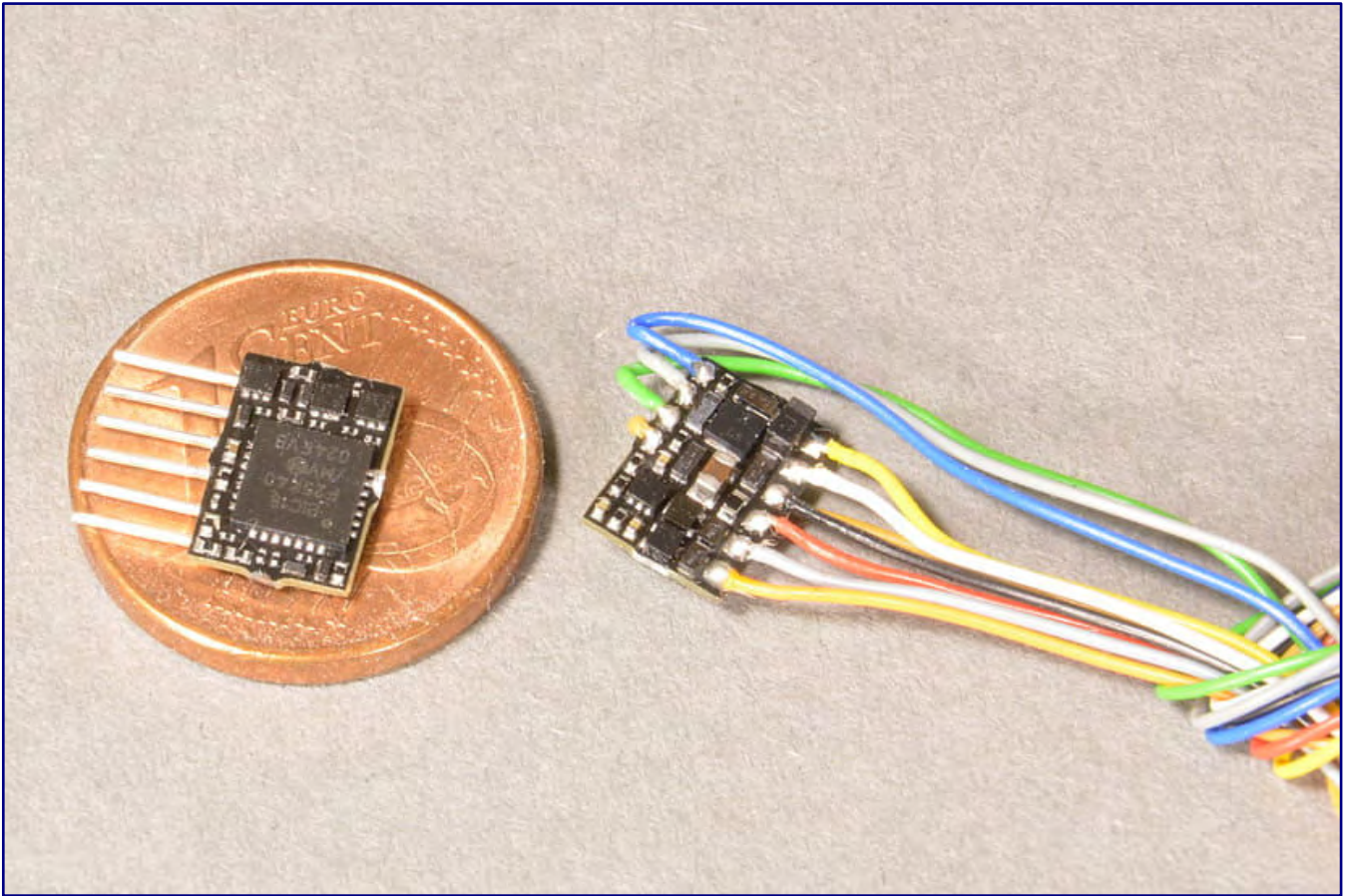
Technically, the model is up to date with the current product maintenance. Unfortunately, it is not without faults, which is almost "Märklin-normal": The old bogies with coil springs were selected from the existing moulds, which the first construction lots of the 218 series had taken over from the 216 – 218 497-6, however, belonged to the last one.

The correct design of the substructure series 218⁴ with Meggi-suspension and different shaped sandboxes has been available in Göppingen since the first appearance of this series. The H0 scale model shows how it should look correctly. Unfortunately, this mistake has been made several times in the past.

New small decoder from Zimo:

Zimo (<http://www.zimo.at>) has always had small decoders in its range. In 2020, this manufacturer built a new production line that can now also assemble components with the component configuration 01005 (0.4 x 0.2 mm). One result is the new MX615 miniature decoder, which measures just 8.2 x 5.7 x 2 mm.

Despite its small size, it even has four amplified outputs, while comparable decoders from other suppliers only have two outputs. Its configuration features are the same as those of the larger small decoders. This also applies to the four outputs, which even in small locomotives easily allow a separately switchable as well as dimmable peak and tail signal. And Railcom, such as the ABC and HLU braking methods, are also available.



Two of the connection variants of the new Zimo mini-decoder MX615 described in the text can be seen in this photo. Photo: Claudia Mühl

The total load capacity is somewhat lower, but at 0.5 A it is perfectly adequate for Z gauge. As connection variants this decoder is available with the six-pole pin header according to NEM 651 (item no. MX615N), with the NEM 651 pin header on wires (MX615F), with the eight-pole NEM 652 interface on wires (MX615 R) and loose wires (MX615).

New seasonal products from Noch:

In its freshly announced seasonal new products for winter 2021, Noch has also listed five vegetation starter packs that are useful across scales for Z gauge as well. Matching the respective product theme, they contain lots of grasses, tufts, gritting material and usually also gravel, rubble or sand with a 10% price advantage.

The following sets are offered: "Wiese" ("Meadow"; art. no. 60771), "Im Wald" ("In the Forest"; 60772), "Am Wegesrand" ("By the Wayside"; 60773), "Am Ufer" ("On the bank"; 60774), and "Bahndamm" ("Railway Embankment"; 60775). The coordinated materials are intended to help beginners, in particular, to create their first works in an appropriate manner.

The quantities supplied are sufficient for smaller areas. Additional purchases are possible at any time, because the article numbers from the individual programme are also given.

Reissues at Spur Z Ladegut Küpper:

Popular inserts for the three pa containers of the BT 10 carrying wagon from Märklin have been reissued by the Aachen-based specialist Spur Z Ladegut Josephine Küpper: Coal (Item No. 82329-K), ballast (82329-S), and broken bricks (82329-Z).

As these products are only available in the range for a short time, it is advisable to take a quick look at <https://spur-z-ladegut.de> if you are interested.

New Herpa accessories for the airport:

The air jet deflectors (item no. 571586) announced a few months ago have now been delivered. These protective fences also convey more safety on airport dioramas and facility sections. Their models are intended to protect individual areas from the hot turbine exhaust gases of the aircraft on the apron.



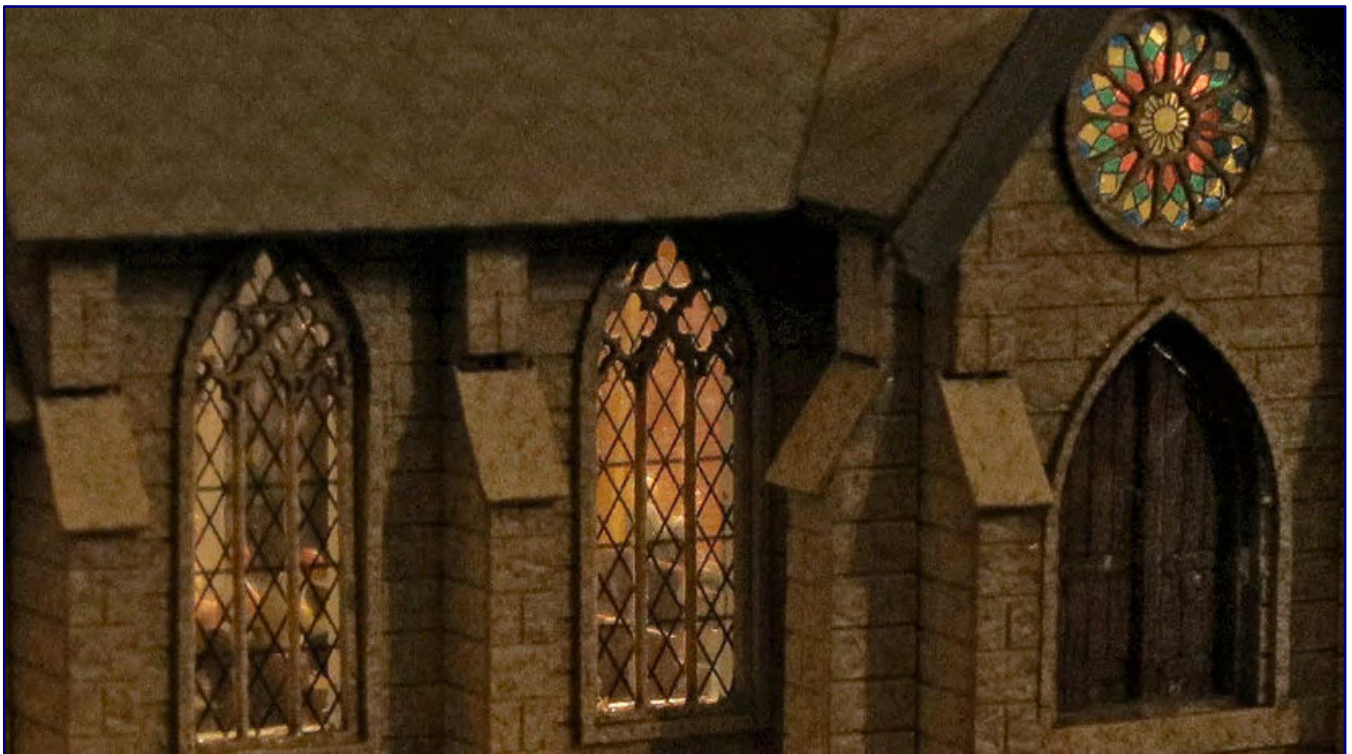
The Herpa pack with the air jet deflectors (art. no. 571586) contains ten elements, including two left and right end pieces and six centre pieces each (photo above). On airport sections they are used where vehicles or buildings have to be protected from the hot exhaust fumes of the aircraft (photo below).

Accordingly, we have designed a scene to illustrate this pictorially. The pack contains a total of 10 interlocking elements with lengths of 5 cm each. Six parts are middle pieces, which are complemented by two end elements each for the right and left side.

The exterior parts can be recognised by the holders for the separately enclosed warning lights, which can be separated from a moulding and glued on separately.

Church furnishings with mini-candles:

Ztrack reports that the church interiors from Z-Doktor Modellbau are available there. In addition to the US village church, they can also be used to complement the church from the Archistories programme, which is based on the Westheimer model.



This photo allows an impression of how well the interior of a church can also be seen from the outside. If you want to enhance it even more, leave the door open and receive more believers for Sunday mass.

The furnishings consist of pews, a cross and altar (3D printed) and the altar platform (hard cardboard laser parts). On the altar there is an open Bible and tiny candles, which can also be illuminated from below with mini SMD LEDs (not supplied).

The manufacturer of these products can be reached via the following website: <https://www.z-doktor.de>.

New tools and landscaping from Microrama:

After a very good start of the grassing device "bFlock", Microrama (<https://microrama.eu>) announces further matching new products. A hose called "bFlock-Flexible" with a small container helps to reach narrow areas or areas further away from the edge of the plant. For large areas, containers with a capacity of 400 ml are also available.

After Magiflor, Magigras and Magispray, which have led to a revolution in grassing, Microrama has now ventured into the development of new fibres with the industry. Extensive research led to results with properties that are so important for model making:

- Easier construction even of tree structures,
- fewer lumps,
- more natural colours, and
- electrostatically improved properties.

Micro-Trains looks ahead to Christmas:

The Christmas wagon from Micro-Trains (item no. 507 00 710) is to be delivered this month. A refrigerated wagon with different printing on both sides was chosen, which is decorated with the company's own advertising figure "Micro Mouse".

Wagon number 8 continues the Sweet Liquid series. This time the black 39-foot tank wagon (530 00 560) carries inscriptions for "Sweet Pure Molasses". The prototype was in use from the mid-fifties to the early sixties.

Two covered 50-foot standard cars in orange livery and with white spring imprint (505 00 481 / -482) are now in service for the former Western Pacific, which later merged into the Union Pacific.

Micro-Trains products are distributed by Case-Hobbies (<http://case-hobbies.de>) and others.



The Christmas trolley 2021 (item no. 507 00 710) is printed differently on each side. Photos: Micro-Trains

New combination signals at Kastenbahner:

Via ZDC - Z Dream Collections we had already pointed out new products that will be available there as finished models in coordination. The kit parts in question come from the manufacturer Kastenbahner, which now itself refers to the soon available Ks signals of the Deutsche Bahn AG. If you are travelling in the modern epochs V and VI, you will not miss these signals.

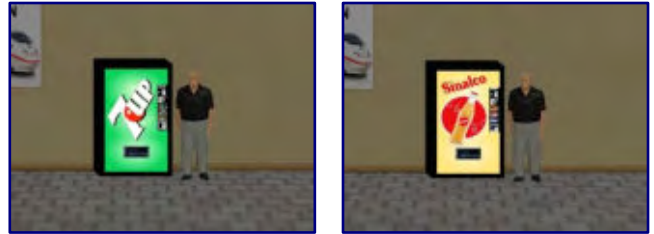
You can choose between the two versions "straight mast" and "bent mast," which can be placed to the left and right of the tracks. The articulated masts should still be available for order in 2021, as test models have already been assembled. The implementation of the Zs3v/Zs3 indicators was also successful and can therefore also be introduced in the current year.

The parts are unpainted 3D-printed or brass parts, which are painted with ordinary modelling paints. The raw parts are available from the manufacturer at <https://www.kastenbahner.com>.

Expanded world of vending machines at Modellland:

At Modellland (<https://www.modellland.de>), six different new food vending machines with factory-integrated LED lighting are available. Depending on the customer's chosen era, different versions are available, all of which may be considered equally successful:

- Pepsi Cola (Art.-Nr. 3013-5),
- Dr. Pepper (3020-5),
- 7up (3022-5),
- Coca-Cola (3026-5),
- Sinalco (3141-5) and
- M&M (3151-5).



Two of the new vending machines, which are now available with LED lighting, are shown here. Photos: Modellland

New models at AZL:

Since the beginning of the US programmes in Z gauge, there has been the question of when a locomotive suitable for shunting would be offered from the large-scale production. AZL has now answered this question with the introduction of the new EMD SW1500 in October.



The EMD SW1500 was delivered new in the colours of the Southern Pacific (item no. 62702-1). Photo: AZL / Ztrack

The prototype of the model was built a total of 808 times between 1966 and 1974 and was used by more than 60 railway companies. The type designation refers to an output of 1,500 hp.

Although actually intended for goods train service, individual machines could also be observed in passenger train service on branch lines.

Even 55 years after the first delivery, many examples are still in service today. The largest operator with 204 locomotives was the Southern Pacific, to which the first edition with three service numbers (item numbers 62702-1 to -3) is dedicated.

The new EMD SW1500 models feature prototype-specific details, traction tyres and dark nickel-plated wheel sets, innovative drive train technology and direction-dependent LED lighting.

As a bonus, two adapters are included that can also be used to connect the 905 couplers from Micro-Trains.

There are also some new colour variants of well-known models to report. The EMD F7 is offered as A-/B- (63002-1) and single B-unit (63002-2) as a Louisville & Nashville vehicle. The EMD E7 is now on the road as an A-/B-duo for the Union Pacific (64600-1 to -3) and then pulls the two eleven-car sets (73X49-1_SET / 73X49-2_SET) for the "City of San Francisco" long-distance train.



The long-distance train "City of San Francisco" is reproduced with the EMD E7 (64600-1; photo above) and an eleven-car set (73X49-1_SET; photo below). Photos: AZL / Ztrack

The R-70-20 refrigerator cars are now rolling out in BNSF red-brown livery and are available as single cars (914802-1) and in sets of two (914832-1) and four (904802-1). Manufacturer photos of the current deliveries can be found at <https://www.americanzline.com>.

Faller autumn new products in its anniversary year:

In its 75th anniversary year, Faller also has an autumn new product for the Zetties. A post office (art. no. 282787) is to appear here, which is derived from the earlier Güglingen railway station kit and was once

also in the range as a town hall in a different plastic colour.



It is a rural building model with a light plaster facade and a wooden roof storey including dormers. On the ground floor is the small post office that gives the building its name.

The Märklin Christmas Campaign 2021: For the fourth time since 2018, Märklin has launched a Christmas campaign on 25 October 2021. It is intended to appeal to newcomers and those returning to model railways. Märklin is promoting it via its own channels in the social media.

The post office (item no. 282787) based on the moulds of the Güglingen railway station has been announced by Faller as an autumn new product in 2021. Photo: Faller

Once again, a short advertising film is the core of the concept and is intended to seduce the

viewer. The theme chosen is a confusion between the virtual and real model railway world. At the end, viewers are led to a promotion page where they can request information material and vouchers, including one to purchase a discounted starter pack.

Friends of the house should watch the film so that they can have their say. They are then welcome to spread the word by recommending and sharing it themselves! The address of this year's campaign page is: <https://www.maerklin.de/kleinsein>.

Large Z gauge participation in Friedrichshafen:

The joy of being able to show up at the exhibition again obviously knows no bounds. If we take a look at the participating exhibitors and their exhibits for the upcoming Faszination Modellbau in Friedrichshafen, this proves an enormous willingness. At the same time, it makes a visit to this fair all the more attractive.

Sylvester Schmidt, exhibition representative of Z-Freunde International e.V., listed the following exhibits:

- Layout „Traumschleife“,
- The so-called “Schweineanlage” (pork processing plant),
- Schloss Neuschwanstein (Neuschwanstein Castle),
- A new suitcase system with camera train and self-propelled bus (if available),

- Harbour layout,
- The modules “Engelfabrik” (“Angel Factory”) and “Seeprozession” (“Lake Procession”),
- “Dead End City” and “Nugget Gulch” (show premieres).

A new demonstration system for digital operation will also be shown and, if available, the operation of self-driving buses (Z-Car system) will also be demonstrated.



The “Engelfabrik” (“Angel Factory” (left)) and the “Seeprozession” (“Lake Procession”) on Lake Staffel (right) are two of the Gauge Z exhibits on display in Friedrichshafen.

Of course, we also want to address the children who are coming to the fair. For them, the “dwarf layout” will be on site and a children's play circle will be set up where they can try their hand at driving a model locomotive.

Collector series from Full Throttle:

Two new collector's packs of well-known models have been put together by the US small series manufacturer WDW Full Throttle (<http://www.wdwfullthrottle.com>). The “Farm Feeds II” pack (item no. FT-COL56) consists of two cylindrical bulk freight cars from the earlier FT-1038 and FT-1040 packs, while the “MKT” pack (FT-COL55) contains one such car and one open bulk freight car from each of the FT-1046A and FT-2059 packs.

In Germany, products from this manufacturer are available from Case-Hobbies (<http://case-hobbies.de>), among others.

Herpa new products for winter 2021/22:

Herpa has now announced the new products that are planned for delivery during the next Toy Fair. From the announcements of the 1:200 scale we have again selected those that are suitable for European layout motifs and do not exceed a maximum length of 30 cm.

The following three models from the Wings programme are thus mentioned:

Belgische Luft-Komponente Lockheed C-130H Hercules “50 Years of Hercules” (Art.-Nr. 571791),
Rossiya Russische Staatliche Transportgesellschaft Ilyushin IL-18 (571807), and
Eurowings Airbus A320 “Teamflieger” (Team Flyer)(571838).



The Airbus A320 "Teamflieger" (Team Flyer) from Eurowings will appear as a new product in 1:200 scale (art. no. 571838), planned for the time window January / February 2022. Photo: Herpa

They are complemented by the Air France Airbus A220-300 (613507) from the Snapfit segment. Also a welcome idea are four historic passenger stairs in different designs from three airlines: Air France (571876), KLM (571883), Pan Am (571890 & 571906). Supplied per pack are two mobile stairs in different heights, and a tractor with open driver's seat.



Historic passenger stairs from Air France (571876; photo left) and Pan American World Airlines (571906; photo right). Photos: Herpa

New great ape from C-M-K:

Creativ-Modellbau Klingenhöfer (<https://www.klingenhoef.com>) reports a gorilla that has escaped from the zoo, and includes the attached proof picture. Fortunately, it is not a live animal, but a successful 3D print.

And, of course, this is also only a story on a scale of 1:220, but the man-ape drumming on the car is certainly a clever idea to transfer this jungle dweller to German landscapes, as well.



The escaped gorilla is the latest new products in the programme of Creativ-Modellbau Klingenhöfer and makes for exciting scenes on the model railway that appear ready for the movies. Photos: Klingenhöfer

According to the presentation on the newly designed website of this supplier, the car is apparently also included in the product, but that should not stop Zetties from turning the unplanned free rider into a terror of the road in other ways, as well.

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Trainini

German Magazine for Z Gauge



Train Formation in five Eras

**Useful Aids for clean Wheels
Modellbau Laffont in Portrait**

Introduction

Dear Readers,

This year is already coming to an end at a rapid pace. As soon as we look forward to the first trade fairs and exhibitions and await our personal reunion, the number of infections goes up through the roof again.

With each passing day, our worries grow as to whether the scheduled events ahead of us will be allowed to take place at all, and if so, what they will look like. Nevertheless, we don't want to let the fun of the hobby be spoiled, because the modelling season is now really kicking off.

I, too, am looking forward to spending a whole month working intensively in the basement. This will certainly result in many more contributions for this magazine, with which we want to enhance the anniversary year 2022.

Hopefully, the topics of this issue also fit in well with the main season of our hobby: Dirk Kuhlmann concludes his small beginners' series (at least for the time being) and releases our newly added readers of this year's edition into their own projects.

At the request of many readers, who have approached us from various angles over the years, we dedicate a separate article to the topic of train formations. After looking at F-trains and InterCitys in the last few months, we have chosen more curiosities for this time.

We have tried to cover as many eras as possible for this article. With the exception of Era I, for which the material offered is very limited, we have succeeded in doing so. Please let us know how you like this contribution and where exactly your wishes lie.

The more comments and requests we receive, the easier it will be for us to continue our work in a focused manner and to prepare similar articles for future years. This is what we mean by "Zetties and **Trainini** in dialogue", the permanent article title for our announcements and letters to the editor. After all, this magazine is meant to please you, not us!

But this time we are also looking a little outside the box: as part of the annual theme, Modellbau Laffont, an important accessories provider that is also active for some manufacturers and listens carefully to its customers, introduces itself.

Two new tools from the Modellbahnunion provide safe operation and clean wheels. We tested the cleaning tools and were delighted. Those who are still looking for exciting books for Christmas presents are just as well served in this edition.

All in all, we hope this will once again be a colourful and appealing mix to get you through Advent entertained and in good spirits. Take care and remember the dialogue!

Sin-Z-erely,

Holger Späing



Holger Späing
Editor-in-chief

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We thank Bruno Georg, Norbert Schmitz and Eisenbahnstiftung for their photo support.

Date of publication of the German language version of this issue: 27 November 2021

Cover photo:

The short D 811 is hauled by 110 226-8 on a September day in 1986 and performs its important connection function between the intercity hubs Dortmund Hbf and Frankfurt (Main) with high-quality coaches.

Train formation proposals

Colourful Trains of all Ages

Time and time again we receive requests from readers to submit train composition suggestions in Trainini®. We regularly comply with this request in our reports on tested new products, but there is also a need for individual wagon models or unusual prototype compositions. Therefore, we have collected and compiled some ideas for different eras.

When we wanted to respond to various readers' requests to work out and publish one or the other train formation proposal, many thoughts ran through our heads. There are plenty of suggestions, mostly for (once) everyday things, in the literature, in the various monthly publications of the trade press and also in a special series at Miba.

So, if we are nevertheless approached with requests, we need to clearly define what is to be shown here, what is helpful and what has not already been presented elsewhere in the same form. This resulted in the following key points for us:

- Due to the core of our readership, we consistently focus on material offered for Z gauge.
- Both high-volume and low-volume products should be included.
- Our proposals should cover as many eras as possible.
- A touch of something special away from "typical" model trains seemed to us to be indicated.



A train formation closely aligned to the prototype can be a factor for atmospheric model railway operation. In any case, it also conveys a piece of history to the viewers of a layout.

We certainly do not need to list here and document in photos what model railroaders across all gauges combine almost instinctively because it corresponds to what they have seen themselves or to the manufacturers' suggestions. Because that is nice, but probably does not need an article.

And, so, we sat down, researched, browsed and received ideas until we had seven proposals ready for publication for model railway eras II to VI. They are either the result of train formation plans and photo documentations or are freely combined, but then closely oriented to templates of the big world. Let us now work our way chronologically through the world of Z gauge.

Unlucky pioneer

V 32 01 (renamed V 120 001 in 1930) was built in 1927, put into service by the Deutsche Reichsbahn-Gesellschaft in 1929 and tested for two years. It was assigned to the Rbd Stuttgart, where it was taken out of service in 1933 after it failed to prove itself.

It is one of the pioneers of diesel traction, whose breakthrough failed for a long time because of the question of power transmission. It received an ordinary steam locomotive chassis similar to that of the class 78, which forms the basis of the model. The diesel engine acted on a compressor whose generated compressed air was temporarily stored and filled into the pistons of the steam cylinders as needed.



We have equipped V 32 01 with an express train of Reichsbahn wagons, for which meanwhile old Märklin models and three wagons from the small series were used.

As the DRG's first large diesel locomotive, intended for use on main lines, it deserves a special role in railway history. The short episode of its use falls in epoch II. There, among the prevailing steam locomotives, the model is able to attract attention due to its unusual appearance.

For the Märklin model (art. no. 88065) we have chosen a passenger train consisting of coaches of the former K.Bay.Sts.B. In view of the low top speed of V 32 01 (80 km/h) we do not like to call it a D-train. After about twenty years of service, the already somewhat aged carriages with wooden clad walls are unlikely to have been in such high-quality service.

The proposed train is made up of all three car classes of that time. The Märklin models are not sufficient for this. Behind the locomotive runs the former Bavarian express train baggage car (8732), followed by the Heckl small series specimens of a three-part pack (without item number) and the express train car 3rd class (8731), which was also chassis donor for the mixed class Heckl models.

The post express

One of unfortunately several “stepchildren” of Z gauge are mail coaches. Although railway post wagons once belonged to the everyday photo and were part of many fast trains, the offer of our scale is so far limited to two pre-war designs. Not a single new-build Deutsche Bundespost car that can run in fast passenger service has been put on the tracks so far.



On 22 September 1961, 41 031 waits in Fulda in front of a post express train to Frankfurt Hgbf. The fast multi-purpose steam locomotive seems predestined for a train of this type. Photo: Herb Clearley / Eisenbahnstiftung

Therefore, in order not to have to completely exclude this chapter and to present it effectively, a thoughtful and creative approach is needed. That is why we have taken one of the earlier express freight and mail express trains as a suggestion.

What is already no longer possible in epoch IV, we can at least recreate for epoch III: Fast-moving steam locomotives were mostly used as draught horses. Options for the small scale therefore arise for the series 01 (88011) and the various versions of the 03 that Märklin has produced up to now.

Only the original model from 1972 (8885) is not suitable here, as it bears the lettering valid from 1968 onwards, which does not match the wagon models that form the core of our train. We have chosen the 41 356 (88275), the latest steam locomotive model on offer. The universally usable locomotive with a speed of 90 km/h in the prototype was a versatile talent and therefore also considered suitable here.



From *Bahnhofsstraße*, the view falls on *Bebra* station, where a class 01 express steam locomotive is waiting on the platform with a *Postexpress* train, which is very close to our train formation suggestion. In the foreground, a class 44 oil locomotive rolls through the photo (1964). Photo: Ludwig Keller / Eisenbahnstiftung

We have attached four wagons to it in which letter and parcel post is sorted and other parcels are transported without sorting: Behind the locomotive hangs an apron mail wagon *Post4* (from 87350), followed by two covered wagons *Glmhs 50* (from 86000) and the short *Post2* (from 87670). This combination also breaks up the appearance somewhat due to the two different wagon colours.

Nevertheless, the covered wagons can also be replaced or extended by the post's own constructions. Almost ten years ago, *FR Freudenreich Feinwerktechnik* converted the parcel post coach *Post 2ss-t/13* (49.341.11), which also fits in well with our train with the addresses of this first edition – and several times, too.



Our *Post express* train is made entirely of *Märklin* material. However, wagons from *FR Freudenreich Feinwerktechnik* can also be used here as an alternative or supplement.

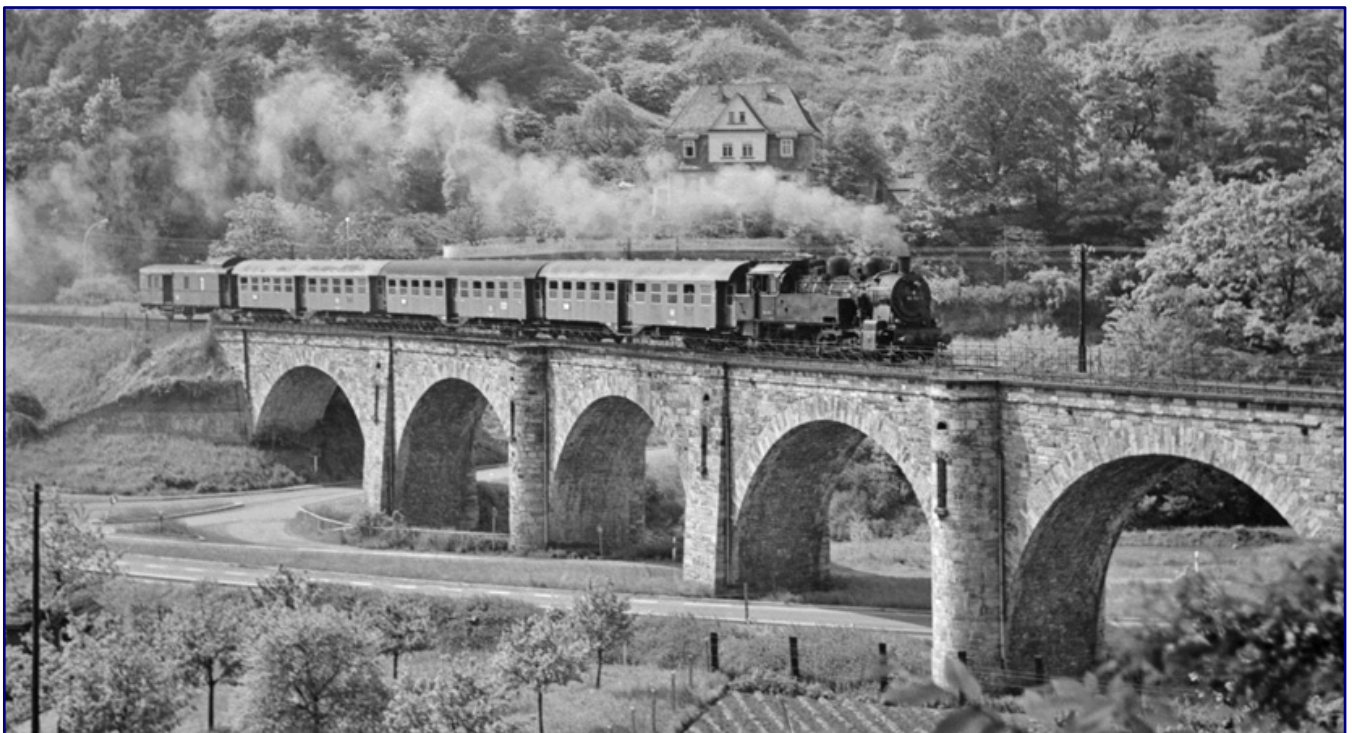
In the direction of Märklin, we leave here the suggestion to take a look at the in-house Minitrix assortment and to have the recently released newly constructed cars of the type Post 4mg-al/26 (later Post mr-a) also follow for Mini Club.

There should be a great demand here, because such models could be used both individually and in pairs in D trains or combined with several of the Bundesbahn's pack wagons. This would result in further mail trains, which in the past ensured that letters were delivered nationwide on the following day. Back then the world was still in order and the postal service had not been privatised.

Locomotives on the move

We have now arrived at epoch IV, whereby we are taking up two ideas, at this point, that can in principle also be represented in epoch III. They are therefore intended to create a transition from the previous proposal (epoch III) to the next one (epoch IV), i.e., to build a bridge.

We used combinations that were commonplace, but also very limited in regional terms. In this respect, they represent something unusual that an outsider would probably not expect.



094 652-5 from the Dillenburg depot is on the Niederschelder Viaduct with P 3218 to Gönnern on 6 June 1970 (06.06.1970). Photo: Gerhard Moll / Eisenbahnstiftung

What these suggestions, documented by photo and train formation plan, have in common is that locomotives are used that were intended for shunting service, and, at best, freight traffic with short transfers. Nevertheless, there were reasons for the Bundesbahn to use them for other purposes.

The first of the two examples take up the former Prussian design of the T 161, which was in service as DB class 094 during the period under consideration here. Due to its high frictional weight and a Riggerbach back-pressure brake, this tank steam locomotive was excellently suited for steep track service.

The Deutsche Reichsbahn had already recognised this before the war and used this steam locomotive to save cost-intensive cogwheel operation and to be able to work with adhesion. In this way, this prototype also found its way into passenger train service, which was also continued by the Bundesbahn and DR after 1945.

In the DB sector, the class 82, which was conceived as its successor, can also be used in the same way. Should this new locomotive ever follow as a Z-gauge model, the slender beauty would be an appealing alternative for the identical line-up of passenger coaches.



Our model proposal for the P 3218 takes up the train formation of the summer of 1970 and uses the conversions of Märklin (wagons) and Bahls Modelleisenbahnen (superimposed locomotive renumbered with signs from HOS Modellbahntechnik).

The P 3218 and the P 3214 (Dillenburg - Gönnern - Wallau) on the Scheldetalbahn from the timetable years 1970/71 became the concrete template for our model idea. We chose the Bahls superstructure of the class 094 (5094) for this, since Märklin has not yet offered the basic model with a current control system.

In tow, the steam locomotive has three Byg 514 2nd class conversion coaches (8754 or from 81400), the 1st class is conspicuously not represented in the original train. If you like, you can of course also add an AByg 503 (8753 or from 81400). The luggage car at the end of the train is an old "Donnerbüchse" D2ie (8752), which has not had an update of its service number in its last years of service.

Bahls also offers its locomotive superstructure for the East German state railway, the coaches are replaced by those from the Donnerbüchsen pack (87513) which appeared this year. All in all, this results in a passenger train, such as was on the road with the later traditional locomotive 94 1292 near Suhl.



The well-maintained 260 112-8 waits for passengers to Gefrees at Falls station on 31 August 1970 with N 3823. Photo: Dieter Junker / Eisenbahnstiftung

But there is also something much more curious! The N 3823 from Falls to Gefrees was not only on 31 August 1970 with an extremely strange-looking crew. The only 5.3 km long branch line at the edge of the Fichtelgebirge was about to be discontinued at that time.

The Bundesbahn reduced the service for such candidates for closure to usually only two or three train pairs daily. Thus, there were no longer any meaningful connections that would have attracted passengers. A single passenger coach was usually sufficient for the modest number of people willing to travel.

On this short branch line, this was the last platform car of the DB, which once formed the transition to the standardised design of the Donnerbüchsen. And we will also use one of these for the model reproduction.

This model has been available in the Märklin programme for many years. A pure 2nd class car Bi-28 (8751) is suitable here, because the DB also did without the upholstered class in the original. This short train is hauled by a class 260 diesel locomotive, which was already available in 1972 (8864).

When there was an “increased number of passengers”, as for example on Whitsun Saturday 1972, a pair of B3y conversion carriages (2 x 8707) reinforced the presented train to a total of three carriages. However, it was always the case that only 2nd class was offered.

One year later, this was finally over, as the DB discontinued passenger services between Falls and Gefrees in September 1973. Since the class 260 did not have a train heating system, the blunderbuss (“Donnerbüchse”) was previously warmed up in winter with the coal stove.



If it were not quite known as a curiosity of railway history, the model realisation of the N 3823 would probably cause astonishment. Arrived at the destination station, the only two passengers have left the train, the train driver also gets off on the open platform.

This proposal offers variation possibilities by exchanging the locomotive for a 236 (from 81772). And the combination could also be taken back to the late Era III, because both the V 36 (from 81770) and the V 60 (from 81564, among others) belong to the lettering versions already offered.

The V 45, which was also photographed in front of such a train and originated in France, is not available as a 1:220 scale model. The train could be transferred to another region by changing the leading locomotive. Comparable passenger train performances have also had to be provided by small locomotives of performance class 2 (Köf 2; Z-Modellbau) and 3 (Schmidt and Z-Modellbau). In the case of the earlier Schmidt model, however, a motorised ghost car would be required for operation.

At the Intercity level

We only have to look back two months to come up with a proposal that fits perfectly into Era IV and is also suitable for small layouts. Fittingly for the fiftieth anniversary of the Intercity, we found a very short train that was formally classified as a D train, but offered its guests IC standards except for the missing dining car.

The D 810 / 811 ran for two years from the summer timetable 1985 between Frankfurt (Main) and Dortmund and used the Ruhr-Sieg route via Hagen - Siegen - Gießen. On this connection between the two end points, which is admittedly much shorter, the journey times were otherwise significantly higher than on the Ruhr axis.

Thus, the new express train's main function was to increase the attractiveness of this route. That is why Siegen-Weidenau and Wetzlar were the only intermediate stops. In summer 1987, further intermediate stops were added, the route was shortened to Hagen - Frankfurt (Main) and the train pair was finally operated as D 2810 / 2811 from summer 1988.



Photo above:
110 406-6 of the Bw Dortmund Bbf passes through Finnentrop station with D 811 on 10 September 1986. Photo: Bruno Georg

Photo below:
A Eurofima car Avmz 207 can also be seen in first place in D 810 behind 110 343-1 as it passes through Dillenburg station on 1 August 1986. Photo: Norbert Schmitz

When the IC-like connection was introduced in 1985, the Bundesbahn wrote in the timetable: "With this train pair, a fast daytime connection between Dortmund and Frankfurt is now offered. The trains only stop in Siegen-Weidenau and Wetzlar in route. This allows a travel time of less than 3 hours."

DB had also thought of a good connection with its IC network: The D 811 offered a connection to the IC line 3 in the direction of Mannheim in Frankfurt (Main) Hbf, the counter-train D 810 took passengers from this line and had a connection to the IC line 1 to Hamburg in Dortmund Hbf.

For model railways, the D 810 / 811 offers the attraction of having a layout-friendly length and still being made of the highest quality car material. We can reproduce it with the class 110 box locomotive, for which we fall back on the first Insider model 8834, which reflects the modern Bundesbahn of that time with red instead of black Ege biscuit.



Our model proposal for the D 810 also received a Eurofima car from the latest Märklin production as a 1st class car.

If you do not own this model, you can fall back on normal catalogue goods with item 8839. A variation is usually documented on prototype photos, but Märklin has not yet offered it in blue livery with Era IV inscriptions.

The train consists of a 1st class compartment car Avmz 111 (from 87281) and two 2nd class large capacity cars Bpmz 291 (8759 or from 87282). Since the Avmz can be seen in prototype photos in a modernised version with swinging-sliding doors instead of hinged-folding doors, a Eurofima car Avmz 07 (87409-04) would be an appealing alternative, which is also documented in the photos.

Colourful border crossers

The following suggestion for early Era V goes back to suggestions we received from readers in Austria. The trigger was the ÖBB's DDm car transport wagon, which Märklin delivered this year – certainly also against the background of other models of the recent past.

It was explicitly pointed out that the prototypes of these wagons were also used across borders and thus also reached Germany. This is still true for the DDm of the ÖBB today, only no longer in the Jaffa colour scheme.

Therefore, we looked at different train formations for the traffic between Austria and Germany before ICE and Railjet dominated here. From this we have derived the following proposal, which does not follow any concrete train formation plan, but does correspond to the practices of the two state railways.

The fictitious section on the tracks of the Bundesbahn is shown. The rolling stock is provided jointly by ÖBB and DB. ÖBB is responsible for the double-decker car transport wagons and an air-conditioned 2nd class wagon.

This is no coincidence, because DB always had a shortage of such wagons due to a misguided procurement policy and was happy that the Austrians stepped in. Otherwise, the railways are obliged to

compensate for kilometres, which can also be compensated monetarily, but was usually compensated for by the provision of vehicles.

And so it is that the train formation appears quite balanced: This train is also hauled by the class 110, in the ÖBB section a model of FR Freudenreich Feinwerktechnik or SMZ would certainly also be conceivable.



With the ÖBB car transport wagons and a “Bügelwalze,” that has long since been out of its original condition, we formed an international motorail train between Austria and Germany for the early nineties.

This time, however, a “Bügelwalze” (pleat) was chosen and, with model 88412, one that falls precisely into this period due to its special features. The sleek electric locomotive still wears its original colour scheme, but no longer the continuous fan band. With the individual fans it also received a central engine room window – previously not a feature of this substructure series.

Behind the locomotive run the two DDm (87095), on which we have placed some contemporary cars (Märklin 8952). The demand from motorists for this train is apparently limited, as it apparently runs on the edge of the holiday season and not on a typical arrival or departure day.

The paying guests can take a seat in the three compartment coaches from pack 87211, which we chose because of the matching lettering (fonts and pictograms). Between them we have placed the Eurofima 2nd class coach from pack 87343.

If you have enough space on your layout, you can also extend this train with the other cars of the ÖBB pack and add a little more colour and even high quality to the layout. This border crosser is certainly classified as a D or FD train.

Modern jubilarian

Our proposal for Era VI provides plenty of colour variety and at the same time honours this year's jubilarian: Lokomotion Gesellschaft für Schienentraktion mbH, based in Munich, started operations in 2001, now twenty years ago.



Founded a year earlier, it provides international transport services to and from Germany, Austria and Italy. The company's two major hubs are München (Munich) and Verona.

DB Cargo is also one of the shareholders with a 30% stake, which is why DB Cargo unit locomotives were repeatedly contributed to the company as a contribution in kind: Classes 139, 151 and 212 as well as small locomotives. For the model, this gives the option of generating eye-catching splashes of colour from familiar shapes.



With the Vectron 193 772 and a class 189 machine, two Siemens locomotives can be seen here in intermodal traffic. Photo: Lokomotion

The reason for this is the striking design of the locomotives, which earned them the nickname “Zebra:” An unmistakable striped pattern is applied to the white base colour, set off only in the area of a driver's cab by a black bar, as a unique selling point of the company.

These stripes are supposed to represent rails and points in a stylised way, the colour of which indicates a location of the company, for example the national colour blue of most of the machines indicates the location Munich in Bavaria.

The main transport axis for Lokomotion is still the Alpine crossing over the Brenner Pass, where modern multi-system locomotives have been running through since 2005. Our train formation proposal is therefore equally suitable for Germany and Austria. However, today's destinations also include Slovenia (via the Tauern axis), as well as the Czech Republic and Hungary (via the Danube valley railway).

continues on page 17



212 249-7 "Clärchen" (photo above) and 151 074-2 (photo below) are two former DB engines in service. Photos: Lokomotion



For our train formation proposal, we have chosen a transalpine container train that also features a former DB unit class 139 locomotive at the head.

Trains of combined transport are often hauled. In addition to containers, however, sliding wall wagons, steel loads or motor vehicles can also be found being towed by these locomotives.

We would like to commend Lokomotion with a train hauled by the Märklin model of the 139 133-3 (88384). As a suitable consist we have used the contents of the wagon pack "Container Transport" (82665), which includes two different types of wagons and is strikingly colourful in terms of container selection.

For those who want a little more variety, we would like to refer to the other series produced by Bombardier and Siemens, whose basic forms are available in 1:220 scale and some of which have already been offered by Märklin in suitable printing: 185, 186 (both Bombardier TRAXX), 189 (Siemens), and 193 (Siemens Vectron). The 151 and 212 series also appear to be within the realms of possibility, and there were suitable designs for the Köf 3 from Schmidt and Z-Modellbau.

Manufacturer large series production:
<https://www.maerklin.de>

Small series manufacturers:
<http://www.bahls-modelleisenbahnen.de>
<http://www.fr-model.de>
<http://heckl-kleinserien.de>
<http://www.z-modellbau.de>

Prototype:
<https://www.lokfotos.de>
<https://www.lokomotion-rail.de>

The first driving tests (part 4)

Subtle Work on the Design

It is often the little things that make a big impact. But a lot doesn't help much, which is why it depends on the considered and thoughtful use of the finest details. But if chosen deliberately and often modestly, they do not fail to make an impact. And they don't always have to cost a lot of money because some of them lie around unused in the household or can be found in the spare parts box. Dirk Kuhlmann sharpens the senses of our newcomers and returners in the last part of our series.

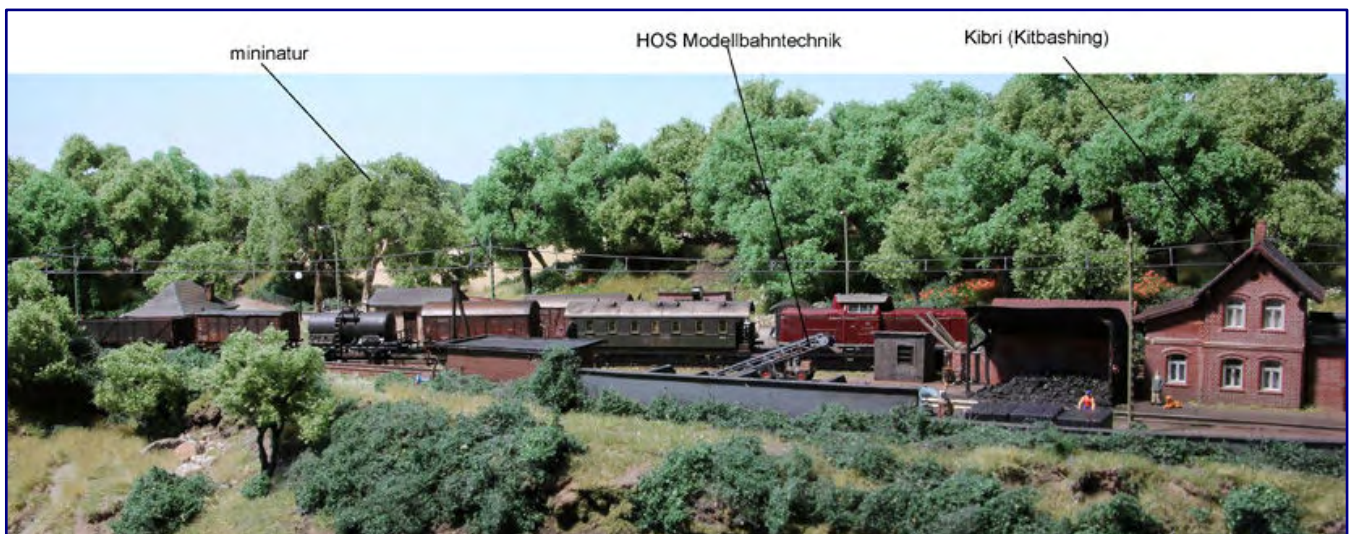
The last part of our small series for beginners and returners starts with a short report about the meeting of three Z-gauge railroaders, of course in relation to the subject.



In the middle of October 2021, two Zetties that I have known for many years have announced their visit to the club rooms of the FdE Burscheid. Both are with their layouts right at the point where the small details are supposed to bring a whole scenery to life – so exactly fitting for our fourth part.

This starts with the placement of model figures and ends with tiny suitcases on the platform. Typical for a long model railway life in 1:220 scale is the “hunter-gatherer quality.” In two large boxes they showcased

their acquirments to me and in the end I was very amazed. Among them were the finest products from companies that have long since disappeared, some of which I had not seen with my own eyes for years.



Picture above:
Welcome to Wakenfeld, a small railway station in the Rothaargebirge, where things are still tranquil at all times of the day. Life only comes into play when the commuters take over the platform and a “coming and going” dominates the scenery.

Picture below:
The train pulls in and the train passengers rub their eyes in amazement. But no, this is not a GmP (freight train with passenger transport), but only the daily local freight train Ng 2334. The V 100 has a rather rusty passenger car on the hook, which is only destined for the local scrap yard.

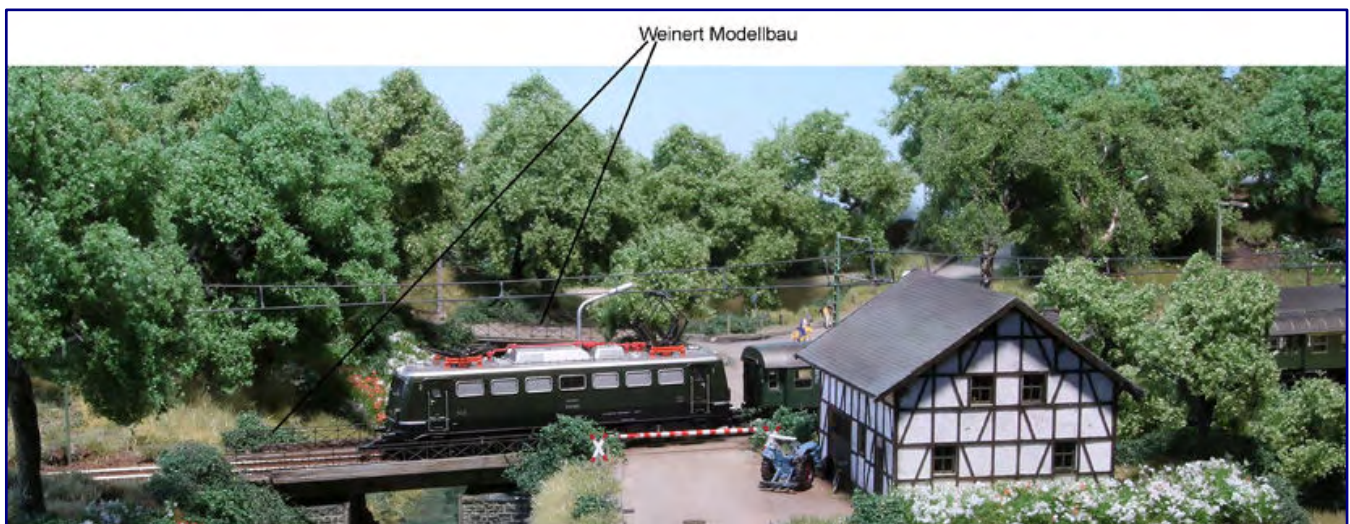
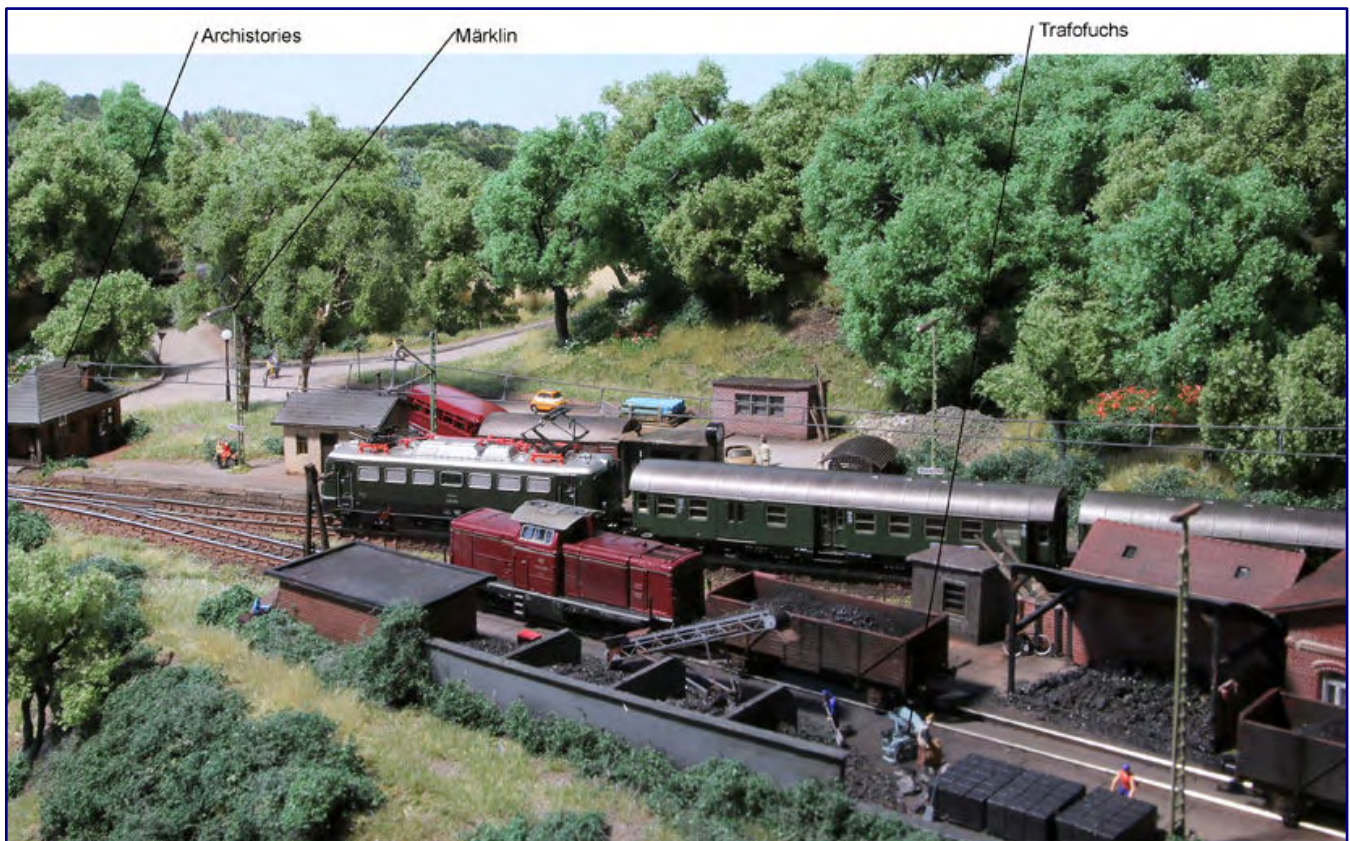
For our **Trainini®** report, this meeting was a real stroke of luck; the written word, nice pictures – all very important for a manual. However, a personal meeting cannot be topped. The probing questions of the two model railroaders had to be answered somehow and finally found their way into this report.

continues on page 22



Picture above:
After the V 100 has placed its load in the turnout track located 300m to the east, the two Omm 34s loaded with "black gold" are transferred to the "Kohlen Schmitz" company in a movement into a closed section.

Picture below:
Now the crew of the V 100 has to uncouple and wait, because the N 5678, a typical slow train in this region, has priority. Mrs. Schmitz has already brought the coffee; there's got to be time for that.



Picture above:

Our N 5678 is setting off again. However, the driver of the V 100 has to continue to wait on the area of the coal yard according to the instructions of the local train dispatcher. Another passage is made. According to the schedule, no other train should actually block the line for the next hour.

Picture below:

The catenary wire has only been hanging here for two years. According to the Bundesbahn, this is an alternative route for many through trains, e.g., the famous "Heckeneilzüge" (hedge train). Alongside it can be seen that the E 40 was often also in passenger train service in earlier times.

Of course, we then took a look at the “Kniephaven” Z-gauge island layout, which has recently been on display here in the club room for the first time since OnTraXS 2017 in its finished state, combined of course with its almost unbelievable variety of small parts added there.

Here, the Z-gauge fans first noticed that the distribution of the smallest parts and decorations is very finely balanced and many items are only caught on second or third glance.

Exactly now, we are at our today's and very important point: the exact amount of the final decoration. This is of course done in accordance with the locality chosen by the original and the sequences of the railway traffic.

A little imaginative story around your exhibit and “life comes to life”. We have consulted with the editorial team and have come to the conclusion that small pointers and corresponding manufacturer's names at the edge of the picture should be enough to support your research.



Unbelievable! A Klv 20 (full of seven men) rushes through the station at high speed. Somewhere, according to the dispatcher, there is a problem on the line.

Nowadays, with the technical possibilities available, almost everything can be bought for the 1:220 scale. Product gaps are recognised and implemented quite quickly by small series manufacturers, 3D printing technology alone is a decisive point for this.

In order for you to be able to dream a bit, there is also the little story around the station as a bonus, in the form of somewhat longer captions. I would like to give you a few implementations, but if you need further suggestions, feel free to let the editorial team know.

As a template, let's take a few pictures of the real world and take a close look at the positions and sceneries in which the little figures are standing. Looking with your own eyes on the spot can also be very helpful in portraying the so-called interpersonal scenes.

The accessories, such as cars and two-wheelers, will then almost come naturally. If you are still a little undecided, the temporary glue from Noch (art. no. 61121) would be the first choice. For the final gluing, I always use a gel-based superglue or as well wood glue. The dispensing is done with a very thin metal wire or a toothpick. Good tweezers for placing should be another prerequisite.

Unfortunately, we are currently experiencing a somewhat sparse supply of street lights, since Brawa, a major manufacturer, withdrew from production a few years ago and the delicate Märklin lights are also mostly not (or no longer) available.



The track is clear again and the V 100 now collects its parked freight cars to serve the next customers. After that, there is a little rest in Wakenfeld again.

In the large-scale production, only Viessmann currently offers Z-gauge lights, but these look very clunky and oppressive to the viewer. At the latest after placing them on your layout, these disproportionately bulky products stand out.

Fortunately, small-series manufacturers such as HOS Modellbahntechnik have some wonderful and, above all, scale-appropriate lights in their portfolio, and it is also worthwhile to take a look outside the box: the company Beli-Beco offers a very (very) delicate light in 1:160 scale, these often fit our gauge because of their extraordinary filigree character or, because they are hand-made, they can be adapted to 1:220 scale on request.

Do you still see small parts in the photos that you can't identify? There is the famous tinker's box with its many leftover small parts from previously built structures and houses.

But it gets even better; in every household there are things lying around that are no longer functional. The inside of a lighter, for example, holds some surprises, e.g., my maritime lifebuoys are made from their seals.



Dusk has fallen and soon life will come to rest in this tranquil hamlet. With this picture and two add-ons on the following page, we bring our little newcomer series to a close.

An old wristwatch contains many gears that, with a little applied rust paint, are good as individual parts for the scrap heap on a small corner. I could probably continue the list endlessly, but just go on a journey of discovery yourself. It's worth it.

And yet, of course, there is the one important destination for an on-site overview: with the Spur-Z weekend in Altenbeken 2022, you could take a look yourself at almost all the products that enhance our scale. Most of the manufacturers will be on site.

It is also worth taking a look at our **Trainini®** pages, where you will find a list of suppliers under the heading "Z World," which is constantly being updated and expanded. You could research this for hours...

This brings us to the end of our series of reports. We hope you enjoy the hopefully new insights and the implementation of what you have learned. Perhaps in the near future there will be a nice layout portrait here in **Trainini®** – of your work.

Websites of supplier as recommended in the report:

- <https://www.archistories-shop.com>
- <https://www.artitecshop.com>
- <https://beli-beco.de>
- <http://www.fr-model.de>
- <http://www.hos-modellbahntechnik.de>
- <https://www.maerklin.de>
- <https://www.mininatur.de>
- <https://www.modellbau-laffont.de>
- <http://www.trafofuchs.de>
- <https://viessmann-modell.com>
- <https://weinert-modellbau.de>

Trainini® supplier list:

- <https://www.trainini.eu/z-world/suppliers>

Two additional picture add-ons to demonstrate the showcase principle and its effect - here also in conjunction with a decorative element as the top finish:



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Maintenance & Care (Part 10)

Useful Aids for clean Wheels

Clean wheels are a precondition for the smooth running of model trains. Two new devices from the company Modellbahn Union have recently come to the rescue. And because we know that there is a great demand for such cleaning aids, we have taken a close look at both, tested them and present them in this article.

Clean wheels, along with equally well-maintained track, are the be-all and end-all of model railways. Z scale is no exception in this respect, although its rolling stock is by no means as prone to electrical contact problems as many outsiders assume.

Especially newcomers to our scale often wonder, however, about the best method for cleaning wheels. Aggressive cleaning substances should be avoided and one should also be careful with mechanical cleaning methods. What is needed are gentle but thorough methods and techniques.



Modellbahnunion offers two devices for a surface-friendly but effective cleaning of Z gauge wheels.

Märklin itself markets its locomotive wheel cleaning brush (art. no. 66623) from the N-scale Minitrix range, also for Z gauge. Its usefulness is controversial for many, however, and we too have reservations about working nickel-plated wheel surfaces with a brass brush.

What if this method of cleaning leaves fine scratches on the surfaces, on which new dirt can adhere all the more? Can it be ruled out that the brushes remove the extremely thin nickel coating from a rotating wheel?

For all those who share these concerns, the company Modellbahnunion offers bought-in proprietary products that are equally suitable for analogue and digital railway enthusiasts and also offer solutions for cleaning locomotive and wagon wheels. The auxiliary device for non-motorised vehicles also comes with a cleaning felt for the track.

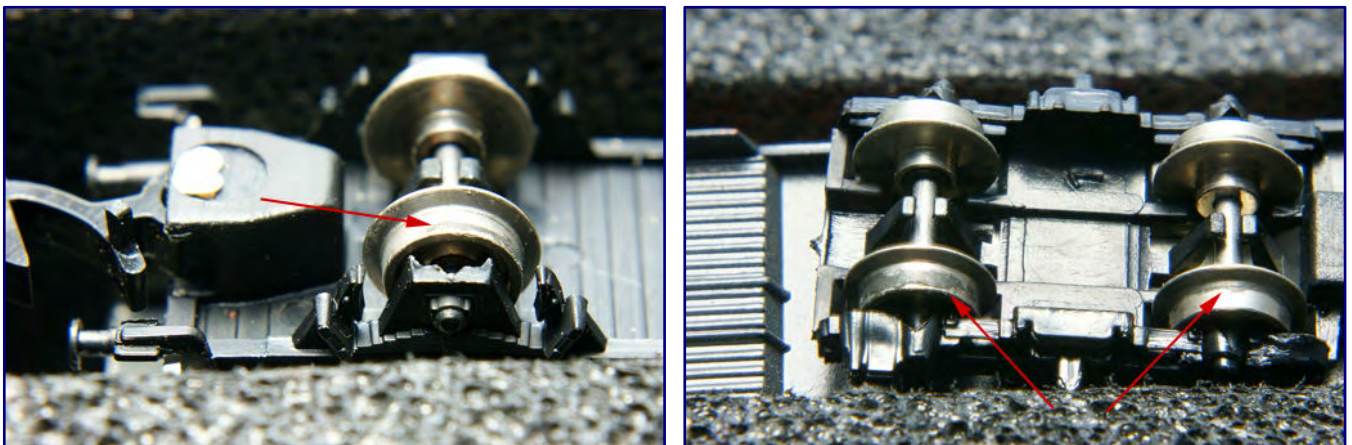
It is precisely these tools that we would like to present, try out and test for their usefulness today. But before we get to that, let us look at the symptoms of dirty wheels, because not everything is obvious at first glance.

Dirt-related failures

An electric transmission wheel usually reveals contamination very quickly: the motor is only supplied with interruptions, the locomotive stutters, and often the lights on the locomotive or on wagons are flickering.

However, these symptoms can remain undetected for a while, provided that buffering with capacitors bridges the interruptions or in the case of a small-series model with an effective flywheel mass.

Derailments are another tell-tale symptom of dirty wheels that mainly affects wagons, but sometimes also powered locomotives.



Dirty wheels (left, freight wagon) cause poor running and contact problems. Derailments are a typical consequence once thick coatings of dirt have accumulated on the wheels (right, dining car).

They do not always occur in the same place and are therefore not necessarily reproducible, as the technicians would say. Often, affected model railroaders wrongly suspect that the derailments are due to a misaligned gauge and start thinking of ways to measure and correct it, if necessary.

Those who are now on the proverbial wrong track may be driven mad by the persistent breakdowns. And the darker the wheels are – think not only of today's black nickel-plated metal axles but also of the plastic wheels of the pioneering days – the less noticeable the true cause.

Already in part 6 of this maintenance and care series (**Trainini®** 8/2016) we dealt with the problems of dirty wheels and gave simple tips for cleaning wagon wheels. Today, we now also include powered axles, because often enough it is not necessary to disassemble a model.

A one-time investment into a cleaning device saves us manual effort and is much easier to handle. And dirty cleaning felts are quickly replaced with spares that are also sold by Modellbahnunion.

A useful tool

Whereas in our first article on cleaning techniques we used kitchen paper as a cleaning aid and rails as a guide, we now have devices that combine both in one cleaning pad. In the version for powered wheels, it also transmits the power – it couldn't be easier.



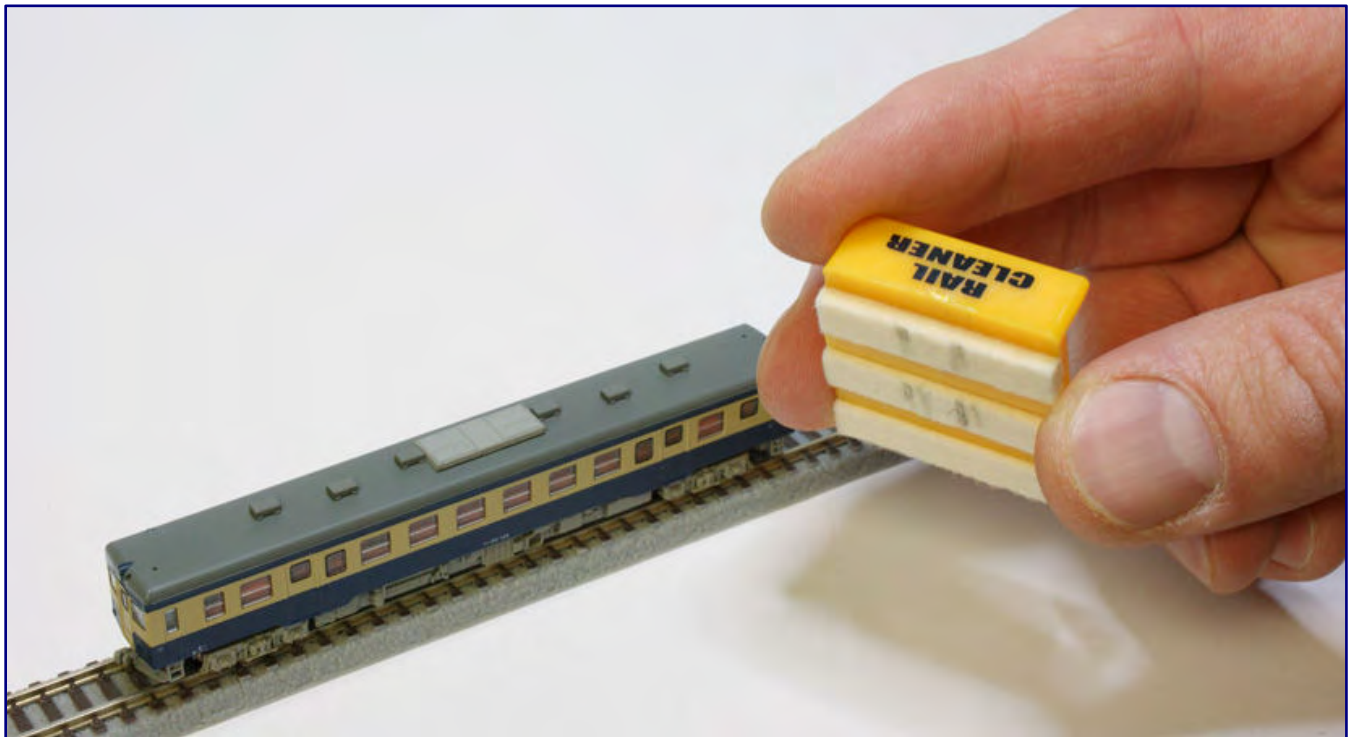
The device comes in two versions: one for motorised (top) and one for un-motorised models (bottom).

Nevertheless, we still had some questions and so we simply tried out the two new devices from Modellbahnunion and tested them extensively for their practical suitability. The German importer and supplier sources them from the Japanese manufacturer Tsugawa and offers two versions:

- Z gauge wheelset cleaning system (item no. MU-Z-A12508), and
- Wheelset cleaning system for vehicles without motor (MU-Z-A12509).

Both devices are about 205 mm long, the usable length as communicated by Modellbahnunion as about 180 mm. This is easily enough for all locomotives and wagons ever found in the Märklin programme.

Only the 18-axle torpedo ladle car (86200 ff.) almost exhausts the space on this tool with its length of approx. 154 mm. But the remaining three centimetres are still enough to clean all around its wheels as well. Only a little caution is required, so that the model is not accidentally pushed off the cleaning pad and possibly damaged.



The version for non-motorised vehicles comes with an additional hand tool carrying three cleaning felts to remove dirt and debris from the track.

The wheelset cleaning system is not suitable for the extreme outliers among locomotives: for example, the class 3900 “Challenger” and the 4000 “Big Boy”, both once produced by AZL, exceed the usable length.

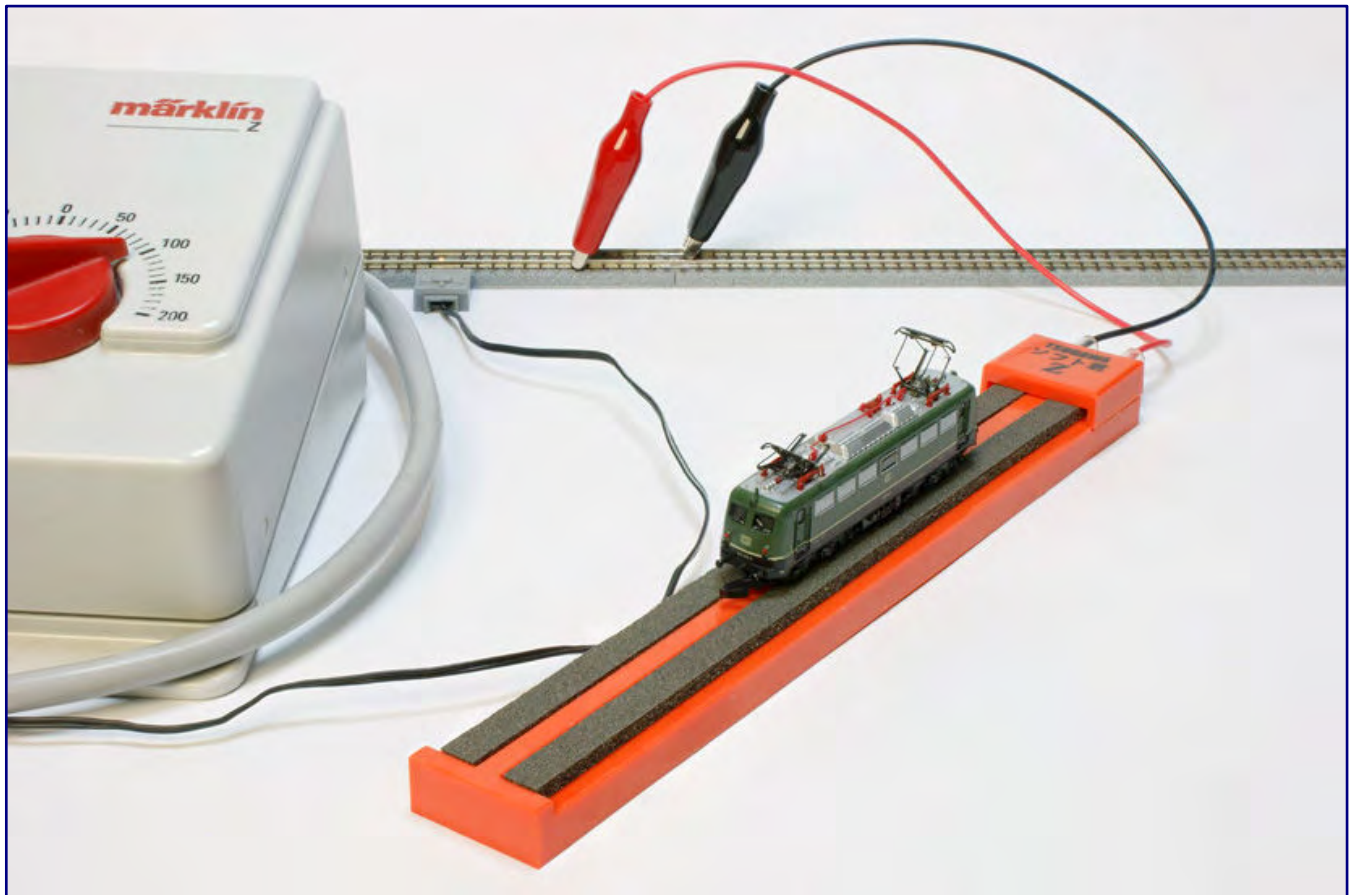
Both versions of the wheelset cleaning system have in common that their polishing pads, a cleaning foam rubber or a cleaning felt, are exchangeable. They sit very firmly on the plastic base, which is why this is not obvious. To loosen them, they only have to be pulled off with a little force.

The adhesive power of the pads is even retained, which is helpful if the positioning doesn't work right away. Since the Modellbahnunion offers replacement sets, nothing stands in the way of a long-term use of the devices.

Easy and effective to use

It is very easy to use, which is all the more welcome because there are no instructions in German or English. All text is written in Japanese, but are accompanied by small illustrations that are quite comprehensible. We do not consider another document necessary.

In the case of the electrically powered tool, two alligator clips are connected to the rails on the layout or the transformer, the speed controller, or the digital control unit.

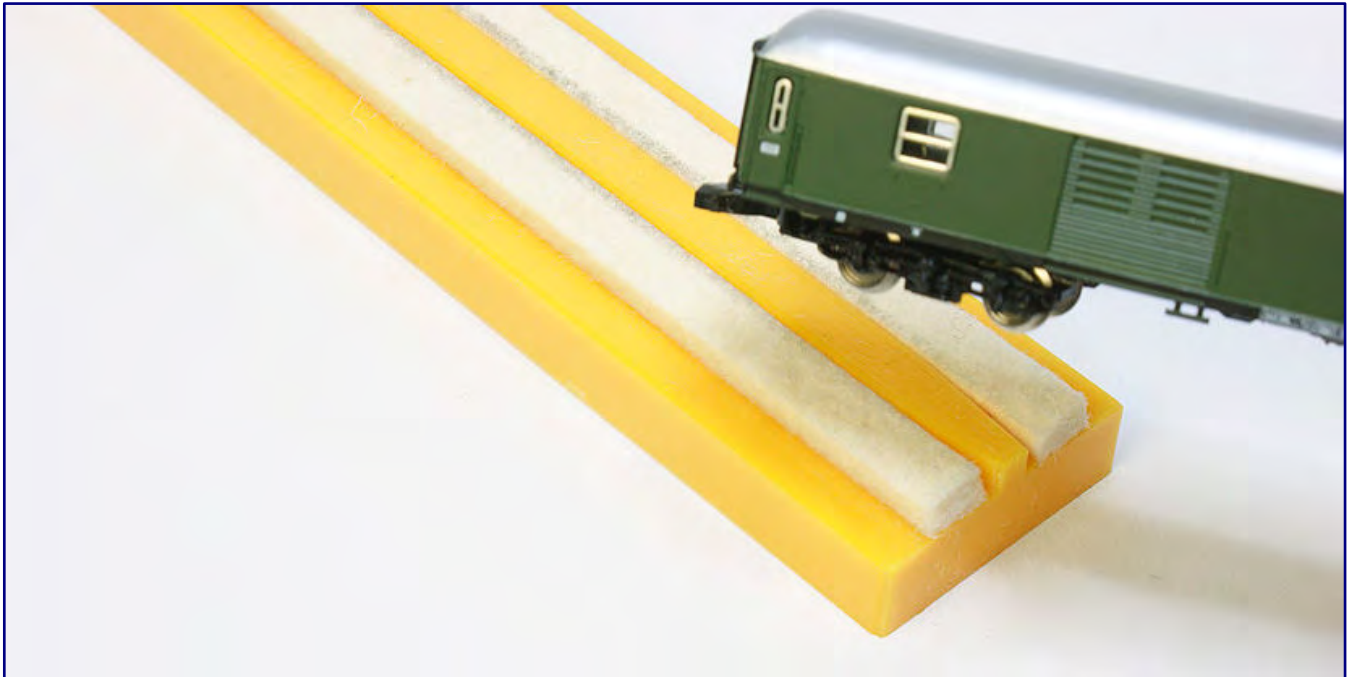


The wheelset cleaner's alligator clips are simply clamped to the rails or attached to the transformer connections in order to draw the traction voltage from there.

As soon as the locomotive is positioned on the wheel cleaner, it is held in place with light pressure and supplied with traction current. The rotation speed of the wheels is determined by the voltage or speed level setting (digital operation).

As unpowered wheelsets cannot be cleaned by this system, the second tool is available without electrical connections. It is suitable for running wheels of locomotives and all unpowered models.

For this purpose, the locomotive or wagon is rerailed in the same way and then pushed backwards and forwards by hand under slight pressure. In this case, rerailing is even easier because a rerailing aid is integrated at one end.



The yellow cleaning device has a guide at one end to guide the wheels cleanly onto the felt. Once in position, the model is pushed back and forth under light pressure.

Dirt that sticks to the felt or foam is gently cleaned without any scratching. The second cleaner just mentioned comes with a small additional tool: A small plastic profile with the inscription "Rail Cleaner" indicates what it is for.

The three felt strips glued to it are placed crosswise on the track, also pressed down lightly and then moved over the head of the rail profile, from which they then also pick up dirt. It works similarly to the cleaning felts from System Jörger, and therefore requires similar access spaces.

Under catenary, on the other hand, things can get difficult. A certain degree of care and repeated reaching around will be unavoidable, which is why a cleaning trolley according to the Jörger system (Märklin 86501) can be an alternative here.

Cleansing additives?

Dry and stubbornly adhering dirt can drive model railway enthusiasts crazy. Therefore, sooner or later the question arises whether the cleaning performance can be increased with liquid additives?

Here we should first think about which type of product can be used: Soaps should be discarded because of a smear film; spirit cleaners and petroleum ether should also be discarded. They also contain gelling agents and other chemical components whose material compatibility and complete evaporation leave room for doubt.

The only remaining solution is pure simple alcohol, such as isopropanol 99 %, which can be obtained from a pharmacy or possibly a drugstore. It degreases, dissolves many substances, cleans well, evaporates completely, and, according to experience, does not damage traction tyres.



We also test whether the cleaning effect can be increased with alcohol, which we apply to the felts with the help of a pipette until they are thoroughly soaked.

So, the questions that remain to be answered are whether an added value can be established and whether problems (short circuits) are to be expected in the electrical design? We got to the bottom of this with some practical tests.

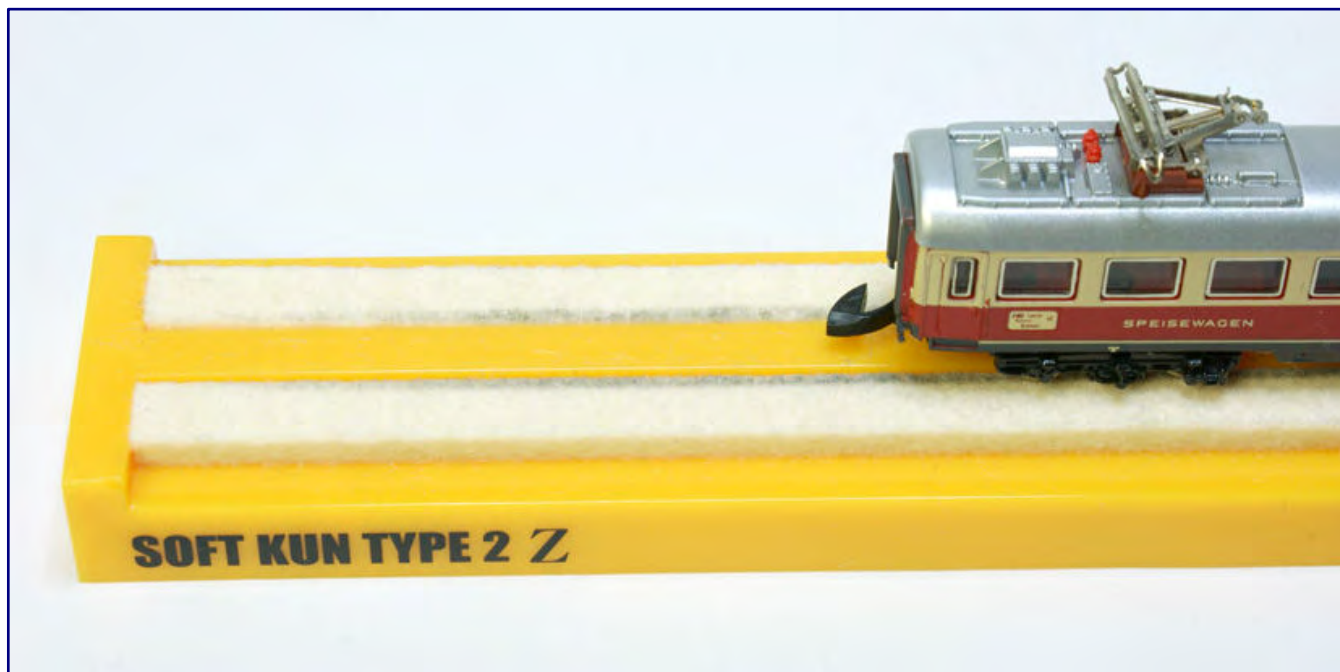
We chose different models in order to make it not too easy for the testers: Locomotives of all traction types (i.e., different construction characteristics), with and without traction tyres as well as coaches in different states of dirtiness.

Particularly in the case of the passenger coaches, specimens were used that showed extremely dirty wheels after many hours of operation at exhibitions and no longer ran reliably. They were to pose a special challenge and to clarify whether the new devices could cope with any condition.

The first test run was for the yellow tool for non-driven vehicles. Here we tested a rolling stock model of the 8970 series, cleaned a steam locomotive tender, a refrigerator car and several long passenger coaches with heavily soiled wheels.

Already after some pushing back and forth, traces of dirt appear on the light-coloured cleaning felt. A check on the wheels shows that the less dirty ones have regained their shine. However, we also notice fibres that have come loose from the felt and are now sticking to the wheel, bogie or wagon floor. Fortunately, they can be blown away and do not pose a problem.

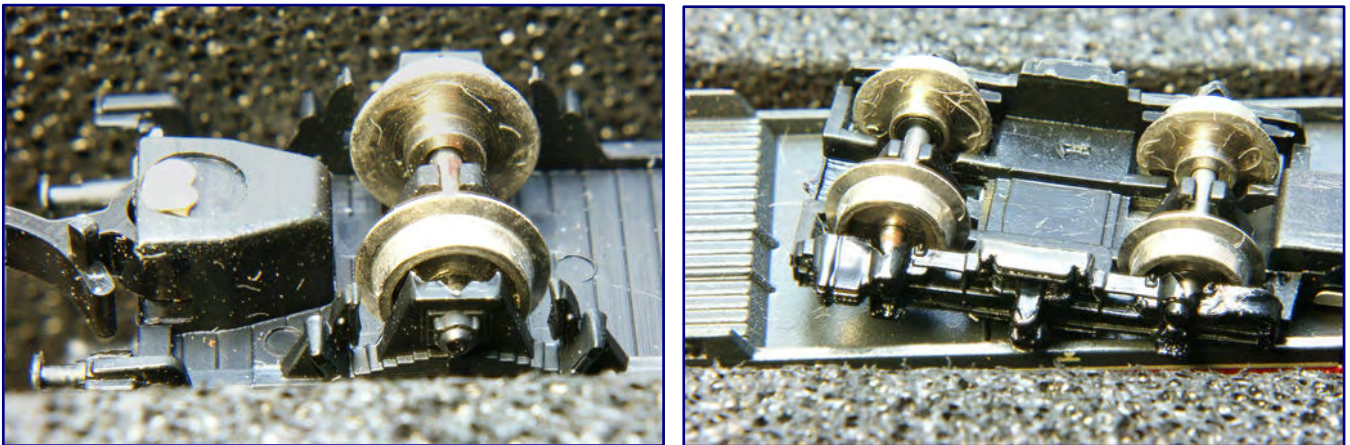
We have noticed improvements on the more heavily soiled axles, but not yet an all-round satisfactory solution. We had expected this, which is why the question of an additional cleaner arose.



Even when dry, the felt removes some dirt (top), but, in combination with isopropanol, the cleaning performance increases even more rapidly (bottom).

So, we applied isopropyl alcohol to the felt with the pipette (available from Faller or Tamiya) until it was well soaked. Afterwards, we repeated the cleaning runs with the test candidates that had been measured.

The cleaning effect increased significantly once again, even these wheels now became completely shiny again without suffering a scratch, as evidenced by our macro photos. The adhesion of the fibres has also increased, due to the moisture of the alcohol.



After cleaning with the help of simple alcohol, the now shiny wheels of the freight car (left) and the dining car (right) shine again - for comparison, see the photos on page 28, which show the condition before the procedure. Only the adhering felt fibres disturb the overall picture somewhat, but they can simply be blown away as soon as the alcohol has completely evaporated.

But as soon as it had evaporated, they could be completely blown away again. This tool impressed us and definitely proved its practicality. Only one question remains in view of the dirt now clearly visible on the cleaning felt: How long can the wheelset cleaner be used?

The Modellbahnunion seems to have asked itself this question as well, because the cleaning felts are also offered as spare parts. This makes the tool a permanently usable investment. Even if one of the felts does not fit perfectly, it can be pulled off, realigned and pressed back on. And felts that are dirty on one side can be turned over and used on both sides before replacement is required. The adhesive strength is high enough in any case.

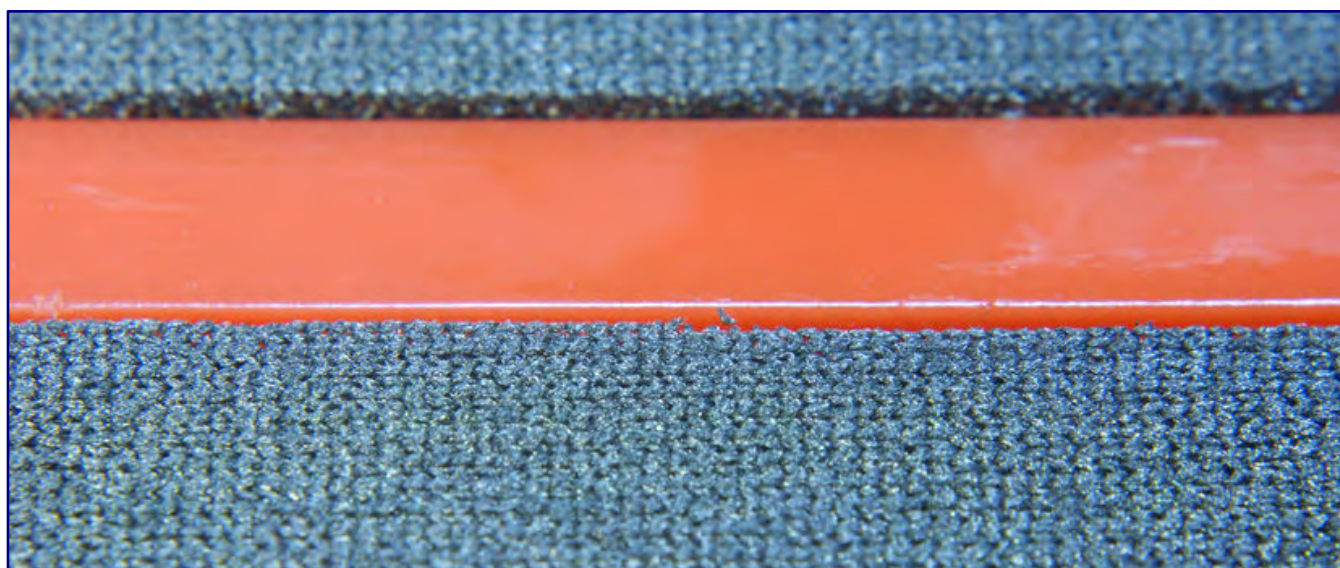
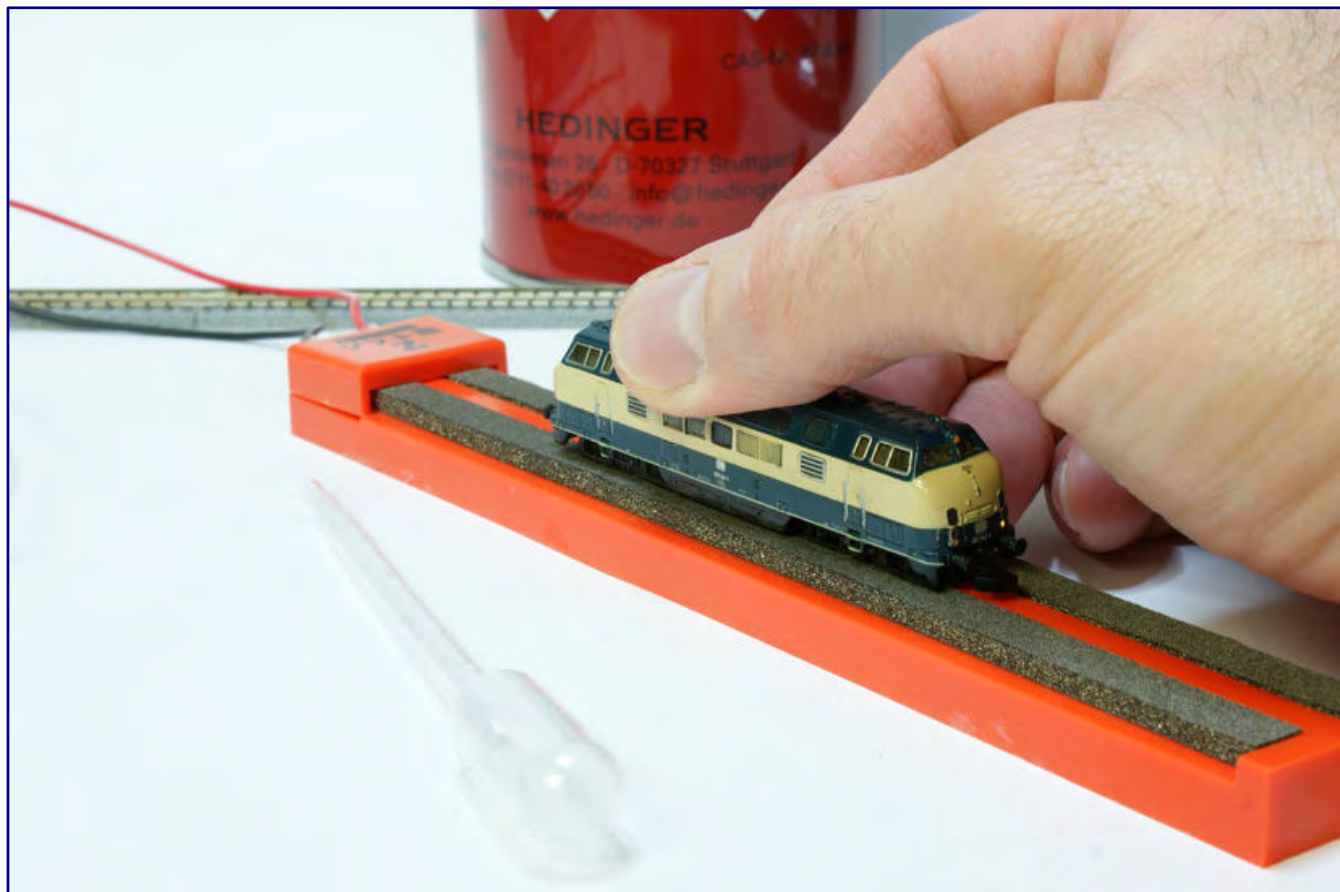
The cleaning dynamometer

We would like to casually refer to the orange tool as a cleaning dynamometer and thus distinguish it from the previously tested model. It is intended for motorised vehicles, because here the rotation of the wheels from the motor is used for the cleaning process.

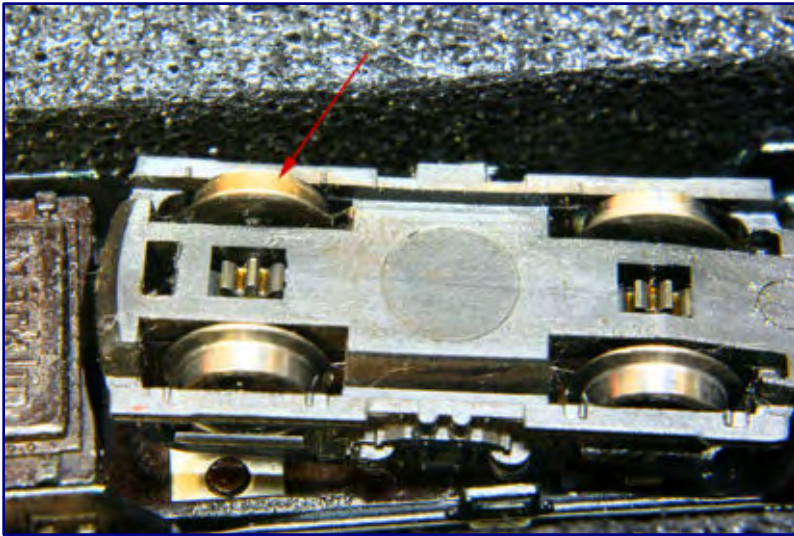
Instead of felts, it has a conductive surface that also performs the polishing function. In the lower part, it consists of a soft foam material that ensures the largest possible surface area on the wheel is cleaned at the same time. On the surface sits a textile-like material that can absorb dirt.

In the macro shot, it looks like the finest fibres containing metal, but it feels far too soft for that and doesn't scratch either. Our guess is a microfibre that is not so easily saturated with dirt. And off we go with another attempt.

After the first impression that this fibre cleans even better than the felts of the previous device, we also started a test with isopropyl alcohol. This tended not to be necessary, because most of the wheels were already very thoroughly cleaned without the adding alcohol.



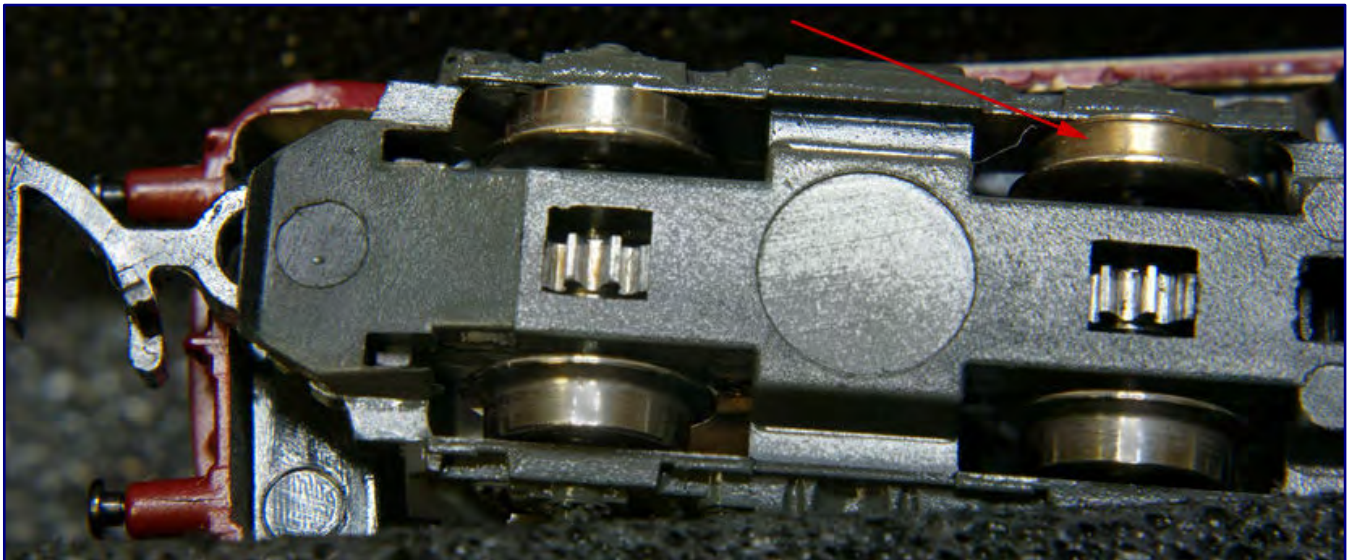
The electric version of the cleaning device also is tested with isopropanol, which produces promising results with this Class 221 equipped with traction tyres (top). Its cleaning strips look like tiny metal fibres when magnified (bottom), but they are not and are also completely scratch-free, which is why they do not leave any grooves on the wheel surfaces and do not damage traction tyres.



However, at least one locomotive wheel still required some additional cleaning and so the anxious question of whether the electrical device would provoke a short circuit when alcohol was used could also be clarified. Proceeding with caution and using exactly the right amount of cleaning alcohol from the pipette, short circuits could be avoided without any problems.

Even the last wheel was spotless, while the traction tyres were not damaged at all. It couldn't be easier to clean a locomotive wheel, which is sometimes necessary at home.

The additional use of isopropyl alcohol seems sensible and possible to us, we can even attest to its added value. At the latest in combination with this, both wheelset cleaners become valuable aids for model railway maintenance, whereby we would even like to give preference to the orange tool for powered models.



The wheels of the Class 221 (top left) and the 218 have also been completely cleaned. The traction tyres of both models (see arrow markings) have not suffered any damage.

The reason for this is that the cleaning power of the surface is higher and also seems to work when a unpowered model is pushed back and forth on it, for example to clean the running axles of a steam locomotive. The fact that no fibres are transferred to the model is an additional advantage, because otherwise they could get into the gearbox when the wheels turn.

And so, the Z-gauge wheelset cleaning system (MU-Z-A12508), as a simple-looking, but nevertheless extremely well-thought-out auxiliary tool, now also receives a nomination for our 2021 best new releases of the year award in the technology category.

Supplier of the cleaning devices:
<https://www.modellbahnunion.com>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Das letzten Aufbäumen der Dampflok **Auf zum großen Dampffinale**

1973 sollte in Westdeutschland Schluss mit der Dampflok sein. Doch 1970 trat ein ungeahnt starkes Wirtschaftswachstum ein, dass der Dampflok noch eine Gnadenfrist bescherten sollte. Viele Eisenbahnfreunde nutzten dies, um die verbliebenen und wieder aktivierten Maschinen auf Filme zu bannen. Einer von ihnen war Rainer Schnell, der seine Erlebnisse in einen packenden Bildband zu fassen wusste.

Rainer Schnell
Schluss-Akkord
Der Bundesbahn-Dampfbetrieb 1970 - 1975

Geramond Media GmbH (VGB Geramond)
München 2021

Gebundenes Buch
Format 22,8 x 29,6 cm
260 Seiten mit ca. 450 teilweise farbigen Abbildungen

ISBN 978-3-96453-294-7
Preis 39,99 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Autor Rainer Schnell hat schon als Jugendlicher Interesse an der Fotografie gefunden. In Verbindung mit einer Passion für Lokomotiven und Züge ergeben sich beste Voraussetzungen für ansprechende Dokumentationen längst vergangener Zeiten.

Zu Gute kommt den Lesern dabei ebenso, dass er nicht Wert auf möglichst vollständige Bildreihen aller Baureihen gelegt hat, sondern auch besondere Freude daran hatte und hat, die Eisenbahn in ihrem Umfeld oder in der Landschaft zu zeigen.

Bereits aus seinem früheren Band „Bundesbahn-Tour '68“ im gleichen Verlag ist er uns als Teil der selbsternannten „Hamburger Fotomafia“ bekannt – es gibt allerdings noch weitere Titel aus seiner Feder. Das vorliegende Buch schließt etwas zeitversetzt an die zuvor genannte Lektüre an.

Inzwischen sind die jungen Fotografen erwachsen(er) geworden und vor allem auch mobiler. Ihre Fotoziele erreichen sie nicht mehr nur per Bahn, sondern sie können Zügen auch hinterherreisen oder sie fallweise verfolgen. Spannende Dokumentationen in einer Zeit, in der der Dampflokbestand rapide abnehmen sollte, sind garantiert.

Möglich machte die vorliegende Zeitreise aber trotzdem nur eine Entwicklung, die dem herangewachsenen Autor und seinen Freunden „außerplanmäßig“ entgegenkam: Nach ihrer ersten



wirtschaftlichen Depression erlebte die Bundesrepublik Deutschland ab 1970 einen rasanten Aufschwung, in dem zeitweise sogar wieder Vollbeschäftigung erreicht wurde.

Dieses außergewöhnliche Wirtschaftswachstum zog derart erhöhte Transportleistungen nach sich, dass die Bundesbahn ihr ehrgeiziges Ziel, die Dampflok ab 1973 endgültig abzustellen, außer Reichweite rückte. Sie musste sogar von der Ausbesserung zurückgestellte und teilweise gar ausgemusterte Maschinen wieder in Betrieb nehmen.

Und so erlebte die Dampftraktion eine letzte und unerwartete Renaissance, die mit der Ölpreiskrise 1973/74 ihren Höhepunkt erreichte. Nach Umbau einzelner Exemplare auf Ölhauptfeuerung folgte nun die Wiederinbetriebnahme kohlegefeuerter Dampflokomotiven.

Im Betrachtungszeitraum des Buches, also von 1970 bis 1975, erlebten Dampflokfreunde einmalige und zutiefst beeindruckende Jahre, die hier in einem ausführlichen Abriss, der sich von der Küste bis an die Alpengrenze erstreckt, wiedergegeben werden.

Rainer Schnell nimmt seine Leser mit auf eine Reise, die im Norden beginnt und, immer nach Osten und Westen blickend, in den Süden der Republik führt. Wir erleben also das große Dampffinale in einzigartigen Ausnahmen, die mit Bedacht ausgewählt und hervorragend reproduziert sind. Dass auch viele Farbbilder ins Buch gefunden haben, lässt es umso lebendiger erscheinen.

Und der Autor hat sich auch nicht allein auf seinen eigenen Bildbestand zuzüglich Aufnahmen seiner mitreisenden Freunde, von denen einer leider bereits verstorben ist, verlassen. Lücken in dieser Deutschlandreise konnten mit ebenso ansprechenden Aufnahmen befreundeter und bekannter Bildautoren bestens gefüllt werden.

So ergibt sich hier ein buntes Sammelsurium an Eindrücken und Geschichtsdokumentation: alle damals noch bestehenden Dampfviehere fanden Eingang und sämtlich Baureihen, die noch aktiv waren, sind auch im Buch wiederzufinden.

Hinzukommen Einzelstücke, Raritäten auf Schienen, einige DR-Fahrzeuge und Splittergattungen – übrigens auch abseits der Dampftraktion wie etwa in Form der 232 001-8 (ex V 320 001). Jedes Dampfviehere wird in einem einleitenden Text kurz vorgestellt.

Über dargestellte Besonderheiten, Erlebnisse, Vor-Ort-Planungen und Randerscheinungen wird der Leser ins Geschilderte und Dokumentierte eingebunden. Er oder sie taucht sprichwörtlich in die Geschichte(n) ein und meint, dies mitzuerleben.

Vor uns liegt deshalb ein erfrischend geschriebener, äußerst unterhaltsamer und bestens bebildeter Band, den wir im Regal gewiss nicht mehr missen möchten. Für Dampflokfreunde sollte er deshalb auch das perfekte Weihnachtsgeschenk sein.

Dies honorieren wir zum Abschluss deshalb auch mit einer Nominierung für die Neuerscheinungen des Jahres 2021 in der Kategorie Literatur.

Publishing pages:
<https://www.vgbahn.shop>

Chemietransporte und die Eisenbahn Spezialthema unter den Güterwagen

Mit Sehnsucht warteten viele Eisenbahnfreunde auf die Fortsetzung der beliebten Güterwagen-Reihe von Stefan Carstens. Während wir hier nun den ersten Band dazu den Chemiekesselwagen vorstellen möchten, ist Band 2 zum Thema bereits im Druck. Wessen Lesehunger also nicht gestillt werden kann, braucht nicht lange auf Nachschub zu warten.

Stefan Carstens / Harald Westermann
Güterwagen Band 9.1
Chemiekesselwagen Länderbahn und DRG

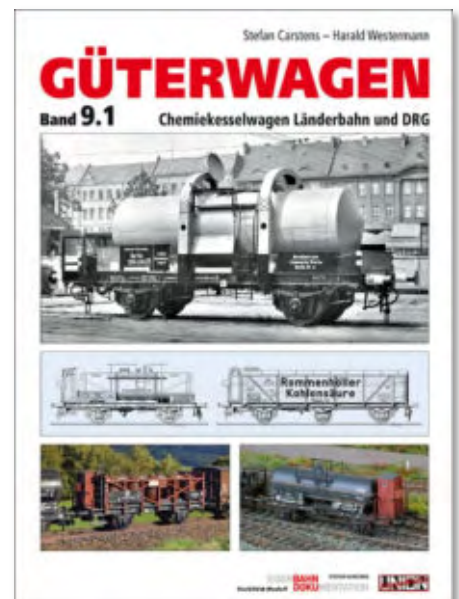
Stefan Carstens Eisenbahn-Dokumentation
Hamburg 2021

Gebundenes Buch
Format 21,8 x 29,7 cm
144 Seiten mit 370 teilweise farbigen Fotos und 90 Zeichnungen

ISBN 978-3-9823-2220-9
Preis 33,50 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Buchautor Stefan Carstens brauchen wir wohl kaum mit vielen Worten vorzustellen: Eisenbahnbegeistert von Kindesbeinen an, in jungen Jahren auf Fotopirsch, dann Berufseisenbahner und seit 2011 ausschließlich Buchautor. Das ist die Kurzform einer Vita, die ausführlicher auch auf seinen eigenen Verlagsseiten nachzulesen ist.



Auch der Rezensent des heute vorzustellenden Buches schätzt diesen Autor durch persönliche Kontakte und durfte schon für eine seiner Broschüren selbst tätig werden. Wie kein anderer Autor in Deutschland gilt Stefan Carstens als hervorragender Kenner von Güterwagen, was er Band für Band immer wieder unter Beweis stellt.

Aber wir wollen auch seinen mehrfachen Co-Autor Harald Westermann nicht vergessen, der mit ebenso viel Herzblut und Fachkenntnis dabei ist. Allein wären die Mammut-Aufgaben dieser einmaligen Reihe wohl nicht zu schaffen, was auch der „Hamburger durch und durch“ immer wieder betont.

Doch auch in kühnsten Träumen hätten wir nicht geahnt, wie vielseitig und schwierig aufzuarbeiten die Geschichte der Chemietransporte auf Deutschlands Schienen sein könnte. Aus einem geplanten Buch wurden zwei, aber selbst darin lässt sich nicht alles unterbringen, was es zu berichten gäbe.

Dem gegenüber steht eine Modellbahnseite, die mit solchen Wagen in allen Baugrößen stark unterversorgt ist. Woran das liegt, wird bei der Lektüre schnell deutlich und helfen kann hier wohl nur der Eigenbau, der sich längst auf den 3D-Druck als perfekte Umsetzungstechnik stützen kann.

Solche Eigenbauten bestimmen auch den Modellteil der Kapitel. Sie sind quasi ein Wink an die Hersteller, welche unentdeckten Tätigkeitsfelder es hier noch so gibt. Auch die Spurweite Z macht hier keine Ausnahme, wobei dies eher einen Aufruf an die Kleinserienhersteller darstellt.

Doch der Reihe nach: Wohl kaum ein Wagenthema ist so vielfältig wie das der Chemiekesselwagen, schreibt auch der Autor. Ein Waggon, der Laugen, Säuren oder andere flüssige Rohstoffe der chemischen Industrie befördern soll, braucht mehr als nur einen Stahltank auf Rädern.

Säuretopf-, Flaschen- und Weinfasswagen als frühe und bekannte Vertreter dieser Spezialgattung sind wohl jedem Modellbahner bekannt und finden sich selbstverständlich auch in diesem Buch wieder. Doch die Ansprüche und auch die Zahl der Transporte wuchsen mit den Jahren, denn die Chemiesparte erlebte, unter anderem durch das Erfinden von Kunstdünger und vielen Kunststoffen, einen erheblichen Aufschwung.

So mussten neue Lösungen her und vor allem entstand auch ein Bedarf an besonderen Werkstoffen, die beständig gegen aggressive und giftige Inhalte waren oder hohen Drucken standhalten konnten. Ein besonderes Problem bereitete auch die feste Montage von Kesseln auf den Wagengestellen, denn Schweißen war hier (noch) nicht möglich.

Die beiden Autoren beschreiben in ihrem Buch die gesamte Geschichte dieser Spezialwagen über alle ihre Erscheinungsformen auf zwei (oder drei) Achsen. Im Fokus stehen spezielle Werkstoffe oder, wo diese noch nicht entwickelt waren, auch die Innenauskleidungen der Kessel. Ebenso wenig fehlen Wagen zum Transport von Druckgasen, Kohlensäure und flüssigen Sauerstoff.

Allen gemein ist, dass sie – mit nur ganz wenigen Ausnahmen, das dann oft kriegsbedingt – in kleinen Wagenserien gebaut und bei privaten Einstellern im Dienst waren. Hier liegen die Ursachen, warum die Dokumentationslage eher schlecht ist und solche Vorbilder auch selten ins Modell umgesetzt werden. Dafür werden die Eisenbahnfreunde teils auch mit bizarren Bauformen belohnt, die umso größeren Reiz für eigene Projekte bieten.

Traditionell wird diese Lesergruppe besonders gut bedient: Es fehlt nirgendwo an Maßangaben neben anderen Betriebsdaten und Beschreibungen und vor allem auch nicht an Zeichnungen, die für Umsetzungen ins Modell unverzichtbar sind. Und natürlich haben beide Autoren all das auch wieder mit umfangreichem Bildmaterial vor allem aus den eigenen Archiven angereichert.

Herausgekommen ist ein erster Band, der die Entwicklungsgeschichte bis etwa 1950 behandelt und mit einer Fülle von Informationen solche Wagen behandelt, die sich erstaunlich lange und teilweise bis in die Neunziger behaupten konnten. Deren Umbauten werden sich zusammen mit den Neubauten ab 1950 übrigens im zweiten Band finden, der noch im Dezember erscheinen soll.

Für wen ist dieses Buch also nun gedacht? Wer die Reihe bereits im Regal stehen hat, wird auch diesen Titel längst erworben haben, denn bereits vier Wochen nach Erscheinen war schon die halbe Auflage verkauft.

Lohnenswert ist er darüber hinaus für alle diejenigen, die sich für diesen wichtigen Wagentyp interessieren, ihre Modellzüge ansprechend erweitern möchten und auch vor dem Umbau nicht zurückschrecken.

Und wer sich einfach für Eisenbahntechnik und -Geschichte interessiert, wird hier ebenso gut bedient. Wir können das auch so zusammenfassen: Wer unser Magazin gern liest, der wird auch an Stefan Carstens neuem Meisterwerk seine Freude haben.

Nur lange warten sollten er oder sie dann nicht, denn lange wird dieser Titel wohl nicht mehr verfügbar sein, sofern sich nicht eine zeitnahe Nachauflage lohnen sollte. Wir belohnen die beeindruckende Arbeit, die hier geleistet wurde, mit einer Nominierung für die Neuerscheinungen des Jahres 2021 in der Kategorie Literatur.

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: Publishing pages with reference possibility: :
: <https://www.stefancarstens.de> :
: <https://www.modellbahnunion.com> :
:.....

Staying in the conversation (part 7)

Modellbau Laffont – Z Gauge in the Blood

Modellbau Laffont has been active for years with its own programme for the Z gauge, but was, for many, surely unnoticed, also, previously as a supplier to well-known manufacturers such as Märklin. However, the kits are always produced with the help of laser-cutting technology and are sent around the world from Aachen. Today, Stefan Laffont introduces himself, his company history, and his philosophy.

By Stefan Laffont. Born in a big city, I became acquainted with railways and tramways at a very young age. Steam trains under large street bridges with tall and spacious buildings had an impressive effect. There were a large number of level crossings with barriers in the city area, at which, to my delight, people often had to wait.

Most fascinating at first were the trams with their bobbing pantographs, constantly moving slightly up and down under a tangle of wire that was completely confusing at crossings.



Stefan Laffont (Modellbau Laffont) has skilfully combined his youthful passion and profession.

That was also the first thing I wanted to try out on a model with friends and acquaintances, but was rarely allowed to. I wasn't allowed to do it with the few friends of my parents who owned a model railway: a child would destroy it immediately. That was how it was in the seventies...

After so much research, studying engineering was not far away. Of course, after a lot of nagging, I finally got my own first model railway at the age of five, a class 89 with K track and, finally, sometime later, a 103 with the corresponding carriages, which had constantly lost its pantograph springs after a lot of bobbing.

I wanted to build an overhead line out of strands and sprue remnants of the usual building kits, of course that couldn't work, but the will was there. The railway grew with me and was extended with the rolling and kit material known from that time.

At the end there was not much space in my rooms and at the then still small, but now very well-known dealer, I got into Z gauge.

I had enjoyed looking at these in magazines in the seventies and eighties, but had not seriously considered them because of their tininess. Also, there were mostly (from my point of view at that time) rather amused fathers and mothers pictured, around whose hats Z-gauge trains were circling.

But now the tininess of this nominal scale came in handy and many landscapes with buildings for more and more trains were built. Of course, an overhead line was always obligatory here as well.

Eventually, however, H0 gauge came back into play and was expanded by the technical and optical improvements that had become available in the meantime. With a lot of passion, I built here as well. There were also smaller excursions into the 1 gauge and N gauge world for a variety of reasons.

But how do you become a series manufacturer of architectural model kits for large and small model building companies?

The leap to manufacturer

With the then emerging laser cutting technology and some stops and coincidences, it suddenly became possible that corresponding activities for the model railway industry were possible, and still are today. In the last fifteen years, numerous kit series for building and bridge construction have come into being.

It is very exciting for me to always find ways and means, not only through technical tricks and aids, but also through planning and optimising organisational processes in terms of production technology, to enable economic operation.



Modellbau Laffont now has a wide range of products, including these rural buildings for Z scale.

It is a blessing to be able to develop creativity. Here, the model railway offers not only for the hobbyist, but also for manufacturers an undreamt-of wealth of design possibilities that always give me a lot of pleasure.

Already during construction, a model is first created in the mind. I already consider how many layers of cardboard the model should be comprised of, in order to achieve as three-dimensional of an effect. as possible.

At the same time, however, I must not lose sight of the cost side. Every model is an interplay of emotions and technical possibilities. At the same time, every model is inevitably a compromise between what is conceivably possible and what is realistically required due to its reduction in size.

The most beautiful thing is when a pattern emerges for the first time from the idea I always have at the beginning. When I see it, it is always a special moment. Through a creative process, the model in my head finally becomes a structure with which I am either satisfied, or to which I still want to make changes and improvements in the construction.

The locomotive line (left) complements the roundhouse designed for Märklin to the latest version of the turntable. The small town station is also likely to strike a chord with many Zetties. The colours available as architectural hardboard also play a major role here; colour nuances are to be specified, as are board thicknesses and substructure materials. Surfaces of façades, i.e., plaster, brick, quarry stone, and sandstone façades, look best made of solid-coloured hardboard, which “by nature” appears dull and matt.



The locomotive management (left) complements the roundhouse designed for Märklin to the latest version of the turntable. The small-town station is also likely to strike a chord with many Zetties.

For some models, plastic parts are also useful, e.g., the coal chute in Hunt's large coaling system. Close cooperation with appropriate partners is then required here.

3D printed parts made of plastics as well as high-strength ceramic material are also used for various models. Advances in detailing are now reaching the next smaller gauge, i.e., what was considered the

finest detailing in N gauge 20 or 30 years ago can now be implemented in Z gauge. The same applies to the other nominal scales.

From my point of view, the special thing about Z scale is that generous building ensembles can be created, even with the finest details. Landscaping, with a pleasantly wide view, is possible here, whereas in other scales this can only occur on show layouts.

When using high-quality machines, laser cutting technology largely avoids undesired colour changes, i.e., small burns on surfaces and edges. This has a very positive effect especially with Z gauge, as the components are sometimes very small. Very realistic surface structures can also be created with the finest micro-engravings.



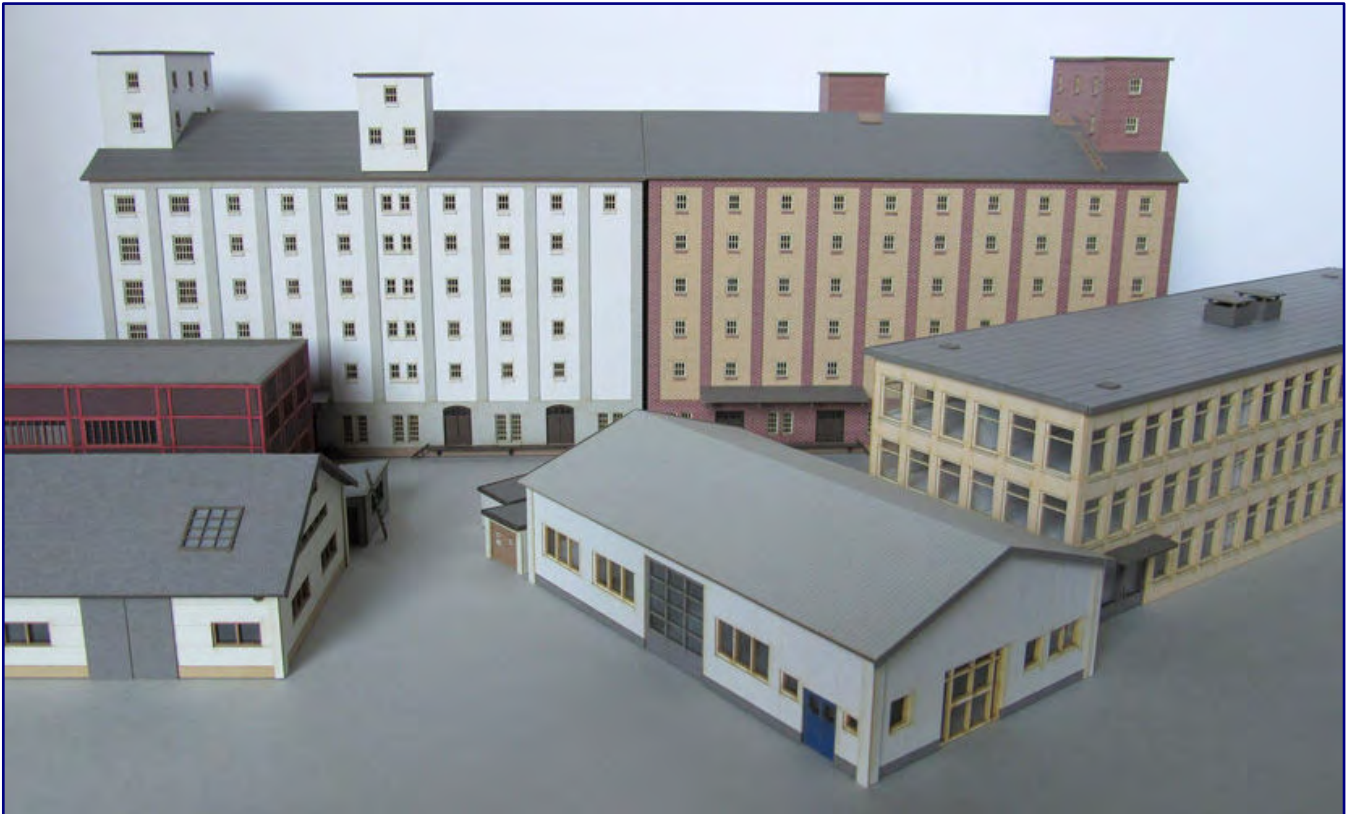
This photo shows a selection of the buildings based on urban models, including the arcade sections for an elevated railway line, which also remains visible between the rows of houses.

Especially for the scale 1:220 a large effort is justifiable, as the areas to be engraved remain relatively small. For the larger gauges, the surfaces to be treated multiply and are very time-consuming, especially for gauge 1.

All in all, hard cardboard models cannot be produced more cheaply than plastic kits, even though the necessary moulding costs are eliminated. The reason for this is that the number of pieces is usually lower, or even significantly lower in the case of small series.

This means that the construction costs per piece are much higher. Smaller quantities are a consequence of the lower demand, at least in recent years (due to age), but in the same way, also of the increased desire for more individual structures.

This is where laser cutting technology comes into its own. That is why I am not only a specialist for Z gauge, but also serve other scales that benefit greatly from the technology I use.



The various industrial buildings form a previously unmentioned thematic focus. With the administration building (far right) and the machine hall (2nd row left), Modellbau Laffont entered the Z scale market.

A lot of important preparatory work

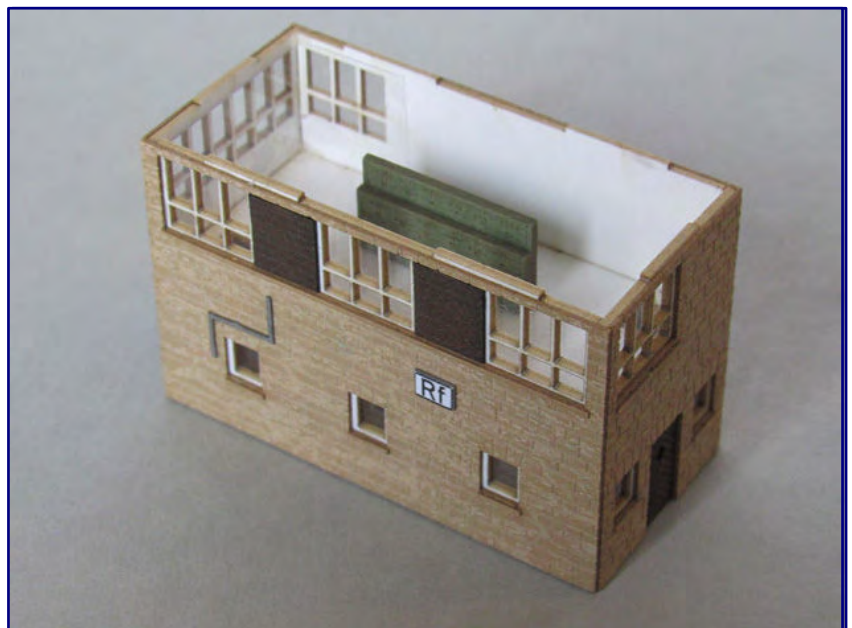
I attach great importance to carefully documented building instructions based on numerous 3D drawings.

All components are numbered and shown three-dimensionally.

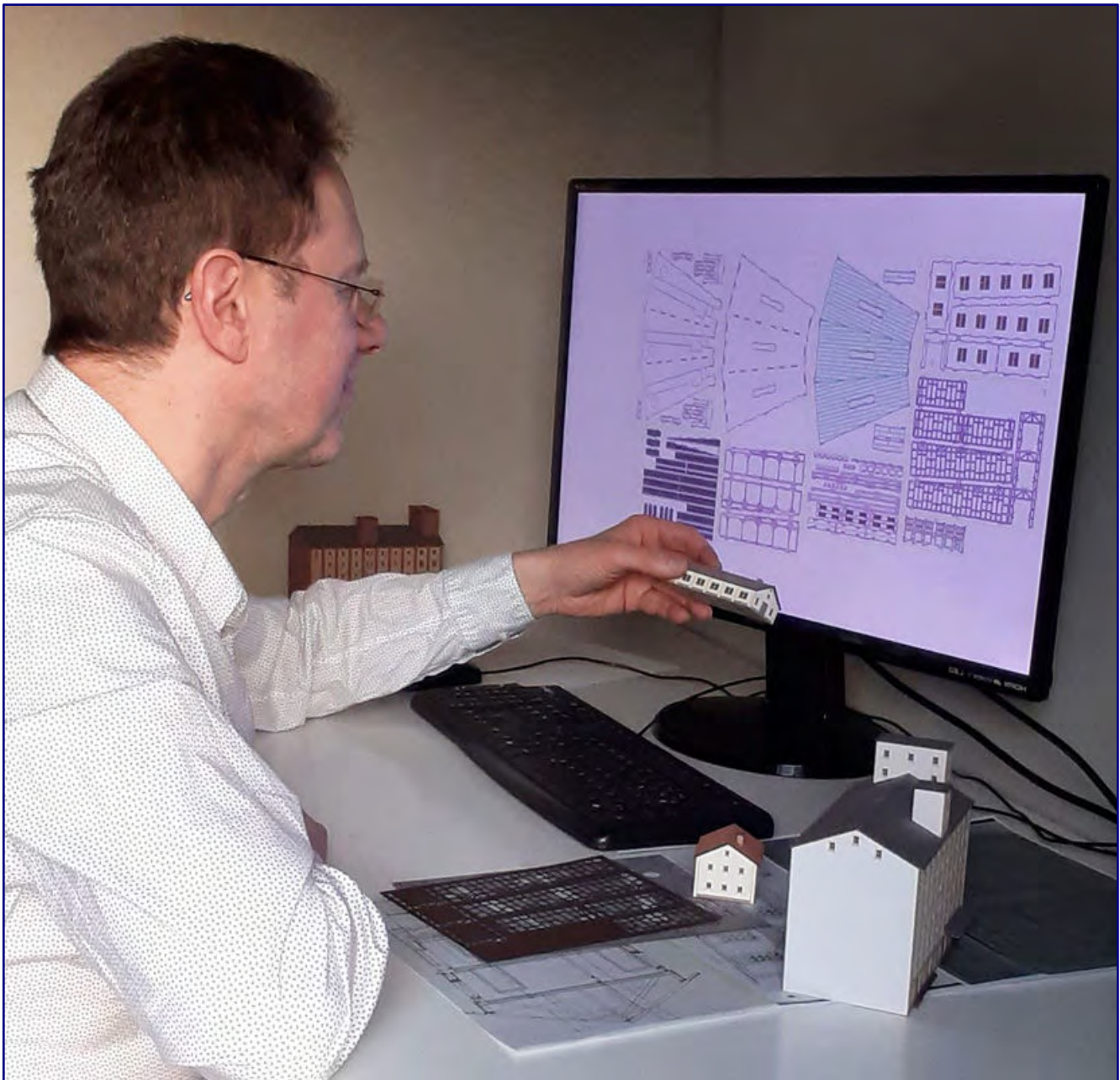
Marked with the exact position by (mostly coloured) arrows, I usually show the component numbers in a different colour. The assembly sequence is clearly visible.

In the next construction step, the correspondingly assembled components are integrated in the next 3D drawing.

Depending on the complexity, the model is completed more and more in numerous steps in the building instructions, so that the hobbyist can compare his own building progress with the result of the drawing.



Visible substructure of the Rodalben signal box with the electromechanical lever mechanism depicted as interior equipment.



The construction of the kits is done by CAD on the screen. The three-dimensionally illustrated instructions are also created with the help of this technology. A constant comparison of the drawing and test constructions leads to an optimised and market-ready product.

By the way, this method also serves my control, because each component is fixed in the CAD programme at exactly the intended position, so that dimensional or other errors are noticed immediately, because the components are transferred with the original dimensions from the 2D drawing into the 3D data. Of course, several samples are also built in each case!

In addition to my extensive activities as a subcontractor, I constantly come up with numerous ideas of my own. And I'm always happy to finally be able to realise some of them. Two very recent new products are the two signal boxes from the Pfälzer (Palatinate) Forest, which can be used in almost all of Germany and even for other countries due to their design:

1. Signal box Rinnthal with plastered facade and colour-contrasting window surrounds (item no. Z3401), and



2. Signal box Rodalben with quarry stone facade and wooden boarding in the window row (Z3501).

In the case of Rodalben, in particular, no further colour treatment is required thanks to specially developed micro-engravings for the representation of the quarry stone and slate roof structures and the resulting colour effects on the façade. The installed window sills also create a visible sculptural effect.

For the future, I would like to implement as many ideas as possible, whereby I always place great value on customer opinions and ideas.

After all, I am particularly pleased when my customers also have fun with the model and thus confirm to me that the described efforts up to production and delivery have been worthwhile.



The two latest ideas from Modellbau Laffont are the plastered signal box Rinthal (art. no. Z3401, photo at the top) and the quarry stone façade variant "Signal box Rodalben" (Z3501), which are announced as new products with this report.

All photos: Modellbau Laffont

Manufacturer and own sales pages:
<https://www.modellbau-laffont.de>
<https://modellbau-laffont.com>

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

We received the following comment on the test report in **Trainini®** 10/2021:

Many thanks for the test of the class 042. It came just in time for the “rotational” repainting of the Märklin buffer planks. At first, I was horrified by their photos of the front. But when I looked at the proportions on my loco and weighed up how often I would see it from the perspective shown, I decided on two small splashes of colour as an alternative.

I'll leave the sandboxes as they are and hope for Kuswa or Ladegut Küpper. Suggestion: Exchange part with 2 boxes each on the old bore... In this sense, once again, always steam under the boiler!

Christian Liebau, per E-Mail

Editor's reply: The old holes from the factory will not be usable for correctly placed sandboxes, as these were not only positioned twice in the 042 series (ex 41 oil), but also in a different place.

But we have already thought about a solution and have also placed a possible solution with a suitable supplier, where this is now being examined. If our proposal is successful and we can implement it successfully, then you will read about it again in **Trainini®**!

Anniversary at the 1zu220-Shop with special trolley:

On the occasion of its tenth anniversary, the 1zu220-Shop had an edition of 150 copies of the tank car “malZ” (art. no. 98183) produced by Märklin. The advertising here can be read twice as “Malz” (for this non-alcoholic beer drink) or also as “mal 2”, because two types of malt (barley and wheat malt) are used in it.



A few brush strokes already bring a clear upgrade of the Märklin model.
Photo: Christian Liebau

The capitalised final letter in the product name of the Westheim brewery also creates an intentional connection to our gauge and the trader's business model. We now let Managing Director Jörg Erkel have his say in person to introduce his latest idea:

“Ten years ago, I was still a tender 40 years young, we as the company HRT Informationstechnik GmbH applied to Märklin as a dealer. My own local dealer had closed his shop at that time and it was 30 km to the next one. Moreover, he could only be reached while I had to work myself.



As we were already an IT company with an electronic sales department, we had everything we needed for the planned business.

All that was left was a brand and since we wanted to concentrate on Z-gauge, the 1zu220-Shop (<https://www.1zu220-shop.de>) was born out of the idea.

In the following years we grew steadily, also thanks to many interesting projects and partnerships with various manufacturers. So, the 1zu220-Shop really became what it was supposed to be: the specialist dealer on the net for Z-gauge.



We solved the quickly cramped space conditions by moving into the administration building of the Westheim brewery. We used the first Corona lock-up to create a showroom for our Z gauge layouts.

It's great fun for us and we really hope that our customers enjoy it as well. Now we can look back on ten years and wanted to honour the anniversary with a special car.

It was clear to us from the first glance at the Westheimer malt beer logo, that the wagon would be suitable for this special celebration.

And now here it is, the wagon for '10 years of 1zu220-Shop': Curtains up for the tank wagon 'malZ'!

Märklin donation for flood victims:

The campaign launched by Märklin in July 2021 for victims of the flood disaster to generate a corporate donation from the sale of a special H0 scale model generated a total amount of 210,000.00 euros. This amount corresponds to the net proceeds from the sales of the model.



Handing over donations at Märklin (from left to right): Ingo Radtke (Federal Commissioner Malteser Fluthilfe), Edmund Baur (State Commissioner of Malteser Baden-Württemberg), Wolfrad Bächle (Managing Director Märklin) and Florian Sieber (Managing Partner Märklin). Photo: Märklin

Managing Partner Florian Sieber and Technical Managing Director Wolfrad Bächle presented a cheque to Malteser Cologne on Monday, 25 October 2021 in Göppingen, who will use the donation to rebuild day-care centres for children in the flooded areas of Rheinland-Pfalz (Rhineland-Palatinate) and Nordrhein-Westfalen (North Rhine-Westphalia).

A first joint aid project, into which 110,000.00 euros from Märklin's donation pot will flow, is now starting with all-day child care in the flood relief centre in Kreuzberg on the Ahr.

Still excellent for user-friendliness:

The accessories manufacturer Noch received an award for the user-friendliness of its electronic sales on 22 October 2021. The new pages that received the award went online only a short time before, namely one month before the award was given.

The “Shop Usability Award” has been awarded since 2008 and is considered the most prestigious award in electronic commerce in Germany. Dr. Rainer Noch was correspondingly proud when he received the award at the company headquarters in Wangen (Allgäu).

In this case, Johannes Altmann and his team drove all the winners off in a bus tour. Usually, however, the award is presented at a gala. The scenes of the visit to the Allgäu can be seen under the following link from minute 50:15:
<https://www.youtube.com/watch?v=S-cEqWDBLVw&t=3016s>.

Many beautiful new products at NoBa-Modelle:

We could fill several pages with the latest new products from NoBa-Modelle (<https://www.noba-modelle.de>), because the pace of new products development continues to be amazing. We are pleased to note that the high-resolution residual print has gained considerably in importance and is producing many very sophisticated models.



Awarded for user-friendliness. Photo: Noch

One of them is the prototypical four-part realisation of the RAm TEE diesel multiple unit train of the SBB, built in five units (2 copies), in identical design classified at that time in the Netherlands as DE IV (3 copies). Blanks are offered as kits (item no. 5214R) and finished models (5214RF).

The drive is located in the dining car. Modified Rokuhan Shorty bogies are used there as well as in the compartment and driving trailer. The three-axle bogies of the motor coach, on the other hand, are the coach's own design, which also has a power pick-up. The wagons are coupled to each other with magnetic couplings.





Hot new products at NoBa-Modelle are the RAM TEE (art. no. 5214RF; photo above and page 52 below) and its Canadian version as Northlander (5215RF). Photos: NoBa-Modelle

The superstructures of this model are printed in resin, the undercarriages in the PLA process. In the meantime, the "Northlander" version, equipped and produced in the same way, was also added to the range (5215R / 5215RF), which was in use in Canada.

Other car models and versions of well-known vehicles have also found their way into the programme. Examples include the Mercedes-Benz O 405 G articulated bus (6315R) or the GT 4 motorised articulated railcar (5711R).



The Mercedes-Benz O 405 G articulated bus (6315R) finds space on modern layouts. Photo: NoBa-Modelle

The excavators with crawler tracks and movable booms (6502R & 6509RN) are interesting for construction sites. We have also found an illuminated excavator team (9013) in the assortment.

To be added are three turnout lanterns (5506R), the Magirus D 256 three-way tipper as a blank (6316R) or finished model (6316RF) and the motor yacht (10477R / 40477RF) offered in the same versions.

With the other new products, it starts to get Christmassy: Four fir trees (10321R) are waiting for their own design, a stand for the Christmas market (4078) frames it appropriately. For many, the Cola Christmas wagons (8080) or the "Christmas truck" (9028) of the well-known beverage brand are also part of it.

Customers should already make a note of the brand's fifth anniversary, which is to be celebrated at the beginning of 2022, because NoBa-Modelle is also planning a campaign for this.

The current new deliveries by AZL:

The EMD SW1500 shunting and mainline locomotive, which has just been introduced as a new mould, is already appearing in further variants: two running numbers for the Reading (art. no. 62712-1 / -2) and Conrail (62710-1 / -2) as well as triple for the Burlington Northern (62707-1 to -3).



The EMD F7 is currently rolling out for the Canadian National. Available are an A-/B-double (63010-1) and a single A-unit (63010-2). Its sister EMD E7, meanwhile, shows a smart exterior in the design of the Great Northern (64614-1 / -2).



The 8,000-gallon 1917 tank car is offered in Texas Company black livery single (915007-1), double (915037-1) and quad (905007-1).

EMD SW1500 Der Reading (Art.-Nr. 62712-1; Bild oben) und Zweierpackung Kühlwagen der ATSF (914833-1; Bild unten). Fotos: AZL / Ztrack

And, so, they also cut a fine figure for Gibson Wine, set at GATX (915004-1 / 915034-1 / 905004-1). ATSF's R-70-20 refrigerated trucks are offered in identical configuration (914803-1 / 914833-1 / 904803-1).

Manufacturer photos of the current deliveries can be found at <https://www.americanzline.com>.

Christmas at Ladegut Küpper:

The load specialist from Aachen (<https://spur-z-ladegut.de>) draws attention to Christmas decorations for the model railway layout in 1:220 scale. Available there is a fir tree with a chain of lights made of ten LEDs or also a colourful chain of lights for outdoor terraces or as house decorations.

New Faller catalogue:

Faller has delivered its new catalogue "Modellbahn 2021/22" to the specialised trade. In addition to the special articles for the Z scale such as figures and buildings, it is certainly also very interesting for Zetties because of vegetation material, adhesives, lighting elements, electronics and tools. After all, this manufacturer has been one of the most active for our scale for years.

Furthermore, the responsible persons from Gütenbach have left a hint on a new model railway portrait for the joint image advertising "We model railway enthusiasts:" <https://wirmodellbahner.de/tobias-baumert>.

Striking refrigerated wagon at Full Throttle:

A 34-foot refrigerator car with wooden walls based on a reconstructed prototype (item no. FT-9011) is new at WDW Full Throttle (<http://www.wdwfullthrottle.com>). The wagon with silver-coloured side walls stands out due to the load "ICE" written in capital letters. The model is delivered in a double pack with two operating numbers.

In Germany, products from this manufacturer are available from Case-Hobbies (<http://case-hobbies.de>), among others.

Trainini Photo Calendar 2022 published:

On 16 November 2022, the long-awaited **Trainini Photo Calendar 2022** was published in German and English. It is intended to be a first framework for the upcoming anniversary "50 years of Z gauge" next year.



Traditionally, it can be printed at home or in a copy shop itself up to DIN A3 (landscape) format.

The 13 different motifs, including the cover page, are intended to give as representative a cross-section as possible of Z gauge.

The aim was to include as many large-series manufacturers, selected small-series suppliers, many epochs, all types of traction and railways from the most important markets of the small gauge as possible in order to show the diversity of our scale in beautiful photographs.

It can be accessed either via the messages on the start page or alternatively on your own target page under the menu item "Magazine".

French small-series manufacturer new to the market:

Distributed by O.L.D Modèles (<https://old-modeles.mozello.com>) in France are new small-series models by Azar Modèles (<https://old-modeles.mozello.com/store/category/azar-modeles/>), another French small-series manufacturer that recently entered the market.

On the mentioned pages you can see very appealing 1:220 scale miniatures of modern grain silo wagons "Transcéréales CTC" (also available in packs of three) and "Herforder Pils." The covered wagon G4 for cattle transport is based on a French model.

In addition, the SNCF diesel locomotive of the BB67400 series, which is to appear as a motorised model, is shown on its webpages. All products are based on 3D printing and are refined with metal add-on parts.

Other vehicles are also available on the supplier pages as 3D printed blanks for your own projects.

Märklin deliveries since the last issue:

Not quite a year after the Insider model, an ordinary catalogue variant of the class 280 (item no. 88804) has been delivered. In this case, 280 005-0 was selected in the operating condition around 1970.

The main difference to the Epoch III model is the painted decorative lines in RAL 1014 light ivory. After rusting appeared on the prototypes in the area of the white aluminium-coloured decorative strips, the Bundesbahn replaced them in this way during repairs.

This difference is also easy to recognise on the new delivery in comparison. What has remained unchanged is the rather tight coupling with a new guide, which sometimes makes automatic coupling difficult, as well as the limited freedom of movement of the bogies due to the design.



The new delivery of the class 280 (item no. 88804) serves era IV and has differences in the trim lines compared to the predecessor model.

Also delivered is the Christmas coach 2021 (80631) in the form of a Christmas printed 3rd class blunderbuss. Placed in a transparent wrapping, it can also be used as a tree ornament.

Also available is the popular steam railcar type Kittel in wine red/light ivory of the Deutsche Reichsbahn-Gesellschaft (88146) from the autumn new products of this year. Due to the great demand and short delivery time Märklin has rationed the deliveries to the dealers.

At the time of going to press, the DR's express train apron car pack for Era II (87352) was still on its way to dealers. The five apron cars consist of one 1st/2nd class (AB4ü), two 3rd class (C4ü), one 1st/2nd/3rd class (ABC4ü) and one red Mitropa apron dining car type WR4üe.

Archistories follows up:

Archistories has developed a modern maintenance hangar as the “Intermediate Service Facility” (art. no. 102211) on behalf of the US dealer Z.Scale.Hobo and is now delivering it. In Germany, the hardboard kit made of high-quality and solid-coloured sheets can be ordered from the manufacturer (<https://www.archistories-shop.com>) and its dealers.



The building proves that modern industrial hall architecture is simple, but does not have to be boring: The hall can be individually varied for two tracks. For example, the side extension can be arranged as desired or omitted altogether.

The main hall allows a variation for driving through or with a closed rear wall. The interior is also varied and worth seeing, as office fixtures and trusses determine the appearance. Skylights and fume cupboards shape the roof.

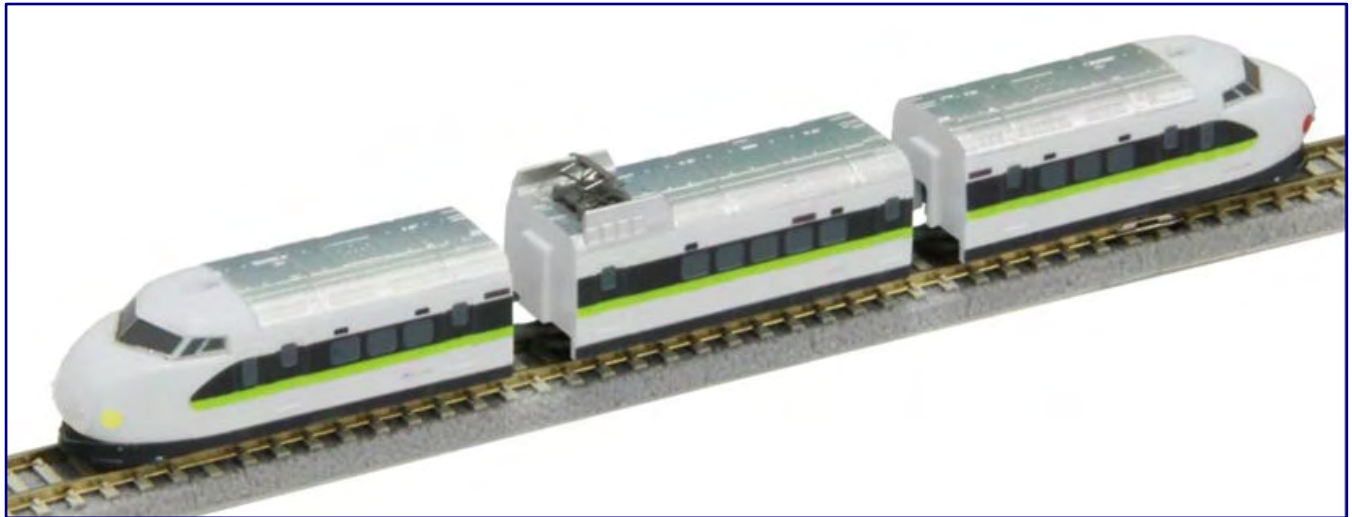
An application option for this kit is also the function as a warehouse in industrial areas, where it can appear, however, epoch IV. The basic colours light grey, sand colour, olive green, and light blue are available.



Modern maintenance hall in light grey (art. no. 102211; photo at the top) and a look inside the light blue version. Photos: Archistories

New Shorty at Rokuhan:

Noch, as European general importer of Rokuhan (<https://www.rokuhan.de>), presents the new Shorty based on the Shinkansen Series 0, the original unit from 1964. The three-piece unit is offered in the fresh green painted Shorty in the livery it later wore (item no. 7297647). The original livery has been on sale since September 2021.



Shinkansen der Serie 0 als dreiteiliger Z-Shorty in der Lackierung „Frisch-Grün“ (Noch-(Art.-Nr. 7297647). Foto: Rokuhan

From January onwards, the EF 65 1000 electric locomotive in JRF test livery (7297648) with the road number 1059 and in the renewed JRF colours (7297649) will also be available as locomotive 2127.

New from Micro-Trains:

In the Sweet Liquid series, it is now the turn of wagon number 9, a black McCahan Sugar Refining Co. tank car (item no. 530 00 580). An Amtrak passenger car set (994 01 270) comes in four pieces for delivery, including two baggage cars.

Furthermore, there are six 61-foot flat cars with fixed end walls in the designs of the TTX (527 00 083 / 084 as well as 527 00 191 / 192) and the Canadian Pacific (527 00 201 / 202).

Micro-Trains products are distributed by Case-Hobbies (<http://case-hobbies.de>), among others.

Smallest Christmas tree in the world on display again:

While the operators of Christmas markets everywhere are worried that another closure might come their way in the middle of the ongoing business, we don't need to worry about a special exhibit: The world's smallest Christmas tree will once again be on display in the shop window of Dortmund's Galerie Lamers, Kleppingstraße 8, from the 1st Sunday of Advent (28 November 2021) up to and including 30 December 2021.

At the edge of the Christmas market there is enough space and no crowds, which is why it can be viewed (and photographed) safely around the clock. Visitors are always out in the open, and, so, do not need to risk their health.

We still recommend putting on a mask, as it is compulsory in queues anyway. If several people arrive at the same time, no one needs to worry too much about minimum distances and is still protected in the best possible way.



The world's smallest Christmas tree is once again on display in a Dortmund shop window for everyone to see during the Advent season.

In this way, the gem that now holds the recognised world record for the 15th year can hopefully once again contribute to the small joys of a pre-Christmas season that unfortunately remains rich in deprivation and restraint.

Insider models 2022 presented:

Die Märklin used the Intermodellbau Dortmund to present its Insider Models for 2022. For Z gauge, the well-known and popular DB loner V 320 001 (item no. 88320) was chosen for the anniversary year. It had been built by Henschel on its own account in 1962 and was leased by the Bundesbahn for ten years after successful testing.

The completely new design rolls Insider Models 2022 presented the Era III in 1965 operating condition. About 105 mm long, the club members can look forward to a metal chassis with plastic superstructure, in which a contemporary drive works with a bell-shaped armature motor.

Both bogies are powered, we expect four of the six axles to be included here. The LED signal at the ends changes from warm white peak to red end signal depending on the direction of travel.

An imitation engine room with lighting has also been announced. We are curious to see if the driver's cab lighting, which is also planned, can be made so discreet that it corresponds to a dark room with switched-on instrument lighting in the prototype. Customers are also promised a high level of detailing, fine and elaborate painting and printing, attached handlebars and good tractive power due to the high dead weight.

While these new products will certainly please many customers, the annual wagon already included in the club price seems rather unimaginative: Here they are treated to a covered freight wagon (80332) with brakeman's cab of the later G-10 type in light grey with inscriptions of the former paper factory Salach.

The model is labelled according to epoch I and reproduces a wagon used by the K.W.St.E. This wagon already existed in an almost identical design: With a stamped and embossed metal sliding door instead of the now chosen closer to prototype version, it was already the museum car in 1996.

In the meantime, the 2021/22 catalogue has arrived in the shops, which is also part of the already paid services for the customer club members and was ordered via a voucher from the dealers.

International Model Railway Day:

Not easy to implement in Corona times is the International Model Railway Day on 2 December 2021, but as usual the activities are to be extended over a whole week as a campaign period.



Model Railroading Day

2. December

Contact sought with a busy Zettie:

Our editorial team is looking for contact with the model railway enthusiast who appears under the alias "Der Z-CRACK" and publishes film contributions., We would be happy to exchange ideas with this gentleman, recommended also by various readers of our magazine, and to report on his projects and conversions.

Therefore, we very much hope that he reads this appeal and gets in touch with editor-in-chief Holger Späing via the contact channels mentioned in the imprint or the e-mail address given in the name entries of our pages (under "Team").

If this works out or someone from the circle of readers can establish this contact, we say thank you very much!

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Trainini

German Magazine for Z Gauge



Winter in Schneeweiler

Short Autumn of Trade Fairs
Ratimo with a new Focus

Introduction

Dear Readers,

I think this is a very colourful edition with many likewise varied topics. With a lot of input from our many helpers, the result is something that brings together many aspects of our hobby.

There are novelties and technologies, about which we report for the first time. I think now of a small garage kit, which is discussed right in the very first article. Redutex foils are just as new to many model rail-roaders and have only just begun to conquer the Z-gauge market.

And in the news, as you are used to from us, you will again find a lot of interesting information, that is just as newsworthy. All this characterizes the current part of this edition. But we have also looked at what makes up and has made up our community in the fall of 2021.

After all, two exhibitions have taken place again, although with restrictions, while, for example, the Advent meeting in Zell an der Mosel, eagerly awaited by many, had to be cancelled in regard to our health. Similarly, other exhibitions around the Model Railroad Day had to be cancelled once again.

How the visitors and exhibitors in Friedrichshafen and Dortmund felt, therefore, kept us very busy. Our correspondents at both events kept watch and summarized their impressions.

The third focus of this edition is the upcoming Christmas season. Traditionally, it is the highlight of every model railroad year. Wishes come true, a layout provides a festive setting, while for others it is an opportunity to dream and escape from the hustle and bustle of everyday life.

Our translator Christoph Maier shows us how his first winter layout came into reality, and how it made even his less model train interested family go into rhapsodies. Günter Falkus has tried to combine his experience in model shipbuilding with his passion for model railroading and pay tribute to his favourite humourist at the same time. Here, too, you can be curious!

On behalf of all editors and translators, I would like to take this opportunity to wish you a merry and blessed Christmas! We are approaching the end of the year in strides. But before we enter the anniversary year of Z gauge, we still have one more article to write about our focus for the year.

Rainer Tielke lets us know how his Ratimo-Z brand is currently developing and changing enormously. We can be there first-hand when he does not only break new ground, but also continues to perfect his techniques.

Join me in looking forward to a worthy final to the outgoing year of 2021!

Sin-Z-erely,

Holger Späing



Holger Späing
Editor-in-chief

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Currently no items

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We would like to thank our guest authors Günter Falkus, Rainer Tielke and Heribert Tönnies for their contributions and Ralf Junius and Jörg Landau for photos.

Date of publication of the German language version of this issue: 23 December 2021

Cover photo:

Branch line in winter - material for romantic stories and exciting photos. Just how atmospheric a red hummer can look in the thick snow is what we are experiencing as it enters Schnee-weiler station.
Photo: Christoph Maier

American Garages from Archistories

Getting started made easy

It does not always have to be large building models that deserve a detailed article. Even small kits can give pleasure, provide experience and make for beautiful scenes on the layout. In this case, all that is given and we have also found a good start for all those who want to practice working with hard cardboard for the first time.

Archistories has produced a small kit exclusively for the US Ztrack Magazine, which its subscribers received as a free gift with the June edition. Contained in a small paper envelope, the "American Garage" matches the farmhouses from the 19th century.

For this reason, the small building was also produced in the same colours and can also be purchased separately in the colour of your choice. Together with the house kit, this allows for an even larger number of possible variations.



The American Garage from Archistories is also quite conceivable on German layouts. Here it can serve as a shelter for a friend of American life, or it can be designed in a Scandinavian style and converted into a garden shed.

The small kit can be ordered from Ztrack directly or from the 1zu220 store by German as well as non-EU customers. The small kit can be ordered directly from Ztrack, or by German, as well as non-EU customers,



The small kit is packaged in a paper envelope, which is also often used for CD or DVD inserts in magazines. Only a few tools and aids are required to construct a small garage.

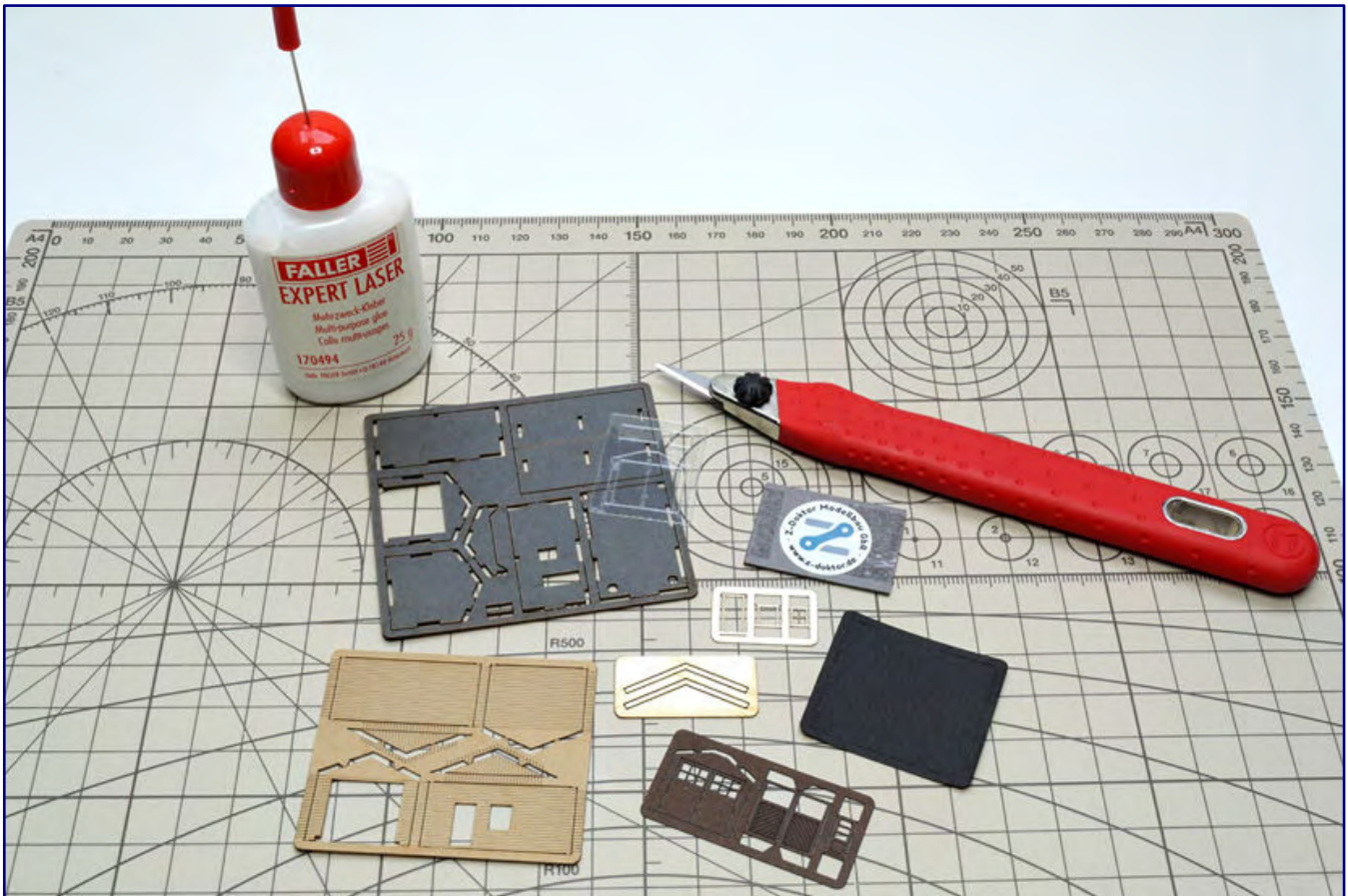
from the 1 zu220-Shop. Because of these options and in view of the excellent learning aspect of practicing the handling of hard cardboard, we decided to briefly present the easy assembly here.

The garage building, which is probably typical for many regions of the United States, was very familiar to us in its basic dimensions, design and also colour: Viessmann marketed a very similar, two-colour moulded model under its Kibri brand, which was also placed as an entry-level model.

Offered also in a Falun red variant with white applications (for Scandinavia) and with exchange parts for designing as a garden shed, it offered a considerable variety that made it credibly usable also in Germany. This also motivated us to write this article, because for seasoned model railroaders there are finally options for personal modifications.

After the paper envelope is unpacked, the project is ready to go: The manual gets by almost without words and works with easy-to-understand illustrations. Their scope also includes clearly assigned part numbers for all cardboard sheets. The only tools needed are a cutting mat (Tamiya), a sharp craft knife (Mozart) and tweezers. A small nail file and a toothpick can help additionally on a case-by-case basis.

A fine hair brush and suitable paints (Noch, Oesling Modellbau and Revell) are also required for a tool bracket. This is to be mounted on the outside, including garden tools, which was created using 3D printing and supplied by Z-Doktor Modellbau. To hold the workpiece securely, we additionally had recourse to a pair of clamping tweezers (Tamiya). The necessary adhesives are supplied by Bindulin, Faller and Uhu.



All sheets, including the pre-engraved window foils, are placed in front of us: the number of parts stays manageable and so does the time required.

And off we go

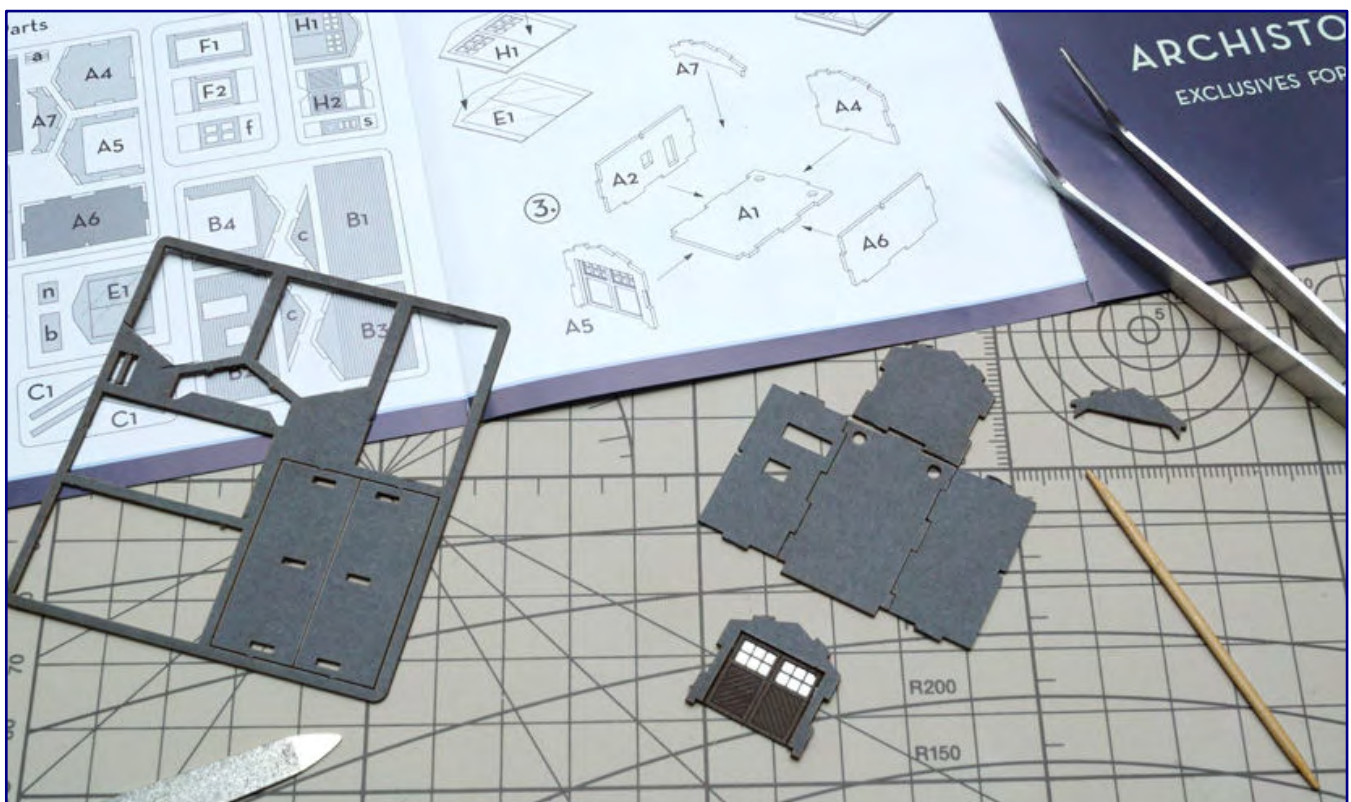
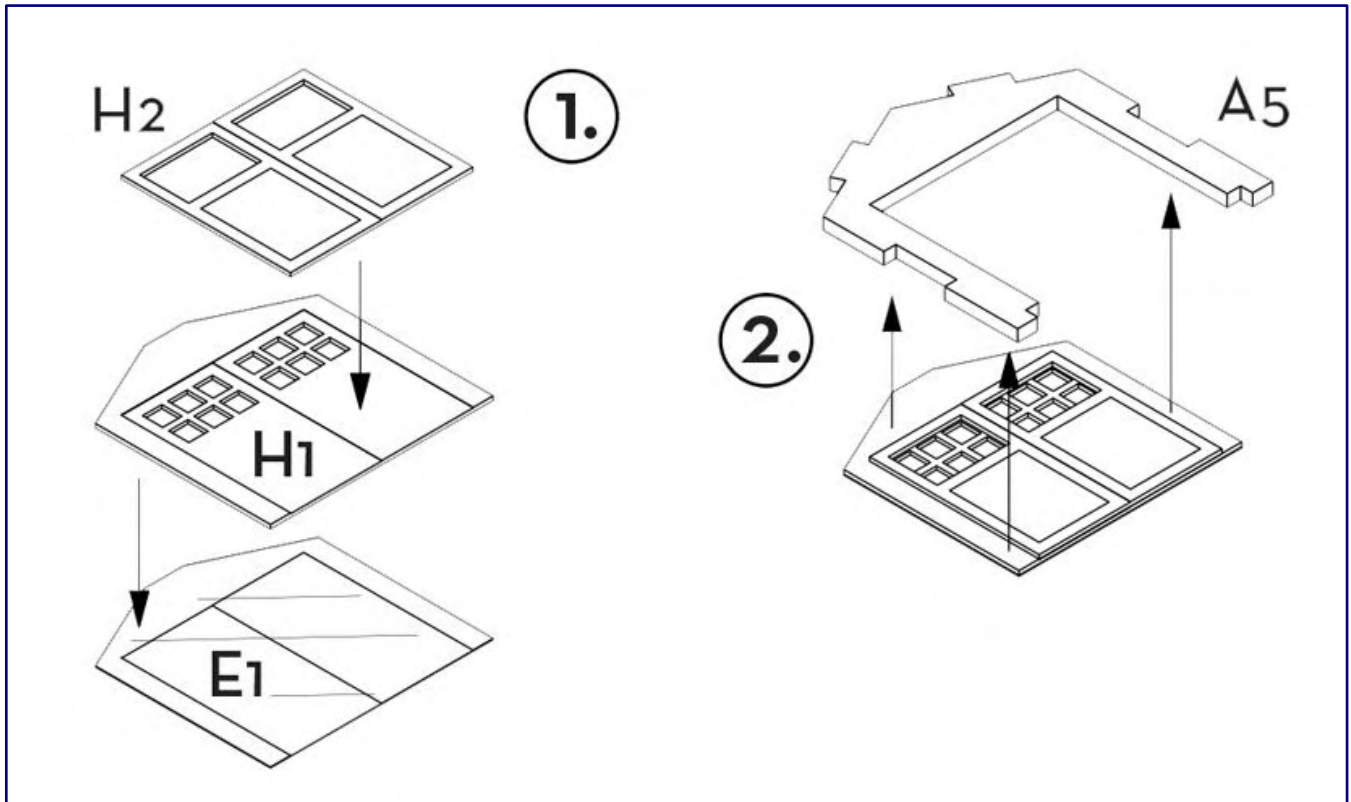
In a deviation from previous practices with Archistories kits, the start of construction here is not marked by the creation of the stable corpus from thick hard cardboard. Instead, we start with the construction of the double-door garage door from three parts, including a window foil as the bottom layer.

Once this has been completed, the element is placed in the hard cardboard frame, which is part of the already mentioned building core. It consists of the four outer walls, a base plate with openings for cable feeds and an additional transverse stiffener in the roof area.

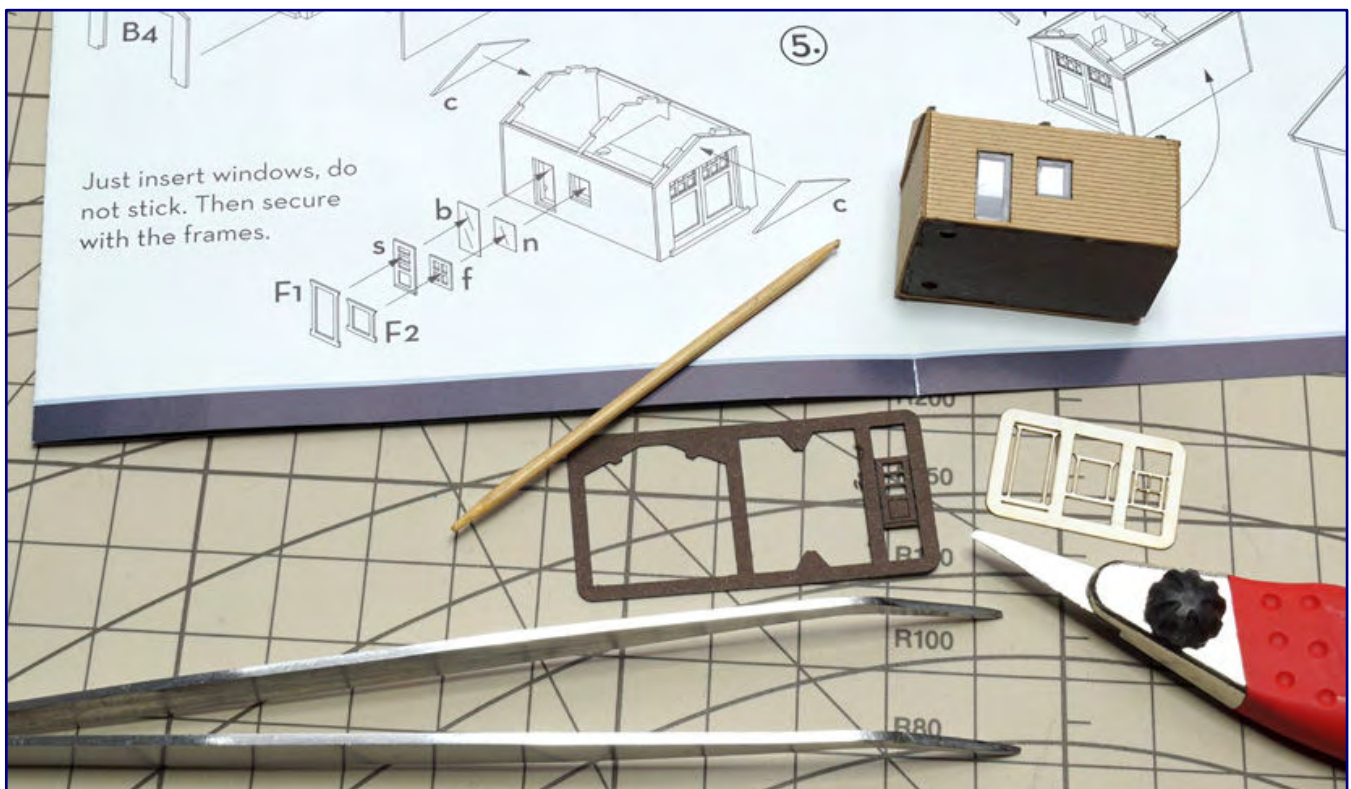
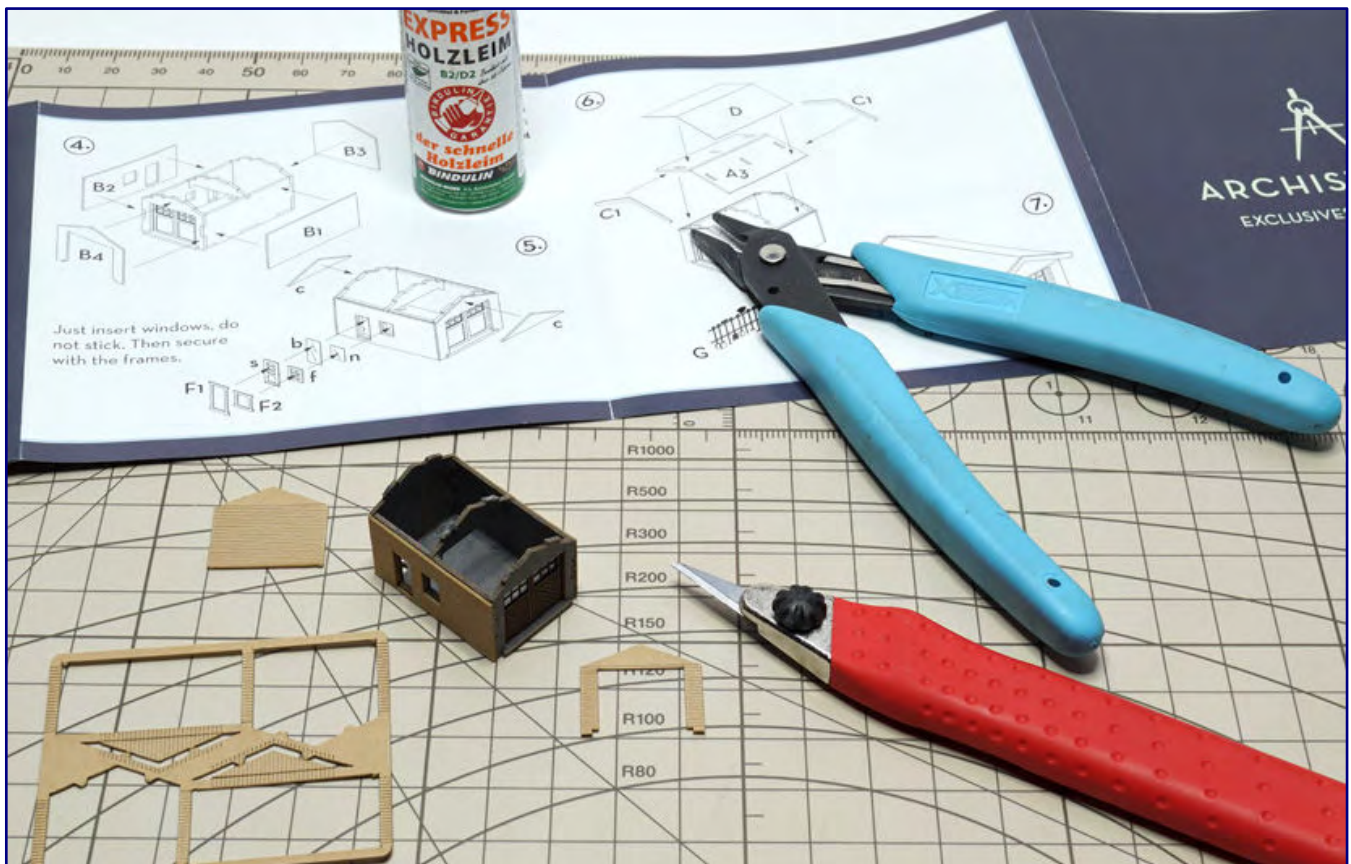
The following three steps now correspond again to what we already know from earlier kits: The core is followed by the outer walls, further applications and the roof. With a thick base and a thin layer of sandpaper to simulate a tar paper covering, there are no surprises here either.

When gluing the outer walls to the body, the sequence determines success or failure: Since the parts are glued butt to butt, it is important to note that the long sides are glued on first. Only after that, the two front sides follow.

continues on page 9



As with other cardboard kits, the first step is to create a stable body as a substructure. For the garage door (picture above), three parts, including the window foil, must first be joined together according to the instructions and inserted into the front body part. Then, all five parts plus the cross bracing are undergoing a dry fit test (photo below) and then glued together according to step 3 of the manual. Photo (above): Archistories



The assembly is progressing, the outer walls follow. Since they are to be glued butt to butt, it is important to process the two long sides first and only then the end walls (picture above). The insertion of the windows is special in this case, because no glue is to be used here (picture below), as also explained in step 5 of the instructions lying on the work surface. They are only fixed in place after the outer frames are glued on.

This remark is one of the few pieces of information that are provided as text in the manual – however, in view of the target market, only in English. Customers outside this linguistic area may refer to our article or use translation tools from the internet.

Step 5 of the manufacturer's document also contains a text note with a special feature unknown to us so far: The windows are now to be plugged in and not glued. The window foil and the cardboard window cross are therefore placed with the help of tweezers and carefully pressed in.

They are to be held in place solely by the outer frame, which is the third and last part to be glued onto the wooden facade structure. However, we don't quite trust this technique because there will be a gap between the two crucial parts.

That's why we use a toothpick to position at least tiny glue dots with the Lasercut-Expert (Faller) in each of the four corners after inserting the clear parts. The window cross invisibly carries this along when it is inserted and thus gets additional support without leaving any traces anywhere.



The work on the building is completed with the laying of the hard roof substructure, the gluing on of the roofing felt made of fine sandpaper and the fitting of the fillets on the two pediment walls.

The last step on the building, after putting on the roof and attaching its front panelling, is a small step at the side entrance. The smallest of all cardboard parts is to be attached with two tiny drops of Faller glue. The garage should be placed on a smooth and firm surface so that there is no gap between the step and the ground.

Thinking of the details

We have still left out one small but exquisite detail. It is however not negligible for the overall effect, which is why Archistories certainly did not want to omit it. It is about a tool bracket for garden tools, which is mounted on one long side of the garage.

We are also familiar with such organisational elements from Germany, where, however, they tend to be attached to a wall inside the building. A laser-cut cardboard part was ruled out here because the garden tools, including rakes and hedge trimmers, should stand out structurally from the holder. A separate mounting was deliberately avoided.

So, the choice fell on a 3D printed part from Z-Doktor Modellbau that is included in the kit – a cooperation that has already proven itself many times before. This part comes from the factory in a dark grey colour.

If you don't want to paint it by hand, you could also apply it unpainted, but this would probably not be very attractive. We have therefore chosen the following procedure: First, the printed part is fixed in the clamp tweezers so that the largest part remains accessible for paint.



The 3D printed part of the tool bracket holding garden tools still needs a triple colour treatment before it can be attached to the side wall of the garage with Uhu power glue.

We then protected the wooden parts on the handle of the Tamiya clamp tweezers from paint mists with masking tape. Then we carefully apply a base coat of Noch acrylic spray in the colour ochre (art. no. 61172). After the paint has dried, reposition the tweezers and repeat the process so that the part is evenly and completely primed.

Then use the brush. All metal parts of the tools get a coat of paint with Revell enamel paint "iron-coloured metallic" (91), handles are painted with acrylic paint from Oesling-Modellbau gentian blue (RAL 5010). In doing so, we basically follow the suggestions shown in the photos on the back of the manual.

After the bracket has dried completely and been glued on with a few drops of Uhu power glue, carefully placed with the tip of a toothpick, the building is ready. Before using the glue, however, we recommend a dry test against a miniature figure, because the mounting height should certainly be suitable for the staff!



This is the effect of the side wall with appliance holder and adjacent end wall with the double-door garage door, which is no longer common in Germany.

So, now it's time for a short review and a conclusion: If we disregard the necessary drying times, the small kit is a perfect way to relax in the evening. Depending on ability, leisure and working speed, 30 to 60 minutes of building time should be estimated.

The number of parts and steps remains more than manageable. And even in view of quite delicate and small parts that are among them, we come to the conclusion that here is a perfect entry into the world of hardboard kits.

The financial expense remains manageable, which is why not even a complete failure will make a big hole in your finances. At the same time, we see no reason to seriously expect such a failure. But it is precisely such worries that often prevent model railway fans from trying out something completely new.



And this is how the opposite side looks, which seems very familiar to us in a function as a garden shed. It could easily be built from two identical kits, otherwise creativity and handicraft skills are required.

There is probably only one kit that could hopefully be priced even lower and would be even easier to build to gain initial experience: That would be one of the typical bus shelters that we have seen in rural areas for decades, made of both concrete and wood. But those who still don't dare to do so will have to wait for this a little longer.

- **Manufacturer of the hard cardboard kit:**
• <https://www.archistories.com>
- **Client and source of supply:**
• <https://ztrackcenter.com>
- **Other providers mentioned in the text:**
• <https://www.bindulin.de>
• <https://www.faller.de>
• <https://www.noch.de>
- <http://www.oesling-modellbau.com>
• <http://www.peter-post-werkzeuge.de>
• <https://www.revell.de>
- <https://www.tamiya.de>
• <https://www.uhu.de>
• <https://www.1zu220-shop.de>

A magic built on cardboard Winter in Schneeweiler

Just in time for this year's Christmas season our reader and translator Christoph Maier presents his small Winter layout "Schneeweiler" (Snowville). In addition to its harmonious design and loving details, the layout surprises with a special feature: Instead of the usual wooden or hard foam base board, its substructure is made entirely of cardboard. The snowy landscape was also designed with the help of unconventional materials.

By Christoph Maier. As a great fan of micro layouts, I had been toying for quite some time with the idea of building a small nostalgic winter layout which could be placed in the living room during Christmas season and entertain the family with one or two trains running in circles.

The project finally got off to a start when reading the online reader forum of the French model railway magazine "Loco-Revue," and coming across building reports about the magazine's "Train'in Box" model railway modelling beginners set, which was on the market a few years ago.

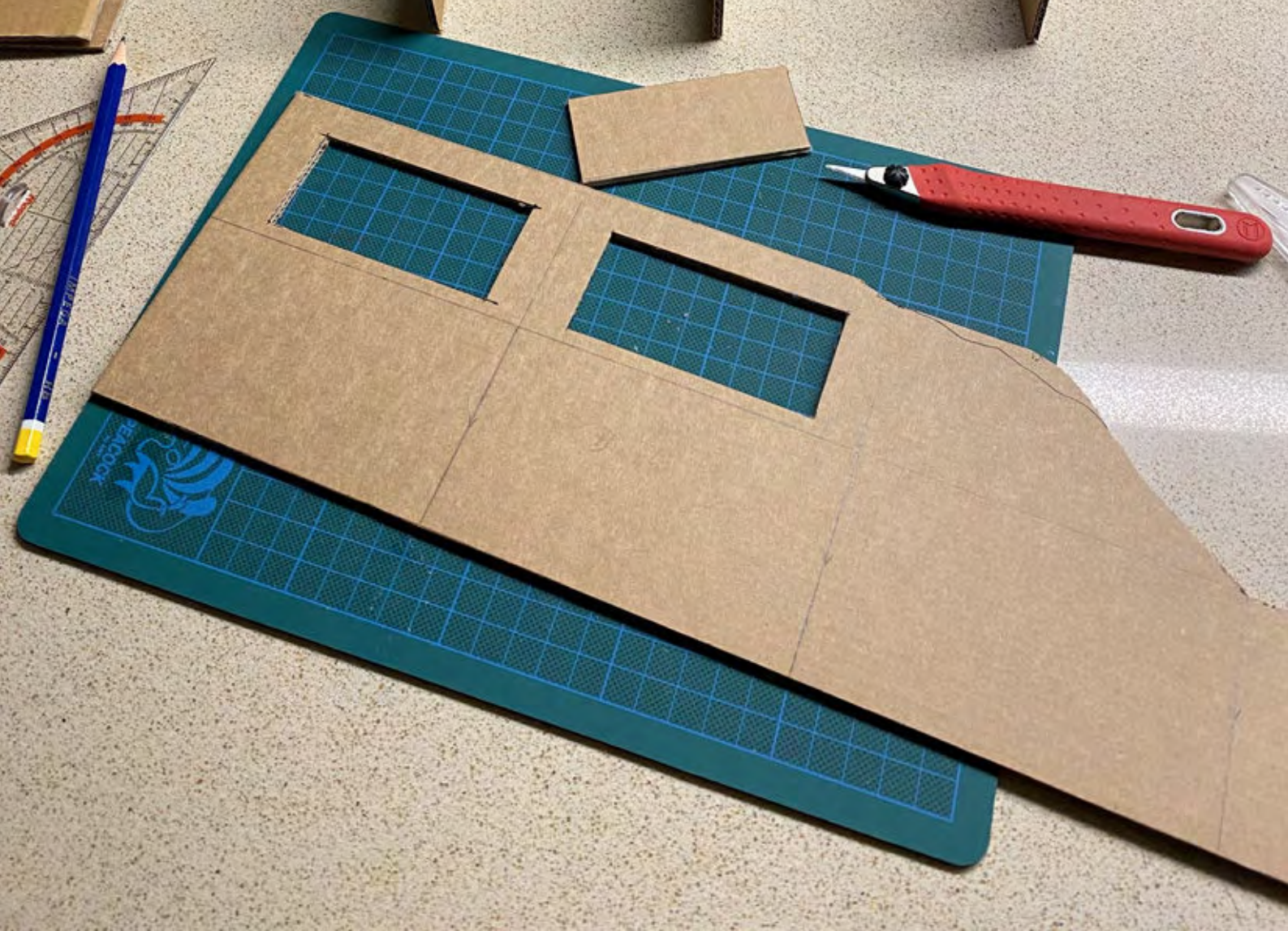


Small layout, big effect: "Schneeweiler" is our translator's first layout build with wintery motifs.

What fascinated me about the "Train'in Box" layout was its simple, yet attractive concept: an oval with some basic operational possibilities, train station, little village, road, mountain and tunnel – all key elements for an archetypal model railway layout concentrated in one little space!

From a practical point of view, I also liked the idea of building the baseboard from cardboard. As somebody who does all his modelling work at the kitchen table, I always try to avoid as much as possible any dusty and complex woodworking operations.

continues on page 16



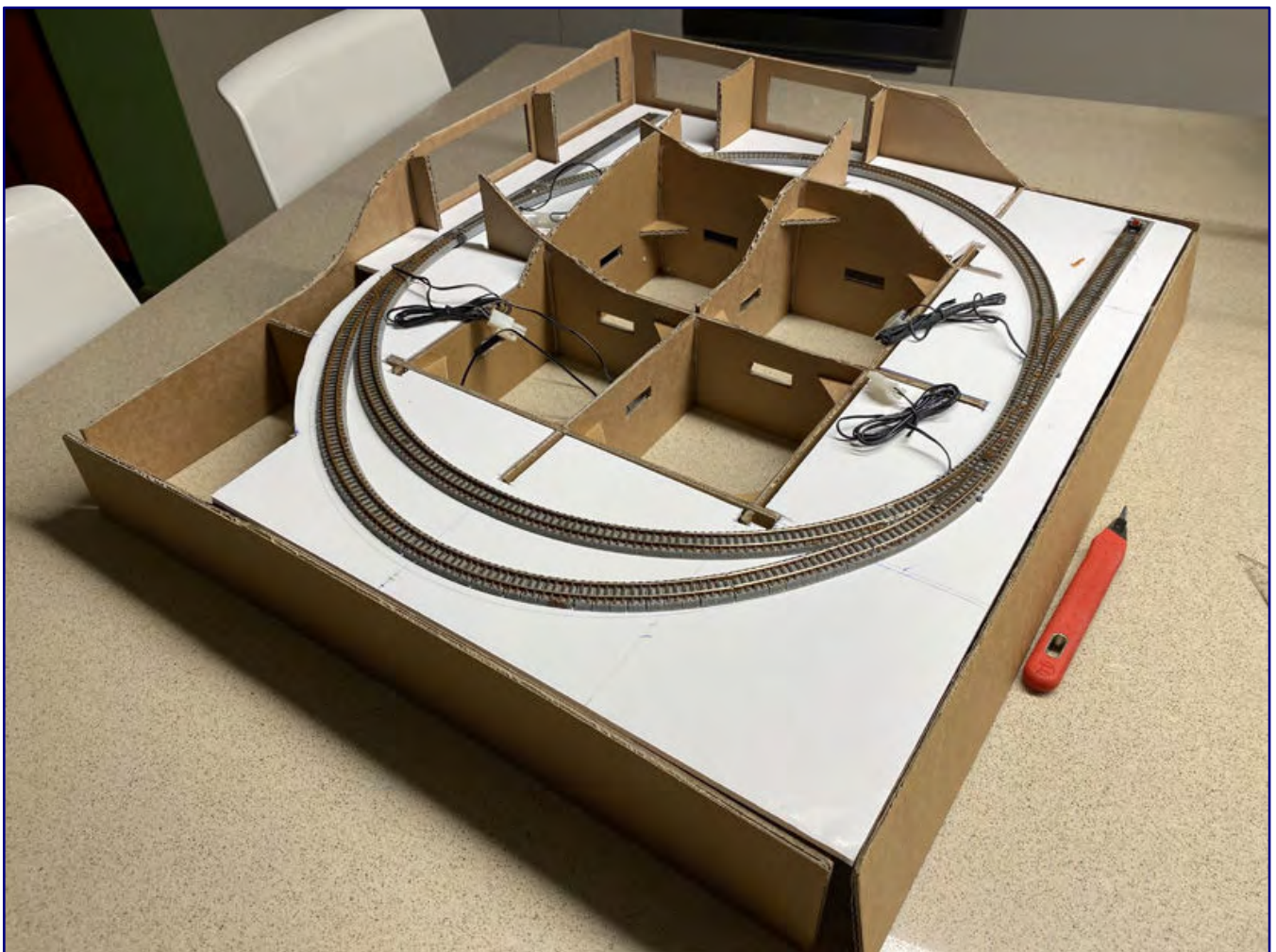


Cardboard and track works

The outer dimensions of the layout – 48 x 56 cm – were determined by the translation of the Train'in Box track plan into scale 1:220: curved track with a 195 mm radius plus two turnouts on each side and a bit of extra space for landscape.

The baseboard is made of 3 mm double wall corrugated cardboard. I used Noch's "Panzerkarton" of which I had a pack laying around, but any double wall cardboard used for packaging should work as well as long as it is strong enough.

The mountain with the tunnel was to be placed in the left back quarter of the layout, and was therefore taken into account when cutting to size the cardboard panels. I also cut holes for the wiring of the layout.



Top and pages 14/15:

Piece by piece the baseboard is assembled from panels made of double-sided corrugated cardboard. The horizontal base plate made from 5mm foam board will support the track and a small village.

After testing their correct fit and some small adjustments, the panels were glued together piece by piece with a hot glue gun and reinforced with small cardboard triangles. Visible cutting edges were smoothed out with filler and sanded. All four sides were later painted with grey acrylic paint.

The horizontal base for the track was cut from a single sheet of 5 mm foam board and glued onto the cardboard structure.

I must admit that I was initially sceptical to build the baseboard from cardboard instead of plywood or hard foam, but the finished layout is surprisingly strong and torsion-resistant.

This construction principle is perhaps not best suited for exhibition layouts, but for home use, and, in particular, for such a small layout seems to be a good alternative which can be implemented easily, quickly and with the most basic tools.

The next step was to install the track: I opted for Z-scale track from the Japanese manufacturer Rokuhan. After feeding the cables for the electrical turnouts and power through the baseboard, several test drives and some adjustments, the track was super-glued to the base and weathered with rust-coloured acrylic paint. This is a bit of a tedious process that requires extreme care, especially with the turnouts, in order to avoid contact problems.

Scenery works

The rough shapes of the landscape were built with landscape modelling foil from Noch (although sheets of packaging paper would have done the same job, especially on such a small surface) and some pre-cast plaster rock faces.



Before the landscape is designed, paint is applied in some places and the mountain road is cut to size from cardboard.

The small frozen village pond was created by painting a piece of foam board blue-grey and covering it with a transparent plastic sheet that had been sandpapered in order to render it opaque.



Landscape modelling foil from Noch and some plaster rocks (top) are used to create main features of the landscape. This is also the moment to test the future position of the buildings. The final shape of the landscape is modelled with Sculptamold (bottom).

The tunnel portals are from Faller and were slightly weathered with weathering powder, before being installed. Several test drives were necessary to determine the exact position of the portal that was placed in the curve in order to ensure a free passage of the trains.

The mountain road was built from two layers of thin cardboard from a cereal box and glued into place before being painted with dark grey acrylic paint. The station road, village square, level crossing, and platforms were built from 1.5 mm and 3 mm foam board and 1 mm polystyrene sheets.

The fine contours of the landscape were modelled with sculptamold, a dry modelling compound which is mixed with water to the desired consistency.

It is easily applied with a spatula or just with ones fingers, dries with hardly any loss of volume, and makes for a strong, but lightweight surface. One additional advantage for this project was its white colour which made an additional coat of white paint unnecessary before applying snow.

Let it snow...

The biggest challenge with this project was to model a convincing looking snowy landscape. This is my first winter layout, and I therefore started with using commercially available snow paste and snow flakes. I quickly realised, however, that this did not produce the natural look I was after.





A detailed scene of the small layout should illustrate how successful the design of snow blankets, piles and small drifts looks.

Photo on page 19:
Snow always falls from above – also in the model. That is why the view from above is worthwhile.

The few deciduous trees around the pond and next to the village were made from sea foam which is available from Noch and other manufacturers.

The fine details

The train station, main buildings, chapel and mountain barn are from Noch's new range of Z-scale laser-cut kits. The other buildings are from Modelbahnmanufaktur Beckert (transformer building) and Modellbau Laffont.

All buildings were slightly weathered with weathering powder and equipped with interior lighting. Together with the street lights they are hooked up to a 9 V battery, with on/off switches hidden in the opening to the tunnel area at the back of the layout.

Firstly, the consistency of these materials is almost too coarse for 1:220 scale. Secondly, I did not like the glitter effect of the material.

An additional problem with the snowflakes is that they do not adhere well to even a generous coat of glue. Just one blow of air, and most of the snow will be gone.

After some experiments with other materials, I finally used a white and very light synthetic filler material from the DIY store (e.g., from Polyfilla).

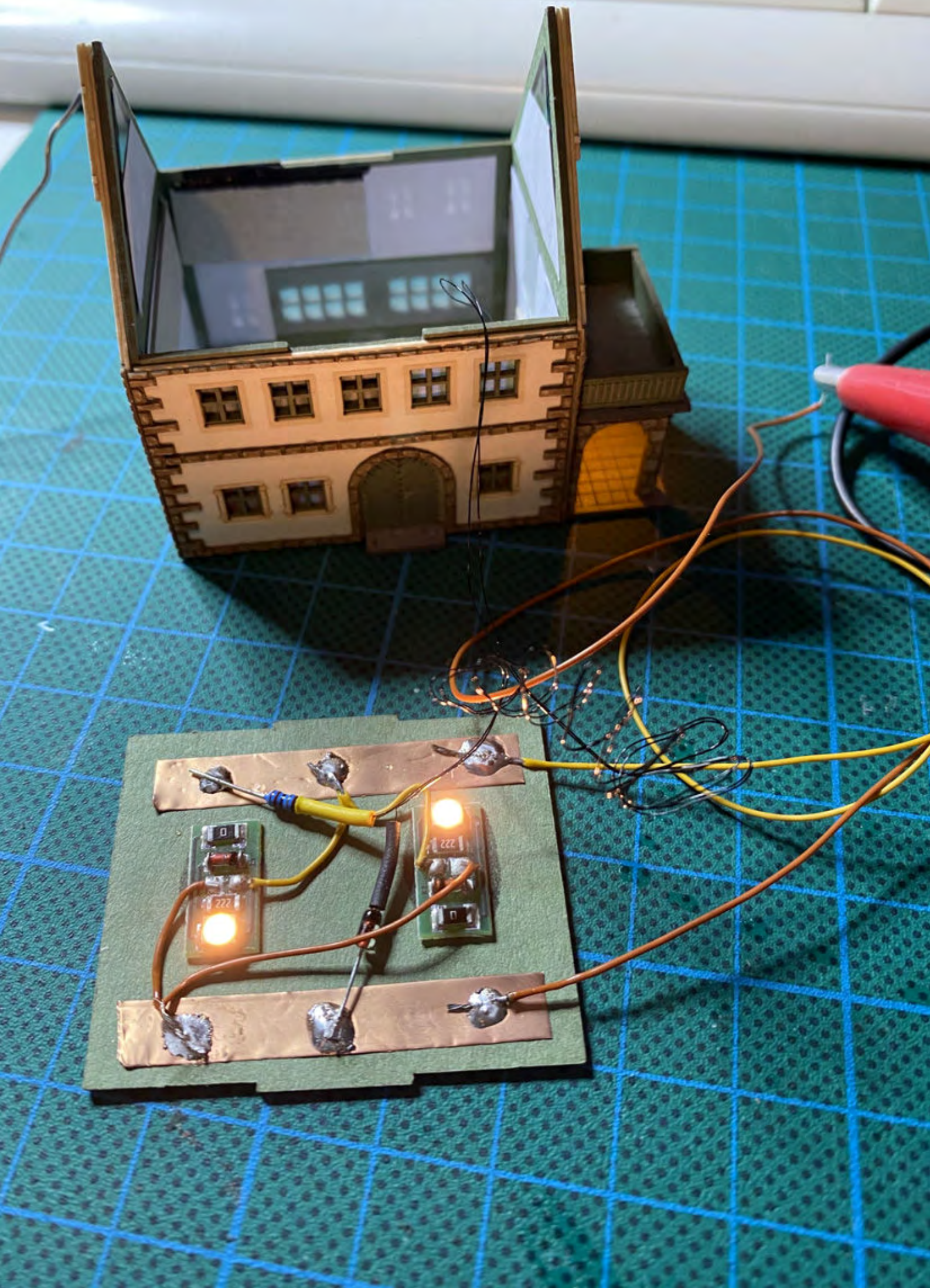
It can be easily mixed with water to any desired consistency and applied to the landscape with a spatula or brush, for small patches of snow also with a toothpick.

Once applied and with a bit of extra water added with the help of a soft brush, the material spreads out naturally and dries quickly.

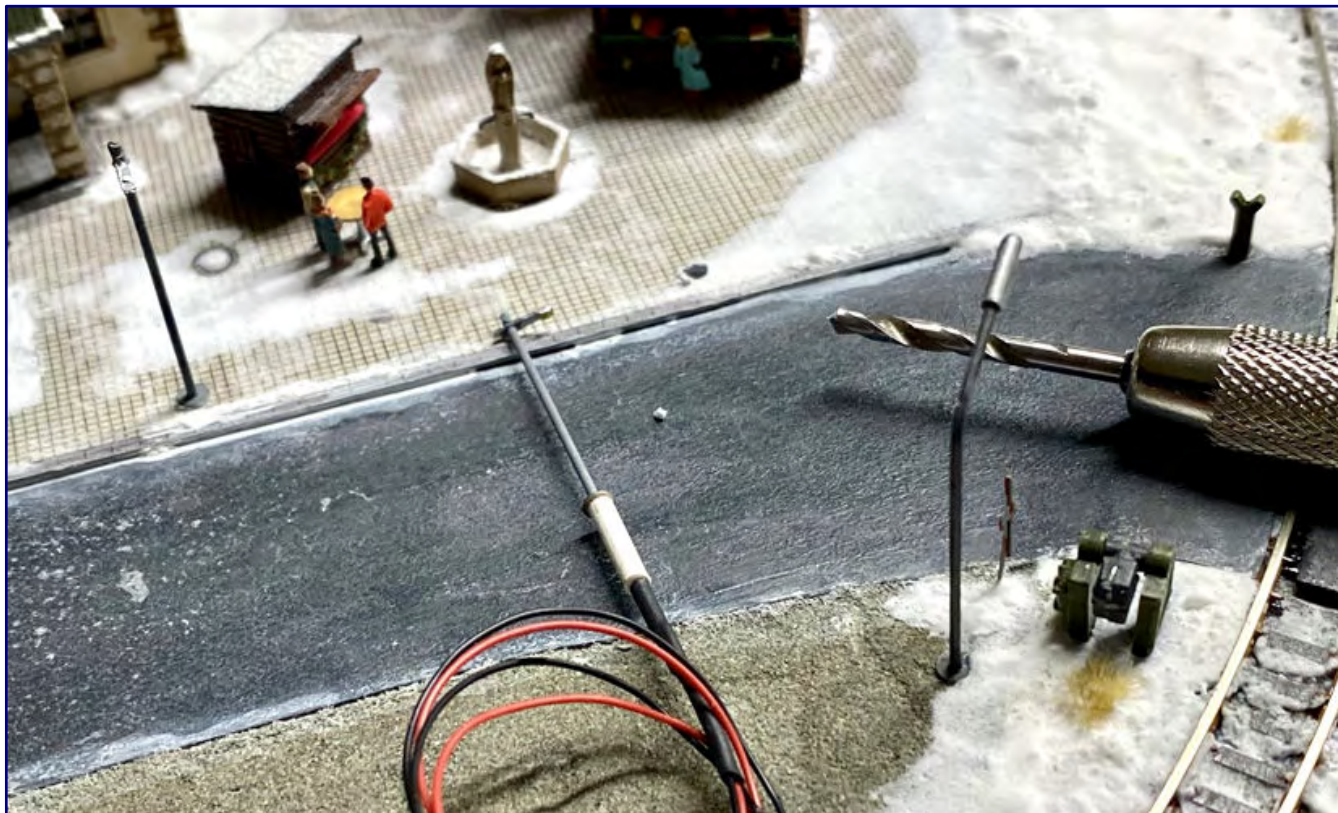
With some patience, this technique produces very natural and realistic looking snowy surfaces.

The pine trees, over 60 of them, are all from Busch. Although they are available as ready-made winter versions, I found that taking the summer version and applying matt titan white acrylic paint makes for a more natural winter look.

continues on page 22



The figures as well as the two Christmas market booths are from Trafofuchs, a small German manufacturer that produces a wide variety of Z and N scale figures. Most of the other accessories such as street lights, traffic signs, guard rails, and the diesel fuelling station are from HOS-Modellbahntechnik.



Part of the detailing is the lighting of buildings (see page 23) and the installation of street lights (top). All this contributes to an atmospheric ambience during night operations (bottom).

In this context I really would like to propose a toast to the small series manufacturers without whom the Z scale hobby would be only half the fun due to the limited choice of Z scale products from large scale producers.

I started working on the Schneeweiler layout in early autumn last year and the original intention was to have it finished by Christmas 2020. As so often with this hobby, there were unplanned interruptions and it finally took around a year to get it all done.



The work is finished, even if it is late compared to the original schedule, and knows how to convince. Its creator truly deserves an after-work beer for this!

But now the layout is ready for this year's Christmas season, adorns the living room and waits to be played with until it will be put into hibernation after Three Kings, to be awoken again next winter.

All photos: Christoph Maier

Manufacturer websites:

<https://www.busch-model.info>
<http://www.hos-modellbahntechnik.de>
<https://www.modellbau-laffont.de>

<http://www.modellmanufaktur-beckert.de>
<https://www.noch.de>
<http://www.trafofuchs.de>

Layout videos:

<https://youtu.be/c-rKMwcu7bk>

Lots of Humour for Christmas Every Year, again?

After 50 years of ship modelling, Günter Falkus has discovered his passion for our Z gauge. He describes himself as a border crosser in the Z-gauge world, because he wants to integrate new impulses from other branches and technical areas. This is also the case with his model-making gem for Father Christmas, which was finished in time for our Christmas edition.

By Günter Falkus. Today, I would like to tell a story that fits in with the upcoming Christmas. It happened at a time when a virus called Corona ruled the world, and I had a lot of time to think.

Memories of a carefree post-war childhood ran through my mind. I was very proud of my first railway back in 1954: steam locomotive class 44 from Märklin H0, a once sinfully expensive model. With it my time as a model railway enthusiast began.



Humorous celebration: This year's Christmas article revolves around this little exhibition layout, which was created during the sorrowful Corona period.

Back then, Christmas meant a fir tree, a circle of rails on the floor, two wagons, a locomotive and a transformer – and my father playing with them. So, in view of the memories of my youth, I had the idea of rebuilding all of this in Z-gauge. I wanted to do it in as original and original a setting as possible. The humourist Lorient, whom I greatly admire, set a standard here with his Christmas at Hoppenstedt's.

I was taken with his film and chose it as the idea for my small building project. The vision quickly became a plan. The track was to run on a scale of 1:4000. I had them as laser-cut plates on 4.4 mm flight plywood, the technology should not be a problem.

I had a slow-running gear motor and my friend – the technical wizard Hermann Kammler. He'll fix it! But then Corona came along, and Hermann was 250 km away – how was that supposed to work?

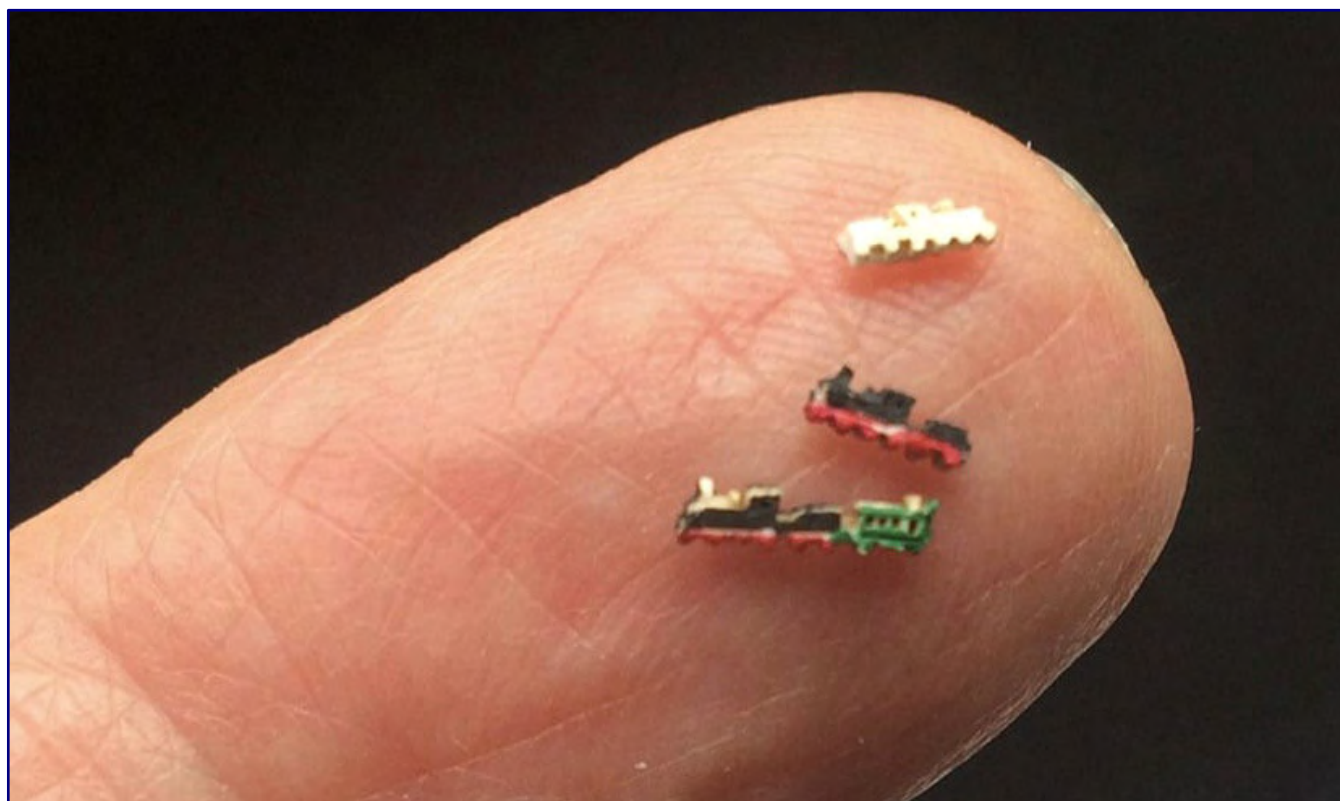
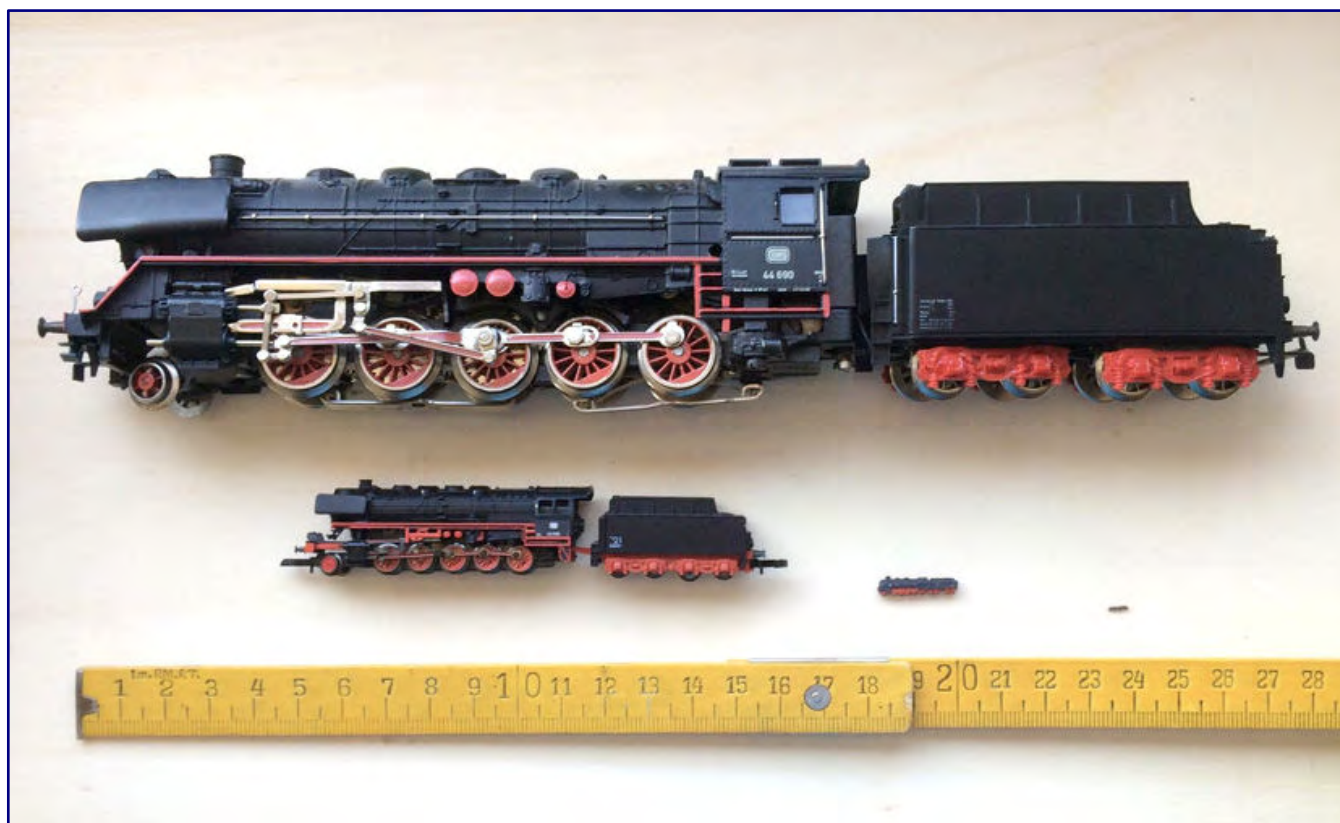


Model making under Corona conditions: Hermann Kammler ("Bagger-Hermann.") in the car park of the rest area testing electrical functions.

Well, we talked on the phone and exchanged sketches as well as photos, many virtual meetings brought the desired structure. The final meeting took place in the summer at the Autorasthof Gießen with the restaurant closed in the car park. There we tested the function and finished the shell.

Now Birgit Foken-Brock from Trafofuchs was faced with the task of creating the right figures. I presented my concept to her and her spontaneous reaction was: "How crazy do you have to be for something like that!"

continues on page 28



Märklin's class 44 in H0 scale (photo above) started a passion. This can be seen in comparison to the Z-gauge model and the own replica in the appropriate scale of 1:4000, which would be required for the exhibition layout portrayed here. The model used in the Hoppenstedt house, however, has a much smaller prototype (photo below).



The integration of the two chess players (photo above) into the building and the particularly humorous "self-realisation" of the two friendly model builders as psychiatric patients (photo below) can be traced back to remarks made by Birgit Foken-Brock of Trafofuchs.



All three scenes presented were integrated into “Hoppenstedt House”, which is now a Christmas diorama.

This has led me to another extension: Two more chess players have taken up residence in the Hoppenstedt house. And the two “crazy builders” have since moved into the psychiatric emergency room on the ground floor. Hermann was immediately present with the house from his 3D printer. A sound card then moved in there, as well.



And, so that the model railway in the model is also visible to everyone, there is an (apparent) public broadcast for Santa's on a large screen next to the building.

When everything was ready, I subjected my little Christmas diorama to a field test with my granddaughters – with an unfortunately devastating result: “The model railway is so small, no one can see it properly.”

Then, after a sleepless night, the idea for publicity was born: The Christmas railway will be broadcast on a large screen so that everyone can see the show. Of course, Birgit Foken-Brock had to work again to find the right audience.

And here is the result: Hoppenstedt on a scale of 1:220, loosely based on the famous humorist Loriot. The tracks have an unbelievably small gauge of 0.4 mm in a track radius of just 4 mm.

Model-appropriate speed, the train made by light-cutting technique, and Grandpa Hoppenstedt enthroned in the background, where the two wall lamps shine in a warm Christmas glow. In addition, there is the fir tree full of candles (simulated by LED), the gift box and the well-known Helen's March on a modern mobile phone.

Now I am very happy with the project, it opens borders in model making, and that is exactly what I want to contribute as a late called Zettie. The video can be seen on my web pages, the address of which you will find in the info box.

All photos: Günter Falkus

The Builder's Webpages:
<http://www.kleinergehtsnicht.de>

Christmas greetings



Dear Readers,

The entire team of our magazine wishes you happy holidays! In these turbulent times full of restrictions, our special wishes go out to a hopefully better year 2022, characterized by contentment, happiness, and above all health.

We would like to thank you for your many suggestions, ideas and your loyalty to our magazine.

A reader's experience report

Working with Redutex Foils

Our reader and book author Heribert Tönnies has studied the Redutex foils, which are also widely used in Z gauge, as an alternative for many other techniques. Today, he reports in particular detail on his experiences, which should also be of benefit to newcomers and those returning to the railways.

By Heribert Tönnies. Various, mostly surface-textured products are available for replicating walls, roof surfaces or even road surfaces. These also include embossed, sprayed plastic sheets in various thicknesses, printed cardboard with lasered or embossed structure, printed special crepe paper as well as types of foils.



The sample fan shows the variety of Redutex foils that we would like to introduce to our readers today as an alternative building material.

Many of them also have a self-adhesive coating. Especially for road surfaces, for the sake of completeness, the so-called stamp/carrier rollers should also be mentioned, such as those offered by Modellbahn-Union or Microrama.

In addition to a different appearance, the individual materials have advantages and disadvantages, both in terms of processing and durability. Later changes cannot be ruled out, especially if the layout is used for a long time.



Design proposal 1: Level crossing with cobblestone road, created by means of a Redutex road foil (Art.-Nr. 160BS124).

In my experience, over the years there has been considerable loss of quality, such as colour changes due to a lack of UV resistance. Incipient chemical processes caused foamed road films to become soft and even sticky-paste-like, and, thus, completely unusable.

Surfaces crumbled or the adhesive, which was partially applied to the back of the material and initially adhered well, changed over the years and led to detachment from the substrate, unless additional special bonding was carried out.

In contrast to the relatively easy processing of cardboard by simply cutting with scissors or knives, the thicker plastic sheets are already more demanding for the beginner without elaborate technical tools or pronounced manual skills, especially when the smallest components have to be worked out, or window openings have to be cut out.

Cardboard, for example, makes special demands on the formation of building corners due to the often-greater thickness of the material, because the thickness of the material, if mitre cuts cannot be used due to the material, remains visible at the edge of the building without later lamination (filling or colour work), which can lead to unwanted effects, especially in a small scale.

Street foils can often not be homogeneously adapted to the course of curves, i.e., pulled around, because a high expansion stress can occur at the outer edge of the curve, depending on the material.

It either impairs the shape of the road or prevents it from sticking to the substrate, while wrinkles form at the inner edge due to the compression. Circular segments must therefore be cut, the butt joints of which remain visible in some road construction materials without the need for time-consuming reworking.



Design proposal 2: Factory relief façade for the plant background using Redutex masonry film for the infill panels. (OFNM064).

These few examples show that special attention should be paid to the choice of suitable materials before construction begins.

The solution is called Redutex

For the self-designed and built objects on my presentation layout “Groß-Bw Herilingen” on a scale of 1:220, I was looking for a material for masonry, roofing and roads that was easy to work with even for beginners, but had an authentic visual effect. After intensive research, I came across so-called 3D foils from the Spanish manufacturer Redutex.

The enormous number of structures offered in the common scales surprised me. Many of these foils are also suitable for Z-gauge, despite the official specification for N gauge. An overview of the 3D foils offered can be found on the company pages, to which a link is included in the info box at the end of this article.

At the time I started building, there were only very few structures available in Germany, so I had them sent directly from Spain. In the meantime, however, the foils are also available in Germany in a large selection, for example, in the 1zu220-Shop.

Because the Redutex 3D foils convinced me in my previous projects in 1:220, I report about my personal experience. I was able to get a comprehensive photo of these structural films on the basis of the objects I have modelled, so far.



The films are approx. 302 to 305 mm long and approx. 123 mm wide. Due to edges that are more or less wide for production reasons, the usable structural area essentially corresponds to the packaging specification of 300 x 120 mm.

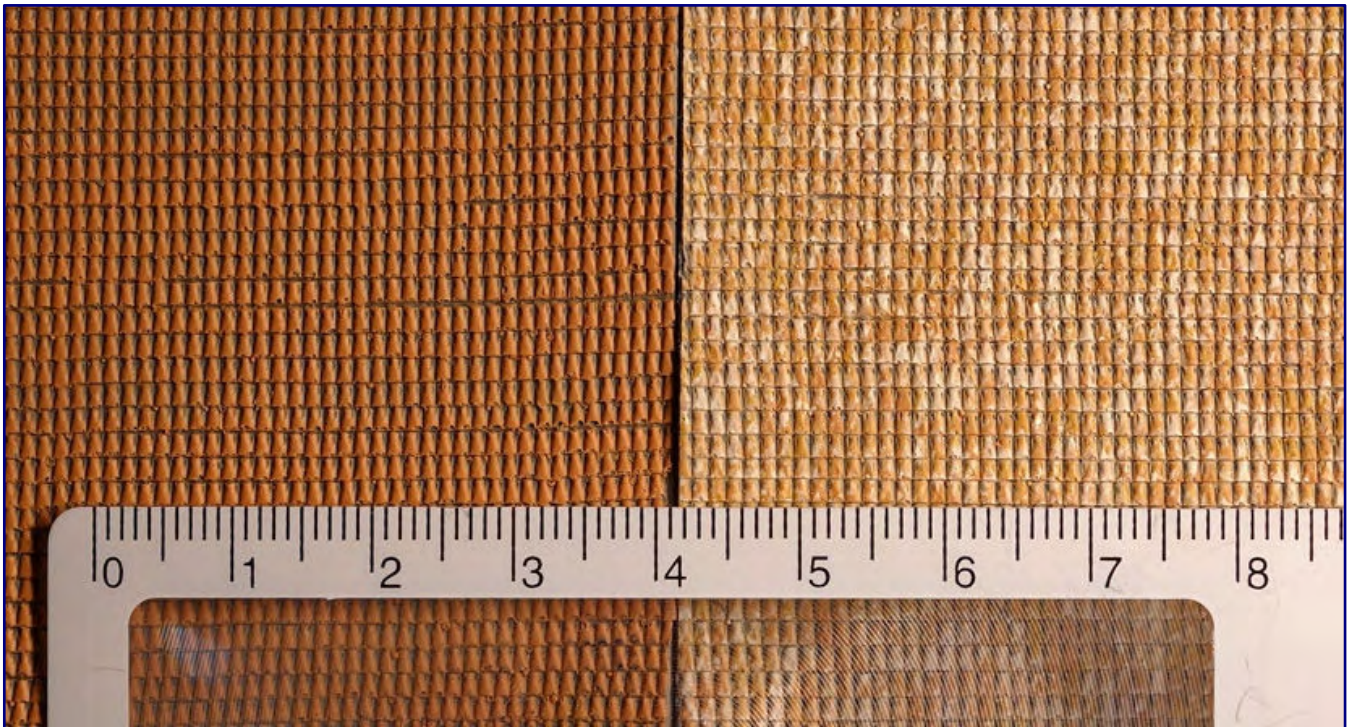
The foils I used for the nominal size Z had a thickness of approx. 0.5 - 0.7 mm, depending on the structure used including the adhesive layer and protective foil (approx. 0.05 mm) of the adhesive lamination. The difference in thickness results from the embossed structure; the finest brickwork is thinner, roof tile structures are regularly thicker. In the H0 scale, the foils are also up to 0.9 mm thick due to the higher embossing depth.

The foils are delivered in a folding bag made of thin cardboard with information about the foil type and scale. The lower and upper pockets created by the folding are ideal for storing leftover pieces in an orderly manner for later use.

In the programme there are muntin windows made of the same material, which already have a transparent foil on the back as an imitation of glass. In terms of scale, these are also suitable for some construction projects in Z scale; I have used them on the signal box.

However, the bars are relatively wide compared to windows made of lasered cardboard. Because the window foils are very soft, they may be more suitable for ambitious modellers.

Two main groups are available, "Standard" (mostly two-coloured; the raised structure in one colour and the joints in another colour) and "Polychrome" (multi-coloured). I have mainly chosen the polychrome version, because the effect is more vivid, the changing colouring looks more authentic and, especially in the scale 1:220, opens up the possibility of concealing any minor inaccuracies that may arise during construction by simple colour work.



A roofing film by Redutex in "Monk and Nun". design on the left in the monochrome (designated as standard), on the right in the polychrome version.

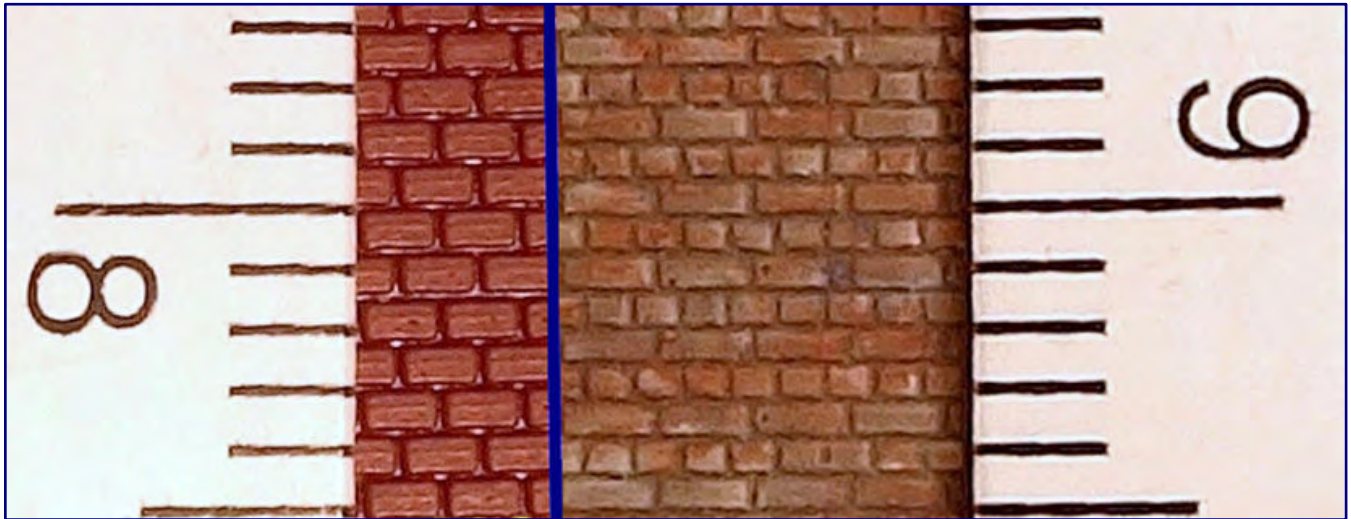
By the way, the foils can be treated very well, e.g., with acrylic paints, sea moss, and water run-off traces on the roof of my signal box or the arcade retaining wall. The surface structure is predominantly clean and distinct, as is the colouring. The appearance of the self-adhesive films is very realistic.

According to the manufacturer, the films are made of an acrylic resin according to company specifications, are EU-certified and, in accordance with the regulations of the European Community, do not contain any components that are harmful to health, and are manufactured in an environmentally friendly way.

One particular point seems to me to be especially important for exhibition installations: According to the manufacturer, the foils have a high fire resistance and can be regarded as "fire-retardant." I cannot verify this information, so I am only passing it on for information purposes.

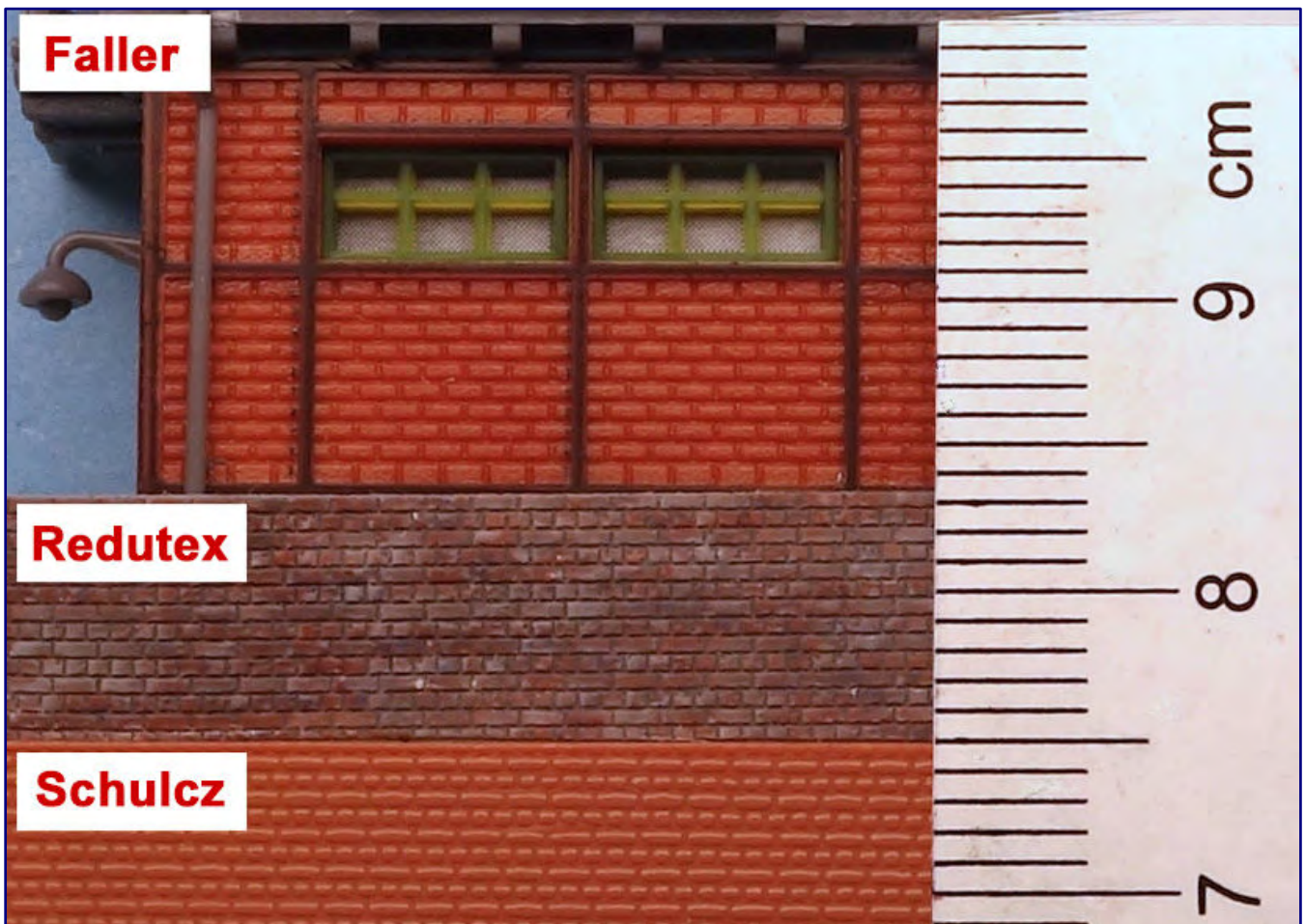
True to scale and exemplary

Although many foils are officially declared for N scale, they are, nevertheless, also ideally suited for Z gauge. One such example is the foil type OFNM079. Approx. 16.5 stone layers are shown here at a height of 10 mm, which corresponds to a layer height of approx. 13.3 cm.



The brickwork of the old Märklin locomotive sheds made of polystyrene (left) is compared with a Redutex foil type (OFNM079; right).

This is higher than the usual classic masonry bricks in the original with a height of approx. 7 cm plus approx. 1.0 cm joint, but still looks much more authentic than many polystyrene sprayings of earlier years. On the locomotive shed from the earlier Märklin programme I counted comparatively only 13.5 layers at a height of 1.0 cm.



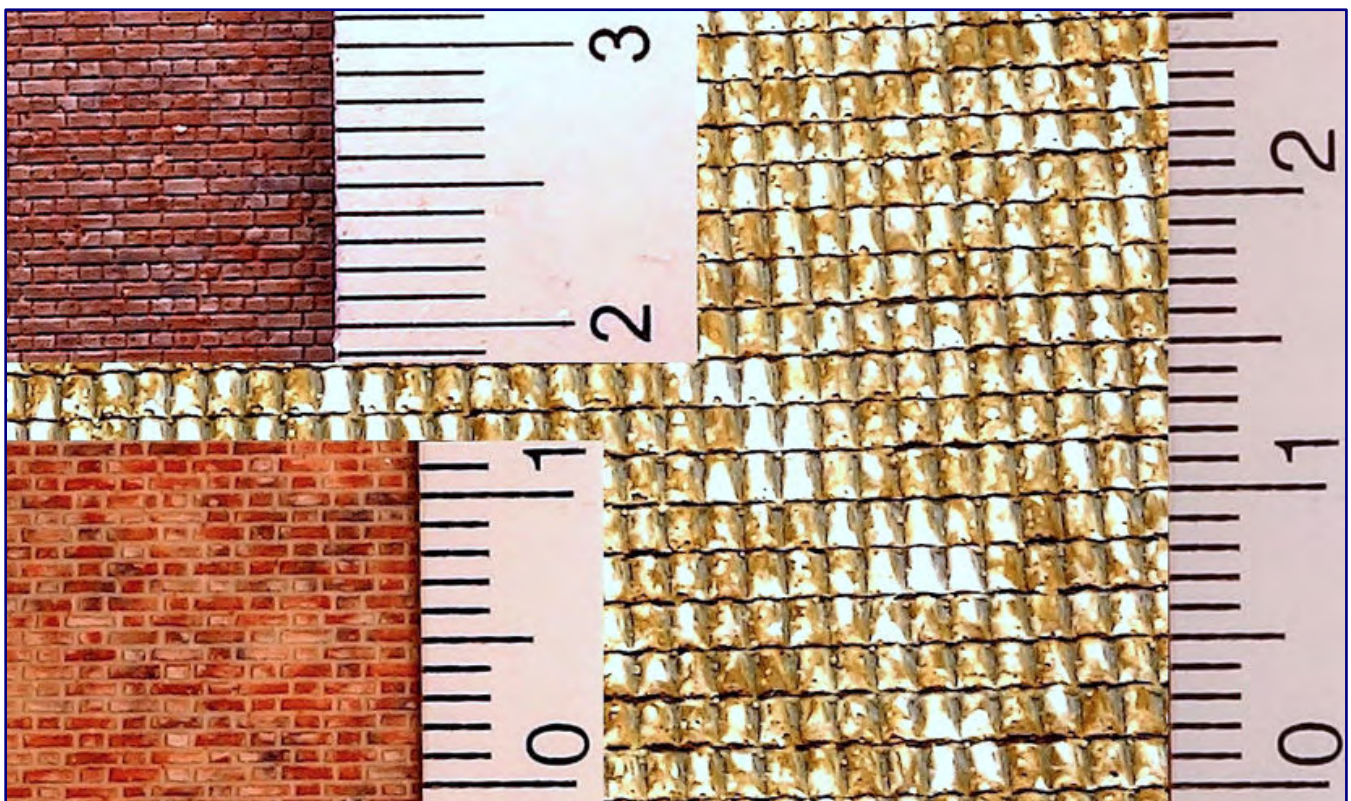
It should also be pointed out that in contrast to its plastic masonry structures, the joints of the Redutex foils are not so deeply imprinted, and thus a more homogeneous and realistic masonry structure is created.

In addition, the structure of the foil is also more prototypical. This concerns that both the length of the bricks – on the plastic model the bricks are much too short, and the masonry bond, which is realistically designed as a cross bond (i.e., with binder and stretcher courses, see photo above).

As a further object of comparison, I show the signal box by Faller (art.-no. 120120) in scale 1:220, which I built, as well as an embossed plastic plate by Schulcz for Z (see page 36 below). This direct comparison shows the strikingly higher closeness to prototype of the Redutex sheet, both regarding structure and its colouring.

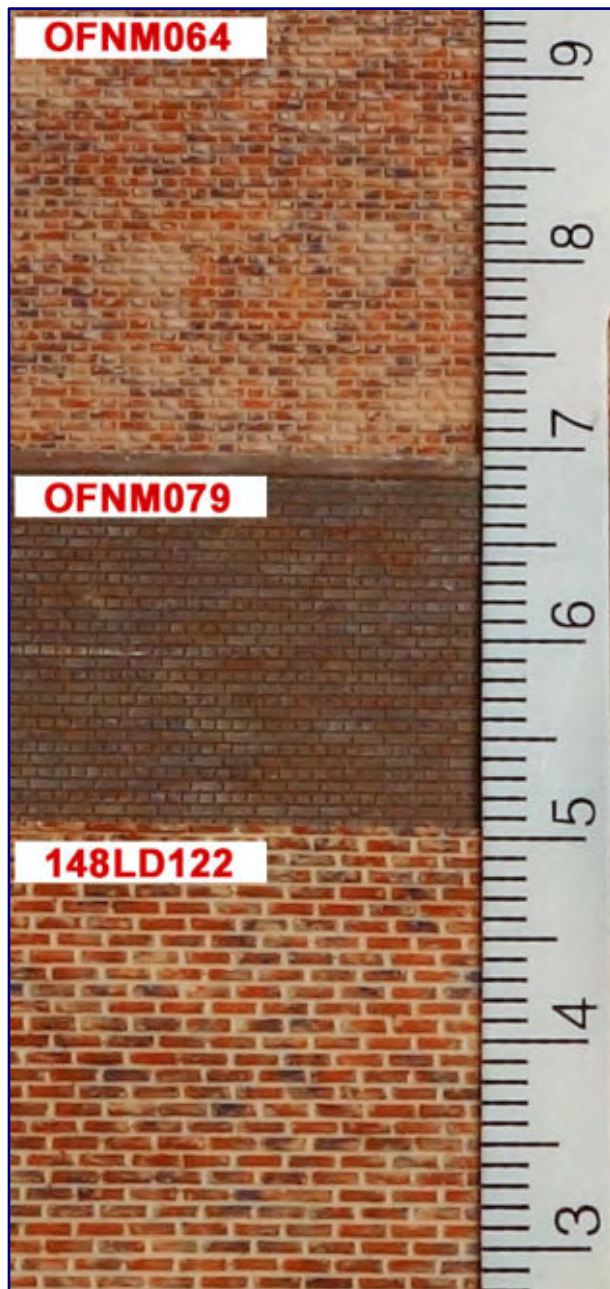
In this context, the distance from which the observer sees a building, usually from the edge of the plant, must also be considered for the individual model. If a structure is chosen that is too small, there is a risk that it will hardly be perceptible from a greater distance. Only for show and photo layouts, as well as ambitious model railway enthusiasts, do other priorities apply, of course.

The structure of the roof foil shown in the photo below is clearly recognisable, even from a greater distance; in the case of the brick masonry structures shown, on the other hand, the realistic, but very small structure, is at some point no longer clearly perceptible from a greater distance.



To illustrate this difference, I superimposed a photo of the structures OFNM079 and OFNM064, each have about 16.5 stone layers at 10 mm height, and 148LD122 has about 12.5 stone layers at 10 mm height (see photo on page 38).

In the case of buildings built with the film with the unscaled greater stone layer heights (148LD122), the stone structure is likely to be more obvious to the lay observer. The more scale-oriented model railroader,



who also wants to put his layout in the right light photographically, will go for the finer structure, even with the risk that the lay viewer will no longer perceive it as brickwork from a distance.

The N gauge roof tile foils, which are carried in the assortment as classic roofing, but also as “monk and nun”, are also very well suited for Z gauge. The comparison to a Kibri plastic roof plate shows that the roof pan sizes of the Redutex foils are almost identical to the dimensions on the Kibri building plate, but also to those on the roof surface of the Faller interlocking.

The 160TT121 foil I used is similar, only deviates slightly in colour and has an authentic-looking appearance on the model. Again, the effect on a photo with extreme magnification is different when viewed with the naked eye, usually more homogeneous and equalising, but not necessarily less realistic.

In this respect, every model railroader should find the right structure in the extensive range of foils. In addition, according to the manufacturer, the range is constantly being expanded and adapted to demand. Special offers often have an individual, exclusive colour scheme.

With the extremely large assortment of different structure and scale foils, it can also happen that individual products do not remain permanently in the range. In the meantime, however, Redutex has started to show suitable foils, especially for Z gauge, in the catalogue, which should simplify the search.

Due to the great flexibility of the foils, they are also very suitable for rounded components such as rounded building edges or curved walls.

For use in curves of model roads, on the other hand, the situation is currently similar to that of other road foils. Only marginal bending radii of the film strips cut to road width are possible. Even prior heating with a hair dryer

does not produce a different result. In this respect, corresponding circular segments or triangular areas must also be produced.

According to information from Redutex, a new type of film is currently in the test phase, with which a two-lane asphalt road can also be adapted to the course of a curve with two approx. 14 mm wide strips. In addition, a narrow, wafer-thin white centre and edge strip for roads is being tested, which should also adhere to porous surfaces.

On individual foils, I noticed minor deviations in the structure pattern, which, however, did not cause any problems in the small scale and the sections that were, therefore, only required in smaller dimensions.

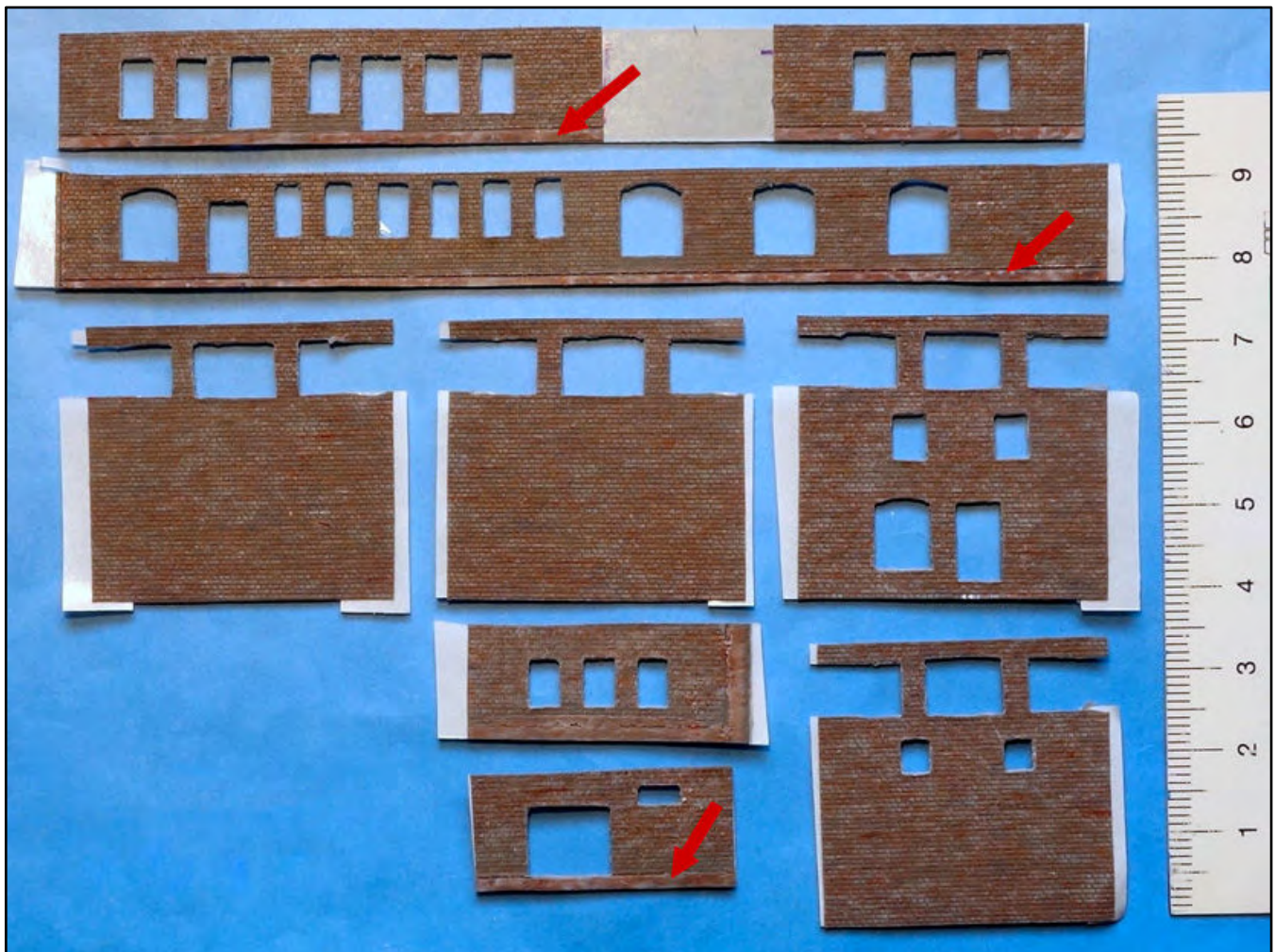
In the meantime, the manufacturer informed me that the final quality controls before delivery would be further improved, so that such films could not be sold in the future.

Foils in road and building construction applications

I now concentrate mainly on the processing of the foils, the construction methods of individual building, and functional models are described, in detail, according to principle and principles in my (electronically available) books. You will also find a link to them at the end of this article.

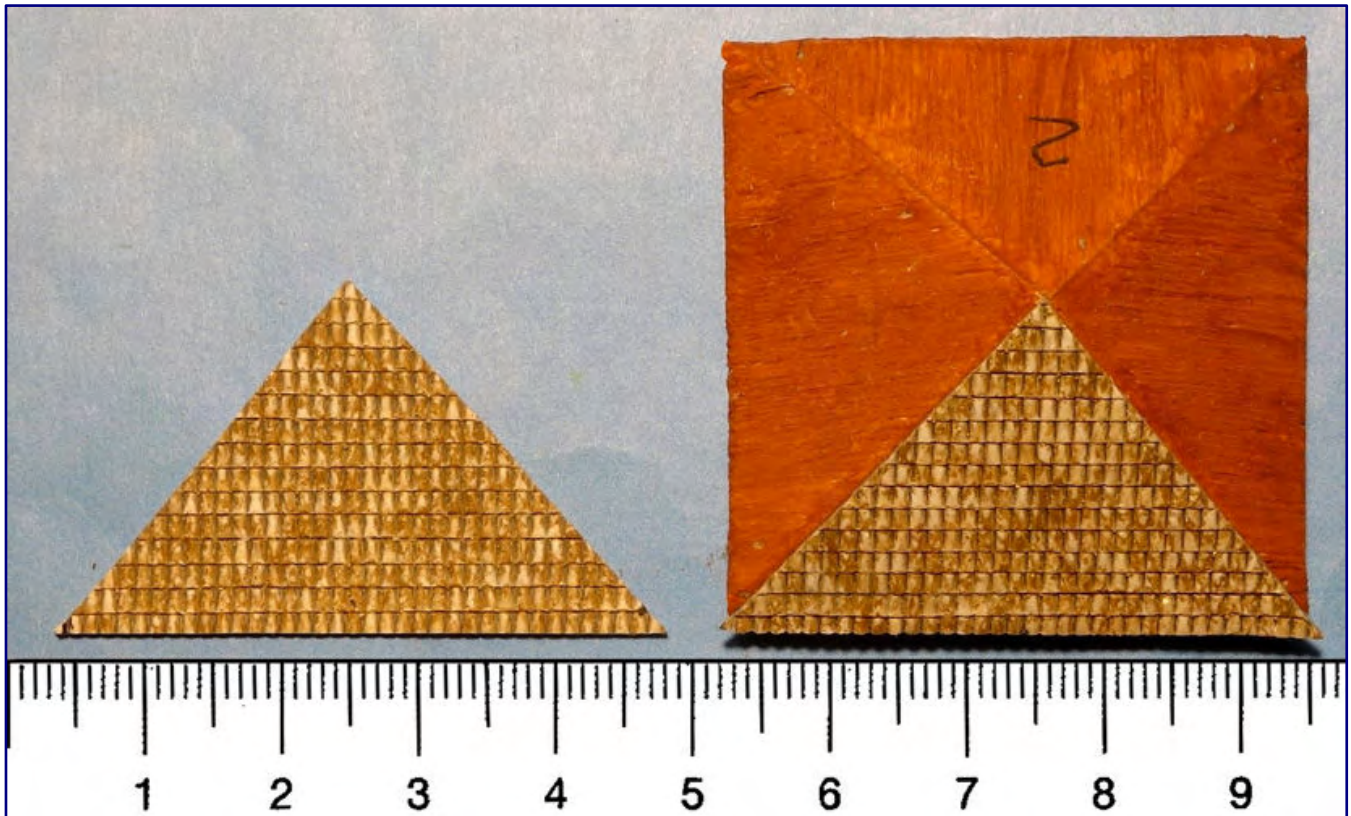
The elastic foils can be cut very easily with scissors or a craft knife. Because the cut can be made more accurately with a knife guided by a (steel) ruler, I have chosen this option throughout.

The surfaces must be free of dust and grease and must adhere well. According to the manufacturer, cardboard, cement, wood, polyethylene foam, polyurethane, and similar materials are suitable.



For building construction, the Redutex film needs a sufficiently stable substrate, here, for example, cardboard. A balsa wood body is also possible.

Immediately after application, the film is still adjustable so that corrections can be made easily. According to the manufacturer, it should be possible to remove and reposition the film for several hours, for example, by heating it with a hair dryer. Permanent adhesion is said to occur after 24 hours.



Joining pieces of foil together is no problem on solid substrates. Such a task had to be accomplished on his roof, for example.

I have not noticed any changes in the durability of the bonding in the last few months, even in critical areas such as narrow corner strips around edges. However, due to the relatively short test period, I am not yet able to make a conclusive statement on the durability.

According to the manufacturer, however, its products have been in use for more than twelve years without any defects. Incidentally, the Miniatur Wunderland Hamburg also uses these foils.

Unlike plastic or cardboard panels, which are usually sufficiently stable, the foils always need a stable substructure due to their flexibility. In the case of roads, this is the prepared substrate to which they are glued.

For my building models, I usually used thin cardboard for wall surfaces in which many window openings had to be cut out, sometimes with stitch bends. For the wall surfaces of the signal box, I was able to make use of the unstructured foil edge by cutting the foil in such a way that the edge is on the lower side of the façade and will thus represent the building base plastered as a splash protection (red arrows in the photo on page 39).

Where a particularly stable base and a permanent fitting accuracy is required, I use balsa wood. The wood is finely sanded until it is smooth. In order to close possible pores and fine cracks, I partly apply an additional base coat of acrylic paint. After drying and dusting, the foil is glued on.

The foils, which are cut to fit exactly, can be easily joined together on the stable base. The elasticity of the foils offers the advantage that the butt edges can be pressed tightly together, so that if they are cut and processed cleanly, a very fine joint is created only by the structured surface.

I glued the brick foil for the arcade supporting wall onto a 1.5 mm thick balsa wood board. The transformer house also has a substructure made of balsa wood. For many models, special attention is paid to the edges of the building. These can be realised with the foil in two ways:

- Either the foil is pulled around the edge of the building, which works very well because of the flexibility. However, with the disadvantage that the edge, especially more visible on a small scale, does not remain sharp-edged but slightly rounded. Here, however, there is the option of carefully “re-sharpening” it by hand or with tweezers.
- The foil is brought right up to the edge and that of the other side of the wall is pushed close to it. This makes the edge very sharp, although a small strip of un-embossed material thickness is often visible.



This narrow strip can be colour-matched to the film surface and, if necessary, the joints can be pressed into the unprofiled surface beforehand with a hard thin sheet metal strip (e.g., the metal strip from a loose-leaf binder) or a fine screwdriver.

In the larger gauges, going around the edges of the buildings will not be so problematic, but in the Z scale the slight curvature is more noticeable in macro photos, less or not at all to the naked eye, i.e., without magnification.



For the level crossing and the field path (above) a somewhat coarser, not quite to scale structure was used, while for the signal box (see photo on page 41) a much finer one was chosen.

Due to the flexibility of the foils and thus a certain softness, narrowest strips can be cut very well. The strips for the lateral edges of the pillar projections of the transformer house are only approx. 1 mm wide. The cornice projection there above the ground floor is also only about 1.5 mm high. This means that narrow strips the width of a roof tile can be cut out of the monk-and-nun structures and glued on as ridge and hip tiles.

In road construction it is also possible, even in Z gauge, to cut off only individual stones to represent a decaying, paved path. In this context I would like to point out that with millimetre-small components the protective foil should be carefully removed from the adhesive layer.

The tip of a craft knife pushed underneath proves useful here, because otherwise there is a danger that the adhesive layer will lift off the film on these tiny areas.

For my level crossing and the field path shown here, I deliberately chose a somewhat coarser and thus not quite to scale structure for reasons of plasticity, but a very fine structure for the pavement at the signal box shown here. The kerbs are made with strips cut from Redutex foils.

Shortly before the editorial deadline for this issue, I received the films with old town cobblestone and cobblestone street structures, newly added to the range under the term "Roman Street." Due to their relevance, I include these 3D structures briefly in my report. For the nominal sizes N and Z, different colours are available, both in standard design and polychrome, each as a continuous square structure and as cobblestone streets.

The surfaces appear relatively even overall, in the darker colours a more vivid structure can be seen, which rather emphasises the character of an old street. In contrast to the more homogeneous-looking



Comparison between a "Roman road" cobblestone sheet for Z gauge (photo above) and one declared for N gauge (photo below).

foils in 1:220 scale, the structures are naturally more visible to the naked eye in the larger 1:160 scale. Here, the modeller must decide on his preference.

Because I have not yet been able to use these foils on the layout, I have alternatively carried out two setting tests with Z-gauge vehicles to illustrate the differences in scale and structure. With these paving foils Redutex has closed an important gap in their programme. With them, the construction of paved plazas and roads is now possible in the early eras, too, but not only.

In summary, because of the enormous choice of different structures, I consider the Redutex 3D films I tested to be very well suited for a wide variety of model building projects due to their realistic appearance in three-dimensional, but not obtrusively strongly emphasised embossing, the nature-identical colouring and especially thanks to the very simple processing, even for beginners. I will increasingly use the foils in my projects in the future.

All photos: Heribert Tönnies

Redutex selection for the 3D foils:
https://redutex.com/index.php?id_category=12&controller=category&id_lang=4

Distribution in Germany:
<https://1zu220-shop.de>

Author's web pages:
<https://bestagernet.de>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Der Einsatz der Baureihe 01 **Standardwerk vervollständigt**

Mit Band 1 hat der EK-Verlag vor knapp einem Jahr bereits ein neues Standardwerk etabliert. Mit dem jetzt vorliegenden Band 2 wird dieses komplett und das Lokportrait der Baureihe 01 vollständig. Bei beiden Büchern wurde nichts dem Zufall überlassen und sie vor allem mit höchstmöglicher Autorenkompetenz unterlegt. Wir sind begeistert.

Frank Lüdecke / Horst Troche
Die Baureihe 01 – Band 2
Der Star unter den Schnellzug-Dampflokomotiven
der Deutschen Reichsbahn-Gesellschaft

EK-Verlag GmbH
Freiburg 2021

Gebundenes Buch
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Frank Lüdecke hat das von Dipl.-Ing. Horst Troche begonnene Projekt eines Baureihenportraits zur Null-Eins fortgeführt und seine Materialsammlung zu diesem Zweck übernehmen können. Folgerichtig wird auch der 2014 verstorbene und hoch geschätzte Spezialist, ohne den das 150-Jahre-Jubiläum der deutschen Eisenbahnen 1985 sicher anders verlaufen wäre, als Co-Autor gewürdigt.

Das sich das gesammelte und aufbereitete Wissen nicht in einem einzigen Band unterbringen ließ, spricht eine deutliche Sprache für Akribie und Sorgfalt, die hier anzuerkennen sind. Strukturell sind die beiden Autoren den EK-Baureihenbüchern treu geblieben, nur wissen sie offenbar weitaus mehr Details, Fakten und auch Bilder zu transportieren.

Deshalb freuen wir uns, dass mit dem Band 2 nun auch die noch fehlende Dokumentation des Einsatzes der Schnellzuglokomotiven bei den Bahnbetriebswerken vorliegt. Einbezogen sind alle Dienststellen der Reichsbahn vor und nach 1945 sowie auch der Bundesbahn, die jemals mit einer Lok dieser Baureihe in Berührung gekommen sind. Auch die Kolonnen des deutschen Ostsektors und Trophäenlokomotiven wurden nicht vergessen.

Mit dem vorliegenden Band wird rund, was rund ein Jahr zuvor begonnen wurde: Vor der Kulisse der geschichtlichen Zusammenhänge erhält der Käufer ein umfangreiches Panorama der berühmtesten deutschen Schnellzugloks, mit dem die Autoren diesen Maschinen ein würdiges Denkmal setzen.



Für den Modellbahnfreund bieten Texte und Bilder wieder viele Anregungen, die mit Freude und Leidenschaft in der kleinen Welt umgesetzt werden können. Technikgeschichtsinteressierte und sonstige Vorbildfreunde füllen eine zuvor große Lücke in ihrem Literaturbestand und angesichts des Gesamtseitenumfangs sicher auch in ihren Regalen.

Bildauswahl, Aufbereitung und Reproduktion aller Aufnahmen fügen sich wieder nahtlos in die lange Reihe weiterer Baureihenportraits ein, die ebenso tonangebend und maßstabssetzend sind wie auch dieser: Wir haben es hier mit Band 2 des Standardwerks über die Baureihe 01 zu tun – nicht mehr und nicht weniger. Seltene und auch unveröffentlichte Fotos haben es in dieses Buch geschafft.

Der zeitliche Umfang reicht bis ins Jahr 1982, denn erst dann wanderte diese Dampflokreihe auch bei der Reichsbahn der DDR aufs Abstellgleis. Exakt, präzise und wohl scheinbar auch lückenlos werden die Einsätze auch abseits großer und bekannter Heimatdienststellen wie den Bw Hof (DB) oder Dresden-Altstadt (DR) wiedergegeben.

Erfasst werden neben den Bw-Beständen und Bespannungen im Statistikeil auch noch sämtliche Lebensläufe, die Kesselverzeichnisse, Lieferdaten und Verbleibe, Kohlenverbräuche, Laufleistungen und Erhaltungskosten. So bleibt kein Wunsch offen.

Vor Augen geführt werden durch gezieltes Aufstellen auch Umstände, die sich jeder Freund dieser Baureihe auch selbst hätte erarbeiten können und die ihm oder ihr sicher unbewusst auch bekannt waren: die kurze Nutzungsdauer der Neubaukessel bei der Bundesbahn.

Ein Phänomen, das auch andere Baureihen wie die 03¹⁰ oder besonders auch die 10 und mit einigen Jahren an Abstrichen auch noch die 01¹⁰, die 041 und sogar die 042 trifft, wird hier sehr deutlich herausgearbeitet. Nur rund fünf bis sieben Jahre waren die Kesselneubauten in diesem Fall im Einsatz, bevor sie den Weg alten Eisens gingen.

Es kam hier der Sonderfall hinzu, dass die leistungsfähigeren NK-Maschinen im wichtigsten Heimat-Bw Hof nie sonderlich beliebt waren und deshalb sogar noch vor vielen Lokomotiven mit alten Kesseln abgestellt wurden.

Dieses Extrem wird hier dazu genutzt, dem Leser vorzurechnen und zu belegen, dass sich die Kosten der Umbauten niemals ammortisieren konnten. Betriebswirtschaftliche Vernunft bestimmte also gewiss nicht bestimmend das Ende dieser Maschinen, eher das Streben nach Prestige und Modernität.

Dampflokfreunden mag dies weh tun, aber es nichts zu ändern und leider hinzunehmen. Es trägt sicher aber auch der Würdigung leistungsfähiger und relativ wirtschaftlicher Maschinen bei, die problemlos noch länger ihren Dienst hätten tun können.

Lassen wir es so stehen und gestehen der Baureihe 01 ihren Ruhm als „größte“ deutsche Schnellzugdampflok zu. Sie glänzte mit keinem Superlativ wie andere Pacific-Maschinen im Ausland. Stattdessen waren Entwicklung, Betrieb und Unterhalt vom Streben nach höchster Wirtschaftlichkeit bestimmt. Und auf diesem Feld hat die Berühmtheit gewiss ihre Lorbeeren verdient.

Fakt ist ebenso, dass an diesem Buch niemand mit Dampflokpassion vorbeikommen wird. Und wer Band 1 bereits im Regal stehen hat, möchte das ja auch gar nicht. Alle anderen werden diesen beiden Werken sicher noch erliegen.

Publishing pages:
<http://www.eisenbahn-kurier.de>
<http://www.ekshop.de>

Rundhauber im Portrait

Eine Magirus-Legende

Ist das wirklich schon siebzig Jahre her? 1951 rollte der erste Rundhauber auf die deutschen Straßen! Heute ist seine Ära längst beendet, aber vergessen ist er nicht. Er ist ein Klassiker und es stellt sich die Frage, warum er das wurde. Ein eher kleines, aber feines Buch beleuchtet und erklärt genau das – auch wenn das eher ein Randergebnis eines leidenschaftlichen Portraits ist.

Wolfgang H. Gebhardt
Magirus LKW
Rundhauber 1951-1965

Motorbuch Verlag
Stuttgart 2021

Gebundenes Buch
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96 Seiten mit 115 überwiegend farbigen Abbildungen

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Autor Wolfgang H. Gebhardt aus Bad Homburg mag in unserem Magazin noch nicht als Autor von Büchern in Erscheinung getreten sein, aber das darf seine Kompetenz und seine Bekanntheit nicht schmälern oder gar in Frage stellen.



Es ist einzig der Tatsache geschuldet, dass Autos im Bereich der Modellbahn zwar wichtig sind, aber eben nur ein Randthema darstellen.

In der einschlägigen Szene gilt er als Nutzfahrzeugspezialist mit den Schwerpunkten deutsche Omnibusse, Lastwagen und Traktoren. Dazu publiziert er auch regelmäßig. Mit seinem zweiten dieser drei „Steckenpferde“ hat er es nun auch zu uns geschafft.

Bewegt haben uns bei der Titelauswahl verschiedene Automodelle im Maßstab 1:220 von mindestens zwei unterschiedlichen Herstellern, die teilweise für Diskussionen und grenzenlose Begeisterung gesorgt haben.

So schien uns klar, dass dieses Fachgebiet auch einmal mit einem passenden auch untermauert werden darf, wenn es um die Frage geht, wann und welchen Diensten die Vorbilder unterwegs waren, was sie voneinander unterscheidet und wie sie sich gestalten lassen.

Bewusst haben wir einen Titel ausgewählt, der auch im Preis günstig und damit den Erwartungen eines Modellbahners gerecht wird. Vermeintlich leichte Kost werden die Käufer angesichts des Kaufpreises und eines überschaubaren Umfangs mit umfangreicher Bebilderung erwarten.

Das dürfen sie auch, aber es sollte nicht dazu verleiten, dem Titel seinen fachlichen Anspruch abzusprechen! Zwar haben wir es hier nicht mit einem dicken Wälzer zu tun, der jede Schraube im Detail erläutert und hinterfragt und womöglich dazu befähigt, ein solches Fahrzeug zu zerlegen, wiederzusammensetzen und auch bei Bedarf zu reparieren.

Vielmehr handelt es sich um ein liebevolles Portrait eines markanten Fahrzeugs in sehr unterschiedlichen Erscheinungsformen und Leistungsklassen, geschrieben für Menschen mit durchschnittlichem Sachverstand in diesem Bereich, aber persönlicher Zuneigung zu diesem markanten und auch geschichtsprägenden Nutzfahrzeug.

Dieser Zielgruppe sollten sicher nahezu alle Modellbahnfreunde entsprechen, die nicht nur für die Vitrine sammeln, sondern ihre „guten Stücke“ auch auf einer Anlage fahren sehen wollen – und dorthin gehören natürlich auch Autos und eben der Rundhauber von Magirus.

„Kleider machen Leute“ ist eine bekannte und anerkannte Redensart. Sie gilt durchaus auch im Nutzfahrzeugbereich, wo ein gepflegter und moderner Laster eine Visitenkarte für Kunden darstellt. Der Rundhauber bediente auch genau diese Erwartungen, denn er verband ein damals revolutionäres Triebwerk mit einer geradezu futuristischen Haube.

Auf dem Höhepunkt der Nierentischära waren Lastkraftwagen keineswegs rund und in dieser Hinsicht modern, sondern sie besaßen lange Hauben, Ecken und Kanten. Sie unterschieden sich oft noch wenig von den Fahrzeugen, die für die Wehrmacht gebaut worden waren. Der Magirus mit Luft- statt Wasserkühlung musste da auffallen und das tat er auch sehr ansprechend.

Er besaß ein eindeutiges Identifikationsmerkmal, das ihn im Rückblick leichter der Ulmer Marke Magirus zuordenbar machte als vielleicht alle anderen Konkurrenzprodukte ihrem jeweiligen Hersteller. Das scheinen ja auch die Modellhersteller erkannt zu haben und für sich zu nutzen.

Was also leistet dieses Buch im genannten Kontext? Natürlich würdigt es dieses Fahrzeug und schafft ihm ein zusätzliches Andenken, aber das wäre noch viel zu kurz gegriffen. Begonnen wird bei der Geschichte seines Herstellers und dessen Weg zum Rundhauber.

Seine Rolle als Teil des Wiederaufbaus und des Wirtschaftswunders erfährt eine Würdigung und eine Einordnung. Dazu gehört auch der bereits erwähnte Motor mit Luftkühlung, der hier als Besonderheit verstanden werden darf.

Wolfgang Gebhardt betrachtet dazu in diesem Band alles Magirus-Rundhauben-Baureihen, mit denen der Nutzlastbereich von 3,5 bis 8 Tonnen abgedeckt wurde. Pritschen-, Kipper-, Kasten-, Kommunal- und nicht zuletzt auch Feuerwehraufbauten waren es, die sie im damaligen Verkehrsgeschehen präsent machten.

Die Texte sind leicht verständlich geschrieben und fördern auch den persönlichen Lesefluss, sie sind gewiss keine schwere Kost. Sie liefern punktgenau die Informationen, die ein Käufer aus der beschriebenen Zielgruppe erwartet und für sein Hobby gebrauchen kann. Wer dabei sprichwörtlich „Blut leckt“, wird sich im Nachgang gewiss nach weiteren Büchern mit mehr Umfang umsehen.

Aber wer eher den großen und unverfälschten Überblick einer längst vergangenen Zeit sucht, um diese auch ebenso authentisch und korrekt nachzubilden, der wird hier abschließend fündig. Magirus-Unterlagen und tolle Aufnahmen aus der großen Zeit des Rundhaubers vollenden diesen Anspruch. Technische Daten und natürlich auch die Quellen des Autors schließen das Buch ab.

Publishing pages:
<https://www.motorbuch.de>



Lokomotiven werden überbewertet.

Für manch einen mag dies eine gewagte These sein, aber mit Statistik lässt sich vieles belegen: Zum Jahresende 1938 verfügte die Deutsche Reichsbahn über 27.379 Lokomotiven, Kleinloks und Triebwagen, 90.970 Personen- und Gepäckwagen, 630.319 Güterwagen und 19.910 Bahndienst und Dienstgüterwagen.

Oder anders ausgedrückt: auf eine Lokomotive kamen 23 Güterwagen, wobei zwei Bauarten herausstachen: G Kassel und München sowie Om Breslau und Essen nach Verbandsbauartzeichnungen mit über 120.000 bzw. annähernd 140.000 Wagen – also mehr als alle Lokomotiven und Reisezugwagen zusammen. Und auch wenn heutige

Fahrzeugbestände nicht vergleichbar sind: Aktuell nennt DB Cargo 1.702 Loks und 72.227 Güterwagen.

Was liegt also näher als diese Wagen auch in der Literatur zu würdigen. Seit 1989 erscheint eine Buchreihe über Güterwagen in der Co-Autoren und ich die zugänglichen Informationen zusammentragen und mit Fotos und Zeichnungen aufbereiten. Zusammen mit den etwas anders aufgebauten Büchern über aktuelle Güterwagen sind bislang zwölf Bände sowie vier ergänzende Broschüren erschienen. Viele der Bücher sind, obwohl z.T. bereits mehrfach nachgedruckt, inzwischen vergriffen.

Im Laufe des Erscheinens der Reihe gab es etliche Änderungen in der Verlagslandschaft, was letztendlich dazu geführt hat, dass ich diese Bücher ab 2021 wieder selbst verlege.

Als neue Reihe geplant sind drei Bücher über Bahndienstwagen. Im ersten Band wollen wir die allgemeinen Themen, Nummernsysteme und Dienstgüterwagen sowie einige besondere Bauarten vorstellen. An diesem Buch arbeiten wir bereits und es soll, wenn nichts dazwischenkommt, Ende 2022 fertig werden.

Die beiden Folgebände sollen dann schwerpunktmäßig alle Bauzugwagen, Mess- und Prüfwagen (Band 2) sowie Schneepflüge und Schneeschleudern, Hilfszüge, Krane und Kranzüge (Band 3) beinhalten.

Stefan Carstens, Wolfgang Henn
Bahndienst- und Dienstgüterwagen
Band 1: Spezialwagen für jeden Zweck

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Stefan Carstens, Harald Westermann
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Auch mit den Güterwagenbüchern soll es weitergehen. Die Reihenfolge und die Erscheinungstermine hängen aber sehr von der beruflichen Belastung meiner Co-Autoren ab. Konkret in Vorbereitung bzw. im Entstehen sind Güterwagen 2.1 über Klappdeckel-, Schiebe- und Schwenkdachwagen und Band 10 über Tiefladewagen.

Stefan Carstens – Harald Westermann
GÜTERWAGEN
Band 10 Tieflade- und Tragschnabelwagen

EISENBAHN DOKUMENTATION UNION

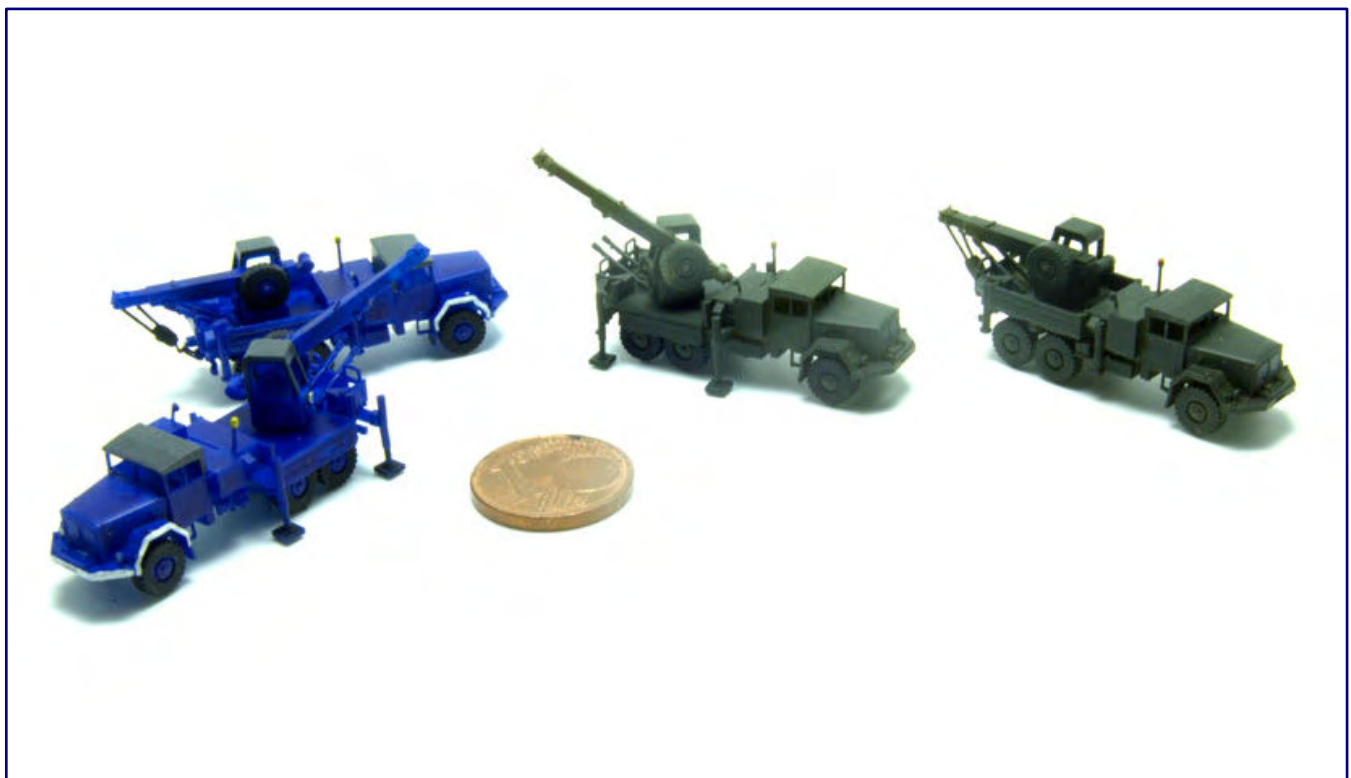
Staying in the conversation (part 8)

Ratimo-Z – Focus on 3D Printing

Rainer-Tielke-Modellbau has been an established small series manufacturer for milling work for several years. In addition to a fixed programme, individual commissioned work is also always on the agenda here. The focus is now on 3D printing. Before this year's theme comes to an end, let's take a look at what new things have come out of the forced breaks in attendance events.

By Rainer Tielke. For almost twelve years I have been producing articles and spare parts for Z gauge, which are sold by myself. In addition, many individual pieces have been produced on behalf of customers, including several larger stations and locomotive sheds or even a customer's own home.

In the past two years, I have started to familiarise myself with 3D printing. As I had previously only designed in two dimensions with Corel Draw, I first had to get to grips with a programme for computer-aided 3D modelling (CAD programme / "Computer aided Design") and familiarise myself with its logic.



New construction methods and manufacturing techniques enable Rainer-Tielke-Modellbau (Ratimo-Z) to revolutionise its product range. The crane based on the Magirus Jupiter, used by the Bundeswehr and THW, is one of the first results.

After trying out programmes from different companies, I decided on the one from Rhinocerus. Working with the 3rd dimension is quite different from creating buildings on drawing paper. Before I always had big problems with the construction of the roofs with different roof slopes, hipped roofs, etc. That's why I often had to redraw and mill parts because they didn't fit in the two-dimensional construction.

Now, I only draw in the CAD programme. If individual parts are still to be milled, then they are converted into a two-dimensional file and prepared for the milling machine. But this work is becoming less and less, as I am now concentrating more on working with the 3D printer. A particular advantage of this type of manufacturing is to create components that would not be feasible with the milling machine, such as the spire in the photo.

Future technology 3D printing

In the course of the last two years, quite a number of different printers have found their way into my workshop. There are now printers with different print space sizes. As the demands on the required print space grew due to some commissioned work, the dimensions of the printers also grew.

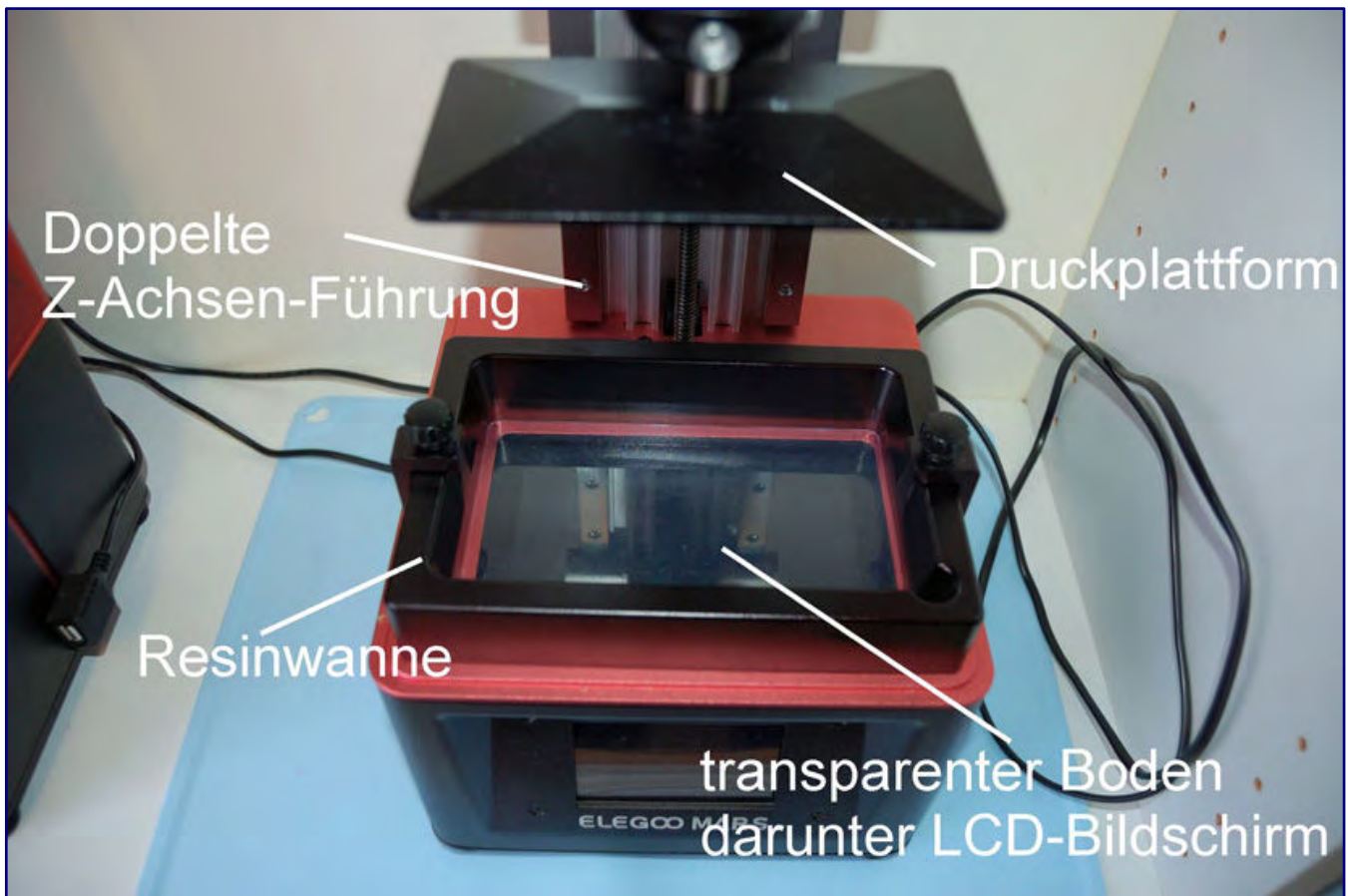
At the moment I have two large printers with print bed dimensions of 120 x 192 mm, a medium printer with 80 x 120 mm and two small printers with 65 x 115 mm. The latter two printers were equipped by me with a more stable, double Z-axis guide and got a mono LCD installed instead of the standard RGB LCD.

This shortens the exposure time required, which then means a shorter time to produce the print. The double Z-axis guide stabilises the print platform better, so it gives a cleaner print image.



A look inside Ratimo-Z's printer room: five 3D printers of different sizes are currently in use, with a sixth due to go into service in January 2022.

However, since the size of the two large printers is not sufficient for all planned projects and the splitting of wall halves is not always advantageous, I am expecting the delivery of a considerably larger building block (156 x 278 mm) in January. This will be used to build larger station buildings, among other things.



The part designation should make it easier for our readers to understand the technical explanations and make them comprehensible.

Using the example of a signal box, I would like to describe the development of a building (see photo series starting on page 52).

As the original version of this signal box has not existed for a long time, the only source available were a few old black and white photographs. In the meantime, I have had some practice in developing the approximate dimensions and proportions of the building from the existing photos. The three-dimensional construction on the computer is very helpful here, because I can already get a feeling for it on the screen.

If the basic proportions match the model photos to a large extent as well as believably, then I can begin with the detailing.

For the roof, I now draw a roof tile, which is then duplicated to form a roof surface. The chimney then gets a brick masonry, and, at the appropriate place, I add the skylight. Finally, I form a gutter.

Something similar is done with the brickwork. Everything that is to be printed must now be made bit by bit. The wall surfaces get their brick structure and can be reproduced with it in 3D printing, even in the depths of the window and door openings, unlike when they were made with the milling machine.

Wall projections now also receive an engraving of the bricks from above. The window sills are also added below the window openings. It is now even possible to create shapes that cannot be produced with the milling machine, see here the roof with attached towers.

continues on page 55

Excursus: Illustration of the construction of an interlocking using a CAD programme



The first step is to develop the dimensions and proportions of the building on the computer. If no (more) drawings with dimensions can be obtained, only reference dimensions, which are or were quasi-standard, can help to mathematically determine all the missing ones. However, the impressions that are perceived by the eye and unconsciously compared with what is experienced are always decisive for the implementation of the model. Only when the basic proportions are right, the work on detailing the surfaces follows.

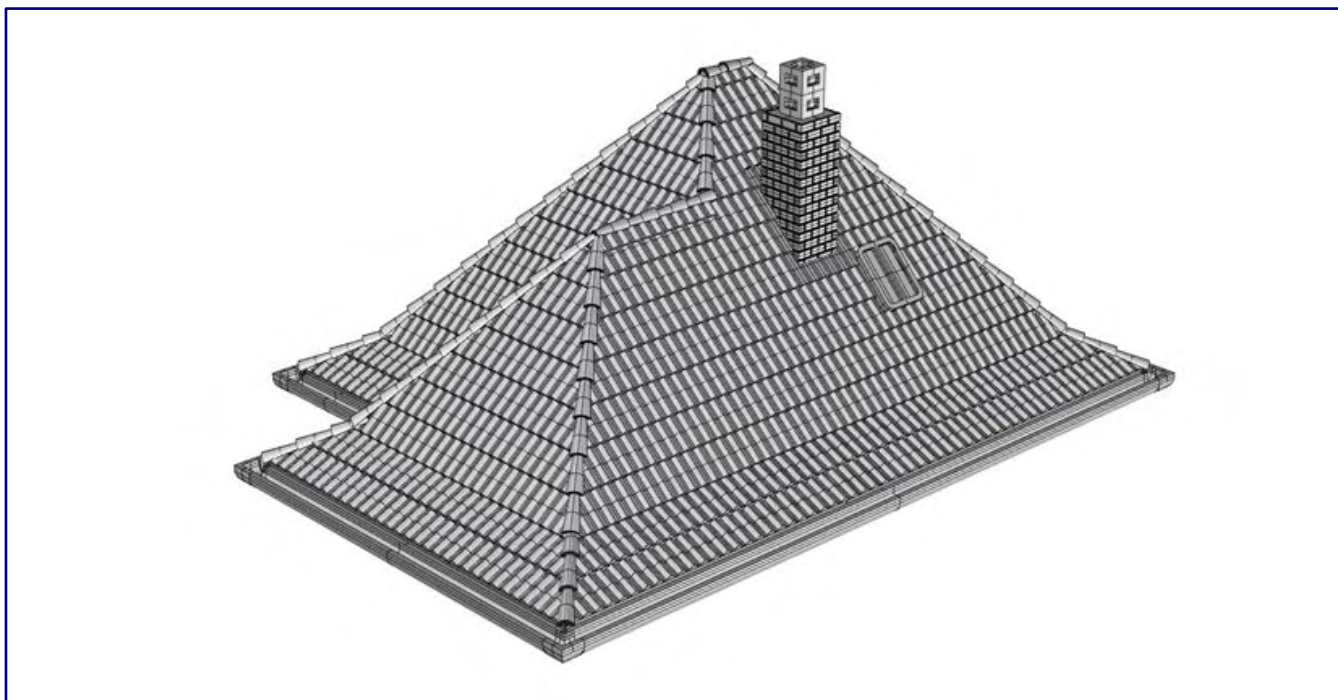


Photo above:

The work of designing the roof begins with drawing a single tile. It is then duplicated to form a roof surface.

In the next step, the chimney stone is given a brick wall and a skylight and the gutters follow at the appropriate places.

This completes the first component of the signal box.

Photo left:

The brickwork will soon follow on the base body as well, because everything that is to be printed later needs structures, little by little.

Unlike when working with the milling machine, engravings can also be made around the corners of the window and door openings.

Before that, however, the wall projections and window frames are drawn, followed by the window sills.



Finally, the masonry structures of the signal box have been added. The simulation as a coloured volume model now gives a good impression of the later building and allows the designer to finally check his work for the intended overall effect before the output processes start.

Ready for output

Once all parts have been designed, a new file must be created with the CAD programme that can give the printer the commands for printing. An STL file is created for this purpose. After a few seconds it is created and loaded into the next programme.

Since the printer cannot print the whole building at once, but only in thin layers of 0.01 mm height, the building has to be prepared for printing one last time.

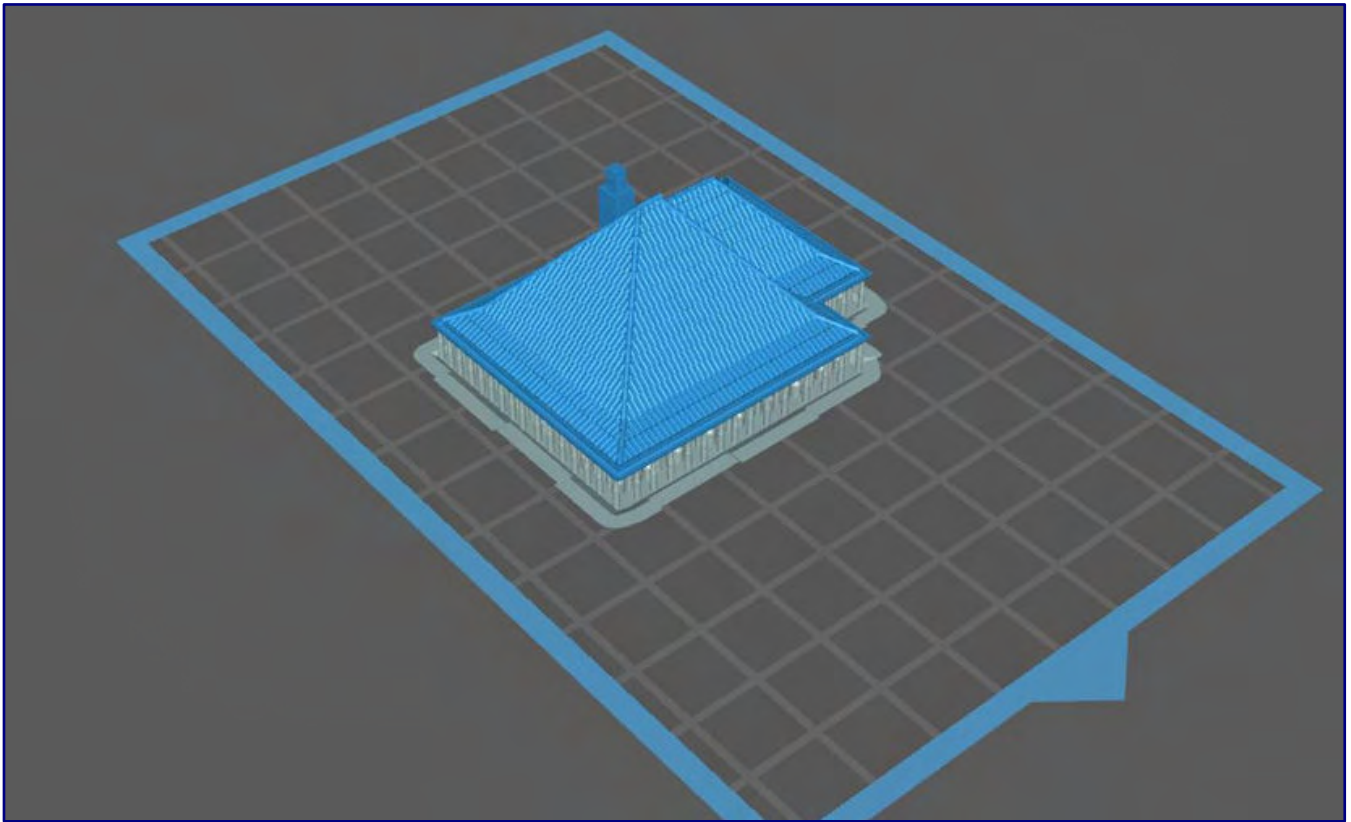


3D printing now also enables construction shapes that could not be produced with a milling machine. This roof with corner towers may express that. Nevertheless, the milling machine is not completely obsolete, because it knows very well how to deliver part supplements.

To prevent damage to the model when it is later detached from the building platform, I provide it with auxiliary supports under the model. I deliberately do not use the automatic function of the programme, but place them individually in the required thickness at the important and necessary points based on experience.

Once all these supports are in place, the programme can divide the model to be printed into slices, which are then reassembled layer by layer in a single printing process to form the roof that I mentioned earlier.

In this case there are exactly 2,660 layers from which the roof is created in the printing tank in 8 hours. On the screen, I can look at each individual layer and discover any defects that may still be present in the printed object.



The tiled roof is prepared for printing: The supports required for error-free printing and clean release from the printing plate are completed.

There is liquid resin in the pressure trough, which is transparent at the bottom. Directly underneath the pressure trough is a screen. And below it are several LEDs that emit UV light. The layered structure of the object is created by the fact that the resin reacts to UV light irradiation with hardening and sticks to the printing platform or the last printed resin layer.

The programme now converts each layer of the model in turn into a photo for the screen, which then allows the UV light to pass through for exposure in the shape of the partial layer to be printed. The printing platform is raised a few millimetres after each exposure and then lowered again to the bottom of the printing tray.

In doing so, it remains one layer thicker above the floor. The resin that is now between the printing platform and the bottom of the printing tank is then exposed to UV light at the points to be exposed, solidifies again and sticks to the previously produced layer. This now happens in constant repetition until all layers have been printed, in this case after eight hours.

Then I can take the printing platform with the printed roof out of the printer and clean it. I do this in the first step with an immersion bath. The printing platform with the attached roof is dipped several times into a container of isopropanol. This rinses off the still roughly adhering resin, followed by a bath of several minutes in the washing cleaner, which is also filled with isopropanol.

Here, with the help of a drive at the bottom of the cleaning container, a flow is created in the isopropanol in it, so that the platform immersed in it and the adhering print are cleaned of all unsolidified resin. Smaller printed parts can also be cleaned very effectively in an ultrasonic cleaner.

After cleaning and drying the printed parts, the next step follows. All the previously attached auxiliary supports must now be removed again. For this purpose, I have had an ultrasonic knife for a few weeks now, which solves this work cleanly and without effort and works much better than the previous removal with the help of a side cutter or scalpel.



After cleaning and drying the parts, the auxiliary supports are removed again. Working with an ultrasonic knife has proved to be a good method.

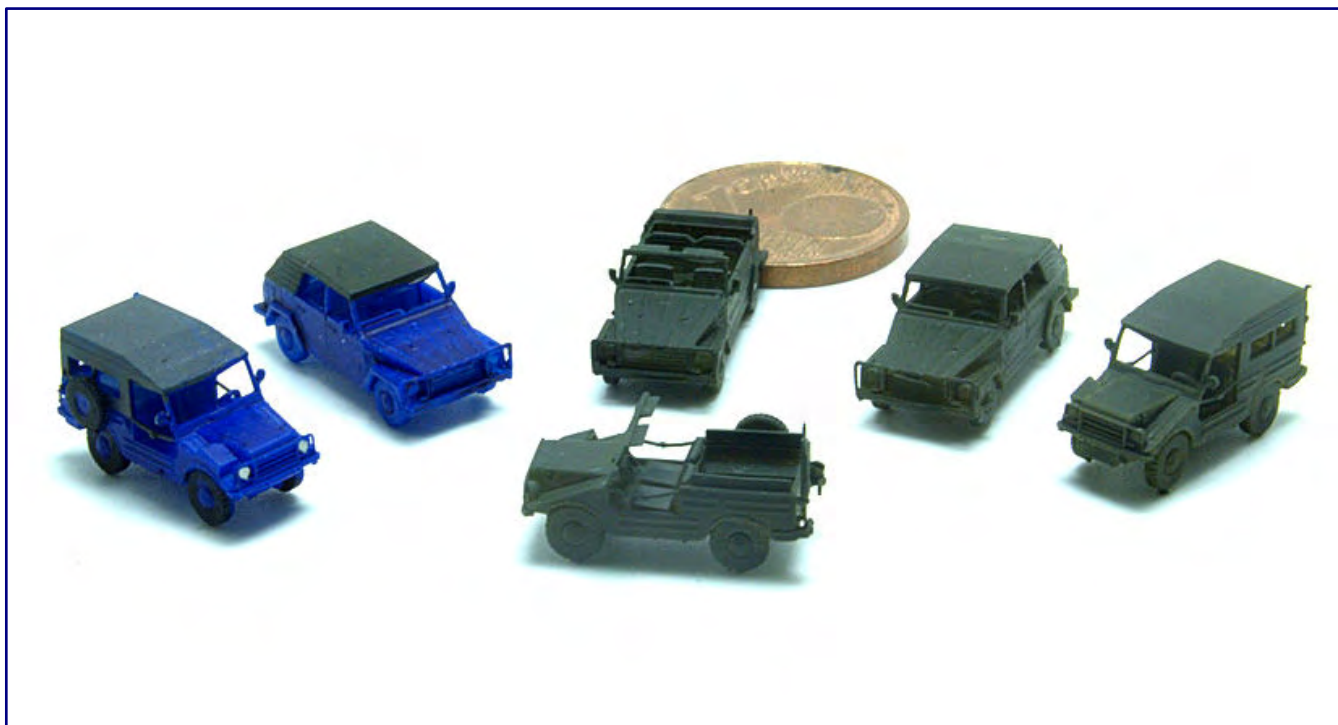
The printed roof is now ready and can be coloured accordingly. I proceed in the same way with the outer walls of the building. After the component has been divided into layers by the processor, the programme displays the expected printing time. In this case, it was divided into 3,526 layers, which will be printed in about 10.5 hours.

Now I can check whether everything turned out as I had planned. This model now gets window frames and doors on the ground floor as well as the window panes, milled out of polystyrene. An intermediate floor for the first floor is also being milled.

At the moment, a large station, which is in production using the 3D printing process, is almost finished. Another station is still waiting to be realised and produced.

More new products

In addition, I have also developed new vehicles in recent weeks. I am continuing the series of Bundeswehr vehicles from the sixties and seventies that I started with the MAN 630. Initially, the small vehicles DKW Munga 8 and VW 181 "Kübel" will be available in both open and closed design.



DKW Munga 8 and VW 181 (photo above) are current new products from Ratimo-Z. In the case of the first-mentioned vehicle, thanks to the 3D printing possibilities, even the chains are reproduced, which should protect the driver and passenger from falling out due to the lack of doors. Rainer Tielke offers the single-axle trailer for the MAN 630 both ready to drive and with offset supports. For all models there is a choice between Bundeswehr and THW design.

The MAN 630 is now supplemented by the characteristic single-axle trailer, which is available in parked form with supports or ready to drive behind the tractor.

Another addition to the programme is the Magirus Jupiter crane. It is available both in the mobile version and in the operational version (see photo on page 49). On this version, the lateral supports are extended and set down on the ground. The crane is supplied separately so that it can be glued to the chassis as desired.

The next new product is very filigree and was quite elaborate in its construction. It is the three-axle Faun 912 as towing vehicle and as trailer a low loader, with which everything from pontoon bridges to light tanks and bulldozers could be transported by the Bundeswehr.



The heavy tractor Faun 912, including low-loader trailer, is also a new product available from January 2022, with which the limits of 3D printing could be explored and exploited. The trailer can be used in two positions.

The low-loader can be dismantled into front and rear chassis as well as the middle beam, plus the associated drive-on ramps. It is also planned to offer these vehicles in the THW livery. The new vehicles can be ordered now. Delivery will then take place from January 2022.

All photos: Rainer Tielke

Manufacturer and own sales pages:
<http://www.rainer-tielke-modellbau.com>

Reports from Friedrichshafen and Dortmund **Only a short Trade Fair Autumn 2021**

After a long period of deprivation, many, exhibitors and visitors alike, were looking forward to the opening of the fairs. But the pandemic is not over yet and it turned out to be a short autumn season of trade fairs that was not free of fears. Stephan Fuchs and Torsten Schubert were on site in Friedrichshafen and Dortmund to share their impressions with all readers.

On 5 November 2021 at 9 a.m. sharp, the time had come: the gates of the trade fair in Friedrichshafen opened for Faszination Modellbau. Apart from smaller exhibitions or meetings, this was the first major model railway fair since the appearance of the virus that has been following us for almost two years.

A new feature in Hall A7 was the “Lego Fan Exhibition”. To say it in advance, you could notice that there was a certain uncertainty in the air. Some exhibitors were missing. To name just one exhibitor outside the narrow model railway sector: We looked in vain for the electronics dealer Conrad.



Many model railway enthusiasts must have longed for a public ride, such as here on Thomas Heß' “Winzlingen” layout at the trade fair in Friedrichshafen. Photo: Stephan Fuchs

The aisles between the stands were certainly wider than usual, not just because of the hygiene concept. The attendance of about 30,000 visitors was also about half as many as before the Covid 19 pandemic. Thus, it was already considerably emptier in the halls from about 2 p.m. onwards.

Regarding the hygiene concept: from my point of view, the rules communicated as 3G were consistently implemented, the obligation to wear masks was almost always observed by the public and the exhibitors. That is certainly unusual, but quite feasible!

But that was it for the negative vibrations. Even the day before, when the ZFI stand was being set up, there was a feeling that everyone was eager to be able to present their hobby to a larger audience again.



A look at the ZFI stand before the exhibition opened makes it clear how the spacing rules were effectively implemented as part of a hygiene concept. Such wide aisles have probably not been seen at an exhibition of this kind before. Photo: Stephan Fuchs

The Z-Freunde International were also a bit shaken, for example Axel Hempelmann was absent, despite vaccination, due to quarantine. But the rest were really “up for showing!” May the readers forgive me for using this casual term, it is the only way to convey the prevailing mood.

I, too, let myself be infected by this atmosphere, not by the virus, so that I got more involved in the overall picture and the mood and went into less detail, for example, when it came to researching new products.

The following people exhibited at the ZFI stand:

- Uli Günther demonstrated two segments of his layout, one not yet finished and the Neuschwanstein Castle. However, the castle was temporarily exchanged for a ruined castle in order to get some variety. He also had three showcases with a wide variety of rolling stock.
- Thomas Heß had a suitcase system and his folding board with him to demonstrate digital control.
- Alois Forstner was represented with a suitcase system. Among other things, a bus and his camera car rode on it. The photo from the camera was transmitted via a beamer to a wall of the booth at the stand.

- Gerhard Maurer presented his autumn layout “Traumschleife.”
- Two smaller layouts came from the Stammtisch Bayern, including a converted tray for breakfast in bed, two hands-on activities for children of all ages and a display case. Another display case came to the stand via Märklin and was equipped with various Christmas carriages and foam modules.



Instead of breakfast in bed, the Z Friends International served a US-style small-scale layout on a tray. Photo: Stephan Fuchs



“Dwarf Ingo” delighted the youngest among the exhibition visitors. Photo: Stephan Fuchs

- Roland Kimmich exhibited his almost obligatory Spur Z television set.
- Ingo Sindermann once again exhibited his fantasy dwarf layout. Even dressed up as a dwarf with a red cap, he found much favour with small children, but also with the “older” ones.
- On Sunday, three exhibition layouts were added by Gotthard Schmidt.

Just as with the exhibitors, it was noticeable among the public that it was generally perceived as a pleasure to be able to get out of the now unloved daily grind and to be able to occupy themselves with the hobby among like-minded people.

Many also just wanted to look around and get some ideas. At the ZFI stand, the camera car, the digital possibilities of Z gauge and the hands-on activities were particularly popular. This resulted in some very intensive and long conversations.



Three exhibits by Gotthard Schmidt completed the stand on Sunday. On one of them, the class 103 was running with an express train of pop colour cars.

The model railways on display ranged from Z gauge to nominal size 1 and 2m (LGB) garden railways. However, some of the layouts and exhibits had already been shown before. I already knew some of the N gauge modular layouts from Stuttgart, and the 1:87 workshop layout was recently reported on again in Eisenbahn-Romantik.

Speaking of Eisenbahn-Romantik: Hagen von Ortloff was again represented with friends at a huge stand with various historical layouts of different nominal sizes. New were two layouts that were operated by cranking a dynamo.

What fun, especially for children! Some of them cranked so hard that the changeover impulse for the locomotives was triggered and everything suddenly shifted into reverse gear to the astonishment of everyone. Several worn cranks, which Hagen then patiently replaced, were the result of this enthusiasm.

There was also a lot of interest in the real steam meeting. There, too, we could gain the following impression: "Finally we are allowed again!" For those who have not yet seen it in person: definitely have a look!

Some of the things the participants come up with are really funny. Just being able to drive your locomotive over a very long distance through two exhibition halls is something very special. You can't enjoy that in this way anywhere else. And the rules are also very simple: whoever drives up is to blame.

In conclusion, I would like to say: We had a lot of fun at the ZFI stand. We can only hope that this will not remain a one-day wonder and that it can continue seamlessly in 2022.

continues on page 65



Two final impressions from Friedrichshafen away from Z gauge: MEC Pirna's TT gauge layout (photo above), and public traffic at the live steam meeting (photo below). Photos: Stephan Fuchs

And I would like to make a personal comment: As model railway enthusiasts we are already somehow “in a funny mood”. Two or three small punctures are no longer important! Vaccination helps us all!

View over Intermodellbau

Everything was different at Intermodellbau this year. After the fair had not occurred in April 2020 and 2021, it should at least work out as a “winter edition” in November 2021. But not only the month was different, but also the days of the week: Wednesday to Saturday (17 to 20 November) instead of Thursday to Sunday.

This is an unusual time slot for the Intermodellbau. If the basic postponement of the date was related to the Corona pandemic, the change in the days of the week was due to the Sunday of the dead, which would have been a silent holiday with a sales ban. But the schedule was also completely different from what trade fair visitors are used to.



That was Intermodellbau 2021 as a “winter edition:” strikingly large open spaces, wide aisles and a noticeably lower visitor turnout.
Photo: Torsten Schubert

Before entering the halls, all visitors and exhibitors were checked for compliance with the 3G rules. After a short waiting period, everyone received a coloured and non-destructively removable wristband as proof of successful testing.

This year the fair also only occupied halls 4 to 8, with model railways as usual in halls 4 and 7. It was also noticeable that many aisles in all halls were significantly wider. On several stand areas we found seating for visitors, which was called the “Intermodellbau Lounge” in the hall plan.

The open spaces in the halls were also much larger during this fair. So, even health requirements and distance bans could not hide the fact that a large number of exhibitors were absent and suspended this Intermodellbau.

The reasons may have been many: low expectations of visitor attendance, a seemingly unfavourable ratio between stand costs and sales potential, or even concerns about one's own health. In most cases, we will probably not know the reasons, but our Z gauge seemed to be particularly shaken this time around.



Due to the lack of Z-gauge participation among the exhibitors, we will take a look at the larger gauges. Photo: Torsten Schubert

Some of our regular dealers and manufacturers, otherwise permanent guests at this fair, were unfortunately not there. This also applied to the entire small series for 1:220 scale, so a visit from the Zetties' point of view might not have been worthwhile just by looking at the list of exhibitors.

But there were also cancellations at short notice: the weekend before, the accessories manufacturer Noch was listed in Hall 4, but unfortunately was not there. Our enquiry confirmed at least here what we had already feared: The steep rise in the number of infections required consideration and led to the cancellation.

One consequence of the cancellations was an unpleasant effect from the visitors' point of view: many stands were set up as island stands and only long, white walls lined the paths through the aisles. This destroyed any atmosphere and made the fair look extremely unattractive.



The special wagon for the Intermodellbau was the one that was actually already planned last year: an open wagon of the type Eaos of the Dortmund railway with scrap load (item no. 80730). Photo: Jörg Landau



The annual car 2022 for the members of the Insider Club was not very well received according to the readers: a covered freight car Gm "Papierfabrik Salach" of era I (80332). Photo: Ralf Junius

One of the few exceptions for the small scale was the Märklin stand. There, in addition to some older and the current museum wagon, the fair wagon was also available, which was already presented for the Intermodellbau 2020.

This is an Eaos 106 type open freight wagon in brown paint with touch-up spots and the green-yellow logo of the Dortmund railway (Item No. 80730), fitted with a scrap load.

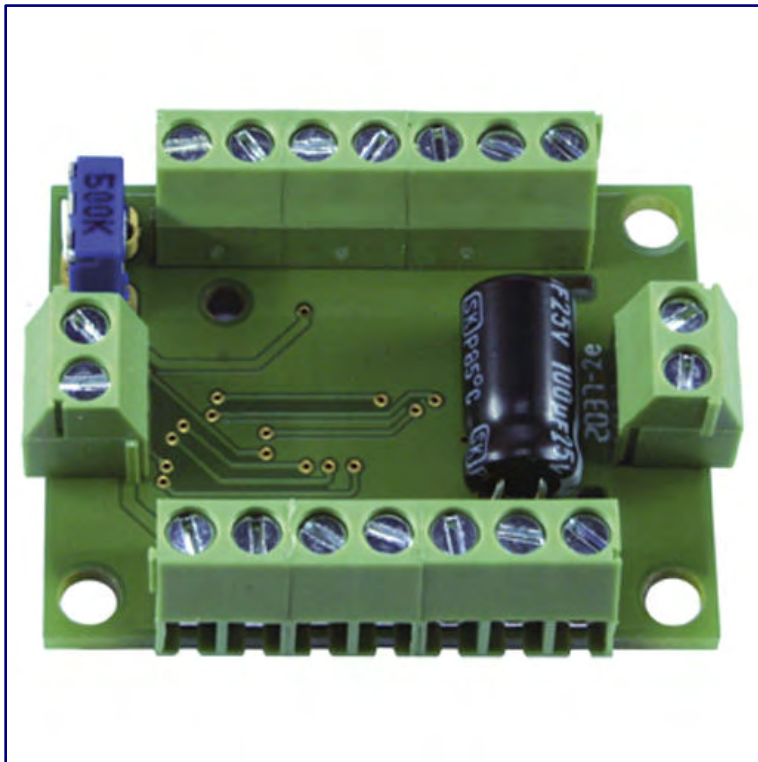
Märklin did without the usual Insider Club stand, where a small gift is normally distributed on presentation of the membership card. On the other hand, it was a pleasant premiere that the new Insider models for the following year

were presented for the first time at this fair.

In Z gauge, the insider car 2022 already included in the article is a later G 10 in light grey livery and design of the Papierfabrik Salach (Salach paper factory)(80332), employed by the K.W.St.E., i.e., intended for Era I.



Germany's most powerful diesel-hydraulic locomotive, the V 320 001 (88320), will be the Insider Club model in the anniversary year 2022. Illustration (3D volume model): Märklin



There were new products at Tams-Elektronik: six more "Next Generation" LC modules were presented. Photo: Tams-Elektronik

As expected, the official figures and exhibitor impressions published by the organiser naturally read more like a success story: despite the pandemic and high incidence levels, around 40,000 visitors would have found their way to Dortmund (usually around 78,000), 9% of whom came from abroad (comparative figure for 2019: 10%). The 325 exhibitors came from 13 different countries (2019: 500 from 19 countries).

According to the press release, the organiser was "very satisfied" with this, although for us the question remains open as to how such a shrunken trade fair can support itself economically at all.

Apparently, the exhibitors' turnover at the fair does not seem to match the widespread reports of success: The per-capita expenditure at the exhibition, which is otherwise so effectively advertised, was withheld from us in this year's financial results.

On the other hand, the discipline of the visitors deserves positive mention: the obligation to wear masks was strictly observed in the halls. Only a very few visitors had not put on their mouth-nose masks, as required. Most of the dealers did not make use of the exemption from the obligation to wear a mask during sales talks, either.

The next Intermodellbau, hopefully again in the usual scope and with Z gauge participation, should not be long in coming. The usual time slot in April is planned again: Thursday, 7 April to Sunday, 10 April 2022. Let's wait and see how the pandemic will develop by then.

Authors:
Stephan Fuchs (Faszination Modellbau)
Torsten Schubert (Intermodellbau)

- **Exhibitor selection with Z-gauge relevance:**
- <https://www.faller.de>
- <https://www.geramond.de>
- <http://www.luetke-modellbahn.de>
- <https://www.maerklin.de>
- <https://www.otto-scrap-design.de>
- <http://www.peter-post-wekzeuge.de>
- <https://tams-online.de>
- <https://viessmann-modell.com>
- <https://www.z-freunde-international.de>
- <https://z-stammtisch-bayern.de>

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Z-gauge in the shop window for 40 years:

Already, in October the children ask: "When is the train coming?" Such a little train goes well with our handmade jewellery.



At the push of a button, the Z-gauge train in the shop window of Goldsmith Otterpohl makes a few rounds at Christmas time, and has been doing so for forty years. Photo: Wolfgang Otterpohl.

At the push of a button, it goes a few rounds and everyone looks: the women at the special jewellery, the husband and child at the train. At least the woman has the feeling that "he" is looking in the same direction with interest – at whatever...

Wolfgang Otterpohl, Halle (Westfalen)

Editor's reply: We remember well the days before Christmas when children everywhere would press their noses against shop windows behind which a railway layout could be seen. We would like to take advantage of our reader's letter to remind us of this and perhaps make this an annual custom again.

Welcome Theme "Train Formation":

Once again, we would like to thank you for your valuable work for our hobby. In the November issue you made train formation suggestions. The class 94 was used on the steep Boppard - Buchholz line from 1927 to about 1956, before it was replaced by the VT 98.



Based on a photo template, our reader put together this train as E 3725. Photo: Hans Helbach

In the book "Erinnerungen an den Schienenbus" (Memories of the Rail Bus) by Joachim Seyfert, I found a train consisting of a three-part rail bus set and an attached Silberling. The express train 3725 was photographed near Schenkenzell in July 1985 (...). I have reproduced this example briefly and send you another photo.

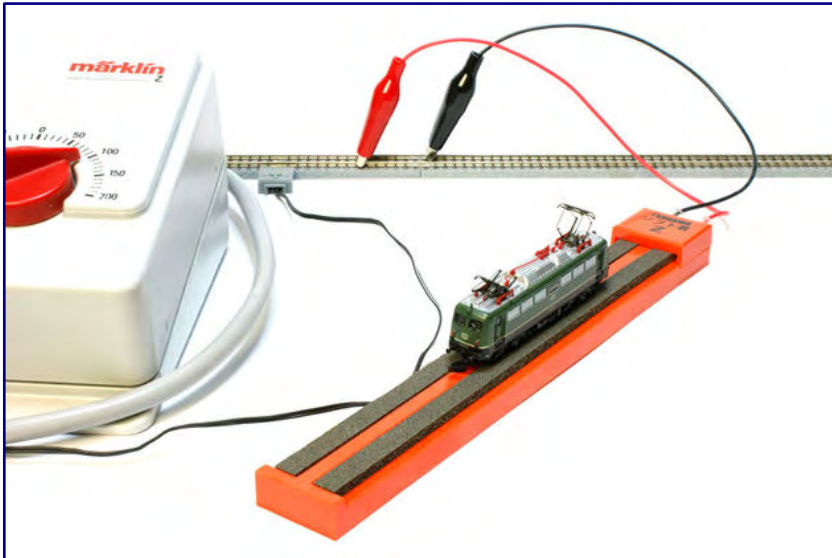
Hans Helbach, Bonn

Editorial response: This letter to the editor is also representative of other positive feedback on the train formation proposals that have reached our editorial team in various ways and through various channels. We would like to thank all those who have given us the opportunity to identify particularly popular reader topics.

Long-term experience with the wheelset cleaner:

I hope you're all well. Thanks for an always fascinating e-magazine. I work in N scale narrow gauge, using a lot of Märklin loco and rolling stock chassis. For a while now I've been using the Tsugawa wheel cleaners and they work very well.

There is a problem that I've just discovered with the powered version that I've been using for some time. It's now not delivering enough current to run a Märklin chassis and it looks like a problem with the foam. Close to the screwed-on wire connections, a short Märklin loco like the 0-6-0 does run. At the opposite end of the cleaner, it will not. I have a chassis which has a low current draw coreless motor and that will run OK at the end furthest from the connections. This indicates that it's a problem with insufficient current (amps).



Our reader Mark Fiedler would like to share his long-term experiences with the wheelset cleaner with us.

Putting a meter across the strips to measure the voltage, it shows around 9 volts using a Märklin Electronic08 controller, and that there is electrical continuity right along the length of the foam. A brand-new powered Tsugawa wheel cleaner fresh out of the packet works as expected, without any problems.

I also have the unpowered wheel cleaner which works as you describe in the magazine, but I've also found the powered wheel cleaner works in the same way.

As a cleaning solvent, I use WD-40 Contact Cleaner, and not the well-known WD lubricant/water dispersant.

There's some interesting discussion of contact cleaners in this article:

<https://s3-us-west-2.amazonaws.com/mrhpublish.com/2019-05-may/online/index.html?page=9>

I hope this is useful information for you. I just love the short train on page 10 of the November issue!

Mark Fiedler (UK), per E-Mail

German translation:

Ich hoffe, es geht Ihnen allen gut. Vielen Dank für ein immer wieder faszinierendes E-Magazin. Ich arbeite in der Schmalspur der Spur N und verwende viele Märklin-Lokomotiven und Rollmaterial-Chassis. Seit einiger Zeit verwende ich die Tsugawa-Radreiniger und sie funktionieren sehr gut.

Allerdings habe ich gerade ein Problem mit der angetriebenen Version entdeckt, die ich seit einiger Zeit verwende. Sie liefert nicht mehr genug Strom, um ein Märklin-Fahrgestell zu betreiben, und es sieht nach einem Problem mit dem Schaumstoff aus. In der Nähe der aufgeschraubten Drahtverbindungen läuft eine kurze Märklin-Lok wie die 0-6-0. Am anderen Ende des Reinigers läuft sie nicht. Ich habe ein Fahrgestell mit einem eisenlosen Motor mit geringer Stromaufnahme, der an dem von den Anschlüssen am weitesten entfernten Ende gut läuft. Das deutet darauf hin, dass es sich um ein Problem mit zu wenig Strom (Ampere) handelt.

Wenn man ein Messgerät über die Leisten legt, um die Spannung zu messen, zeigt es etwa 9 Volt an, wenn man einen Märklin-Electronic08-Transformator verwendet und dass es auf der gesamten Länge des Schaumstoffs elektrischen Durchgang gibt. Ein brandneuer Tsugawa-Radreiniger, frisch aus der Packung, funktioniert wie erwartet ohne Probleme.

Ich habe auch den nicht angetriebenen Radreiniger, der so funktioniert, wie Sie es in der Zeitschrift beschreiben, aber ich habe auch festgestellt, dass der angetriebene Radreiniger auf die gleiche Weise funktioniert.

Als Reinigungsmittel verwende ich den Kontaktreiniger WD-40 - nicht das bekannte WD-Schmiermittel/Wasserdispersiermittel. Dieser Artikel enthält eine interessante Diskussion über Kontaktreiniger:

<https://s3-us-west-2.amazonaws.com/mrhpublish.com/2019-05-may/online/index.html?page=9>

Ich hoffe, dass dies nützliche Informationen für Sie sind. Ich liebe den kurzen Zug auf Seite 10 der November-Ausgabe!

Editor's reply: We have written to the German sales partner and asked whether the phenomenon is known and whether further information can be provided. For our part, however, we would like to remind you that both the cleaning felt and the cleaning pad of the electric cleaner are wearing parts, and are therefore also offered individually as replacements.

Impressive narrow-gauge models in scale 1:220:

Firstly, many thanks and compliments for **Trainini**. It's great what you're doing, it always gives me pleasure.

For the last few years, I have been busy making narrow gauge models in Ze, gauge 3 mm. In the meantime, I have more than 60 passenger and goods wagons and several railcars and locomotives. All my own work.

Would this be something for **Trainini**? I am sending you some photos so that you have an impression. To prevent them from getting stuck in the email, I have reduced their size and compressed them a bit. The resolution of the originals is much higher, and they are in TIFF format.





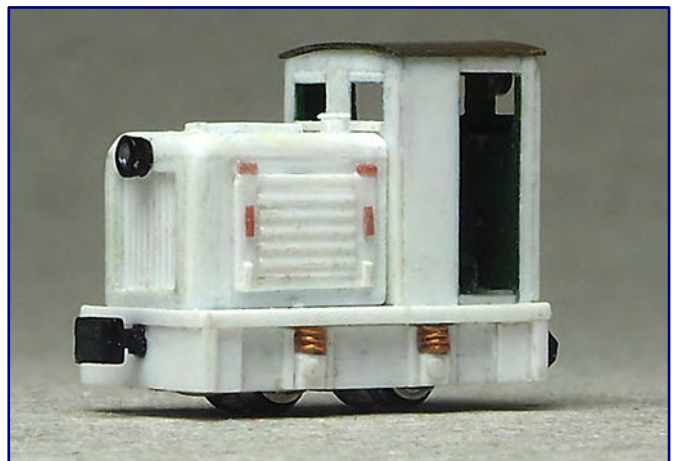
above and on page73:

The selection of narrow gauge Ze models on 3 mm track from our reader's own construction shows mainly powered (locomotives) or photos at least rolling (wagons and diesel locomotive in the last photo) vehicles, partly in comparison to similar standard gauge models. Only the covered wagons in the third photo and the steam locomotive in the fourth photo are purely standing models. The diesel locomotive without drive is moved by a ghost car. All photos: Reinder Rutgers

I have many more photos of my Ze models. I could also write something about how the models were constructed, etc.

Motorisation of the models is by means of T-gauge chassis (done). All four-axle wagons and some two-axle wagons are mobile. There are several standing models for loading roads, sidings, etc. The models have been aged because they are intended for a layout, the 'modern' railcars and conversion wagons least of all because that is prototypical for the early sixties.

I photographed against a white background. This makes the models look like free-standing. I photographed the models under construction against a grey background because the polystyrene is white.



These two photos give an impression of how wagons and locomotives are made mainly from Polstyrol. If readers are interested, we will be happy to report in more detail about our reader's projects. Photo: Reinder Rutgers

You do your work on a voluntary basis. It would be an honour for me, after many years of reading, to be able to contribute something and not just 'consume'.

Reinder Rutgers, Waalre (Netherlands)

Editor's reply: We thank you for the praise and the many great photos. We would also like to publish a more detailed report on this, which would portray further models and hopefully also explain how they came into being.

Bridge to Rio de Janeiro opened:

The bridge over the Fleet in Hamburg's Speicherstadt warehouse district has been opened, adding (initially) 46 m² of layout space to the Miniatur Wunderland. On 1 December 2021, Hamburg's First Mayor Dr. Peter Tschentscher and Frederik Braun sent the first train over the bridge towards the South America section in the presence of Brazil's Ambassador Roberto Jaguaribe.

The milestone of a new section in a new storage block with an almost completely different world was celebrated vigorously in the presence of the press and staff, albeit in a manageable circle due to the nature of Corona.

After a French TGV had crossed the bridge at high speed with the track motifs "The World from Above," the guests of honour and press representatives also crossed the glass bridge high above the water.

Before they reached the entrance area, designed as the gate of a sunken Mayan temple, colourfully dressed samba dancers lined their way.



Dr Peter Tschentscher (left) and Frederik Braun give the go-ahead for the first train to travel from the previous storage block over the glass bridge into the new section. Photo: Miniatur-Wunderland

Once they arrived in the new warehouse in Block L, their gaze first fell on Sugar Loaf Mountain, Copacabana and Corcovado with the statue of Christ, and then on the famous Carnival of Rio de Janeiro. One day later, on 2 December 2021, the "new world" was opened to all visitors of the Miniatur Wonderland.

We will report in detail on the many special features and sights of the Rio section as the first part of South America and Antarctica in words and photos in January. Exciting and varied reports can also be expected in the print press in the near future.

A new Viennese Club car:

The Vienna Z Club is once again launching a regulars' car pack in 2021. The partner for the production of the project was once again the small series manufacturer FR Freudenreich Feinwerktechnik from Sanitz in Germany.

Two freight wagons of the Villach type were chosen. After the end of the Second World War and the capitulation of Germany, about 1,500 to 2,000 of the approximately 50,000 wagons are said to have remained in the rebuilt Austria. However, many of them were severely damaged.



The two Villach type open goods wagons for the Vienna Z club are part of the ÖBB stock. Photo: FR Freudenreich Feinwerktechnik

Some were therefore scrapped, immediately; others were converted into different wagon types. The number of Villach type freight wagons in use in Austria was probably around 500.

The wooden walls of the superstructures of many of them were later replaced by sheet metal walls in order to achieve a higher load-bearing capacity. Their use will have ended in the late seventies of the 20th century.

The model corresponds technically to the previous Villach variants with a running gear made of finely etched and soldered sheet metal, a polystyrene superstructure, wheel sets insulated on both sides, and couplings compatible with the Märklin system.

We don't know if there are still copies available for sale. If you are interested, please contact us directly at [zspur.stammtisch\[at\]gmail.com](mailto:zspur.stammtisch[at]gmail.com).

American Z Line's (AZL) deliveries:

AZL, too, does not want to miss out on Christmas business and delights its customers with a whole series of new deliveries. The EMD SW1000 for the D&RGW is one of the first to roll off the production line with three service numbers (item numbers 62601-1SW to -3SW).



EMD SW1000 der D&RGW (Art.-Nr. 62601-1). Foto: AZL / Ztrack.

The EMD E7 is currently on offer in A-B units of the Southern Pacific in Daylight livery (64602-1 / -2). As EMD E8 of the same railway administration it wears at the same time the livery "Bloody Nose" (62616-1 / -2). The already delivered passenger coaches in Loewy design of cult status are now followed by matching EMD F7 A and B of the Northern Pacific as a double pack (63004-3), supplemented by another A unit (63004-4).

Two passenger car sets each for the Southern Pacific are available for the "Cascade" and "Lark" trains (73X46-CascadeA / -CascadeB / 73X46-LarkA / -LarkB). The orange painted R-70-20 refrigerator cars with Tropicana inscriptions are available individually (914812-1) and as double (914842-1) and quad packs (904812-1). Similarly configured are the black 8,000-gallon "Phillips 66" tank cars (915005-1 / 915035-1 / 905005-1).

Manufacturer photos of the current deliveries can be found at) <https://www.americanzline.com>.

Faller's contribution to the Christmas business:

The Christmas business was already drawing to a close, but Faller knew how to get in on the act with a new products: The new polystyrene kit "Post Office" (art. no. 282787), which had only been introduced as an autumn new product, arrived at the dealers in good time.



R-70-20 refrigerated truck with Tropicana inscription (914812-1).
Photo: AZL / Ztrack

219 parts in 4 colours, plus window foil, curtain mask and building instructions invite model railway enthusiasts to tinker over the holidays. By the way, the building is already known to many years of Zetties in other basic colours.



The new post office (item no. 282787) based on the Güglingen station has already been delivered. Photo: Faller

Its basic forms go back to the Güglingen railway station, which has been modified in the meantime; it was already offered once in a configuration of the same form as the town hall. Its true origin can be guessed at the rear by three doors placed next to each other.

Ongoing projects at NoBa-Modelle:

Before we announce the current deliveries, we would like to point out an ongoing project that will bring an exciting spring new product for friends of epochs V and VI. NoBa-Modelle has discovered one of the larger market gaps for itself, and is already on the home stretch to close it.

In order to slowly build up the suspense and keep it going until late January (announcements on our website) or even the February issue, we will not reveal anything at this point and will observe the requested silence.

In the meantime, the ETA 1505 battery railcar (later class 515) was revised. It is now driven by two Rokuhan Shorty bogies and all housings receive pressed-in threaded bushings for easy and safe mounting of the bogies as well as a magnetic coupling.



Stadler's Regio Shuttle RS1 (item no. 5216R; photo above) is now also available as a drivable Z-gauge model from NoBa-Modelle. In addition, there are two new cars: the MAN 8x4 tow truck (6388R; photo below, right) and the Büssing President (6387R; photo below, left). Photos: NoBa-Modelle

Since the end of November, i.e., shortly after the editorial deadline of the last issue, the Stadler Regio Shuttle RS 1 (art. no. 5216R) is another new Epoch VI model in the range. The one-piece railcar, which is used by many private operators, as well as DB Regio, is one of the frequently requested models for modern local transport.

Furthermore, the MAN 8x4 tow truck (6388R) is now in the range, and the Büssing President (6387R) is a historic bus. These new products and all other offers can be found on the Internet at <https://www.noba-modelle.de>.

Rather lull in Märklin deliveries:

The following sentence from a Märklin dealer information accurately describes a dilemma that many manufacturers currently have to deal with: “Unfortunately, due to the increasingly worsening situation on the procurement markets for electronic components, we are forced to postpone some new products planned before Christmas and expected by you to the first quarter of 2022.”

In gauge, too, quite a few models that were still scheduled for the Christmas season are affected, including the insider model 2021 of the double diesel locomotive V 188, but also the train set “Deutsche Weinstraße”. Updated delivery date lists are now available from the Märklin pages.

Our list of what still made it to the dealers from Göppingen is correspondingly short. A very small subset of the crimson class 218 mainline diesel locomotive (item no. 88792), which was announced together with a silver train set including a new “bunny box”, is among them.



The situation is somewhat better with the sliding roof / sliding wall wagon Tbes-t-68 (82153), whose housing has been newly constructed for an existing chassis.

This wagon will be delivered in a double pack for late epoch III and will follow four times for epoch IV.

This delivery will be examined in detail and with a view to the new releases of the year 2021 in the January issue 2022. In this context we will also briefly present the prototypes.



Class 218 diesel locomotive (88792; photo above) and sliding roof/sliding wall car Tbes-t-68 (82153; photo below).

Herpa changes to new products:

For some years now, Herpa has announced new products four months in advance, but experience has shown that the actual delivery dates have usually been well outside this time window; anyway. Now this manufacturer has reacted and changed its approach with reference to delays in the supply chains across the industry.

Since it is increasingly difficult to date delivery dates exactly, new products will now be announced close to the time of delivery. The idea is to announce one month in advance. For this reason, the aircraft models for late spring 2022, which some readers are surely expecting, are now missing from this issue.

However, we will certainly learn something or other about planned new moulds for 2022 at the Spielwarenmesse (International Toy Fair) and be able to pass it on in the magazine.

Magazine recommendation for Zetties:

A very worthwhile, because lovingly written portrait of the layout "Wiesenthal", which we awarded as layout of the year in 2021, is contained in the current Modelleisenbahner 1/2022.

Editor Andreas Bauer-Portner, who has a passion for especially small model railways because of his passion for N gauge anyway, also knew how to put this especially neat showpiece in the right light.

The author's enthusiasm is clearly noticeable, because he has managed to capture the special features and historical conditions that were implemented there very accurately in conversation with builder Wilfried Pflugbeil and transport them to the reader.

Our recommendation to buy the book could not be clearer. The leading trade magazine in the print sector can be found in model railway shops, but also in publishing houses and in well-run station bookshops.



Abbildung: VGB | Geramond

Ladegut Küpper suitable for the season:

Spur Z Ladegut Josephine Küpper from Aachen currently has beautifully glittering stardust to offer as Christmas-setting inlays for various freight cars from Märklin. The open wagons of the type Omm 52 (Märklin 8622) and Eaos 106 (Märklin 8650) are covered, other models were also supplied in the same way earlier.

Incidentally, anyone who wants to get a personal impression of the effect can do so in Dortmund in the shop window of the Lamers Gallery at Kleppingstraße 8: The low-sided wagon behind the steam locomotive of the Christmas Express, through which the world's smallest Christmas tree is framed there, is also effectively loaded in this way!

Level crossing post from Archistories:

The "barrier post 255" (item no. 106211) produced for the 1zu220-Shop and matching the style of their own building series is about to be delivered. The prototype near Grottenberg in Sauerland was on the line between Messinghausen and Beringhausen.

Archistories has developed a highly detailed kit based on this picturesque model, despite its small size, which can be realistically built 10 mm deep into the railway embankment. The front and the steel staircase on the side are at the level of the track, while the coal cellar at the rear can be reached at ground level.



The barrier post 255, produced by Archistories (art. no. 106211) exclusively for the 1zu220-Shop, will soon be ready for delivery. Photo: Dirk Kuhlmann

With the characteristic slate cladding of the upper floor, the barrier post 255 complements prototypically especially the related station 'Westheim', which also belongs to the exclusive series of the 1zu220-shop.

Micro-Trains also continues to deliver:

MTL has added six more examples to the 61-foot flat cars with fixed end walls reported in the November issue: Two each will pull up for Illinois Central Gulf (Item Nos. 527 00 211 / -212), the NOKL (527 00 221 / -222) and the Union Pacific (527 00 231 / -232). The Sweet Liquid series continues with wagon number 10 (530 00 570), which was leased in bright red livery from adjuster GATX to Union Starch.

Micro-Trains products are sold, among others, by Case-Hobbies (<http://case-hobbies.de>).

Märklin Museum Car 2022:

As we went to press, we also received information about the new Märklin museum car 2022 (art. no. 80033), which can only be purchased at the Märklineum. The prototype this time is a type Ootz 43 self-unloading wagon, which was designed for the Göppingen public utility company (SWG) and is labelled in such a way that it would have been used by the German Federal Railway. The model has a load insert of real coal and the advertising lettering "Göppingen Brechkoks".

New products at EtchIT-Modellbau:

We are pleased to finally be able to present a new model from EtchIT-Modellbau, because these vehicles are certainly among the best that Z gauge currently has to offer. Under the item number XD080_Z, a Fleischer S2 bus is now available as a complete kit. It can be ordered at <http://www.etchit.com>.



The Fleischer S2 bus (art. no. XD080_Z) is published by EtchIT as a complete kit and consequently also includes parts for the glazing, trim elements for the sides of the body and wheel trims with chrome effect. Photo: EtchIT-Modellbau

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