

**Trainini**

**Full Issue**

**2023**



*International Edition*

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for railroad enthusiasts  
in the scale 1:220  
and Prototype

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# Trainini

German Magazine for Z Gauge



**Build a Signal Box by yourself**

**Japanese Classic  
Best New Releases of the Year**



## Introduction

Dear Readers,

before we know it, yet another new year has begun. Time appears to fly as our jubilee year is now already over. These days, we are finding out about the spring new products of the various manufacturers, which we will present in detail in the next edition.

Today, it is time to look back and review the past twelve months. What was delivered during this period? How did we like it? What particularly impressed us and filled large gaps?

These and even more questions are what keep us editors busy immediately after the turn of the year. We have to make sure that we haven't forgotten anyone and exchange our views on our impressions. At the end, we vote on which product, which services, and which manufacturers will be awarded.

In the January edition you will traditionally find the results of this process. Be curious to find out how we have decided and what the reasons were that led us to do so. We very much hope that our readers agree with our results.

A retrospection of last year is also the event report by Markus Gaa, who reports on a privately organised exhibition in Marktredwitz. The USA layout on display there was truly gigantic in size and therefore powerfully fascinated the visitors because of its size alone.

But, we have compiled even more in this edition: Oleksiy Mark introduces us to a new Rokuhan model and classifies it appropriately in our scale across manufacturers. In a short article we also present an airplane miniature, whose prototype was indeed a milestone.

Just for this we built a new diorama and used techniques that we have not yet shown in the magazine. The construction report on this will be reserved for a special issue, which will be published later this year as our number 1.

A sequel will be published in January by Jochen Brüggemann. His self-constructed building was very well received by our readers. There were questions about the film plates he used to make the windowpanes.

Not everyone is still familiar with this relic of the analogue age, and so he also gave us a digression on window construction to answer the questions that came in.

We have also included two literature recommendations for you: The book to be presented deals with excellently illustrated layout proposals. The style of drawing will certainly be familiar to many.

A film portrait of the E 69 series will certainly remind many of the model from ZModellbau, which is no longer available. Anyone who owns such a locomotive will be pleased to see the five prototypes in the film and to experience three of them on many train rides. I hope you are now curious enough - I hope you enjoy reading this edition!

Sin-Z-erely,

Holger Späing



**Holger Späing**  
Editor-in-chief



Our editorial awards 2022

## Best New Releases of the Year 2022

*As before, the number of models delivered in the third year is lower than we were once used to. And not every new model has the potential to be nominated by us for an award. Only the best of the best should compete, which promises to be exciting for our readers and a difficult task for us. Now, we will share which innovations and achievements received the highest recognition from us.*

Let me start by saying this right away: New products that have not been or will not be nominated here are not necessarily bad. Experience has shown that not every new design that is produced and launched on the market in time can be considered here.

This may be due to the fact that, especially in the case of small series, no model was available to us for personal examination, exhibition, and evaluation. Since we want to honour the performance rendered and not an idea, illustration, claim or brochure reproduction, this is inevitably an exclusion criterion.



The three editors (from left to right: Joachim Ritter, Holger Späing, and Dirk Kuhlmann) are also the jurors for our magazine's awards. Every year, after the process of deliberation, they have a difficult decision to make.

A member of the editorial team or a neutral tester commissioned by us must be able to hold the object of desire in his hands in order to also be able to discover and evaluate deviations, faults or weaknesses.

Another point is that, in addition to the quality delivered, the proximity to the prototype and the market significance are also evaluated. Thus, double developments or competing models of the same templates

will have to convince considerably more in the quality and implementation criteria, as they can no longer close a noticeable gap in the market.

The purchase price alone, however, is not decisive for us. We deliberately put what the customer has to spend into the context of the manufacturer's possibilities. These are determined by production techniques, technical feasibility, and the expected number of units, which also has a significant influence on the price structure.

In this way, the many criteria we include in our discussions and decisions allow us to put large- and small-series implementations on an equal footing, as well as equal opportunities when it comes to awarding votes.

And so we would now like to let you know which models and achievements were particularly outstanding for us in the past year 2022 and were able to snatch one of the promotionally effective awards.

## Locomotives Category

In the category for locomotive models, three models are competing at once, including two from Märklin. The latest candidate is Märklin's Insider Model 2021 (art. no. 88150), which follows seamlessly in the footsteps of its recent predecessors: announced at the end of the previous year for which it is intended, but only on its way to customers well after its expiry.

But the long wait was worth it, as our test showed. It was a similar story for the Siemens Vectron (88231), for which buyers also had to be very patient. It is an important electric locomotive model for depicting the present and has been realised very consistently and absolutely correctly in the area of running gear and housing.

There is little hope for a large-scale production model of the SBB's RAM TEE 502 diesel multiple unit: it seems to be too special, as its prototype hardly played a role in the core Z Gauge market in Germany and only operated there for a short time on a single route.



**Weighing up all the criteria to be considered, Märklin's V 188 001 a/b (item no. 88150) emerged victorious in the locomotive category.**





But Swiss model railway enthusiasts have a very close relationship with him, and he also knows how to inspire people in the Netherlands. In addition, this model was available for the test in a digital version with a sound decoder for the DCC protocol (5214RF). So NoBa-Modelle also gained a claim to the title in this category.

The editors' decision was made after extensive deliberation and voting: They found the TEE multiple unit too special to be awarded the title in this competition, while the Vectron failed due to its clumsy single-arm pantographs, four of which “adorn” its roof.

A new design without any gain in filigree was not necessary from the jurors' point of view, and so the unanimous choice fell on the double diesel locomotive of the class V 188 (88150). Dirk Kuhlmann aptly summed up the partly polarising effect of this prototype: “The prototype (and thus also the model) are more than ugly in my eyes, but it fills gaps in the range and will find its fans.”

Joachim Ritter added to this harsh-sounding verdict with the impressions that are still missing here: “The V 188 is

a great locomotive, something special in style and implementation. The model is truly a heavyweight for the anniversary year, because it really pulls off something. For friends of Era III it is perfect and gives a lot of pleasure in this realisation.”

In fact, the V 188 does not offer much potential for optimisation, Märklin has exhausted what is optically and technically possible today. And so it wins the category locomotives as new release of the year 2022.

## Wagon Category

In the case of the wagons, only two models are competing against each other, and both of them are classified as freight transport. This also shows a dilemma, because here the choice has always been severely limited. Although a new passenger coach has finally been announced in the form of the “Hasenkasten,” it too has been massively affected by delivery delays.

Therefore, the SNCF's G 40.2 boxcar (W02-ST) from Azar Models and the DB's Pwgs 41 goods train accompanying car from Märklin (86070) are competing for the title. Both are accurately and convincingly realised. Also, in their other qualities they leave nothing to be desired or even reveal weaknesses.

The SNCF is seldom covered with models, which can be seen as a gap, especially in freight traffic. But it is not too big, because especially covered freight wagons have received the Zetties, in abundance, in recent years, while other wagon types were sometimes served much worse.





The new Pwgs 41 from Märklin (from the 86070 pack) is successful and ground-breaking. It will certainly accompany us for many years in the programme and there will be a lot to write about it.

That's why it wasn't quite enough for the title. Here, too, the decision was unanimous after consultation and discussion: Märklin gets the title of the new release of the year 2022 for the Pwgs 41 of the DB (86070), because this wagon was overdue for years and could be seen in many trains. It will now soon do the same as a model.

Joachim Ritter and Holger Späing emphasise how much it has been longed for to finally - after now fifty years - succeed its aged Prussian predecessor also in the model. Joachim Ritter adds on a very personal note: "We can also praise Märklin that such important models are finally being realised. Our patience has been rewarded, even if it has been strained for a long time."

There is nothing more to add to that, is there?

## Accessories Category

In this category, too, there are only two candidates for the past year. The car ferry from Artitec (322.034) was one of the new products that we missed in the previous year and that we were immediately enthusiastic about. That's why we are nominating it here – a more detailed portrait is planned and can only be accommodated in the year that has just started.

The etched and coloured brake discs for the Vectron (ZM-MD-006) are Zmodell parts for the Märklin model, which bring it even closer to the prototype. However, the editors also had a hard time with this, because the small series manufacturer, originally from Ukraine, is currently still facing a new start, for which many things still need to be clarified.



Artitec was therefore also able to get a unanimous vote in the accessories category: "It is a very special model, but it will probably do well, at least according to Dirk Kuhlmann. He goes on to say: "Now the Z-gauge railwayman must really get to grips with the construction of river landscapes!"



The car ferry from Artitec (322.034) is not just another model ship: it occupies a new segment within water vehicles and also offers great opportunities, for example, to effectively display the most beautiful car models in one's own collection.

This very clearly highlights the once again outstanding quality of Artitec's new products. Joachim Ritter adds the following statement to his voice: "Artitec can simply make wonderful models, and they show it again and again. The realisation is excellent, as is actually always the case, and it is a pleasure."

All three agree that the ferry is of course very special, but that it is now even more worthwhile to build motifs to go with it. Because with it, scenes can finally be recreated that simply could not be equipped appropriately before. Quality like this convinces us.

## Technology Category

In the technology category we first nominate the Ardelt steam crane 57 to. (5318R) from NoBa-Modelle. We deliberately chose the kit, which offers the buyer a lot of tinkering fun and allows various assembly options for the finished model. That's why this car duo is better off here.

Among other things, it will compete against the model speedometer from Halling Modelle (MTS-100), which proved to be a very helpful tool in the test, and not only for us. We also received similar feedback from our readers, which confirmed our nomination.

The individual multiple booms (without item number) from HOS Modellbahntechnik also close gaps in the range, with which the overhead line equipment of stations and line junctions certainly does not look overloaded. The special feature is their dimensionally adapted design, which does not force the tette to standard track distances and radii.



The Ardelt steam crane 57 to. (5318R) from Noba-Modelle, shown here assembled and ready painted, proves to be an excellent eye-catcher on the layout and also inexpensive. It can be used in working as well as transport position. Photo: NoBa-Modelle

However, the steam crane was unanimously the winner. The editors here were unanimous in saying that in terms of type, size and conception it is no match for the lightweight Märklin switch crane, where the mounted crank also massively detracts from the appearance.

The range of larger rail cranes was rather thin before and it also remains a challenge to make a model filigree and appealing, to keep it transportable and yet not lose sight of the function.

The duo from NoBa-Modelle has managed exactly this balancing act in an impressive way. That's why we are currently preparing a feature on this model for the magazine and also for **Trainini TV**, in order to convey this to the readership in a vivid way.

Joachim Ritter could hardly put his enthusiasm into words: "I like to reveal myself here as a friend of railway service vehicles. This realisation of the 57 t crane arouses great desires. It's great what the Zetties are offered here - this steam crane in a high-quality and even affordable version is unique."

## Literature Category

There were also many problems to solve on the paper market. For many publishers, sufficient high-quality raw materials were not available or not available in time, which is why the publication date of many titles had to be postponed.

Nevertheless, we have reviewed some good books in the past year, but only two recommended themselves for an editorial title. These are "Faszinierende frühe Bundesbahn" by VGB / Geramond (ISBN 978-3-96807-999-8) and "Baureihe 120" by Transpress (978-3-613-71616-2).

Both are highly different and yet unique in their field. This makes it difficult to compare them and decide who deserves the title in this category. In the end, the editors came to the view that the Palm recordings are still something special, but to a large extent also published and well-known.







The need for a “compilation” of the best photos of his work is therefore probably less than that for an affordable title on one of the most important electric locomotive designs of the last hundred year.

The 120 series marked a historic turning point in propulsion technology, found Dirk Kuhlmann. Joachim Ritter emphasised the favourable price and the pleasure that this book gave and still gives.

Editor-in-chief Holger Späing only had one more point to add: Here, exactly the middle course has been found, not to overtax a large part of people with extremely technical-heavy explanations or, as a consequence of a lack of ambition, to prescribe them to an equally large part of railway enthusiasts.

Only the chosen way and scope could ensure to address all target groups equally and to establish a standard work here that also promises sustainable sales success. This book is, therefore, quite rightly awarded the title in the literature category.

#### Category Exhibits

In the meantime, we look around every year at exhibitions and trade fairs, where we follow various exhibits under construction or enjoy premieres. This was also the case with the layout that will receive our award for the most outstanding exhibit this year.

Dirk Kuhlmann had not only nominated it this year, but had drawn attention to it early on, stating that something really big was coming our way here. He had been quietly following its development since 2017 in a Belgian model railway forum.

In 2022, the time had come: Pieter Willems and Jan van Casteren presented “Boischot Statie”, realised as a stage in a peepshow, at the Gauge Z weekend in Altenbeken. His impression was shared by the two editorial colleagues.

There are many reasons for awarding this masterpiece. A weighty one compared to comparable pieces of similarly high quality is that a lot of the work was done by hand. Instead of buying ready-made kits and putting them together, almost everything is based on complete self-construction.

This gives an exhibit its unmistakable face and a recognition value that should not be underestimated. In the Benelux countries, this has a great tradition and clearly stands out from the “German monotony” that is often seen.



Layouts based on Belgian motifs are rare when it comes to the 1:220 scale. In “Booischoot Statie” by Pieter Willems and Jan van Casteren, the unusual motif is combined with lively design and perfect craftsmanship.

“Simply beautiful and deservedly chosen”, is Joachim Ritter's short conclusion, which the other two editors also agreed with. With the editorial award, we want to draw attention to the work done by the Belgian duo and encourage our readers to be equally creative.

## Complete Works Category

As a body of work, we always award special achievements that work in their sum and of which a single one would probably not have won a title. It does not matter whether a person or a manufacturer has already won an award here or in another category.

In the 50th year of the Mini-Club's existence, there was a unanimous vote for this honorary award that did not require much discussion. The motives, motives and impressions of the three jurors were homogeneous. For “50 years of Z gauge” Märklin alone deserves our award.

**Photo right:**  
When Märklin went on the market with Z gauge in 1972, the manufacturer had to act as a full-range supplier. This early starter set with a train, rails, transformer and accessory kits is impressive proof of that. 50 years of consistent continuation are worth an award to us today.





We have deliberately put the size and not the product name in front. This model railway scale was invented and introduced to the market in Göppingen. With it, a seed was sown that could only flourish because it was continuously nurtured and also further developed.



It is only thanks to seamless operation that it has truly developed to this day and has therefore long since become more than "just" the Mini-Club. Especially in the last few years since our last award, Märklin has achieved a lot and with impressive consistency. Proverbially, silver is now followed by gold.

Joachim Ritter aptly summarises the motives that have guided us: "Märklin launched Z gauge and has always stood by it for 50 years. Innovations and low points, that's life, but the mother has never abandoned her child. With always great models, she rekindles the fire of passion."

Despite all the criticism that must and may be voiced from time to time, these are merits that must be shown just as clearly. We are pleased and happy to award this distinction to.

Of course, we congratulate all the winners of our categories on their achievements and also encourage the losers not to slacken their efforts. Because whoever made it into this article is not a loser. Here is one more time the final out of twelve preliminary rounds is played out.

In the end, only one can make it to the podium, but still only a few make it to the last round. And we are sure that our readers appreciate this and reward it with a purchase on the market.

After all, what counts for a tradesman in the end is the turnover, not the applause that artists otherwise publicly ask for, but from which they ultimately could not live either.

**Webpages of our winners**  
<https://artitec.nl>  
<https://www.maerklin.de>  
<https://www.motorbuch.de>  
<https://www.noba-modelle.de>

**Here you will find Trainini TV:**  
<https://www.youtube.com/TraininiTV>

## JNR electric locomotive EF58 from Rokuhan **Classic of the Japanese Railways**

*The JNR's EF58 electric locomotive has made history. It pulled (not only) the imperial train and is a railway history celebrity in the Land of the Rising Sun. Rokuhan has now also produced it as a Z gauge model. Today, it has been examined under the critical eye of our translator Oleksiy Mark.*

By Oleksiy Mark. JNR's EF 58, a legendary electric locomotive from the old days of Japanese railways is a must in the collection of anyone interested in Japanese railways.

For those interested in the history and background of this locomotive, we have provided a source of information at the end of the article. The first version announced by Rokuhan and now delivered comes in dark brown colour (item no. T039-1), the standard livery for Japanese locomotives from this historical period.



The EF58 61 preserved in the original colour of the JNR was the model for the Rokuhan model (23 August 2008). Photo: Lover of Romance (CC-BY-SA-3.0-migrated)



The matching set of passenger coaches can be found under item numbers T036-1 or T036-2 as the Emperor's Train in the range. Of course, the EF 58 locomotive is not only intended for use with these cars. It is expected that other colour variants will follow in the future, such as dark blue/beige and green/yellow.



EF58 61 was regularly used in front of the imperial train, as here on the Omeshi-Ryomo line on 24 October 1986 during a state visit from Belgium. Photo: spaceaero2 (CC-BY-SA-3.0)

A long time ago I found a simple stand model of this locomotive in green/yellow colour scheme (similar to the models of Atlas Minitrains and Hallmark Lionel Train Christmas Ornaments) on the net. Unfortunately, I don't remember the manufacturer and the source is hard to find today.

A long time ago I found a simple stand model of this locomotive in green/yellow colour scheme (similar to the models of Atlas Minitrains and Hallmark Lionel Train Christmas Ornaments) on the net. Unfortunately, I don't remember the manufacturer, and the source is hard to find today.

Now it doesn't make sense to build this model anymore, as there is a complete locomotive available for purchase. The model comes in the standard plastic box from Rokuhan with a detailed description, and with windows showing the locomotive and accessories.



In a small plastic bag, accessory parts such as whistles, GPS antennas, lightning rods, and prototypical couplers are included. They are made of coloured polycarbonate plastic, a very flexible material that is almost impossible to break.



The model comes with several plastic parts, a metal part (flag holder; bottom right) and decals with replicas of the flags of Japan, Belgium, the Netherlands and Norway.

There are also very finely printed name plates on silver-coloured plastic. In a small compartment there is also a tiny photo-etched metal flag holder that can be mounted in front of the driver's cab and can hold two flags. The corresponding flags (including those of Belgium, the Netherlands, and Norway) can be found together with instructions under the plastic insert.

### New products under the magnifying glass

The locomotive is very nicely designed; the bogies with their many details are particularly well done. It is very good that the bogies are implemented open-worked, and all individual components are, therefore, reproduced very accurately. We are not familiar with this kind of bogie detailing from most Märklin models.



The very finely designed bogies are the outstanding feature of the EF58 conversion by Rokuhon. They transfer the look of the prototype very well to the new model.



Rokuhan has also given the bogies more mobility than Märklin did with those of the US GG-1, which has an identical wheel formula. The two-axle front bogies of the EF 58 are connected to the middle, three-axle bogies via two articulation points – the GG-1 has only one at this point. In addition, the front bogies are easily removable. For even greater flexibility, the couplings are located in the middle of the front bogie, not on the edge.

Obviously, Rokuhan chose this design to ensure smooth and faultless running of such a long locomotive over switches and curves with small radii. For some unknown reason, the couplings on this model have a different grey colour.

As a standard in Japanese Z gauge, Rokuhan uses scaled-down Arnold couplers for all its locomotives and wagons except for the Shinkansen trains. Märklin couplings are available as exchange parts (short version: Noch 7297421 / Rokuhan A021; long: Noch 7297422 / Rokuhan A022) and are not included.



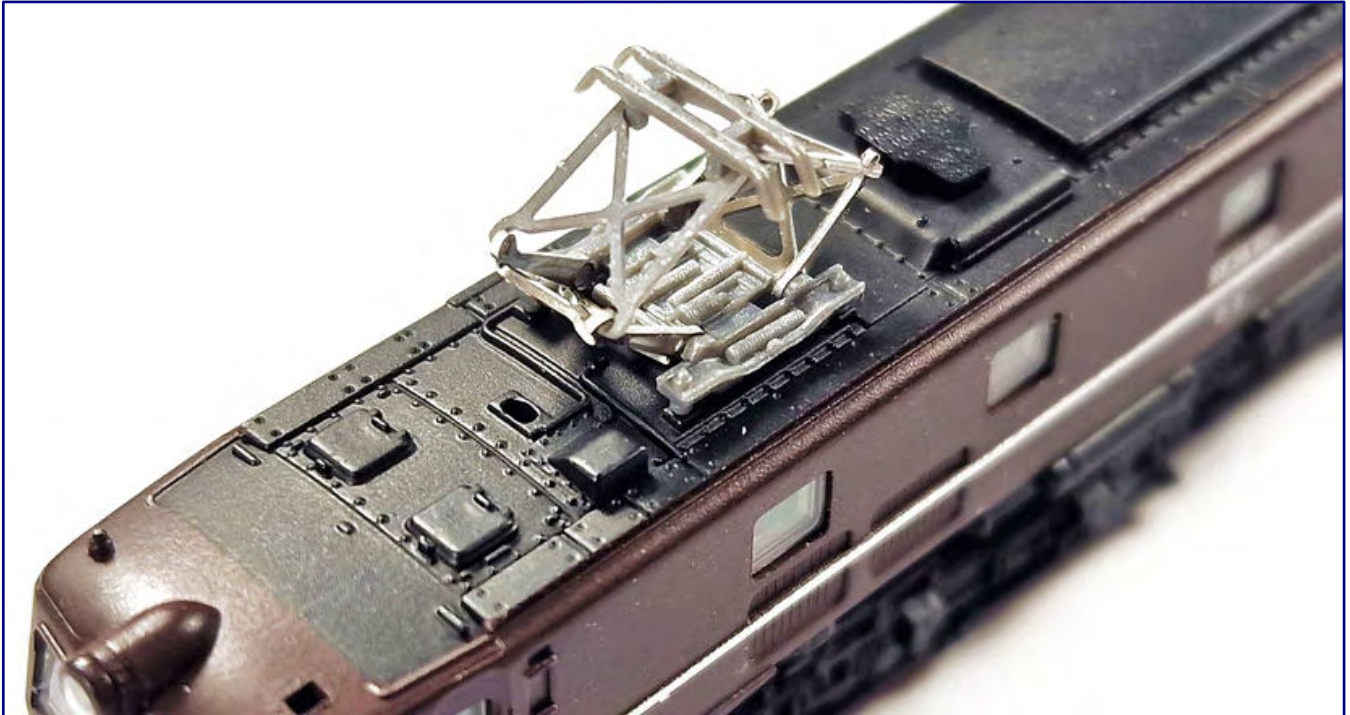
The usual standard in Japan are couplings that follow those of Arnold for N gauge. Interchangeable couplings for the Märklin system are not included with the model, but can be purchased separately.

The wheel sets of the inner bogies are arranged asymmetrically. Nevertheless, all six are driven. Two wheel sets are additionally fitted with diagonally offset traction tyres on one wheel on each side of the bogie.



The tampon prints on the locomotive body are very fine. They are applied with colours in gold, silver, white, and black. The macro photos were taken with the help of a digital microscope at 1,200x magnification.

Let's now take a look at the scissor pantographs. They are made of metal photo-etched parts, while their bases and the contact strips are made of metallic grey plastic. Their colour corresponds to the prototype, but, unfortunately, they are not sufficiently filigree.



The scissor pantographs look quite rough and do not please me, after Rokuhan had previously delivered very filigree parts that looked very close to prototype.

This is a small point disappointment for me. Maybe the ones used so far tended to break and increased the number of complaints? In any case, I liked the filigree Rokuhan pantographs because of their prototypical appearance.

As far as the usual mass of Rokuhan's rolling stock is concerned, this model is very heavy - it weighs a whole 33.6 grams. That is significantly more than the EF210 (T018) electric locomotive, which weighs only 26 grams.

What does a look inside reveal? Almost all of the space is taken up by the metal casing - that's a good point. However, there is an obvious downside to this fact - there is almost no space for a digital decoder. We only have about 0.5 mm free height between chassis and roof.

The tail lights are served by a light guide insert made of red-transparent plastic, which is obviously intended as a light filter. However, there is no red LED on the board, although there would be enough space for it there. Of course, end signals are not absolutely necessary, because most of the time the locomotive runs in a train formation, but the complete lack of them can't be an advantage anyway.

By the way, this loco, like almost all other Rokuhan models, got yellow LEDs for the headlights. I don't know why Rokuhan doesn't keep up with the times and use warm white ones - in any case, I replaced them immediately in my model.

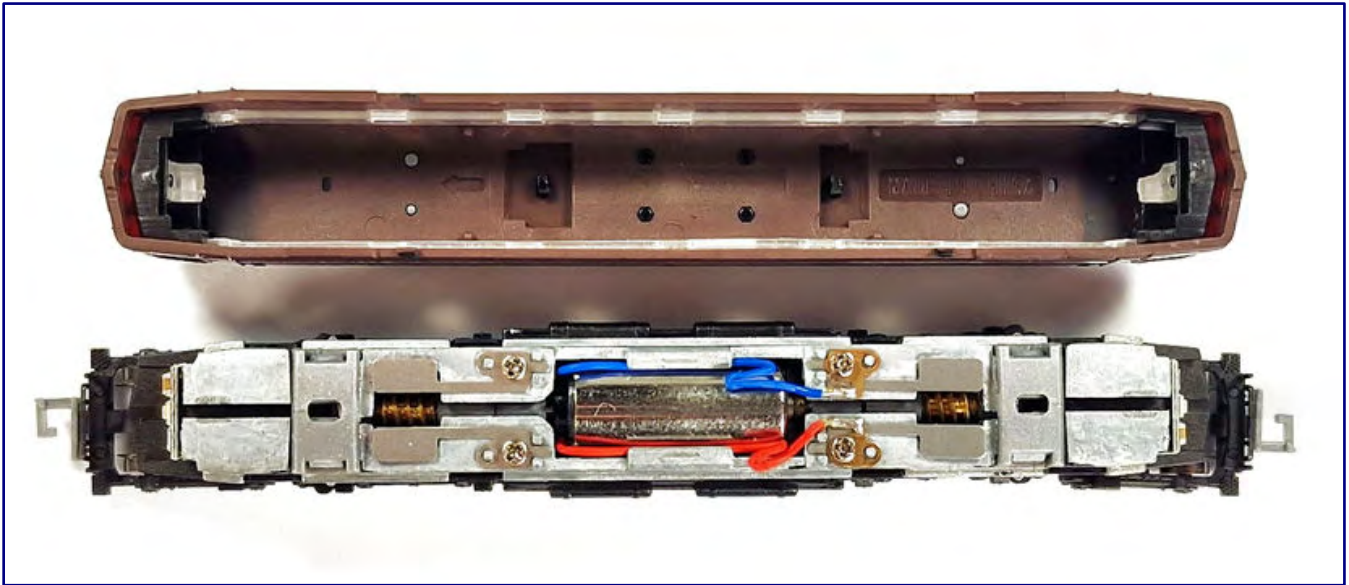


Photo above:

The view inside reveals the small space under the housing. There is hardly any space left for the installation of a digital decoder.

Photo below:

As the photo on the left shows, Rokuhan had red-transparent light guides installed in the tail lights. However, no LED was provided for this on the circuit board (photo, below right). The headlight is also illuminated in yellow, which is absolutely out of date.

## Scale and proportions

To clarify the size of the heavy locomotive, comparative photos may serve. It is clearly smaller than the GG-1 from the USA; the comparison with the German class 103 illustrates it for most readers, as well.

Let us talk a little about dimensions, scale and gauge in relation to the Japanese models. The scale and dimensions of the Japanese models, whose prototypes run off the standard-gauge Shinkansen lines, are always a compromise.

The Japanese railways there run on narrow gauge with 1,067 mm, the so-called Cape gauge. On a scale of 1:220, this would correspond to a gauge of 4.9 mm instead of the usual 6.5 mm. That is why the loading gauge is also different: it is smaller than the European and American dimensions. Only the Shinkansen run on the standard gauge of 1,435 mm and use a significantly larger clearance gauge.





The comparison with the (repainted) GG-1 from Märklin, which has the same axle arrangement, is intended to clarify the size ratios and proportions of the EF58 running on narrow gauge in the prototype.



For our readers in the core market of the Z gauge, the size comparison with the with the German class 103 will be more comprehensible.

In order to reproduce the prototypical dimensions of tracks and rolling stock, the manufacturers would have to use a different gauge for our scale. This would probably bring chaos, not only in our scale.

In all nominal sizes, they therefore do it this way, using the usual standard gauges and changing the dimensions.

Thus, H0 size does not correspond to 1:87 but to about 1:80, N gauge uses 1:150 instead of 1:160.

At the same time, the regular scale conversions are used for the Shinkansen trains. This compromise simplifies many things, as only one track system is required for all trains and can be used uniformly.

With these thoughts, let's return to Z gauge. Unfortunately, there is no uniform result here. The length of the EF58 without couplings is 94.5 mm, which corresponds exactly to the length of the prototype when converted to the 1:220 scale (19,900 mm). So, we have a correct length dimension here.

But, the track gauge is 6.5 mm, which does not correspond to the converted body gauge! As a result, the body and running gear of the locomotive have been converted in a different scale. The following facts illustrate the dilemma: In order to maintain all proportions and at the same time keep the gauge of 6.5 mm, this locomotive would have had to be exactly 121 mm long. With a scale of 1:164, this would correspond almost exactly to N scale!

And so, it is now time to summarise the strengths and weaknesses of the Rokuhan new products and to draw a conclusion. This model is a very welcome addition to the Rokuhan range based on Japanese models.



Despite some weaknesses, the new products from Rokuhan are characterised by a high level of detail and many filigree features.

Its pattern is quite striking, as it features a wheel formula that is not commonplace. So, it was rather a matter of time when an EF58 would appear from the most important Z gauge supplier from Japan.

It also shows a high attention to detail and filigree features. Thus, it comes very close to its prototype, even taking into account all the limits of our scale. But the devil is usually in the detail: The missing option to digitise this locomotive will be missed by some customers, especially since Rokuhan has its own decoder boards in its programme!

The missing tail lights are very regrettable, because there is no apparent technical reason for this. Yellow as a light colour is also completely out-of-date. The bulky pantographs are understandable, although they detract from the overall impression of the model.



In the model, EF58 61 can now also be used to haul the imperial train, which obviously fascinates Japanese model railway enthusiasts, in particular.

And so, despite the listed weak points, the EF58 is a must for collectors of Japanese rail vehicles, because quality and other details speak for themselves. And, it is one of the most famous locomotives in the history of Japanese railways.

All unmarked photos in this article: Oleksiy Mark

**Base model manufacturer:**

<https://www.rokuhan.com>

**Webpages about the prototype and its history:**

[https://en.wikipedia.org/wiki/JNR\\_Class\\_EF\\_58](https://en.wikipedia.org/wiki/JNR_Class_EF_58)



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## Märklin Neuheiten 2023



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## Vickers Viscount 814 of Lufthansa **A Milestone of Aviation**

*After presenting the Vickers Viscount as a 700 version model, focusing on the seemingly mismatched type to its owner, today we look at the extended and reinforced 810 version. Herpa recently delivered this aircraft, which made history, as an example to Lufthansa.*

In **Trainini®** 9/2020 we already gave a brief history of the Vickers Viscount. It referred primarily to the most frequently built type 700, which is why we will focus here on the differences and features of the types 800 and 810.



The Vickers Viscount 814 of Lufthansa from Herpa (item no. 572255) has landed and deserves our attention today, because it is one of the models that can definitely find a place on a gauge Z layout.

As already mentioned, the Vickers Viscount was an important commercial aircraft built by British Vickers-Armstrongs (Aircraft) Ltd. This aircraft was built between 1948 and 1964. Its low-vibration turboprop engine was revolutionary and ushered in a new era in aviation.

A total of 443 aircraft of this type for medium and short-haul routes were delivered to more than 50 owners worldwide, a large part of them to Canada and the USA. The two large versions, the 800 and 810, were stretched by 1.20 metres and structurally reinforced. Thus, they offered space for up to 75 passengers. In contrast to the 800, the 810 also had a weather radar in the bow.

The more powerful engines compared to the 700 version enabled better performance data, such as a cruising speed that was about 60 km/h higher. Further changes to the fuselage and engines were not made, however, and led instead to the successor model Vickers Vanguard.

The German Lufthansa also used a total of eleven aircraft of the 810 series, referred to here as 814 and delivered from 1958. They were procured in order not to fall behind technologically after the first jet aircraft





The aircraft type designation is proudly emblazoned on the rear part of the fuselage. The shapes of the tailplane and rudder reveal the era from which the Vickers Viscount 814 originates: When it received Lufthansa's newly introduced livery, it was already on the verge of being flown out of service. Photo: Oleksiy Mark

had been ordered in 1956, but these were still considered too expensive to operate on short and medium-haul routes.

Some of them were also leased to Condor Flugdienst and enjoyed accident-free service. But as early as March 1971, the last one was retired from Lufthansa's fleet. But that was still enough to be able to experience the "fried egg" livery introduced in 1968 with the Boeing 737 on some examples.

The current Herpa new products (art. no. 572255) with the registration D-ANAC has such a prototype. This example was taken into service by Lufthansa in July 1961 as the penultimate "Pfeif-Kaunt" (approximately "Pipe Count") - a nickname assigned because of the turboprop noise.

We were able to identify a total of three Lufthansa specimens as preserved aircraft: The flying exhibition L. + P. Junior in Hermeskeil owns the aircraft with the registration D-ANAM. The former D-ANAB without engines and propeller has been converted into a restaurant and can be seen in Hanover-Ricklingen.

Because it had been used as a training object for Lufthansa Technik training in Frankfurt (Main) since 1972, the D-ANAF did not go the way of old iron either. On 18 September 2012, it was dismantled and taken to the Technical Museum in Speyer, where it has been on display since spring 2013.

## The model from Herpa

Like all Herpa aircraft models that we present here, this time it is also one in 1:200 scale. For the viewer this is not disturbing or noticeable, as even at airports no suitable objects of comparison can be found that give away the slightly different scale.





The aircraft with the registration D-ANAC was one of the last examples of its type in Lufthansa service. When it saw the light of day, it was a pioneering aircraft from which many successors still benefit today. Photo: Oleksiy Mark

Only the measuring tools reveal the difference. Nevertheless, the plane can also be set up together with figures and car models of Z gauge, which are actually slightly too small. For our photographs we have resorted to Preiser figures, which come from the 1:200 series and are also gladly used as long people for the nominal size Z.

**Dimensions and data for the Vickers Viscount 800 and 810:**

|                      | <u>Prototype</u>         | <u>1:200</u> | <u>1:220</u> | <u>Model</u> |
|----------------------|--------------------------|--------------|--------------|--------------|
| Length               | 26,11 m                  | 130,6 mm     | 118,7 mm     | 129,2 mm     |
| Wingspan             | 28,55 m                  | 142,8 mm     | 129,8 mm     | 142,7 mm     |
| Height               | 8,15 m                   | 40,8 mm      | 37,0 mm      | 43,8 mm      |
| Max. take-off weight | 32.840 kg                | ---          | ---          | 160 g        |
| Cruising speed       | 522 km/h                 |              |              |              |
| Engines              | 4 x Rolls Royce Dart 510 |              |              |              |
| Propeller            | Four blade               |              |              |              |
| Years built          | 1948 – 1964              |              |              |              |
| Retired (Lufthansa)  | in July 1971             |              |              |              |

The Vickers Viscount 814 is interesting for model railway enthusiasts because it is a classic of aviation and has great significance for the history of technology and transport. Especially in the Lufthansa colours, the model, which does not overstretch the dimensions of generous layouts, should appeal to a wider clientele.

A look at the scale shows the deviations explained, but also proves the relationship of the 700 and 800/810

versions in model and prototype in comparison to the one from **Trainini®** 9/2020. The realisation also makes a good impression independent of the dimensions taken off.

This is mainly supported by a clean paint job, which is characterised by the bright and metallic shining underside of the fuselage as well as the wings of a Lufthansa aircraft at the beginning of the seventies.

The white and midnight blue of this airline has also been applied opaquely and without flaws. The same applies to the many printings with writings and markings, even on the propeller blades.



The shiny fuselage and the similarly designed wings look good - exactly following the prototype (photo above). The view from the front (photo below) reveals the finely designed landing gear and the details in the area of the four engines. Photos: Oleksiy Mark

Immediately noticeable are the three silver-framed windows above the wings, which could be opened as emergency exits, and, therefore, stand out from the others, which also have the typical and once appreciated size and oval shape. The features of the model also include the windscreen wipers on the cockpit reproduced by pad printing or the position lights.

But that was no different with the shorter version we presented a year and a half ago. The two antennas on the canopy are separately attached. We are also used to the fine engravings on the wings and tail surfaces, which reveal the rudders and flaps.

Filigree on the cast metal model are the flaps that cover the retracted landing gear after take-off. However, the landing gear is rigid in the miniature, which is also not surprising. This is detailed, but still sufficiently stable to carry the 160 gram mass of the conversion.

As expected, the engines show little detail: at the front they are almost completely covered by the propeller hubs, at the rear they are limited to the exhaust openings, which, for our taste, should be engraved a little deeper.



Based on the historical scene from Düsseldorf Airport on the packaging, we have recreated this scene with Preiser figures (scale 1:200) in which the aircraft is being prepared for the next flight and is about to be refuelled.

All in all, Herpa's aircraft is coherent and appealing. It recognisably marks the transition from classic to modern aviation. Traditional lines in the area of the wing and rudder are contrasted by a nose that already looks quite modern, with the very distinctive shape of the Viscount visible behind the cockpit.

Aircraft connoisseurs will certainly notice this model and it will help them to classify the events on the layout correctly in time and to experience an important piece of aviation history.

**Model manufacturer:**

<https://www.herpa.de>

**Figures used for design:**

<https://www.preiserfiguren.de>



Build your own structures (part 2)

## Special Effects at the Signal Box

*In part 1, Jochen Brüggemann explained how the locomotive management of the Ottbergen depot was created as a model for his layout. However, a signal box was also required there, the construction of which was not to repeat the mistakes of an earlier model. Our author would like to pass on his learning curve to all readers today.*

By Jochen Brüggemann. I had already built the signal box "Gf" a few months before the locomotive line (see part 1 in **Trainini**®12/2022). Unlike the locomotive line, however, I did not use a real building as a model here, but my old signal box.

I had made this out of cardboard around 1982 for my predecessor layout, inspired by several signal boxes based on Prussian construction principles from the Länderbahn era (see **Trainini**® 10/2013, page 10 ff.).

Such signal boxes, partly built entirely in brick, partly with a half-timbered upper storey, were still in operation on the Bundesbahn and the Reichsbahn in the former GDR well into the 1980s.

They were often rebuilt and modernised, sometimes in combination with newer interlocking technology.

Even today, quite a few signal boxes of this type can still be seen, but they are gradually becoming extinct.

In the past decades, I had taken a closer look at a whole series of Prussian signal boxes on railway journeys, in magazines and books and had to realise that my old signal box was not quite prototypical.

Now, I had the opportunity to avoid the old mistakes when building a new one.

When I built my predecessor almost 40 years ago, I had drawn my true-to-scale plans directly on cardboard, mainly



The Gf signal box is shown here during a rehearsal with finished interior furnishing on the upper floor, installed desk lamp, and still loosely laid roof.

on the basis of side views of a Prussian signal box in Walkenried (Harz), but I had slightly modified the number and arrangement of the windows.

For the new building, I have approximately taken over the dimensions of the upper floor and the shape, size and arrangement of the windows; on the other hand, I have made the basement a little more voluminous and adapted the construction more closely to the Walkenried model. For the roof construction, I was able to use the roof of the old signal box as a model.



The view into the storeroom of the upper floor shows the elaborate construction of the interior including a functioning desk lamp for night-time use. The construction method described in the article is also clearly visible here.

I started building the signal box with the upper floor. I drew the eight wall sections of the upper floor on the back of a wall panel: Front and back (with identical dimensions), two equally sized head sides and four equally sized wall parts (slightly narrower than the head sides) for the bevelled corners.

Then the eight identical windows were drawn: four in the front side and one each in the two head sides and in two of the corner parts. I used a matching coin as a template for the round arches. The back and the two rear corner pieces remained windowless.

continues on page 31

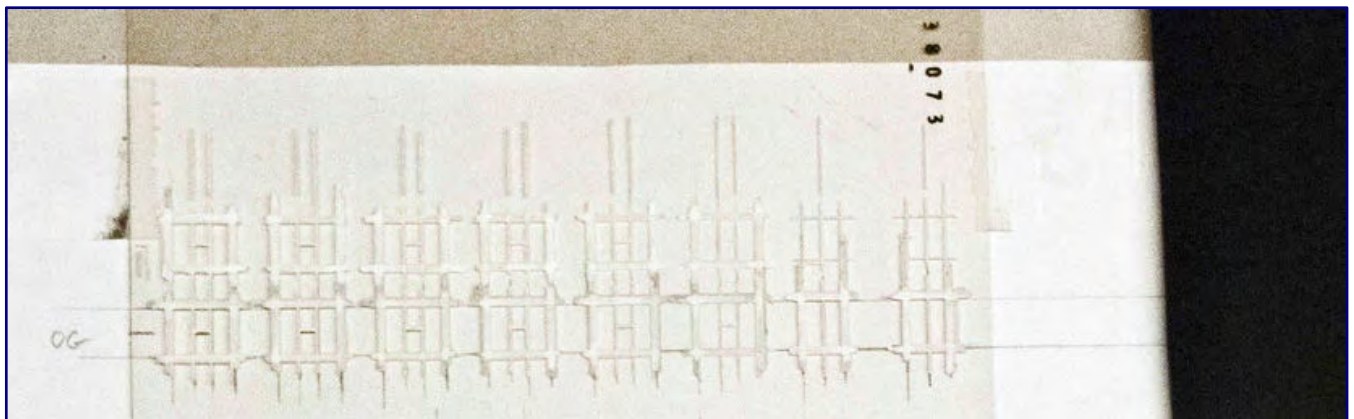
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## Foray into “window construction technology”

Over the decades, I have used various materials to make my model windows, as long as they met my requirements: transparent stiffeners from shirt packaging (collars, cuffs), blisters and also photo negative plates.

Suitable pieces had to be transparent and easy to cut, glossy on at least one side, flat and free of scratches, streaks and fingerprints. If they were also free of distortion and accepted drawing ink or Revell colours, they were used.

The photographic negative plates I used here were a professional addition to my collection. They had a much stronger backing than ordinary roll film and were supplied in standard plate form. When I had to cut them to size in the darkroom, I ended up with unusable remnants that I fixed and saved. Unexposed remnants provided clear window glass, otherwise - depending on the degree of exposure - more or less apparently dusty or sooty panes resulted.



In this photo you can see the number-coded photo negative plate fixed to a sheet of paper with the pencil-drawn frame pattern. The drawn, white frames can also be seen and still have to be cut out of the film.

Decades ago I refrained from the too rough reproduction of plastic window frames. Since then, I have depicted frames and muntins by applying paint to the transparent panels.

In addition to the method described in part 1, another method was used: Here, too, the window panel was first placed on an elevation drawing and secured against slipping. Then, however, I first masked off the horizontal frames and muntins with narrow strips of adhesive tape I had cut myself (not exactly on the lines, but with a little addition in the direction of the glass pane).

Then I drew the edges and stripes with thinned, matt Revell paint along the Tesafilm. After the paint had dried, the tape was carefully removed so that the paint could dry through. I drew rounded frame areas freehand with a brush, using the elevation as a guide.

Which of the two possible methods was used depended on the frame colour used in each case: For drawing ink/ink the technique described in part 1 was recommended, especially with Revell colours the alternative method described here gave the best results.

Finally, a note on interior lighting: Often an unavoidable light transmission remained in the frame and transom area, especially with light colours and thin layers. I have reacted to this with adapted ballast resistors for the LEDs. In the buildings described in Part 1 and this episode, I can also choose between brighter daytime interior lighting and much weaker night-time lighting.



In addition, I drew on paper an exact outline of all the windows of the signal box, including the muntins. With the help of this outline, in the same procedure and on the same kind of material as for the locomotive management, the signal box windows were created.

I cut out the window openings with a scalpel, then brought them to size with a file and sandpaper and checked the accuracy of fit of the signal box windows (and touched them up if necessary).

Then, I cut out the eight wall sections and provided the vertical edges with a 22.5° mitre, so that ideally there would be a 135° angle between the wall sections during assembly. On an old plastic card I drew, as an assembly aid, a slightly enlarged ground plan of the upper floor, with as exact 135° angles as possible.



**This positioning test with the interior lighting switched on and the roof on illustrates very well how much the effort put into the window construction and the interior pays off.**

On this assembly aid, I glued the side panels together one after the other and aligned them with the help of the ground plan lines, making sure that all the outer walls were vertical with the help of a triangle. I did my best to avoid gluing the walls to the plastic card.

After the vertical seams had dried, a bottom plate was cut out of 0.5 mm thick PS to fit exactly into the upper floor from below and glued flush. I used the same material to make an octagonal lid that, placed on top of the storey, is flush with the outer walls on all sides.

This lid was given a central rectangular opening; on the underside I glued on three 0.5 mm thick and only approx. 2 mm wide PS webs, the length of each of which corresponded exactly to the inside width of the upper storey and which were intended both to hold the lid on the walls and to spread the walls slightly when finally assembled. (Note: With my available means, it was not possible to avoid the mitres at the vertical wall edges being slightly smaller than 22.5°.

When gluing, this meant that the angles of the wall parts to each other turned out to be slightly less than 135°, which initially caused the long sides to bow inwards a little. They were largely straightened out again by the inserted base plate and later the webs under the lid.

The basement was also created from eight PS wall panel sections: Front and rear (with identical dimensions), two head sides of equal width and four wall sections of equal width (but somewhat narrower than the head sides) for the chamfered corners.

These wall sections were made a few millimetres narrower than those on the upper floor and mitred at 22.5° on the vertical edges to create an octagonal body on which the upper floor forms a uniform projection of about 1.5 mm on all sides. The height of the basement wall sections corresponds to the floor height plus the upper basement section (as far as it protrudes from the floor).



Two windows in the base storey were shown in a bricked-up state. Since this is not easy to see in the photo, the arrows mark these places.

I cut the opening for the entrance door out of one of the two head sides (to match the floor height of the basement) and carved two “bricked-up” smaller windows on the outside of the front side with the scalpel. I glued the UG wall parts together using the same procedure as for the upper floor, with the help of the assembly aid and the geo triangle.

Then two strips of different widths had to be cut out of the PS wall panel. The width of one strip corresponds to the height of the basement floor above ground level (i.e. lower edge of the door), the other strip is about half as wide.

From the wider strip I cut 8 pieces individually, mitred them at 22.5° and glued them flush to the UG body at the bottom as wall reinforcement of the basement foundation. Eight corresponding pieces were created from the narrower strip as reinforcement for the upper wall termination.



**Additional details to be added to the model were the gutters and downpipes (on the rear side of the building) and the self-bent railing of the stairs to the entrance door.**

Then the top floor was glued to the bottom floor with the same overhang on all sides. I built a staircase from staggered strips of 0.5 mm thick PS board, about as wide as the entrance door, covered the sides with PS wall board, sanded all sides and edges and placed the finished staircase in front of the entrance door.

This “shell” was given a coat of slightly diluted acrylic paint on the outside; the interior surfaces were given this with black acrylic paint to make the walls opaque, the treads of the stairs in grey paint. After drying through, the exterior surfaces of the shell received a glaze of diluted anthracite acrylic paint, the interior surfaces of the upper floor walls received an acrylic paint coat of white, and the floor received a coat of dark grey.

For cutting out and mounting the prefabricated signal box windows I proceeded as with the locomotive management. Finally, inside the first floor, i.e., in the actual signal box, I divided off a narrow area open on one side on the rear wall with two inner walls arranged at right angles (made of 0.5 mm thick PS), in which you can imagine the staircase to the signal box.

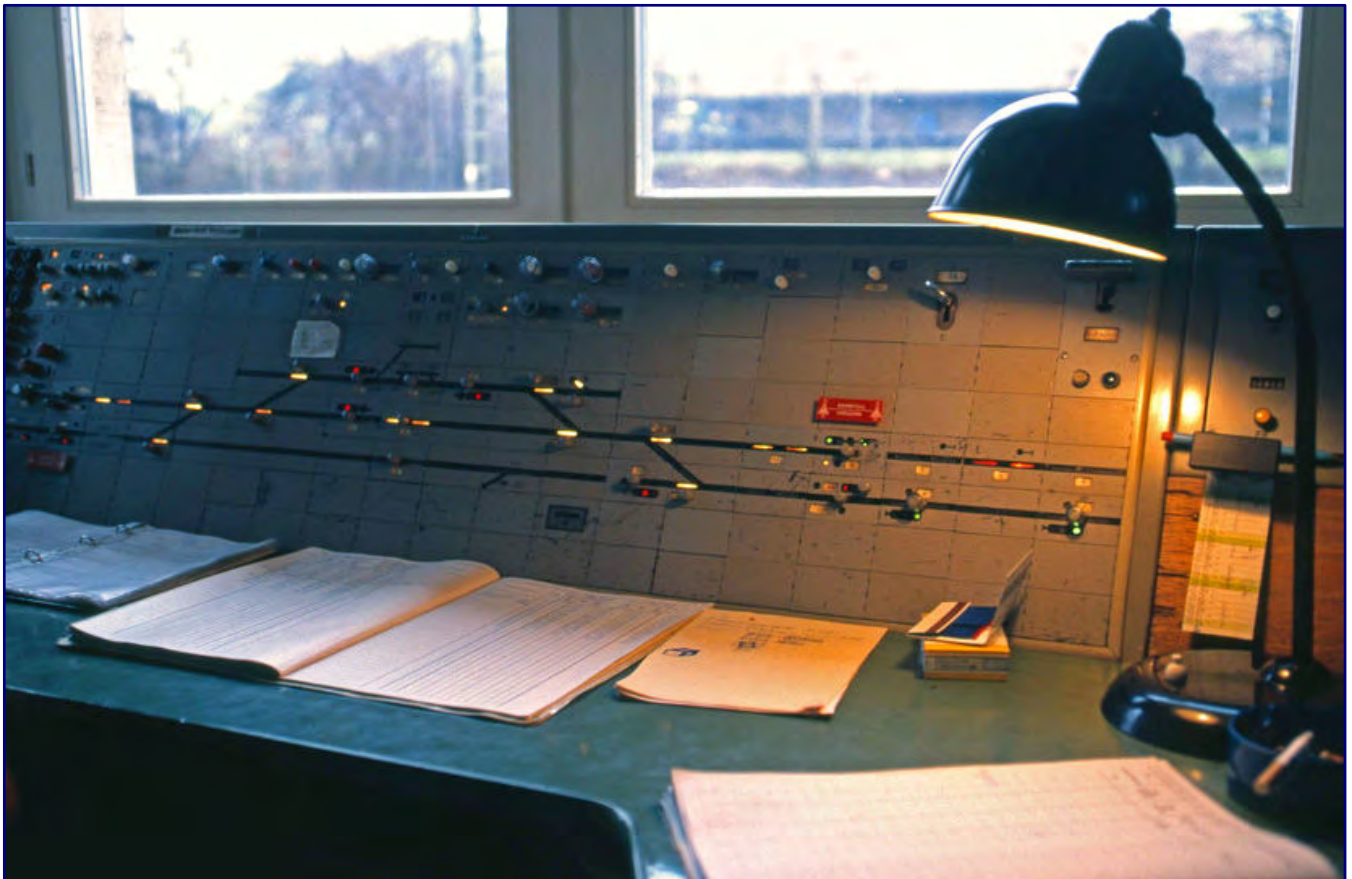


For the entrance door in the basement, I cut out a rectangular piece with an addition from a PS remnant, painted it dark grey, painted a door handle on it in black and glued it in front of the door opening from the inside.

## Interior design is mandatory

Since the signal box was to be exhibited only two hand widths from the edge of the layout, I had decided in favour of an interior arrangement from the outset. I was inspired by photos from a visit to a small signal box on an electrified DB main line a good 30 years ago.

Inspired by these impressions as well as some photos in books and magazines, the stand table with two SpDr control desks was created from PS plate remnants, PS round profile, thin brass wire, paper, etc.: the larger one for the southern station area, the smaller one for a double-track branch 2 km before the station entrance.



Interior views like this one of a small dispatcher's signal box, taken in February 1989, provided the inspiration for the interior design of the self-built model.

The same material was used for a locker, a filing cabinet, a small additional writing desk, a stove, 2 chairs and the equipment of the desk (official telephone system, public telephone, a microphone for station announcements, and the train dispatch book).

On the back wall, simplified replicas of switch hand locks, tools, switch and barrier hand cranks, switch keys, etc., as well as a (signal) horn (made of brass wire) found their place. The wall behind the dispatcher's chair is adorned with a track plan of the entire station, actually a much-reduced track plan of the station on my layout.

The necessary lighting is provided by a kind of enlarged desk lamp, which I built myself using a 0402 SMD LED. The conductive connections to the lamp were made with silver conductive lacquer from Busch. The corresponding protection diode (universal diode of the type 1N4148) found its place under the desk; here I also pulled the two connection wires of the lamp through two holes in the floor into the basement.

The ballast resistor (3,3 k $\Omega$ ) for the lamp is located at the distributor at the bottom of the layout, where it is easily accessible (see further down in this report). From a Preiser bulk pack of unpainted miniatures (art. no. 88500), a railwayman in a suitable pose was selected, painted with acrylic paints and placed in front of the layout table.



The switched-on desk lamp allows a view of the completed interior of the signal box, which can also be easily seen by the viewer from the outside.

At first, I had made the roof out of thin cardboard exactly according to the dimensions of the old signal box; however, the white glue used had caused unsightly deformations when drying. Therefore, all parts (floor, two long sides, two narrow sides and four corner pieces), with the same dimensions as the cardboard roof, were made of polystyrene (0.3 mm thick) and assembled: first the two long sides were glued to the floor and aligned, then the two narrow sides were attached and finally the corner pieces were adapted and inserted.

I glued eight matching pieces of 1.0 mm wide PS profiles with a semi-circular cross-section (from Evergreen) to the bottom of the roof edges as guttering, with a good 0.5 mm overhang. I painted the roof

with black acrylic paint, the gutter and the underside of the roof with medium grey acrylic paint. I glued the finished roof construction to the upper floor with Noch grass glue, so that there was an even overhang on all sides.

Finally, some remaining work followed. The chimney was made from four strips of PS masonry panels, each 3 mm wide, was mitred at 45° on both sides and joined together to form a body with a square cross-section. I measured the length so that the top edge overhangs the ridge of the roof; I bevelled the bottom end to match the pitch of the roof, glued it to a piece of paper and cut it out with some overhang.

The chimney was painted brick-coloured on the outside and sooted with anthracite glaze; I blackened the inside and the top, and coloured the lower edge of the chimney and the projecting paper (representation of roofing lead) with dark grey acrylic paint.



**At the rear of the building, the downpipes made of brass wire reveal their appearance. The chimney for the signal box's heating system, which was added later, can also be seen.**

I attached the finished chimney to the roof with Noch grass glue. For the now missing rainwater downpipes, a 1 mm thick Evergreen plastic round rod was carefully heated over a candle flame and pulled out.

I bent two pieces of about 0.6 mm diameter into the desired shape, painted them with medium-grey acrylic paint and attached them to the two rear corner walls in the middle of the gutter and the lower edge of the building with plastic glue.

Two identical stair railings were made from 0.5 mm thick brass wire, which I fixed with plastic glue in matching holes on both sides of the entrance door and in the bottom step of the staircase and coloured with grey acrylic paint.

I cut two earthing rods from 0.3 mm thick needles and glued them parallel with some distance to two thin, short PS pieces.

The rods were painted white with grey tips at the top and three red rings at the bottom and were placed on the outside of the signal box's UG. Two square signs with the imprint "Gf" ("f" stands for "Fahrdienstleiter; engl. dispatcher), attached to the front corner walls of the signal box, completed the equipment.

The signal box was installed and connected in the same way as the locomotive wiring: I soldered somewhat thicker extension cables to the two rather short connection wires and led them through two 1.2 mm wide holes under the layout plate where the signal box was installed, whereby the soldering points got stuck in the hole at the top due to their larger diameter.



I applied some grass glue to the underside of the signal box walls and then aligned the building on the layout and pressed it down lightly. I led one of the two connecting cables under the layout to a luminous flux-ground distributor, the other to the same distributor for the phases, which is also used for the lighting of the locomotive line.



At its installation site, the Gf signal box makes a very good impression, especially as another self-built building can be seen in the immediate vicinity (on the left behind the coal bin).

Here, I have also connected a 3.3 k $\Omega$  ballast resistor (R2) at the signal box connection to protect the LED (according to the circuit diagram); then there is a connection to the connection (phase) of the locomotive line and from there together to the control panel. In the control panel I can now use a switch (1/0/1) to set two different brightness's for both buildings or to switch off the current to the LED.

In the future, I plan to build more buildings for the depot myself using the methods described above, including a workshop building with a locksmith's shop, a forge and a magazine, a sand processing plant and an administration building that will also contain a canteen, social rooms, and overnight accommodation for locomotive staff.

If you feel like it, you are welcome to try your hand at reconstructing buildings on the basis of my building descriptions, of course at your own risk. If my projects and ideas in this report meet with approval, I will be very pleased.

All unmarked photos in this article: Jochen Brüggemann

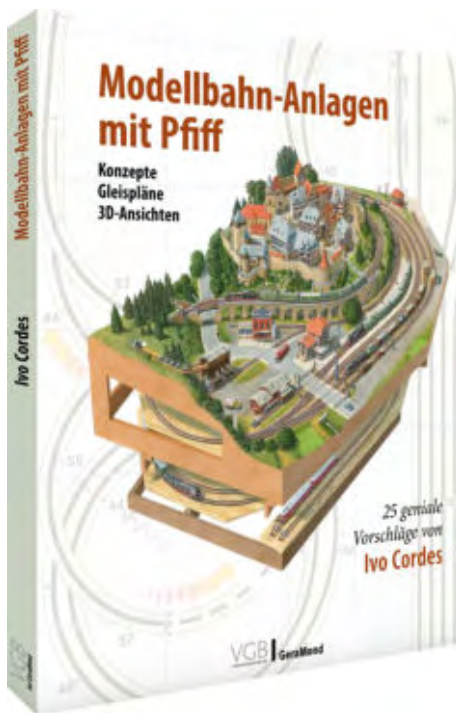
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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Sammelwerk an Konzepten

## Anlagenpläne zum Anfassen

**Mehr als 40 Jahre ist Ivo Cordes in der Fachpresse präsent. Jeder Modellbahner wird schon handgezeichnete Anlagenillustrationen von ihm gesehen haben und im Geiste daran hängen geblieben sein. 25 Jahre Werke seines Schaffens aus den letzten knapp 30 Jahren sind nun zu einem Buch gebündelt worden.**



Ivo Cordes  
Modellbahn-Anlagen mit Pfiff  
Konzepte, Gleispläne, 3D-Ansichten

Geramond Media  
München 2023

Gebundenes Buch  
Format 22,8 x 29,6 cm  
128 Seiten mit 217 farbigen Abbildungen

ISBN 978-3-96453-362-3 (ISBN-13)  
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Erhältlich direkt ab Verlag  
oder im Fach- und Buchhandel

Langjährigen Lesern des Eisenbahnmagazins (EM) sollte Ivo Cordes ein Begriff sein - auch dem Rezensenten: Seine Anlagenentwürfe und Illustrationen waren zusammen mit den für Um- und Eigenbauten so wertvollen Lokzeichnungen eine wichtige Marke dieser Zeitschrift.

Der Herr aus Bremen verstand es schließlich wie kein zweiter, seine Ideen nicht allein in schnöden Gleisverlaufslinien zu Papier zu bringen, sondern die dreidimensionale Wirkung höchst lebendig und wirkungsvoll aufzuzeigen – Jahrzehnte, bevor rechnergestützte Planungsprogramme wie Wintrack Vergleichbares bieten sollten.

Auch heute unterscheiden sich diese Relikte des analogen Zeitalters äußerst angenehm von den statischen und vergleichsweise leblos wirkenden Animationen, an die wir uns inzwischen gewöhnt haben. Vor diesem Hintergrund war es eine gute Idee, Entwürfe aus seiner Feder in einem Buch zusammenzufassen.

Auswahl und Wiedergabe sind tadellos gelungen und unterstreichen die angenehme wie auch unterhaltsame Wirkung. Immerhin hat Ivo Cordes in Jahrzehnten zahlreiche Vorschläge zu höchst unterschiedlichen Modellbahnanlagen erarbeitet.

Veröffentlicht wurden sie mit Plänen und Schaubildern in Alba-Büchern und verschiedenen Zeitschriften. Neben dem EM waren das Miba-Sonderpublikationen und die Modellbahnschule. Doch um Ivo Cordes ist es inzwischen seit Jahren sehr ruhig geworden.

So darf der Käufer dieses Buches nicht erwarten, hier über- oder gar neu erarbeitete Pläne zu finden. Bei den 25 illustrierten Entwürfen zu Modellbahnanlagen, versehen auch mit Skizzen und Hinweisen zur Betriebsabwicklung, handelt es sich um einen Querschnitt aus rund dreißig Jahren seines Schaffens.

Ob der Autor sie selbst als seine subjektiv besten Werke wertet oder es eher um einen repräsentativen Querschnitt ging, ist uns nicht bekannt. Jedenfalls folgen sie unterschiedlichen Flächenbedarfen von klein bis groß, verschiedenen Ansätzen als stationäre oder Segmentanlagen, Betriebsdioramen oder auch Bühnen (Guckkästen) sowie auch wechselnden Anlagenformen und teilweise auch Spuren.

Das bringt eine ansprechende Fülle und Themenbreite mit sich, auch wenn die Spurweite Z nicht explizit berücksichtigt wird. Nicht wenige Anlagen dürften auch das Steigungsvermögen unserer Lokomotiven mit anhängenden Wagen überfordern. Dennoch haben die ausgebreiteten Ideen auch einen großen Wert für unseren Maßstab.

Durch den geringeren Platzbedarf bieten sich schließlich auch Möglichkeiten, Steigungen durch Strecken der Trassen zu verringern oder Schattenbahnhöfe anderweitig zu verstecken statt sie in die Tiefe verlegen zu müssen.

Wertvolle Hinweise für einen abwechslungsreichen, vorbildorientierten Betrieb liefern die Illustrationen tatsächlich für jede Nenngröße. Vielen Lesern entgegenkommen wird auch, dass für dieses Buch vielfach Projekte ausgesucht worden sind, die sich für das eigene Umsetzen nicht übermäßig anspruchsvoll zeigen.

Gleichwohl wurde auf ansprechendes Gestalten Wert gelegt und das Betriebskonzept so nah wie möglich am Vorbild orientiert. Anfänger können daher auch gut verfolgen, wie eine gute Planung entsteht: Abschätzen der Raummaße und nutzbaren Fläche, Auswahl eines Themas und Betriebskonzepts, Planen der dazu passenden Anlage und erst zum Schluss der Kauf des thematischen passenden Materials.

Anlagen die eher in umgekehrter Reihenfolge entstehen, können niemals überzeugen, vermitteln keine wirklichkeitsnahe Atmosphäre und stimmige Zuggewegungen: Sie bleiben ein Spielzeug, das bald langweilig wird, und werden nicht zu einer Traumwelt, die im Gedächtnis des Betrachters bleibt.

All das macht in Summe dieses Buch so hilfreich und wertvoll, es ist spurübergreifend für viele Modellbahner empfehlenswert. Für den einen ist es eher ein hilfreiches Nachschlagewerk, für den nächsten eine kreative Inspiration und Planungshilfe, für wieder andere mag es vielleicht auch einfach eine nostalgische Erinnerung bedeuten.

Festhalten müssen wir beim Stichwort Nostalgie auch, dass aus unserer Sicht über viele Anlagenentwürfe die Zeit hinweggegangen ist. Zum einen wiederholt sich beispielsweise das Grundmotiv „Eingleisige Nebenstrecke mit Stichstrecke und Güterthema“ auffallend häufig.

Zum anderen entspricht auch eine parallel zur Anlagenkante verlaufende Streckenführung und am äußeren Umrissverlauf ausgerichtete Bahnhofslage nicht mehr dem heutigen Verständnis eines modernen Gleiskonzepts.

Doch die Art und Weise der begeisternd lebendigen Illustration ist aktueller denn je. Wer hortet heute noch 20 bis 30 Jahre alte Modellbahnhefte daheim? Beim Lesen entdecken wir schnell, wie sehr wir diese Darstellungsform insgeheim vermissen. Und so ist sie uns auf jeden Fall nicht endgültig verloren gegangen.

Publishing pages:  
<https://www.vgbahn.shop>



## Die Baureihe E 69 im Filmportrait Fünf beliebte Einzelgänger

**Die Baureihe E 69, ab 1968 als 169 geführt, blieb stets etwas Besonderes im Bestand der Staatsbahn: technisch nur eine Splittergattung, noch dazu aus fünf teilweise völlig unterschiedlichen Exemplaren bestehend. Doch die kleinen Maschinen waren urig, stets beliebt und blieben nicht zuletzt auch wegen ihrer Spitznamen bekannt. Wohl deshalb standen sie auch schon recht früh im Interesse der Modellbahnhersteller.**



CFT Video Berlin  
Die Baureihe E 69  
Die bayerischen Lokalbahn-Elektroloks

EK-Verlag GmbH  
Freiburg 2022

DVD-Video  
Bildformat 16:9  
Tonformat Dolby-Digital 2.0  
Sprache deutsch  
Laufzeit ca. 58 Min.

Art.-Nr. 8623  
Preis 19x80x EUR (Deutschland)

Erhältlich direkt ab Verlag  
oder im Fach- und Buchhandel

Einst stellten sie die dienstältesten Elektrolokomotiven der Deutschen Bundesbahn: Bis Anfang der achtziger Jahre war die Baureihe E 69 im Einsatz, die in vielerlei Hinsicht so besonders war. Bei den Eisenbahnfreunden erfreuten sie sich stets großer Beliebtheit.

Das galt auch schon, als die Diesel- und Elektrotraktion noch als „Totengräber der Dampfloks“ verpönt war. Immerhin konnten die gerade mal fünf Exemplare dieser Baureihe dem Dampffrost nicht ernsthaft etwas anhaben. Außerdem waren sie teilweise bedeutend älter als so manches der schwarzen Ungetüme auf Schienen.

Zudem ist die Geschichte der E 69 auch noch höchst interessant. Dies beginnt schon damit, dass die Baureihenbezeichnung erst seit 1938 existiert. Bis dahin gehörten die kleinen Maschinen noch zur Münchner Localbahn AG (LAG), die von der Deutschen Reichsbahn schließlich samt Material übernommen wurde.

Gemeinhin wird die 1906 in Betrieb genommene LAG 1, die spätere E 69 01, als älteste der ungleichen Schwestern betrachtet. Bei genauerem Hinsehen stimmt das so nicht – genau das bildet den perfekten Einstieg in die spannende Geschichte der kleinsten und leichtesten Ellok der Reichs- und Bundesbahn.

Außerdem waren sie auch noch Pioniere für das Einphasen-Wechselstromsystem, das heute nicht nur in Deutschland vorherrschend ist. Und die spätere E 69 04 war zuvor gar eine Drehstromlok bzw. die Hälfte einer solchen Versuchslok. Gebaut wurde sie bereits 1901, später mittig durchtrennt und elektrisch neu ausgestattet.

Als sie sich in dieser Form nicht bewährte, bekam sie schließlich einen völlig neuen Aufbau, der dem Äußeren ihrer vier Schwestern folgte. Ihr offizielles Baujahr, das sie deutlich jünger erscheinen lässt, ist

darauf zurückzuführen. Im Filmportrait wird übrigens auch die 100. Wiederkehr dieses Ereignisses gefeiert.

Der Faszination der kleinen Ellok können sich auch Zetties nicht entziehen, weshalb wir dieses Filmportrait zur Rezension ausgewählt haben: Seit vielen Jahren hat Bahls Modelleisenbahnen eine rollfähige Miniatur im Programm, bei Z-Modellbau war die angetriebene E 69 02 zu finden. Die rote Schwester E 69 03 wurde dort zumindest als Einzelstück gebaut.

Im Film finden wir vor allem mit den musealen Maschinen gedrehtes Material aus Bayern. Dort sind die beiden betriebsfähigen Exemplare vor allem, aber nicht nur, auf ihrer früheren Stammstrecke Murnau – Oberammergau unterwegs.

Nicht vergessen wird auch der Einsatz durch das Bayerische Eisenbahnmuseum Nördlingen, sowie der seit 2008 aktuelle Aufenthalt der E 69 03 im DB-Museum Koblenz. Was wir vermissen, sind Betriebsaufnahmen aus Heidelberg, wo E 69 02 und E 69 03 kurze Zeit im Rangierdienst dienten, bevor sie zurück nach Oberbayern kamen.

Integriert worden sind hingegen historische Fotografien aus den frühen Jahren der Loks. Sie zeigen die Ära unter der Ägide der LAG und der Reichsbahn. Wertvoll ist der Ausflug zur Heeresversuchsstrecke Zossen – Marienfelde, wohin die Ursprünge der E 69 04 führen, deren andere Hälfte später bei Siemens diente und heute im Technikmuseum Berlin ausgestellt ist.

Erwähnenswert ist an dieser Stelle noch das Umstellen des Stromsystems von 5,5 kV 16 Hz auf 15 kV 16,7 Hz, das bereits in die Zeit der frühen Bundesbahn (1954 /55) datiert. E 69 01 wurde als einzige der fünf Baureihen-Vertreterinnen nicht mehr dafür umgebaut und ausgemustert – die Filmemacher besuchen sie als Denkmal im Lokschuppen Freilassing.

Das filmische Denkmal für die fünf Lokomotiven, von denen keine einzige der anderen völlig gleich, spannt einen großen Bogen vom Ursprung über die Betriebs- und Einsatzgeschichte bis in das Jahr 2022. Auch die Ausmusterung der letzten Lok liegt bereits mehr als dreißig Jahre zurück.

Mitfahrten im Führerstand vermitteln ein Gefühl für die betagten Urahnen der elektrischen Traktion, was uralte Technik und erfolgte Modernisierungen gleichermaßen einschließt. Fast sicher erscheint uns, dass selbst gute Kenner dieses Loktyps dort noch das eine oder andere an zuvor unbekanntem finden.

An vielen Stellen scheint uns dieses Filmportrait deutlich über den Umfang vergleichbarer Abhandlungen zu anderen Fahrzeugtypen hinauszugehen. Das macht diese DVD nicht nur unterhaltsam, sondern auch gleichermaßen wissenswert – wir möchten sie deshalb nicht mehr missen.

Umgesetzt wurde das alles in bewegten Bildern hervorragender Qualität, beinahe perfekt getroffener Auswahl und guten Schnitts. Auch die eher zurückhaltenden Moderationen stören zu keinem Zeitpunkt das Erlebnis der Betriebsgeräusche, die hier einfach dazugehören.

Es gibt da nur eine einzige Ausnahme: Auf einem Feld, von dem ein musealer Sonderzug mit der Kamera samt Landschaft eingefangen wird, ist neben dem Kameramann wiederholt auch das Klackern eines Fotoapparats zu hören. Zuvor stößt der vermeintliche Bediener beim Erscheinen des Zugs einen Freudenlaut gen Himmel.

Das stört das Idyll gewaltig, ließ sich aber wohl nicht herauschneiden oder durch Nachvertönen kaschieren. Insofern ist es auch nicht dem Produzenten anzulasten, sondern gedankenlosem Verhalten eines Dritten. Der Film verzichtet glücklicherweise nicht gleich auf die gesamte Sequenz.

**Publishing pages:**  
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<https://www.ekshop.de>





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## Model railway exhibition in Frankenland Marktredwitz on the 3rd Sunday of Advent 2022

*Many events, operating days and meetings have been cancelled in the last two years. Many model railway enthusiasts, therefore, have a strong desire to finally get together again, in person, and indulge in their mutual hobby. In December 2022, this led to a special exhibition on the so-inclined plane, where a Z gauge layout of gigantic proportions was on display.*

By Markus Gaa. The last public model building exhibition where we were able to exhibit the US Z-Lines modular layout was in Mannheim in March 2019 (Faszination Modellbahn). So, the desire for a renewed assembly was already quite great.

This desire is driven by the motives of being able to ride long trains and to do so in a community of long-time friends. The first thoughts about this came about 18 months ago. For various logistical and personal reasons, however, the demonstration was to take place in the Upper Palatinate region.



Even the view from above can only capture parts of the enormous US-style Z-gauge layout. It continues in the foreground of the viewer.



# Trainini® *International Edition*

German Magazine for Z Gauge

A public event brings with it various requirements: additional model railway gauges, suitable premises, supply facilities, accessibility, permits, etc. So, it's not easy when the organisers are still working full time at the same time. Two days were planned for construction, which proved to be necessary in the end, especially to have the modular layout ready for operation. Dismantling then, takes only half as much time.



This level crossing called "Joe's crossing" (photo above) was also to be seen, to be found probably only in the United States. The other routes often looked more familiar, especially when they lead through landscapes that could also be found in Central Europe (photo below).



There was a lot worth seeing. Let's start with the module layout of the US-Z-Lines. Peter Simon brought his new shunting and layout element (fiddle yard). With a length of about 7.5 metres, this is a real eye-catcher, and in Z gauge. This gave us fellow players the opportunity to set up and park several trains in parallel.

Peter Schwager brought his single-track modules and integrated them into the large module layout by means of individual transition pieces. Especially two single-track sections guaranteed an interesting operation.



Peter Schwager clearly indicated the authorship on one of his modules. He contributed the single-track sections, which are very common in the USA.

Since the operation was digitally controlled, and each train was accompanied by its "engine driver", consultation was always necessary, which additionally promoted the fun of the hobby.

There were also special highlights, such as "Big Boy" and "Challenger" or even an 11-metre-long train with the harnessed power of six locomotives. For me, this part was one of the highlights of the Z gauge anniversary year.

Peter Dumler brought his H0m layout with motifs from Graubünden. New segments are partly still under construction, but it was already possible to see from the photos of the original which new motif he had come up with. With his underground sidings station, a varied operation was always guaranteed.



Peter Dumler's contribution in H0 scale was this funfair, where our reporter was particularly enthusiastic about the white-water ride.

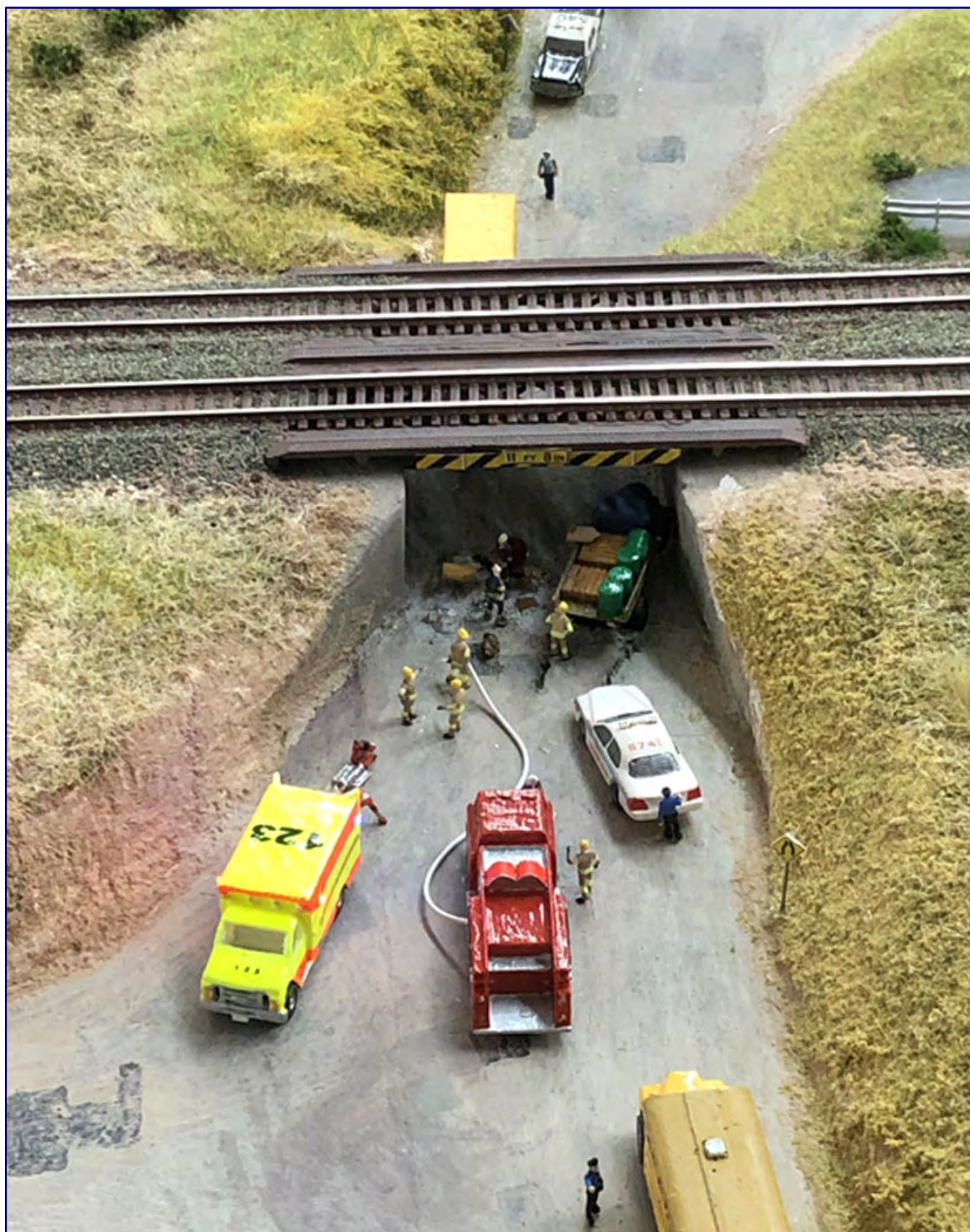
continues on page 48





Peter Simon driving a goods train (photo above), behind him on the stage the gauge 1 is set up as a ground layout. Volker Töpfer (photo below) prepares a train in the fiddle yard.





This accident scene with the load on fire is an eye-catcher on the double-track section. The emergency vehicles in this scene clearly reveal which country we are in.



Operational safety, which is usually not noticeable to the ordinary visitor, has always been a given. Visitors only notice problems in the event of derailments or collisions. This is obviously not the case with Peter or the Rhaetian Railway.

Andreas Dumler builds a funfair in H0 scale. And he doesn't do it "just like that", but with lighting effects and, of course, moving rides. For me, the fully functional white-water ride is special. By the way, more segments are planned here as well.

On the stage in the hall there was a gauge 1 railway. It was particularly impressive with its many operating noises and silky-smooth ride. It was controlled digitally. For this size of model railway, however, we should not expect a large, transportable landscape, space is unfortunately limited.



The biggest wish for Christmas? Snow! At least this exhibited gauge N layout was able to satisfy the longing of many visitors so close to the festive season.

N gauge was represented by Peter and Thomas Korzendorfer, the latter known through his Youtube channel "Alter Klapperknochen". The layouts on display were impressive for their diversity of detail and, despite comparatively simple traffic, showed many technical features, for example moving cars or a functioning bascule bridge. For the smallest visitors there was a Brio wooden railway to play on.

I want to send my thanks especially to Axel Hempelmann and Sylvester Schmidt: They both took a long weekend to present the ZFI, to represent the Z gauge and also to answer other model railway questions. Further thanks go to Carolin Dumler, who supported us with many administrative tasks before and during the exhibition in a clever, energetic way, always with a smile, and in a good mood.





Sylvester Schmidt enjoys looking after the ZFI stand and was always available to answer questions. The 50th anniversary of Z gauge is also clearly indicated here with the community logo.

Personal summary: All the work in the run-up was more than justified by great model railway enthusiasts with heart. The visitors were enthusiastic and many asked for a repeat performance. This is exactly the applause it takes to get back to work again.

All photos: Markus Gaa

**Exhibitors appearing in Marktredwitz:**

<https://www.bemo-modellbahn.de/galerie/anlagen-unserer-kunden.html>

<https://z220bahner.de/Home>

<https://www.youtube.com/@AlterKlapperKnochen/featured>

Readers' letters and messages

## Zetties and Trainini in Dialogue

*Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.*

An already traditional New Year's greeting from Switzerland:

With the photo of a typical SBB goods train service of the seventies, once again a heartfelt thank you for all the great reading material we were able to enjoy again this year.



Every year we look forward to the already traditional New Year's greeting from our reader. This time he is accompanied by a goods train of the seventies, with the Ae 3/6II at its head. Photo: Martin Zimmermann

The "Ae 3/6" is still overshadowed by the "Crocodile" in the rating of the Z Freunde, but in 1993 and 1999 it was first class in terms of printing. No wonder it is in the lead in my photo.

Martin Zimmermann (Schweiz (Switzerland)), per E-Mail

**Thank you and inspiring ideas:**

I, too, would like to take this opportunity to thank you for the great work you are doing. This anniversary year in particular, together with Covid's initial restrictions, has meant a particularly great deal of effort. Bravo!

In connection with your magazine, I have another suggestion:

I would like to see an article exchange among the readers! We are quite a small community on this planet, so it would certainly be feasible to contact us by e-mail or telephone with a photo and an idea of price.

Every model railroader has a lot of material that is no longer needed and in the usual platforms such as eBay and other small ads, you are hardly noticed as a Z railroader. Wouldn't that be worth considering?

Josef Gurber (Frankreich), per E-Mail



**On our own behalf – prize dispatch failed:**

Unfortunately, the anniversary book that our reader Mathias Wozniak from Switzerland won in our anniversary competition was returned by return of post at the turn of the year 2022/23. A sticker shows that the envelope box was successfully cleared through customs by the Swiss postal service.

It is not clear why it was sent back to Germany and could not be delivered. Due to the lack of e-mail contact data and a possible change of address, we therefore ask our readers to contact the editors so that this book can still find its future owner.

**Error correction to the December issue:**

In the article about the model development of the class 151 from the Märklin programme we wrote that the changeover screw for the overhead line operation was moved to the inside with the latest model (item no. 88262).

In fact, this already happened with the previously offered, traffic-red Railion variant (88261) from 2015, as we found out during the translation work for this issue. This is also stored in the description of the Märklin product database. Accordingly, we would like to have our history corrected with this addition.

**A delivery at MTL:**

Micro-Trains has shown only one model for delivery in the latest reporting period. This is car number 11 (item no. 502 00 650) from the Railroad Magazine series. This covered wagon in red-black basic design again shows a historical motif of this former magazine.

**Re-launched loads:**

Spur Z Ladegut Josephine Küpper (<https://www.spurzladegut.de>) reports that after a long time pit wood inserts (art. no. Z-96) are again available. The same applies to the sewer pipes (Z-160-1) with tie-downs. Both loads are designed for the old standard Märklin bogies, i.e., they fit their models 8622 or 8610.



**Current AZL new products:**

The ALCO PA1 of the Pennsylvania is the subject of this month's American Z Line. In the Tuscan red livery with the typical antenna lines on the roof, it is available individually (item no. 64409-1 / -2) or as a duo together with a B-unit (64409-1\_SET / -2\_SET).



The ALCO PA1 is a striking appearance in the design of the PRR (art. no. 64409-2; left). It is joined by Southern's EMD E7A (64606-2; right). Photos: AZL / Ztrack

These locomotives are joined by Southern's two-tone grey EMD E7A (64606-1 / -2), where the colour areas are separated by a yellow trim line.

The MAXI-IV car units also get another edition with the brown examples of the Iowa Interstate (906556-1 to -3). All three packs are loaded with six different container sizes from different owners, and, thus, also clearly differ in colour. No less than five packs are available in yellow livery with the old TTX logo (906552-1 to -5).



Entering with the old TTX logo are the MAXI-IV carrying wagons in brown (906552-3; left), while BNSF's Bethgon bulk wagons with hold covers (90109-1P; right) make a brief return to the range. Photos: AZL / Ztrack

The Bethgon bulk freight wagons of the BNSF, which have now been retrofitted, are something special: BN already retrofitted a number of these wagons for the transport of proteins. For this purpose, a cover was retrofitted on the wagons to protect the weather-sensitive contents. These wagons are still in use at the BNSF today and appear in packs of four (90109-1P to -3P).

All manufacturer photos of the current deliveries can be found at <https://www.americanzline.com>.

**Useful tool:**

The mini hot air blower (art. no. 170555) recently published by Faller is a helpful tool that we are happy to recommend to our readers. There are three reasons for this, which set it apart from the alternatives with which we are familiar.



The mini hot air blower from Faller (art. no. 170555) impresses because it is well thought out and sensibly equipped.

On the one hand, the Gütenbach specialist includes a heat-resistant glove that protects against burns. This is particularly useful for what we consider to be typical applications for repairing of cracks or shaping of polystyrene parts, because the user comes quite close to the nozzle. The air exiting the nozzle reaches a temperature of up to 190° C.

Occupational safety is also served by the protective sheathing of the nozzle, which prevents direct contact with this metal part from almost any direction and thus also protects against serious heat injuries. As a third advantage, we have identified the integrated stand, which holds the hot air gun horizontally.

This way, the hot parts do not come too close to the work area, which could be damaged or possibly even flammable immediately after operation. The position also allows the appliance to be placed safely on a (non-flammable) work surface and keep the hands free when operating it in the aforementioned manner, and not to direct the heat jet onto one's own limbs by unintentional movements.

The screwdriver, shown in the official product illustrations, is not included, and is not required for the appliance. Consequently, it does not appear either in the product description or in the explanations on the packaging.

#### **Märklin deliveries at the turn of the year:**

As soon as the Christmas business is over and the annual stocktaking is due, the dealers' shelves are filled with Märklin new products, without exception belonging to epoch VI. Since the last issue, for example, the heavy freight locomotive 162 007 of the Swedish railway company Hectorail (item no. 88262) has now made it onto the market.

The locomotive, which originates from the DB class 151, wears the grey-orange livery of its adjuster and is still active in Germany in the prototype. We already dealt with the locomotive in more detail in an article in the last issue.



162 007 from Hectorrail (Item No. 88262) is here harnessed to a KLV train with AAE's pocket wagons, carrying trailers from Tuborg and Carlsberg (82290).

The four AAE Cargo pocket wagons (82290) can perhaps be attached. They are loaded with four green semi-trailers of the Carlsberg group, two of which are labelled "Tuborg" and two "Carlsberg".

Also, to be reported are two other locomotive new products: The Deutsche Bahn express locomotive 101 088-3 (88679) advertises the Saxony steam railway route with pictorial motifs and text and is technically up to date. The class 193 Vectron is a new design. It has now appeared in the design of SBB Cargo (88232) and follows its DB counterpart, which was already delivered in spring 2022, with a long delay.



101 088-3 with the advertisement "Dampfbahn-Routе Sachsen" (Steam Railway Route Saxony) (88679; left) is differently printed on both sides. The opposite side can be seen in our messages of our portal pages. Vectron friends had to wait a long time for the SBB Cargo version (88232; right).



**First information about Intermodellbau 2022:**

The planned dates are 20 to 23 April 2023. Tickets can already be purchased in electronic advance sales (<https://www.intermodellbau.de>).



We do not yet have any information about Z Gauge participation. The venue is the Dortmund exhibition centre, which is part of Westfalenhallen. It can be reached from Dortmund main station via two underground lines and various regional train lines. For those arriving by car, there are large car parks in the vicinity of the event centre, from where the halls can be reached on foot or by a short underground ride.

Illustration: Messe Dortmund GmbH

**New refrigerated version at Full Throttle:**

William Dean Wright, trading as WDW Full Throttle, has added a wagon new for December 2022, which only became known after the editorial deadline. The reconstructed 34-foot refrigerated wagon with wooden walls now wears a yellow livery with red trim lines. It is on the road for PRR Empire (item no. FT-9025-1).

**Panzer-Shop.nl discontinues its Z-gauge range:**

The Panzer-Shop.nl impressed us with its high-quality and finely detailed 3D printed articles. The product range included figures, military vehicles, construction machinery and civilian commercial vehicles, the realisation of which can be described as outstanding. However, this product line was discontinued on 31 December 2022 due to lack of demand.

In an exchange with our editors, owner Kevin Boogaard regretted his decision, but the nominal size Z was difficult to print in this quality and had never become profitable for him. Despite the reports in our magazine, advertisements on social networks and active information in the forum of Z-Freunde International, there was no basis for continuing his Z scale operations.



Highly detailed models like these construction machines were part of the gauge Z programme of Panzer-Shop.nl, which are now no longer offered.

Even after a period of three years and the addition of 25 figure sets as well as the vehicles of the MAN F2000 series, it had remained with a handful of orders that did not justify the effort.

In a dialogue with us, he was particularly disappointed that there had never been a response to his ideas in the forum, and that after the end of the Z Gauge series had been communicated, more disappointed e-mails had been received than there had been paying customers.

#### **Lego model in scale 1:220:**

Our reader Johan te Riet from the Netherlands drew our attention to a new Lego product that has been released in 1:220 scale. It will be much too large for almost all layouts, but those who collect the clamp bricks and indulge in our gauge will certainly know how to use the model sensibly.

We are talking about one of the most famous sights in the world: the Eiffel Tower (art. no. 10307), the original of which was built in 1889 in Paris, has been adapted for an adult target group.



**The Eiffel Tower (item no. 10307) from Lego is made in 1:220 scale and could therefore also find some lovers in the Z gauge area. Photo: Lego®**

Exactly 10,001 bricks and elements let it grow to a height of 149 centimetres. Like the prototype, it has three viewing platforms, an office and lifts as well as the transmission mast with the tricolour at the top. When fully assembled, it occupies a base area of 57 x 57 cm.

The kit can be purchased in the brand's own shops ("Lego Stores") and by online order (<https://www.lego.com>).



**Our focus for the year 2023:**

This year, we are once again focusing on an annual theme in our articles. After the 50th anniversary of Z gauge in 2022, we are now focusing on “marginal phenomena” in the twelve issues of this year.

The main topic is deliberately formulated to be both meaningful and meaningless at the same time, because it is intended to stimulate our readers to think. That is precisely what our contributions will be about: We will work on the things that “just happen” to be on the layout or that seem to be an element of the surrounding design without any further plan.

But, it is precisely these appearances on the periphery that decide whether the photo depicted appears artificial and static, lively or even hectic, and in any case natural. What looks like a coincidence must not be a coincidence in the process of designing.

After an earlier thematic focus on plant design, we would therefore like to enable our readers to consciously perceive even the small things in our everyday life, to select them purposefully, and to reproduce them skilfully in miniature.

These fine things ultimately decide whether the eye lingers longer on the landscape and whether thoughts and memories are awakened in the viewer's mind.

**Märklin publishes the spring new products:**

As the first supplier for the Z gauge, Märklin announced its spring new products 2023 on 17 January. Among them, in addition to the already known Insider annual model with a newly designed V 60 and the Sgns 691 container car, there are two more new product body designs.



The diesel shunting locomotive labelled as V 60 334 (item no. 88651) is one of the moulded novelties from Märklin's spring new product announcements. Photo: Märklin

The first impression seems balanced in terms of epochs and railway administrations. To be found among the colour variants are long-standing desired models, sensible gap fillers, as well as, those models that hardly generate any interest.

We will present them in detail, together with the new products of all other manufacturers, in the February issue. In it you will also find background information on the models that are not explicitly mentioned in the product descriptions or could be carelessly overlooked.

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# Trainini

German Magazine for Z Gauge



The Spring 2023 New Products

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Z Gauge Exhibition in Bad Schwartau  
A Piece of History in a Model

## Introduction

Dear Readers,

We had just been discussing and deciding which models and works should be honoured for the past year. Only one month later, we are now looking ahead to the future.

As with every year, there was a huge amount of work to be done: brochures had to be reviewed, websites had to be checked, manufacturers had to be contacted and, finally, we had to interview some of them face to face.



**Holger Späing**  
Editor-in-chief

Groundhog Day: there still seem to be vendors who perhaps don't want to advertise and sell their products at all. Information? Not a thing! I find it sad when someone apparently does not even consider it necessary to reply to an enquiry.

Several great vendors who have done good information work over the years have withdrawn because it was not financially worthwhile to serve the Z gauge – most recently the Panzer-Shop.nl. We were able to personally assess the quality of the products, and do not find it understandable that they did not have sufficient interest.

However, we consider whining about a lack of customer interest to be unjustified if the officially offered communication channels are not even monitored and served. If you do not answer customers and the media, you should not be surprised if they turn away from you. In such cases, I cannot offer any sympathy for economic failure.

Why am I writing this? The answer is simple: to interpret such cases as rare exceptions, their number is too high after all. Quite a few small-series suppliers seem to be artists at best, but by no means commercially active businessmen.

But, I don't want to complain either and promise you a detailed report with exciting spring new products. We Zetties can expect some great models, smart ideas and also two real innovations. So, even in year 1 after our big anniversary, things are moving fast!

During our visit to Nuremberg, however, we couldn't help but notice that the time of model railways at the International Toy Fair will probably come to an end for good very soon. I'm not quite sure yet how I should interpret this personally: somehow such a fair seems outdated in our market, but on the other hand, a lot can be presented there in a short time and in a small space. And new suppliers also have the chance to introduce themselves to interested retailers.

Certainly, fixed distribution channels play a much smaller role in Z gauge, because in our scale, what constitutes individuality runs through direct channels. And, with that, I touch on another topic of this edition: We report, by the way, also in parallel on **Trainini TV**, on the successful Z gauge exhibition in Bad Schwartau.

This model railway exhibition was a showcase and an expression of creativity and individuality at the same time. Enjoy reading and admiring the photos!

Sin-Z-erely,

Holger Späing

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We thank Jan Tappenbeck for his contribution to the exhibition report.

Date of publication of the German language version of this issue: 24 February 2023

### Cover photo:

The diesel locomotive DE10 1099 of the Tobu Railway is as-signed to rail service and is pulling a passenger train today. Its attractive livery harmonises well with the green of the land-scape. Photo: Rokuhan



## The Douglas DC-6B from Herpa **An Aircraft made History**

*The Douglas DC-6B became an important landmark in the history of civil aviation. With the Pan Am colours, it also bears the livery of a true icon in this case. This combination results in an aircraft that was also significant in German history at the Iron Curtain. We recognise this with a new Herpa model.*

The four-engined Douglas DC-6 marked a turning point towards modern pressurised-cabin aircraft, but still belonged to the era of piston engine propulsion. Its further development into the DC-7 finally made it the last Douglas passenger aircraft with piston engines.



Loosely based on a historic photo on the Herpa product packaging, this shot shows the Douglas DC-6B of Pan American World Airways from above.

Manufactured from 1946 to 1958 in four different versions, it was the first Douglas Aircraft Company aircraft to have a pressurised cabin. This enabled it to replace its predecessor, the DC-4, which we have also presented here before, on long-haul and overseas routes.

It was in direct competition with the Lockheed Constellation, but was distinguished by its high efficiency and reliability. This became a sales success, which is reflected in over 700 aircraft of this type, some of which are still in service today in South Africa and South America.



As a model, the aircraft commemorates a turning point in the history of technology: it was the first Douglas model to receive a pressurised cabin and (almost) marks the end of the piston engines. Photo: Oleksiy Mark

The DC-6B version being presented today is closely related to the DC-6A cargo version, which was created in 1949. Like it, the -6B is an extended version that has to become the most successful of the aircraft family. It differs from its sister version by the cabin windows, the omitted cargo doors and the omission of the reinforced cabin floor.

It was designed for 54 passengers, but with maximum seating it could even carry 102! Pan Am introduced an "Economy Class" for the first time with the "Super-6" version, which led to cheaper fares with narrower seating and a reduced number of seats on board, today a standard in air travel.

The first flight occurred on 2 February 1951, and the total number of this version reached 288. The choice of a Pan Am example for today's model presentation was not made by chance. For one thing, Pan American World Airways was an icon of international air travel until its demise in 1991.

Furthermore, this type of aircraft was not only a landmark for routes originating in the United States, but also a special aircraft for Germany in particular. During the years of German division, the western part of Berlin could only be served by airlines of the three Allies, and Pan Am was the leading airline of the three.

Pan Am also operated twelve DC-6Bs from Berlin-Tempelhof on the routes leading into Germany ("Intra German Services"), which thus also became symbols of freedom. The airline's total fleet was much larger, with 54 aircraft of this version alone. They were purchased from 1952 onwards.

The propeller-driven aircraft had a range of 7100 km, which was significantly greater than that of the first jet aircraft and thus led to time advantages on many routes despite the lower cruising speed.

continues on page 7



The typical line of the aircraft betrays the time in which it was created. Together with the distinctive engines, the silhouette undoubtedly speaks of the early years after the Second World War, which belong to the "golden age of aviation." Photos: Oleksiy Mark



The “changing of the guard” at the airline, which once operated under the name PAA, took place in 1968. The Douglas DC-6B was replaced by the Boeing 727 on Berlin routes, and by the 720B on intercontinental routes.

## The Herpa Model

Herpa has chosen the Douglas DC-6B with the US registration N6523C as the prototype for its new metal model. With the christened name “Clipper Betsy Ross” (art. no. 572187) it was in service with Pan American World Airways.



The front view of the Douglas DC-6B is also elegant and impressive. Photo: Oleksiy Mark

As explained in the prototype section, it is equally suitable for layouts based on German and North American models. Its dimensions are sufficiently close to the scale of Z gauge, and do not impose the impression of an obvious size deviation.

Like almost all aircraft of that time, the “Super-6” remains within a size range that large layouts with an airport section allow. Of course, an aircraft taking off can also be suspended on silk threads above the layout action and thus become part of the sky scenery.

That is why we took a closer look at this model. Because of its cast metal construction, the model weighs

### Dimensions and data on the Douglas DC-6B (“Super-6”):

|                      | Prototype                              | 1:200    | 1:220    | Model    |
|----------------------|--|----------|----------|----------|
| Length               | 32,18 m                                | 160,9 mm | 146,3 mm | 160,4 mm |
| Wingspan             | 35,81 m                                | 179,1 mm | 162,8 mm | 178,3 mm |
| Height               | 8,86 m                                 | 44,3 mm  | 40,3 mm  | 45,1 mm  |
| Max. Take-off weight | 48.534 kg                              | ---      | ---      | 214 g    |
| Cruising speed       | 509 km/h                               |          |          |          |
| Engines              | 4 x Pratt & Whitney R-2800 Double Wasp |          |          |          |
| Power                | 4 x 1.760 kW / 2.400 PS                |          |          |          |
| Propeller            | Three blades                           |          |          |          |
| Construction period  | 1951 – 1958                            |          |          |          |
| Number produced      | 288 units                              |          |          |          |
| Retired (Pan Am)     | through 1968                           |          |          |          |

a lot and is noticeable in the hand. This is almost indifferent for our purposes, but it gives a good stand in case the layout is knocked sideways.

Our scale table shows the distance between the conversion scale and the calculated target values for the Z gauge. Mathematically, the distance is once again significantly greater than the perception by the human eye: the model does not appear too large to observers.

It is realised in the way Herpa is known for: Many details have been reproduced

by printing, which can be found in several colours on the cleanly painted fuselage. The underside of the fuselage and the wings are predominantly reproduced in a shiny metallic finish, which follows the typical appearance of many airlines at that time.

On the wings we find two matt stripes each, which run lengthwise at the height of the engines on the upper and lower side. The elevators at the tail are attached and are the only plastic parts on this miniature.

The parts of the landing gear and the opened flaps, between which they would disappear after take-off, are also made of metal. Compared to the prototype, only the sloping support struts at the rear undercarriage area look a bit too rough, despite the material thicknesses required for this. The tyres on all wheels are reproduced in the best possible way using rubber.

Fine engravings are not to be expected on the fuselage of an aircraft, because it is shaped like a smooth tube in order not to offer any airflow resistance. In addition to the landing gear, we have already looked at, we have to look for the flaps of the wings and tail units as well as the engine.



**With the exception of the attached antennas, the fuselage offers only a few details. To discover and admire them, you need to look at the tail units and the elaborately reproduced undercarriage. Photo: Oleksiy Mark**

There we also find what we are looking for. The engines in particular deserve a commendatory comment: not only are the outer contours correctly and strikingly reproduced. We also find the features belonging to the radial engines in the narrow spaces between the propeller hubs and the outer casing.

The four three-bladed propellers and two antennas on the upper side of the canopy are added details. The front one has the shape of a boomerang and was seen in this design on many aircraft of the fifties. Consequently, we could not find any deviations from the prototypes.

continues on page 10



The "Super-6" also flew for Pan Am from Berlin-Tempelhof until 1968 in "Intra German Service" and thus also wrote a piece of German post-war history. Photos: Oleksiy Mark



This also applies to a feature that astonished us greatly: We initially looked in vain for the red (left) and green position lights (right) on the wings. However, prototype photos also proved that these are integrated into the wing tips in a relatively small form.

Landing lights are not to be found on the model. According to our recollection, Douglas had hidden them under the flaps of the landing gear in such a way that they sit inside and fulfil their function as soon as the landing gear has been extended by the pilots.

The only other signal light we can make out sits centrally on the top of the vertical stabiliser. The flashing signal is visible from all sides, but the pad printing is only on the upper side. In the side view, therefore, only its “cone” can be seen in white basic colour, contrary to the original.

Otherwise, there is nothing to criticise about the paintwork in original colours and the elaborate printing. Tiny windscreen wipers can be seen on the wide, inner cockpit windows as well as the fine and small christening name on the cockpit sides, for example.



The crew boards the aircraft and prepares it for take-off. The passengers are sure to be brought in soon, because the luggage loading is already in progress, announcing the not-so-distant take-off to new destinations.

In the same breath we should also mention the registration in the rear area, the lettering “Super 6” only a little lower in the light blue window band, the registration abbreviation 23C on the front landing gear doors and the black surfaces of the propeller blades. Separately printed frames are only found in the area of the emergency exits.

Company lettering on the sides and wings, along with the Globe logo on the tailplane, unequivocally identify the operator of this aircraft model. All in all, we have another attractive 1:200 scale aircraft model in front of us. It is to be hoped that many aviation enthusiasts will appreciate it in their collection and will know how to assign it a place on the layout, perhaps also in alternation with other models.

Model Manufacturer:  
<https://www.herpa.de>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

## Die Zeit des Turnusverkehrs **Nächster Halt: Urlaub**

***Mit dem Zug statt mit dem Auto in den Urlaub zu fahren, war einst selbstverständlich. Die große Zeit von Urlaubszügen begann mit dem Wirtschaftswunder. Doch Massenmotorisierung und aufkommender Flugverkehr sorgten zunehmend für Konkurrenz. Seit 1993 ist die Ära von Reisebürosonderzügen beendet, ein interessantes Buch arbeitet deren Geschichte auf.***

Armin Gärtner  
Reisebürosonderzüge – Urlaub von Anfang an  
Die Geschichte des Turnusverkehrs von 1948 bis 1993

Eisenbahn- und Heimatmuseum Erkrath-Hochdahl e.V.  
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Es ist verblüffend, wie wenig Literatur es zum sogenannten Turnusverkehr der Deutschen Bundesbahn gibt. Mit dem Zug in den Urlaub zu fahren, galt als selbstverständlich, kaum dass die Reiselust der Deutschen im Wirtschaftswunder erwacht war.

Auch heute ist das Thema durchaus noch präsent, auch wenn viele keinen Gedanken daran verschwenden, wie er erfunden, geplant und organisiert wurde. Und doch lieben Modellbahner die Fahrzeuge, die für diese Art des Reisens und für Fernweh standen. Auf vielen Anlagen finden wir Relikte jener Zeit.

Das allein war es schon wert, dieses Thema ins Bewusstsein geschichtsinteressierter Eisenbahnfreunde zu rücken. Immerhin bietet ein Buch wie das nun vorliegende die wohl einmalige Chance, Wissenswertes zu den Zügen und Wagen zu erfahren, die so gern auf der Modellbahn gezeigt werden.

Interessant sind in diesem Zusammenhang sowohl die Reisebürosonderzüge, die ausschließlich aus Liegewagen und einem Speisewagen gebildet wurden, als auch jene Wagen, die ausgestellt und einzeln oder in Gruppen an ihre Zielorte gebracht wurden. Nicht vergessen werden dürfen die einst noch alltäglichen Kurswagen.

Das nun besprochene Werk gibt den dafür erforderlichen Überblick, aus welchen Anfängen und Ursprüngen sich der Turnusverkehr entwickelte. Der Rückblick erstreckt sich bis auf die Zeit vor dem Zweiten Weltkrieg, als der Ausflugsverkehr aufkam und an Beliebtheit gewann.

Abrupt durch den Krieg beendet, setzte 1948 zaghafte wieder ein langsam wachsendes Interesse ein. Der Erfolg der fünfziger und sechziger Jahre war dennoch kein Selbstläufer, wie hier herausgearbeitet wird.



Altbau-Reisezugwagen boten nur wenig Komfort und ließen die Fahrt zum Erholungsort zur Strapaze werden.

Den Durchbruch brachte erst die Erfindung des Liegewagens, der sich klar vom Schlafwagen abgrenzt. Um ihn musste aber hart gekämpft werden und der jungen und chronisch klammen DB ein Konzept auch finanziell schmackhaft gemacht werden.

Deshalb wird dieser Aspekt besonders ausgiebig aufgearbeitet. Eng verbunden ist dieser Teil der Geschichte mit dem Namen Dr. Carl Degener: Er hatte die Idee zum Liegewagen und ihm fiel es zu, diese in den Verwaltungsapparat der Bundesbahn zu tragen.

Dort stieß er nicht auf offene Ohren, denn von einem „Laien“ wollten sie sich keinesfalls in ihr Geschäft hineinreden lassen. Doch er schaffte es, die Beamtenobrigkeiten von seinem Konzept zu überzeugen und sie für seine Idee zu gewinnen. Dies klappte wohl nur, weil sich die interessierten Reiseveranstalter auch finanziell stark engagierten, als es ans Beschaffen der neuen Wagen durch die DB ging.

So entstanden früh Kooperationen, die für besondere Strukturen sorgten. Zu nennen ist beispielsweise die Marke Touropa, die SBG (Sonderzug-Betriebsgesellschaft) als ausführendes Unternehmen und die besondere Rolle des bahneigenen Deutschen Reisebüros (DER). Bis heute ein Begriff ist auch der Alpen-See-Express, der ebenfalls eng mit Bahnpauschalreisen verbunden ist.

Armin Gärtner ordnet Geschichte, Beteiligungen und Strukturen, stellt alle wichtigen Unternehmen und Verbände vor und schlägt auch die Brücke zur Touristik-Union International (TUI) – bis heute Europas größter Reiseveranstalter. Sein Ursprung liegt im wachsenden Konkurrenzdruck durch die zunehmende Flugtouristik, bekannte Unternehmen wie Touropa, Scharnow-Reise und Hummel-Reisen gingen in diesem Verbund auf.

Gleichzeitig wuchs das Interesse der DB, selbst direkt an Bahnpauschalreisen zu verdienen und nutzte sein in Ameropa umbenanntes Unternehmen, um den etablierten Anbietern das Wasser abzugraben. Die TUI schickte konsequent, aber leider zu spät, ihren TUI-Ferienexpress ins Rennen. Damit sind wir in der letzten Phase der Turnuszüge angekommen, die 1993 ein abruptes Ende fand.

Doch damit sind die Inhalte längst nicht erschöpft. Auch Berichte von Reiseleitern und Begleitern fanden Eingang, denn sie erlauben auch einen Blick hinter die Kulissen. So lesen wir kleine Dramen, die sich in den Zügen abspielten, von Schikanen ostdeutscher Grenzpolizisten oder Feiern in Dienstabteilen mit polnischen Kollegen.

Diese zutiefst menschliche Seite gehört dazu, um sich in den Zeitgeist und in die Atmosphäre vor Ort hineinzusetzen. Sie machen ein gutes Werk erst rund. Schwierig war gewiss auch die Bildauswahl, die sich nicht auf Bilder von Wagen und Zügen beschränken kann. Auch diese Aufgabe wurde hervorragend bewältigt und alle Abbildungen sind tadellos reproduziert worden.

Vermitteln konnten wir nun hoffentlich, wie viel Stoff und wie viel Spannung in diesem vielen noch unbekanntem Thema stecken und warum wir dieses Buch deshalb so dringend empfehlen möchten. Es ist ein wahrer Geheimtipp und fand sogar eine zweite Auflage. Da diese schon fast vergriffen ist, sollten alle, die nun Gefallen am Thema gefunden haben, nicht lange warten und zugreifen!

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## Deutsche Kriegslokomotiven Geschichte aufgearbeitet

**Mit Alfred B. Gottwaldt ist im Sommer 2015 einer der bekanntesten und anerkanntesten Eisenbahnhistoriker von uns gegangen. Seinen Ruhm begründete er mit seinem Erstlingswerk „Deutsche Kriegslokomotiven“, das wir hier besprechen werden. Es handelt sich also nicht um ein neues Werk, aber einen bedeutenden Klassiker deutscher Eisenbahngeschichte, der endlich wiederaufgelegt worden ist.**

Alfred B. Gottwaldt  
Deutsche Kriegslokomotiven 1939 bis 1945

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Der Name Alfred B. Gottwaldt dürfte unseren Lesern bekannt sein. Er leitete bis zu seinem Eintritt in den Ruhestand 2014 die Abteilung Schienenverkehr im Deutschen Technikmuseum Berlin (bis 1996 „Museum für Verkehr und Technik“).

Diese Funktion übte er seit dem Entstehungsjahr 1983 des Museums aus. Die Eisenbahn-Ausstellung „Züge, Loks und Leute“ trägt seine Handschrift und wurde als eine der ersten des Museums 1987/88 eröffnet. Besonders intensiv hat sich der Autor zeitlebens mit der Eisenbahn im Dritten Reich und ihrer Rolle in Politik, Verkehr und Massenmord auseinandergesetzt.

Schauen wir auf sein Leben zurück, dann passt der Ausdruck „Verfasser zahlreicher Bücher und Veröffentlichungen“ auf ihn besser als wohl jeden anderen Eisenbahnhistoriker. Viele Jahre hat er auch bei Transpress veröffentlicht, wo er folglich auch seine Spuren hinterlassen hat. Ebenso wirkte er an mehreren Ausstellungen mit, die die Rolle der Reichsbahn bei den Deportationen 1941 bis 1945 aufarbeiteten.

Von sich reden machte er erstmals 1970, als er im jungen Alter von gerade mal 21 Jahren sein erstes Buch bei der Franck'schen Verlagshandlung Stuttgart veröffentlichte: Deutsche Kriegslokomotiven 1939 bis 1945. Genau dieses Werk hat Transpress nun in seiner Klassik-Reihe wiederaufgelegt, weil es unverändert am Markt nachgefragt wird.

Was zeichnet dieses Werk aus? Wie aktuell ist es heute? Und wie geht der Verlag damit um? Drei Fragen sind hier beispielhaft genannt, die wir mit unserer Rezension beantworten möchten. Immerhin spielt das Thema Kriegslokomotive auch auf der Modellbahn der Spur Z eine Rolle: Die Baureihe 52 gehört ebenso zum Märklin-Formenbaukasten wie die nie gebaute Baureihe 53, die ein Entwurf von vielen für die dritte Kriegslok war.

So beschäftigen diese Dampflokomotiven also durchaus die Gemüter der Modellbahner und wecken Interesse an den Hintergründen ihres Entstehens. Für Technikbegeisterte ebenso spannend ist die mit

ihnen vollführte Abkehr von der „Einheitslok 1925“, an der Richard Paul Wagner dogmatisch bis zum erzwungenen Pensionseintritt festhielt. Dies ist ein Teil des vorliegenden Werks, der in allen Facetten hinterfragt und beleuchtet wird.

Wer die Quellenangaben des Autors verfolgt, der ahnt, welche umfangreiche Forschung der Verfasser zuvor betrieben hat. Obwohl vieles verloren ging oder zum Zeitpunkt seiner Arbeit wegen geschlossener Grenzen nicht zugänglich war, ist das Ergebnis wahrhaft beeindruckend. Es verwundert nicht, dass dieses Buch einen Paukenschlag bedeutete und zum Standardwerk wurde. Und Alfred B. Gottwaldt war zudem der Erste, der sich diesem Thema überhaupt zuwandte.

Neben seinem Vorwort aus der letzten eigenen Überarbeitung des Jahres 1996 hat der Verlag auch eine ausführliche Einleitung vorangesetzt. Sie würdigt Leben und Wirken des bekannten Bahnhistorikers und geht auch kurz auf den aktuellen Forschungsstand ein. Genannt werden auch Quellen, die neu entdeckt waren, aber vom Urheber selbst wegen seines plötzlichen Todes nicht mehr ausgewertet werden konnten.

Wichtig erschien es den Entscheidungsträgern, das Werk des Autors zu bewahren und nicht von einem Dritten überarbeiten zu lassen. Das Fortschreiben und Ergänzen der Geschichte wird also anderen Autoren in eigenen Veröffentlichungen überlassen sein.

So liegt nun ein Klassiker der deutschen Eisenbahnliteratur in unverfälschter Weise vor uns. Er ist gewiss nicht mehr durchgehend aktuell, aber fundiert recherchiert, leidenschaftlich und verständlich geschrieben und hinterfragt die Geschichte und ihre Handelnden kritisch.

Diese Aufgabe dürfte umso schwieriger gewesen sein, als dass in der Zeit, in die wir im Buch eintauchen, Propaganda und Zensur zum Alltag gehörten. Jedes Schriftstück musste mit Blick auf die Motivation seines Verfassers, Zweck und allgemeine Zugänglichkeit auf seine Aussagekraft und den Wahrheitsgehalt überprüft werden.

Die Aufgaben, die es zu bewältigen galt, waren gigantischen Ausmaßes: In erzwungener Zusammenarbeit musste die Deutsche Reichsbahn mit Lokomotivfabriken, dem Rüstungsministerium und der Wehrmacht zahlreiche Probleme lösen und eine anspruchsvolle, aber leistungsfähige Dampflok liefern.

Gefordert war, deren Produktion auf 500 Maschinen pro Monat zu steigern. Dieses Ziel wurde zwar früher als gefordert erreicht, aber eben auch nur für kurze Zeit. Zum einen erledigte sich der krasse Lokmangel mit dem Verlust der eroberten Gebiete ab 1943 schnell wieder, zum anderen war die für knapp drei Monate erreichte Rekordzahl auch eher eine Propagandablase als Ausdruck nachhaltiger Leistungsfähigkeit.

Um die Zusammenhänge zu verstehen, bedarf es einer umfassenderen Betrachtung der Zeitumstände. So spielen die einzelnen Verkehrsträger und ihre (perspektivische) Bedeutung eine Rolle, das starke Fixieren der Regierung auf den Lastkraftwagen – erinnert sei an die strategische Bedeutung des Autobahnbaus – und ebenso Verlauf und Schwerpunkte der Rüstung.

Erst das Scheitern des Blitzkriegs wies der Eisenbahn eine kriegsentscheidende Rolle bei Nachschub und Versorgung zu. Entsprechend schnelle Lösungen waren plötzlich gefragt. Alfred B. Gottwaldt hat alle diese Aspekte geordnet und strukturiert in Rolle, Entwicklung und Bau der Kriegslokomotiven eingeflochten. Übrigens beschränkt sich der Begriff Kriegslok bei weitem nicht auf die Baureihen 52 und 42. Hier erwarten den Leser also noch viele weitere Hintergründe und Erläuterungen.

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HRT Informationstechnik GmbH  
Kasseler Straße 7  
34431 Marsberg

Tel.: 02994-9889 60  
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E-Mail: info@1zu220-shop.de  
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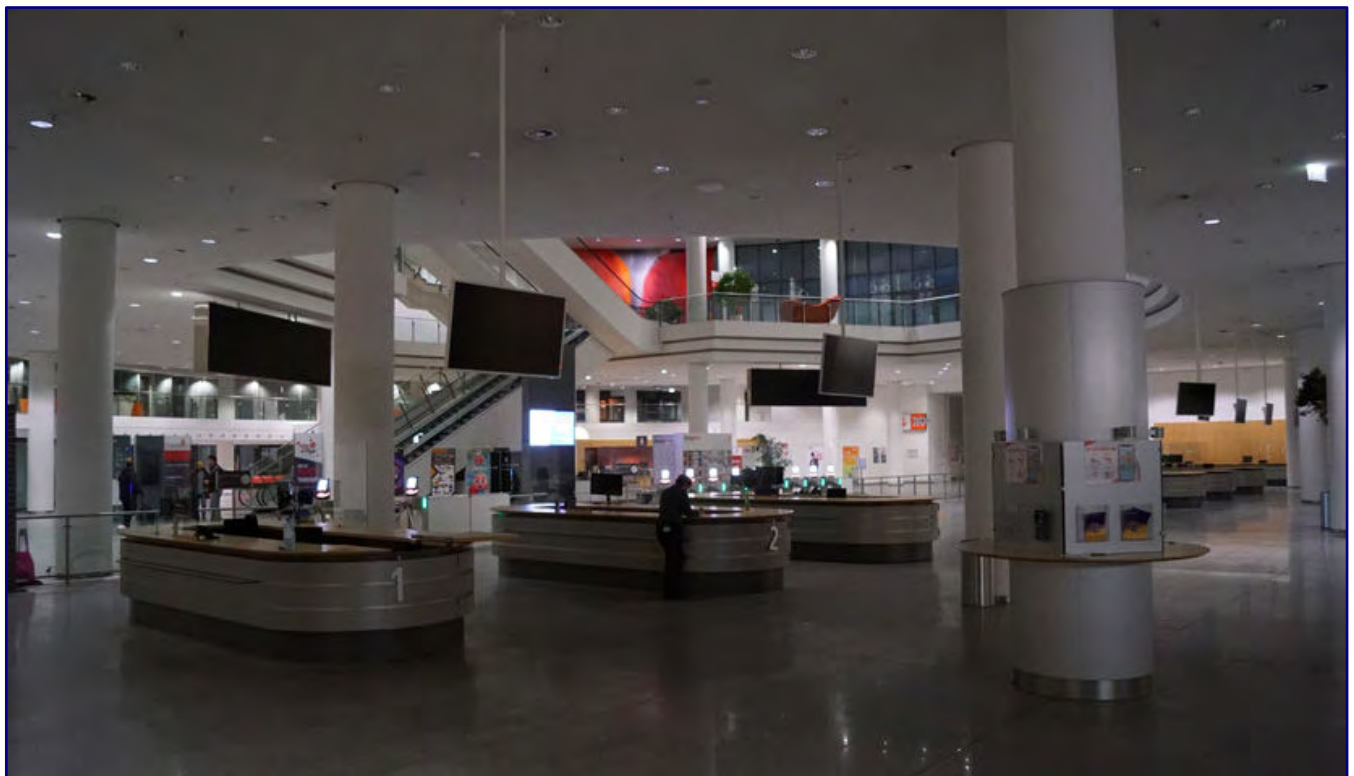
## Spring new products 2023 for Z gauge **Nuremberg almost insignificant?**

**At the Nuremberg International Toy Fair from 1 - 5 February 2023, model railways – once a key category since the fair's founding days, were almost no longer noticeable, with just four exhibitors from the Z gauge sector present. As expected, the fair's management celebrated the success of its leading fair, but we ask ourselves whether this might have been the last edition at which the model railway industry was even represented.**

Comment by Holger Späing. For many years, some model railway manufacturers had fought to open the fair to private individuals on Saturday and Sunday, when fewer professional visitors come to Nuremberg. In 2023 the time had come, but former proponents of the initiative still stayed away from the exhibition.

After a forced break of three years, the International Toy Fair presented itself in a pitiful state and a swan song like fashion in Hall 7A. The suppliers from the model railway industry who took part this year could be counted on one's fingers, for Z gauge there were only four.

Whereas interested small exhibitors had once been turned away citing limited space, an area originally designated for small-series manufacturers was now completely occupied with tables from the hall's catering facilities.



**Dusk in Nuremberg: Was that it for the model railway sector at the International Toy Fair? Unfortunately, it is clear that only a few dealers found it worth their while to come here in 2023.**

At no point during our visit would the empty aisles in Hall 7A have come into conflict with the minimum distances between individual visitors known from Corona times. Their great width provides further food

for thought. And those who walked down the side aisles towards the window front to the end came across huge open spaces that could not be concealed even with seating.

Every exhibition struggles to restart successfully after the pandemic. But in Nuremberg a trend has now rapidly accelerated that has been observed for at least ten years. The question now arises as to whether the model railway industry can and really wants to do without such an exhibition. In any case, there can be no “more of the same”.

That's why the official conclusion of the so-called “Open Day Model Railway & Model Making” only makes me shake my head in disbelief. Have those responsible still not understood? Or do they not care about model railways? Do they no longer see it as an opportunity to attract reports in the daily press?

The two visitor days for private individuals are officially described as highlights. Well, we interpret “More than 3,000 consumers” as the exact opposite. Furthermore, no information is available or shared on how far they travelled and how reasonable they consider the total cost of the visit to be.



**A cheerful end to a busy day at the fair in a communicative and creatively inspiring atmosphere: Noch Managing Director Sebastian Topp (3rd from right; in white shirt) delighted his customers and guests with self-mixed cocktails.**

We are quite sure that lost exhibitors will weigh their 2023 sales well against their expectations and are rather likely not to return next year. For the remaining ones, the enormous costs of such an event are then all the more questionable: “Imagine it's Nuremberg and nobody will come!”

But there are also good things to report from the International Toy Fair, and that is not limited to the new products on show. On Thursday after the fair closed (until 10 p.m.) there was an invitation for a “Red Night”.



Exhibitors were allowed to receive remaining visitors at their stands, offer drinks and a snack, thus creating the opportunity for exchanging ideas in a pleasant atmosphere. Märklin and Noch also took part in order to foster personal contact, as well as business. This did us good, was well received and also made our visit complete.



The Spanish paint manufacturer Vallejo presented itself with a modern and refreshing brand appearance at the Toy Fair in Hall 7A.

### +++ Acrylicos Vallejo +++

The first thing that caught the eye at the Spanish paint manufacturer's stand was a new brand identity. A turquoise colour now serves as the company's identification colour, and the company logo has also been modernised.

This is intended to signal a generational change in the management of the company.

A fresh breeze is therefore also evident in the new products. There is now a new supplier for the high-quality brushes in the range. Consequently, they are also available in a neutral anthracite with a matching lacquered ferrule, which specifically emphasises the brand's characteristic colour.

In this way, Vallejo wants to draw attention to the fact that the brush quality has been improved, yet again.





New in the range are the “Xpress Colors,” which belong to the “Game Colors” series, but have a viscosity suitable for airbrushing. Applied very thinly, they still appear transparent, but with a further layer of paint they cover excellently. This also allows for targeted effect work with the airbrush.

In addition, the new paint bottles are now made of a crystal-clear plastic, which makes it easier to assess the radiance, effect and other properties of the paint contained in them. When the bottle is opened for the first time, the new cap makes a sound to confirm that the product is fresh.

[www.acrylicosvallejo.com](http://www.acrylicosvallejo.com)

### +++ Álvaro Cortes +++

In this year's spring survey, Álvaro Cortes is rather reserved. This is due to an ongoing switch to crystal-clear epoxy resin, with which he casts his models. This is intended to make them look more realistic, because then the window surfaces can remain untreated and offer a clear view into the vehicle.



The current product range revision involves a switch to crystal-clear epoxy resin for many car models. Photo: Álvaro Cortes

The new products to be listed were created entirely in 3D printing. These are mainly the Suzuki Jimny, the Range Rover classic as a three- and five-door model (3- and 5-door), the McLaren GT, the Mini Moke (off-road utility vehicle similar to the Austin Mini) and some US vintage vehicles.

He offers the Porsche 991/992 as a coupé, cabriolet, targa and touring car. Two small-scale workhorses, whose photos are very familiar to us, look downright cute: the modern Still forklift truck and the Bobcat compact loader used on small construction sites.



The small truck Renault Midlum comes in five different variants. Photo: Álvaro Cortes



In addition to some historic US vehicles, Still forklifts and the Bobcat compact loader are also on offer, as well as rolling tyre drums, gas cylinders, crates and pallets loaded with cement bags (photo above). The new car transport trailer can be combined with the Mercedes-Benz Actros, Volvo FH and Scania tractors (photo below). Photos: Álvaro Cortes

To ensure that they also find suitable tasks and environments, miniaturised rolling hoop drums, gas cylinders, crates and pallets loaded with cement sacks are also available as suitable cargo.

The commercial vehicle range is expanded by three car transporters of different brands: Mercedes-Benz Actros, Volvo FH and Scania. The Renault Midlum is a mini truck that is fitted with various superstructures. These are the platform of a tow truck, an open loading area and tank containers for Avia, Aral, and BP.

[alvaro\\_mcortes@hotmail.com](mailto:alvaro_mcortes@hotmail.com)

<https://www.facebook.com/My-Z-scale-models-1648628058694887>

### +++ Ammo by Mig Jimenez +++

Some time ago, this Spanish paint manufacturer launched a programme that, under the product label "Rail Center," specifically lists colour shades for painting railway models, for weathering them, or for designing landscapes.

This is a specified excerpt from the overall programme. The paint packs for the railway sector differentiate, for example, between locomotives and freight wagons, as well as countries and railway administrations, in order to be able to specifically take into account their individual painting schemes in one's own work.

Since we cannot provide an overview of the entire programme here, we recommend that interested parties browse through the range themselves.

[www.ammo.es](http://www.ammo.es)

### +++ Archistories +++

Shortly before the end of 2022, we were already able to report to our readers about Archistories' new products. Therefore, there is currently nothing new to add, although we can be sure that development and production will continue at the Hannover-based company.

[www.archistories.com](http://www.archistories.com)

[www.archistories-shop.de](http://www.archistories-shop.de)



The Hanomag K5 bulldozer (item no. 322.040) is painted in a typical orange. Photo: Artitec

widespread on German construction sites and many people from today's parents' generation will remember them from their youth.

[www.artitec.nl](http://www.artitec.nl)

### +++ Artitec +++

We find many beautiful products and ideas again when leafing through Artitec's new products brochure. But there is only one announcement for Z gauge.

Before we present them, we would like to say that our good dialogue with this manufacturer leaves no doubt that it will continue to work intensively in the Z scale segment.

The only reason for our reluctance to announce new products is the existing backlog from previous years. Artitec has the ambition to satisfy these first, before new dreams are awakened.

The Hanomag K5 open bulldozer (art. no. 322.040), painted in a bright orange colour, makes a very attractive impression.

This is a type of construction vehicle that used to be



### +++ Artmaster +++

An interesting discussion at the fair revealed that new products in gauge Z are being considered again. With foresight, it was suggested that the Märklin Days in September could be a suitable occasion to talk about new developments. We mentioned to this supplier our own ideas and suggestions that would go with their product range.

[www.das-kantoor.de](http://www.das-kantoor.de)

### +++ Atelier Dietrich +++

The individual landscape backdrop series from Atelier Dietrich are divided into modules and can be ordered in all gauges. For Z scale, the formats are 60 x 34 cm. Depending on the desired type of installation, the scenery prints can be delivered on different materials and optionally with matt protective foil.

The following additions and extensions determine the current range: For the Alpine panorama, there are new special modules that allow other configuration options. With a length of 11.37 m in 19 modules, this is the longest backdrop in the range.

The city modules have also been extended and now allow a continuation of the backdrop on the right-hand side from module 7 onwards. The popular "Obergrabern" series based on the Lower Austrian Weinviertel (total length 8.97 m) has been extended by four village modules.



Motifs from the backdrop series "Obergrabern", shown on the exhibition layout by Manfred Göppel (gauge 0). Photo: Atelier Dietrich

It can already be used from Era III onwards and can now also be combined into a 360° backdrop when module 15 is seamlessly connected to module 1 again. In this way, it can give a model railway room a complete appropriate and lovely-looking ambience.

[www.atelier-dietrich.at](http://www.atelier-dietrich.at)

### +++ Atlantic Case-Diorama +++

This supplier from Istanbul (Turkey) was a little surprise for us at the fair. They presented different, very appealing table showcases of different price ranges for 1:18 scale car models.

The dust-proof surface presented under an acrylic glass cover is also suitable for creating a small diorama in Z gauge, which can then also be set up in the home in a decorative ambience.



The table showcase model "Dieppe" (item no. 10152) is certainly a good example of an appealing piece that is also suitable for presenting a small Z gauge diorama in a living room. Photo: Atlantic Case-Diorama

Many materials and colours allow an individual choice to match one's home style and furniture. Depending on the choice of material, there are different prices and quality levels. We have selected the model "Dieppe" (art. no. 10152) as an example, which belongs to the inexpensive versions. It has a metal frame in which a black acrylic plate is inserted.

[www.atlantic-case.com](http://www.atlantic-case.com)



### +++ Atlas Model Railroad +++

The US-American supplier Atlas, who so far only has turnouts and flexible track in their Z gauge programme, has announced two new street lamps. Both are designed after models found in the United States.

The following models with LED bulbs are announced in packs of three: whip lantern, warm white (item no. 70090000) or cool white (70090001) and street lantern with double bracket, warm white (70090002) or cool white (70090003).

[shop.atlasrr.com](http://shop.atlasrr.com)

### +++ Avantgarde Modellbau +++

Based on his own design experience, Phillip Meyer has developed two complementary modelling compounds: a filler (coarse-grained) and a fine filler (fine-grained and earth-like). They are supplied ready to use and set slowly.

The idea behind these products is to build up a terrain from scratch instead of removing it from a hardening mass by cutting processes. This saves time and time for cleaning from the dirt that would otherwise be created. Nevertheless, carving with craft knives or engraving tools is of course possible at any time.



Photo: Atlas Model Rail-road.



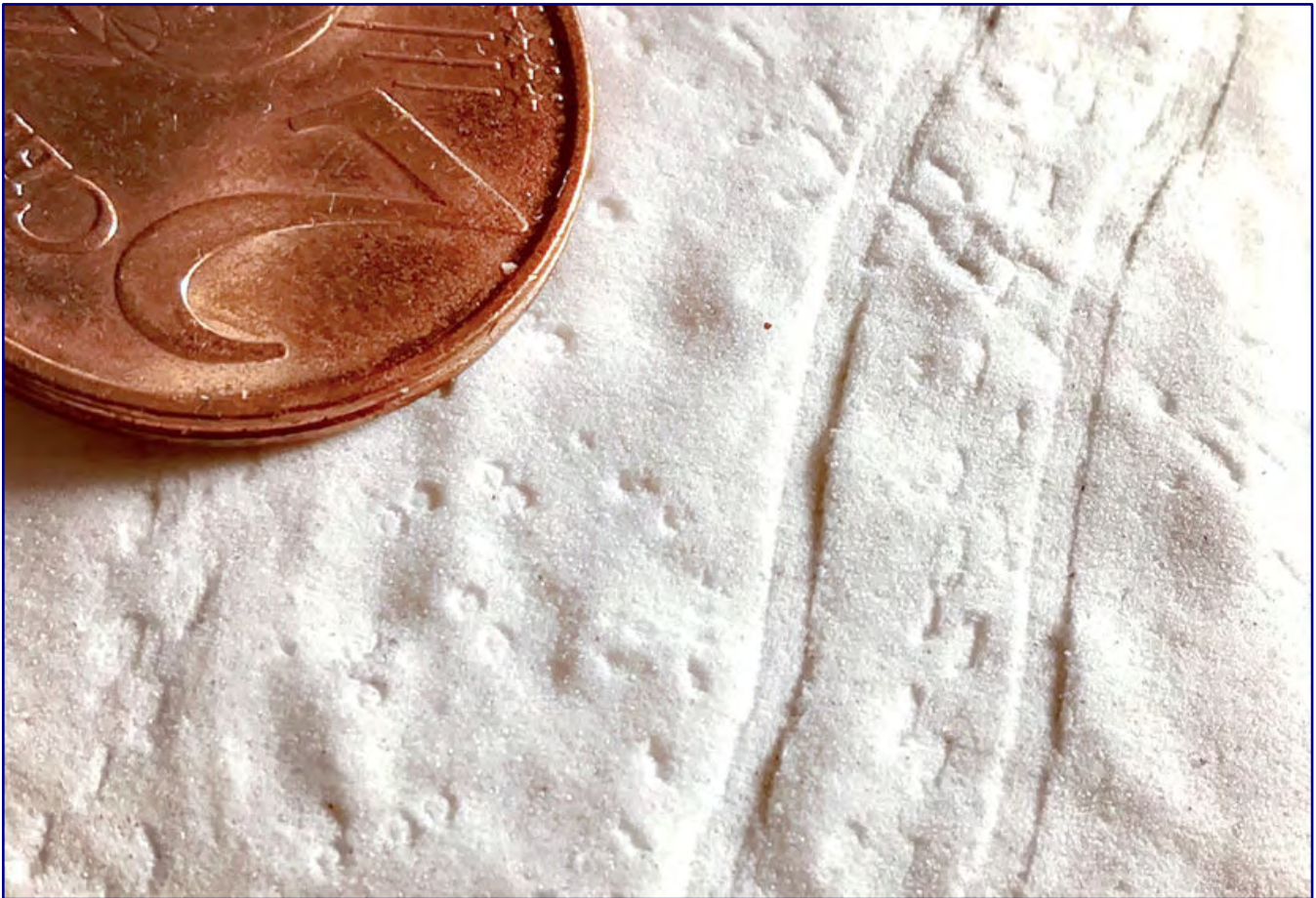
Half-timbered house and bare deciduous tree, both also new products, were treated with the company's own putty and icicles. Photo: Avantgarde Modellbau



The fine filler can therefore also be used as a rock filler. Important to know: Excess material goes back into the putty bucket and can be reused. Avantgarde Modellbau has also developed a new adhesive applicator for setting tufts of grass.

Also, very interesting is an extremely fine multi-purpose granulate which, when mixed with water and glue, becomes flowable and can then be used as snow, sand and plaster powder (for facade replicas).

It can be used in wintry landscapes with set, thin, thawed or slushy snow cover, but when spread dry it looks like fresh powder snow. Driving tracks or footprints can also be added. For sandy areas and muddy paths, pigment colour is added. On half-timbered facades, the mixed compound ensures that the beams do not rise up in a way that is contrary to the model, but that the compartments are filled correctly. At the same time, they lose their plastic sheen.



Incorporated gauges of horse, man and car in the new filler, which is mixed from the multi-purpose granulate and is used here to recreate snow. Photo: Avantgarde Modellbau

In the area of new vegetation material, bare deciduous trees, shrubs, bushes as well as wild herbs are in the works to complement the already mentioned programme in a wintry and meaningful way. This also fits in with various firewood piles and pasture fence posts. The latter are modelled on the widely used rough log stakes. For the winter landscape, small icicles to scale are also still in development.

The aforementioned snow powder provided the initial spark for superimposed Faller and Vollmer half-timbered buildings, but also their own developments. This is aimed first and foremost at half-timbered buildings, which can then appear in a variety of selectable colour combinations.

On a Kibri basis, various variants are planned that differ in shape and colour from the original kit: new framework and basic colour, covers for existing window openings, new windows, window frames and shutters are just as much part of the repertoire here as new roofs, plinths and side walls or dormer windows. The supplier also makes use of various laser-cut elements and resin castings.

Missing from the 1:220 scale so far are crooked and crooked half-timbered houses that once followed the specifications of the beams used. This is where the announced new developments come in, among which a medium-sized farmhouse/residential building makes the start.

In this case, all the walls and beams are crooked and uneven, the windows adapt to this, are of different sizes and also offset in height, some of the compartments are missing the plaster, revealing the interior with broken stones and woven branches.



The small natural stone shed exists as an original model, which will only be transferred to series production if there is sufficient demand. Photo: Avantgarde Modellbau

This new product is designed as a resin casting kit. The body of the building is a single, pre-coloured casting to which the roof, windows and small parts made of laser cut parts are to be attached. A southern French farmhouse is also envisaged, which is to be used in Provence as well as in Italian landscapes.

A small natural stone shed already exists as a master model, as it is often found in the Swiss Ticino, but could just as plausibly be located in the south of France. However, actual production will depend on interest amongst potential customers: The last two projects in particular require a sufficient number of advance orders in order to be able to assess the market response and avoid economic risk!

[www.avantgarde-modellbau.de](http://www.avantgarde-modellbau.de)  
[motewe\(at\)gmx.de](mailto:motewe(at)gmx.de)



## +++ Azar Models +++

Two new colour variants of the covered SNCF freight wagon G 4 are currently listed on the manufacturer's webpages, also marked as already available: These are a light grey version with markings from Sernam (art. no. W02-SNG) for era VI and a green version from SNCF Fret (W02-FR), which is located in epochs IV/V.

The first building kit from the French small-series manufacturer is now also to be released: The three-tier station building (GAR701) follows a typical Eastern French architectural style and is suitable for replicating smaller or medium-sized railway stations.



The three-door station building (item no. GAR701; photo left) is accompanied by the matching goods shed (HAL701; photo right). Photos: Azar Models

The kit is made of printed and laser-cut cardboard, so there is no need for colour treatment. The building stands on a base plate which reproduces the pavement (street side) and the platform towards the track. Interior lighting can be installed and three different place name signs are included for individual selection.

The small station can be complemented with a goods shed (HAL701). Both kits are exclusively available from Régions & Compagnies in France.

[azar-models.com](http://azar-models.com)

## +++ AZL (American Z Line) +++

American Z Line does not follow a fixed release schedule and announces its current new products throughout the year, usually every two weeks or so, which are then ready for delivery. Currently, this is the New Haven pullman-green ALCO PA1 (item no. 64413-1 /-2) with pale yellow trim lines, which is



The New Haven ALCO PA1 (item no. 64413-1; photo left) along with the EMD F7 (63006-2; photo right) in SP "black widow" livery. Photos: AZL / Ztrack

The Southern Pacific EMD F7 A & B comes in the "Black Widow" livery. Offered here are an A-B double pack (63006-1) and a single A unit (63006-2).



Five new packs are available of the MAXI-IV articulated cars, now in brown for the BNSF with the "Swoosh" logo (906558-1 to -5). They are equipped with different sized containers from various companies.

[www.americanzline.com](http://www.americanzline.com)



### +++ Bahls Modelleisenbahnen +++

Michael Bahls is also planning new projects for this year, but they will not be announced until summer at the earliest. For the time being, the focus is on working off the backlogs from previous years so that the waiting list of his customers does not become even longer.

[www.lokomotiven-bahls.de](http://www.lokomotiven-bahls.de)

MAXI-IV articulated cars, now with the BNSF swoosh logo and a load of containers from various companies. (906558-2). Photo: AZL / Ztrack

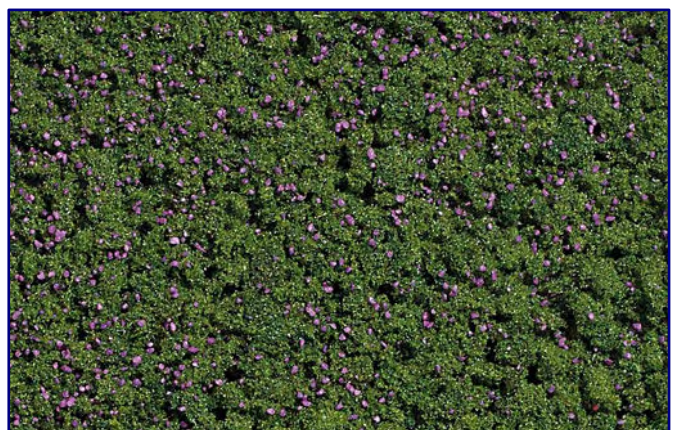
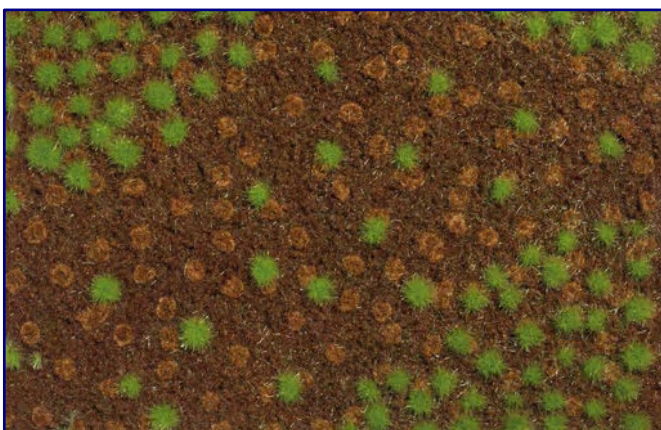
### +++ Böttcher Modellbahntechnik +++

On the pages of this supplier we find under their own brand aluminium wire mesh in the format 100 x 75 cm (art. no. BM 7000) as well as an ochre-coloured rock filler (BM 7661), which is delivered in 1 kg units. It is prepared by adding 450 ml of water.

[www.boettcher-modellbahntechnik.de](http://www.boettcher-modellbahntechnik.de)

### +++ Busch +++

We find many appealing new products at Busch and some of them can also be used in 1:220 scale. The focus is on the forest in all its facets: ground mats (19 x 25 cm) reproduce summery (art. no. 1321) and dry forest soil (1322). Also, newly available is a Forest Blueberries groundcover foliage (7348) measuring 15 x 8 cm.



For forest modelling, Busch offers, for example, the summer forest ground (Art. No. 1321; photo left) and forest blueberries foliage (7348; photo right). Photos: Busch

The "Modelling kit for forest edge, undergrowth or embankment" (6804) satisfies further needs. It helps to create a naturally grown forest edge, an embankment or dense undergrowth. It provides sea foam for the necessary branches, grass (for spreading) in two shades of green, and tufts of grass that harmonise with it.



Busch design suggestion for the “Modelling kit for forest edge, undergrowth or embankment” (6804). Photo: Busch

Let's now leave the forest and look at the cork sand (7520), which is fine-grained and is supposed to fill subsoils realistically. 40 g are in the bag supplied. The quartz stones (7534) weigh 320 g, which Busch recommends for streams, quarries and as loading material. Some of these can also be used for Z gauge applications, if selected by size.

The novelty programme in the area of grass fibres and grass tufts is rich. The following four colours appear with 2 and 4.5 mm fibre length each: spring (3471 / 3481), summer (3472 / 3482), late summer (3473 / 3483) and autumn (3474 / 3484).

Attached to a base sheet and surrounded by small stones are the following “grass tufts with stones,” which will complement the existing assortment of 16 differently coloured and coordinated tufts: 40 pieces each for spring (3551), summer (3552), late summer (3553), and autumn (3554) as well as 35 pieces for summer (3557) and late summer (3558).





The shrub mat “white flowering” (3595) is intended to give an impression of how this landscaping offer affects the user. Photo: Busch

The new shrub mats in popular spring and summer colours are particularly productive. These 35 mm high fibre mats in 12 x 8 cm format are cut, plucked or torn into small sections and then glued in place. Choose from “white flowering” (3595), spring (3596), summer (3598), and late summer (3599).

The following four new trees (marketed for H0 gauge) are very large for use in Z gauge, but still do not exceed the height of their prototypes even in 1:220 scale: avenue tree (3739), forest lime (3797), forest beech (3798), and forest maple (3799).

[www.busch-model.com](http://www.busch-model.com)

### +++ Deluxe Materials +++

We find a new plastic adhesive in a precision dosing bottle (art. no. AD92) on the website of this supplier. It is non-toxic and has a low odour, as the supplier writes on its pages. Under the product name “Roket UV” (AD88 / AD89), it also enters the market for light-reactive adhesives.

[www.deluxematerials.com](http://www.deluxematerials.com)

### +++ Dietz Elektronik +++

The new DSE-F1 / DSE-F3 function decoder is suitable for all sizes. It offers modes for welding light, fire flicker, random timer, adjustable PWM with dimmed output, and an adjustable switch-on delay.



The new turnout and universal control panel STP-16 offers an extension with the new function “indirect turnout route.” This makes it possible to trigger a programmed turnout route by calling up a single turnout address.

This can also be done by older hand controllers that do not have this function themselves. This new products is available in light grey and black cases (STP-16SW).

Photo: Dietz Elektronik



A new automatic shuttle system for analogue controlled model railways appears under DPA-RC. It enables the simple construction of a shuttle track with adjustable shuttle time.

[www.dietz-modellbahntechnik.de](http://www.dietz-modellbahntechnik.de)

### +++ Digimoba Elektronik +++

A track module (art. no. 2028) for DCC digital systems has been newly developed, which makes it possible, without PC control, to let vehicles move slowly at specified points or also to stop them with pinpoint accuracy. The locomotive decoders do not have to support ABC or stop with asymmetrical track signal or stop in DC sections. Sound and other functions are also retained during stops.

Thus, prototypical situations in front of signals or in station entrances and exits as well as at line construction sites can be represented without additional devices. In addition, it can also be used as a stand-alone block module in combination with other units of this type, including a corresponding control of block signals.

The new module is fully compatible with the previously offered product 2025. Great importance was attached to the calibration for the measuring section. The traction current capacity in all three monitored sections is rated at 2 A, which is significantly higher than required by the small vehicles of Z gauge.



Photo: Digimoba Elektronik.

In front of signals, it is able to allow vehicles to pass in the opposite direction without interference, or to pass through the entire block during passage, also at slow speed. All connections are designed as removable terminal strips and simplify both assembly and easy servicing.

[www.digimoba.de](http://www.digimoba.de)

### +++ D&H Doehler und Haass Steuerungssysteme +++

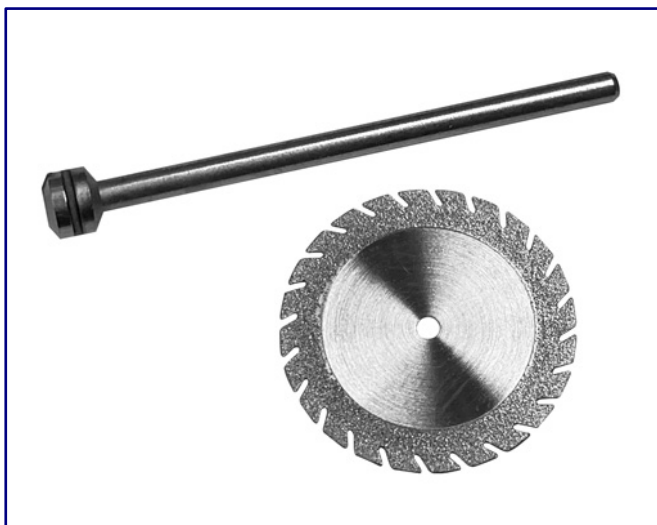
As the situation on the chip market remains tense, D&H is not currently announcing any new products and is focusing on product maintenance and announced new products from previous years. These include the FCCX digital centre (new product 2020) and the booster (new product 2021), which D&H is confident will finally be delivered by the end of 2023.

The SX-Bus voltage supply and the adapter for low-impedance coil drives as well as the adapter with potential-free changeover contact (new products of previous years) are currently in the sampling process, as reported on their own website. Here the hope is for deliveries in the middle of the year.

[www.doehler-haass.de](http://www.doehler-haass.de)

### +++ Donau Elektronik +++

A new addition to the range is a diamond cutting disc with a diameter of 22 mm and a cutting width of 0.23 mm (art. no. 1646) for classic fine tools in model making. This new product is characterised by flexibility and running transparency, has a double-sided coating and is suitable for cutting through glass, metal and epoxy.



Diamond cutting disc (art. no. 1646; photo right) and diamond saw blade (1647; photo right) are newly available from Donau Elektronik. Photos: Donau Elektronik

The diamond saw blade (1647), which is also new, is identical in diameter, cutting width and individual product characteristics, and is of course toothed compared to the cut-off wheel.

[shopware.donau-elektronik.de](http://shopware.donau-elektronik.de)



This production hall of an old spinning mill (art. no. 282742) is offered by Faller as a hard cardboard kit. Photo: Faller

### +++ ESU electronic solutions ulm +++

The new LokSound 5 Nano DCC sound decoder measures only 19.6 mm x 8.5 mm x 3.2 mm, but due to its thickness it will only be usable in a few Z gauge models. It only reads the DCC protocol and comes

with open cable ends for free wiring or E24 interface. It also includes a “sugar cube” loudspeaker (11 x 15 mm) together with a sound capsule kit.

[www.esu.eu](http://www.esu.eu)

### +++ EtchIT-Modellbau +++

This supplier has made a lasting impression on Z scale enthusiasts and will continue to deliver new products. However, they will only be announced when they are ready for production and delivery.

[www.etchit.de](http://www.etchit.de)

### +++ Faller +++

The production hall of the old spinning mill (art. no. 282742; photo on page 32), which is newly released by Faller as a lasercut kit, has a single nave.

The brightly plastered factory building has high mullioned windows and gets its special appearance from brick window frames. The manufacturer classifies this building in era III and plans to deliver it in April of this year.

Under the name “Water Tower Burgstadt” (222234), the popular model from Haltingen is also being reintroduced as a colour version for the N scale.



The “Water Tower Burgstadt” (222234) is already known under a different name and is also suitable for conversion to Z gauge due to its small prototype size. Photos (above and below left): Faller



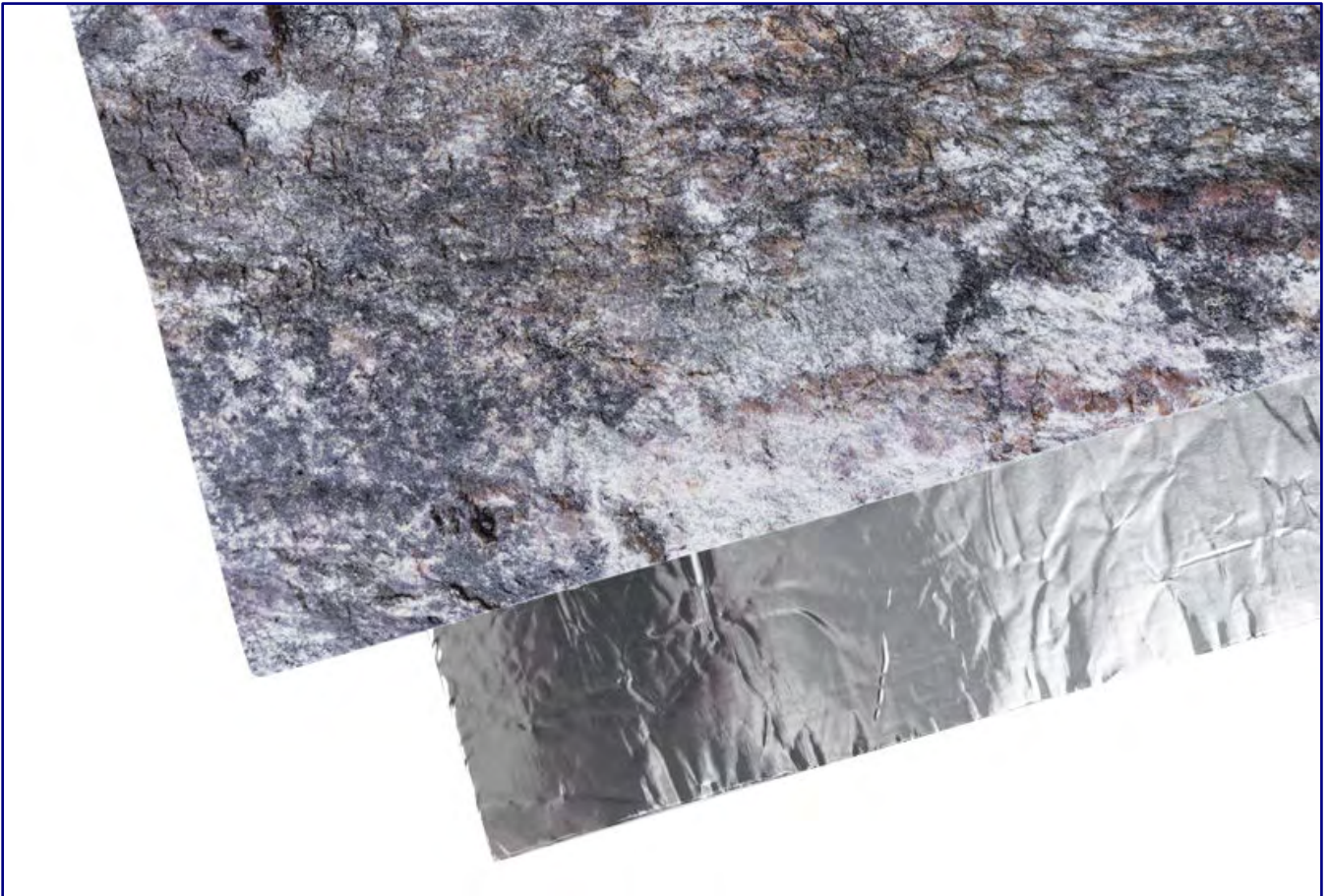
Since the prototype with its ball tank on a steel frame is rather a low-built model, it can be modified and used for scale 1:220 with little effort.

The “3 Poplars” (181576) with a height of 130 mm and the “4 Columnar Oaks” (181577), which rise 90 - 110 mm from the ground, can also be used on Z-gauge layouts. The new products brochure also refers to the distribution of Vallejo products via Faller.

The rock and terrain foam (171800), which promises easy modelling of rocks, mountains and terrain transitions, will only be available in Germany. It is easy to cut and carve and can be coloured afterwards. The indication of a yield of 16 litres from the 400 ml pressurised can leads us to assume that it could be a polyurethane foam, as it is also used for installing door frames.

A more familiar option for designing rocks is offered by two new rock foils in grey (171801) and ochre (171802). After this trend has found its way to various suppliers, Faller wants to go one step further and specifically stand out: A photo print is used and the foil is mounted on an aluminium lamination, which is supposed to guarantee dimensional stability.





Faller picks up a trend, but puts its new rock foils in grey (171801) on an aluminium lamination so that it is also well suited for small gauges. Photo: Faller

In discussions at the trade fair, Faller representatives emphasised that the company will continue to well cater to the Z gauge segment. Two concrete projects were in the pipeline and would certainly be ready to be launched by next year.

[www.faller.de](http://www.faller.de)

### +++ Freudenreich Feinwerktechnik +++

At the end of 2023, Harald Thom-Freudenreich wants to part with his workshop in Sanitz for reasons of age. This means that he will no longer make investments in tools for new moulds, which take too many years to economically break even.

However, there will still be new colour and printing variants in this last year. For example, a four-pack of the Eanos type open freight wagons is planned with specimens from different owners: DB Schenker, AAE / VTG, SNCB and NS.

A container wagon Os of the private Swedish railway company Three T (item no. 46.818.01) should be available already in March. On request, this model can also be equipped with containers from Maersk, MSC or DB Schenker.



After the conversion of the prototype, it no longer fits into the main type O (open wagons of the standard type), but the Os of Three T (item no. 46.818.01) continues to run as such. Photo: FR Freudenreich Feinwerktechnik

Furthermore, the SJ's electric locomotive Rc5 1377 in white-silver trial livery and Grenland Rail's Ma 404 are planned. Several Z scale clubs have also ordered special series, which are not reported on here yet.

[www.fr-modell.de](http://www.fr-modell.de)

### +++ Heki +++

Heki's 2023 novelty programme consists exclusively of tree material. We have selected those compilations that remain true to scale in the conversion of the growth heights - many model railway enthusiasts may nevertheless find the trees too high, as they are always depicted far too small across all gauges.



3 deciduous trees with 14 cm height (Art.-No. 1926). Photo: Heki

The "3 birch trees" with a height of 14 cm (art.-no.: 19116) from the Realistic series are praised by the manufacturer as almost unique specimens. The trunks are coloured by hand and the foliage is applied in several steps, according to the manufacturer.

The "3 deciduous trees" with the same height (1926) have a slender growth reminiscent of poplars and belong to the Artline range with their micro leaves. Three mini forest economy packs are also interesting: "Mini forest set 40 trees and firs 5 - 18 cm" (1962), 30 trees and firs 5 to 11 cm high (1963) and 27 deciduous trees 11 to 14 cm high (1325).

[www.heki-kittler.de](http://www.heki-kittler.de)

### +++ Herpa +++

First of all, there is new information to report about the future of the customer clubs and the Wings World magazine: Anyone can register for the clubs and in this way gain access to attractive models and discounts.

No firm commitment is to be entered into, nor is a membership fee to be paid. The aircraft magazine Wings World (<https://www.herpa.de/de/Club/Magazine/Wingsworld>) is to be published electronically and without access restrictions on the Herpa pages from spring onwards. In addition, there will no longer be a fixed publication rhythm and the articles will be published regularly.

Herpa has not yet announced any new moulds or larger projects for the model year 2023. Previously announced new products refer to the planned delivery months of March and April 2023. The Ilyushin IL-62M of Air Ukraine (item no. 572699) focuses on the past of this country, when its world was still in order.

The same aircraft type is also represented by a model for LOT Polish Airlines (572682) from its western neighbouring country. This aircraft was christened "Wladyslaw Sikorski" and is no longer part of the airline's fleet.



This Airbus A350-900 model "Marcello Lippi" from ITA Airways (Art.-No 572620) is part of Herpa's spring new products. Photo: Herpa

Modern turboprop aircraft are the ATR 72-600 of Brazilian Azul (572675) in a livery showing the national flag on a black fuselage, and the somewhat smaller version ATR 42-300 in service with ATI Aero Trasporti Italiani with the name "Siena" (572668).

ITA Airways is the successor to the long-established Alitalia. Its Airbus A350-900 with the name "Marcello Lippi" (572620) is registered in Ireland and, thus, is easily recognisable as a leased asset. The same might be true for Philippine Air's Douglas DC-6B "Magellan's Cross / Cruz de Magallanes" (572545), which would have long been a flying piece of history today.

Two snap-fit models in simplified design and flight display with retracted undercarriages complete this line-up. These are the Airbus A330-200 of TAP Air Cargo (613736) and the Boeing 787-9 "Proud of Poland's Independence" of LOT Polish Airlines (613781) presented in special livery.

[www.herpa.de](http://www.herpa.de)



### +++ High Tech Modellbahnen +++

In the hope of eventually catching up with the existing backlogs, High Tech Modellbahnen is again planning new products in the long term. This may include interior lighting for Märklin express carriages, new switch modules for the turntable and transfer table (for analogue and digital operation) and special plug-in bases for various locomotives.

[www.z-hightech.de](http://www.z-hightech.de)

### +++ Imprintium +++

This manufacturer, previously unknown to us, also has Z scale models in its 3D printed range, including caterpillar and steam tractors, small boats and a motor ship. Unfortunately, Thorsten Zwarg did not respond to our enquiry as to which of these models are new and whether further new products are planned.

[imprintium.de](http://imprintium.de)

### +++ IMS Modellbau-Manufaktur +++

After the new products announced last year, which we added in the March 2022 issue, the Hartmühle stop and four light models, were not requested even once, this manufacturer is concentrating on larger gauges in the current year.

[ims-modell.de](http://ims-modell.de)

### +++ JMC Scale Models +++

The industrious car model designer, who introduced himself in person at Altenbeken 2022, had to solve



The new-form Henschel HS 160 USL-G articulated bus can be ordered in these three configurations. Photo: JMC Scale Models

unusual problems with his latest project: He ran out of resin for casting his models and the local dealer was unable to supply more.

Now that he has found an alternative, production could restart. The first Henschel HS 160 USL-G articulated buses are currently being built. Three versions of this very attractive model are on offer: straight ahead and right or left turning.

When ordering, the customer has to choose one of these versions, because the joints are not movable. Depending on the direction of travel, the front wheels are also suitably turned in the direction of travel.

[jmc-scalemodels.blogspot.com](http://jmc-scalemodels.blogspot.com)

[www.facebook.com/pages/category/Games-Toys/JMC-Scale-Models-341193572911400/](https://www.facebook.com/pages/category/Games-Toys/JMC-Scale-Models-341193572911400/)

### +++ Joswood +++

After the overseas transport boxes for heavy goods wagons have been well received by customers, Joswood is expanding this series further. The three new products from this series for the small gauges are also suitable again for Z gauge.

These spring new products consist of wooden boxes whose lid can also be built as a detached part. They are therefore also very suitable for creating loading scenes, for example in a machine factory.

Three different combinations in different sizes are offered: "small wooden boxes set 1" (art. no. 40427; 8 pieces, L 19 x W 11 x H 10 mm), "small wooden boxes set 2" (40428; 4 pieces, L 35 x W 11 x H 10 mm), and "small wooden boxes set 3" (40429; 4 pieces, L 35 x W 11 x H 12.5 mm).

Interesting for the timber yard of sawmills or for loading onto various stake wagons of classic or modern design is also the "Timber Stack Set 2" (40449). It contains three different sized saw log stacks. The twelve stacks supplied with this pack are distributed as follows L 52 x W 9 x H 10 mm (3 pieces), L 30 x W 11 x H 5 mm (4 pieces) and L 25 x W 8 x H 8 mm (5 pieces).

<http://www.joswood-gmbh.de>

### +++ Kastenbahner +++

Andreas Herzog continues to be active and busy. He let us know that there are currently no new products to report. The last ones (Ks signals with different masts) had already been announced in reports of previous issues. All types of signals can be ordered with or without beacons.

[www.kastenbahner.com](http://www.kastenbahner.com)

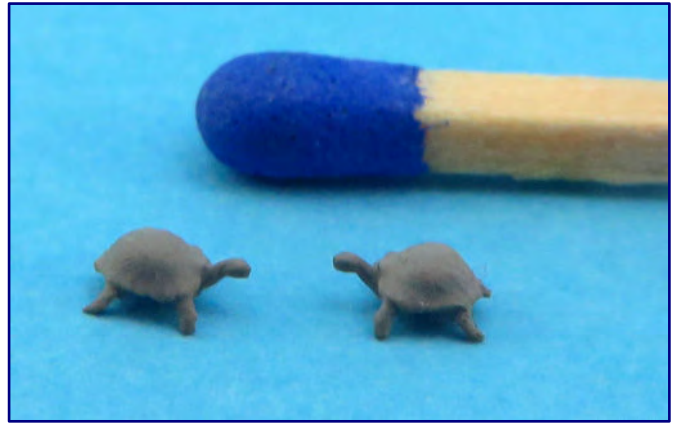
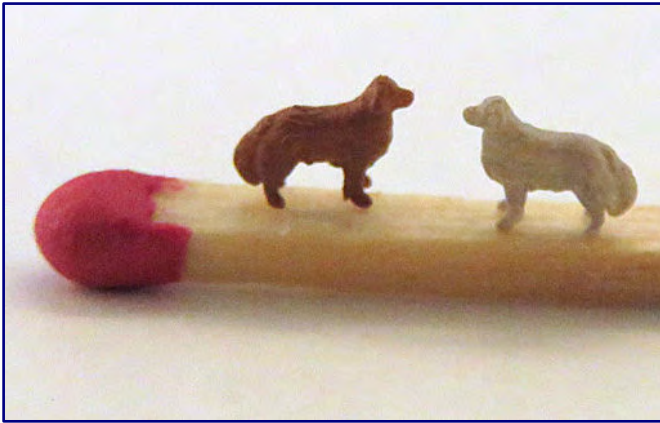
### +++ Klingenhöfer Miniaturen +++

Klingenhöfer Miniaturen is starting the year 2023 with three new animal figures. The first is already familiar to us from the programme, but the figure announced in **Trainini®** 10/2021 was designed differently. The gorilla on the rampage on a car (art. no. Z-TA04) is a real new product

The new Galapagos tortoise has a shell length of 134 cm in real life and will enrich one or the other zoo on a



Crouching gorilla on top of a car. Photo: Klingenhöfer Miniaturen



Two golden retrievers (left) and the Galapagos giant tortoises (right) make up the other spring new products. Photos: Klingenhöfer Miniaturen

scale of 1:220 in the future. Also new are two golden retrievers, which are certainly better off in the care of a family with house and garden.

The outlook for this year is that the assortment of small animals such as squirrels and birds will be further expanded.

[www.klingenhoefer.com](http://www.klingenhoefer.com)

### +++ Kreativ3.de +++

Jürgen Dill-Schilling told us that this supplier focuses on H0 and TT scales. After the exhibition in Dresden (10 - 12 February 2023), Kreativ3.de will also bring individual products from the existing portfolio of larger scales as new products for Z gauge. The company's own website will keep customers and interested parties up to date on the latest developments.

[www.modellkreationen.de](http://www.modellkreationen.de)

### +++ Krüger Modellbau +++

This small series manufacturer announces a snow plough (item no. Z98052.04) for the 52 series. Consisting of etched parts also for the front frame, it is to form an exchange element. Furthermore, alternatives for the running boards (Z98814.01) of four-axle Märklin compartment coaches are to be offered with etched parts.

[www.krueger-modellbau.de](http://www.krueger-modellbau.de)



Photo: Lenz Elektronik

### +++ Lenz Elektronik +++

Many customers found the digital control units of the DCC inventor too complicated. This is especially true for those model railroaders who do not want extensive controls and only drive a few vehicles.

Lenz has reacted to this and now offers "Starter-Digital" (art. no. 60120), a combination of a small starter control unit, a matching hand controller and a powerful power supply unit. An additional hand controller (21020) can be purchased separately.



This unit offers only the basic functions for up to four traction units with ten switchable functions per model. Ten turnouts can also be controlled.

Four turnouts or signals can be connected to the LS101 switching decoder. For the use of LEDs it is important to know that there is direct current at the outputs. A connection to the central unit enables the switch positions to be reported back for display on a hand-held device or to be passed on to a control programme.

[www.digital-plus.de](http://www.digital-plus.de)

### +++ Markenburg +++

The Z gauge assortment of the Dutch lasercut specialist currently comprises 39 items. Examples from this are the telephone boxes, which are offered in grey and green according to the Dutch model. We had already mentioned them in an earlier new products report. What is new, however, is the information that they can also be manufactured in yellow for use in Germany or in red for the British Isles.



The “Heistraat 109” residential house is the latest Markenburg new products for Z gauge. Photo: Markenburg

The houses, farms and factory buildings in the programme can also be adapted in the area of window frames or roof tiles to give them a different appearance and to use them in a different region. In addition, pavement slabs, brickwork, and bedding in 10 x 25 cm format are also available for Z scale, so that skilful model railway enthusiasts can put together their own creations.

The module tables in the programme are intended for designing your own layout and are suitable for the three gauges HO, N, and Z. The latest product for the scale 1:220 is the residential house "Heistraat 109", which we can also show here with product pictures that reveal its unique style.

[www.markenburg.nl](http://www.markenburg.nl)

### +++ Märklin +++

Our talk with Märklin at this year's fair was very informative and ranged from a look back at last year's anniversary to the prospects offered by the latest new products for our gauge. The repeated postponement of the insider model 2022, the V 320 001, caused irritation recently. Some customers expressed their concern to our editorial team that the Wiebe version might be available earlier than the Era III version of the Bundesbahn.

This would then call into question the exclusivity of the Märklin Insider Club models, which is why we approached the manufacturer with this question. He was able to reassure us that this would not happen at all, because the reasons for the rescheduling concern both versions.



This refrigerator car (art. no. 80133) with different printing on both sides was the Märklin trade fair gift to the dealers.

It was explained to us that the Insider Models are, in principle, ready for delivery. However, the final quality control still revealed a weak point that is to be fixed first in the interest of Märklin and its customers. Since the production release samples did not show the criticised feature, two alternative solutions must now be tested, which may affect the final delivery date to varying degrees.

Collectors will again be delighted with the special exhibition models. As a fair gift for the dealers, there was a refrigerator car (art. no. 80133) with different printing on each side with the motifs "100 Years of the FD Long Distance Train" (black and white) and "150 Years of the Black Forest Railway". Especially the colourfully printed second motif looks very appealing with a class 601 diesel multiple unit as the Alpen-See-Express.

The DB Regio class 143 (88430) was chosen as the exhibition locomotive. In the Era VI version, it has a black basic colour with two red decorative stripes as a rental locomotive with advertising inscriptions from DB Gebrauchzug.

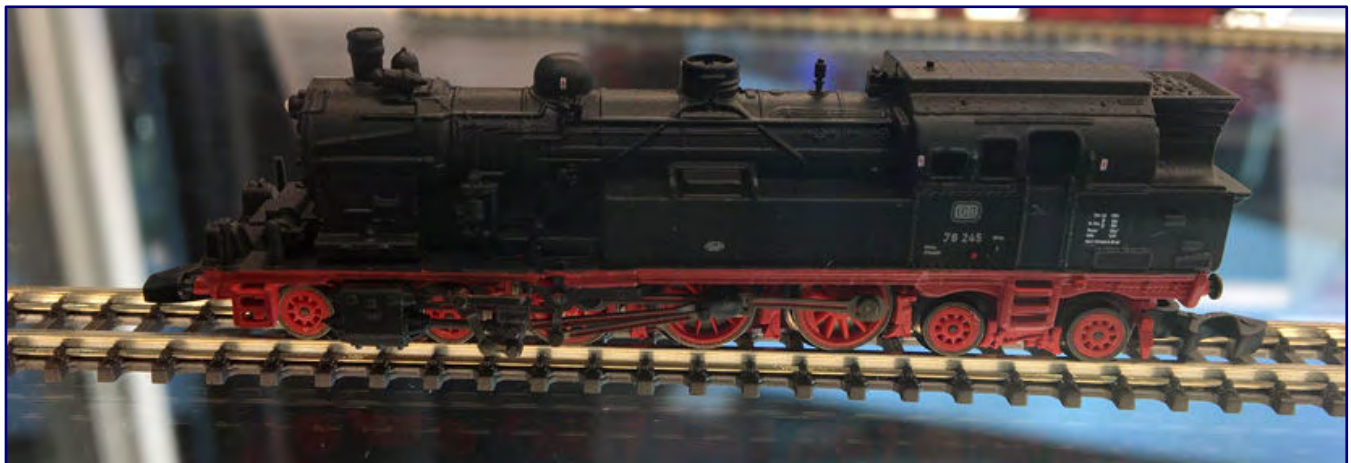




A class 143 locomotive in special livery (88430) is the Nuremberg trade fair model 2023. Photo: Märklin

Märklin has a little hit in store for the MHI (Märklin Dealer Initiative): for the first time, the Göppingen-based manufacturer is using a 1:220 scale reversible steam locomotive. This is the class 78 tank steam locomotive (88068), which was also issued last year after extensive product maintenance. But, this time it appears with a fan attachment on the driver's cab roof, which already sets it apart from its direct predecessor.

What is really amazing, however, is the white/red light change installed for the first time with LEDs. With the tail lights it can also run at the end of the train, as was the case with its prototype from the Bw Essen in the Ruhrschnellverkehr of the sixties. Märklin indicates an operating condition of around 1965.



For a push-pull train, the class 78 (88068) also gets tail lights for the first time and appears as a limited MHI special model.

MHI also offers a matching car set (87074). It consists of two permanently coupled pairs of AB3yge/B3yge and B3yge/B3yge type conversion coaches, as well as a BD4nf-59 driving trailer. For those who are not familiar with the designations, it should be noted that this is a "bunny box" from the "Silberling/silver coin design" commuter car family, which is now appearing for the first time in this five-part set for Era III.



The class 44 steam locomotive with oil tender (88975) has a special charm. Behind this simple product name hides the Märklin's exhibition locomotive, a locomotive that has been set up on the grounds of the Märklineum.



With the MHI reversible train set (87074), the bunny box will also appear for Era IV. The pre-production sample now available (photo above) promises a successful realisation of the car sides, as well as the head. The exhibition locomotive 44 1315 appears together with its shelter as a kit (88975; photo below).

Consequently, it also appears in the model with a kit made of solid-coloured hard cardboard from Modellbau Laffont, which reproduces the shelter, which is supposed to be reminiscent of a Märklin H0 package. During the exhibition, some viewers asked whether the Märklin lettering on the shelter could be optimised.

It has a thin strip on which the letters sit to make manual alignment unnecessary, it should be borne in mind how tiny these are in Z scale. We were told that the individual letters are engraved on the wall surface as an adhesive aid. This facilitates exact positioning, but also opens up the option of removing the retaining strip for purists with a talent for craftsmanship.

We have an addendum to deliver to the insider model 2023, the battery railcar ETA / ESA 150 (88250). Known since the end of November 2022, it is formally one of this year's new products. The quickly created product image initially still contained some errors, which we would like to briefly address.



Except for the not yet corrected position of the 1st class compartment, the CAD volume representation of the ETA / ESA 150 (88250) now corresponds to the expected model. Photo: Märklin

For example, the ETA 1505 underbody series is not reproduced, and the four-leaf folding door in the area of the luggage compartment has been subsequently removed from the illustrations. Also, the 1st class compartment still has to be relocated, which will of course be done on the delivered models, but was still incorrect on the sample illustrations.

It remained unclear how the product description “rear top lighting on the motor coach can be switched off” is to be understood. There were concerns that the driving trailer will nevertheless illuminate its leading unit, as it is not explicitly mentioned. We received feedback that we can assume that the lighting is correct. The issue is therefore known.



So far there is no hand-built prototype model of the V 60 (88651), but only this photomontage based on the H0 scale version. The shape of the body will finally be correct, two alternatives for the lighting are currently being examined. Photo: Märklin

A new starter set (81875) with track oval and rolling stock is dedicated to the current Era VI. Three different VTG freight cars are included, one in red-brown (Eaos) and two in blue (Falns and Shimmns). This is the first time that a Rhein-cargo locomotive has been published for Z-gauge. The choice fell on the silver-painted class 285 diesel locomotive.

The class 86 tender steam locomotive (8893) of the Deutsche Reichsbahn for Era II and the class 38 steam locomotive (88997) with box tender and large wind deflectors, which will be placed in Era IIIa for the first time, require less explanation.



The announcement of a new shunting diesel locomotive of the V 60 series was a sensation for many, although it had been “in the air” for years, because Märklin had already developed the suitable running gear for the V 36: We noticed at the time that this locomotive in the model ran on the chassis of the V 60, and the first development step was thus already done.



As part of a new starter set (81875), a Rheincargo locomotive in Z scale appears for the first time.

51 years after the release of a great compromise with a completely wrong chassis, the time has come and a new model (88651) will take over from its aged predecessor. That's why we are providing some details that are not in the product description: It will have a white-white light change, but no tail lights. The lanterns will probably be illuminated by light guides, but alternatives are also currently being examined.

The new model with metal body is a completely new design, which is why the cab shape will finally be correct. The upper side window area is thus retracted and the roof sits lower, as is the case with the originals. That the new products will be equipped with window inserts can already be seen in the product description.

The DB class 41 (88277) with new boilers and coal firing is from epoch III. The product illustration shows buffer plate warning paint, the model is also equipped with Indusi magnet on the locomotive driver's side and window ice sets in the driver's cab. Unfortunately, it has to make do with the old 2'2' T 34 tender, which is no longer convincing in some details.



The Klv 20 (88026) is now on the track for the Wolff company from Walsrode. Photo: Märklin

With this, it is time to also weave in some wagon new products: The DB's chrome oxide green crew car (87002) for construction trains is of the type Ci wü 04/05. It is equipped with an interior. The two Prussian compartment coaches (87042), 2nd class without and 1st/2nd class with brakeman's cab, can extend a coach pack from previous years.

They belong to the two-axle conversions with sprinklers in the middle of the vehicle and correspond to Era IIIb (around 1958). In keeping with the prototype, they do not carry a Bundesbahn “biscuit” logo. This also applies to another reissue for which we had to wait a long time.





**A four-part carriage set (87565) and the individually available baggage car (87566) make for an early era III Prussian train formation before the introduction of the “Ege-biscuit” type Federal Railway. Photo: Märklin**

We are talking about the four-axle compartment coaches of Prussian design, which are now also assigned to Era IIIb again. The car set (87565) contains four cars with the following equipment: 1 car AB4 (1st/2nd class) with brakeman's cab and 3 cars B4 (2nd class), one of them with brakeman's cab. In the first edition many years ago, they were still printed with the “Ege-biscuit” logo, which this type never received.

The cars can be dated to the last period of use after the class reform. The matching baggage car Pw4 (87566) of the Prussian type with a central driver's cab is offered separately. It also does without the biscuit, but outlived the passenger coaches in service by a few years.

The small car Klv 20 has become a bestseller. Märklin is therefore reissuing it in blue paint (88026) and with the addresses of the Wolff company from Walsrode in the Lüneburg Heath. The tiny model made of metal-filled plastic thus belongs to epoch IV.

There were only 15 examples of the type Tams 886 in the prototype. The rolling roof cars of the German Federal Railways based on the Eaos 106 appear as a set of three (86682) for Era IV. The roll-roof top comes even from new moulds.

The stake car Kklm 431 could save itself from the Länderbahn era into the computer age. Its DB model for Era IV (82103) has three double-walled oil containers in dark grey paint as a load, secured on a loading frame according to the prototype.

The series of museum locomotives is continued. This time the choice fell on E 19 12 (88085), as it presented itself to the viewers in 1977. At that time, it wore a cobalt blue base colour on which white decorative lines were applied.



The series of museum locomotives continues with the attractive looking E 19 12 (88085) in 1977 condition. Photo: Märklin

However, their arrangement did not correspond to the DB operating condition in this colour, but erroneously followed that from the time when it still wore wine red. Old black-and-white photos had misled the restorers at the time. The model rests in a real wooden box.

A reunion with the class 139 freight locomotive (88386). On the Höllentalbahn, it was also used for many years in push-pull service due to its electric resistance brake, which had opened up a wide range of applications for it.

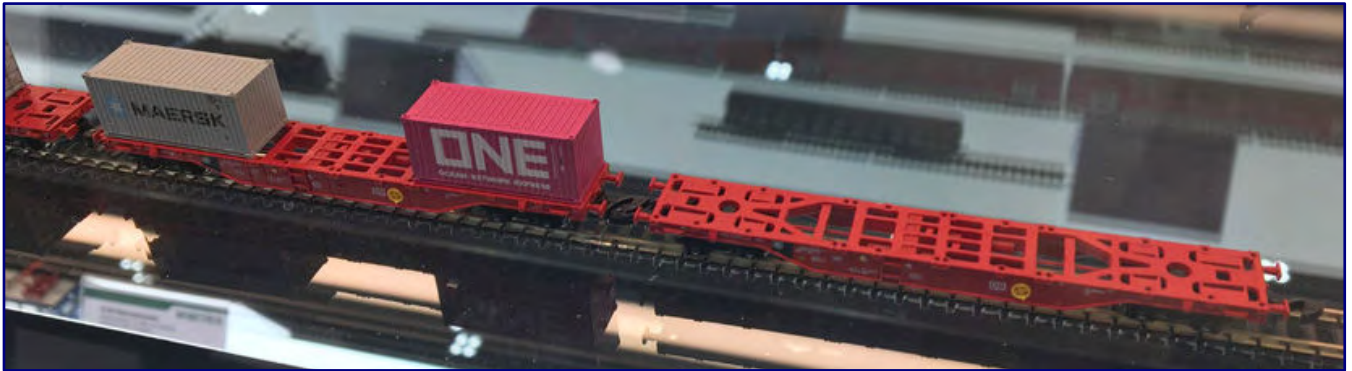
With it, this class is now also being converted to bell armature motors together with warm white LED lighting. It remains unclear why Märklin has chosen an ocean blue-ivory coloured model for the second time. Nevertheless, it will certainly stand out visibly from its predecessor from the eighties in terms of paint and printing. The operating condition around 1978 is reproduced.

In the future, the goods train escort car Pwgs 041 (86061) will be available separately. It will still be without a driver's cab, but will now have Era IV markings (without the DB biscuit). The Rlmp 700 heavy goods wagons, which appear as a pack of four (82228) with armoured loads, also slip into this time window.

With regard to the inscriptions, they now also match the Leopard 1A1, with which they are also loaded this time. Three more tank models (89025) are available as a supplement in a separate pack.

A TRAXX locomotive returns to the range with the class 186. The modern four-system electric locomotive (88487) belongs to Railpool and bears the design of the Lineas with which it is currently on the road. It can also be coupled with the three new Sgns 691 container wagons of Deutsche Bahn AG (82640).





The Sgns 691 freight wagons of the Deutsche Bahn AG (82640) are new designs and made from metal-filled plastic. Their pre-production samples show very filigree and fine structures.

They are made in metal-filled plastic to achieve good running properties. Their loading has deliberate gaps, which are more common in modern Klv trains: One wagon is unloaded, the other two are loaded with two 20-foot containers (One and Maersk) and one 40-foot container from Maersk respectively, which still leaves capacity gaps.

Part 6 of the bronze casting collector's series is dedicated to the Swiss Re 4/4II (88597). With black tones in the undercarriage area, reduced printing and protective paint, it presents itself just as stylish as the previous models in this series. The locomotive is delivered in a wooden box.







SBB's green Re 4/4<sup>II</sup> (88593; see photo on page 48 below) and SBB's Re 460 (88468) together with a Siemens Vectron of BLS serve the Swiss market this year. Photos: Märklin

The same model also finds its way into the new product line-up as a green locomotive with round headlights at the bottom (88593). With the exception of the paint scheme and the new single-arm pantographs, it thus technically corresponds to the locomotive from the TEE train set 81593.

Staying in Switzerland, the SBB's class 460 electric locomotive is being reissued in its standard red livery in circa 2018 condition (88468). It is now also powered by a bell armature motor and without electrical functioning pantographs.

The Siemens Vectron now changes to BLS as class 475 (88236) and thus receives probably one of the most beautiful colour schemes of the prototype. Technically, it is unchanged compared to the previous editions and also belongs to Era VI.

Two pocket wagons Sdkmms of Hupac (82273) are released to match. The black painted versions have close couplings and carry semi-trailers of the Planzer and Schöni haulage companies. For the SNCB Märklin presents a five-piece car set (86358) with telescopic bonnet and sliding tarpaulin cars of the types Shimmns and Shimms.

The models show four different designs (from various colour schemes) and some also have touch-up spots. We already presented this year's museum car in **Trainini®** 12/2022.

For the fourth time since 2017, the Insider Club annual wagon 2023 will be a covered freight wagon of the GI Dresden type (Item No. 80333). This time, it bears era IV markings and is in the service of the electrical appliance manufacturer Saba.

[www.maerklin.de](http://www.maerklin.de)

## +++ MBZ Modellbahnzubehör +++

What is new about this manufacturer is that it now also clearly identifies newly added products on a separate page of its website. Unfortunately, no specific information is provided to our editorial staff.

Unfortunately, the date of inclusion of new products is not marked, which is why it is not possible to see how long kits are considered new there. We are therefore filtering out some highlights of this list and ask our readers to refer to it, if necessary.

The new primer Paraloid B72 (art. no. 72226) is supplied as granules dissolved with acetone and in this liquid form is intended as a primer for the manufacturer's kits. A burnished brass chain (86307) with 500 mm is also offered for Z gauge.



The Goldberg Castle (art. no. 16623) consists of several kits that can be used individually or together to recreate the model as a whole or in parts. Photo: MBZ Modellbahnzubehör

The prototype of the Meckenbeuren station kit (16630) is located in Baden-Württemberg and belonged to the Swabian railway, which is the protagonist of a well-known German children's song. The appealing model is reproduced in its state of construction of 1905, thus quite appropriate for the time when that famous farmer was not allowed to take his goat on the train and tied it to the coupling of the last wagon without further ado. Other stations can also be found among the new products.

With the Goldberg Castle (16623) a manorial country estate from around 1820 finds its way into the programme, which looks appealing and striking and does not immediately claim the dimensions of a castle complex. For this kit there is also the option of some additional buildings, with which the ensemble can reach quite considerable dimensions.



The Gothic chapel (16612) can also become an eye-catcher because of its beautiful architectural style and consequently deserves to be mentioned here. For model railway enthusiasts who build according to concrete models, various kits are recommended that deal with the layouts of the railway maintenance depot Bebra (16564 et al.), which as a DB station close to the border was once of great importance for transit traffic to the East Germany.

A technical highlight is certainly the 10-metre wagon turntable (73626) driven by a stepper motor, which makes the disc move 45° at a time. The turntable itself is made of laser-cut and engraved cardboard.

[www.mbz-modellbahnzubehoer.de](http://www.mbz-modellbahnzubehoer.de)

### +++ MHN Horneber Collection +++

This supplier actually specialises in shop fittings and shop fixture systems, but also offers glass display cases. At a higher price and quality level, the range also offers options for solutions in the home environment.

The range of showcase types, heights and designs is extensive and, with various lighting options, offers additional attractions to equip your own hobby room in a sophisticated way.

[www.glasvitrienen.de](http://www.glasvitrienen.de)

### +++ Microrama Model Decor +++

Microrama renews its range of Magitree tree armatures (trunks and branches). The new products are made of a resistant unbreakable plastic. The branches can be shaped into the desired form with a heat gun.



Microrama's new trees with refined branch structures, and, above all, a considerably more realistic looking trunk and bark structure.  
Photo: Microrama Model Decor



The trunks and branches have a realistic shape and are offered in many variations. Details of the bark can easily be highlighted with a small brush and paint. The flocking of the last branching levels is ideally done with an electrostatic flocking device, for example with the manufacturer's "bFlock" device. Also recommended is the use of their Platter device, which statically charges the foliage (Magileaf) and makes it jump to the branch tips.

The new trees are to be labelled "New Magitree" on the company's own website. Precise construction instructions will also be provided to introduce even beginners to this modelling technique, which produces particularly realistic results.

[www.microrama.eu](http://www.microrama.eu)  
[www.microrama.online](http://www.microrama.online)

### +++ Micro-Trains (MTL) +++

Micro-Trains has presented car no. 12 of the Railroad Magazine special series (item no. 502 00 651). It follows the familiar design, again shows a historical cover (from February 1941) and is to be delivered before the end of February.

[www.micro-trains.com](http://www.micro-trains.com)



MTL's Railroad-Magazine series has now arrived at wagon no. 12 (Art.-Nr. 502 00 651). Photo: Micro-Trains

### +++ Modellbahn-Decals Andreas Nothaft +++

As an exception, Andreas Nothaft does not offer new decals for model railways, but something across gauges: As a special series he offers a wristwatch for steam locomotive enthusiasts. Dial and hands are based on the speedometer in the cab of the class 74. On request, the watch can be delivered in a gift box for a small extra charge.

[www.modellbahndecals.de](http://www.modellbahndecals.de)



Photo: Modellbahn-Union

### +++ Modellbahn Union +++

An interesting new product comes from this model railway dealer, which is for N scale, but could be also used for our scale and might even have chances to be produced in scale 1:220. Under the name "MUFlex" a neutral grey foil is offered, which thanks to its flexibility can be brought into almost any shape.

It is easy to cut and glue and is available with different three-dimensional structures. It is 2 or 3 mm thick and can be painted with acrylic paint. Sand can also be worked into the stone joints, which is intended to create the most realistic effect possible for the designed scenes.

[www.modellbahnunion.com](http://www.modellbahnunion.com)

### +++ Modellbauatelier Steinbrecher +++

André Steinbrecher told us that he is currently working at capacity with an annual production of around 4,000 trees. For this reason, there are currently no new products to report. The existing price list is currently being revised and will be published on their website.

[www.modellbau-steinbrecher.de](http://www.modellbau-steinbrecher.de)

### +++ Modellbau Dr. Schroll +++

This manufacturer does not have any new products to report in the spring of 2023. Nevertheless, Dr. Schroll was pleased to point out that his new website is finally up and running, and now provides his customers with the necessary overview of the product range. In addition, these new pages also look modern and appealing.

[www.modellbau-dr-schroll.de](http://www.modellbau-dr-schroll.de)

### +++ Modellbau Laffont +++

This laser-cut kits supplier from Aachen is setting off a veritable fireworks display of new products. The first two are based on the Eschelbronn railway station. The original station building (Item No. Z3701) can be found near Heidelberg and has two storeys with a gable roof, partial framework and wooden boarding in the gable area. It can be used from era II and can be extended with the matching Eschelbronn goods shed (Z5101).



Combination of the Eschelbronn station building (item no. Z3701) with the corresponding goods shed (Z5101). Photo: Modellbau Laffont

The railway post hall (Z5401), once found in many towns, can be extended as desired by placing it next to or behind each other. This new products offers many assembly options: The back wall can be omitted, as well as it can be built up side- and mirror-inverted. One side is built half-open with steel framework, the opposite side shows a closed wall with tile engravings.



The new railway mail hall (Z5401) offers a wide range of construction and deployment options and, as seen here, can also be installed in such a way that it allows the viewer to look inside. Photo: Modellbau Laffont

Such buildings were not only used for loading the weather-sensitive postal goods in the sixties and seventies. Such buildings were also used (or continued to be used) as parking halls for lorries or buses. An open parking deck for cars is also conceivable.

In company backyards or allotment gardens close to the railway, you can sometimes find abandoned freight wagons (Z6401) which, stripped of their chassis, have found a new function as a summerhouse or storage facility.

With the finest engravings, this kit can also be used from era I, but the peak of this type of usage was certainly reached in the times that model railroaders recreate with eras III and IV.



Many customers will be grateful for the storage facilities suggested by the parked freight wagon (Z6401). Photo: Modellbau Laffont

All new kits have, in common, that they are made of solid-coloured, precisely cut hard cardboard and can be glued with standard white glue.

[www.modellbau-laffont.de](http://www.modellbau-laffont.de)

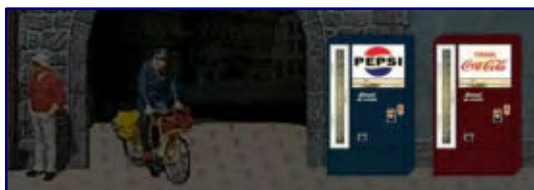


Photo: Modellland

### +++ Modelland +++

Z gauge vending machines with LED lighting for Era III are currently being planned. Waterproof printed on plastic, the vending machines in classic design will then be available for two well-known cola brands.

[www.modellland.de](http://www.modellland.de)



## +++ NoBa-Modelle +++

Before we present the current new products, we would like to take the liberty of pointing out the product maintenance that is being carried out in the NoBa-Modelle range. For example, the double diesel locomotive of the V 188 series, which we presented in detail in the resin print kit version, has been reworked.



An example of the ever-ongoing product maintenance is the shell of the V 188, which received some new details in the roof and chassis area.

It now convinces with many new details, especially in the roof area, which include, for example, the angled mounted compressed air whistle. Some other aspects have been also corrected or improved. For example, the current models have received an apron for the undercarriage area, which covers the bogies of the Rokuhan shortys and depicts prototype features.

Another area of product maintenance is the replacement of the PLA models still in stock with the much finer looking resin models in the course of 2023. Remaining stocks of PLA prints will be sold as discontinued models at special prices.

This brings us to the current new products: the designs offered for downloading and self-printing against payment have been expanded as follows: transformer station (art. no. 23001), Mercedes-Benz Actros 4x2 with NoBa trailer (26006), Claas tractor (26002), Mercedes-Benz Actros waste collector 6x4 (26003) or 4x2 (26004) as well as Iveco-Magirus 256 D 26 (26005).



The Unimog U 406 with snow plough is now available as a finished model (art. no. 6240RF; photo on the left) as well as a two-way vehicle in unfinished form (6242R; photo on the right). Photos: NoBa-Modelle

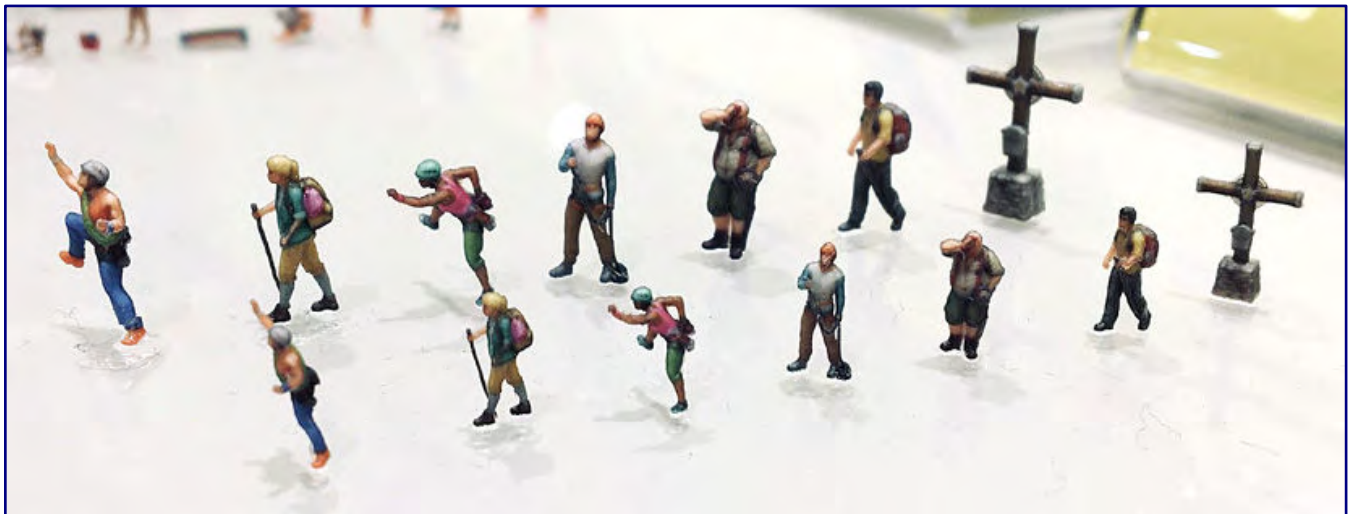
The Unimog U 406 with snow plough (6240RF) is now offered as a finished model. A new tandem trailer is also available for this vehicle, which was previously only available in unfinished form (6217R). The same applies to the quaint two-way vehicle based on the Mercedes-Benz Unimog U 406 (6242R), which can also perform shunting tasks in works traffic in conjunction with a ghost car.

[www.noba-modelle.de](http://www.noba-modelle.de)

### +++ Noch +++

Noch came up with almost revolutionary new products in Nuremberg. Multicolour 3D printing has not just been invented, but the Wangen-based company is the first manufacturer of this size in the model railway sector to venture into this technology.

The result for the Z gauge is also unparalleled, because the new figures impress with their excellent fineness, strong colour, smooth surfaces and no plastic shine. We were able to convince ourselves of this in person at the international toy fair.



This photo should underline our anticipatory praise for the new figures: In front you can see Z gauge mountain hikers with summit cross (item no. 44531), which are appealing in terms of size, proportions, colour as well as postures and offer a change from the mass-produced figures that have been common so far.

It is also commendable that the figures have been conceived with thought and creativity. They show far more than the typical postures that we have known from mass-produced figures up to now: A fireman climbs up a ladder, workers squat or kneel, a train conductor pulls a suitcase behind him, while a sun worshipper reads the newspaper and people sitting on a bench casually cross their legs.

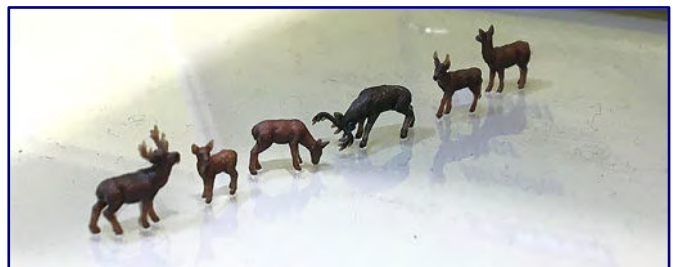
The mountain hikers, who show very different statures, and the animal figures, which are very well proportioned throughout the series, also look appealing. They are a clear leap forward, as can be seen in direct comparison with the cows from the injection moulding premiere. The production technology of the new 3D Master figures makes a wide range possible, even in smaller editions, so we hope for many more ideas from Noch!

Eight sets of human figures and three sets of animal figures (six figures per pack) are in the starting programme: firemen (art. no. 44500), construction workers (44501), railway personnel (44510), shunting personnel (44511), travellers (44520), seated persons (44521), swimmers (44530) and mountain hikers with a summit cross (44531).



Firefighters (44500), railway staff (44510), shunting staff (44511) and seated persons (44521) are four more of the eight new figure packs. Photos: Noch

The patterns of the cows (44540) and horses (44542) already look very authentic. For more than twenty years, Zetties have had to wait for a new edition of deer figures, but with Noch, a new supplier has now answered these calls: the deer (44541), as well as the two other animal figure sets, each include a young animal.



The three sets of animal figures each include a young animal. The black-coloured cows look much better than the injection-moulded figures from the same company. Deer (44541) have been a gap in the Z-gauge programme for more than twenty years!

The accessories specialist wants to continue a tradition with new XL tree packs from its Classic series. For years, the organisation “Plant-for-the-Planet” has been planting a real tree for every 100 model trees sold from these low-priced sets. More than 6,000 trees have been planted so far. In addition, the new packaging does not use any plastic.

This year, the range continues with the following: five fruit trees (25610) green or flowering (25615), deciduous trees (25620; all 8 cm high up to this point), autumn trees (25625; 8 - 10 cm), birches (25630; 10 cm), poplars (25635; 12 cm) and eight firs (25640) with heights between 8 and 12 cm.



New mini grass mats in the 45 x 30 cm format are intended for quick and uncomplicated use on the small surfaces of dioramas. They are included in the range in the versions "Flower meadow" (00008), "Meadow" (00007) and "Spring" (00006).

We hadn't noticed before that the sea foil, a classic for covering water surfaces, was only ever offered in colour at Noch. Now it has finally been added to the range with a wave structure and complete transparency in the 41 x 26 cm format (60852).



This photo shows a step in the application of the crumpled rocks: The cut to size and previously crumpled laminating paper is pulled apart again in large sections. Photo: Noch

The Knitterfelsen® (invented by Atelier Dietrich (see separate section in this article) seem to go down well: The three most popular designs now appear in XL format of 61 x 34.5 cm at Noch: "Wildspitze" (60307), "Großvenediger" (60309) and "Seiser Alm" (60311).

[www.noch.de](http://www.noch.de)

### +++ RATIMO Rainer-Tielke-Modellbau +++

Here, too, there are exciting and useful new products to report. Large air-conditioning units (art. no. 50075 & 50076) for mounting on the roof of large commercial and factory buildings are equipped with different air ducts.

The Kaelble Z6 W2AL 30 (50074) is a heavy-duty tractor with three axles that was built from 1939 onwards. The prototype was used by the Reichsbahn and after the end of the war also by the Bundesbahn in door-to-door traffic and heavy transport.



The road vehicles from the spring 2023 new products are compiled at a glance. Photo: Rainer Tielke Modellbau

The different versions of the Faun 912, which are new in the range, are based on military models. The heavy, three-axle vehicle was in the service of the young Bundeswehr and is offered without (50068) as well as with a tarpaulin (50069) and in each case with the bonnet open at the side.

Another new product is the Bundeswehr flatbed truck with accompanying trailer on which the M-3 push and tow boat is loaded (50070). Truck and trailer can also be separated.

[www.ratimo-z.de](http://www.ratimo-z.de)

[www.rainer-tielke-modellbau.de](http://www.rainer-tielke-modellbau.de)

### +++ Rokuhan +++

No new Rokuhan products are currently expected from the German distributor. However, two new versions of the DE10 diesel locomotive have been announced on the Japanese pages of the manufacturer.



Tobu Railway diesel locomotive DE10 1109 (item no. T012-11) in blue livery. Photo: Rokuhan

The Tobu Railway's road number 1099 (item no. T012-10) has the standard red-orange livery, the blue version with road number 1109 (T012-11) of the same company once belonged to JR Hokkaido. Parts and badges for the front railings are enclosed with the models.

Ztrack reports the availability of two single locomotive sheds with automatically closing doors (S070-1 / -2) as well as a locomotive wheel cleaner with power supply from the track (A063), which is already sold in Germany by Modellbahn-Union and has been presented and tested by us.

[www.rokuhan.com](http://www.rokuhan.com)



## +++ Schreiber-Bogen Kartonmodellbau (Aue-Verlag) +++

Today, the ruins in Berlin serve as a memorial against war. At Schreiber-Bogen, the Kaiser Wilhelm



Kaiser-Wilhelm-Gedächtniskirche (Art.-Nr. 809). Photo: Aue-Verlag

Memorial Church appears as a kit that reproduces the condition before the destructive fire in 1943. The miniature (art. no. 809) is on a scale of 1:250.

In the same scale, the fast steamer "Bremen" IV (3302) from the Hamburger Modellbaubogen Verlag is designed for very experienced modelers.

It consists of 1,915 parts, with alternative parts from the two lasercut accessory sets 4302 (details and railing) and 4303 (interior) even 4,982.

The finished model has a length of more than 1 metre. It can be flagged over the tops (pennants are included) and also shows an interior of the bridge, first class dining room and smoking lounges in its full configuration.

The new booklet 22 of the series "Aus der Geschichte des Kartonmodellbaus" (From the History of Cardboard Model Making) can certainly please friends of this model-making branch as well and regardless of scale.

[www.schreiber-bogen.de](http://www.schreiber-bogen.de)

## +++ Silhouette Modellbau +++

Silhouette sees its salvation this year in a new fantasy series that is to be further expanded. But orange, azure or bright red grass tufts and scattered flocks will hardly be an option for a model railway enthusiast.

Four new grass tuft variants with a fibre length of 12 mm - equivalent to 2.64 m - are not suitable for our needs either.



Fast Steamer "Bremen" IV (3302). Photo: Aue-Verlag

What remains can also be found in the same or similar form in the Busch range, especially as both suppliers belong to the same owner. The grass tufts with stones are varied here as follows: with dark stones for spring (art. no. 748-21S), summer (748-22S), early autumn (748-23S) and late autumn (748-24S) as well as in the same seasonal order with light-coloured stones (749-21S bis -24S).

[www.mininatur.de](http://www.mininatur.de)



## +++ SMZ Sondermodelle Z +++

On its pages, the Austrian small series manufacturer announces the SBB's double multiple unit Re 4/8 301 in the condition from 1939. This corresponds to its original appearance when it entered service.

This model is also driven on a bogie by an iron-core-less motor and can be ordered with either analogue or digital main board (with Next18s interface). In digital mode, the switchable functions include a white-red light change and an interior lighting.



The SBB's double multiple unit Re 4/8 301 "Churchill-Arrow" appears in various equipment versions. Photo: Sondermodelle Z

As a premium model, the model even has numerous sound functions, which are realised via an SD18a decoder from D&H including loudspeaker. The product equipment also includes true-to-size SBB pantographs from Sommerfeldt.

[www.sondermodellez.eu](http://www.sondermodellez.eu)

## +++ tams elektronik +++

Currently, no new products have been reported on its webpages. However, we would like to remind you that the popular Heißwolf speed controllers for analogue operation have been manufactured and distributed by Tams since February 2022.

[www.tams-online.de](http://www.tams-online.de)

## +++ Trafofuchs +++

Trafofuchs has three new figurine creations in its programme this spring: the barrel organ player with a small monkey is declared for era I, but this duo also fits into the two to three following eras - today, this relic of the past has admittedly become rare. It also fits on the weekly or Christmas market, in front of a railway station or once even in the beer garden.



Organ grinders (photo left) and the five dogs (photo right) are new to the Trafofuchs programme. Photos: Trafofuchs

A pack of figures with dogs had already been shown in Altenbeken 2022 as a pre-production model. These have been revised again for the 2023 programme so that the dog breeds depicted come out better: St. Bernard, Husky, Bernese Mountain Dog, Border Collie, and Labrador.

Two couples will soon be placed on the platform to greet each other affectionately or to say goodbye. After all, railway stations are symbolic places for great feelings, which give them their special charm in the model. More new products are to follow in the course of the year.

[www.trafofuchs.de](http://www.trafofuchs.de)

### +++ Uhlenbrock Elektronik +++

Last year, the Intellibox 2neo (art. no. 65150) was announced as a new product, but no explanatory information could be provided. Only its integrated WLAN function was already known, which also enables the use of the Rocrail control programme, for example.



Farewell and welcome scenes enable the two couples. Photo: Trafofuchs



It is also worth mentioning that the delivery package includes an adjustable 75 VA power supply unit. This is the key for Z gauge use: a switchable output voltage of 11, 15, 17 and 21 volts is now possible. This is the first time that this manufacturer has explicitly declared its control unit to be suitable for Z gauge.

Photo left: Intellibox 2 neo (Art.-Nr. 65150). Photo: Uhlenbrock

The new control unit is also compatible with the ABC braking system and now also supports the MFX data format. These and other innovations are of little or no interest for our scale, but if you are thinking about buying a new device and want to use it in different scales in parallel, you should read up on all the features on the manufacturer's pages and include the Intellibox 2neo in your decision making.

[www.uhlenbrock.de](http://www.uhlenbrock.de)

### +++ Velmo +++

Velmo will announce new products when a delivery date can be planned and foreseen. In recent years, high product demand and a simultaneous chip shortage have stood in the way of business plans.

There are also open development projects that had to be postponed for a longer period of time for the reasons mentioned above.

[www.velmo.de](http://www.velmo.de)

### +++ Viessmann +++

Viessmann is also the owner of Kibri and Vollmer, which is why we want to look at all three brands in a joint article here. Under the core brand Viessmann we have discovered an electronic module that we would like to present in more detail.

This new module "LED dimmer, double" (art. no. 5590) enables the operation of individual light-emitting diodes and LED light strips with stable, adjustable brightness. For this purpose, it supplies the consumer with a current that can be adjusted in steps and is largely independent of the supply voltage of the module and the characteristics of the LED. LED light strips may have up to 7 LEDs connected in series.



The module "LED dimmer, double" (art. no. 5590) supports lighting wishes on the model railway layout. Photo: Viessmann

Of course, it is also suitable for the manufacturer's LED strips. Independently of each other, the brightness can be set for each of the two outputs with four binary DIP switches each via the current intensity.

The adjustable currents are approx. 1 to max. 31 mA, the maximum input voltage must not exceed 24 V.

At the same time, Viessmann introduced a new material for kits from its Kibri and Vollmer brands.

Going by the name "Polyplate, it is based on a new manufacturing process. We were allowed to hold a building sample made of this material at the toy exhibition and are thrilled with the feel and look of it.



The new material is a composite material that shows a realistic matt and fine surface and feels something like a real plaster surface. Viessmann explains that kits made of this material can be provided with the finest engravings and structures. Further advantages are listed as follows:

- Paintable, UV-resistant and insensitive to moisture,
- Extremely stable, shock- and tear-resistant,
- Mitre joints at building corners can be implemented without joints, and
- Gluing is possible with commercially available white glues and special glues.



The small workshop with plaster / half-timbered façade (Vollmer 49590) raises great expectations for us, because the material used inspired us at the International Toy Fair. Photo: Viessmann

This material is celebrating its premiere in the Vollmer range for Z gauge: The small workshop with plaster/ timber-framed façade (49590) is ideally suited to introduce this material to customers and finally bring some welcome variety to the layouts of the smallest scale. We are curious and would like to share our own experiences with our readers.

[viessmann-modell.com](http://viessmann-modell.com)

### +++ WDW Full Throttle +++

From the series of reconstructed wooden wall refrigerator wagons with 34' length, a new example "Hormel" appears for food transport (art. no. FT-9203-1). The brown-red wagon with yellow side walls bears the unmistakable green logo of the advertised brand. The wagon is registered to the advertiser URT.

[www.wdwfullthrottle.com](http://www.wdwfullthrottle.com)



"Hormel" refrigerated wagon for food transport (Item no. FT-9203-1). Photo: WDW Full Throttle



The telephone box (item no. 6861) and the conveyor belt (6863) are shown here as exemplary H0 gauge models. Photos: Weinert Modellbau

### +++ Weinert Modellbau +++

Injection-moulded from plastic is a new, curved corrugated sheet roof (art. no. 6862) for do-it-yourself construction. Two pieces are supplied per packaging unit. A conveyor belt (6863) made of brass etched parts and an era IV telephone box with closed door (6861) made of brass etched and cast parts are supplied as kits.

[www.weinert-modellbau.de](http://www.weinert-modellbau.de)

### +++ Werkzeuge Peter Post +++

A new "flat-nose pliers 45° oblique" (art. no. 071008) has entered the tool range. They are 135 mm long and have 11 mm long and 3 mm thick jaws. The length of the bevel is 4 mm. It weighs 50 g and is intended for holding, attaching and closing eyelets or chain links.

These remain straight because, depending on the size of the workpiece, almost a semicircle can be gripped. The pliers are also suitable for bending photo-etched parts.

Furthermore, there are plans to include some Microscale products in the range; contact has been made with the manufacturer accordingly.

These are Micro Set and Micro Sol for applying and fixing decals to models, i.e., adhesion promoters and softeners, as well as Micro Mask for covering model parts (such as windows) before spray painting. However, a date cannot yet be given for this.



The new "Flat-nose pliers 45° oblique" (art. no. 071008) is useful for bending eyelets and chain links. Photo: Werkzeuge Peter Post

[www.peter-post-werkzeuge.de](http://www.peter-post-werkzeuge.de)

### +++ Yellow Dwarf +++

The new “Flat-nose pliers 45° oblique” (art. no. 071008) is useful for bending eyelets and chain links. Photo: Werkzeuge Peter Post.

Particularly impressive and unique is its fountain (art. no. 60020), which becomes the eye-catcher of a market or town hall square. For water sports, 2 inflatable boats (60106) and 6 canoes (60101) are offered.

The 5 beehives (60301), where the contemporary bee boxes have different heights, and the bouncy castle (60352) for children's parties were a first in our scale. We also cannot remember having seen whirlpools (60466) from any manufacturer.



The fountain (art. no. 60020) is a small but effective eye-catcher. Photo: Yellow Dwarf

More familiar are the following items that complete the starting programme: 10 wooden crates (60031), 5 pallets with bags (60204), 10 metal barrels 200 l each (60012) or PVC barrels 220 litres (60011) and 20 wooden barrels (60015). Familiar, but nevertheless well done, are also the 2 skips (60209), one of which is reproduced empty or fully loaded.



Bouncy castle (60352; photo left) and the 2 inflatable boats (60106; photo right) close assortment gaps in Z gauge. photos: Yellow Dwarf

At the time of our editorial deadline, 9 public waste bins (60046) and 10 plastic barriers (60237) for blocking and guiding traffic in construction areas were added to the programme.

[www.yellowdwarf.eu](http://www.yellowdwarf.eu)



### +++ Zahn Pinsel +++

The company Zahn Pinsel presented itself at the Toy Fair as a hitherto rather unknown supplier of paintbrushes in the model-making sector. The company from Bechhofen manufactures a wide range of brushes of high quality for various brands across all forms of application.

However, the trade fair presentation was probably important against the background that this company has already bundled its competences for model-making and miniature painting brushes with Springer Pinsel GmbH in spring 2022, a quality brand to which we had already referred in a report on Intermodellbau.

Their profound expert knowledge in this area, which has grown over decades, was to be retained and, if possible, further expanded within the framework of a regulated company succession. And so all Springer employees transferred to this company in spring 2022.

For the customers who appreciated the outstanding quality of Springer products, this means preserving the brushes they have come to love. They now only bear this new name.

It is important to know, however, that brushes can only be ordered ex works in quantities of 120. End customers should therefore turn to retailers for high-quality brushes.

[www.zahn-pinsel.com](http://www.zahn-pinsel.com)

### +++ ZDC Z Dream Collection +++

Some developments are in progress, but not yet ready for release. They will be announced in the current monthly issues.

[z-dream-collection.jimdosite.com](http://z-dream-collection.jimdosite.com)

### +++ Z-Doktor Modellbau +++

Björn Pluka is currently producing John Deere tractors for the American market (photo on page 69 below). Thanks to a new 3D printer, print lines are now almost invisible. With the Kö 1 small locomotive, a stock product also benefits from this.

[www.z-doktor.de](http://www.z-doktor.de)

### +++ Zimo +++

Since December 2022, a programme update for the MX10, MX10EC, MX32, and MX32FU units has been offered on the manufacturer's pages. Production of the MN series decoders (without sound functions) is to begin soon. The MN180N18 model, intended for N gauge, could also be interesting for Z gauge, but no dimensions have been given yet.

[www.zimo.at](http://www.zimo.at)



“Pin Point Brush” with curved ferrule (angled tip). Photo: Zahn-Pinsel

## +++ Zmodell +++

Before the attack on Ukraine, at the request of numerous customers, a new load was developed for the Snps 719 type double stanchion wagons from the Märklin programme. In the meantime, the prefabricated parts could be shipped to Germany and handed over to the ordering dealer.

For Zmodell, it is one of the most advanced cargo inserts, using the highest quality materials and the best technologies for a realistic look. The tubes are thin-walled and made of 304 stainless steel, with a spray-painted finish on the outside and inside: On the outside, the photo is characterised by a thick and silky-matt glossy coating in black, while on the inside, each tube has a matt reddish-brown appearance.



The new tube loads are available in three different printed versions from well-known German steel producers. Photo: Zmodell

A high-quality UV print with 1,440 dpi resolution has been applied to both sides of the tubes. These show the logo of one of the following three tube manufacturers: Mannesmann (art. no. ZM-MRK-SNPS-014A), Hoesch (ZM-MRK-SNPS-014B), and Thyssen (ZM-MRK-SNPS-014C).

The tubes are bundled in threes, secured by bands of 0.2 mm thick, photo-etched and orange-painted nickel silver. On the underside of the load there are non-slip supports made of 0.5 mm thin natural wood veneer. They fit exactly between the ribs on the underside of the wagon and prevent the load from slipping during the journey.



The elaborate detail design is particularly evident when looking at the tube heads. Photo: Zmodell

The range of these loads is to be expanded in the future with various pipe sizes, including, for example, those for the flat wagons Res of the standard design.

[www.facebook.com/Zmodelltrains/](https://www.facebook.com/Zmodelltrains/)  
[zmodell\(at\)ukr.net](mailto:zmodell(at)ukr.net)

### +++ 1zu220-Shop +++

A current new product is the "Westheimer" beer tank car (item no. 98198) produced by Märklin in an edition of 200 with an ivory-coloured background and harmonious effect. It completes Märklin's own beer wagon series with an additional model which, like all its predecessors, has been designed to be very close to operation and is not only intended to appeal to collectors.



The ivory-coloured Westheimer tank car (item no. 98198) is the latest Märklin special model of the 1zu220-Shop.

Further expansion of its own products and special series are certainly to be expected in the course of 2023, but also require free production capacities and a receptive market. We will then promptly provide information in our magazine on what can be expected in the area of our own building series and further rolling stock appearances, as well as additions to Märklin Insider products.

[www.1zu220-shop.de](http://www.1zu220-shop.de)



The new John Deere tractor from Z-Doktor Modellbau (see page 67) was designed after US models and is shown here in used as well as new condition. Thematically it fits very well with the American barns from Archistories.



## Large exhibition in Bad Schwartau Northernmost Z Gauge Exhibition

*After four years since the last meeting, the Eisenbahnfreunde Bad Schwartau were delighted to be able to invite people to a model railway exhibition again. Many participants followed the call, particularly from the Z gauge community. Ralf Junius from the Trainini® team was also there as an exhibitor, and now reports on the impressions from the far north.*

By Ralf Junius and Jan Tappenbeck. On the weekend of 21 and 22 January 2023, the Eisenbahnfreunde Bad Schwartau were finally able to hold a model railway exhibition again, after the Corona pandemic also demanded a forced break in the previous two-year rhythm there.

A total of three exhibition halls at the Elisabeth Selbert Community School were transformed into a Mecca for model railway fans of all gauges after school closed on Friday afternoon. Exhibits in gauges 1, H0, H0e, TT, N, and Z now arrived for the eagerly anticipated weekend.



The exhibition in Bad Schwartau became the "northernmost Z gauge convention" and was also characterised by a lot of local colour: The modules of the Eisenbahnfreunde Bad Schwartau namely followed well-known, regional blueprints.

The question of how this meeting would start up again after four years remained exciting until the end: Would the model railway fans and those who wanted to become one take advantage of the opportunity, or would they stay at home? When the doors opened on Saturday, it was soon clear that the northernmost Z-gauge exhibition was going to be a success.

Numerous vendors also participated and made sure that missing material for one's own model railway could be purchased or even a long-desired train car could be found. In the following, I would like to give you a few of my impressions from on site, which are limited to our Z gauge, as an exhibitor, I did not get around any further.

The organisers reserved the entire Krummlandhalle for us Zetties and proclaimed the meeting “Germany’s northernmost Z scale convention”. I can confirm that this title fits, because there were really many and above all very interesting model railways in 1:220 scale to be admired. Even the visitors who were not familiar with the scale quickly noticed that.



Apparently, the joy of personal meetings and exhibition visits outside virtual spaces was high, because the hall was always well attended: After all, 3,100 people found their way to Bad Schwartau.

Torsten Schubert presented his illuminated models and dioramas equipped with them. He gave soldering demonstrations several times a day, which were always very popular, also high up in the north. One of the longest journeys was made by Matthias Vogel, who presented his “Dingenskirchen” console railway. This layout does not require any landscaping and is limited to rolling stock, railway lines and a few buildings. With this “minimal concept” it unfolds very special and probably unique charms.

Small-series manufacturer FR Freudenreich Feinwerktechnik exhibited his own models and his Swedish layout as well as a new diorama by Avantgarde Modellbau (Phillip Meyer). On this diorama, the FR models presented were impressively reflected on the water surface of the lake in the display. Georg “Schorsch” Müller showed his “Papperlapapp”: Quite a few people were amazed at what can be built from paper on a scale of 1:220, if someone creatively knows his craft.

Kai Wüstermann's Era I diorama “Müsum 1896” once took part in the layout competition of the (now discontinued) Eisenbahn-Journal. Trains shuttled from the staging yard to the pier. During the construction, Kai even attached great importance to the smallest details, as can be seen for example on the wooden landing pier.

continues on page 74





Torsten Schubert (photo above) also gave soldering demonstrations in Bad Schwartau, while Matthias Vogel gave a vivid demonstration of the minimal concept of model railways with his console railway "Dingenskirchen" (photo below).





The new Sweden diorama (photo above), which FR Freudenreich Feinwerktechnik presented, comes from the workshop of Avantgarde Modellbau. Georg Müller's "Papperlapapp" (photo below) bears its name because of paper, the material used here.





A feast for the eyes is the operational diorama "Müsum 1896" by Kai Wüstermann, which skilfully turns its attention to Era I.

The layout "L'Isle sur la Sorgue / Provence" celebrated its premiere. It was the last project of the deceased Wolf-Ullrich Malm and was originally supposed to be on display for the first time at the Intermodellbau in Dortmund in 2020. But this did not happen, because its creator had to cancel his participation due to illness even before the start of the Corona pandemic. The family of the deceased, who had travelled to Dortmund, was visibly thrilled by the premiere, which has now been made good for.

So, this beautiful France layout not only reminded us of beautiful landscapes and special lifestyle, but also of a very valued member of our Z-gauge community who we miss very much. Some members of the Eisenbahnfreunde Bad Schwartau put the finishing touches on this layout to make it ready for display.

On site, they were always available for questions and explained everything about the reproduction of the small town in Provence. In the meantime, a TGV, among other things, repeatedly whizzed past the attentive spectators.

Together with the "Freundeskreis der Spur-Z Hamburg", the organising group showed a modular layout. One of the most prominent motifs on it is the wooden Rhine bridge built by Gaston Burkhardt, the former woodwork talent of the Miniatur Wunderland.

I would also like to briefly mention the "Ratekau" section by Hans-Jörg Ottinger ("Otti"). Here, many buildings from his homeland, including the fieldstone church, have been faithfully reproduced, and not only that: a current topic has also found its way onto it. A demonstration at the railway station against the Fehmarnbelt tunnel project was joined by local politicians in the miniature.

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Wolf-Ullrich Malm's Provence layout (photo above) celebrated its exhibition premiere in Bad Schwartau and set up a worthy memorial to our sadly deceased community member. The weekly market recreated on it (photo below) offers everything required in exquisite, French cuisine - including fresh herbs and spices.





The modules of the Eisenbahnfreunde Bad Schwartau also demonstrate against the fixed Belt crossing (photo above). After all, this large-scale project is also associated with ecological concerns and problems, and in the north, it is also about jobs in the vicinity of the previous ferry operation. The arched bridge over the Rhine (photo below) is the pride of its model operators because it was built by Gaston Burkhardt, the former “woodworm” from Miniatur-Wunderland.





The model of the fieldstone church is located in Ratekau, and the exhibition co-organiser Hans-Jörg Ottinger set it on the module of the same name.

Well-known, but always beautiful to look at, are the modules presented by Peter Burgard and Birgit Foken-Brock (Trafofuchs). Whether it's a fairytale forest, a small-town railway station, a brewery or romantic railway film footage, the viewer can't help but revel. The same was true for Rainer Tielke's Bullay double-decker bridge over the Moselle. In addition to the many trains, visitors were once again delighted by the equally mobile car models one floor below.

The local club modules of the Z-Freunde Region Hannover, which were put together to form a circular track, were used to demonstrate the digital possibilities of Z gauge. The Mobile Station 2 from Märklin was used for control, which was made really mobile for this purpose, i.e., it could be operated independently of the grid.

Bernd Ruck had something different to show. He had set up a layout with its own atmosphere out of small bricks. More familiar was the landscaping on his winter layout, which, however, put the focus on a season that is so often neglected.

Variety was also provided by the tracks laid out on a layout poster from the Märklin magazine 5/2022, which is intended to suggest a simple start in our scale. Another exhibit with an equally simple track plan showed the next level of design.

Not missing was also a demonstration layout with teaching aspiration by Ulrich Theile, which we had also previously admired in Altenbeken. I myself had my "strawberry box" with tiny oval for the youngest visitors



The track figure, which was laid out on a layout poster of the Märklin magazine, was limited to the minimum: This is another way for interested folks to get started with model railways!

and my new peek-a-boo box “PeZuBox - Am Freizeitpark”, which could give me further construction experience.

I have not yet mentioned the layout “Along the Rhosel” by Regina and Jürgen Wagner. Here, the beautiful motifs of the wine-growing areas on the Rhine and Moselle are representative of a world cultural heritage site and inner-German tourism.

Equal mention should be made of the Stammtisch Untereschbach e.V., which had brought along various objects, including the football stadium or a new paper replica of the former district court of Bad Schwartau or Dietmar Stäbler's scrap yard, which was also shown in Altenbeken. A tablet control was also demonstrated on a showpiece.



The opposite approach of a fully equipped layout with individual background scenery is taken by Rainer Tielke's Bullay double-decker bridge. It could also be viewed and admired in Bad Schwartau.

As a souvenir of the exhibition, there was a special railcar from “Nordic Rail Service,” a company from Lübeck, from the Eisenbahnfreunde Bad Schwartau, in matching style with the venue. This side unloading wagon was produced by Märklin (item no. 98180).





The exhibitors of the Stammtisches Untereschbach e.V. had recreated the former district court of Bad Schwartau out of paper, to offer visitors something familiar from their surroundings.

Another one was contributed by FR Freudenreich Feinwerktechnik (edition: 51 copies), which also showed a local reference. A modern, open freight car of the Eanos type (49.337.11) was chosen here, which featured the design and address of Richter Baustoffe. This company is also based in Lübeck.

Let's return to the initial question of how well this exhibition would be received. Hans-Jörg Ottinger, Jan Tappenbeck and the rest of the team did their best to organise an all-round successful event; last but not least, energetic public relations work was part of it.

And this, we may now resolve. It was well attended on both days and resulted in a new record number of visitors. 3,100 visitors found their way to Bad Schwartau and left the hall visibly pleased.

With this northernmost Spur Z convention, the Z gauge world finally has a third location for such a meeting of our scene. In addition to Altenbeken and Zell an der Mosel, we should also include Bad Schwartau in the calendar in the future!



Last but not least, we show the two special wagons for the exhibition: staged on the "PeZuBox - Am Freizeitpark" by Ralf Junius, here are the Märklin model (left) and the souvenir of this successful event which was produced and marketed by FR Freudenreich Feinwerktechnik.

If there is to be another model railway exhibition like this in Bad Schwartau in two years' time, which I firmly believe there will be, then I will try to be there in any case.

Alle Photos: Ralf Junius



**Z gauge Exhibitors:**

<https://www.efs-badschwartau.de>  
<https://fr-model.eshop.t-online.de>  
<https://paperlapapp220.de>

<https://www.rainer-tielke-modellbau.com>  
<https://stammtisch-untereschbach.de>  
<http://www.trafofuchs.de>

<http://www.z-lights.de>  
<http://zotti.lena-johannson.de>  
<https://www.z-spur.de>

**Video reports from the exhibition:**

<https://www.youtube.com/TraininiTV>  
<https://youtu.be/qrMg0xVUvxc>





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Readers' letters and messages

## Zetties and Trainini in Dialogue

**Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.**

**A thank you on behalf of all the winners:**

I received the 'Z scale 50 years' book this week. It's wonderful, thank you so much. Beautifully made in content and quality.

Even without speaking German, there is so much visual material, but if you made an English version, I'm sure it would sell very well. The book is a credit to the world of Z, to Märklin, and to all at Trainini®. Well done.

Rick Pullin, Quarteira (Portugal)

**Thank you and suggestion:**

I would also like to thank you for the great work you are doing. Especially this anniversary year, together with the initial restrictions of COVID, has meant a particularly great deal of effort. Bravo!

In connection with your magazine, I would like to make a suggestion: I would like to see an article exchange among the readers! We are quite a small community on this planet, and, therefore, it would certainly be feasible to contact us by e-mail or telephone with a photo and an idea of price.

Every model railroader has a lot of material that is no longer needed, and on the usual platforms such as eBay and other classified ads, you hardly get noticed as a Z model railroader.

Wouldn't that be worth considering?

Joseph Gurber (France), per E-Mail

**Gratitude for the editorial award:**

This is a big surprise for both of us. We are very happy about this recognition. It's always nice to get a confirmation for the work Jan and I have put into this layout. It is also an incentive for us to keep working on layouts now and in the future.

Pieter Willems (Belgium), per E-Mail



**A little too short-sighted:**

While reading your wonderful articles, something struck me, possibly?

Comparison of the photos of the Märklin models 8857 (BR 151 green) and the Märklin Hectorrail 88262 grey! All it takes is a different paint job, a different pantograph colour and you have a Hectorrail 88262? Bell-shaped armature motor, of course!

Karl-Heinz Neumann, Lohfelden

Editor's answer: Unfortunately, it is not quite that simple! The 162 007 from Hectorrail is one of those machines that was modernised by the Deutsche Bahn AG years before it was sold. They received single-arm pantographs and angular high-performance buffers, which Märklin has also correctly reproduced on its model through modifications. From our point of view, it would not be worthwhile to build this version on the basis of an old model because of the manual effort required for repainting and re-labelling, as well as the financial cost of procuring the necessary parts.



The differences between the green Märklin model 8857 and the recently delivered Hectorrail variant (art. no. 88262) are not only the colour and a newer drive: the pantographs and buffers of the models are also different.

**Current Märklin deliveries:**

There was only one delivery during the reporting period. Shortly before the editorial deadline, the MHI anniversary starter pack "50 Years Mini-Club" (art. no. 81873) was put on sale. Its design and composition are reminiscent of the first starter pack from 1972.





The train from the anniversary starter set 81873 is shown here in full length and detail shots of two cars. Photos: 1zu220-Shop

In addition to rolling stock and track, a class 216 diesel locomotive (from the more recent design) and three passenger coaches printed to today's technical standards are supplied. The contents thus highlight how impressive the progress in the production area of Märklin has been over fifty years.

#### **A visit to Mannheim:**

From 10 to 12 March 2023, Faszination Modellbau (<https://www.faszination-modellbahn.com>) will once again take place in the Maimarkthalle in Mannheim. Z gauge will also be represented there again, organised by the Z-Freunde International e.V.

In addition to some regulars with their layouts and dioramas, an exhibit will also be announced under the title "The High Seas Port", which is unknown to us so far. The concept idea is to combine various paper models with the model railway.

It is true that not all scales of the shoring fit exactly to each other, but the elevated suburban railway line is supposed to remind of the port of Hamburg, although the motif was designed completely free. In addition to our scale, all other common scales are also represented on site.

#### **A conclusion from Märklin for the years 2021/22:**

In a press release Märklin looked back on the past business year and expressed its expectations for 2023. The trade and also Märklin itself had adjusted quickly and well to the new challenges and mastered this period well.

"In the past 2021/22 fiscal year, we achieved our sales targets and even slightly exceeded them with 131.4 million euros", said Florian Sieber, Managing Partner of Märklin, expressing his satisfaction with the development during the Corona period.



Florian Sieber reported on the 2021/22 business year of the Märklin company and gave an outlook on the current year.

In the current business year Märklin has been and still is confronted with new, equally great challenges: Delivery problems with electronics, cost increases in production processes, raw materials and energy, and even wages.

And so he sums up: "We were not able to achieve the result of the previous year, but we are still well above the level of the years before the pandemic. For the current business year, we plan to achieve a turnover of around 128 million euros."

There is also a look ahead to 2023: after four years, Göppingen is once again to become a Mecca for the model railway industry, so the Märklin Days with the International Model Railway Exhibition will take place again.

The three days are planned from 15 to 17 September 2023, when there will once again be an opportunity to visit the only exhibition that brings together model railways with their historical originals.

#### Things to see on the On traXS:

Finally, the model railway exhibition at the Utrecht Railway Museum is back! And finally it deserves its traditional name again, because the layouts exhibited there are now once again selected by travelling ambassadors of the Mitropa model building collective. You can't apply for this exhibition, you will be discovered.



With Boisschot Statie, an exceptionally successful gauge Z layout will be on display at this year's On traXS!



And for the second time, a Gauge Z layout is among the chosen exhibits: The layout "Boischoot Statie" by Pieter Willems and Jan van Casteren, which we awarded as the new publication of the year 2022, will be on display there from 17 to 19 March 2023.

This is a special honour for our gauge and of course its two builders. But it is also such a great pleasure that a visit to the exhibition is also planned on the part of the editorial team. A trip to Utrecht is always worthwhile.



From 17 to 19 March 2023, the Utrecht Railway Museum once again invites you to the probably most renowned model railway exhibition of Europa.

This is not only due to the other high-quality layouts of many scales, but also to the participation of small-series manufacturers from the Benelux region, and especially to the special atmosphere of this railway museum, which is unique in its form.

If you are interested in visiting the museum yourself and would like to underline the interest of our construction size, you will find all the necessary information on the following museum and event pages:

<https://www.spoorwegmuseum.nl/de/ontdek/nu-in-het-museum/modellbahnausstellung-on-traxs/>

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Board of Editors:  
Holger Späing (Editor-in-Chief)  
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Joachim Ritter

Correspondent North America:  
Robert J. Kluz

English translation:  
Alexander Hock, Christoph Maier, Oleksiy Mark, Martin Stercken

Further voluntary work: Stephan Fuchs, Ralf Junius (**Trainini TV**), Torsten Schubert

Licensed **Trainini Discussion Group** (<https://www.facebook.com/groups/1597746057122056/>): Michael Etz (**Trainini Locomotive Doctor**)

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# Trainini

German Magazine for Z Gauge



On the Move in Narrow Gauge

Permanent shortage V 100  
New Ways of Cleaning

## Introduction

Dear Readers,

Since the outbreak of the Corona crisis, that is, for about three years now, I am probably not alone in feeling that the number of new products supplied has been declining. In fact, this is not just a feeling, but a verifiable fact.

If we follow, for example, the delivery schedules, means the delivery schedules of Märklin or also the explanations of annual financial statements and sales, we notice, on the one hand, repeated postponements and are also directly told how bottlenecks on the world market continue to the end of supply chains.

This is unfortunate, but cannot be changed. After all, in our hobby it should not annoy us. Let's make the best of this situation; that is also the common thread running through this edition.

It is number 212 and the first topic is about the class 212 of the Bundesbahn. It simply couldn't fit any better. For years, the Märklin model has been in great demand, each new edition is sold out in no time at all. So, what to do when one's desire does not come true?

Michael Pleiner, who likes to build with paper and cardboard, has acted from necessity. He has found a way to build his dream model himself. He tells us about it in this edition and gives an idea to those who have experienced something similar.

Reinder Rutgers also had no other choice but to help himself, because he is a narrow-gauge fan, and that is a largely neglected subject in Z gauge. If it becomes smaller than the converted metre gauge, then there is no longer anything to buy.

He once briefly presented the solutions he had found for himself in a letter to the editor and thus triggered many calls for a detailed report on this project. He has prepared two parts, the first of which we will begin with today. You can be curious to see how creative this path has been!

There is also a shortage of supplies on the book market. High-quality printing paper has been scarce and thus expensive for some years. That's probably why many titles seem to be on hold and are substantially delayed. This is also noticed by us, who are aiming for two reviews per edition.

In addition to a reissued classic, we are therefore also presenting an antiquarian book in this edition and, after a long time, would like to focus once again on great publications that are not known to many or have largely been forgotten.

Of course, I have not summarised all the contents of this edition, but that is probably negligible. You will soon have an overview when you have read through everything. Don't be surprised that this time you have a thinner edition in front of you. We haven't run out of topics — we're just investing our time in new projects on which articles will soon appear here. Enjoy reading!

Holger Späing



**Holger Späing**  
Editor-in-chief



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Currently no items

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Currently no items

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We thank Michael Pleiner and Reinder Rutgers for their contributions.

Date of publication of the German language version of this issue: 26 March 2023

### Cover photo:

The romantic idyll of the narrow-gauge railway is deceptive. The small diesel railcar has already been in service for a few years and will certainly not be able to prevent the ending of this narrow-gauge railway. Photo: Reinder Rutgers

V 100 on a Rokuhan base

## Born out of Necessity

*For 22 years, the DB's V 100<sup>20</sup> has been part of Märklin's Z gauge programme. Every new edition is out of stock at lightning speed and carries fantastic second-hand prices. This creates longings by many Zetties and leaves them with unfulfilled wishes. Our reader Michael Pleiner therefore took the initiative and shows a way out for those who like to tinker.*

By Michael Pleiner. When I tried to buy the V 100 offered in the current Märklin catalogue in October 2022, the dealers in the München (Munich) area did not have the locomotive in stock, nor was the locomotive available for dealers to order from Märklin.

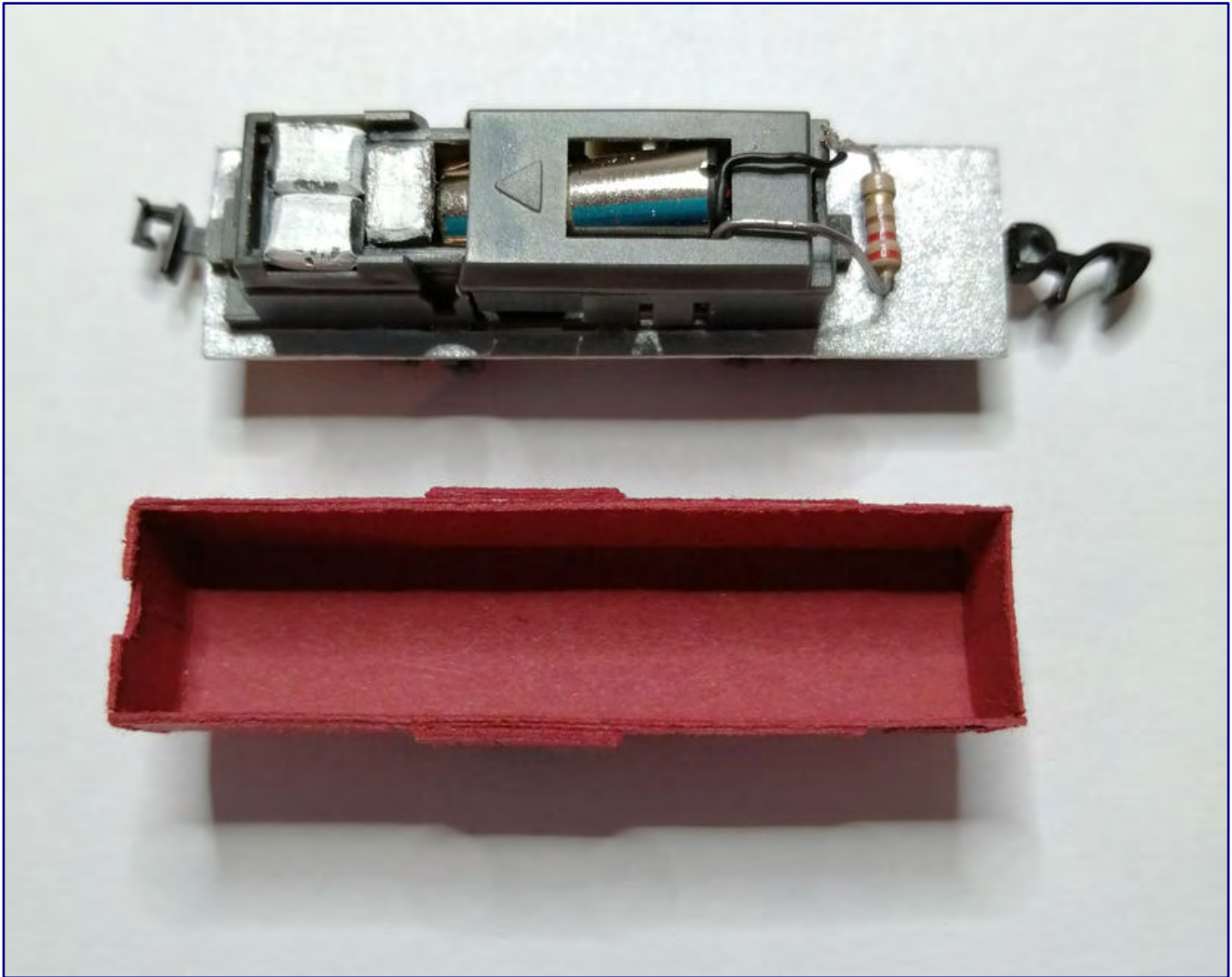
However, I had a Rokuhan shorty at my disposal and found that the 10 mm axle spacing in the bogie corresponds to that of a V100.



Compromises are unavoidable, but on the layout the completely self-built V 100 cuts an undoubtedly good figure in front of its train.

In many YouTube videos you can see the Shortys cruising leisurely over the tracks. However, if a Märklin transformer is used as the power source, the driving characteristics of the Shorty resemble those of a New Year's Eve rocket.

A series resistor of 110  $\Omega$  solves this problem. Higher resistance values allow an even softer and smoother start-up, but the final speed is then pretty low.



For acceptable driving characteristics at the Märklin transformer, a 110  $\Omega$  resistor has been soldered in (top right).

The width of the superstructures of the V100 bonnets is 9 mm in 1:220 scale. However, the chassis of the Shortys are just under 11 mm wide and have a length of 40 mm. Without taking into account the detents for Shorty housings, the length of the chassis remains 37 mm. The scale length of the chassis of a V 100 is 50 mm without buffers.

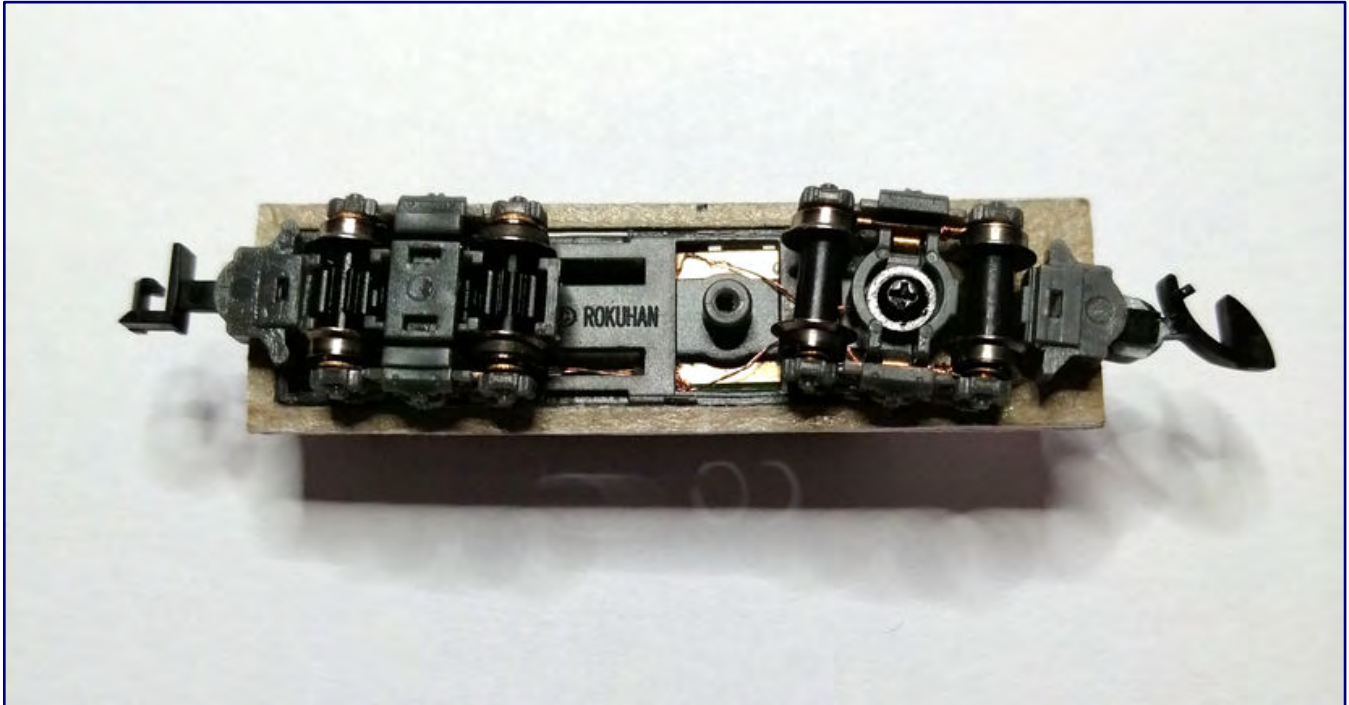
The basis of my V 100 is a 1 mm thick piece of cardboard 50 mm long and 14 mm wide. A rectangle of 37 x 11 mm was cut out to hold the shorty. The distance to the long sides and to one of the narrow sides is 1.5 mm. The resulting base plate was glued to the detents from below.

For the bogie of the Shorty a new pivot was mounted, which is offset by 10 mm. It has a height of 2 mm, an outer diameter of 3 mm and an inner diameter of 1.5 mm for the fixing screw.

On the bogie, the current conducting plates pointing vertically upwards had to be bent horizontally outwards. These plates normally contact the traces of a printed circuit board inside the shorty, and because the bogie that has just been moved, this is no longer useful.



Therefore, 2 mm wide copper sheet strips now conduct the traction current from the running bogie to the circuit board. On the base plate the superstructures of the V 100 have a scale total length of 49 mm and a length of 48 mm at the top because of the slant of the fronts.



The basis of the self-build is a cardboard plate in which a shorty bogie was placed. The non-powered bogie was placed further out to increase the overall axle base prototypically.

The superstructure of my locomotive consists entirely of purple construction paper in several layers. I started with a U-shaped blank that extends over the entire length. Then came the fronts, the sliding bonnet, the ventilation grilles, and the driver's cab.

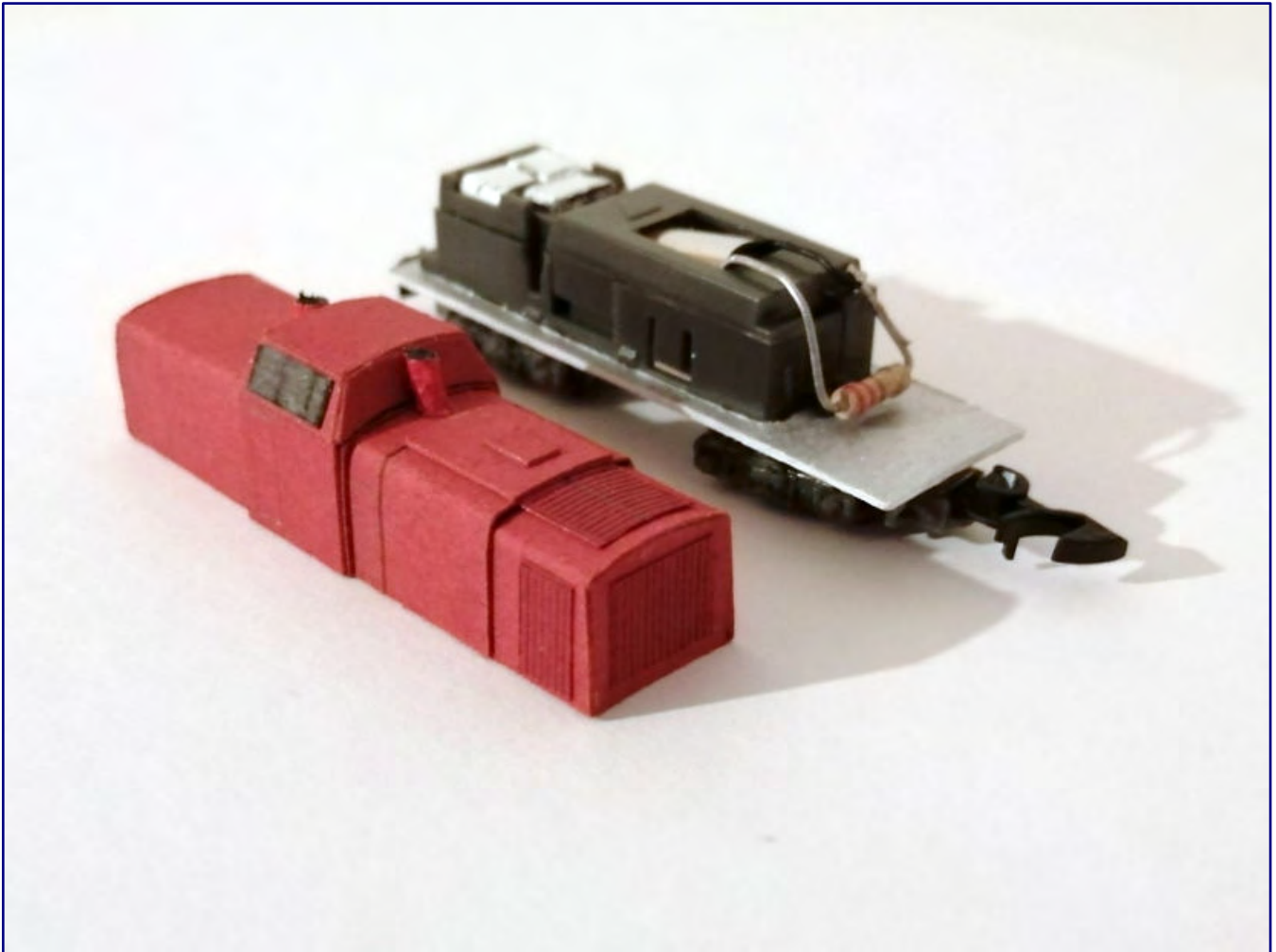
I drew the maintenance hatches and doors with a pencil. The windows of the driver's cab, on the other hand, are glued on and are made of anthracite-coloured construction paper. The exhaust pipe and air intake are imitated with red painted toothpicks.

Silver-coloured paper was then used for the top. The white trim strip is glued on, the locomotive's lamps I painted with thick white paint almost plastically. Because of the detent of the shorty, a small piece had to be cut out at the end of the shorter bonnet, so that the body can be put on the base plate.

The buffers could be reproduced with the help of black painted pin heads. The side plates underneath the base, as well as the fuel tank, are again made of paper. The couplers are regular exchange couplers from Rokuhan.

Since the Shortys are made of plastic and therefore very lightweight, I found it necessary to use lead to weigh down the loco. I also replaced the standard metal weights with lead.

Unfortunately, the Shortys have only one driven bogie. This factor as well as the low friction weight take their toll and limit the possibilities of use. Originally, my V 100 was intended for light passenger train service with two four-axle rebuilt coaches, but even in tight curves and slight gradients the locomotive stops with spinning wheels.



You can easily see how the locomotive body of the V 100 is made of different layers of construction paper. The exhaust pipe was once a toothpick that has been painted to match.

The reason for this is the mass of coaches, because they are equipped with metal weight from the factory. Nevertheless, my V 100 is not a pure showcase model! I use it consistently in light goods train service.

Due to the out-of-scale width of 11 mm (instead of the correct 9 mm) the locomotive looks a bit squat.

However, this is not noticeable for the viewer at first, as the locomotive shows all the characteristic features of a V 100, and is also perceived as such.

It remains impressive how a personal dream model can be created with relatively manageable effort and a reasonable degree of difficulty.



I, for one, enjoy my Rokuhan-Shorty-based locomotive, which was born out of necessity and lack of availability, and I hope that this will inspire other Zetties who feel a similar lack to build their own.



For our reader, a wish has definitely come true, after he was unable to get hold of a Märklin model several times and was also able to experience some tinkering joy in his search for a suitable replacement.

Alle Fotos: Michael Pleiner

Vertrieb des Basisfahrwerks:  
<https://www.rokuhan.de>



Only 3.0 instead of 6.5 mm track gauge (part 1)

## May it be a little less?

*For our author Reinder Rutgers, Z gauge still doesn't seem small enough. Therefore, today he describes his experiences with building rolling stock in gauge Ze. These narrow-gauge models also follow the scale of 1:220, but have only 3 mm gauge. In this tiny scale, the challenges are even more tricky.*

By Reinder Rutgers. Narrow-gauge railways always found my special interest in prototype and model. From the eighties onwards, I was occupied with the gauge H0e, later also with the scale 2m. Many real prototypes were visited and many books were read.

When, about ten years ago, a Japanese model railway on a scale of 1:450, with a gauge of 3 mm, was brought onto the market under the name of T gauge (for English "three"), a thought immediately occurred to me: Now it should theoretically be possible to realise a more extensive narrow-gauge railway in Z scale.



The comparison of the gauge Ze diesel locomotive with a V 100<sup>20</sup> from Märklin illustrates the extremes that were successfully exhausted here by our reader.

In 2012, in order to investigate whether a model railway can be seriously operated on 3 mm tracks, I built a small layout in their nominal scale (see photo on page 11). It turned out that T-gauge trains run surprisingly well, albeit with some conditions.

Pulse width modulation is a must. The system's own controller works well in this respect. Secondly, the driving characteristics are better when at least two coupled drive units are combined, which pays off especially on turnouts with an insulated frog.

Thirdly, the tractive power of the drive units is limited, although magnetic adhesion should make it possible to cope with larger gradients. In my experience, there must be at least one motorised unit per three to four non-driven wagons.

Fourthly, only relatively long four-axle motorised wagons are available from the manufacturer, and in addition, two-axle wagons are completely absent in gauge T, as they are, on the other hand, typical for a narrow-gauge railway. As a consequence of these operating experiences, I have built nine ghost cars in addition to the locomotive and traction unit models.



These nine ghost cars have gauge-T drive units and operate with an operating voltage of 4.5 volts.

## Special challenges

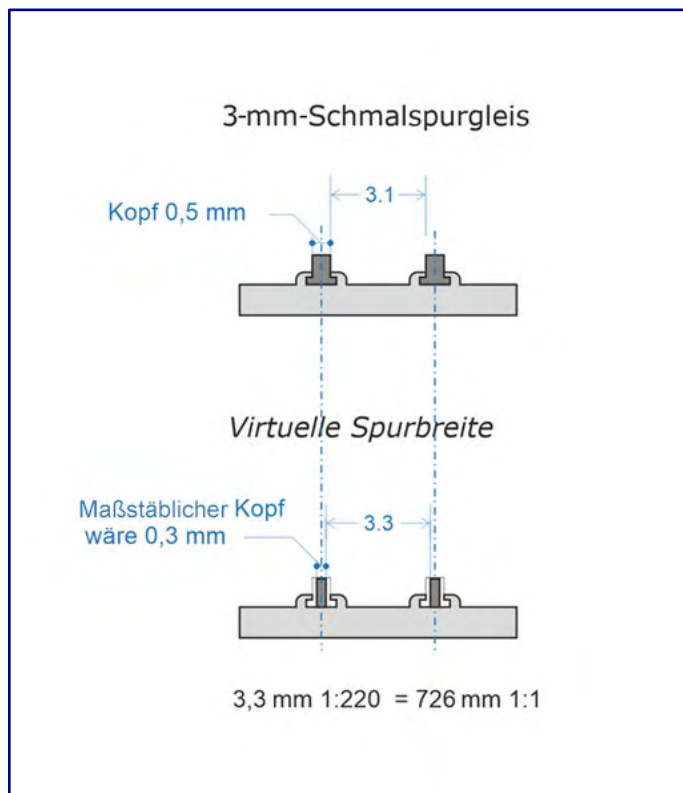
As far as driving my own narrow-gauge trains is concerned: narrow gauge railcar models run as good as their 1:450 scale counterparts, but hauled trains are more problematic. Even setting up and coupling the wagons is considerably trickier than with standard gauge wagons in 1:220 scale.

Sometimes this is, to be honest, a bit frustrating. The available rerailling aid only works to a limited extent for my narrow-gauge solution. Maybe I will develop something of my own. But this is a project for the future.

Many T-gauge parts are available individually, but there are only a few sources of supply. In Germany there is Lemiso; in the UK the site T-Gauge.com. The last one offered a bit more parts so far, such as long couplers. This supplier also makes its own products, including a narrow-gauge track ("Wide Sleeper Track") suitable for nominal size Ze.



With this test layout on a scale of 1:450, the necessary operating experience was achieved in a typical Dutch landscape.



The rails for T gauge have a pivot spacing of 3.6 mm. A to-scale narrow gauge rail head would be about 0.3 mm wide. This means that the virtual rail spacing would be 3.3 mm (see lower part of the graphic).

This equals about 720 mm in reality. Many German narrow-gauge railways had 750 mm gauge. On a layout, the small difference will not be noticeable.

However, a narrow-gauge turnout does not exist yet, only a strange turnout with a fixed counter curve, so a lot of own initiative is needed here. Building your own track and points is already difficult as the trains have to run on steel track because they have magnetic wheels.

But steel rails are hard to come by and steel cannot be soldered at low temperatures. This brings special challenges to the user that we are not familiar with from standard Z gauge.

After the trial phase, the parameters were clear. The next question was whether I could produce my own narrow-gauge models for the Ze size on the basis of the T gauge material.

My first idea beforehand was to convert Märklin rolling stock. I converted several models accordingly, but had no success with locomotives. Their metal chassis proved to be unsuitable. Only a sliding bus could be adapted to the (converted) 750 mm gauge conditions in a more or less acceptable way.

Next, I built four-axle narrow-gauge examples from Märklin freight wagons. To do this, I made the car bodies considerably narrower and mounted new, lower underframes. The use of optically similar or identical side walls of standard gauge wagons is realistic.



Even in the prototype, they were often no smaller than those of their standard-gauge siblings from the period that we are recreating with Era I. With the exception of a few very small open wagons, however, only four-axle wagons can be produced in this way, because almost all standard-gauge wagon bodies are too long.



In this, three-part gauge Ze diesel railcar train, all three vehicles are powered to achieve good and safe driving characteristics.

Further attempts to convert Märklin material into passenger coaches and two-axle coaches by shortening the car bodies did not satisfy me. In the end, I decided to build the whole thing myself in order to achieve convincing results.

## From plan to model

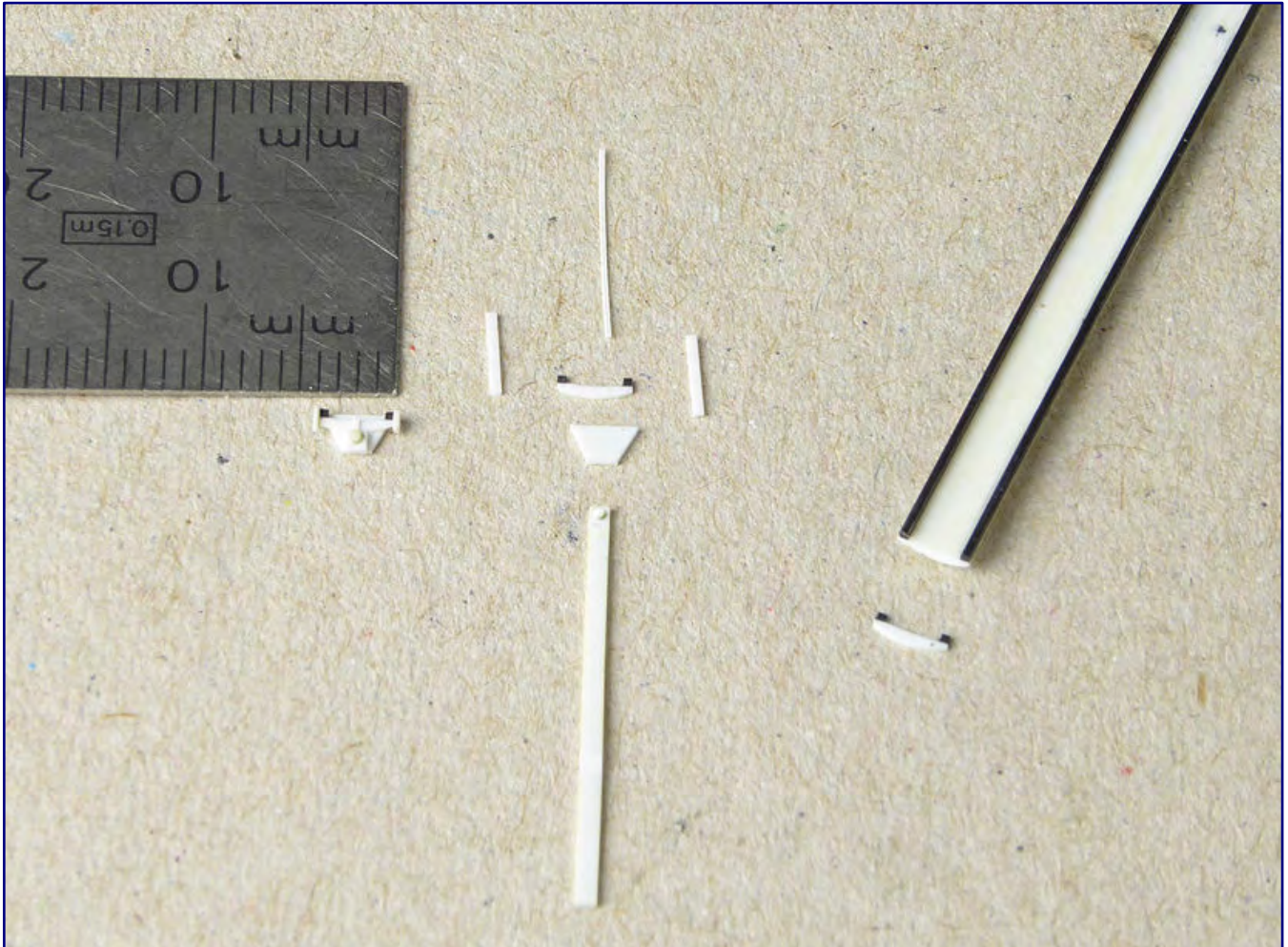
In nominal size H0e I have always reproduced real prototypes precisely and in detail. For my new passion in gauge Ze, however, I have decided for various reasons to design my models solely in my own way.

This does not mean that I do something arbitrary – on the contrary. For me it is very important that the models give their viewer a correct impression, a kind of summary of typical features from several prototypes.

That means studies, a lot of thinking and also some experiments. But it is also a lot of fun, because I feel like the chief engineer of my own railway company.

An important prerequisite for self-building is a good, scaled drawing. I usually use a scale of 2:1 or 4:1, i.e., two or four times larger than the later model. It is important to consider the available or yet to be developed technical means, skills and materials already when drawing. For example, if I use 0.3 mm thin wire for handles, then I also draw with 0.3 mm.

I would like to give the readers a suitable example for looking at limits and what is possible: In the case of the axle brackets of the two-axle wagons, the technical framework must be examined and used sensibly. In the spring set, for example, no individual leaf springs are indicated here, and the hooks are reproduced vertically instead of obliquely for manufacturing reasons. If we look at the finished models, the axle holders nevertheless look good overall, and are above all coherent.



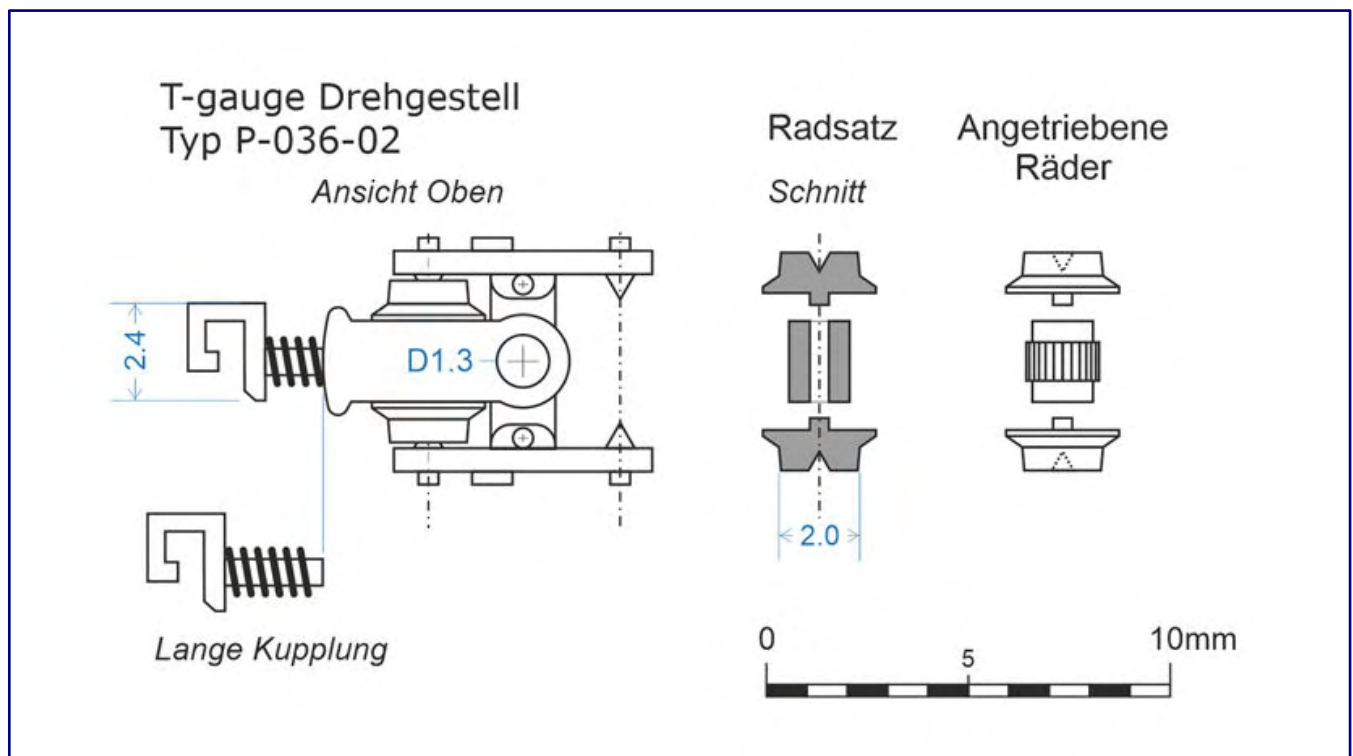
The axle beam replicas consist of nine individual parts each (above the centre). For the spring assembly, a bar profile with the desired crescent-shaped cross-section is first created. Then 0.3 mm narrow discs are cut off (top right). This method ensures that all springs are exactly the same.

The axle bearing is made from a round profile piece that is embedded in a polystyrene strip (centre). The surrounding plates are made with the help of a fine-cutting device. Some strips are only shortened to their final dimensions after assembly (top left). A total of four gauges are used for accurate reproduction.

By the way, I should mention that I had first tried to modify the axle holders of Märklin wagons. But these were too big and thick for me. If that doesn't bother you, you could of course go that way, but for me the proportions are more important than the detailing.

Last general consideration I have to make here: I make my models from polystyrene and self-adhesive film. Primarily I choose this approach because I have a lot of experience with it, but also because of the possibilities of both materials.

The results are quite close to those of the etching technique. A CNC foil cutter is used to cut the foil. When I started my tiny adventure, 3D printing was not yet an option!



This drawing explains the special features of axles and tip bearings described in the next section, as well as the types of couplings used.

But even today, foil on transparent material is still an excellent technique for creating glazed walls with little relief. The advantage of handicraft remains that I don't have to sit for hours behind a computer screen, because nowadays work already forces us to do that to a considerable extent. All too often, idiosyncratic computers tend to cause stress, whereas real handicraft can have a therapeutic effect.

## In a new world

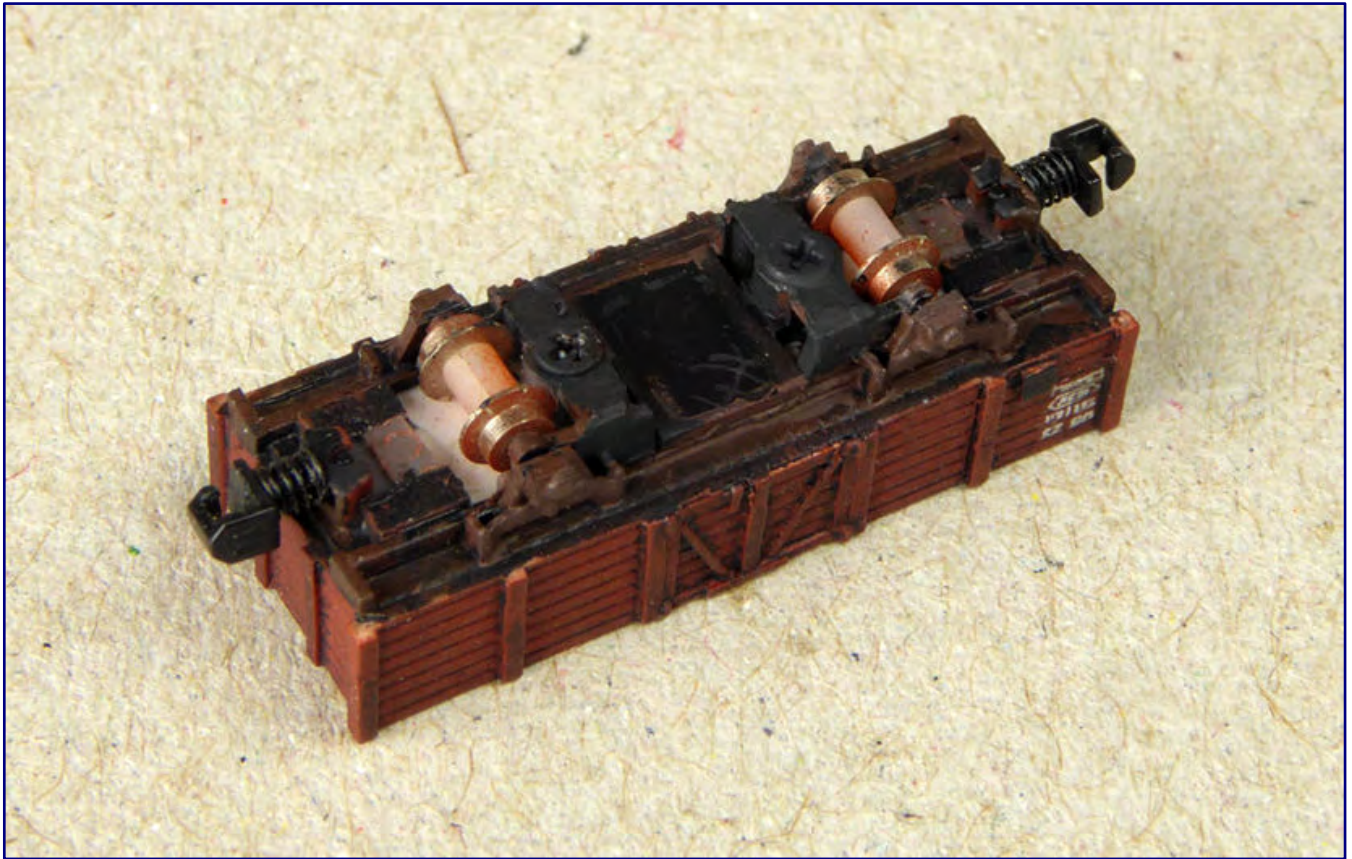
I would like to share some knowledge about T-gauge parts with the readers of this report. The couplings are very small claw couplings. Unfortunately, only those couplings that come with finished models work right away. Separately supplied couplings, on the other hand, jam and require reworking.

Since they are made of soft plastic, probably POM (Polyoxymethylene, a kind of thermoplastic), this is only possible by cutting away material, which is a truly tricky job. There are, however, eye couplings that safely and easily connect two carriages.

The only problem is that these are not offered in a long version and it is not possible to shunt with them. However, shunting is an illusion in this extreme scale anyway. This is also the reason why I have built stand-alone models for side tracks in addition to the rolling material. These have prototypical, but non-functional central buffers.

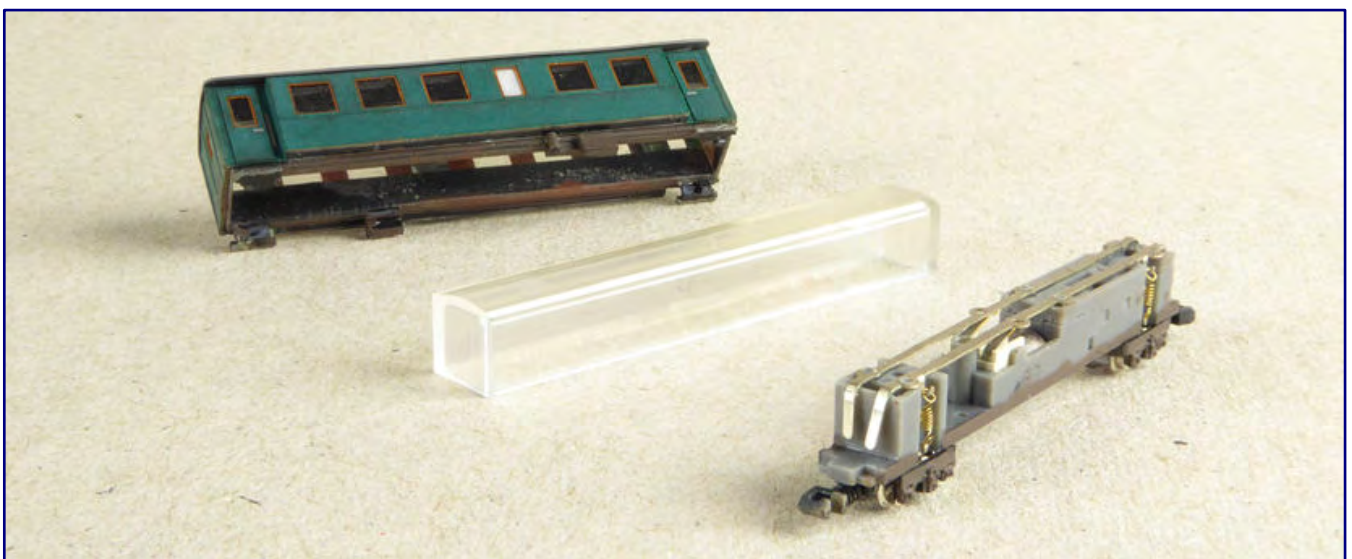
The wheelsets are also mounted differently in T gauge. Normally, in model making, a point bearing by means of pointed axle ends in tapered bushings is common. Here, it is exactly the other way round: The points sit on the frame and the bushings form the outer sides of the wheels. Two wheels are connected by a plastic sleeve as axle. This is a very clever design, especially in view of the tiny size, because it makes insulated metal wheels possible in the first place.





The underside of a two-axle freight wagon suitable for rolling illustrates the modifications made to obtain necessary parts from the bogies and to install the matching coupling.

The disadvantage of this design is that it is difficult to construct a well-functioning and smooth-running wheel bearing. Although T-Gauge.com also offers wheel sets with conventional tips, these have wheels made of plastic. But that does not seem to me to be recommendable in this scale.



Here you can see the drive unit of a ghost car. The corresponding transparent cap was integrated into the car body.

After a few attempts, I then decided not to use them. Instead, half of gauge T-bogies were used for two-axle wagons that had to be drivable. I covered them with my own axle holder imitations.

For the scale 1:450 there are even different bogies available. I like the models P-004 without magnetic wheels and P-005 with magnetic wheels best. How many magnetic wheels are needed for non-powered models is not a settled matter for me.

Although these wheels “stick” well to the track, they also cause jerky driving. If a wagon is big enough, adding a weight is a real alternative that promises more operating realism.



**These three different types of open wagons were designed for sidings as floor models and patinated.**

By the way, the tiny wheels have a diameter of 2 mm. This corresponds to 440 mm for our scale and is therefore actually a little too small, because most wheels on the 750 mm narrow gauge have a dimension of about 500 to 600 mm. However, it is not noticeable when looking at it, because the wheel flanges are too large.

Four-axle drive units are available in five different lengths. Recently, T-Gauge.com has also introduced a six-axle drive unit and a three-axle locomotive. Both are extremely interesting as a basis for narrow gauge models. Unfortunately, they were not yet available when I built the models I present here.

The drive units are only held in the supplied housing by clamping force. As it is difficult to achieve the precision required for this when building your own, it is advisable to use a series housing as a basis for driven models. Moreover, it is transparent, which is of considerable advantage for the construction of railcars and passenger coaches.

continues on page 19



## Supplementary photo series on selected self-built models by Reinder Rutgers (2 pages)



For size comparison, the narrow-gauge steam locomotive number 6 and a Märklin model of the class 01 (behind) were placed together. The proportions are truly impressive. Without this opportunity for comparison, the tiny locomotive would appear larger, especially since it has also been perfectly aged based on prototypes.



The snow plough is closely based on a Rügen model, but is sufficient as a standing model on a summer layout. Behind the model are two water wagons, which were also typical for the service fleet of many small railways in the past.





Everyone is probably familiar with freight wagons attached to a rail bus (at the rear). On small railways, however, individually running diesel railcars were used far more frequently as locomotive replacements for goods trains because of the low volume of traffic.



The modern times of narrow-gauge light railways are embodied by these new-built cars in the style of the fifties and sixties.

## Creative diversity of types

It is clear that a large layout requires a lot of wagons, especially if the main operating point of a narrow-gauge railway is to be represented. And this is not only true for the heyday of narrow-gauge railways.

Later, when there was not much going on at such railways, there were often long rows of parked wagons on side tracks. Since I had sometimes seen exactly that in reality, it was my plan from the outset to recreate something like that in the model as well.

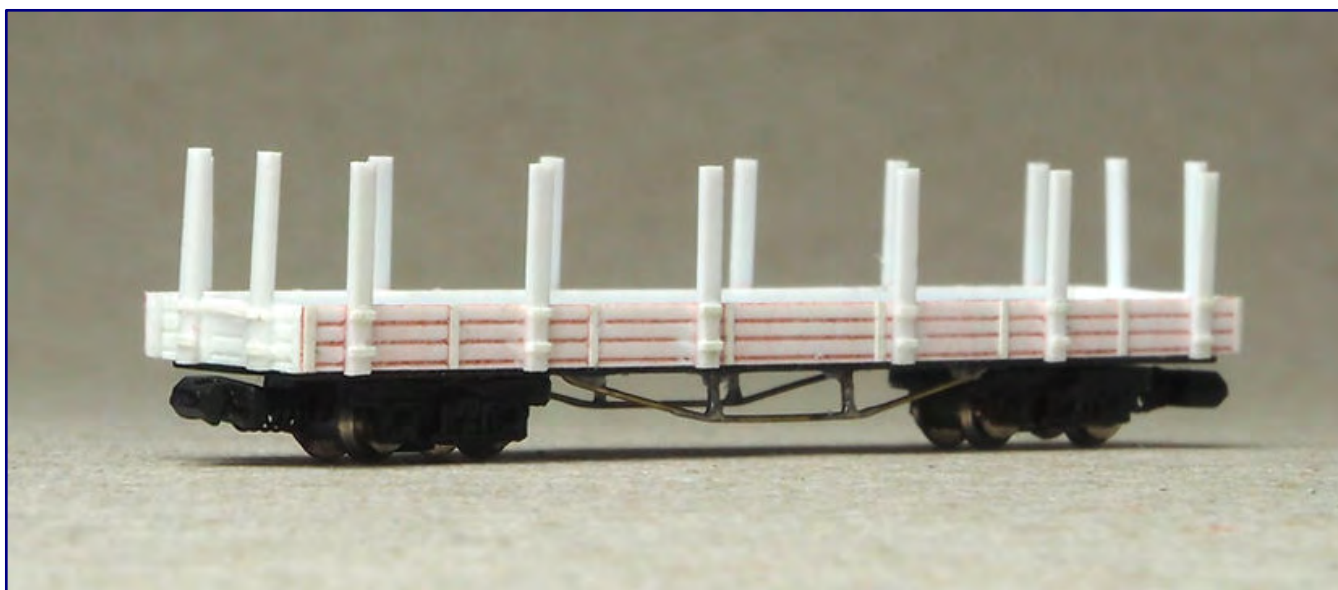
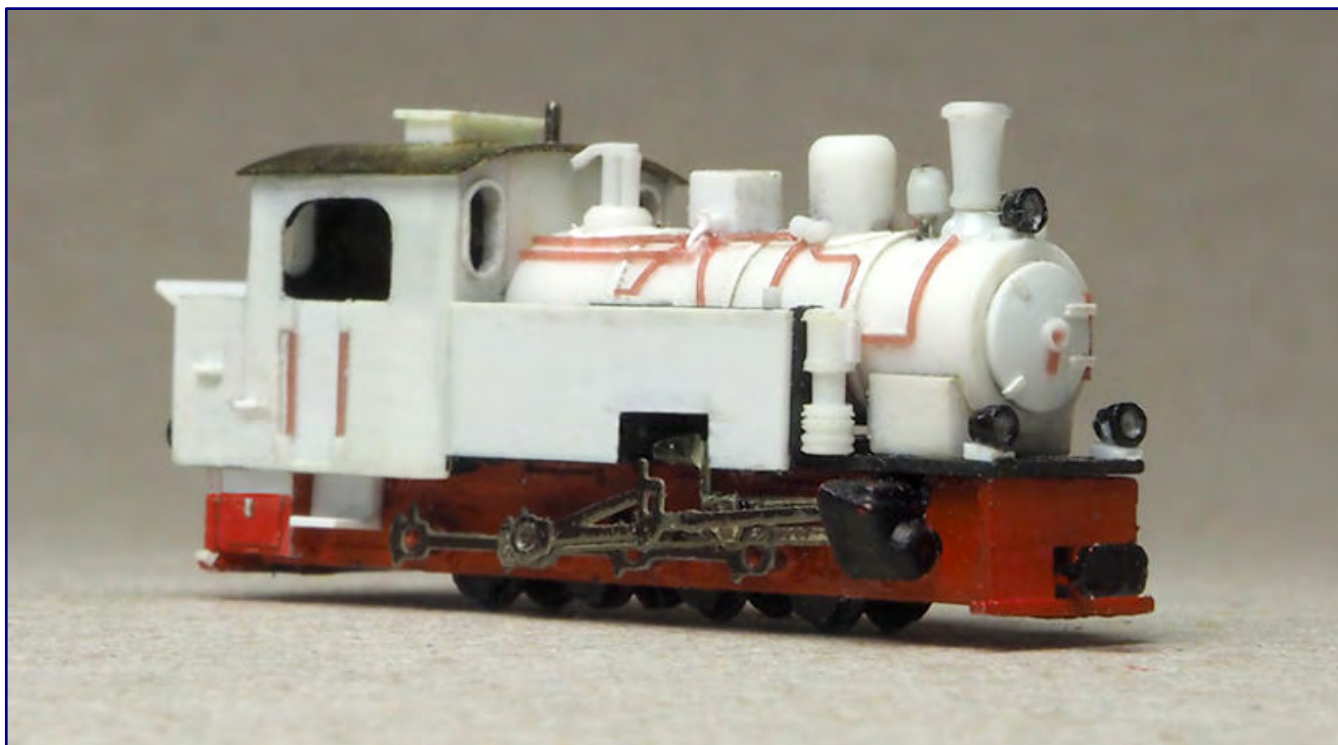
Besides the already mentioned nine ghost wagons, I therefore also produced several types of freight wagons, so that their variety can reproduce the typical photo of a small railway. In addition to older and younger examples of the main types G and O, wagons that would normally be found in a depot also found their way into the model: snow plough, water or equipment wagons. In addition, there were old wagon bodies that are used as accommodation or sheds.



**This photo, which shows all the freight wagons being spray-painted, illustrates the incredible scale of the home-grown fleet – locomotives, railcars and passenger coaches are not shown here!**

My 'modern' railcars are inspired by those of the Kleinbahn Osterode - Kreiensen. An old-style railcar, on the other hand, is based on Prussian models, as is the steam locomotive with the outer frame. After thorough deliberation, I made this one as a stand model.

It was a kind of 'extra': from the outset, the idea was to represent a railway company that switched to railcars in the early sixties. Then I had the idea that it would be nice to put an old, rusty and worn-out steam locomotive on the siding. I used parts of a gauge Nf kit by Roy Bergauer for its steering and frame, the rest is self-made.



The construction photo of the steam locomotive clearly shows the pipes on the boiler (photo above): They are made of 0.25 mm thin polystyrene round rods. Other parts like the chimney, the air pump or the lights were turned. On the four-axle stake wagon (photo below), which was also under construction at the same time, the trussing made of 0.3 mm wire is impressive.

For the driven, four-axle diesel locomotive, however, there is no direct prototype, as the design here was dependent on the conditions of the drive unit. This time I was able to use parts from the tinkering box for the superstructure, and for the rest I had to build my own.

Parts that were needed several times, such as locomotive lanterns, roof ventilators or axle bearing holders, I produced in "mass production". The reason for this is not primarily economic or time-saving. Rather, it often requires certain skills, sensitivity, tricks or even adjustments. It then takes too much time to get to grips with this again and again if a certain part is missing later. In the worst case, it is no longer there as a pattern or master model.



All models were aged in colour. I orientated myself on historical photos. Older passenger and goods wagons have a sun-bleached colour, some wagons have repaired areas, and the steam locomotive has been heavily patinated.



The small diesel locomotive with four different carriages (upper photo) has the effect of gently and carefully applied operational dirt. On the old building railcar (lower photo), which has also been aged, the sidecar is also motorised.

In general, however, I have kept the patina discreet, although there were exceptions even with the prototype: For example, the metre gauge material of the Albtalbahn at that time was in an externally poor condition. Nevertheless, most of the more modern locomotives and wagons of the small railways in the sixties looked well maintained.



The stop of the old-building railcar conveys the idyllic impressions we associate with the long-defunct narrow-gauge railways of years gone by. Thanks to Reinder Rutgers, they skilfully find their way into the 1:220 scale.

All in all, building took more time than I thought. I had to try out a lot of new things, painstakingly learn them and sometimes there were setbacks. Nevertheless, these challenges are always important for me in model building. And, this is what has made me a dedicated Zettie in the past years.

All photos and drawings: Reinder Rutgers

Suppliers for the basic models:

<https://www.lemiso.de>

<https://www.tgauge.com>



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Maintenance and care of small railways (part 11)

## Böttcher's Miracle Cleaning Agent

***Sooner or later, every Z scale enthusiast is faced with the task of giving their rolling stock a good cleaning. And the most desired approaches are those that do not require the complete dismantling of the models. After all, reassembling a steam locomotive can be quite a challenge. And that's why we want to see how a relatively new product can help us to keep life as simple as possible.***

The cleaning of wheels and track concerns and interests all model railway enthusiasts, at least judging from the frequent feedback that we have received in response to our series of articles on "Maintenance and Care of Small Railways", which has been running for ten years.

Especially in response to the test report on the wheel cleaning device distributed by Modellbahn-Union (see **Trainini®** 11/2021), we received many enquiries. But we also received questions about stubborn dirt that could not be removed with this tool even with the use of isopropyl alcohol.



After heavy use of the layout and especially at exhibitions, more cleaning is required. Today, we are looking for new ways to get on top of the dirt.

This clearly shows that more "manual work," disassembling of models and other means are required on a case-by-case basis. We already gave some advice on bogie locomotives in episodes 1 (August 2013) and 7 (October 2018), and wagon wheels were the subject of episode 6 in August 2016.

Today we would like to combine a few tricks with a fairly new cleaning agent that impressed us in the previous tests. The product we tested is called "Steam Oil & Cleaning Oil" (art. no. BM 7503) and comes from Böttcher Modellbahntechnik.

We presented it for the first time with the spring new products in **Trainini®** 2/2022. The manufacturer promises that it immediately dissolves dirt, does not damage plastic, and can also be used with rail cleaning wagons.

The professed properties of the product tempted us to plan for a test over several days at the Intermodellbau exhibition in Dortmund against the type of stubborn dirt that accumulates inevitably on locomotive wheels and in their gears during under railway exhibition conditions.

The plan was also to use the agent as a cleaning fluid on Modellbahn Union's wheelset cleaners.

We wanted to find out whether it would be able to gently remove the type of firmly attached dirt, which some of our readers were unable to remove even by using the wheelset cleaner in combination with regular isopropyl alcohol.

Unfortunately, as we also had to report a little later, the exhibition was cancelled. Suitable test models with comparably heavy dirt accumulations have therefore not been available to us until today.

Therefore, our report comes late and is also limited in its informative value at this point. However, we have no doubt about the Böttcher product and its efficacy, because our experiences, which we share here, are quite impressive.

## Health and safety advice

Before we describe the use and the results achieved, however, it is equally important for us to communicate some background on the choice and decision: The terms "oil" or "distillate" in connection with chemicals of this kind do not come about by chance.

These are regularly petroleum products, which are known to have contents that are hazardous to health, including various hydrocarbons. This places special requirements on their safe use and therefore subjects them to the requirements of the Hazardous Substances Regulation as the national implementation of EU Directive 2014/27/EU for occupational safety and also consumer protection.

Special requirements also apply to the packaging of substances and mixtures, many of which are even internationally coordinated and harmonised. Among the important safety features within the EU is, for example, the UFI code ("Unique Formula Identifier"), which must be indelibly affixed to the container label with various transitional periods — the relevant ones here expired on 1 January 2021.

This follows the realisation that in the event of medical emergencies caused by the product, the information urgently required for health care must be obtained quickly. It allows a clear prescription



A year ago, we introduced the quite inexpensive steam and cleaning oil from Böttcher Modellbahntechnik as a new product. Today, it is to prove itself in various applications.





Harmonised law within the European Union stipulates that dangerous substances must be labelled with hazard and safety information as well as other characteristics.

identification, because it leads to the so-called distributor. This non-public information is accessible to the poison control centres.

If this sixteen-digit alphanumeric code is missing, faulty, or even deliberately falsified, it does not take much imagination to imagine the fatal consequences if a hazardous substance enters the human metabolism.

Since hazardous substances are not only found in the workplace, but also in the home, child safety devices on bottle caps are now commonplace, but, unfortunately, not always taken for granted.

The hazard pictograms, signal words and hazard (H) and safety (P) phrases, which were revised in the last amendment, occupy a much larger space than before. They are intended to be clearly recognisable, to create awareness and to help derive the correct handling to avoid accidents and to contain their consequences.

Those who cannot read are warned clearly by the pictorial language, but all others should take the hazard information H304 very seriously. It reads: "May be fatal if swallowed and entering airways." We have not chosen it arbitrarily, because it also belongs to the product we want to use today.



Warning word and hazard statements should be taken seriously in any case and lead one to take appropriate protective measures: We have only one health! Unfortunately, even today some chemicals do not meet the legal labelling requirements.



Perhaps some of our readers now doubt whether they have already seen and read such notices on the liquids they have already held in their hands as part of their model railway hobby. To our dismay, not all products that pose a health risk and are freely available on the market comply with the applicable protective regulations, and, unfortunately, this also includes very popular products.

This is the reason why we keep our hands off some chemicals and do not present them in this magazine. Stefan Böttcher has met our expectations and clearly labelled and secured his steam and cleaning oil.

## Our real life experiences

Our central questions for the test were whether and how well the Böttcher distillate was compatible with the wheel set cleaner from Modellbahn-Union, and what cleaning performance was achieved when combining both products.

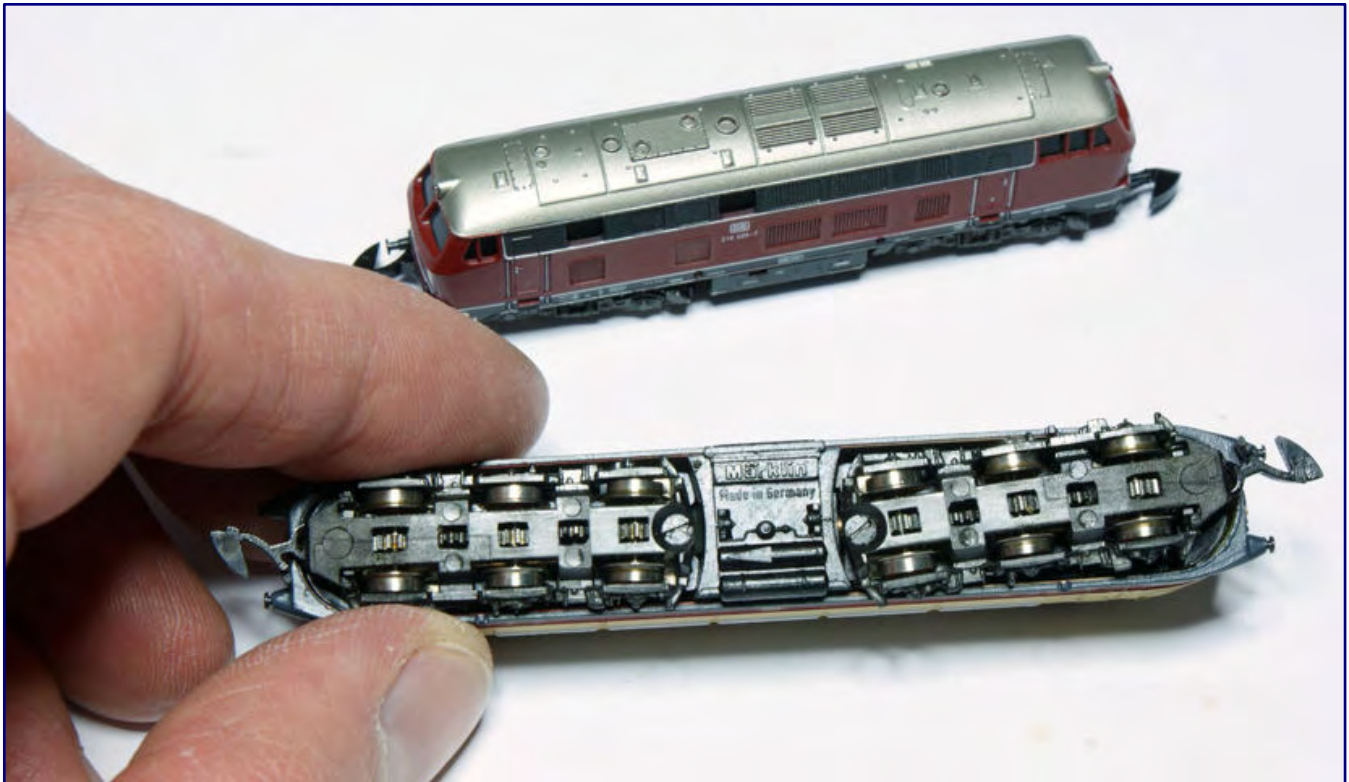
A four-day test at the Intermodellbau was supposed to provide conclusive results, but this did not materialise. So, all that was left as a substitute were runs on our home layout. The two locomotive models in the inventory with the most dirt on the wheel surfaces were selected, but they were not able to accumulate the amount of dirt that occurs during a whole day's use at the trade fair.

Nevertheless, this test provided usable information: we did not find any damage to the device, which confirms the plastic compatibility promised by the manufacturer. However, we have to put a caveat for surfaces that are characterised by varnish and printing, i.e., almost all surfaces of vehicle models.



On Modellbahn-Union's locomotive wheel cleaner, the Böttcher distillate noticeably increased the cleaning effect; it did not damage plastics either on the device or on models. But here, too, there are limits to the cleaning power, which make it necessary to combine it with other mechanisms.

It is not possible to make a general statement here, because we have certainly found colours that are dissolved and smudged by the cleaning oil. It is therefore advisable for model railway enthusiasts to be careful here, to carry out their own tests in inconspicuous places beforehand, or, in the best case, to remove the shell before use, in order to avoid contact with the cleaning agent.



The wheels of our class 103 (Märklin 8854) have become respectable clean again. But caution is advised with regard to the tampon printing, especially of older models, such as here the class 216 (Märklin 8875) produced from 1972 onwards, which at that time did not yet receive a protective coating.

This behaviour towards paints does not come as a surprise to us, as it was once also observed with various steam oils that we tested for the Eisenbahn-Journal about 15 years ago and which represent a thoroughly expected product characteristic.

But what about the cleaning performance? At the time, we were largely enthusiastic about the wheelset cleaner, but it also had its limits when it came to dirt accumulations which go beyond normally observed levels.

Together with the Böttcher cleaning oil, the effect could be increased and should thus solve the basic problem of many model railroaders. But even this time there are limits, as dirt that has caked on for years resists even the gentlest attempts to remove it.

Before reaching for the brass cleaning brush, however, alternatives may be examined. After all, the supplier describes the 1-litre bottle as suitable for ultrasonic baths.

When asked, he explained that he had not thought of adding it to the water, but that the liquid was recommended in concentrated form for ultrasonic cleaning. We then complied with this advice.



Undiluted, the Böttcher product is used in an ultrasonic cleaner and achieves best results against grease, oil and dirt. Sensitive parts such as painted shells and motors are not ultrasonically cleaned! Ideally, dismantled parts are cleaned individually in this way; here resin-covered deep groove ball bearings.

In view of our initial experience, we have of course removed the locomotive shell, and we have also removed the motor, as its windings can be damaged by the strong vibrations and cause a short circuit in the copper coils, even without additives.

### The little miracle cure against dirt

After the ultrasonic bath, we were pleased to find that the chassis was remarkably clean, but the plastic injected into the wheel surfaces showed no signs of damage. The black discoloration of the previously slightly yellowish, but completely transparent distillate, is very telling.

This certainly speaks volumes, and the accompanying photos impressively document what has been written here. It is now clear how much dirt was stuck in the gears and on the wheel treads: old lubricants, dust and particles, which are only partly noticeable during visual inspection.

#### **Always keep an eye on your health**

When using cleaning oil in baths or on a wheelset cleaner, it should be noted that this agent is much less volatile than alcohols. In comparison, this means significantly longer drying times for the cleaning equipment, and also a much larger time window in which the evaporating components are released into the room air.

This should always be taken into account with regard to health hazards when inhaling! Wherever possible, work should be done outdoors and the equipment should be left there to dry (protected from children and animals). The minimum requirement for occupational safety at home should be good and constant ventilation of the room.

We take this as evidence of the good cleaning performance of the agent, but the decisive factor is probably also the exposure time to dirt and base surface. The cleaning oil needs a contact surface and must be able to penetrate the dirt.

Apparently, the short contact time on the wheelset cleaner's stand is simply not sufficient for such "difficult cases". The permanently wet surface in the ultrasonic bath is larger, the vibrations generated by the unit will be the decisive factor.



What comprehensive basic cleaning solution can we then offer to those readers who do not have access to an ultrasonic device? Here we are thinking of a bath in cleaning oil that takes more than just a few minutes. Ideally, the receptacle should have transparent walls all around.

Periodic visual inspections are advisable, because we cannot make generally valid and binding statements about material compatibility with all conceivable materials of rolling stock. In principle, the procedure described here is also suitable for the old three- and five-pole motors.

According to our experience, the Böttcher product does not cause any damage to their parts, as the vibrations of the ultrasound do not damage them. What remains is a cleaned, but also completely degreased motor. In the further course, they have to be lubricated again with a drop of lubricating oil, otherwise the drive will screech later on.

Back to the cleaning process: After sufficient exposure time, a lot of dirt is liquefied and rinsed out, but we also find components that require mechanical reworking.



**You don't necessarily need an ultrasonic device for perfect cleaning results. In the test, placing the parts to be cleaned in an old wurst or jam jar and brushing off any remaining dirt with an old toothbrush proved to be just as effective.**

This is done with an old, but not frayed, toothbrush. The parts to be cleaned are held in one hand, please use protective gloves, and carefully brushed with the soaked toothbrush. The bristles brush off dried, adhering lubricant residues from the spaces between gears and worm gears very well.

Once the cleaning is complete, the processed parts are allowed to dry in the ambient air. If they can tolerate a water bath, they can also be rinsed clear. After complete and thorough drying, it is time to reassemble components such as undercarriages and gearboxes.

All parts are given a new and careful coat of suitable lubricants (greases or oils depending on the component). Less is more should be the way to go here.



Old versus new: The difference between the used and dirty liquid and the crystal-clear liquid from the product bottle proves what a miracle the cleaning oil performed in our test. Filtered out, by the way, it can be used several more times.

As an important final sentence, we explicitly state at this point that Böttcher cleaning oil is not a lubricating oil! This is also explicitly written on the bottle label and should be taken into account.

If parts are mounted and put back into operation without proper lubrication, they will jam or run dry and with significantly increased wear. Also major gearbox damage can then be expected.

**Manufacturer and supplier:**  
<https://www.boettcher-modellbahntechnik.de>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

## Rückblick auf Dampfexoten **Was wäre, wenn...?**

***Auf unseren literarischen Ausflügen begegnete uns ein längst vergessenes, aber unverändert lesenswertes Buch. Wer es antiquarisch findet und erwerben kann, darf sich glücklich schätzen. Es behandelt die beiden Franco-Crosti-Baureihen der Bundesbahn, die als regelrechte Exoten in ihrem Bestand zu finden waren. Bekannt sind sie Modellbahnern wegen ihres ungewöhnlichen Erscheinungsbilds durchaus – das Wissen um ihre Technik und Bewährung birgt Lücken.***

Jürgen-Ulrich Ebel / Rüdiger Gänsfuß  
Franco-Crosti – Die letzte Chance der Dampflok  
Technik und Geschichte der Baureihen 42.90 und 50.40 der DB

Lok Report e.V.  
Erlangen 1980

Gebundenes Buch  
Format 21,0 x 30,0 cm  
142 Seiten mit 136 S/W-Abbildungen und 52 Zeichnungen / Tabellen

ISBN 978-3-921980-03-3  
Preis bei Erscheinen 33,50 DM (17,13 EUR)

Nur antiquarisch erhältlich

Am Anfang war es nur eine Zufallsbegegnung: Da strahlt den Rezensenten aus dem Regal ein auffällig orangefarbenes Buch an. So fällt der Blick ungewollt auf den Titel. Er verheißt: „Die letzte Chance der Dampflok“. Was also ist der Grund, dass diese letzte Chance doch noch vertan wurde?

Und warum sollte ausgerechnet die Franco-Crosti-Technik diese letzte Chance sein? Unweigerlich ziehen sich die Gedanken in Richtung Gegenwart und drängen ins Bewusstsein. In der Phantasie erscheint das moderne Leben, begleitet von Dampflokomotiven. Das wirkt wie Science-Fiction – nur halt mit Technik von vorgestern.

Nein, die Dampflok hatte eine solche Chance nicht, sich über die letzten fünfzig Jahre retten zu können und schon gar keine auf Weiterentwicklung. Zu groß waren die Mengen an Energie, die ungenutzt durch ihren Schlot auf Nimmerwiedersehen verschwanden.

Wer unseren Zeilen bis hierher gefolgt ist, den sollte es nun gepackt haben. Schuld daran ist eben der beinahe provokant wirkende Titel mit dem Foto einer so ungewöhnlich wirkenden und doch faszinierenden Dampflok darauf. Der grelle Einband erlaubt es nicht, dieses Buch zu übersehen.

2002 fand es eine weitere Auflage im EK-Verlag, in seiner Seitenanzahl kräftig gewachsen. Das dürfte neben neuem Bildmaterial sicher an den dort üblichen Statistikeilen liegen, die viele Käufer schätzen, aber nicht alle Interessenten brauchen. Wer zur zweiten Gruppe gehört, der ist mit diesem, antiquarisch übrigens auch günstiger zu beziehenden, Buch gut beraten.





Gelungen konzentrieren sich die beiden durch ihre vielen Publikationen bestens bekannten Autoren auf die wichtigen Inhalte und präsentieren sie dem Leser in leicht verständlicher und gut strukturierter Form. Sie stellen voran, welche Faktoren über den Erfolg einer Maschine bestimmen und arbeiten den großen Schwachpunkt der Dampflok treffend heraus: den viel zu geringen Wirkungsgrad.

Ein historischer Abriss erläutert die Anstrengungen, diesen über die Speisewasservorwärmung durch Oberflächen- und später Mischvorwärmer zu erhöhen. Ein weiterer Versuch richtete sich auf das Ausnutzen der Rauchgase durch Attilio Franco zur thermischen Verbesserung. Dieser Weg führte zu den Franco-Crosti-Lokomotiven, die vor allem in Italien eine respektable Verbreitung und Einsatzzeit erlebten.

Herausgearbeitet werden dann die Vorbehalte, die im deutschen Dampflokbaubau galten. Auch nach dem Abtritt Wagners und dem Ende seiner Einheitslokomotiven auf technischem Stand von 1925 war der deutschen Entwicklung unter Friedrich Witte ein gewisses Dogma nicht abzusprechen.

Es hieß „neue Baugrundsätze“ und hatte neben konsequentem Anwenden der Schweißtechnik mit der Verbrennungskammer und den Mischvorwärmern zwei unverrückbare Vorgaben. So fehlte anfangs die Bereitschaft, eine konkurrierende Technologie zu akzeptieren und praktisch zu erproben.

Doch die Widerstände konnten geschickt überwunden werden, an zwei nach dem Krieg fertigzustellenden Exemplaren der Baureihe 52 sollte sie getestet werden. Wegen ihres höheren Gewichts wurden diese beiden Lokomotiven schließlich aber als 42 9000 und 42 9001 in den Bestand eingereiht.

Schwerpunkt des Buches ist die technische Beschreibung dieser Exemplare, die mit ihnen gemachten Erfahrungen und (zu erwartenden) Probleme, denn hier wurde ja nicht von Grund auf eine Konstruktion erarbeitet, sondern der Rauchgasvorwärmer an einen vorhandenen, völlig überdimensionierten Kessel gesetzt.

Immerhin konnten die beiden Versuchsträger dahingehend überzeugen, was in der FC-Lok stecken würde, wenn alle Komponenten aufeinander abgestimmt sein würden und sich an der geforderten Leistungsfähigkeit der Lok und ihrem Einsatzprofil ausrichten würden.

Die Antwort darauf war die Baureihe 50<sup>40</sup>, die die DB in 18 Exemplaren umbauen ließ. Mit der ölgefeuerten 50 4011 fand diese Baureihe ihre Königin, die den meisten DB-Dampflokomotiven überlegen war.

Aber das Buch verheimlicht auch nicht die anhaltenden Probleme mit den FC-Maschinen, geht auch auf die „50.40-Krise“ ein und schaut in die einzelnen Betriebswerke und den von ihnen zugeordneten Aufgaben. Deutlich wird, dass die DB viel erreicht hat und eklatante Kinderkrankheiten abstellen konnte, aber ein Grundproblem dennoch nicht in den Griff bekam.

Es war die Achillesferse der Baureihe 50<sup>40</sup>, die ihre Wirtschaftlichkeit nach anfänglich großen Erfolgen zusehends in Frage stellen sollte. Die Autoren wagen auch einen Ausblick und ziehen ein Resümee, wie diese Technik doch noch zu großen Dampflok-Erfolg der Bundesbahn hätte werden sollen und können.

Spannend geschrieben sowie ansprechend mit gut reproduzierten Bildern und aufschlussreichen Zeichnungen angereichert, macht dieses Werk aus jedem einen FC-Liebhaber. Das Buch begleitet nicht nur Märklin-Freunde mit ihren Modellen, es macht einfach Lust auf mehr und weitere Miniaturen.

Antiquarian reference (suggestions):  
<https://www.booklooker.de>  
<https://www.zvab.com>

## Dreibändiges Wagen-Archiv Reisezugwagen bis 1993

*In den Reihen „Deutsches Lok-Archiv“ und „Deutsches Wagen-Archiv“ erschienen vor Jahrzehnten wichtige Nachschlagewerke für beinahe alle Schienenfahrzeugkategorien, die in Deutschland relevant waren und sind. Nach den Standardwerken für Dampflokomotiven legt Transpress nach 30 Jahren nun auch die Reisezugwagen-Trilogie neu auf. Wir haben einen Blick in diese genommen und fassen unsere Eindrücke zusammen.*



Peter Wagner / Sigrid Wagner / Joachim Deppmeyer  
Reisezugwagen 1 - Sitz- und Gepäckwagen - Privat- und Länderbahnen -  
Private Reisezugwageneinsteller - Deutsche Reichsbahn-Gesellschaft  
aus der Reihe „Deutsches Wagen-Archiv“

Transpress Verlag  
Stuttgart 2023

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Peter Wagner / Sigrid Wagner / Joachim Deppmeyer  
Reisezugwagen 2 - Sitz- und Gepäckwagen -  
Deutsche Bundesbahn - Deutsche Reichsbahn  
aus der Reihe „Deutsches Wagen-Archiv“

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Wolfgang Theurich / Joachim Deppmeyer  
Reisezugwagen 3 - Speise-, Schlaf- und Salonwagen -  
Deutsche Bundesbahn - Deutsche Reichsbahn  
aus der Reihe „Deutsches Wagen-Archiv“

Transpress Verlag  
Stuttgart 2023

Taschenbuch mit Klebebindung  
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Titel-Nr. 71684  
Preis 49,90 EUR (Deutschland)

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Im vorliegenden Fall eines zeitgleich in drei Bänden (wieder)erscheinenden Werks macht es keinen Sinn, jedes Buch einzeln vorzustellen: Sie bilden ein zusammenhängendes Verzeichnis und dürften daher regelmäßig auch als Trio den Weg in heimische Bücherregale finden. Nur wenige Käufer dürften ihr Interesse derart eingeschränkt haben, dass sie mit einem Einzelband bereits vollends bedient wären.

Zu den Autoren Peter Wagner und Wolfgang Theurich werden wir keine großen Worte verlieren müssen. Ihre Namen sind durch einschlägige Veröffentlichungen seit vielen Jahren bekannt. Gleiches gilt für Joachim Deppmeyer, der sich bereits vor mehr als vierzig Jahren an das Katalogisieren von Reisezugwagen in Buchform machte und dies bis heute tut. „Deppmeyer“ wurde in dieser Zeit durchaus zum Synonym für ein Standardwerk.

Transpress hat dies zum Anlass genommen, die dreibändige Personenwagen-Reihe des „Deutschen Wagen-Archiv“ in ihrem Klassiker-Sortiment wiederaufzulegen. Da die Bücher lange Zeit vergriffen waren und auch antiquarisch gute Preise erzielen, ist von einer guten Nachfrage auszugehen und die Entscheidung als richtig zu werten.

Hingewiesen sei an dieser Stelle aber auf einen Punkt, den auch der Verlag selbst hervorhebt: Es handelt sich nicht um eine überarbeitete und erweiterte Auflage, sondern einen unveränderten Nachdruck der ersten Auflage von 1993!

In den Bänden 2 und 3 nicht enthalten sind folglich Um- und Neubauten der Deutschen Bahn AG (ab 1994). Diese Lücke bleibt überschaubar und ist auch zu verschmerzen, denn die moderne Bahn setzt ja für die Zukunft bekanntlich fast ausschließlich auf Triebzüge. Auf der anderen Seite ist der Nutzen damit natürlich auf diejenigen Leser beschränkt, die sich für (weitgehend) historische Vorbilder interessieren oder Unterlagen für Eigenbauten brauchen, die vorrangig die Epochen I bis IV bedienen.

Die Inhalte der einzelnen Bände fassen wir wie folgt zusammen: Band 1 thematisiert die Entwicklung der normalspurigen Sitz- und Gepäckwagen bei Privat- und Länderbahnen inklusive privater Einsteller sowie der Deutschen Reichsbahn(-Gesellschaft) bis 1945. Band 2 setzt die Historie für diesen Wagenbereich bei Bundes- und Reichsbahn nach 1945 fort. Sie bilden folglich für die Sitz- und Gepäckwagen eine in sich abgeschlossene Einheit.

Der dritte Band ist den normalspurigen Speise-, Schlaf und Salonwagen gewidmet und verzichtet auf eine zeitliche Einteilung. Er bildet zusammen mit Band 2 ein in sich geschlossenes Werk für die Wagen der Reichsbahn in der früheren DDR und die der Bundesbahn im Westen. Berücksichtigt werden auch Veränderungen, die sich durch das Zusammenführen in der Deutschen Bahn AG ergaben – aber nur für die zum Übergangzeitpunkt existierenden Wagenbauarten.

Identisch ist jeweils der inhaltliche Umfang: allgemeine Ausführungen zu den einzelnen Wagengattungen, Beschreibungen der Bauarten, Angaben zu Nummern und Gattungsbezeichnungen sowie tabellarische Aufstellungen mit wichtigen Daten. In den Büchern zu finden sind aber bei weitem nicht alle Wagentypen, die bis 1993 auf Deutschlands Schienen unterwegs waren.

Die Lücken sind vermutlich größer als die hier erfassten Bestände, was bei einem Blick in die spezialisierteren Werke anderer Verlage schnell klar wird. Erkennbar ist allerdings, dass hier das oberste Ziel darin bestand, einen Überblick nach Typen und Grundkonstruktionen zu schaffen, ohne einen auf die Mehrzahl der Nutzer ausgerichteten Umfang zu sprengen.

Gerade deshalb halten wir diese drei Bücher für einen guten Einstieg in die Technikgeschichte der Reisezugwagen, der einen ausreichend großen Überblick schafft und mit den zum Inhalt gehörenden Zeichnungen auch wertvolle Hilfen für Bauprojekte liefert. Besonders diejenigen Menschen, die keinen Wert auf tiefgreifende Technikbeschreibungen und Betriebsstatistiken legen, finden hier einen bezahlbaren Zugang zu ihnen hilfreichen Informationen.



Das galt schon, da inhaltlich und auch im Erscheinungsbild unverändert, für die vor 30 Jahren erschienene Auflage. Im direkten Vergleich zu dieser müssen wir aber attestieren, dass die Bildwiedergabe seinerzeit etwas besser war, was an der damaligen Papierqualität gelegen haben könnte.

Alle in den Büchern abgebildeten Fotografien wurden nicht neu reproduziert und ebenso unverändert übernommen. Das schließt damit leider auch Mängel aus zu kontrastarmen oder unterbelichteten Aufnahmen mit ein. Wertvoller für den Modellbahner sind jedoch eh die Zeichnungen, denn Wagennummern und Betriebsanschriften würden sich aus Fotoabbildungen eh nicht entnehmen lassen.

Publishing pages:  
<https://www.motorbuch.de>

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Readers' letters and messages

## Zetties and Trainini in Dialogue

*Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.*

To the end of the gauge Z programme at Panzer-Shop.nl (Trainini® 1/2023):

Thank you for the magazine, which is as always interesting and worth reading. I now have clarity regarding Panzer-Shop.nl, after I recently noticed that the Z range was missing there. I don't want to criticise an entrepreneurial decision.

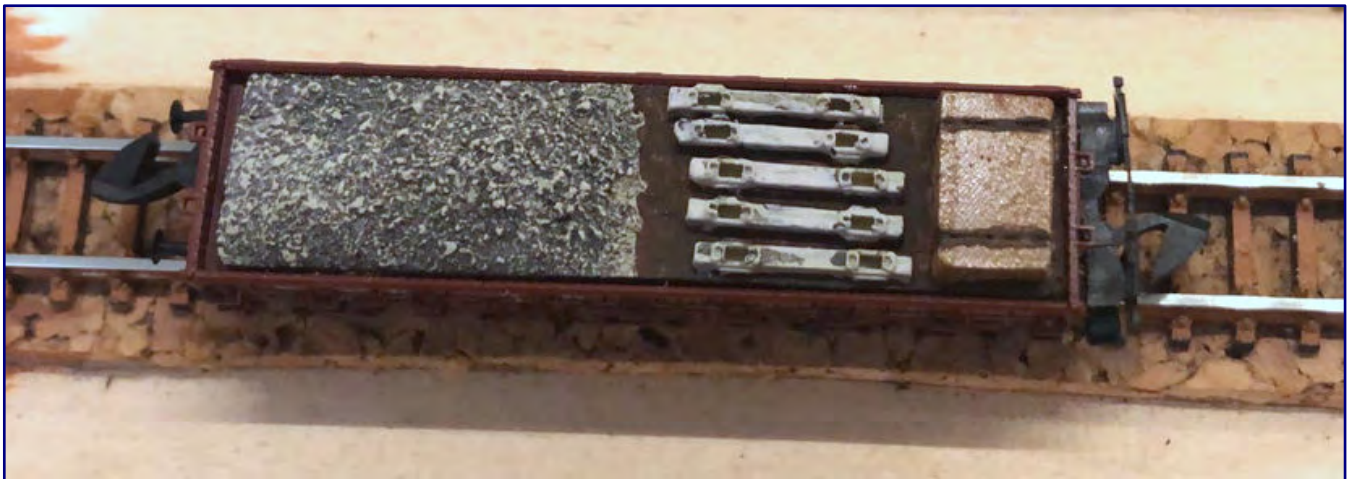
However, I would like to say that the assortment may have been too specialized. For this reason, I wrote (an) e-mail (with concrete product suggestions: editor's note) to the company (in mid-June 2022).

(...) The answer was prompt, short, and dismissive. Perhaps, I will now contact others on this subject.

Jörg Endreß, Bremen

A suggestion from Trainini® 12/2022 has been followed by this reader:

I read with interest the article about the DB's Rmms 33 rail service stake wagon (82133) in the December 2022 issue. I "souped up" the enclosed load insert according to your suggestions. The load, made of resin, was glued into the stake wagon at the end of the treatment with the Noch-Hin-und-weg (removable and reattachable) glue. Now, the whole structure looks much more realistic.



Following our suggestion from Trainini® 12/2022, the colours of this load insert have also been improved and is now more convincing. Photo: Harald Fried

Thank you very much for the optimisation suggestion. On the attached photos you can clearly see the improvements after the optimisation.

Harald Fried, Hermsdorf

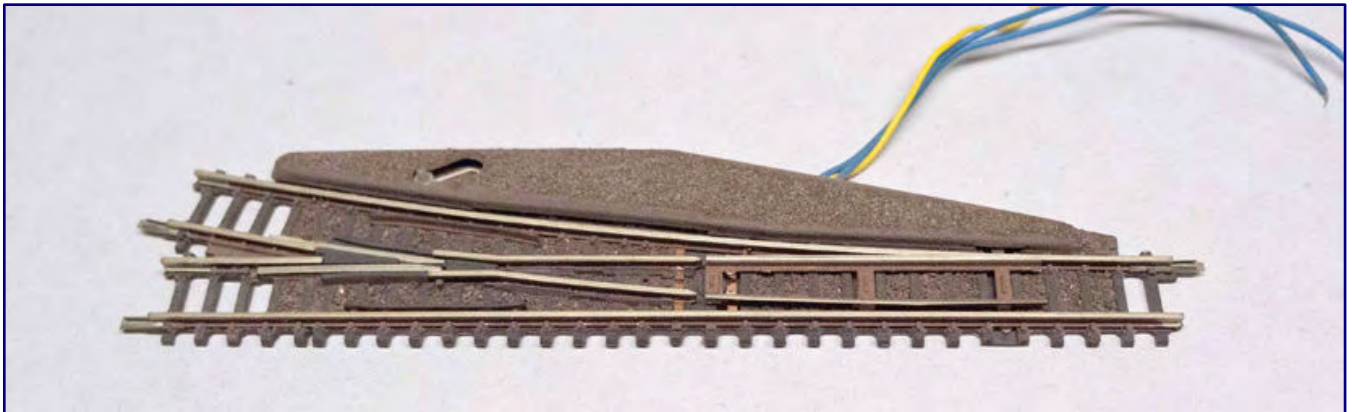
**Question about the signal box construction report from Trainini® 1/2023:**

First of all, a huge thank you for your magazine, sometimes I wish I could have a printed version at home. Yes, I am old fashioned, and I like flipping pages.

But, I am writing to you to understand better how the model maker (Jochen Brüggemann; editors' note) managed to hide so well the turn out motors.

I tried the same thing, but the trains were derailing, the glue build up was too thick. May I ask some advice?

Olivier Bébox, by E-Mail



**For camouflage purposes, the switch machine boxes were also ballasted. This must only be done so thinly and carefully that the clearance profile near the track is not impaired. Photo: Jochen Brüggemann**

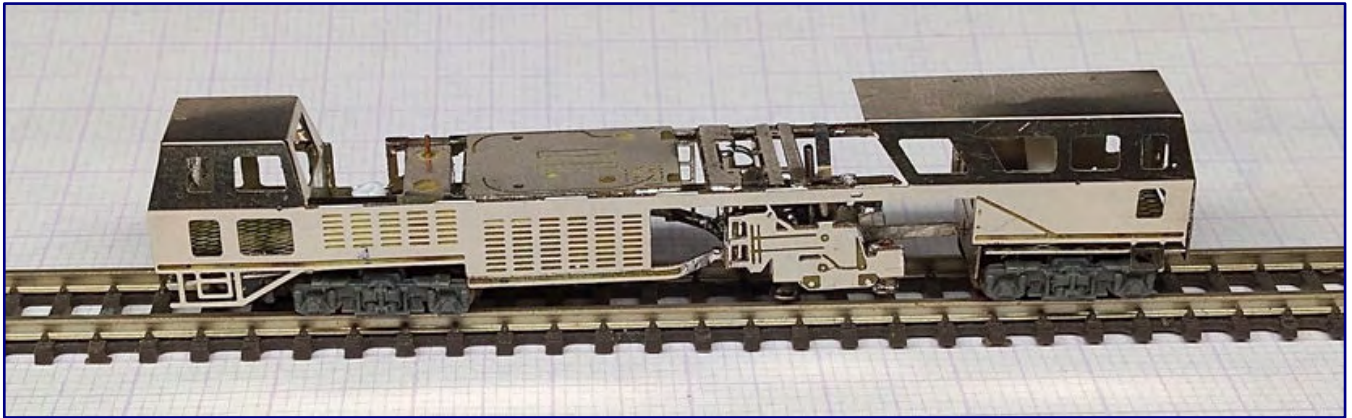
Answer from our author: Especially in Z gauge it is very important to keep the clearance of locomotives (and wagons) free of any obstacles to avoid undesired halting or derailling of trains. This also includes that the hiding of point machine housings may only increase the housing size very slightly. This applies especially to the two lateral edges of these housings. In addition, the turnout drive must not be blocked. (Editor's note: Jochen Brüggemann has offered to prepare a separate article on the subject of "Camouflaging turnout mechanisms".)

**Otti's model kits:**

After the publication of the February issue with the spring new products, Hans-Jörg Ottinger drew our attention to his range of kits, which he sells as a small entrepreneur under his own trade name "Z-Otti" (<https://zotti.lena-johannson.de/shop>).

On his pages we find many parts and kits made of etched metal and printed resin that can enhance many model railway layouts. A large part of them could also be seen on their own modules at the exhibitions in Altenbeken and Bad Schwartau. Among them, for example, was the Duomatic-07 track tamping machine (art. no. 2104-1-GSM).





The track tamping machine Duomatic-07 (item no. 2104-1-GSM; photo above) still gives a view of the kit and its assembly. The Re 4/4 of the BLS (2201-1-Re4/4-set; photo below left) and the Ae 8/8 (2207-Ae8/8-set; photo below right) on the other hand already have been assembled, painted, and lettered. Photos: Z-Otti

Equally remarkable are the Swiss electric locomotive models Re 4/4 of the BLS (2201-1-Re4/4-set) as well as the BLS double locomotive Ae 8/8 (2207-Ae8/8-set), for which a motorised running gear is planned by FR Freudenreich Feinwerktechnik. An assembly can also be ordered from Carlo Mercuri (Italy) or Bahls Modelleisenbahnen.

This does not apply to the latter service provider for the glass train of class 491 (2109-1-ET91), which is also offered there as a separate and technically unrelated model. The 3D-printed resin Rawie buffer stop (23-03-RP) provides variety at the end of the track.



The glass train of the class 491 (2109-1-ET91; photo left) can also be found in the Z-Otti programme. A speciality that is not available from large-scale production is the Rawie buffer stop (23-03-RP; photo on the right) made of resin. Photos: Z-Otti

Those interested in the extensive range of accessories are advised to browse the pages in full for helpful finds.

## Märklin deliveries in March:

Märklin has delivered two new products in the reporting period, neither of which completely thrilled us. At first glance, the Messelok 2022, a traffic-blue diesel locomotive of the 218 series (item no. 88806), which is now in service for the Pressnitzalbahn GmbH (PRESS), looks very good.

The model arrives in a real wooden box and is powered by a bell-shaped armature motor. The light change is warm white-red and the model also scores with enlarged buffer plates. However, what does not match the prototype, provided that today's road number refers to the correct series, are the bogies with coil springs.



Despite correct parts in the mould kit, Märklin also delivered the class 2184 of the Press (item no. 88806) with incorrect bogie plates of an earlier prototype series.

These belong to the class 216 as well as the pre-series and the first two series of the 218. The locomotive 218 448-9 assigned to the sub-series 218<sup>4</sup>, on the other hand, would have to show so-called Meggi springs, and, subsequently, also other sandboxes.

Märklin has both variants in its mould construction kit. The manufacturer has made this mistake several times in the past, but, fortunately, it is avoidable.

Almost at the same time, the three-part goods train car sets for Era IV (82596), which Märklin has published as the MHI series, have arrived. The contents are a double stake wagon Spns 719, a KLV carrying wagon (labelled as type Lgs) with Genstar container and a gas tank wagon "Tycza Gas" without sunshield.

What we disliked about the delivered models was the reduced load safety device compared to the product illustration, which includes all double stanchions in the prototype, but not (any more) in the now delivered wagons.



Likewise, the wagon carrying the load is only a compromise, which, however, could be recognised early on from the product illustration: The running gear is the modified unit running gear from 1972, which was used at that time for such wagon loads and other models.

Since it has now been reused in the same way, the container model is firmly connected to the running gear and is also too short: it is not a 40-foot container, as the description suggests, but a visibly too short one that does not correspond to any standardised model. In the formation of a block train, this is certainly a shortcoming that is perceptible to the observer.

#### **Eurospoor 2023 is cancelled:**

Eurospoor 2023 (<https://eurospoor.nl>) in Utrecht at the Jaarbeurs exhibition centre has been cancelled. This is announced by the organiser on its webpages. The reason given is continuing uncertainties and cost increases, which mean that the risks are too high. A sufficient and stable calculation basis for an event of this size does not seem to exist at the moment.

#### **Polyplate kit delivered by Viessmann:**

Originally announced for autumn 2023, the small workshop (item no. 49590) from the Vollmer range has surprisingly already arrived at the dealers. "Polyplate," a new composite material for us, was used as the material for this kit.



The surfaces of the plaster and doors look a lot better on the original kit than on the product illustration of the small workshop (Vollmer 49590). Photo: Viessmann

It gives a good impression and is strongly reminiscent of familiar hard cardboard kits. We will check what we can expect during assembly in a building project and report back soon. Viessmann also offers its own adhesive in parallel: the container for the Polyplate adhesive (39997), which the manufacturer recommends, contains 80 ml.

#### **Reader's layout in the railway museum:**

Eckard Jehle's mountain facility, which we presented in **Trainini®** 8/2020 under the title "Alpenidyll im Badischen" (Alpine idyll in Baden), has since found a new home. At the time, our reader was looking for someone interested in taking it over.



In the meantime, a future operator has been found in the form of the Black Forest Railway Museum (<https://eisenbahnmuseum-schwarzwald.de>) in Schramberg, Baden. This museum, which houses the world's largest gauge 2 collection, is currently presenting exhibition layouts and models in the special exhibition "Small meets Large".

The models of the large gauge are all handmade and not industrially produced. The special exhibition was initiated on the occasion of the 50th anniversary of Z gauge and will run until 17 March 2024.

The builder reported on the difficult relocation of the layout from the Karlsruhe area to the Black Forest as follows: "Before the transport of the over 3.70 m long and very heavy layout, extensive stabilisation and packaging measures were necessary. The layout survived the transport over 140 km (...) without damage. At the installation site, the layout (...) is fully functional."

If you would like to get a personal impression and see the enormous mountain massifs, including the cable car, please check the opening times and plan a visit to the site as part of a weekend trip or holiday.

#### **AZL deliveries for March:**

This month, the EMD F7s are rolling out as A and B units from D&RGW (Rio Grande). An A-B combination (item no. 63003-1) and a single A unit (63003-2) are offered. The MAXI-IV carrier units are now printed with the new TTX logo. The yellow-painted units (906553-1 to -5) are, as usual, equipped with different container loads for varied trains.



**EMD F7A of D&RGW (item no. 63003-2; photo left) and ALCO PA1 of ATSF as demonstration locomotive "Gold Bonnet" (64425; photo right) are current new products for March. Photos: AZL / Ztrack**

For the sake of completeness, we would like to mention one new product from the beginning of March that was sold out immediately at the factory: ATSF's ALCO PA1 (64425) with the road number 53 was in such high demand. This locomotive was part of the General Electric advertising train and received a design in the Warbonnet scheme with the colour gold ("Gold Bonnet").

In February 1960 it was on the road in Southern California with one baggage car and three parlour cars to promote GE's "Golden Values." The train ran between Los Angeles, Pasadena, San Bernardino, Fullerton, and San Diego. At the end of the campaign, the locomotive and cars were returned to their usual colours and returned to regular service.

Shortly before the editorial deadline, the Norwegian-American manufacturer introduced a new shape to the market. The five-piece Trinity RAF semi-trailer wagons with a length of 53 feet with inscriptions from TTAX and their old logo are celebrating their market premiere.

The freight cars running on Jacob's bogies have five 53-foot semi-trailers from Roadway (905205-1).

The models are made of metal and have attached parts made of photo-etched sheet metal and plastic for different loading situations with semi-trailers and containers or without load. The trailers are also offered separately in a double pack (954005-1).



The TTAX Trinity RAF 53C Spine Cars (905205-1), which run on Jacob's bogies, are five-part and are being launched on the market as a new mould. Photo: AZL / Ztrack

#### Neighbourly help as a locomotive doctor:

An offer by Daniel Probst ([spur-z-nachbarschaftshilfe\[at\]web.de](mailto:spur-z-nachbarschaftshilfe[at]web.de)) is intended as neighbourly help against pure reimbursement of costs: He helps with maintenance, cleaning, repairs or motor changes for Märklin models with brush motors of all generations, i.e., the classic three- and five-pole drives.

Models with bell-shaped armature motors are not included here, nor are conversions to such motors or LED lighting and digital equipment. It should go without saying that this offer can of course only be made on a small scale and that the owner of a locomotive that is sent in bears the costs for postage, working materials, and spare parts.

It is our wish and idea to gather and deepen our own experience, but also to help hobby friends who do not have the necessary manual skills at this point. Getting to know each other in this way can perhaps also lead to a mutual exchange of personal skills for the benefit of others.



The BLS electric locomotive Re 4/4 (art. no. 5019RF) is also available as a finished model from NoBa-Modelle on the basis of a 3D print model. Photo: NoBa-Modelle

If you would like help here, please contact us directly so that we can exchange the necessary data personally.

#### Something new again at NoBa-Modelle:

No sooner have we announced the spring new products than NoBa-Modelle (<https://www.noba-modelle.de>) is already following suit. The electric locomotive Re 4/4 of the BLS is now available as a kit blank (art. no. 2019R) and as a finished model (5019RF). The double locomotive Ae 8/8 of the same railway administration has also been added, which is only offered as a blank (5020R), at least for the time being.

#### New from Micro-Trains:

This month MTL starts its new series "The War of the Worlds" based on the literary work of H. G. Wells. Car number 1 is a boxcar (item no. 518 00 840) with a motif from the 1906 edition of the book.

number 1 is a boxcar (item no. 518 00 840) with

Meanwhile, the light 83-foot sleeping cars return to the range in three versions: Union Pacific (550 00 011), PRR (550 00 061), and Norfolk & Western (550 00 240).

**Programme at Yellow Dwarf expanded:**

The Yellow Dwarf (<https://www.yellowdwarf.eu>) programme, which was only introduced last month, has already been expanded with some new articles. Construction sites can now be designed with the two assemblies “Construction Machines I” (art. no. 60220) and “Construction Machines II” (60221). Rubble can be dumped in the “Containers I” (60208), because they are open large capacity containers.

Transformer station and distribution cabinets (60251) are modern concrete buildings that we often encounter, but they are by far not as conspicuous as the earlier transformer houses. On the other hand, we are not supposed to look past the advertising billboards (60042), which are supposed to publicise products and brands. The boats (punts)(60107), which are already the third type of watercraft, will be the first to be built on the water.

There are also provisions for the beginning outdoor season: the “Garden Tools I” (60468) with ride-on mower and small shed, and the wooden flower beds (60460) create an attractive garden. After the work is done, you can enjoy a barbecue in the garden (60463).

**Message wagon for the first time for Z gauge:**

On 10 March 2023 Märklin offered a new creation from the “Message Wagon” series, which feature motifs and quotes from famous artists. For the first time, this offer was also valid for the Z gauge (art. no. 82391) – here, too, the models, for which the manufacturer does not give any information regarding quantities, were quickly sold out.



The first “Message Wagon” for Z gauge (item no. 82931) is dedicated to the Dutch painter Vincent van Gogh. On our portal pages we also show both wagon sides in close-ups.



The blue-and-yellow dominated copy was dedicated to the artist of the century Vincent van Gogh, who died at the age of 37 at the height of his creative powers. The motif was based on the oil painting "Cornfield with Crows".

#### Modelplant has closed:

On 17 March 2023 owner Thorsten Loth announced that he wanted to limit himself to his private hobby with Z gauge in the future. For this reason, he closed his Modelplant electronic distribution business for self-produced 3D printed parts, with immediate effect. After about ten years, it was no longer possible for him to operate this business, he said, explaining his decision.

The remaining stock has been taken over by the 1zu220-Shop and is being sold. If individual interior parts are missing to equip a coherent train, Thorsten Loth has agreed to produce these parts in addition. This is how an orderly end is to be brought about.

#### New refrigerated wagon variant from WDW Full Throttle:

William Dean Wright (<http://www.wdwfullthrottle.com>) offers a new double pack of the reconstructed 34-foot refrigerator cars with wooden walls. The red models with yellow side walls are discontinued at the CRX and carry Cudahy advertising (Item No. FT-9209-1).

The Cudahy Packing Company, whose design the cars show, was founded in Omaha (Nebraska) in 1887 and was one of the largest meat packing companies in the United States.

Around the turn of the century, railways converted many of their older wooden cars for modern use, using steel underframes, Bettendorf bogies and new braking systems.



The current 34-foot refrigerated trucks (item no. FT-9209-1) are representative of those from the former meat packer Cudahy. Photo: WDW Full Throttle

#### Range expansion at Werkzeuge Peter Post:

As announced in our new products reports in the last issue, there is an expansion of the range at Werkzeuge Peter Post (<http://www.peter-post-werkzeuge.de>). Jürgen Schirmer informed us that selected Microscale products will already be available from him at the Intermodellbau in Dortmund (20 to 23 April 2023).

These are Micro Set (adhesion promoter) and Micro Sol (softener) for applying and fixing decals on models, as well as Micro Mask (masking agent for paintwork).

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Board of Editors:  
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English translation:  
Alexander Hock, Christoph Maier, Oleksiy Mark, Martin Stercken

Further voluntary work: Stephan Fuchs, Ralf Junius (**Trainini TV**), Torsten Schubert

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Publisher and Responsible in terms of German press law (V.i.S.d.P.) is Holger Späing, Am Rondell 119, 44319 Dortmund; Contact: 49 (0)231 9598 7867 or by e-mail to [redaktion\[at\]trainini.de](mailto:redaktion[at]trainini.de).

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# Trainini

German Magazine for Z Gauge



**On a Narrow Track in Construction Fever**

**Building Vollmer Houses  
On traXS! 2023 in Utrecht**



## Introduction

Dear Readers,

Each of us is captivated by the sight of a special model railway layout. Even when the rolling stock comes to a standstill, our eyes continue to wander purposefully over the exhibits.

Of course, it is mostly the outstanding realisation of the miniature world with all its modern aids, but sometimes it is the small peripheral phenomena that we only notice at a second or even third glance.

This is exactly where we would like to focus our **Trainini®** annual theme for 2023. It is precisely the supposed “little things” that turn a model railway layout into an almost unique work of art, whereby metering and positioning are the highest art.

Just with the well-considered placement of miniature figures, lively scenes can be developed. In the landscape design, a well-dosed confusion brings about the chaotic character of the model: the nature that surrounds us. Various decorations in the right places create further dense atmospheres.

Such unique impressions can also be achieved when building a house. With the first part in this issue, Holger Späing really brings us to “life” with the construction of Vollmer’s “little workshop”. A complete interior with appealing lighting that follows the period depicted amazes the observer, even if it is only an appearance.

In the second part of his 3.0 mm narrow gauge project, Reinder Rutgers shows the many possibilities, but also the feasibility of building the appropriate rolling stock. His declared intention is to also make our readers “want less,” whereby this expression only refers to the distance between the two track insides.

If he can inspire more Zetties for his idea, it will probably not only please him. We, too, would be thrilled to be able to report on many new models and creative realisations!

Two reports on the model railway exhibitions that have now started again round off this issue. Stephan Fuchs visited the Faszination Modellbahn 2023 in Mannheim for us and reports on his impressions from the venue.

A little later, part of the **Trainini®** editorial team visited the On traXS! 2023 in Utrecht, especially for the reason that the Gauge Z layout “Boisschot Statie” by Pieter Willems and Jan van Casteren was there.

Another reason was of course that it is also the exhibition with the highest requirement profile in Europe. You can read all about it in the report in this issue. And, so, I hope you enjoy reading and leafing through this issue.

Sin-Z-erely,

Holger Späing



Dirk Kuhlmann  
Editor

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We thank Reinder Rutgers for his contribution.

Date of publication of the German language version of this issue: 30 April 2023

### Cover photo:

The modern diesel locomotive shows us typical sixties small railway operation with its PmG as it crosses the bridge. On the model railway this looks like an almost perfect idyll, but such narrow-gauge railways had long been a discontinued model at that time. Photo: Reinder Rutgers

Only 3.0 instead of 6.5 mm gauge (part 2)

## In Do-it-yourself Construction, the Path is the Goal!

*After our reader Reinder Rutgers from the Netherlands presented his narrow-gauge project in March, the promised continuation now follows. While the first part dealt with the special challenges and the variety of models achieved, today he tells us how the beauties shown can be achieved by hand and what challenges await those interested in rebuilding them.*

By Reinder Rutgers. A chronology of the building process or its detailed description would be probably too boring for many. Instead, I'm going to explain a few things today, not just in the narrow-gauge area can be useful and hopefully not known to most.



For the sparse freight traffic on the narrow-gauge railway, a dedicated goods train is often not worthwhile. The diesel railcar, therefore, has a covered freight wagon in tow as it approaches the stop.

Common tools such as needle files, miniature forceps, tweezers, etc., do not require further explanation. I would therefore like to mention here only a few working materials that are important to me and perhaps less known to third parties. In my opinion, the most important 'insider tips' are the following tools:

- A flat, 10 mm thick steel plate to be able to glue or assemble parts accurately,
- MEK (methyl ethyl ketone) for bonding,
- a series of rectangular brass blocks with coordinated dimensions,
- laboratory-grade single sided razor blades,
- three types of toothpicks, each with its own possibilities,



- a pin in a pin block and a strong, yet sharp and hard, conical steel needle in such a holder,
- miniature precision angle with and without stop, and
- a mechanical miniature calliper (length 70 mm).



This photo shows some tools and aids typical of our author's method of working.

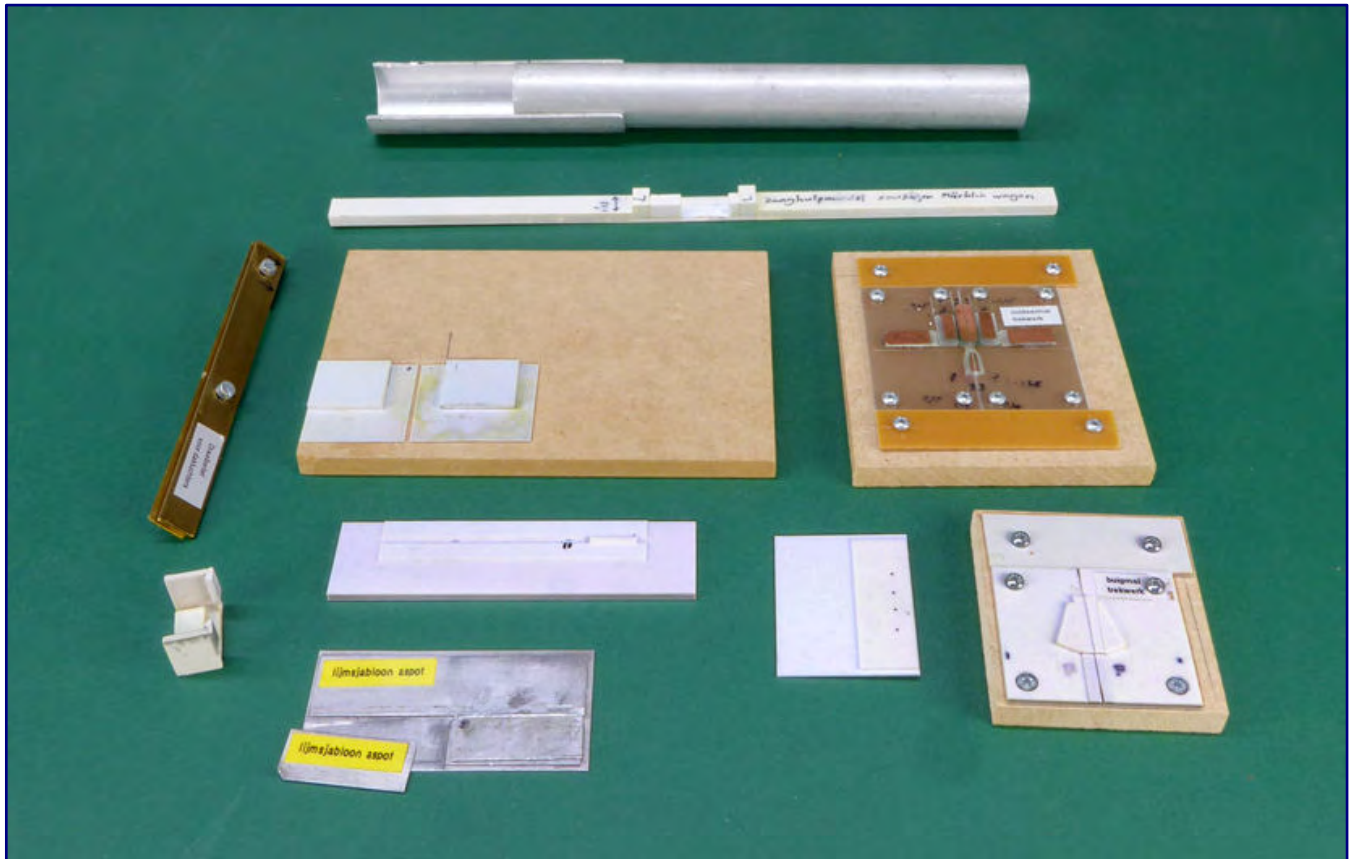
The brass blocks are among my most important tools. I have about 50 blocks ranging in size from 20 x 20 x 10 mm to 2.4 kg blocks of 60 x 60 x 80 mm. There are endless possibilities to use them in model making.

Brass simply has the advantage that it is heavy (higher specific mass than steel) and, in contrast, does not need painting. It is expensive, but I have had my blocks for 40 years now, so they were a good investment. Besides that, I also use several small brass and aluminium strips as aids.

The razor blades from the laboratory sector are much thinner than the finest knife. This means that they really enable fine work, as was always required here in my project. Not 'secret,' but still important: commercially available polystyrene glue and very fine tweezers of high quality are further indispensable aids.

But perhaps the most important tool of all is the eye. The human eye is capable of perceiving differences of a hundredth of a millimetre. In my experience, I can measure what I want. But, if the eye says that something is crooked or unequal, then it likely is.

Those who are well acquainted with human vision may now want to object that the eye, or more accurately the brain, can also be tricked into perceiving. You are absolutely right, but if it is taken into account when building models and landscapes, this ensures the only correct perception!



This small selection gives an overview of the gauges as they are used in large numbers in do-it-yourself construction.

Nevertheless, gauges are and remain a special category. They are simply essential for some work in order to achieve correct and reproducible results or to be able to manufacture a part at all. I have made over twenty such tools for the production of my Ze gauge rolling stock. Often, they are simple in principle, the difficult thing is always the required accuracy.

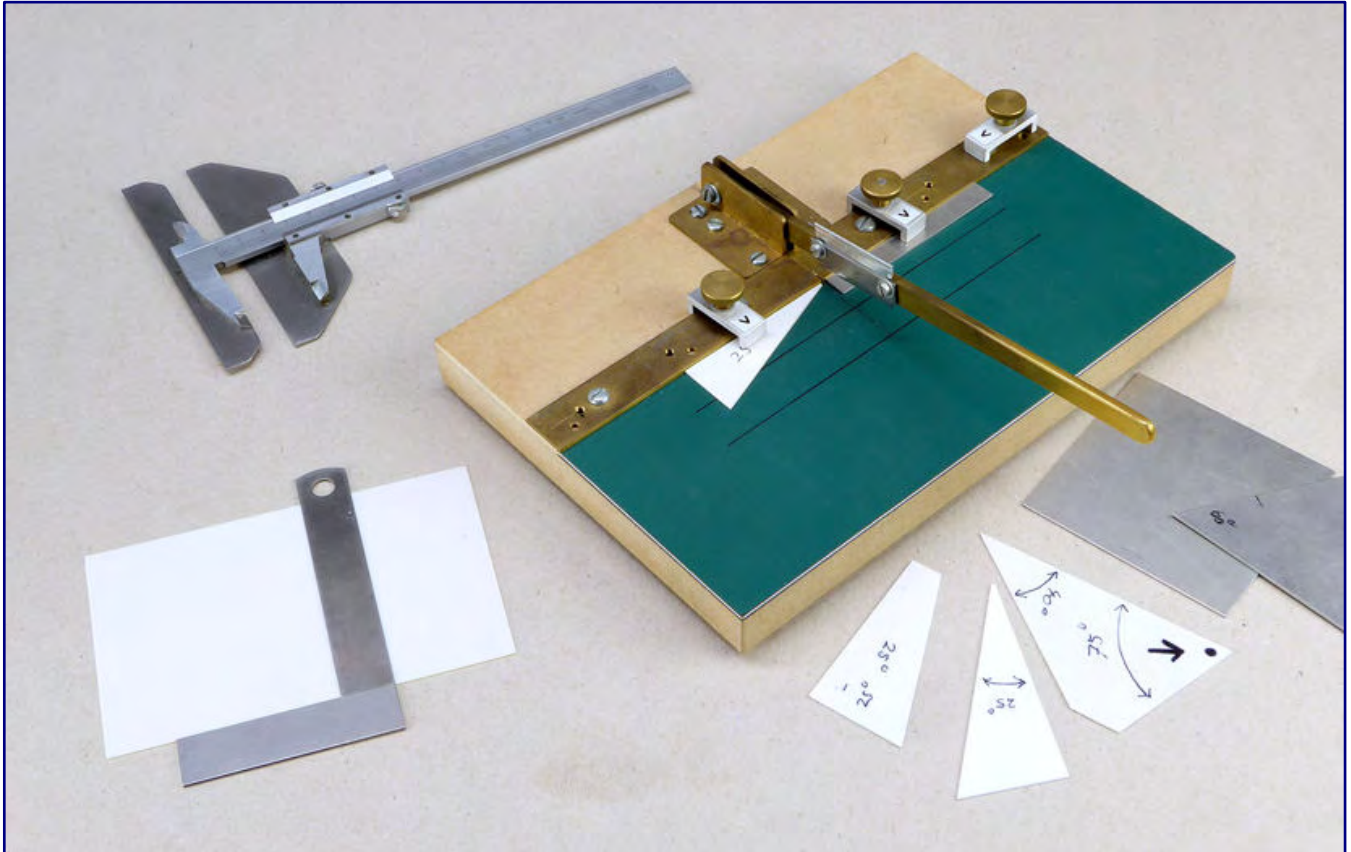
## Self-designed tools

Over the years I have built several devices myself because they were not available commercially. For the production of parts with high precision and/or the same size, a fine trimming device is almost indispensable.

When I built mine many years ago, there was nothing comparable in the market. My own construction allows a very good view of the blade, which is ideal for precision work. For example, I have cut discs only 0.3 mm thick from polystyrene round profiles and other parts are only 2 x 2 mm in size with this device.

I constructed a parallel calliper myself from an ordinary one and a steel ruler. It makes it much easier to draw or cut exactly parallel lines. A flat angle that I built myself is only 0.5 mm thick. It was constructed

from three small steel rulers with the help of a precision angle. It is extremely practical in model making when working with panels and shortening strips at right angles.



Here you can see three of the special tools made by the author, which have served very well in the construction of the narrow gauge models.

Perhaps readers are familiar with the following, but to be on the safe side, I don't want to omit it: Indispensable method for my construction is gluing with methyl ethyl ketone (MEK, also called butanone). MEK is used in industry to remove plastic residues in machines.

It is cheap if it can be bought in litre bottles. In model shops it is also offered in small bottles. But care must be taken when handling this hazardous substance: The vapours are toxic and also explosive, it is imperative to ensure good ventilation!

Personally, I always use "Slater's MEK-PAK," a liquid that smells less strong than butanone but, according to my impressions so far, sticks just as well. In addition, it should be mentioned that a re-evaluation of this substance has been underway in the EU since 2018 with regard to the dangers it poses.

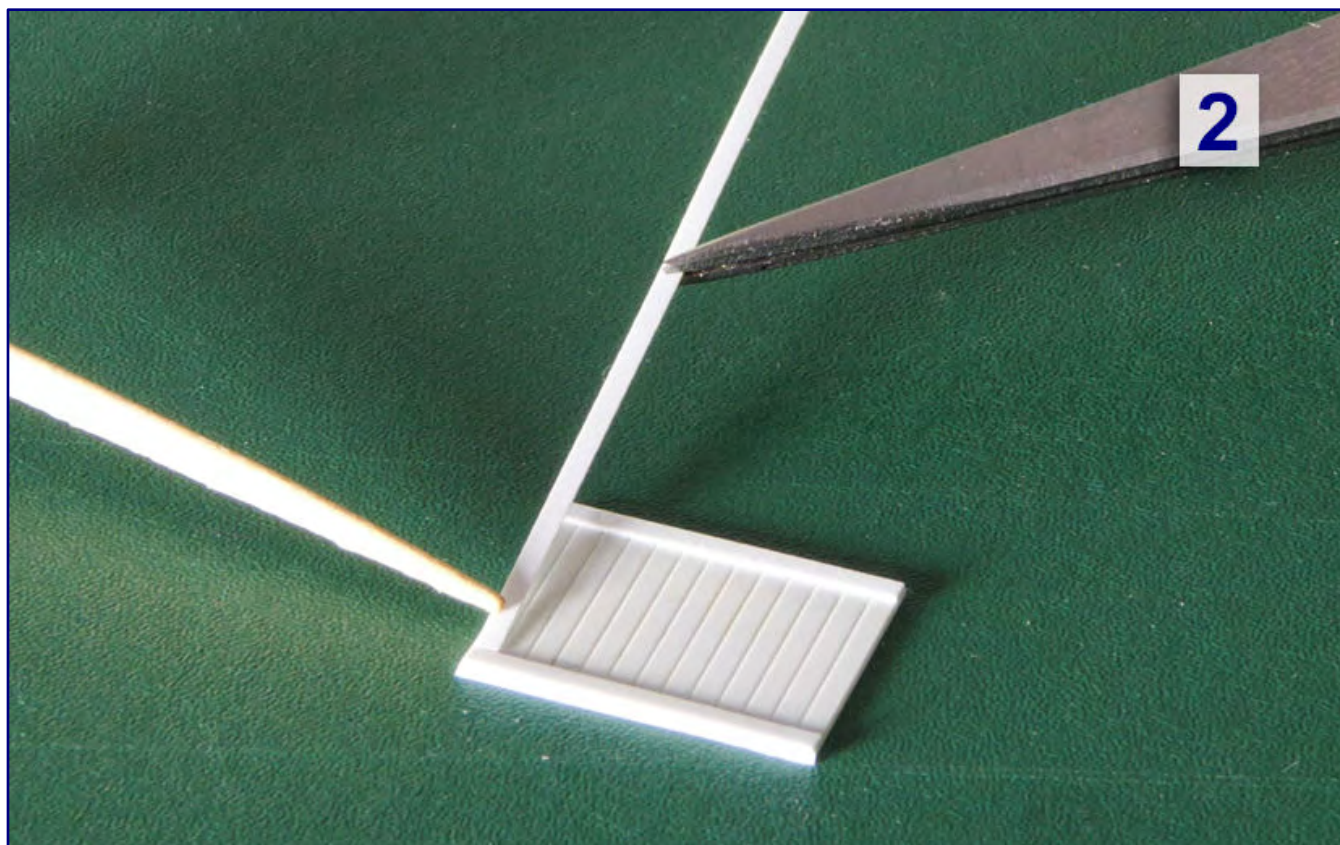
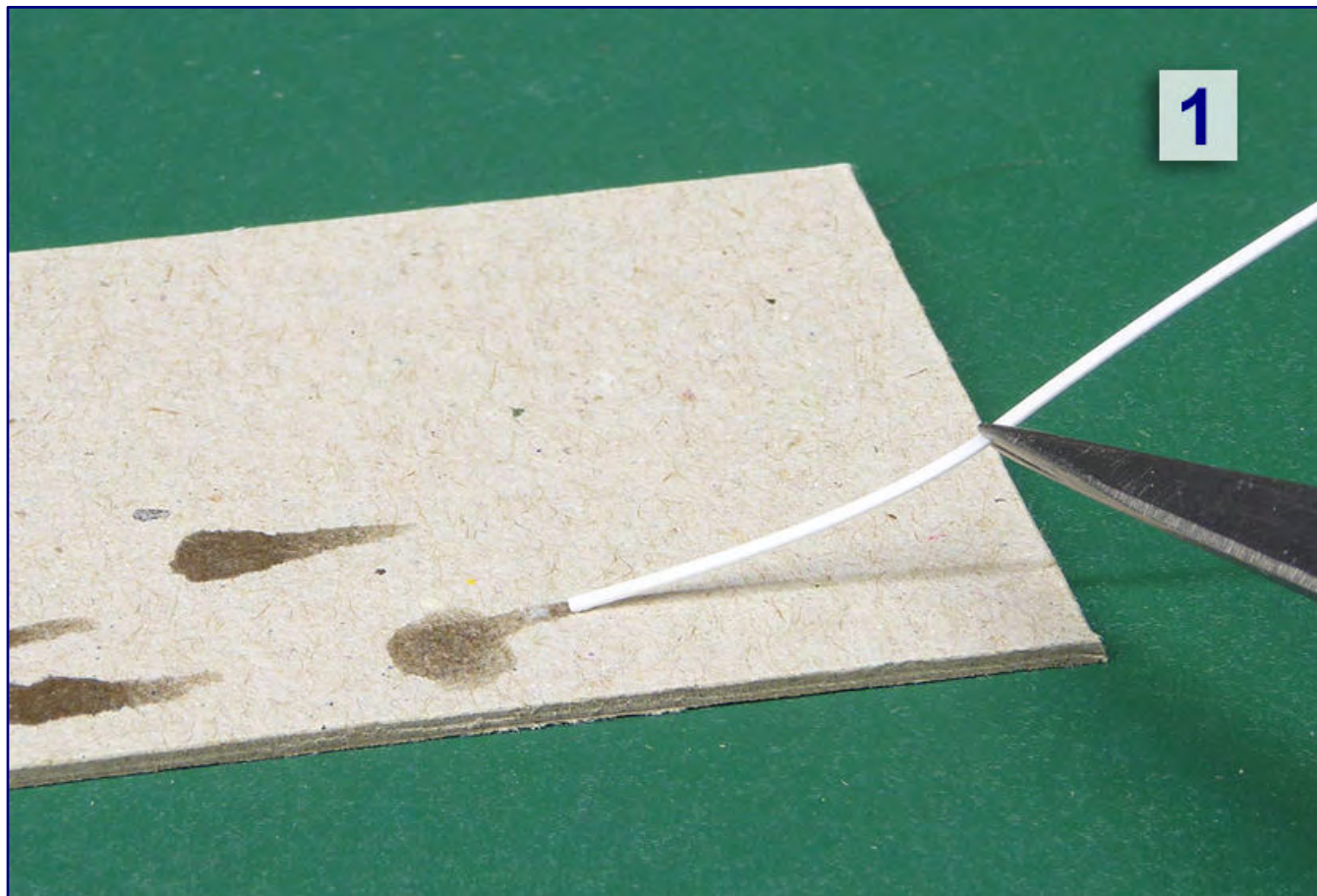
Bonding with MEK is done as follows: We press the parts together and brush some of the chemical over the seam with the brush. The liquid is absorbed by capillary action and fuses the part.

Residues are invisible on the model because the liquid evaporates immediately. Consequently, one drop too much does not ruin the model. Smallest parts can therefore be glued invisibly. Through practice, develop a feeling for how much liquid can be used without parts deforming or melting completely. It is also important to press the parts together, because even the smallest gap will not work.

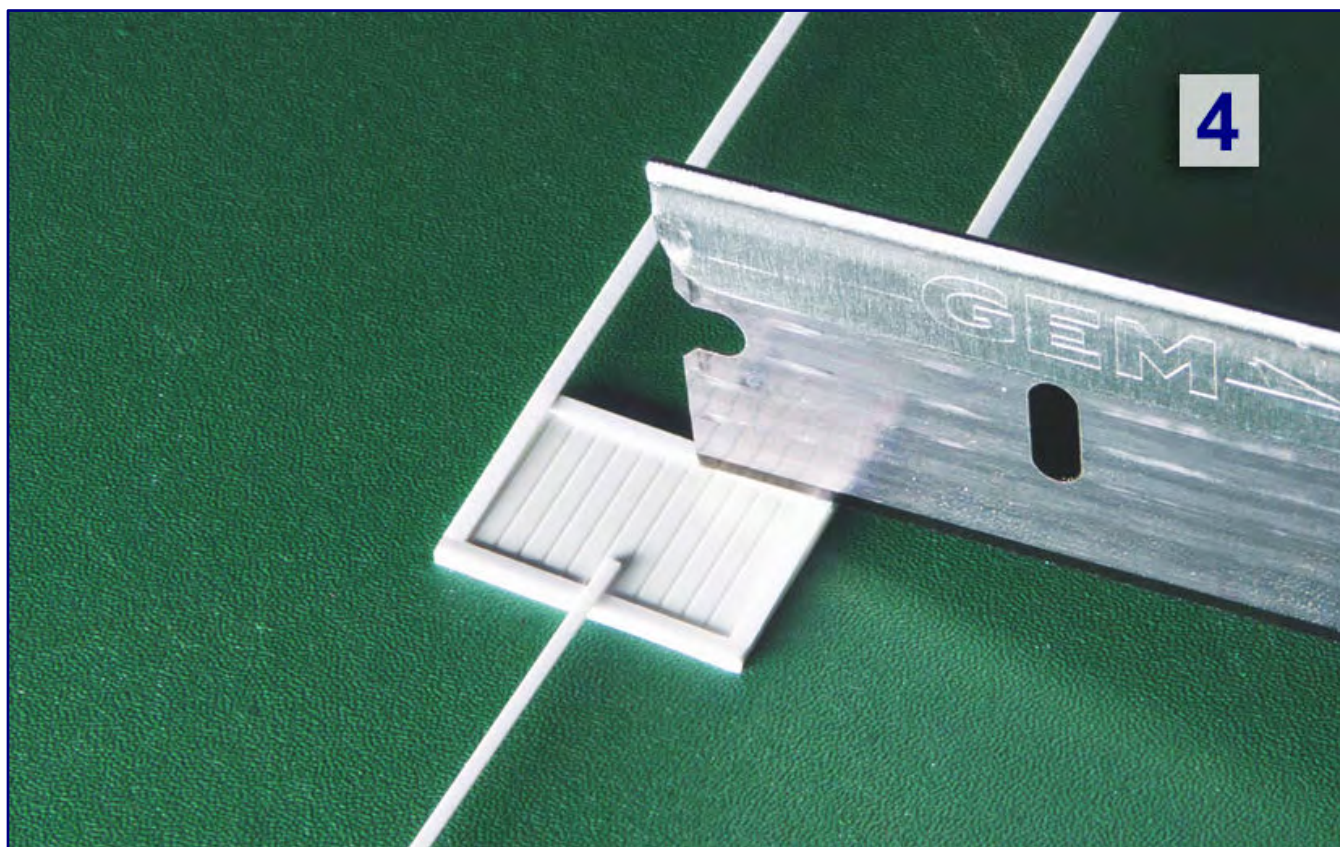
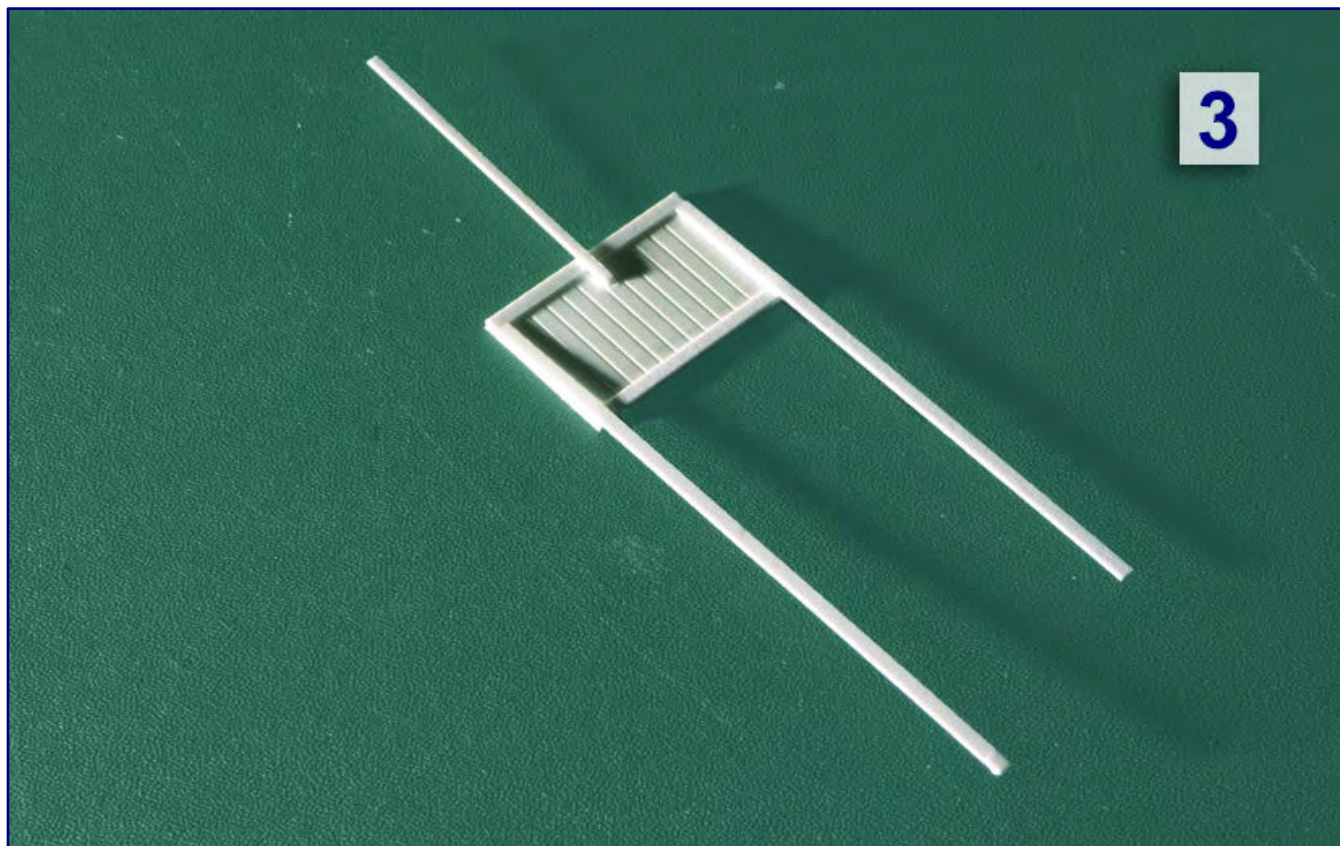
continues on page 10



## Photo gallery for assembling the smallest parts











Explaining the assembly steps in a two-step method:

A droplet of polystyrene adhesive is placed on a slightly absorbent substrate (paper or cardboard). The end of the part to be assembled is pulled through it with its underside and then wiped off (step 1). The prepared part is still much larger than it needs to be in the end. This makes it easier to handle (step 2).

The part is now placed on the workpiece; in this case it is a car door on which the side stripes are already attached (step 3). A sufficient drying pause must now be allowed. Only then the part is cut to size. A single-edged razor knife makes this possible with precision (step 4). This method of working can greatly simplify the production of exactly fitting parts. A razor knife is also a valuable tool for parts that are to be mounted freely, because the unavoidable cut in the substrate becomes invisible after painting. Finally, the part is fully glued with MEK (step 5).

MEK initially dries very quickly, much faster than polystyrene glue. So we must work quickly. However, curing also takes quite a long time. If the joint is critical, it is not a bad idea to wait a day before loading the joint.

In addition, I would like to introduce a two-step method for mounting small parts (see picture sequence): A drop of polystyrene glue is applied to a slightly absorbent surface (paper or cardboard).

The end of the part is dipped in it and wiped off. The future part is much larger than it will and must be in the end; this makes it easy to handle. Then this part is placed on the workpiece, in my example case a car door on which the side strips are already present.

Only after it has dried through the strip that is too long is cut to size. A single-edged razor knife is the tool of choice here. This method of working can make it much easier to produce precisely fitting parts. It is also a good method for parts to be assembled freely, because the probably unavoidable cut in the substrate is invisible after painting. Finally, the part is glued in place with MEK.



Probably important to know: The butanone is invisible, but it affects the surface properties of the material. I have tried three different brands of acrylic lacquer and primer, but in my experience it covers less well on the areas treated with it.

This can be remedied by lightly roughening with sandpaper or a fibreglass brush. By the way, classic enamel paints such as Humbrol or Revell show this problem less or not at all.

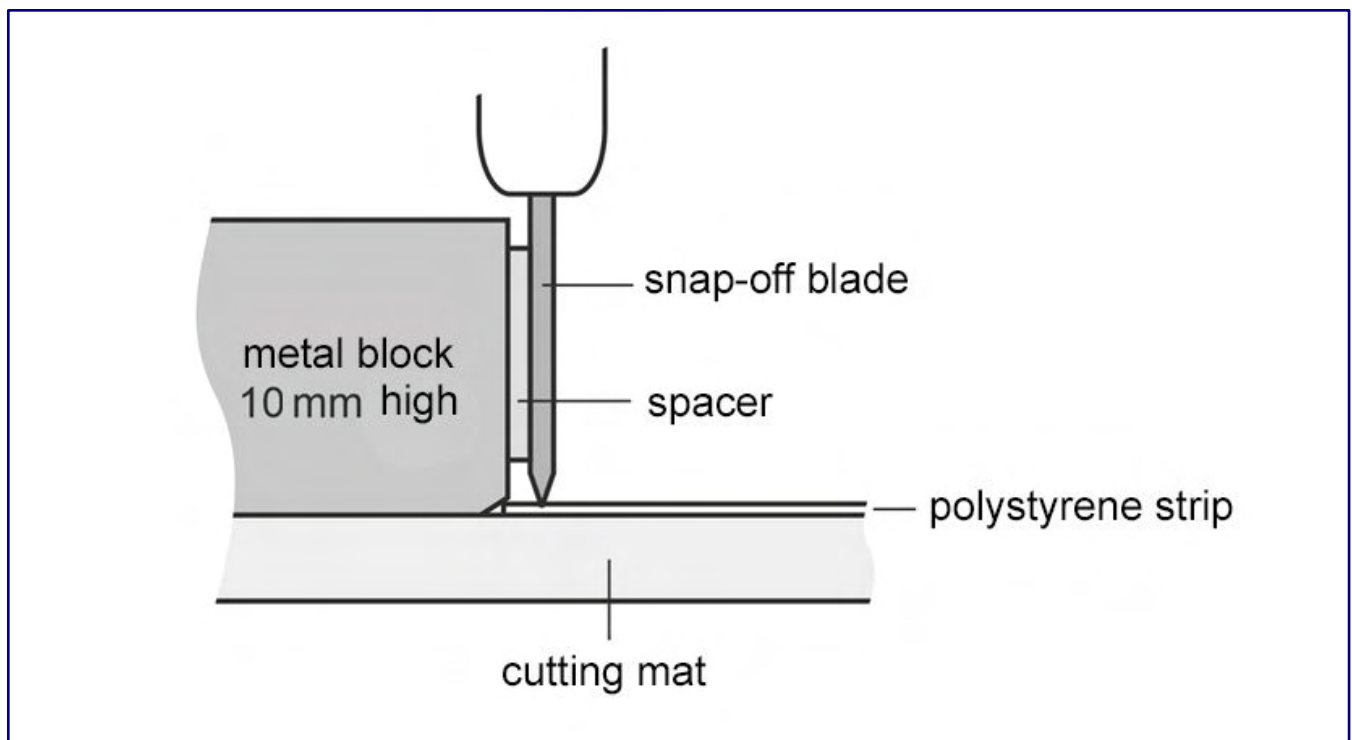
## Inscriptions and superstructures

I designed the locomotive and wagon inscriptions myself. On the one hand, this was necessary because there are hardly any for narrow-gauge vehicles in the market and the ones offered are limited to very narrowly defined prototypes.

This made it possible for me to give each wagon its own individual inscription. Following my drawings, wet sliding patterns were made as a special production. They are not as sharp as Märklin pad prints, but they were still better than expected. The smallest letters are only about 0.25 mm high and are still recognisable as such. Larger ones are also legible.

The foil base of the decals is still noticeable despite the extra thin foil, special adhesives and softeners. A material thickness of only 0.07 mm still corresponds to 1.5 cm on the prototype.

Therefore, with a few exceptions, I have refrained from using push-off pictures for passenger coaches with smooth walls, because it could look disturbing. It is probably also acceptable for NBE small railways, as historical photos prove: sometimes railcars and passenger coaches in the sixties did not carry any conspicuous inscriptions.

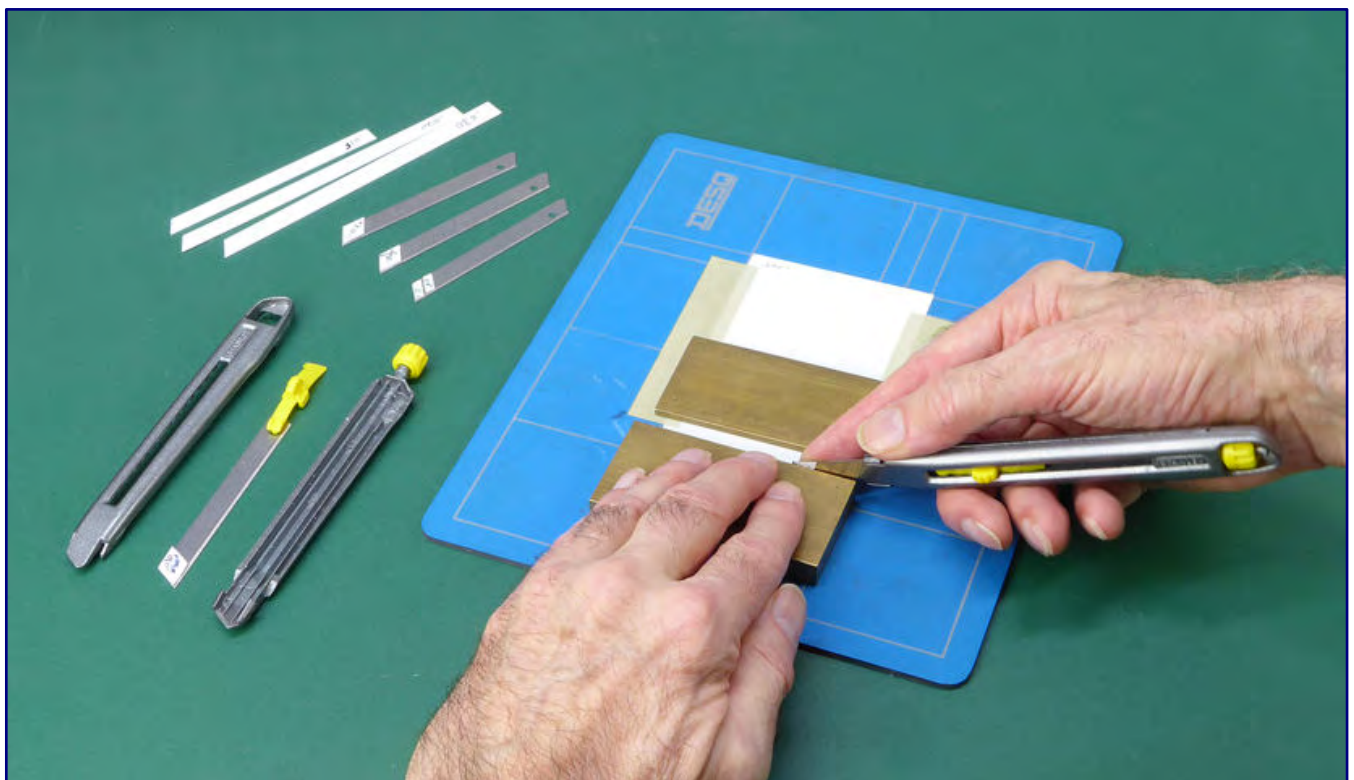


This illustration clarifies the principle of cutting very fine polystyrene strips. Particular attention should be paid to the small opening on the underside of the metal block.

Polystyrene strips are fundamental building blocks for finely detailed plastic models. Evergreen (distributed by Faller) offers them in dimensions down to 0.25 x 0.5 mm. That means 55 x 110 mm in prototype size, which is sometimes too coarse. I therefore make my own smaller strips from 0.13 mm polystyrene sheets from this supplier.

This process is as follows: A polystyrene sheet about 70 mm wide is fixed to a cutting mat with adhesive tape. On one of the sides (without adhesive tape) I place a rectangular metal block 10 mm high. It is important that the block has a slightly bevelled or rounded edge so that the 0.13 mm plate is held exactly at its end (see diagram).

A 9 mm break-off blade is prepared by gluing on a small piece of polystyrene as a spacer. The width of the polystyrene strips cut off thus depends largely on the thickness of the spacer!



This is how the cutting process shown on page 11 works in reality. On the left are the dismantlable blade holders and the snap-off blades with different spacers for different sized strips.

If the blade of the knife is pressed against the metal block while cutting, we get strips of always the same width. It takes a little sensitivity and practice to do this well. It takes some experience not to press too hard horizontally and vertically and not too lightly on the knife.

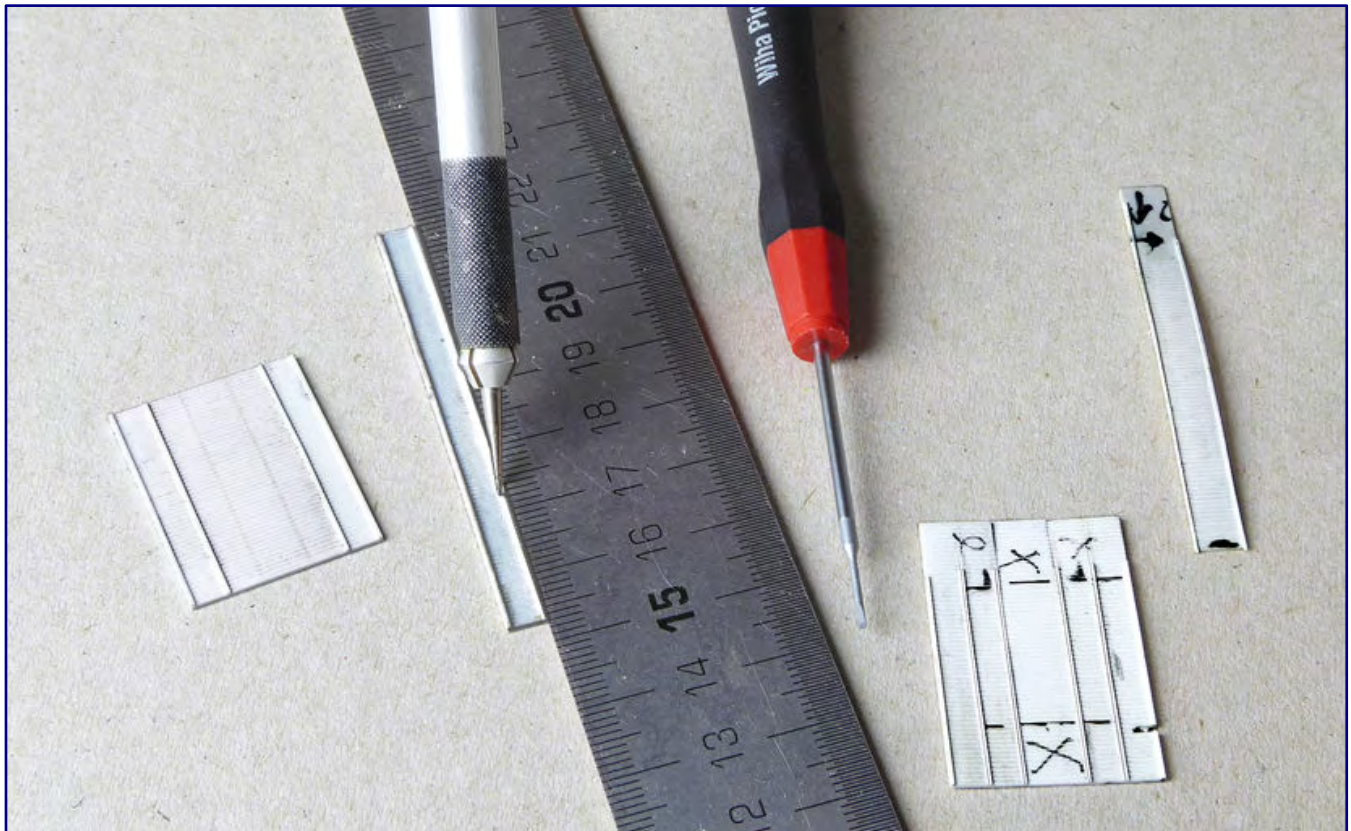
Good observation is also important to stop immediately when a strip comes loose. Then there are slightly curved strips of identical width. A strong curvature, on the other hand, indicates too much pressure during cutting.

Of course, the process only works with a fresh, very sharp blade. I recommend a holder from Stanley, because it clamps the blade securely and I can also replace blades with a glued-on spacer here.

Another tip for those who want to copy: Evergreen offers polystyrene round profiles from diameters of 0.5 mm. Even that is rather thick for my purposes. Slaters, on the other hand, also offers profiles with a diameter of only 0.25 mm. I have fallen back on this very often.

In reality, there were also wooden box struts on narrow-gauge freight wagons, for example on the Rügen small railways. Mostly, however, steel U-profiles were used. I am reproducing these, as I will describe in the following.

Polystyrene strips of 0.13 x 0.6 mm are fused to the substrate using MEK. It is important that they are properly glued! However, too much glue dissolves them, so the right sensitivity is required.



**The individual stages of side wall construction: On the left is a prepared side wall panel with strips. The scribing is shown in the middle. Finished side wall panels with the surfaces crossed out as unusable can be seen on the right (top right: door panel). About 50 % of the processed surfaces were suitable for further use - so it takes a lot of practice!**

After thorough drying, a steel ruler is placed against the side of a strip. With a sharp needle, I first carefully scribe a groove in the middle of the strip. Then it deepened carefully several times. Here, too, care must be taken never to apply too much pressure. Patience is needed.

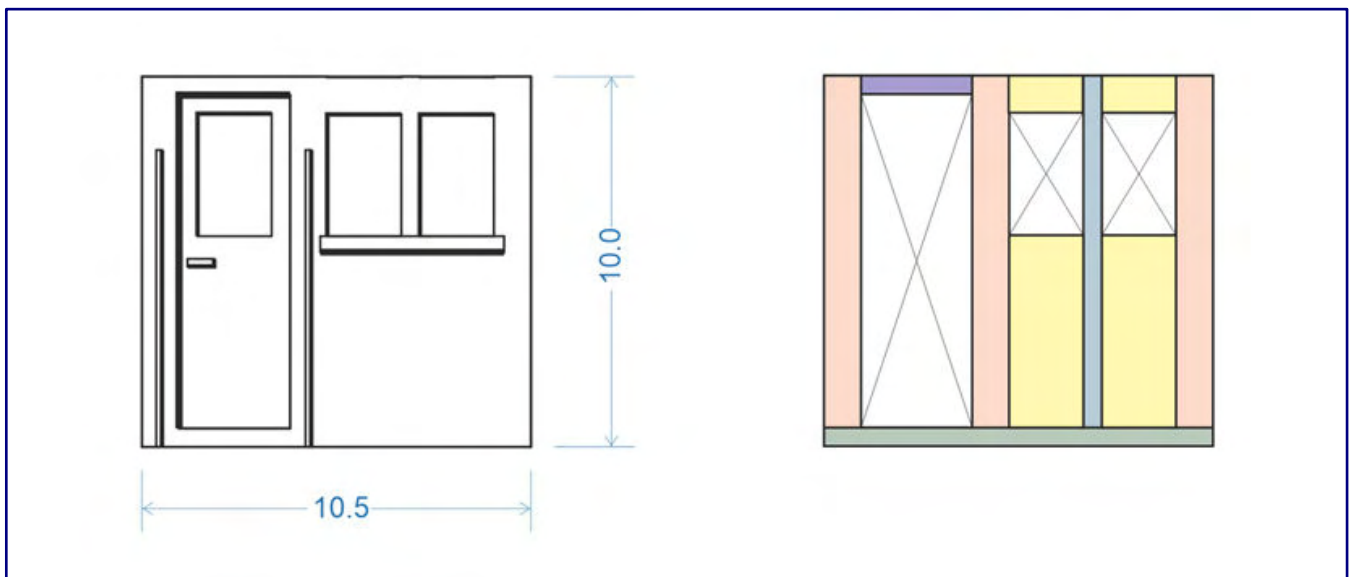
Finally, the "channel" can be reworked with a sharpened mini-screwdriver or brass rod. A perfectly rectangular finishing is not recommended, because then the part would be too vulnerable. Mostly a shadow falls into the U-profile anyway and from a normal observer's distance its shape cannot be perceived correctly at all.





This is how the two-axle freight wagon looks with the mounted side walls including the box struts made of U-profiles after painting and labelling.

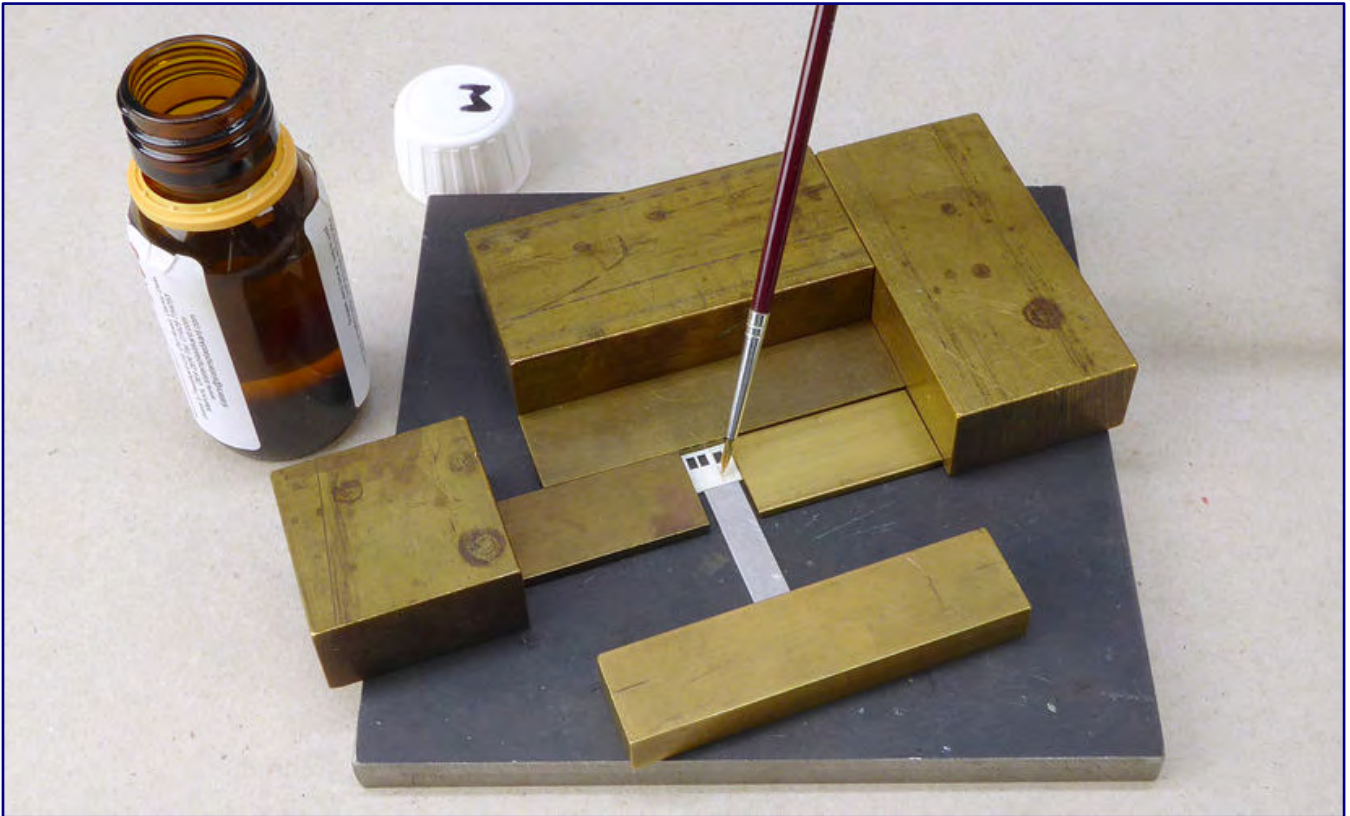
In 1:220 scale it is also quite difficult to make precise openings in polystyrene walls – windows are meant to be here. Therefore, I turn the matter around, so to speak: a wall is built up around these openings using parts. Thanks to MEK, the strips can be fused so well that there are no visible joints afterwards.



On the left is the construction drawing for the housing wall of a gauge Ze diesel locomotive. It is converted into a drawing with wall parts (right): crosses are openings. The same colours represent the same width. All parts should have the same thickness, here 0.4 mm.

Two things are also important here: The parts must be precisely manufactured. The fine trimming tool mentioned above is the tool of choice for this. Secondly, the strips to be glued must be joined together on a flat surface so that they lie absolutely flat.

continues on page 16





**Picture texts for page 15:**

The parts of the enclosure wall are glued together on a steel plate (photo above). Heavy, right-angled brass blocks fix the parts in position, the low ones allow a good view of the workpiece. The drill stand was modified with fine height adjustment and locking facility to obtain a (only limitedly stable) device for milling micro-grooves (photo below).

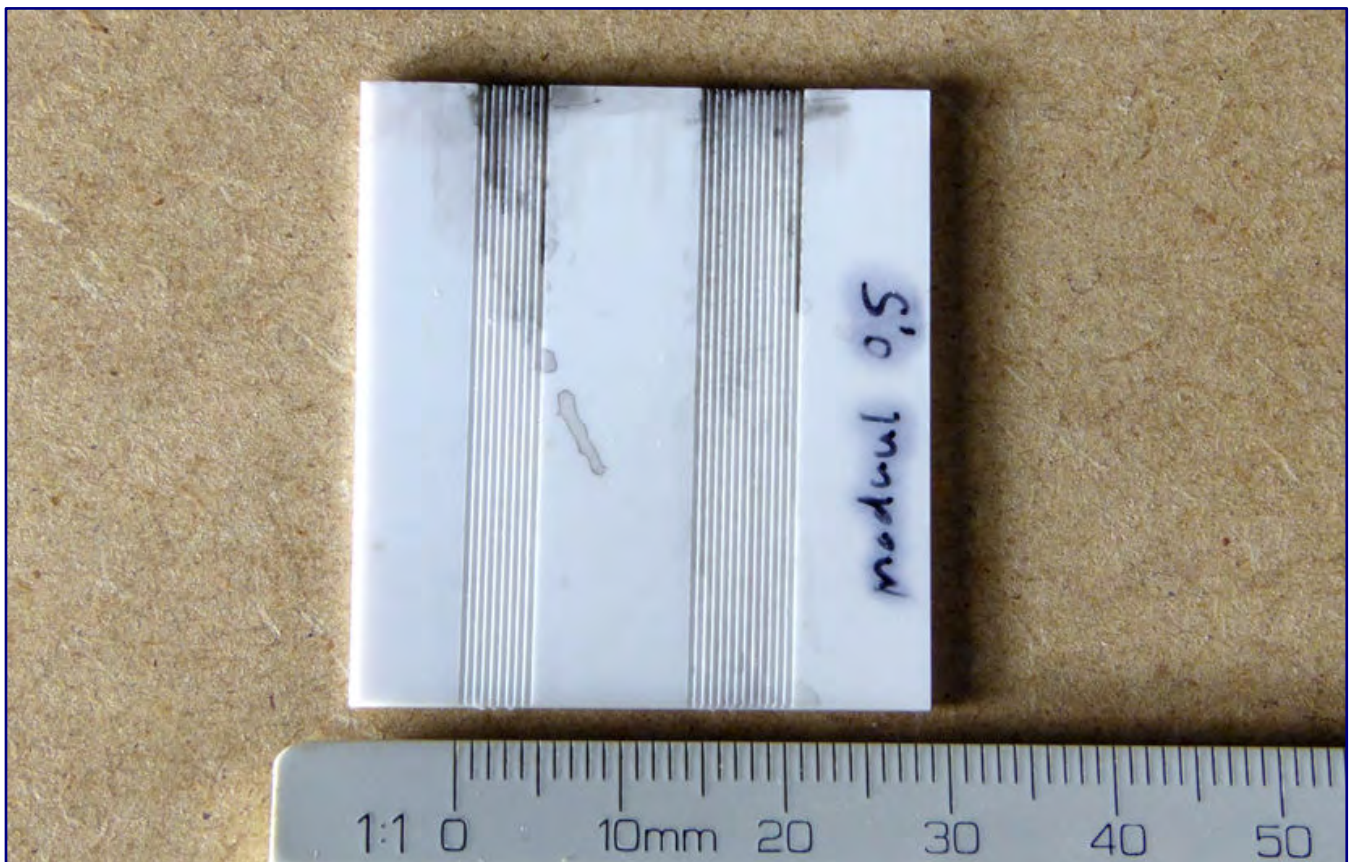
I use a 10 mm thick steel plate for this. On the one hand, this ensures that there are no height differences, and, on the other hand, I protect against the unavoidable excess of glue. Polystyrene and glue (almost) do not stick to the metal, so the finished part can be easily removed from the surface.

### Difficult: Construction of gratings

Grills and rust plates are often found on diesel and electric locomotives or in the ventilation openings of wagons. I have therefore tried out several methods to be able to recreate this in Z gauge in a believable way, preferably as simple as possible.

It works best when I mill grooves in polystyrene with very thin miniature saw blades. I found paper-thin saw blades of only 0.12 mm on offer from Proxxon. I prevented the feared melting of the plastic under the influence of heat by not sawing too deeply, and a circular saw blade also generates considerably less heat than a router.

If this still does not work, cooling with a drop of washing-up liquid is advisable. The smallest grates I make have a module of 0.35 mm. So, I can't get around special tools like a mini drill-milling machine, drill stand, and cross table.



Here you can see a milled grate plate in module 0.5.

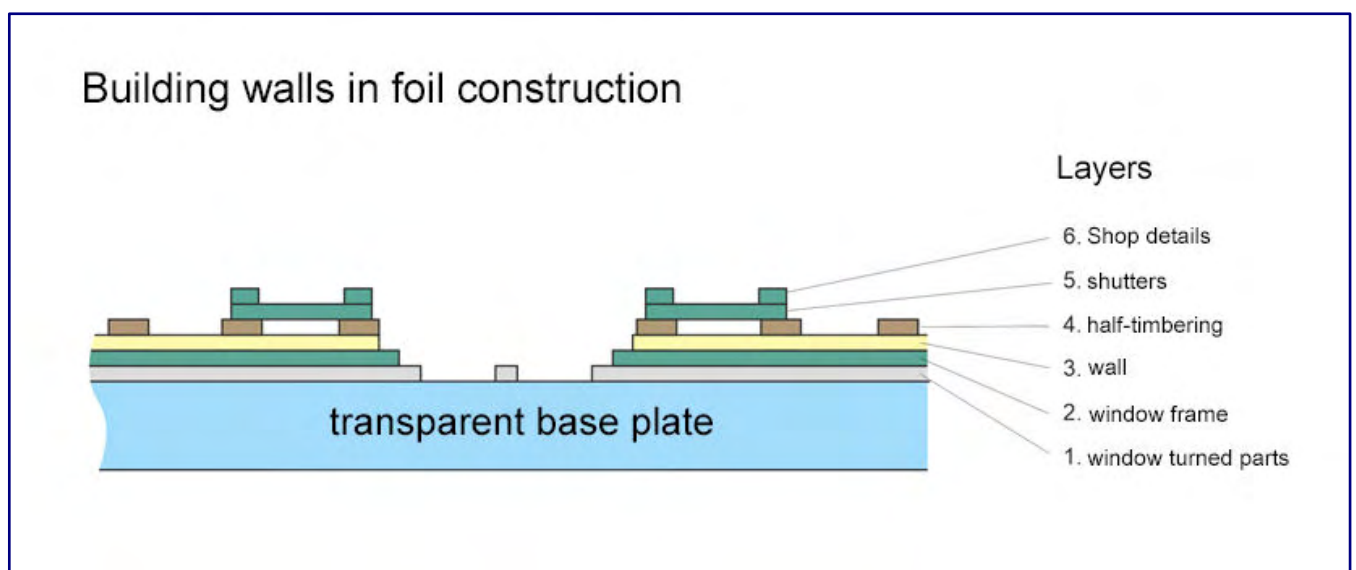


The side walls of railcars and passenger coaches, on the other hand, are made of self-adhesive film on transparent polystyrene. Foil has some advantages in the N or Z sizes. The result looks filigree because the foils are thin.

DC foils, for example, only have a material thickness of 0.1 mm. This corresponds to 2.2 cm for the prototype, which is quite realistic for wagon side walls, but also nice for building windows. Windows made of cardboard (laser-cut kits) or polystyrene often look much too thick.

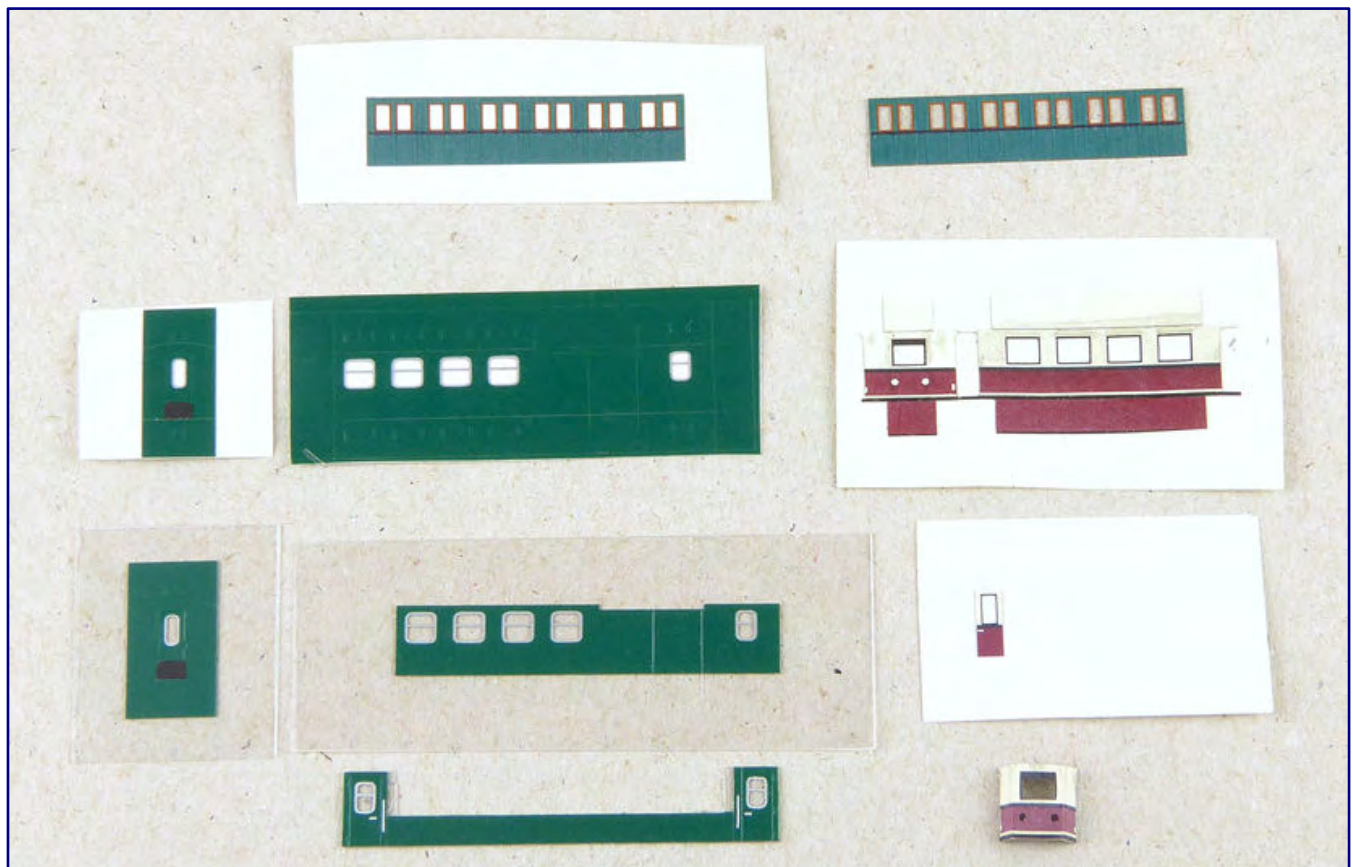


The self-made lay-on plates on the small Silhouette CNC cutting plotter make it possible to make changes to the foil package during a break in the cutting process.



Construction of the wall of the Göppingen House in 6 layers (see page 18 below)

continues on page 20







Here the foil was cut by hand (photo above). After painting, the house was given a patina. The passenger and luggage wagons (photo below) also have the foil walls described. These were matted and tinted before cutting.

Photos on page 18

The passenger car parts are either cut directly on a transparent background or the cut walls are brought over to the transparent background using a transfer foil (photo above). The photo below shows the gable of a prototype from the Märklin town of Göppingen (H0 size). The foil is on 2 mm thick transparent polystyrene here. The house wall is shown without aging, but the foils can be excellently treated with colour and also embossed.



I described my way of working in detail seven years ago in four articles in the Dutch magazine 'Rail Magazine'. The number of articles already indicates that I cannot describe it nearly as precisely in this framework. I will mention some of the most important points here in telegram style:

- Self-adhesive foils from reliable manufacturers are sustainable. I have 30-year-old self-adhesive wagons with films that are still good.
- The surface structure of self-adhesive films can be easily changed by sanding or embossing. Embossing also lasts for a long time.
- The colour of self-adhesive foils can be influenced by spraying thinned paint over them. Normal modelling paints also adhere well in the long run.
- Beautiful results come out of multi-layered workpieces.

## Narrow-gauge Ze for beginners

One of my first experiments with the Ze size was the conversion of an Uerdingen railbus. I narrowed it down to 11 mm. The width of the side is still prototypical. With this measurement, the two lower headlights have just an acceptable distance.

I used a circular saw to make two halves of 5.5 mm width from the Märklin housing. These were then also shortened in height by sawing off the black frame edges. This could also be done by hand with a fine saw and then finished with sandpaper. After gluing the halves together, the roof has a small fold, which I sanded out.



**A first step into gauge Ze: the powered railcar (right) was created as a conversion from a Märklin railbus.**

Next I mounted a new, upper top light on the roof in holes on both sides. These consist of a polystyrene round rod (PS) in a drilled out PS tube of slightly larger diameter. If you want it to be easier, use only one round rod. The third top light can also be omitted if the layout is set in epoch IIIa.

If disturbing glued seams are visible, there is no way around filling. At the ends, however, a buffer bar may also be able to hide seams. However, it is best to keep the seams as clean as possible. Then the builder also saves a new windscreen! By the way, a two-part windscreen is quite prototypical.

The next step is to fit the new housing to the transparent body of the T-gauge driving unit using PS strips. A trick to fit this housing without it sitting too low or too high is to glue a transparent 3 mm polystyrene strip on top of it. It exactly fills the distance from the top of the part to the railcar body. Because it is transparent, it will be imperceptible later.

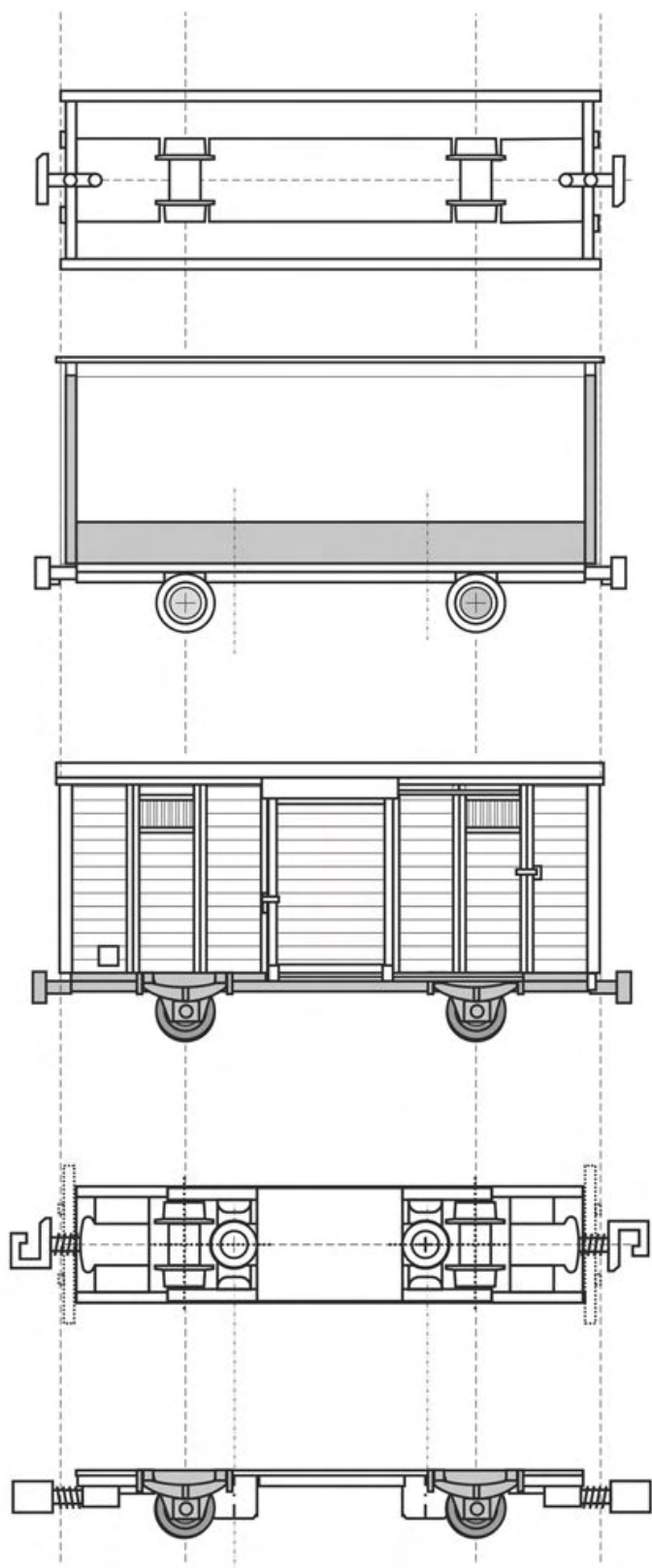


The size comparison between the light railway diesel locomotive and the V200 with passenger train illustrates once again the size differences between narrow and standard gauge in 1:220 scale.

After gluing in the drive housing, footsteps are attached to the railcar. Finally, couplings may have to be attached. Either simple eye couplings can be made or alternatively coupling holders from T-gauge bogies can be sawn off. These can then be attached to the body with two-component or super glue.

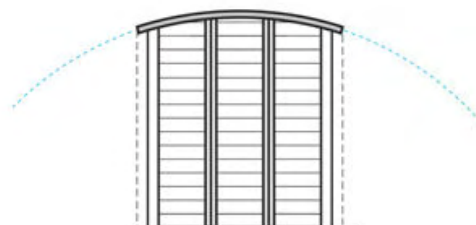
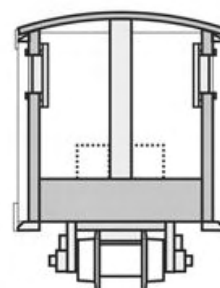
Finally, it is time to paint. This can be done only by hand around the windows, because they are firmly moulded in Märklin's rail bus. Precise covering all the windows would be a Sisyphean task. However, the roof and the lower part of the coach body can also be spray-painted.

## Digression: drawings for self-construction of a boxcar



26 mm tweeasser

SCHAAL 2:1





Such a conversion could be a relatively easy introduction to the gauge Ze for everyone. After all, there were many small railways with single-drive railcars. And thanks to the holding magnets, the drive units can also run on very steep tracks on their own. Consequently, a steep or cogwheel track can be built without much effort.



These four-axle narrow gauge wagons were based on Märklin wagon bodies. If you study drawings of narrow-gauge wagons, you will notice that the side walls were often about the same height as those of standard-gauge wagons of the formation type. The open wagon was created from a low-sided wagon, the body of which resembles a high-sided wagon on narrow gauge.

It is comparatively easy to build four-axle narrow gauge freight cars from Märklin's Era I models. These were usually narrower than passenger cars, so their car bodies should also be narrowed to about 9 mm. In any case, it is important to pay attention to credible proportions.

It should also be noted that narrow-gauge wagons always stand much lower on their underframes than standard-gauge wagons. This also requires structural adjustments or consideration. It is best to look at photos and drawings of originals beforehand and to diligently try things out until the necessary "knack" develops.

All photos and drawings: Reinder Rutgers

**Sources used for tools:**

- <https://www.conrad.de>
- <https://goldschmiedebedarf.de>
- <https://www.gw-werkzeuge.de>
- <https://www.hbm-machines.com>

**Sources of materials used:**

- <https://www.faller.de>
- <https://slatersplastikard.com>

**Sources of supply in the Netherlands:**

- <https://www.meijerenbleasing.nl>
- <https://www.metaalwinkel.nl>

Layout details (part 1)

## Innovation with inner Life

*Polyplate is Viessman's name for a composite material which the manufacturer uses for its entry into the market for laser-cut kits under its Kibri and Vollmer brands. The first Z gauge release of these type of kits is a small workshop building, which allows experiencing this new material. We have tested the kit and immediately filled it with life at the start of our annual focus theme on layout details – thus equipped, it immediately takes on a leading role on the layout.*

Two things that complement each other perfectly are more than just the sum of their individual parts. This also applies to the “small workshop” kit, which Viessmann recently released under its Vollmer brand (art. no. 49590).

With this and other kits for the larger scales, some of which are also offered by Kibri, the accessory manufacturer is now, as the last “house builder”, finally entering the era of laser-cut technology. For Vollmer this is completely new territory, whilst at Kibri there was at least a first attempt, when it was still an independent brand, which probably only a few will remember.



Viessmann shows innovation with its Vollmer brand and presents Z scale enthusiasts with a small workshop (art. no. 49590), which proves to be an extraordinarily good kit.

Also, the option of producing attractive kits that promise more individuality, while avoiding expensive mould costs seemed too tempting. They have this advantage because the production technology is designed for smaller quantities and allows faster model changes.

In the meantime, however, it has become difficult to impress the public with new hardboard kits. After all, this technology began to establish itself already a little more than fifteen years ago! In 2009 we gave our editorial award to a kit from this segment for the first time. Much time has passed since then.

The second generation at the helm of Viessmann also seems to be aware of this. Once having been attached to a very conservative manufacturing and product philosophy for decades, the company is now taking more and more innovative approaches. In 2023, these will finally also reach the Z gauge market, but in this case, they will also bring new products for the larger scales.

In order to create the marketing buzz needed for refreshing the partly old-fashioned brands, something new was needed that goes beyond the exterior design of the buildings. Therefore, and instead of using “simple” hard cardboard, the kits are manufactured with a new composite material.

Viessmann did not disclose any details, but our impression is that the new material makes use of liquid synthetic resins and paints, presumably acrylic-based. We were already able to get a personal impression of the feel of this “Polyplate” material at the Spielwarenmesse (International Toy Fair) in Nürnberg (Nuremberg) earlier this year: A plaster facade on the sample piece felt identical to the original. We have already summarised the most important features in issue 2/2023.



**Before our workshop looks like this, there is still a lot to do. However, Viessmann has done very good preparatory work and has impressively narrowed the weakness it had with respect to the use of laser cut production methods.**

To impregnate a solid-coloured hardboard with a synthetic resin (or comparable material) and thus make it even more dimensionally stable is a good idea, the advantage of which we have now been able to experience and appreciate ourselves during assembly. The fact that the new Vollmer building has a realistic matt surface is in line with expectations - anything else would have been a step backwards.

Two further arguments indicate that Viessman is experimenting with this new material: They test the market with a small kit, the cost of which remains manageable and losses would be bearable even in case of an (unexpected) failure.

Modelling habits do not need to be changed either, because both the preparation of the parts and their assembly with white glue corresponds to other laser cut kits. Viessmann also offers its own Kibri-branded polyplate glue (39997), which we will come back to later.



We start the assembly with the realisation that the kit's designers did not want to leave anything to chance, and wanted to catapult themselves as far as possible into the top field of kit suppliers. Therefore, we hope that the market will vindicate the new approach and that it will lead to more kits being released in the future.

## Before we begin...

Before starting any assembly, the enclosed assembly instructions should be checked for the specificities of individual steps, the sequence of assembly, and markings of the enclosed sheets and parts.

In this context, you should also check whether everything is complete or whether parts might be missing. We did not have a problem in this respect and the well-illustrated instructions, which make do with only little text (German and English), does not contain anything unexpected.

However, some things already stand out that are worth mentioning: The appearance of the solid-coloured hardboard parts will be appealing and realistic, for their absence of any unnatural gloss. Consequently, there is no need to additional paint cover.

The half-timbered wooden framework of the shed is (as with other manufacturers) set on top of the brickwork, which does not correspond to the construction method of the prototype.

However, these parts are deliberately made of such thin cardboard that it will not be noticeable after assembly. And after all, they do not have a load-bearing function in the model.

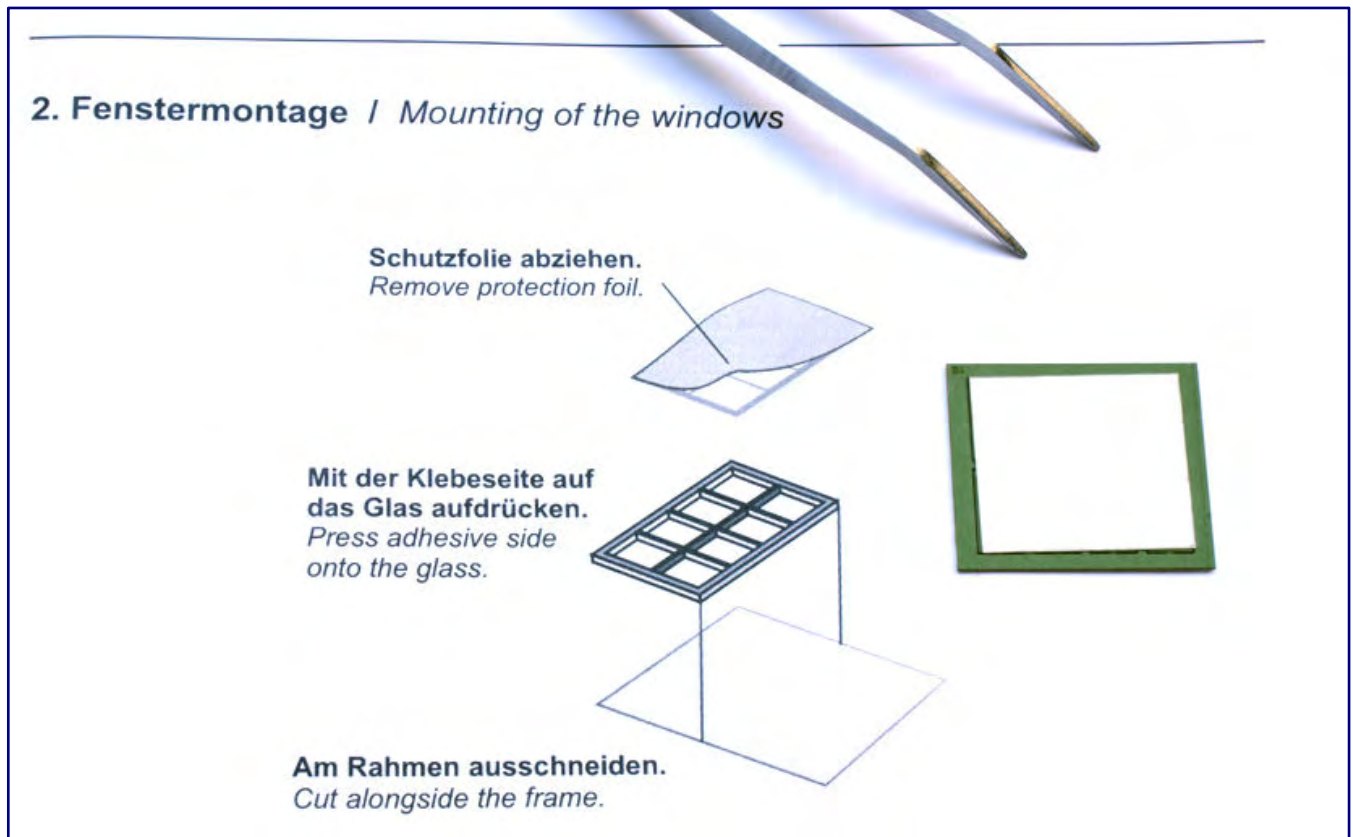
Particularly important are the following features, which make for an enormous difference to the product photo on the packaging: The engravings of the gates and plaster surfaces are considerably better and are on a par with kits from other manufacturers. Obviously, the product picture on the packaging shows a pre-production prototype.

The colours of the kit parts are slightly different: The plaster is grey instead of beige and the wooden beams are black instead of brown. We have no information about the reason for this variation, but the increased neutrality opens up considerably more combination possibilities with the half-timbered buildings already available on the market.



**Before we start assembling, we check the completeness of the parts and familiarise ourselves with the instructions. In the process, we also discover possibilities for adding our own features to the kit.**

And with a footprint of just 4.4 x 3.4 cm, there should be a corner on every layout where this workshop can be integrated. This also seems worthwhile because thanks to the interior division into an office and a large workshop room, it virtually invites one to detail its interior it as well.



The kit comes with some new ideas. One new approach is the use of self-adhesive window panes. We will see how this will perform in practice.

The manufacturer has obviously also thought of this, because the doors can be installed both in closed and open position. Viessmann has even taken into account an integrated lighting option and designed the kit in a way, which leaves room for the in-house floor interior lighting and further facilitates the view into the interior.

This option has also motivated us to make this report on building the small workshop the first article in a series around our annual focus theme "layout details:" After all, many enthusiasts underestimate the positive impact hidden details that reveal itself only at second look can have on viewers.

Therefore, various machines and workshop accessories from Modellbahn Union, as well as office equipment and figures from Trafofuchs, will be included in the build. We are also curious to see how the self-adhesive parts that was included in the Vollmer kit for the windows will work out in practice.

## Finally: the assembly

Not all steps of the enclosed instructions need to be followed in the exact, suggested order. While prepared parts are drying, which are to be processed immediately in the following, elements for later sections can already be prepared.

This can shorten the time for assembly because as work can continue whilst the glue on some components is still setting. In total, the kit will cost us about two hours of work, including the planned interior, but only counting the placement of the necessary parts, and not their assembly and finishing and painting.



The self-adhesive parts for the windows do not work quite as expected. We, therefore, resort to the usual method and glue them in place with Uhu Kraft, which we spread thinly in the window corners with the help of a toothpick.

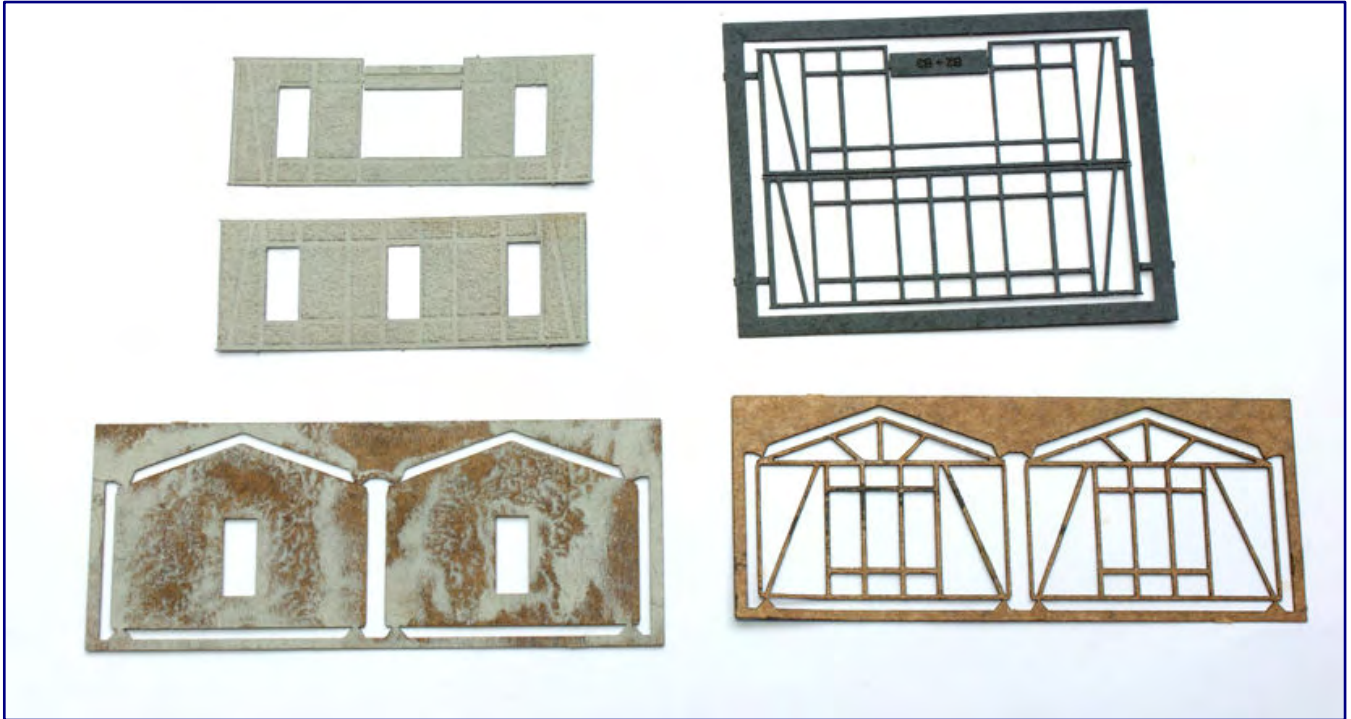
Let's start with the windows of the workshop, which are all the same size and made of a green painted cardboard. The enclosed self-adhesive window sheet is to be cut out along the frame. To do this, we take a sharp craft knife, align the frame and mark the end points of the cut. We then guide the blade along the steel ruler.

This needs to be repeated seven times and the measurement of the sheet seems tight, but it has been perfectly sized with it. Unfortunately, the disappointment comes when peeling off the protective film, as the sticky side underneath does not adhere at all to the window frame.

That's no big deal, because now we reach for the Uhu Kraft and glue the windows the traditional way: A tiny drop of glue applied with the tip of a toothpick in all four corners solves the problem, and maybe even another one. We don't know how Viessmann intended it. But if the glue had not simply evaporated before the frame was put on, it the window could have attracted dust, rendering the panes opaque.



The next step is to clamp the frames into the inner walls from the inside and then fix them to the edge with a little glue. Caution is advised here; any traces of glue should be wiped off immediately with a damp cloth. Afterwards, these wall parts can be placed on the base plate and glued to it and to the adjacent wall.



The plaster and truss parts are colour-treated on one side (photo above), which can only be an advantage for gluing them in place. The identical size of the two sheets is helpful when positioning the timber frame on the plaster wall surface (photo below).

This is where the first waiting time would arise, which we bridge by preparing the exterior walls. The finely engraved, black half-timbering is to be glued onto the parts with plaster reproductions. This seems almost child's play.

Both corresponding parts are pre-cut so that they can be placed congruently on top of each other and are thus, easy to position. All attention can therefore be devoted to the correct application of glue, which is applied in tiny dots to the intersecting struts so that it does not ooze out sideways.



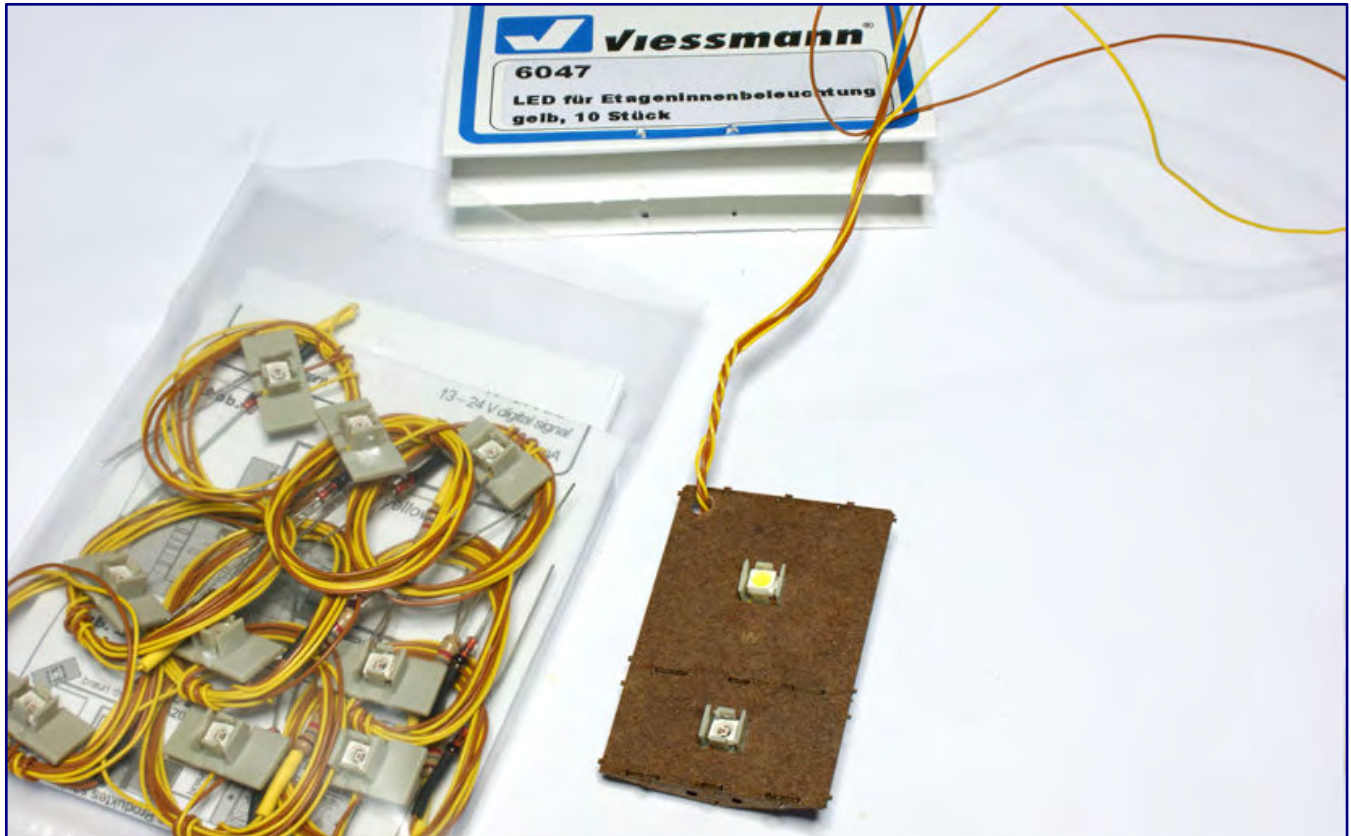
The polyplate glue from Kibri (39997) can be applied to the gluing point in very small quantities because of the fine dosing tip on the bottle and its low viscosity.

By the way, the proprietary Polyplate glue (Kibri 39997) is a great help here: We were not able to detect a difference to ordinary white glue, but its dosing tip, in combination with the high viscosity, proves to be very useful and also prevents the clogging up of drying glue in the bottle.

Once the really attractive outer walls are assembled and glued, another important step is completed. Model building clamps fix the parts to the already finished building core until the glue has set. Finally, the interior wall (with hinged door) separating workshop and office is glued in place.

As mentioned at the beginning, Viessmann has already prepared the kit for interior lighting. Therefore, we now insert the three rafters into the ceiling base plate. While these parts are drying, we already prepare the lighting elements.

The kit is designed to use Viessmann's interior floor lighting LEDs. Two LED elements can be inserted precisely and glued into the gable from the side that will not be visible later. We choose one yellow (Viessmann 6047) and one white (6048) lighting element. The cold white corresponds to the colour temperature emitted by fluorescent tubes and fits perfectly to a workshop. Yellow comes very close to the "sparkly" light of the incandescent lamp era that dominated small offices.



Two different light colours from Viessmann's interior floor lighting range were used for the light in the workshop and office space. The feed-through openings for the wires, diodes and resistors had to be cut out.

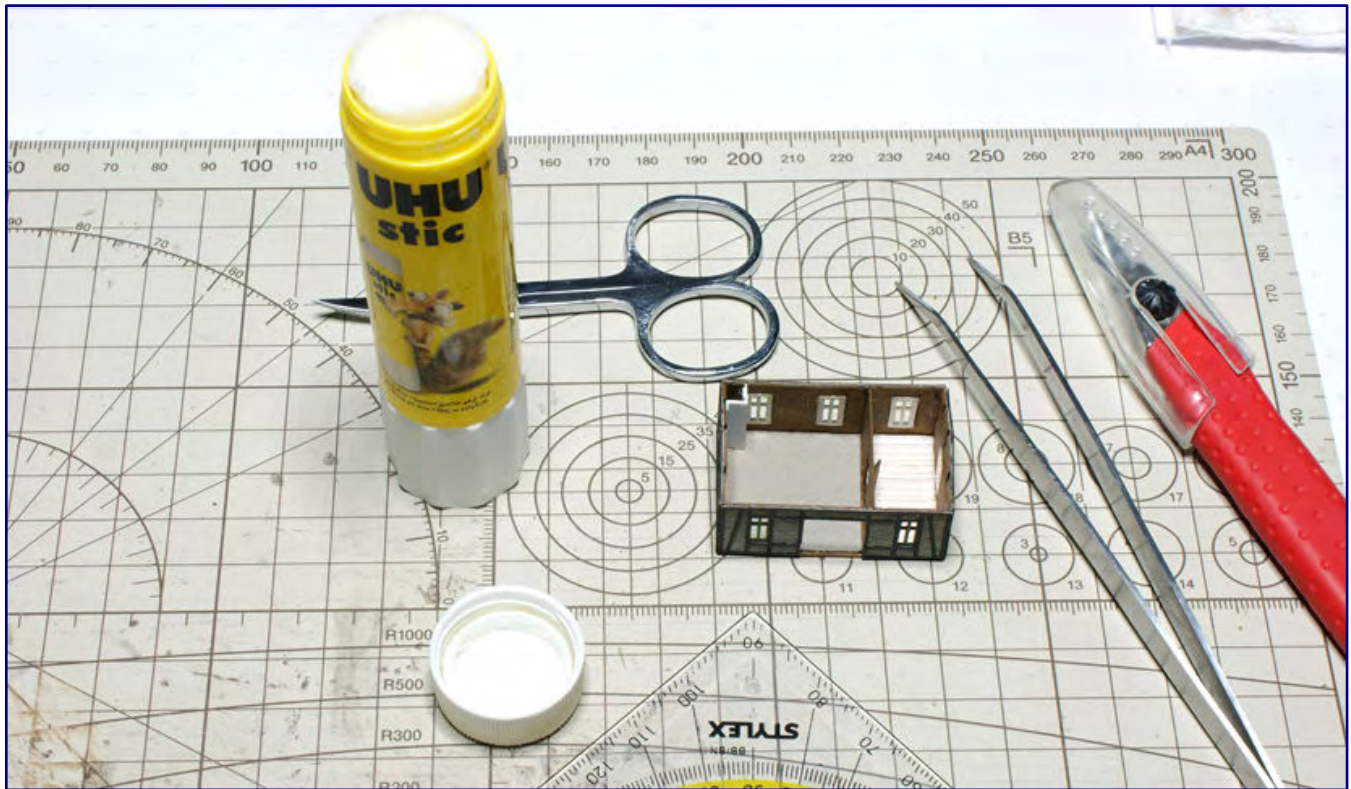
Now it's time for the cables to be routed to underneath the layout. The rafters as well as the ceiling and floor slabs have holes for the cables to pass through, but they are too small. If the diameter in the ceiling and floor is still sufficient for the blocking diode, it and the resistors do not fit through the holes in the rafters.

Consequently, the holes along the wires' routing have to be expanded with a round needle file. This can be done quickly and accurately. In the floor slab it is more difficult because the walls are already in place. Here we take the craft knife and create an almost square opening whilst avoiding to damage the walls.

When the work on the electrical system in the roof area is complete, we can finish the upper part. All that is missing now are the two cardboard parts to be placed on top (still connected in the sheet for production reasons), which rest on the rafters and ensure dimensional stability. The thin roof covering, an imitation of tar paper sheets, is then glued onto them.

By the way: The completed roof does not have to be glued to the building, but can be seamlessly inserted into its walls and easily clamped in place. This allows it to be removed at any time, as long as the wires for the lights are long enough to follow the movement.





Before the interior is fitted out, the floors in the workshop (made from grey photo cardboard) and in the office (wooden floorboards made from a print) get their appropriate surface. A cable shaft made of Evergreen profiles is inserted in the left backhand corner of the building.

Especially at this point the kit is very well thought out and the Vollmer brand currently has a unique selling point here. This construction is virtually predestined to give viewers a look inside! And that is exactly the challenge we will tackle now...

## Important experiences

In Z gauge, windows are usually very small. Nevertheless, the view through the windows is sufficient if light falls from the building to the outside. As long as a house is placed at the edge of the layout and the view is unobstructed, the viewer can capture many angles of the interior with a slight movement of the head and in our imagination a floor plan of the room and its furnishings is created.

The camera can capture this to a limited extent only and for a fixed angle of view. The removable roof facilitates things a lot in this respect and answers questions about the areas that are not visible. Therefore, this workshop deserves furnishings in its interior, which in principle could also include the wall design, although for us these appear sufficiently neutral.

Lockers, machines, workbenches, a fire extinguisher and tools can be found in Modellbahn Union's own range: lockers (MU-Z-A00008), workshop equipment (MU-Z-A00206), lathe and workshop trolley (MU-Z-A00006) as well as a workbench (MU-Z-A00007). They are supplied as 3D printed parts that are quickly coloured. We use Oesling colours and choose reseda green as the base colour.

A trial set-up leads us to the final plan of which unit to place where in the large room so that as much of it as possible is visible without obstructing the visual axes into the interior. A drop of Uhu Kraft and a pair



The master craftsman, journeyman (not in the photo), chair and desk with typewriter were made by Trafofuchs and keep the small workshop bustling.

of tweezers will do the rest, after the workshop floor is created with matt grey paper.

A locker is used in the office, because it serves both master and journeyman as a changing room. What else is missing is supplied to us by Trafofuchs: the two custom-made craftsmen are part of their core competence anyway and are now created there via 3D printing, chair and desk (with typewriter, paper and pen) are made by Birgit Foken-Brock at the same time.

Here, too, it is essential to carry out tests to ensure that as much as possible remains visible once the roof is in place. Soon enough, the decision is made to give the office a wooden floor. Veneer is out of the question, because the floor would then be much higher than in the workshop, and we have left the door open between the two.

In the Viessmann self-printing decor sheets we find a curtain structure that looks very close to a parallel running floor, certainly, at least, under incandescent light. We cut them out to fit and lay them with the help of the Uhu glue stick before furniture and the master move in here.

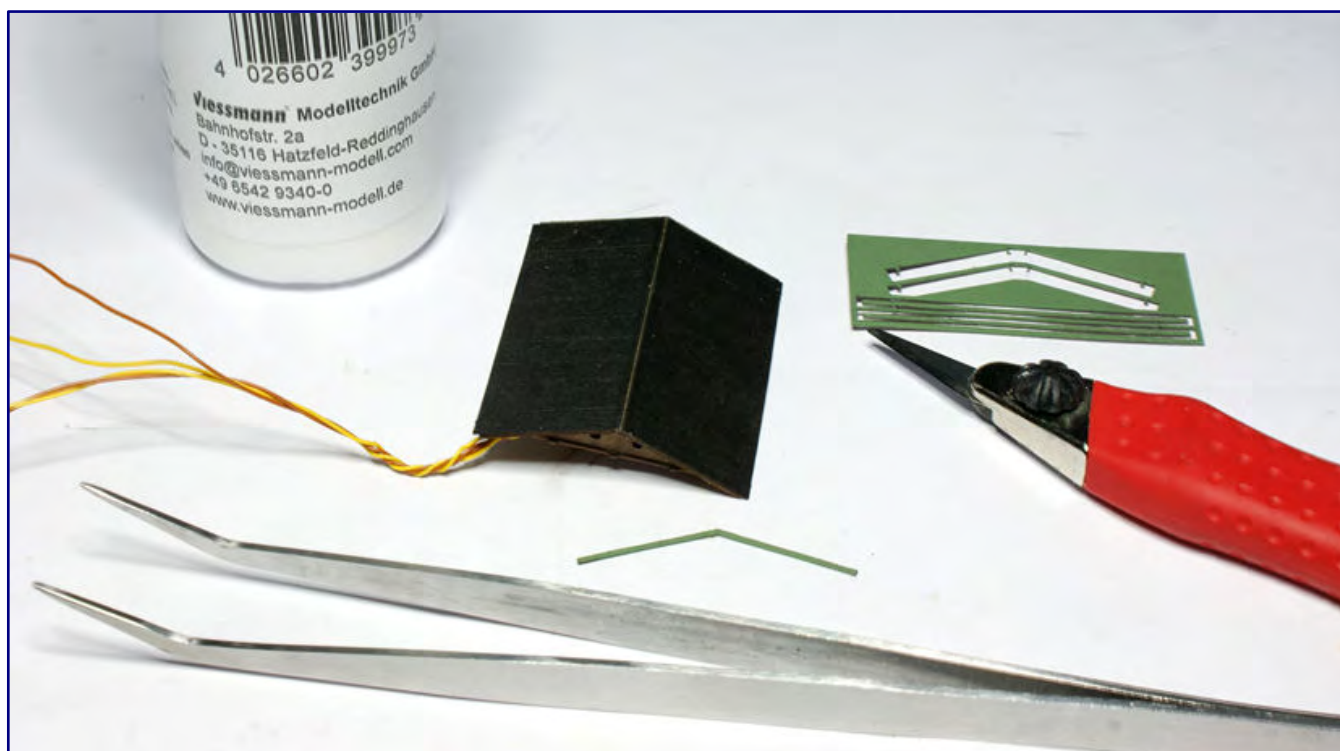
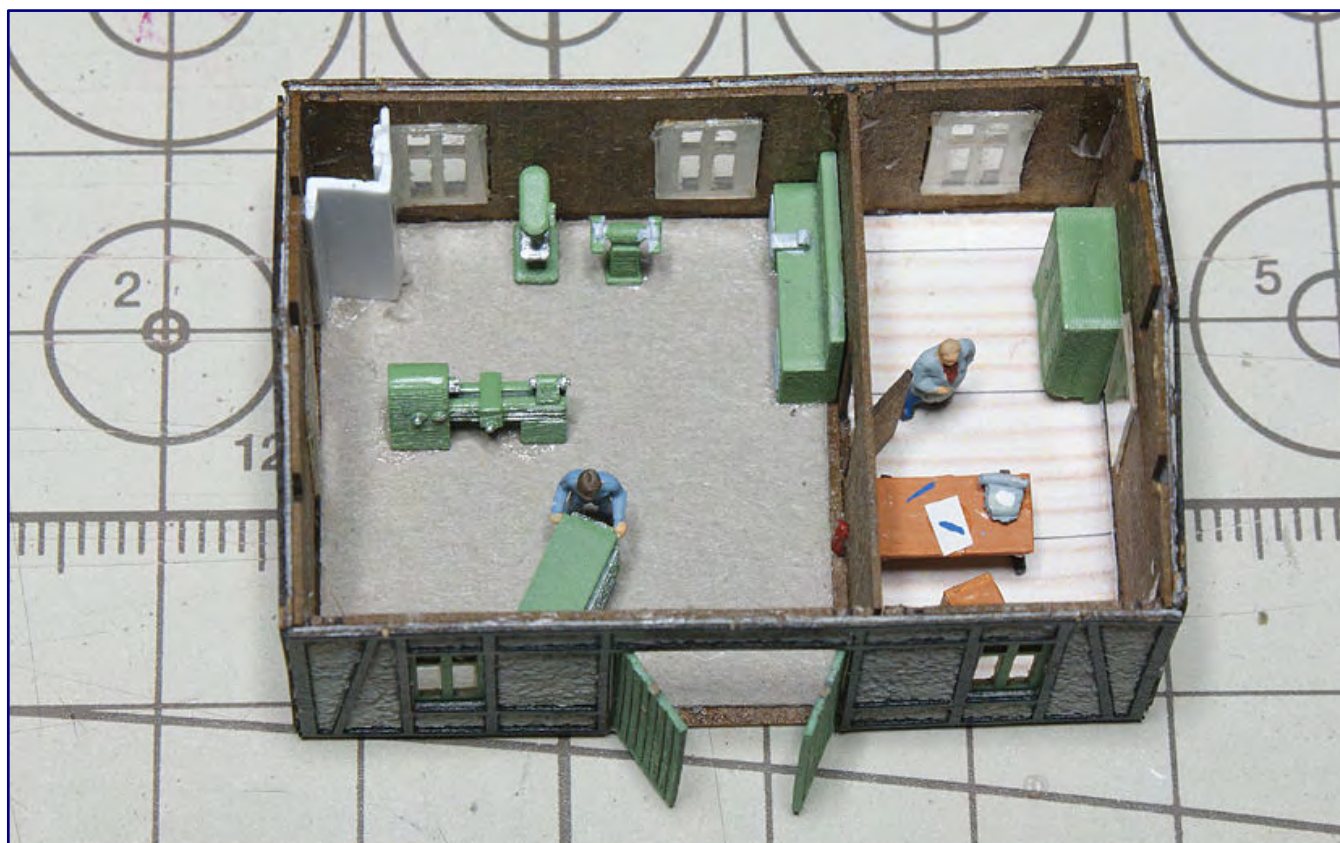
In the workshop we still need to camouflage the yellow and brown wires that lead from the ceiling through to the floor in the corner. They otherwise would be visible through the gate, whose wings we also glued wide open. There is potential for improvement here, which we have indicated to Viessmann / Vollmer.

Two Polyplate parts, which could be cut and added, would make a shaft in the right size, we think of the diameter of the resistors, which would cover the power supply. Unfortunately, such a shaft cannot be built from cardboard scraps, which is why we use white Evergreen profiles made of polystyrene (sold by Faller). They remain untreated in colour. This makes them stand out, but they actually look like cable ducts laid on the plaster.

Our little workshop is almost finished. Almost, because of four parts which we deliberately have left for the end of the article and where the instructions still contain an error. This has also been reported back to the manufacturer and will be corrected in the next printing.

The gables are to be covered with green lath imitations. They can be identified without doubt by comparing them with the product photo on the packaging, but in step 9 of the instructions they have a





Before the roof is put on, there is one last chance to get an overall view of the interior: In the workshop we see drill stands, grindstones, workbench with vice, lathe and workshop trolley from the Modellbahn Union (photo above). The locker has found its place in the office. The last step of the build is to glue the decorative panels to the roof section (photo below), and where the instructions contain an error.

wrong part number that does not appear in the kit at all. However, there is only one sheet left at this point anyway.





The camera can only capture a fixed perspective when looking inside. However, the macro view gives a good impression of the different light colours in the workshop and office, including the figures at work; on the left the journeyman at the workshop trolley, on the right the master craftsman who visits his desk to draw up invoices.

This also contains two other parts to be glued to the side edges of the roofs, as the small building has no gutters (and downpipes). They were completely forgotten in the construction steps. Again, a look at the product photo helps.

### Marginal errors and a conclusion

Making one suggestion for improvement and pointing out two small mistakes very close to the end of this article should not be overemphasized. Viessmann gratefully received our feedback and attached great importance to getting feedback on user experiences.

The release of the kits was originally planned for autumn 2023 and was brought forward by more than half a year. This makes small mistakes comprehensible and at the same time emphasises how important innovation is to the manufacturer.

Viessmann celebrates its 35<sup>th</sup> anniversary as an accessories manufacturer in 2023, and the company's anniversary should certainly be significantly influenced by something that gives new impetus to the market in all scales. And the Polyplate product series will certainly do that. In Z gauge, it also meets the already legendary precision fit of the Vollmer kits. With this, those responsible are now opening a new chapter and we are curious to see how this story will continue.

We recognise the good idea, the almost perfect realisation and our hopes by nominating the small workshop from Vollmer (49590) for our best new releases of the year 2023 award in the accessories category.

We want to explicitly link this with the catchword “layout details” from the headline, our annual focus theme for 2023. It has been a long time since we have had a kit in our hands that cried out as much as this one to be fitted with an interior.



We take one last look at the richly textured and realistic-looking surfaces of the small workshop. The new building is literally made to be lavishly fitted out inside as well and thus draw more attention to it.

For many model railway enthusiasts, this is a peripheral phenomenon, which we have tried to formulate in an understandable way. However, when used and effectively placed on one’s own layout, the tiny building becomes a focal point for viewers, because there is truly bustles with life. And to make sure that this is also noticed, there is also illumination: “Lights on!”

**Kit supplier:**

<https://viessmann-modell.com>

**Built-in accessories:**

<https://www.faller.de>

<https://www.modellbahnunion.com>

<https://www.oeslingmodellbau.com>

<http://www.trafofuchs.de>



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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

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Autor Jan Reiners lebt und arbeitet in Bremen und zeichnet auch für die Vorgängertitel zum vorliegenden Buch verantwortlich. Er interessiert sich seit seiner Jugend für Eisen- und Straßenbahnen und hat insofern einen typischen „Werdegang“ vorzuweisen. Bekannt ist er auch aus einigen Beiträgen in der Zeitschrift „Modelleisenbahner“.

So lag es aus der Historie der Transpress-Reihen wie auch allein aus seiner fachlichen Kompetenz heraus nahe, ihn für dieses Buch als Autor heranzuziehen. Zu verwundern wusste zunächst allein der Erscheinungstermin seines neuen Werkes.

Gedacht ist es als Nachfolger des Typenkompass „Loks der DB AG“, der zuvor in mehreren Auflagen erschien. Kompakt und in jede Jackentasche passend, übersichtlich und fürs schnelle Nachschlagen geeignet, erschien dieser 2010 und 2021. Beide Auflagen haben wir in unserem Magazin vorgestellt, die letzte Besprechung liegt fast exakt zwei Jahre zurück.

Bei zuletzt elf Jahren Abstand zwischen den Aktualisierungen überrascht daher, dass es dieses Mal so schnell gehen würde. Dies wollen wir im Folgenden erläutern.

Ein Grund besteht sicher darin, dass das Format geändert worden und in eine neue Buchreihe integriert worden ist. Das kommt den Inhalten auch zu Gute, denn die Fotos werden dadurch größer abgebildet und liefern dem Leser mehr Informationen.



Gegenüber der letzten Kompass-Auflage wurden aber nur wenige Motive getauscht, eine bessere Qualität oder aktuellere Darstellung scheint in solchen Fällen den Ausschlag gegeben zu haben. Die Reproduktion der Aufnahmen ist fast ohne Ausnahmen gut gelungen und in seinen Helligkeitswerten, Sättigungen wie auch Kontrasten auf das verwendete Papier abgestimmt worden.

Unverändert geblieben ist die Konzeption, zu jeder Baureihe (oder Baureihenfamilie) mindestens ein Foto, eine Tabelle mit den wichtigsten Kenndaten und ein Kurzportrait der Lok mit ihrer Geschichte und Technik auf ein bis zwei Seiten zu präsentieren.

Das dürfte der Mehrheit an Vorbildfreunden und Modellbahnern für einen ersten Überblick reichen. Das Buch ist quasi ein Fachlexikon der Lokomotiven und Triebwagen, die bei der Deutschen Bahn AG seit ihrer Gründung zum 1. Januar 1994 im Einsatz sind oder waren. Dass auch Triebwagen und -züge zum Buchumfang gehören, war bei den früher verwendeten Titeln nicht gleich zu erkennen.

Der Zwischenstand unserer Besprechung bis zu dieser Stelle lässt aber noch nicht erkennen, warum dieses Werk empfehlenswert ist und wer als dessen Käufer überhaupt in Frage kommt. Dies wird nun klar, wenn wir den Grund des so kurzen Erneuerungsintervalls anschauen.

Der Bestand an Fahrzeugen, den die DB heute einsetzt, befindet sich unverändert in einem starken wie auch schnellen Wandel. Alte Fahrzeuge, die noch von der Bundes- oder Reichsbahn übernommen wurden, sind bis auf wenige Ausnahmen inzwischen ausgemustert.

Beschleunigt haben diesen Prozess die Ausschreibungen von ÖPNV-Leistungen, die häufig Neufahrzeuge verlangen, Experimente mit Hybrid- oder Alternativantrieben und zuletzt auch die politischen Vorgaben, mittelgroße Städte wieder ans Fernverkehrsnetz anzubinden.

Dadurch finden wir beispielsweise die Baureihe 563 („Mireo Plus H“) hier schon wieder, die als Wasserstofftriebzug erst im letzten Jahr in Dienst gestellt worden ist. Gegenüber dem letzten Typenkompass wiederaufgenommen wurden die Schmalspurdampflokomotiven aus der Baureihe 099, die 1994 für kurze Zeit in den Bestand der privatisierten Staatsbahn gelangten.

Sie waren zuletzt aus Platzgründen entfallen, die höhere Seitenzahl von 144 statt 128 hat ihre Rückkehr trotz der „Neulinge“ im Buch ermöglicht. Nicht enthalten sind unverändert – und vom Autor im Vorwort explizit auch so erwähnt – Leihlokomotiven oder DB-Maschinen, die bei Auslandstöchtern im Dienst stehen und teilweise auch Deutschland erreichen.

Ebenso fehlen in diesem Buch alle Bahndienstfahrzeuge, die für den Unterhalt der Strecken oder für Messfahrten bestimmt sind. Auch das entspricht dem gewohnten Inhalt und ist nicht neu, sollte aber erwähnt werden.

Ein wenig überraschend ist es schließlich, denn die Zahl solcher Spezialfahrzeuge ist ja eher gering und lohnt sicher kein eigenes Buch. Konzeptionell reiht es sich so aber lückenlos und ohne Widerspruch an die weiteren Bände dieser neuen Reihe an, die als Nachschlagewerk für daheim gedacht sind und dank des festen Einbands schnell aus dem Regal gezogen werden können.

Und so empfiehlt sich dieses Werk für alle diejenigen, die mit dieser Reihe eine neue Enzyklopädie an Nachschlagewerken begonnen haben oder beginnen wollen, sowie alle Leser, die ein möglichst vollständiges wie auch aktuelles Verzeichnis wünschen. Der Redaktionsschluss liegt nach unserem Eindruck sehr zeitnah zum Dezember 2022.

## Preiser-Geschichtsdarstellung **Figuren mit Emotionen**

***Bücher aus dem Haus Preiser sind selten. Erst zum zweiten Mal veröffentlicht der Figurenproduzent aus der Nähe von Rothenburg ob der Tauber ein eigenes Druckwerk dieser Art. Mit Blick auf die lange Geschichte des Hauses und das breite Programm über viele Spurweiten der Modellbahn haben wir uns den bunten Bildband im Nachgang zu einem runden Firmenjubiläum genau angeschaut.***

Kleinkunst-Werkstätten Paul M. Preiser GmbH (Hrsg.)  
Preiserfiguren – zum Verlieben schön

Kleinkunst-Werkstätten Paul M. Preiser GmbH  
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Das vorliegende Werk ist das zweite Buch aus dem Eigenverlag des Figurenherstellers Preiser. Dieser verspricht ein reichhaltig illustriertes Stück Literatur mit einem Feuerwerk an Impressionen zu den Höhepunkten aus 70 Jahren Firmengeschichte, die 2019 vollendet worden sind.

Dem Käufer soll einen Einblick in die Geschichte, die Arbeit und die Philosophie des Unternehmens erhalten. Mit Blick auf die Bezugsmöglichkeiten und die eigene Darstellung der Inhalte verstehen wir Modellbahner aller Baugrößen als Zielgruppe dieses Buches.

Wohl mehr als 90 % des Textes, den das vorliegende Buch enthält, begegnen wir gleich im ersten Kapitel. Um keine Missverständnisse aufkommen zu lassen, ergänzen wir, dass diese Zahl sich nicht auf die Menge an Seiten bezieht! Der Einstieg mit der zusammengefassten der Firmengeschichte ist – neben dem Vorwort - das einzige, das auf geschriebenen Informationen statt Fotos aufbaut. Daraus erklärt sich die sehr ungleiche Verteilung.

Im Vergleich zu den folgenden fünf Kapiteln ist es sehr ausführlich, hinterlässt beim Leser aber auch viele Fragen. In der abschließenden Tabelle wichtiger Meilensteine der Firmengeschichte ist auch die Übernahme der Marke Merten im Jahr 1996 hinterlegt. Im ausführlichen Portrait fehlt diese jedoch völlig, was die wohl größte Informationslücke ist: Hintergrund der Übernahme, deren Ziel und Auswirkungen auf die Firmenpolitik bleiben unklar.

Ebenfalls lückenhaft bleiben die Informationen im Rahmen der Generationenfolge: Der Leser erfährt viel über die familiären Hintergründe von Paul M. Preiser und die Ursprünge der Firma, die schon früh in den Kunststoffspritzguss einstieg. Horst Preiser übernahm nach dem plötzlichen Tod die Geschäftsführung und entwickelte das Unternehmen in seinem Sinne weiter. Er galt als kreativ und auch als guter Modellbauer.





Heute ist mit seinen beiden Söhnen die dritte Preiser-Generation in der Verantwortung. Ob und was sie anders machen wollen als ihr Vater sowie welche Entwicklung das Unternehmen nehmen soll, bleibt offen.

Wird der Wechsel der Figurenbemalung von Indien nach Mauritius noch erläutert, fehlt Vergleichbares für die zweite Produktionsstätte Madagaskar. Auch Elastolin mit dem eingebrachten Standort spielt im Buch keine Rolle. Hier kommt beinahe der Eindruck auf, dem Autor sei die Lust am Schreiben in diesem doch sehr interessanten und wichtigen Kapitel vergangen.

Besonders spannend ist auch das Entstehen einer Figur – zweites Kapitel des Buches. Sechs Seiten, eine ausreichende Zahl an Fotos und nur wenig Text sind dafür verwendet worden. Für einen nicht fachkundigen Kunden bleibt hier leider offen, wie der Verkleinerungsprozess vom Urmodell im Maßstab 1:10 auf den Zielmaßstab funktioniert.

Unvorstellbar ist, dass CAD-basierte Konstruktionen bis heute kein Thema sind. Genau das legen die einleitenden Ausführungen des Buches zusammen mit den Fotos dieses Kapitels aber nahe, denn Preiser sieht hier offenbar ein Alleinstellungsmerkmal.

Das folgende Kapitel ist den Preiser-Produkten im Laufe der Jahre gewidmet, die Kriterien, nach denen die Auswahl getroffen wurde, bleibt unklar: Eine strenge Chronologie ist nicht erkennbar, ebenso wenig bildet die Auswahl alle Epochen der Firmengeschichte ab.

Figuren der Spurweiten 1, TT, N und Z sowie der außerhalb der Modellbahn üblichen Maßstäbe fehlen vollständig – nur beim Vorwort zeigen sich alle Maßstäbe mit je einer Figur vergleichend abgebildet. Zusatzinformationen zum Sortiment, wie blaue und schwarze Figurenserien in der Spur H0, wären hier hilfreich. Stattdessen sind gewisse Themenschwerpunkte erkennbar.

Schön anzusehen sind die künstlerischen Fotos mit Preiser-Figuren im vierten Kapitel. Kreativ zeigen sie eine weitere Einsatzoptionen auf, beschränken sich aber fast vollkommen auf den Maßstab 1:22,5. Einen Zusatzwert hätte hier das Einbinden von Aufnahmen fremder Arbeiten wie die des Künstlers Volker Kühn sein können, denn so hätte sich die Bandbreite an Kreativität noch weiter präsentiert.

Markus Tiedtke als bekannter Modellbahnfotograf kommt im fünften Kapitel zu Ehren. Dort werden einige der schönsten Preiser-Dioramen ausdrucksstark gezeigt. Viele sind von Messen bekannt und dürften auf Horst Preiser zurückgehen, Werke der letzten Jahre sind uns hier nicht aufgefallen. Gesamtansichten der Dioramen vermissen wir.

Zum Abschluss folgen Titelseiten von Katalogen und Prospekten seit 1949. Sie zeigen in anschaulicher Weise, wie sich der Geschmack im Laufe der Zeit verändert hat und mit ihm sicher auch das Programm. Mangels begleitender Erklärungen wird nicht klar, ob es sich um eine vollständige Auflistung handelt oder sie nur mehrheitlich, aber willkürlich abgedruckt worden sind.

Zusammengefasst liegt ein bildgewaltiges und emotionsgeladenes Buch guter Wiedergabequalität vor uns. Es unterstreicht damit die Rolle, die Figuren auf einer Modellbahn spielen – und das übrigens zu einem als günstig zu bewertenden Preis.

Das Durchblättern bereitet Freude, doch allzu viele Informationen darf der Käufer leider nicht erwarten. Als großer Schwachpunkt bleiben die vielen Informationslücken, weshalb die wenigen Texte, deren Urheber nicht erkennbar ist, laienhaft wirken. Und genau das wird dem Werk eigentlich nicht gerecht.

An dieser Stelle bleibt neben dem repräsentativen Einbinden aller produzierten Maßstäbe Luft nach oben für künftige Bände, die laut Vorwort zu erwarten sind. So hoffen wir, dass ein erfahrener Autor zumindest beratend hinzugezogen wird und die aufgezeigten Schwächen dann behoben werden.

**Publishing pages:**  
<https://www.preiserfiguren.de>

Faszination Modellbahn 2023

## Second Attempt in Mannheim

*Slowly, trade fair life is getting back into gear after the pandemic, although the joy is also countered by a lingering concern for one's own health. The Spring Fair in Mannheim was in particular focus, because after four years since its first edition there, it first has to establish itself sustainably.*

By Stephan Fuchs and Holger Späing. From 10 to 12 March 2023, Faszination Modellbahn was finally able to open its doors again after an enforced break due to the pandemic. The venue was the Maimarkthalle in Mannheim for the second time after 2019.

After criticism about inadequate lighting and poor air supply, there was eager curiosity as to whether the hall operator would be able to address these problems at the new start. And indeed, the lighting concept had been worked on intensively — modern light-emitting diode technology made it possible.



A lot had been done in the Maimarkthalle to adapt it to the requirements of a model railway exhibition. So, the first edition after the pandemic can probably also be described as successful.

In the opinion of our local representative, the ventilation could be further improved, because the hall smelled of many people. Good ventilation also seems important to us because Corona infections have disappeared from our consciousness, but not from our lives.

Only an estimated 3 % of visitors preferred to voluntarily wear an infection protection mask in the hall to avoid the risk of taking home an unwanted “souvenir”. The health-promoting distances, however, were met with what we would like to benevolently describe as an escape route concept already known from Friedrichshafen: The aisles between the stands were conspicuously wide.



Even though there were undoubtedly around 20 times as many model railway manufacturers present as at the Spielwarenmesse, which was really a washout in this respect, this speaks volumes. Apparently, the interest in exhibiting at exhibitions has clearly declined among various suppliers. When asked by absentees, we often hear that the increased stand costs could no longer be recovered, and this does not only apply to this exhibition.

The very, very wide aisles, unfortunately, also quickly give the impression that the exhibition was poorly attended. This is also due to the fact that the number of people present noticeably decreased from 15:00 onwards. Only on Sunday did the flow of visitors drop only slightly until the official closing of the fair.



In the Miba private plant competition, Heinz-Ulrich Grumpe was the winner with the "Harzer Baryt-Industrie mine railway" (gauge H0e).

In this respect, we consider the 15,000 visitors mentioned in the press release at the end of the fair to be a credible figure. And these people clearly enjoyed what was on offer. However, we did not discover anything new or previously unknown for Z gauge.

The list of exhibitors and layouts was broad: 17 model railway layouts were on display, with exhibitors from Germany, France and the Netherlands. Many gauges were represented and these offered a surprising variety of themes from the tin age to the digital age.

The Miba private layout competition was also held again as a tradition. This seems particularly worth mentioning to us because this magazine celebrates its 75th anniversary in 2023. Our congratulations therefore go especially to Editor-in-Chief Martin Knaden, who represents this traditional magazine.

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Our author's favourite was the third-placed layout "In Provence around 1925" by Ronalf Kramer (photo above). The small layout "Altbach 1963" by Ronald Heijne (photo below), which had travelled from the Netherlands to Mannheim, was particularly impressive.



Eight layouts competed for the favour of the expert jury. The winner was Heinz-Ulrich Grumpe with his "Grubenbahn der Harzer Baryt-Industrie" (H0e gauge). All vehicles and buildings were discreetly aged with powder paints and pigments and the figures were painted themselves. Our local representative's personal favourite was the layout that made it to third place, because everything there looked "as it might once have actually been".

We almost overlooked the small gauge H0 layout "Altbach 1963" by Ronald Heijne. That would have been unfortunate, because it was particularly convincing in terms of the model construction on display. It was one of the absolute highlights of this exhibition. Many other layouts, on the other hand, had long been known, photographed, and no longer offered any discoveries.



"Saturday Afternoon" is the title Ulrich Günther gave to his arrangement of a metropolitan railway station, the city centre and Neuschwanstein Castle.

This risk certainly did not exist to the same extent at the ZFI stand. Here, too, we met "permanent exhibitors" such as Gerhard Maurer with his "Traumschleife" or "Dead End City" and "Nugget Gulch" by Michael Bernhard. The Railex sale by Christine Specht and Roland Kimmich is also part of the familiar, but also appreciated part.

However, the focus here was much more on the fascination of the tiny, advice and information. For example, we would like to mention the self-driving buses that Oliver Kessler demonstrated and that many friends of larger gauges could not have imagined.

The on-site expertise was also appreciated by manufacturer Märklin. It was explicitly appreciated, people with special gauge Z questions were gladly sent over, not only apart from the in-house products. This led to further visits as well as discussions and, as was to be expected, was very well received.

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Looking at the two motifs on the deep-sea port layout presented by Gotthardt Schmitt, the viewer quickly realises which port and which city provided the necessary inspiration here.



Thomas Heß certainly belongs in this category with his digital demonstration layout, which in the meantime has also been landscaped. Here, visitors to the stand who are unfamiliar with the subject can see impressively that the latest technology has long been compatible with the small models of our nominal size, even if Märklin itself does not offer anything of the kind.

Due to illness, Jürgen Walther, whose “Hirschsprung” layout in the Höllental valley would have been worth a longer journey, had to dismantle and unfortunately leave. We wish him a speedy recovery and hope to see him again soon at one of the next exhibitions.

A deep-sea harbour layout celebrated its premiere at the stand. It was refurbished by Gotthardt Schmitt and presented on site. Thematically, too, it offered plenty of variety from the usual and widespread model railway themes. This is probably why it was one of the exhibits that was explicitly mentioned in the press information before the start of the fair.



Claudius Veit's chemical plant provided variety and special accents, and Gerhard Maurer's “dream loop” can be seen behind it on the right.

Claudius Veit's chemical plant is no exception. An industrial motif of this kind and abundance can hardly be credibly realised on a larger scale and offers, in addition to the unknown, quite unique charms. Nor can it be said that this layout, which is still being worked on, is already widely known.

“Saturday Afternoon” was the motto of Ulrich Günther, who had also modified his module “Neuschwanstein Castle” during the exhibition break. No one can get past one of Germany's most famous sights, even in a model, and, consequently, also a crowd puller. The smallest guests, on the other hand, were again entertained with the “dwarf train”, on which things were certainly not as serious as with the “grown-ups”.

For the children, however, there were also other playgrounds at Märklin and Piko, but they were also enthusiastic about the layouts on display, which makes us confident. However, we still have to criticise the fact that many layouts were still so high that children and wheelchair users cannot see what is presented. The increasingly popular footsteps sometimes provide a remedy, but this does not apply to disabled people – accessibility remains a permanent task for the majority.



Z gauge seemed to be pushed to the side somewhat uncharitably by Märklin, which was represented there with a new trade fair layout and a model showcase.

Hagen von Ortloff once again proved to be a very patient contemporary. Especially the “older generation” wallowed in nostalgia at his tin tracks and vehicles.

But his exhibits were also very interesting for children, who were surprised to discover that grandpa “must have played with something like this”.

The largest stand went to market leader Märklin. We don't want to say anything special about it, because it was, as usual, displaying layouts and showcases with new products for various gauges.

This can be described as unspectacular and surprising, because almost every trade fair presentation follows the same concept. There was no special car at Faszination Modellbahn.

With this end of our tour of the fair, it is time to draw a final conclusion: The revival of the exhibition at its new location seems to have been successful.

Mannheim has established itself as the venue for this exhibition. All in all, however, the painful impression remains that interest in model railways seems to have declined again.

Let us all hope that this is due to the current rather gloomy economic outlook and lingering health worries, i.e. that it will remain temporary. For all of us readers, helpers and editors know that model railways are the most beautiful and creative hobby in the world!

All photos: Stephan Fuchs

- Selected exhibitors with gauge Z relevance:**
- <https://www.eisenbahn-kurier.de>
  - <http://www.mazero.de>
  - <https://www.noch.de>
  - <http://www.peter-post-werkzeuge.de>
  - <https://www.rokuhan.de>
  - <https://viessmann-modell.com>
  - <https://www.vgbahn.shop>
  - <https://www.z-freunde-international.de>
  - <https://z-stammtisch-bayern.de>

## Reboot of On traXS! 2023 in Utrecht **Model Making at the highest Level**

*The trade fair business is slowly picking up speed again, but many organisers are still struggling. Attracting commercial exhibitors and ensuring a strong flow of visitors is not a foregone conclusion. The On traXS! in Utrecht, the Netherlands, which opened its doors in mid-March, had a completely different effect and, with its tried and tested concept, met with a great response and a lot of enthusiasm.*

Also, the “On traXS!” (its own spelling) in Utrecht also faced a new start this year after a forced break. We were pleased to see that the organising railway museum had resumed its cooperation with the model building collective Mitropa, a decision that undoubtedly benefits this exhibition.

In the last editions before the Corona interruption, the organiser withdrew the already almost traditional name more and more and placed the name “Modeltrein Expo” in the foreground. This means no more than “Model Railway Exhibition” and was then no more than that.



We are welcomed at the Spoorwegmuseum in Utrecht as if at a railway station for On traXS! 2023. The square in front of the historic building fills up visibly until the exhibition finally opens its doors.

The special attraction of the event in the railway museum of Utrecht had always been to be something special and for a long time also unique among the many model railway shows in Europe. Team Mitropa always attached great importance to the quality shown and had defined binding criteria for the presentation.



Above all, this included separate and uniform lighting for each exhibit and the preferred set-up in a peep-box. This did justice to the special lighting conditions at each location in the museum and also protected against disturbing light from outside. Mitropa also always applied the highest standards to the theme and landscaping.



The traditional exhibition concept of layouts and stands placed around the historic vehicles has proven its worth. So, it is always crowded here, but there is never a feeling of confinement, the photo was taken a few minutes after the ticket offices opened before the rush of visitors.

Because there are not many layouts that can meet these high demands, the selectors do not work on the basis of applications, but travel (unrecognised) through many exhibitions and select those layouts that are newly presented and meet the high exhibition requirements. Mostly unexpectedly, an invitation to On traXS! arrives at the plant owner's door, which he or she will certainly not turn down.

For reasons unknown to us, this concept was changed a few years ago when the name was changed and the cooperation with the Mitropa model building collective was suspended. Now model railway enthusiasts could freely apply to "Het Spoorwegmuseum" to participate.

The glamour of the special was thus abruptly gone, the quality of what was shown immediately and noticeably decreased. Permanent visitors to the exhibition who had internalised the previous concept and consistently built according to these standards now refused to participate. Other model railway enthusiasts saw their chance had come to participate in an exhibition with (supposed) world fame and to enhance their own work.

This was the low point in over ten successful years, the event had become interchangeable and was no longer worth travelling far from abroad. However, before it had come to this, our small scale had also reached the required level with the Gauge Z layout "Kniephaven" by Dirk Kuhlman.

Consequently, it was possible and also desirable to be discovered for On traXS! From 17 to 19 March 2023, the popular exhibition finally took place again, and at the same time with its old concept with Mitropa integration.

All visitors can now once again see in person, year after year, how model making is developing at the very highest level. And for the second time, a Z Gauge layout has also successfully passed the strict selection process.

The “Boisschot Statie” layout by Pieter Willems and Jan van Casteren, which was already on display in Belgium and Altenbeken in 2022, was on show in Utrecht. It received a good response throughout, and our editorial team was also convinced and awarded it the 2023 Prize of Honour as the best exhibit of the past year.



Thanks to the low afternoon sun, the lighting mood of an approaching thunderstorm could be conjured up on the small but fine layout based on the Belgian model.

This unique layout was built after a historical station from Belgium. The station still exists today, but here it is shown in the historically different condition of the sixties. This makes it possible to run steam locomotives in addition to the diesel traction in the form of the SNCB round noses.

As the range of models based on Belgian prototypes is extremely limited, this also allows the greatest possible choice, which at the same time ensures a varied operation for visitors.

By the way, Jan van Casteren was responsible for the buildings that are presented next to the tracks. Nothing is “off the shelf” here, creativity and skill were required, instead. The two exhibitors were able to show us a large selection of historical documents on which they based their work.

In addition to the craftsmanship, this makes the layout even more interesting. This was also the case for the many guests who inspected the many details and even took out their cameras. No one could complain about a lack of visitors; rather, they had to wait until they got a place in the front row, where they could enjoy an unobstructed view.





The layout is named "Boisschot Statie" after the depicted station. Apart from "round noses," steam traction still dominates here – we are in epoch III with the SNCB.

We will share our impressions with our readers in a new episode 12 of **Trainini TV**. In it, Pieter Willems will tell us how he received the invitation to On traXS! and give some background information about the layout. Other exhibits of the exhibition and the regular museum operations will also be portrayed.

## Small tour

Of course, the journey to Utrecht was not only worthwhile because of 1:220 scale. The other gauges were also represented with great works, none of which made an exception. We can therefore only show a small selection here, but it is deliberately intended to differ from the established film sequence in order to give a more colourful photo together.

We have tried to capture a selection that is as inclusive as possible of all the nations represented. In this way, we also want to show how much a common hobby connects us across borders in the sense of the European community idea.

From Belgium came a new layout that was in great demand and dedicated to HSB motifs from the Upper Harz region with the theme "Hasselfelde". Arnold van Waes was responsible for this realisation in H0m gauge and we will certainly see it in German trade magazines soon.

A popular motif of the Utrecht presentations are always light railways in different scales. For example, we found what we were looking for at the Escadrille St. Michel (ESM) from France, for which Didier Mozer, among others, exhibited. A regular guest in Utrecht is Marcel Ackle (Switzerland), who showed his current exhibition layout "Feldbahn kreuzt!".

Leszek Lewinski from Poland was on the road in nominal size H0, albeit narrow gauge, and presented his layout Kwyzdyn. "Cold Water" offered varied contrasts. This work was dedicated to the United States of America and had its origins in the Netherlands with Gerrit Schoemaker, so in a way, a home game.





Among the eye-catchers at the On traXS! 2023 also included "Hasselfelde" by Arnold van Waes, which presented Harz HSB motifs in H0m scale. This layout is currently in great demand for reports in print magazines.

At this point we would like to dedicate an insertion to the commercial exhibitors. Many suppliers with high-quality products from the small series were represented here. For our scale, we mention Microrama from France or Artitec from the Netherlands as examples.

Even some model railway and larger accessory manufacturers were on site. In most cases, however, they were represented by third parties, presumably larger distributors or dealers. Particularly interesting for the inexperienced with a penchant for getting into the hobby were demonstrations for the visitors: before their eyes, a layout was being worked on in the large locomotive hall, the landscape of which was yet to be created.

Let's now take a leap into the Ore Mountains. There was also a layout from there whose motifs were practically on the doorstep. Jens Petermann ("De Bimmlbahner") demonstrated the Wolf connection, which can be found on Saxony's 750 mm narrow-gauge railways.

On the other hand, his stand neighbour Sebastian Schmid took us to the Selketalbahn with his layout "Strassberg." For many years, this station was the terminus of the line coming from Gernrode. The barite from a nearby mine was loaded here.

One of the longest journeys to Utrecht was certainly made by the "Cortesella" layout from Turin (Italy), which was considerably large at 5.50 x 1.50 metres. Andrea Giolo (Arcamodellismo Torino) presented here a journey on the Piedmont branch lines of the seventies. It had no exact model and was created from imagination, but technical drawings were used. This was to the benefit of the many self-built trains shown.

As a summary of our walk through the museum and the entire exhibition, we can say that no journey could have been too long to take part in this edition of On traXS! We are very pleased that it has successfully ventured a new beginning and returned to its roots.

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“Caution, light railway crosses” was the motto of Marcel Ackle (photo above), while Andrea Giolo was inspired by motifs from his homeland in Piedmont for his “Cortesella” layout (photo below).



We believe that the field is well prepared for the next few years and can only encourage our readers to use all their skills and to take the requirements for presentation form and illumination to heart, so that our scale can also earn a permanent place there.



“De Bimmlbahner” were represented by Jens Petermann in Utrecht. He demonstrated the very successful Anschluss Wolf from his home in the Erzgebirge.

Exactly then, this great exhibition will certainly develop even more attraction on our scale and become a permanent meeting. It is worth it, because where else do so many European nations meet harmoniously and enthusiastically in the best exchange?  
isch und begeistert aufeinander?



- **Commercial exhibitors with gauge Z relevance:**
  - <https://artitec.nl>
  - <https://microrama.eu>
  - <https://scenery.shop>
- **Selected exhibitors from this article:**
  - <https://www.arcamodellismo.eu/cortesella.html>
  - <https://www.debimmlbahner.de>
  - <https://www.feldbahn-modellbau.ch>
- **Report from Trainini TV:**
  - <https://www.youtube.com/TraininiTV>



# SONDERAUSSTELLUNG KLEIN TRIFFT GROSS

50 Jahre Baugröße Z, 1:220  
19. März 2023 bis 17. März 2024



**märklin**



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78713 Schramberg





Readers' letters and messages

## Zetties and Trainini in Dialogue

*Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.*

### No connection under this number?

In one of the Trainini issues last year, you had mentioned the company CRZ (...) as a manufacturer for Z-gauge decoders. I also bought and installed their decoder in mid-2022 for the BR 132 with bell-shaped armature motor. Everything is fine, the model runs very well.

Now, I have been trying for some time to get in touch with CRZ again about a decoder for another model, but in vain. Contact requests via the portal remain unanswered, and I only ever get a voicemail on the phone. Can you tell me if this manufacturer still exists?

Peter Taubert, Schwerin

Editor's reply: Unfortunately, we have heard news like this several times. To our knowledge, the provider is still active, and we have no information to the contrary. Consequently, we cannot explain or resolve the situation described.

### Error on the Herpa aircraft model?

The Douglas DC-6 N6523C "Pan American" by Herpa (Trainini 2/2023) has the flag of the United States of North America mirror-inverted on the tail unit.

Uwe Pfannenschmidt, Schluchsee



The mirror-inverted USA flag on the tail of the Douglas DC-6B from Pan Am caused irritation, but corresponds to the general prototype.

Editor's reply: This phenomenon has already been seen on other Pan Am aircraft. Up to now, we have had no reliable evidence as to whether this corresponds to the prototype or is to be regarded as an error. In the course of correspondence with our reader, he was able to provide us with pictures of the prototype which document this mirror-inverted representation on the right side of the vertical stabiliser.

**Many AZL deliveries in April:**

Blood red is the new ALCO PA1 of the Lehigh Valley (Item No. 64416-1 / -2), which American Z Line (<https://www.americanzline.com>) is offering this month with two different car numbers. It will be joined by Kansas City Southern's EMD E8 (62615-1 / -2), which will be released to coincide with this railway's merger with CP on 14 April 2023.



Its blood-red colour makes the Lehigh Valley's ALCO PA1 (item no. 64416-1) stand out from afar. Photo: AZL / Ztrack

The Trinity RAF carrying wagons with a length of 53 feet, which were introduced in new form last month, are now followed by semi-trailers of the same length from JB Hunt (905204-1). The yellow boxcars are lettered for TTAX. The semi-trailers can also be purchased separately as two-packs (954004-1).

The MAXI-IV boxcar units are available in four different colours. The wagons are painted brown and are used by the AOK. There is a choice of four compositions differently loaded with containers (906557-1 to -4).



The brown MAXI-IV carrying car units (906557-4; photo left) are discontinued at the AOK. The Baltimore & Ohio half luggage car (74010-2; photo right) is the latest form novelty. Photos: AZL / Ztrack

Shortly before the editorial deadline, news reached us about the delivery of heavy semi-baggage coaches ("Combine Coaches"). This is a new design whose prototypes were on the road with two- and three-axle bogies.

The debut in April is the appealing version of the Baltimore & Ohio, which runs on three-axle bogies. Three different road numbers (74010-1 to -3) are offered for this model.

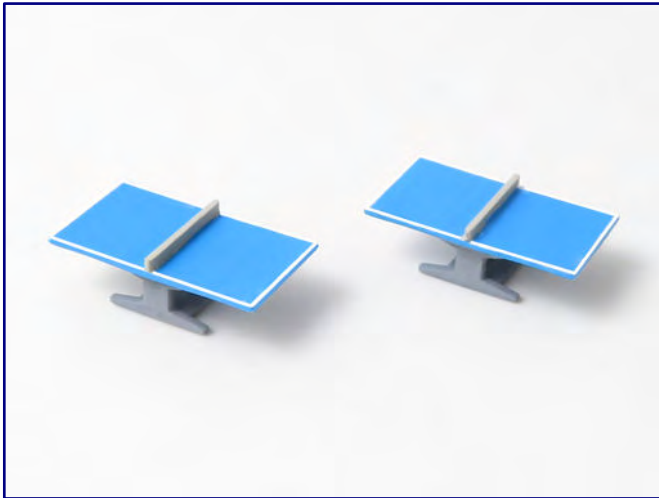
**Yellow Dwarf sets the pace again:**

The models from Yellow Dwarf (<https://www.yellowdwarf.eu>) in Prague are impressive. In the meantime, we also have some prints from this production and we can confirm the good detailing and excellent quality from our own impression.

We are currently preparing some of the models from the previous programme and the following new products for installation in a diorama and for suggestions as part of our annual theme. In April, the following new products were added to the range: beer barrels in aluminium (art. no. 60013U and KEG version (60014), and concrete barriers (60236) for various uses on roads and construction sites.

A previously completely overlooked accessory for schoolyards, parks or even playgrounds are the new table tennis tables (60353). In the "Wedding Ceremony" pack (60402) we find chairs with covers for the





Both the table tennis tables (art. no. 60353; photo left) and the tents (60501; photo right) can be used in a variety of ways on the layout. Photos: Yellow Dwarf

guests, a white carpet and a decorated wedding altar for the bridal couple. The outdoor ceremony can then also be witnessed by viewers of the layout.

Perfect for excursions and campsites are the new “Tents I” (60501), which already suggest that a small series is being set up here for leisure activities.



The idea for the edition of the “Wedding” pack (60402) seems unique. In any case, it allows for special layout scenes with an eye-catching effect, as long as it is not used for crafting an original wedding gift. Photo: Yellow Dwarf

## Fire brigade new products at EtchIT-Modellbau:

EtchIT-Modellbau (<http://etchit.de>) has introduced two new fire-fighting vehicles based on the Magirus Rundhaubers (round bonnets). They are supplied unchanged in the form of kits in the highest print resolution. The vehicles are constructed in individual components, which makes painting much easier. The parts are assembled after the painting has been completed.



Both the 20-metre turntable ladder (art. no. XD181dl\_Z; photo left) and the water tender (XD181tlf\_Z; photo right) are based on the Magirus-Deutz truck. Photos: EtchIT-Modellbau

The turntable ladder on the Magirus-Deutz chassis (item no. XD181dl\_Z), whose 20-metre ladder also consists of three individual elements in the model, looks particularly impressive. This vehicle of a voluntary village fire brigade should complement the tank fire engine (XD181tlf\_Z), also based on Magirus-Deutz chassis, with a large driver's cab, which offers further seating for the "bluecoats" on its rear bench.

## The current Märklin deliveries:

These days, two new products for friends of the ÖBB are reaching the dealers: The "Bubikopf" is rolling out as class 64 (item no. 88745) of this railway administration in Era III condition. Technically, it is up to date, the design from 2014 is driven by means of a bell-shaped armature motor. On branch lines, the small tender steam locomotive can haul the three-part tank car pack for the Martha oil company (82320) for the same era. It consists of old two-axle tank cars with brakeman's platforms.



The class 212 (item no. 88697) was one of the locomotives that looked good in the ocean blue/ivory colour scheme.

Only a small subset arrived so far from the MHI special series from autumn 2022: The light diesel locomotive of the class 212 of the German Federal Railways (88697) in the condition of the mid-1980s wears the ocean-blue-ivory-coloured paint that was common at that time and “dresses” this locomotive type very well. The technical features include a bell-shaped armature motor and warm white-red top and tail lighting. The factory has already sold out this series, so only the confirmed and, as yet undelivered orders, of the dealers have been served.

Also, only a partial quantity of the express steam locomotive class 01.5 (88019) has been delivered as a museum locomotive of the Eisenbahnfreunde Zollernbahn (EFZ), Rottweil. In the meantime, the delivery has been stopped, as we have been informed by Märklin. The background is the correction of a prototype error.



The express steam locomotive 01 519 (88019) has been delivered in parts. In the meantime, the delivery has been stopped again. We will explain the reason in the test report in the next issue.

The prototype of the 01 519 comes from the DR stocks, but as a model from the museum locomotive series it reflects the operating condition of 2016 and is consequently to be classified in Era VI. This is a new model, which we will discuss in detail in the May issue.

Finally, those who have dedicated themselves to the Bundesbahn era around 1963 will also be considered: The train set “Deutsche Bundespost” (81341) is located in epoch III, consisting of a post office-owned V 36 in red, two covered freight cars Gmhs 53 (one with EUROP lettering) as well as a stake car Rmms 33 with brakeman’s platform. A yellow Tempo tricycle is included as a load.

#### **Spare parts at Küpper:**

In the quarterly new products letter, the cargo specialist Küpper from Aachen (<https://spur-z-ladegut.de>) reminds us that spare parts are also available for purchase there. Helmut Küpper has listed a few examples of parts that are always missing, such as pressure springs for couplings or tension springs for pantographs, in his information.

#### **NoBa-Modelle before the summer break:**

Before NoBa-Modelle (<https://www.noba-modelle.de>) begins its annual creative break until mid-October, during which new ideas are born again, this manufacturer has gathered together the new products that are still to be announced here.



The Swiss electric locomotive Ae 8/8 of the BLS, which we reported in the last issue, is now also available as a finished model (art. no. 5020RF). The Davos Monstein station (4133R), which was added to the range a little later, also belongs in these alpine realms.



With the Mercedes-Benz O6600 (6391R), NoBa-Modelle is releasing a bus from the era of the long trucks before the summer break. Photo: NoBa-Modelle



The three bottle banks (10263R) have a special history as a model. Photo: NoBa-Modelle

The Mercedes-Benz O6600 omnibus (6391R), which still belongs to the era of the Langhauber (long bonnet), can be classified in epoch III. The new products “3 old glass containers” (10263R) have a special history: A good customer from Vienna had received waste containers for better painting with the support construction not removed.

He discovered that these supports could also be painted and used as six champagne bottles. In order to be able to recycle the empties left over after the party, the 3D print provider now provided the matching collection containers.

#### New animal figures at Klingenhöfer Miniaturen:

Klingenhöfer Miniaturen (<https://www.klingenhoefer.com>) has introduced some new animal figures for use in human environments, in the wild, or in zoological gardens. The Kalong flying fox, one of the largest living bat animals, looks spectacular. It can also be used in scary scenes at funfairs, or in American Halloween decorations.

Ten meerkats, on the other hand, can probably only be used in a zoo, where the small predators, which seem so cute and peaceful, are often among the most popular animals with visitors. This effect is sure to create excitement on an exhibition layout as well.



The flying fox (photo left) and the pair of wild boars (photo right) couldn't be more opposite, but in their own way they enrich Z gauge model railways. Photos: Klingenhöfer Miniaturen

Since a single squirrel will hardly be noticed on a layout and these animals populate many parks and also residential areas, the manufacturer now offers these cute animals in a pack of six.

Two snow-white and beautiful Samoyed dogs now accompany their master or mistress on the model railway layout. Originally, these herding dogs were also used as sledge dogs.

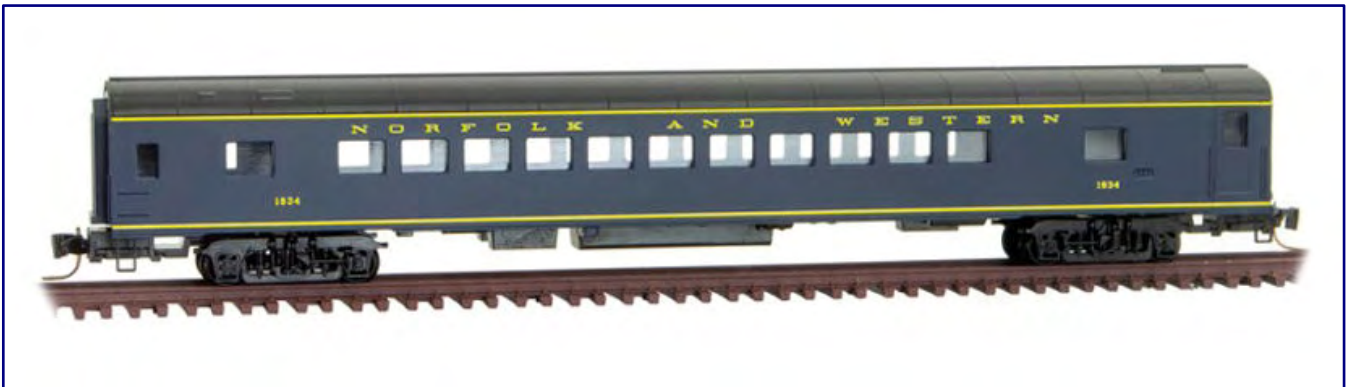
New models represent sows and boars, which can now populate Central European forests on the model railway. We don't want to meet them during a walk in the woods, but it's a completely different story with the miniatures, because they bring life under the trees.



Two Samoyeds are waiting for dog-friendly model railroaders of our size. Photo: Klingenhöfer Miniaturen

#### Passenger coach special editions at Micro-Trains:

MTL (<https://www.micro-trains.com>) has continued the "War of the Worlds" series with wagon number 2 (art. no. 518 00 841). A motif from the literary model of 1906 again adorns a boxcar.



This Norfolk and Western passenger coach (item no. 552 00 240) was not previously in MTL's range. Photo: Micro-Trains



Three light 83-foot passenger coaches will also be added to the programme this month. The Union Pacific car with road number 5484 (552 00 011) and the PRR car with number 4231 (552 00 061) are reissues. The Norfolk and Western car (552 00 240) is completely new.

#### Rokuhan announcements in Japan:

For some time now, information has been leaking out that Rokuhan is planning to produce a 1:220 scale version of the Shinkansen series 0, the train that opened the high-speed era worldwide at the 1964 Tokyo Olympics.

This was recently officially confirmed to us and shortly afterwards the responsible persons presented the first samples of unpainted bodies at a Japanese exhibition. In the meantime, this can be read on the company's website and accompanied by photos (<https://www.rokuhan.com/news/2023/04/855/#>).

The choice fell on the twelve-piece original version of the bullet train, which stands out from the later modernised version not only because of its paint but also because of the small side windows.

As soon as further details, new photos or a delivery date are announced, we will provide this information in this magazine.

#### Interesting Wagons from Full Throttle:

WDW Full Throttle (<http://www.wdwfullthrottle.com>) has released two car packs for March and April, each differing only in their road numbers. In the past, this supplier has already produced refrigerator cars for the Pacific Fruit Express, which were lettered for either the Union Pacific or the Southern Pacific.



These ice cream refrigerator cars (Item Nos. FT/B-9026 & FT/B-9026-2) were in use for both the UP and SP as part of the Pacific Fruit Express. Photo: WDW Full Throttle

Refrigerated wagons now appear in two double packs (Item Nos. FT/B-9026 & FT/B-9026-2), bearing both logos and having transported bar ice to or from both companies. The lettering is designed accordingly. Owner William Dean Wright commented enthusiastically: "A really nifty wagon, I'm glad to finally get it for the gauge Z hobby!"

#### New special model from the Stammtisch Wien:

In cooperation with FR Freudenreich Feinwerktechnik, the Z-Stammtisch Wien has produced a new freight car for its club members and friends of ÖBB. Those interested in the model, which we will now present briefly, may please contact the e-mail address [zspur.stammtisch@gmail.com](mailto:zspur.stammtisch@gmail.com) directly.

ÖBB enthusiasts will be supplied with a two-pack of open freight cars of the Klagenfurt type (item no. 43.336.02), which come from the ÖBB stock. The models are packed in a plastic box and additionally in a nicely printed cardboard slipcase.



These two cars of the type "Klagenfurt" from the stock of the ÖBB (item no. 43.336.02) were issued by FR Freudenreich Feinwerktechnik for Z-Stammtisch Vienna. Photo: Dr. Helmut Engelbrecht

The model is the type Klagenfurt, which was procured by the Reichsbahn from 1942 onwards and was used in large numbers, mainly for military purposes. Of the approximately 23,000 cars, around 4,000 are said to have remained in Austria after the end of the Second World War.

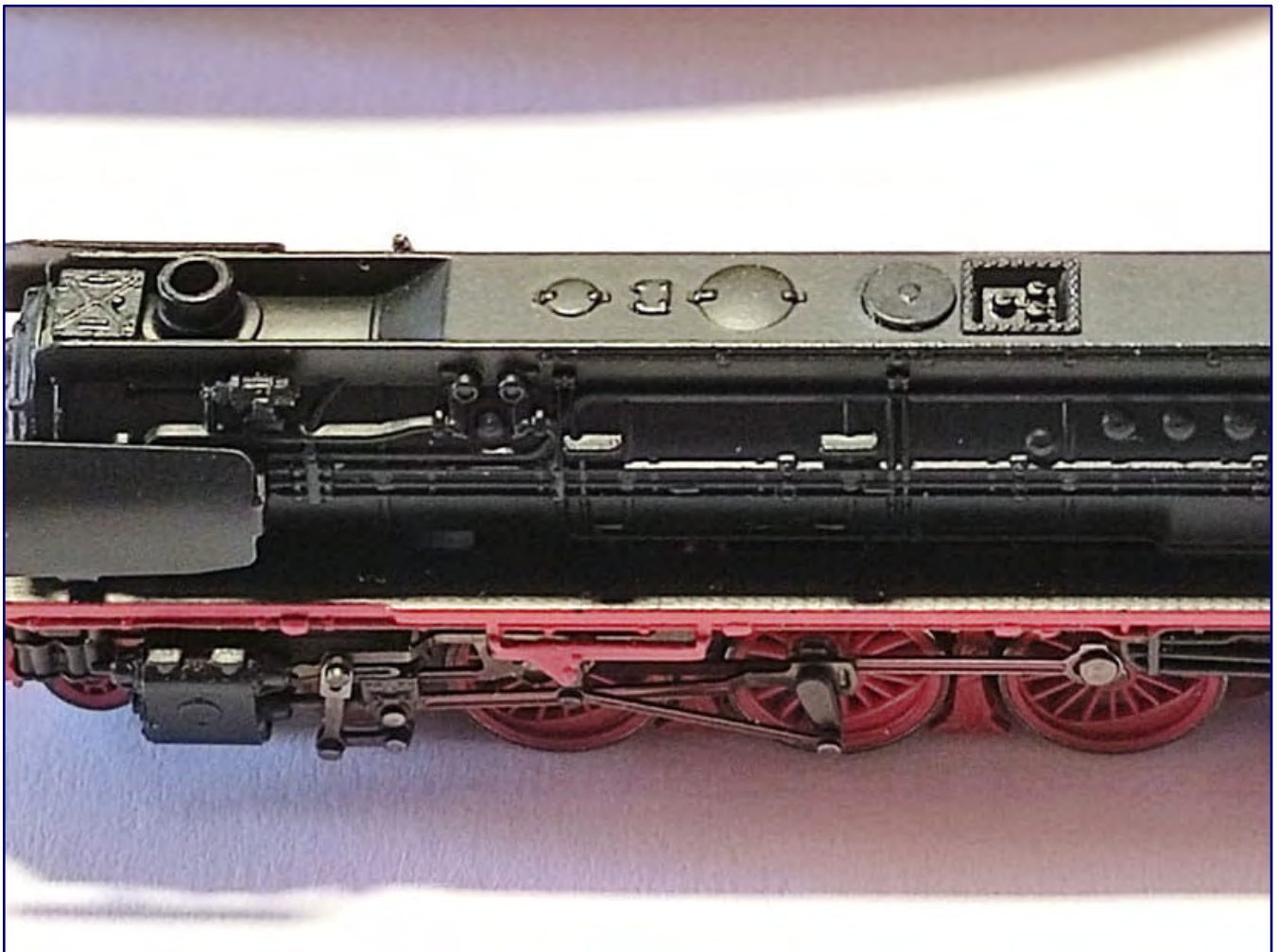


Some of these cars were converted to other types during reconstruction, scrapped or even transferred to the Soviet Union by the Russians who had occupied eastern Austria. It was not until the 1960s that ÖBB's stock began to decline rapidly, because in the meantime there were enough wagons with modern metal bodies in service. The last representatives were later withdrawn to construction train service.

#### Parts from Bahl's model railways:

Unfortunately, some inconsistencies, weaknesses and missing parts spoil the pleasure of the new 01 519 from Märklin. Steam locomotive specialist Michael Bahls (Bahls Modelleisenbahnen) took up the challenge without delay and developed a kit that eliminates the most serious errors. Designed on the computer and produced by 3D printing, the small series manufacturer is ready for delivery in record time. His set of finishing parts is made up as follows:

- Replica of the missing mixer preheater box
- Reproduction of the three safety valves
- Dome cover for the housing screw (2 x in the delivery)



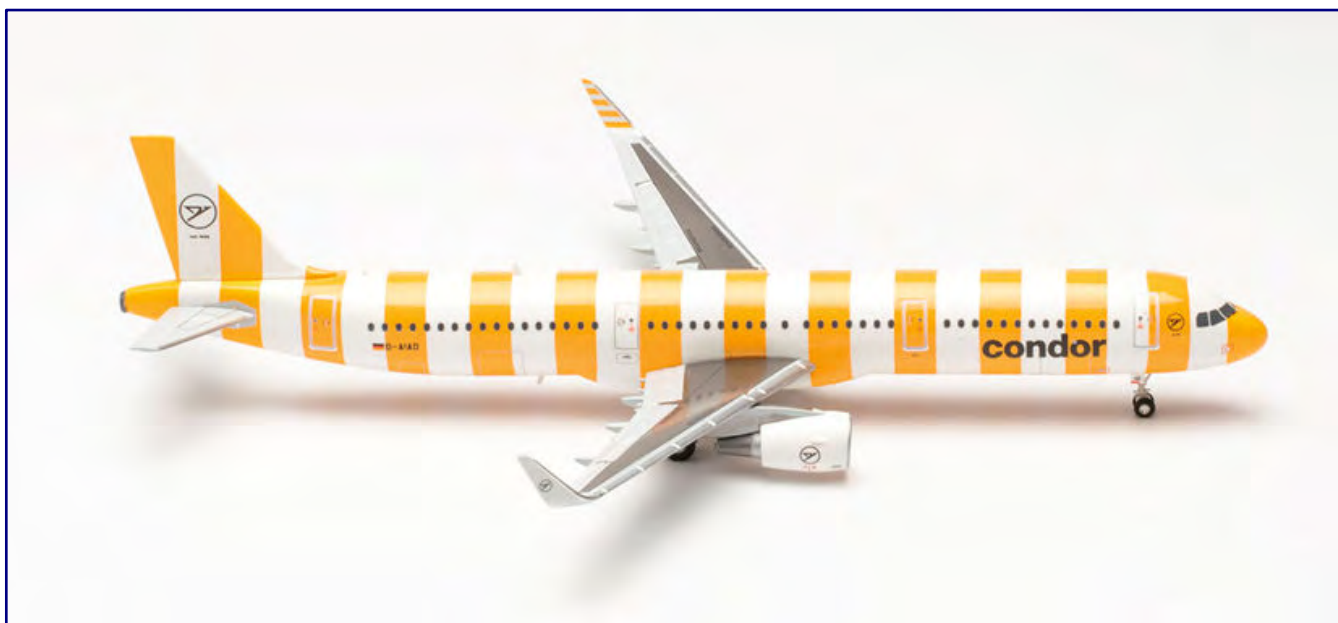
With the Bahls add-on kit the incorrectly delivered series of the class 015 by Märklin gets the missing mixing preheater box in front of the chimney (far left), replicas of the three safety valves (rectangular recess in the dome casing far right), and a cover for the housing screw (left next to the valves). Photo: Michael Bahls

The accessory kit can only be purchased from the 1zu220-Shop, which also sells the basic model and can thus prevent additional postage costs for the customer.

#### New products from Herpa:

Herpa is again planning new aircraft models in 1:200 scale for May and June in the high-quality Wings series (floor models with landing gear). The following models can be used on European layouts:

British Airways Airbus A321 neo (572422),  
Condor Airbus A321 "Sunshine" (572576),  
Air France De Havilland Canada DHC-7 (572644),  
Spanish Luftwaffe Airbus T.23 / A400M "Atlas" (572729), and  
Lufthansa Airbus A320 neo "Lovehansa" (572743).



Condor's current livery follows the pattern of sun awnings and is used in various colours. The version shown on this Airbus A321 is called "Sunshine" (Item No. 572576). Photo: Herpa

Appear as slightly simplified models in flight display from the Snapfit series with an overall length suitable for turbines up to a maximum of 30 cm:

Icelandair Boeing 737 Max 8 (613743),  
Bulgaria Air Airbus A220-300 (613811), and  
ITA Airways Airbus A350-900 (613750).

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Board of Editors:  
Holger Späing (Editor-in-Chief)  
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English translation:  
Alexander Hock, Christoph Maier, Oleksiy Mark, Martin Stercken

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Publisher and Responsible in terms of German press law (V.i.S.d.P.) is Holger Späing, Am Rondell 119, 44319 Dortmund; Contact: 49 (0)231 9598 7867 or by e-mail to [redaktion\[at\]trainini.de](mailto:redaktion[at]trainini.de).

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# Trainini

German Magazine for Z Gauge



Encounter with the Rabbit Hutch

A View of Nature  
Intermodellbau 2023

## Introduction

Dear Readers,

An exhausting, but also exciting month, is now behind me, and also behind all my co-workers for this magazine, and especially for this edition. It began with an invitation to Hamburg, where we were able to attend the premiere of a new section in the Miniatur Wunderland.



**Holger Späing**  
Editor-in-chief

It was a very long, but above all eventful day, which will also keep us busy for a while: Among everything we were making and bringing back with us, there is also some exciting film material. We would like to process this into a new episode of **Trainini TV**.

The only thing we are short of now is time because many events this month have jumbled up all our schedules: On the one hand, there are the print magazines of various publishers who also wanted to be part of the Hamburg premiere. In their editions you will see different perspectives and more photos.

But there is also a lot going on around new product deliveries: Märklin has supplied us with a whole pile of new models, including three new models from new moulds. To measure and test them all in time was too much for us. A little later, we learned of an improvement that had to be considered in our test in any case.

As a result, topics had to be swapped and postponed. This is unpleasant for me, because once again reports are affected that I hope will get a good response from you. But time is limited, especially free time. I have felt that again this month.

Regarding the publication date of this edition, which has also been pushed back, too far for my personal liking, I therefore ask for your understanding. Editors, translators, and all other helpers carry out their tasks with passion in their spare time. Quality is always more important to us than rigid publication deadlines.

I therefore hope that you will forgive the delays and look forward even more to the contents of the May edition. A new model presentation, two event reports and a continuation of the annual focus on 2023 should guarantee a colourful mix.

In the book and film reviews, too, we are going far off the beaten track: with a focus on editing images and photos, we are devoting ourselves to an important marginal topic of our hobby. In the case of film, it is a true discovery that only came into focus with the recently launched sale.

The list of reports we had to process seems endless to me, after all, we also had to draw a line and set the well-known editorial deadline. In short, on behalf of everyone involved, I hope that the effort and work was worthwhile to satisfy you. Have fun browsing through the lines of this edition and stay well-disposed towards us!

Sin-Z-erely,

Holger Späing

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Currently no items

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Currently no items

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We thank Eisenbahnstiftung for their photo support.

Date of publication of the German language version of this issue: 31 May 2023

Cover photo:  
BDnf 738 “rabbit hutches” were once commonplace on main and branch lines. They ran behind all reversible locomotives of all series and traction types. And so here we have two examples in front of the lens at the same time.



## A rabbit hutch for Spur Z Märklin fills the Gap

*The driver's cab and train driver's compartment were at the opposite end of the car from the toilet, their sizes were identical: The "Silberlinge" control cars procured from 1961 onwards were therefore disparagingly referred to as the "rabbit hutch" and even the "driver's toilet" by the operating personnel. Operationally, they quickly gained in importance, which has now finally led to a 1:220 scale model at Märklin.*

After the Bundesbahn had procured prototypes for the type 4n coaches, which became known as "Silberlinge" (silver pieces), from 1958 onwards, matching control cars also followed from 1961 onwards. Like the regular seat cars, they were given an unpainted outer skin of stainless steel (German abbreviation "Nirosta") and the typical beadwork in a peacock's eye pattern below the window band. The end faces were excluded from this pattern.

These new control cars, later designated as BDnf 738, were intended to support and accelerate the replacement of outdated cars with wooden superstructures. In addition to two large 2nd class compartments, they also had a luggage compartment and the driver's cab for pushed operation.



A "Silberlinge" reversible train could also not be missing from the anniversary parades for "150 Years of German Railways" in Nürnberg (Nuremberg). On 14 September 1985, this BDnf 738 is there, showing exactly the operating condition chosen by Märklin. Photo: Manfred Kopka (CC-BY-SA-4.0)

As the last one was extremely cramped, the coaches were soon referred to as the "bunny hutch" and did not enjoy too much popularity. The control compartment could be divided with two swing doors so that a central aisle was created between the control stand (right side of the vehicle) and the driver's cabin (left side).

A rubber diaphragm, which was narrower in the vertical area, and transition plates made it possible to use this central aisle as a transition to the next car when the front door was opened. This was intended for the case when the control car was not used, as such, and ran in the middle of the train set.

Behind the driver's cab was the luggage compartment, which could be loaded on both sides via two bi-fold doors. Adjacent to this was a large passenger compartment, and behind it a small one. At the end of the smaller one was the toilet and two folding seats. The two door systems separating the three aforementioned areas were different: behind the luggage compartment was a single folding door, and between the passenger compartments was a double folding door.

From the passenger's point of view, the new carriages were very well thought-out: upholstered seats with imitation leather upholstery were not yet taken for granted at that time, and this seating comfort also led to the use of "Silberlinge" in faster trains for a long time, up to high-class express trains.

The maximum permitted speed was 120 or 140 km/h, depending on the equipment. The successor type BDnrzf 739, built from 1969 onwards and largely identical in appearance, received improved and reinforced brakes throughout and was allowed to run at 140 km/h. It also no longer had steam heating.

However, the bogies of the Minden-Deutz type remained light as a common feature with its predecessors. From 1971, the Bundesbahn reacted to the criticism that the driver's cabs were too narrow (front to back) with the BDnrzf 740, which was given the so-called Karlsruhe head.

Its own design was already shown in 1:220 scale in the Märklin models 8718 produced from 1978 onwards. Even "bunny hutches" were converted accordingly at the same time as the Mini-Club models appeared and then designated BDnf 735.



The class 280 reversible train, which was already offered as an Era IV version, was also used with bunny hutches. Here, 280 010-0 is on its way with N 772 near Ebermannstadt on 12 August 1975. Photo: Werner Vierling, Eisenbahnstiftung

However, the appearance became even more colourful: Since the connecting doors were hardly used, DB removed them during stays in the repair works. Thus, the transition doors were closed, which certainly also curbed draughts due to leaks, and the transition plates were removed. The rubber diaphragms on the driver's cab side also disappeared, often with a time delay.



A variant of the wagons that should not be neglected is the type Bdnb 742. It was created by conversion when the driver's cabs were severely damaged in accidents and the wagon was no longer needed with the push-pull function. The signal horn and headlights were then removed, the transitional equipment was then fully represented. This also included a functional rubber diaphragm which stood out from the control wagon in its standard design.

A final look at the prototype is the colours used. Even though the side walls remained unpainted, the frame was colour-contrasted from them. As delivered, the cars had a black stripe here. From 1975, the side beams were then repainted ocean blue.

## An overdue decision

After 45 years of waiting, the Göppingen-based manufacturer is finally offering the distinctive head shape of the "Silberlinge" control cars, which is so popular with model railway enthusiasts. This is an important step because the previous offers could only cover the prototype period from 1971 onwards.

Accordingly, it was not a single control car from Märklin for Era III in the past. The once most modern face and the modernity of the DB associated with the reversible trains was therefore completely missing in an important, but, so popular era.



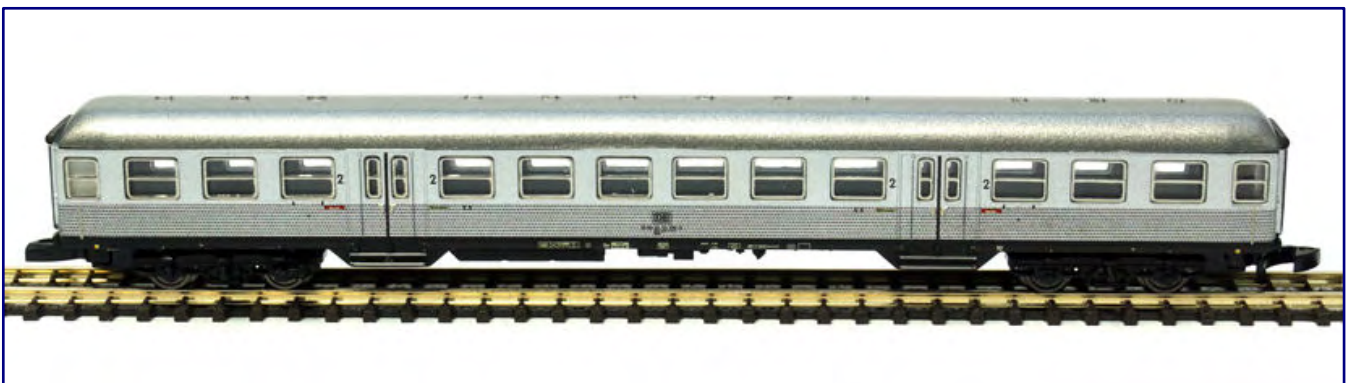
The new local transport car set (art. no. 87189) comes in three parts and features the long awaited BDnf 738 "rabbit hutch" as a new tooling.

This will now finally change, because our prototype section also shows which variation possibilities are still available here and a model has already been announced for Era III. This is also extremely important news against the background of several locomotives in the product program that are suitable for reversible trains and have appeared in new form since 2010: V 200<sup>0</sup>, E 41, and V 80.



Added to this is the fact that the 8718 “Silberlinge” control cars and all their successors to date are perceived as technically outdated because, until recently, they were still fitted with incandescent lamps whose light emission is barely perceptible when not travelling at ICE speeds.

The MHI car pack 87189 that we are looking at today is therefore an important signal to customers and arouses joy for possible combinations in the future. At the same time, the aim is to finally catch up technically to the current state of the art, which seems all the more important, as this has long been done with the locomotive models almost through the series.



In identical size we show here the three silver pieces from the new MHI wagon pack to make their respective design visible: above the control car BDnf 738, the 1st/2nd class car ABnb 703 in the centre, and the 2nd class wagon Bnrzb 719 at the bottom.

So, our expectations for this report are high and now we will see if the BDnf 738 control car, which has not even been delivered in full numbers yet, meets the expectations placed on it. Beforehand, it should be noted that the MHI car pack also includes a silver Bnrzb 719 with 2nd class and an ABnb 703 with 1st/2nd class.

These two cars were created with the help of the moulds that were first used in 1976 for the “Silberlinge” premiere in the Z scale product program, i.e., two years before the control wagon with the Karlsruhe head.

The chosen printing variant is not new either, and corresponds to the one from the train set 81356. Qualitatively deviating features also apply to the rabbit hutch are now to be considered and find their way into this article via it. We will therefore not look at the two passenger cars separately.

## Märklin's model implementation

The new BDnf 738 control wagon has one thing in common with the classics from the Mini Club range: it is based on the chassis of the former 8718 model, on which its housing was placed according to an identical concept.

This means that the new mould does not have a removable roof, but that the front and side walls together with the roof form a single injection moulded part. If you want to get to the interior, which seems superfluous in view of the lack of interior fittings (insert figures) and the need to replace lamps, the body must be lifted off the chassis.



The “face” of the control wagon looks convincing. Only the area of the buffer beam looks a bit bare due to the lack of coupling hooks and brake hoses. By the way, the plugged-in control cable for the reversing cable was also omitted, which should be at the front right of the head in the top view (compared to the prototype photo on page 4).

If the car floor and the Minden-Deutz bogies with the axle wipers for current collection are “old acquaintances”, the circuit board inside the car is not. It was newly developed and now finally works with warm white and red LEDs to display the direction-dependent head or tail lights.

The light output at the driver's cab end is good; the light was perceptible even at low track voltage and did not flicker in the test. We are completely satisfied at this point. As with the prototype, the lanterns for the three-light headlight and the two red tail lights are structurally separated.



The new design also included a circuit board that now finally works with light-emitting diodes. Its design will also allow it to be used in the Karlsruhe head: an identical position of the light channels can be seen on the housing lying on the head. The one above also clearly shows that the light guide for the tail lights is coloured red.

Märklin has also handled this correctly from a functional point of view, which does not seem at all self-evident: We are thinking of VT 08<sup>5</sup> from the construction year 2005/06. It would have been desirable if the lanterns of the tail light had appeared in a perceptible red.

Then the ends would also have given the prototypical impression of a red cover glass, because the light guide is indeed coloured through. But this is not a real point of criticism, because we also have to keep in mind the small size of the diffusing lenses in 1:220 scale. They appear very dark when not illuminated, so that their colour is almost impossible to make out.

The dimensional accuracy of the new products is also good and they fit in well with the already existing “Silberlinge” cars. Minor deviations concern the height and especially the width, which are always the most critical points in our scale. However, the percentage classifications easily hide the fact that these are probably mostly unavoidable measuring errors.

The engravings of the model are no outlier compared to the already existing cars of this type family. Since we would like to rate the “Silberlinge” as a whole, as successful and still contemporary, this also applies to the new rabbit hutch. Thus, the window inserts are also made without gaps.



**Dimensions and data for the DB BDnf 738 control wagon:**

|                                   | <u>Prototype</u> | <u>1:220</u> | <u>Model</u> | <u>Difference</u> |
|-----------------------------------|------------------|--------------|--------------|-------------------|
| Length over buffers (LüP)         | 26.400 mm        | 120,0 mm     | 119,9 mm     | - 0,1 %           |
| Height (roof edge) above railhead | 4.050 mm         | 18,4 mm      | 17,8 mm      | - 3,3 %           |
| Width (car body)                  | 2.825 mm         | 12,8 mm      | 13,1 mm      | + 2,3 %           |
| Distance between pivots           | 19.000 mm        | 86,4 mm      | 86,5 mm      | + 0,1 %           |
| Total axle base                   | 21.500 mm        | 97,7 mm      | 97,3 mm      | - 0,4 %           |
| Bogie wheelbase                   | 2.500 mm         | 11,4 mm      | 11,2 mm      | - 1,8 %           |
| Wheel diameter                    | 950 mm           | 4,3 mm       | 4,4 mm       | + 2,3 %           |
| Service weight                    | 27,4 t           | ---          | 16,5 g       |                   |
| Permissible maximum speed         | 120 or 140 km/h  |              |              |                   |
| Construction year(s)              | from 1961        |              |              |                   |
| Number of units                   | 350              |              |              |                   |

We particularly like the implementation of the wipers on the driver's cab side (see photo on page 8), as they also played a decisive role in shaping the face of the prototype.

The gap between the car body and the window insert is barely noticeable, and the colour of the transition has been improved.

The only small compromise is the typhoon's horn moulded on the roof.

This can only be seen at a closer look, but an attached component, which was developed for the V 36 and could also be used for a new railbus construction, would have promoted a more filigree appearance.

We have to take a closer look at the chosen design of the front side with the driver's cab: A transition door with window (and handle) has been reproduced there, but no transition plate that could be folded down when used in the prototype.



The horn above the driver's cab (view from the front on page 8), which would have been a better choice as a separately attached part, looks a bit too plasticity. The black frame edge that does not continue to the buffer beam is really disturbing.

This obviously points to the operational abandonment of the transition function, which became common in the seventies, at the latest. The mounted rubber moulding with its narrower vertical stripes does not

seem to fit in with this. However, this corresponds to our own memories of a transitional period and also matches the prototype photo from the railway anniversary in 1985 (on page 4).

More decisive here will be whether Märklin will change this on the Era III model from the 87074 car pack from the 2023 spring new products! Unfortunately, the hand sample does not suggest this so far. However, the colouring of the frame replica in the front area will probably be better.

By using the chassis from the Karlsruhe head, the black long girder replica does not extend all the way to the buffer beam. Where it ends prematurely at the chassis, it picks up the body's housing again and runs through it.

On the model we are looking at here, Märklin has neglected to paint the corresponding part of the "little hat" in the buffer area black. This looks a bit strange and unique, and does not correspond to the prototype in any way.

Otherwise, the painting is done cleanly and in the right colours (painted areas of the prototype) and accurately (replicating the stainless-steel skin). As with all recent models, the appearance has been chosen to be quite matt with a clear coat, which is closer to the operational impression.



The printing of the model is very elaborate and clean under a magnifying glass: It includes the operating inscriptions, as well as the door handles and the step plates. Also worth noting is the imprint "Handbrake here" between the two steps. In contrast to the other passenger cars, the vertical handle bar at the end of the car is also printed on the rabbit hutch, and not raised as a moulding.

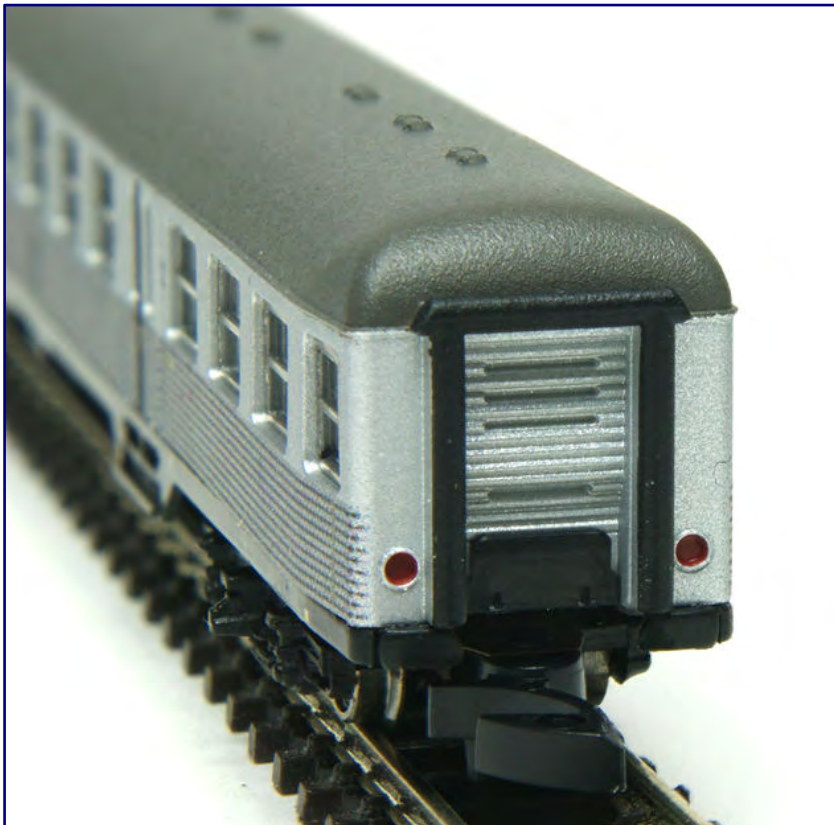
The roof colour of the control car deserves a few words, as it differs from the other two coaches. The BDnf 738 shows an umber grey roof according to RAL 7022, which corresponds to the painting regulations from November 1964. The black Ege biscuit in regulation design also matches this. The inscriptions of the BD Karlsruhe and the revision date, by the way, indicate an operating condition of the year 1977.



The other two seat cars still have the roof in RAL 9006 white aluminium, which was the standard at the time of delivery to DB. The negative version of the Ege biscuit on them, which was characteristic of many "Silberlinge" for a long time, is then also conclusive.



The roof colours of the driving trailer (right) and the other two silver cars differ, which is quite prototypical. Further printing elements can be seen here, which underline the high quality. The sides of the vertical rubber buffers elements, which have not received any black paint, are not liked.



At the opposite end of the car, we find a black transition plate. The tail lights are red. This also corresponds to the reproduction of the other two wagons in the pack.

This design was originally based on a misinterpretation of the labelling regulations for light-coloured backgrounds.

Later, it was specified and supplemented to the effect that the letters DB must always appear light and are always surrounded by a wide, dark border on the outside.

This brings us to the last point, "printing." It is on the usual high Märklin level and leaves nothing to be desired.

The inscriptions on the side walls and frames are in the correct colours and are complete. They also include class numbers in the old typeface directly next to the doors and smoking/non-smoking signs.

The peacock's eye pattern underneath the window sill is also reproduced by pad printing, since this has been the case with all "Silberlinge" editions, so far.



Correctly, it does not run over the front sides and corresponds in the print image to recent editions of this type. Differences may become disturbingly visible if the model railroader combines older and younger editions.

Except for the red tail light dummies at the non-driver's cab end, the front sides of all wagons of the pack remained unprinted. We would like to describe the rubber diaphragms as incompletely coloured: While the horizontal parts appear completely black, i.e., also, on the outer sides, all vertical elements have only been printed in black on the front. A few individual improvements could be made here.

This is not necessary on the steps of the entrances. As far as we know, this is the first time that Märklin has tried to reproduce the lattice plate supports of the steps by printing a thin strip in white aluminium. And that works very well on the model.

## Summary

In the final overall assessment, this is an important wagon that has been sorely missed in the product program for a long time. Technically, it has been reproduced in a contemporary manner and does not afford any weaknesses in the mouldings.



The train encounter of the front page from a different perspective: We now see the three-light headlights of the new “bunny hutch” BDnf 738.

The paintwork is also one of its strengths, as is the adherence to the most important prototype dimensions. With the driver's cab end we hope that Märklin has left room in its moulds for the variants of the prototype by means of sliders, and that in the future it will also be able to present us with a complete transition with tread plate or an example with dismantled diaphragms.

# Trainini® *International Edition*

German Magazine for Z Gauge

It would certainly be a special treat if a rubber diaphragm could also be represented in the standard version and thus also the version as it was presented after the driver's cab had been removed. Such half luggage cars were used for a time in many night trains that were not operated as push-pull trains. Especially well known is the use with the tiny electric locomotive E 69.

The only weakness of the model is the printing on the rubber diaphragms, which could have been better. The edge of the frame, which is not painted or printed in black all the way to the buffer beam, disturbs the overall appearance, which apparently Märklin has already noticed. We conclude this from the hand sample of the announced Epoch III production model.

Consequently, we experience a model that gives a lot of pleasure and should have a great future in the Mini Club assortment. Almost all criticised points can be eliminated without shape corrections, which is why they do not carry much weight here. We, therefore, nominate the BDnf 738 control wagon from the 87189 local transport car pack for the best new release of the year 2023 in the category cars.

Manufacturer of the basic model:  
<https://www.maerklin.de>



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Layout details (part 2)

## Our chaotic Nature

*Our editor Dirk Kuhlmann shares his reflections about layout detailing beyond tracks and buildings. How can a track plan be complemented with a beautiful landscape and what are the key aspects to look out for? The following photo essay provides a good illustration of the chaos of nature and how its replication can make all the difference towards a successful layout.*

What is the secret to a scenically coherent model railway layout? It is the unbelievable diversity in real nature, which often may appear chaotic to the observer, but is nowadays the yardstick for authenticity even at the reduced scale of our model railways.

The exemplary view of a quite ordinary meadow landscape shows us a multitude of different vegetation, partly bare layers of earth or even accumulations of stones, and all this, of course, always in the context of the respective regional setting.



A narrow rural road to a farm in Ostfriesland (East Frisia): nature is present in great variety and certainly in an unorganised, even chaotic state. This is a photo from a 1:220 scale layout.

Only we humans have added straight lines and other geometric shapes to the environment (although there are certainly naturally occurring regular structures such as honeycombs with their hexagonal cells). And this man made mark on nature has influenced and even changed our habits and how we look at nature.

Therefore, it is not surprising that our human centered perception of nature is so often extended to model railway layouts, almost as an "ideal of beauty".





**Man has left his marks, but nature is reclaiming its place.**

One good example is the transition from a paved road to its green shoulder and beyond. Most layouts, even real top ones of any scale, feature lush and densely applied grass fibres right next to the tar surface, as if it was drawn with a ruler. On a brand new road with a still dark asphalt surface this would be absolutely acceptable, but otherwise?

Of course, it is up to each model railroader how he sees his “ideal world”. The only thing to observe is that these exhibits usually have a short lifespan, the landscape somehow doesn't seem really exciting – rather boring. The subsequent dismantling of the layout is usually only a matter of time.

Here I may simply invite you to repeatedly take a walk into nature and reality, preferably with a camera at hand. This will, in a very short time, sharpen your eye for the chaotic conditions out there.

A subsequent look at the programme of various landscape material manufacturers will also show how diverse the range of products has become and that the all basic prerequisites for modelling authentic looking landscapes are available. But what is the most sensible way to proceed?

## Do your research

Usually, the subject of a ones model railway layout is searched for in ones immediate vicinity, track plans of the prototype are converted to the respective scale and, if everything goes well, we will find matching kits for building and other structures.



Four different roads from very different regions. Apart from the condition of the road surfaces, the respective shoulders may also serve as prototypes for a modelling project.

Further research can be done on the internet and eventually this will produce a more concrete idea of the landscaping part of the project. With the photos you have already taken of the desired area, a new layout can now be conceived at the same time. And, it really helps to consider the landscaping part as being of the same priority of a modelling project, as the rolling stock and track plan parts.

**Using self-collected natural materials**  
Stones, sand and soil appear in the programmes of many accessory suppliers. There is nothing that speaks against using these for railway modelling.  
However, when using self-collected materials please bear in mind that they can also contain invisible and unwanted components, such as fungal spores or insect eggs.  
Before using them on your layout, self-collected material should therefore, first be sterilised. This is most easily done by first moistening the material, then spreading it as easily on a baking tray and finally putting it in the oven.  
Exposure to a temperature of at least 120°C for 30 minutes will destroy all proteins.  
If the layer to be sterilised is thicker, it should be done in between, or exposed to the heat for longer. The target temperature must be reached in all areas within the full exposure time.

The next step is to venture out into nature and collect stones, sand and various types of earth. This will be yet another opportunity to become aware of the complete mess that exists next to a normal paved footpath.

This is exactly the process for how I approached my first “chaotic” model railway layout many years ago, “Helenensiel”, set on the North Sea coast.

When researching the project, many fellow holiday-makers wondered about my photo work, with the well adjusted and sinfully expensive camera mostly pointed at the ground and instead of the “blue sea and blue sky with white clouds.”

continues on page 20





In the model and along the road's pavement runs a sandy shoulder. No straight lines to be seen there, and low-growing plants will slowly overgrow the scenery, unless humans intervene (top). Plants will seek out the smallest niche or crack in the road surface to grow. To us, it is just weeds and it looks messy. In reality, it is another hallmark of chaotic nature (bottom).





Many contemporary model railway layouts are designed with have a stony, sandy and earthy base, and low-growing plants modelled with turf materials. Real pieces of wood convey a visually varied forest floor (top). In the finished layout, there is not much left to be seen of the detailed preparatory work (bottom). Instead, the chaotic character of nature comes to the fore.



I had chosen early autumn as the season I wanted to model and noticed already the different shades of colour of the grass. On a rather run-down side road, I noticed the repeated lack of greenery on at least 50 cm to the verge, with only individual pioneer plants to be found here.



In recent decades, the clearing of vegetation next to the tracks has been less intensive than before. This is also worth of a design attempt. Whether in Düsseldorf (left photo) or in Portugal (right photo), the scenes look very similar.



With sidings that are hardly used, it is sometimes only a matter of time before everything is overgrown.

The verge simply consisted of very fine gravel that is often found at the edge of country roads, but nature is reclaiming lost terrain bit by bit. Somewhere between the grass there was still a complete section of railway track, overgrown and forgotten. Such a scenery is of course perfectly suited to the year 2023 and has become a trend with many manufacturers: the so-called “Lost Places”.

continues on page 22





On this photo diorama, everything seems to be in order. However, grass and bushes are spreading more and more along the railway embankment. The first pioneer plants have found a home in the ballast bed. Even the artificial drainage ditch next to the construction remains is barely visible.



A nearby row of trees sparked some further curiosity in me. It looked as if man had planted the birches here in a row, but far from it! He had created a drainage ditch decades ago and where there is water, there is life. A careful look showed that nothing had grown in a line here, and that things were rather a complete mess.

Besides, even the wild and taller plants towards the edge of the field was remarkable. Days later, elsewhere, I examined the plant species at the edge of a natural mixed forest. An elderly couple approached me and I was astonished to learn that both were botanists in their professional life.



If a house is not maintained, nature will soon take over from all sides.

They explained a few things to me and of course the names of the individual plants until my notepad was almost full. A little further on, the forest cleared a bit and a dark green carpet spread out in front of us. I was told that it was a wood anemone.

It grows in light shade, for example at the edge of shrubs or under deciduous trees. Due to its early vegetation period, it can also be observed under otherwise heavily shaded plants and has white flowers. In fact, I transplanted this plant species years later on one of my dioramas.

## Implementation

I would like to spare you a lengthy list of tips here and instead refer you to our **Trainini®**- issues 6/2017 to 12/2017, which will allow you to study the extensive steps of landscape design at your leisure and implement them, if necessary, or even supplement them with your own ideas.

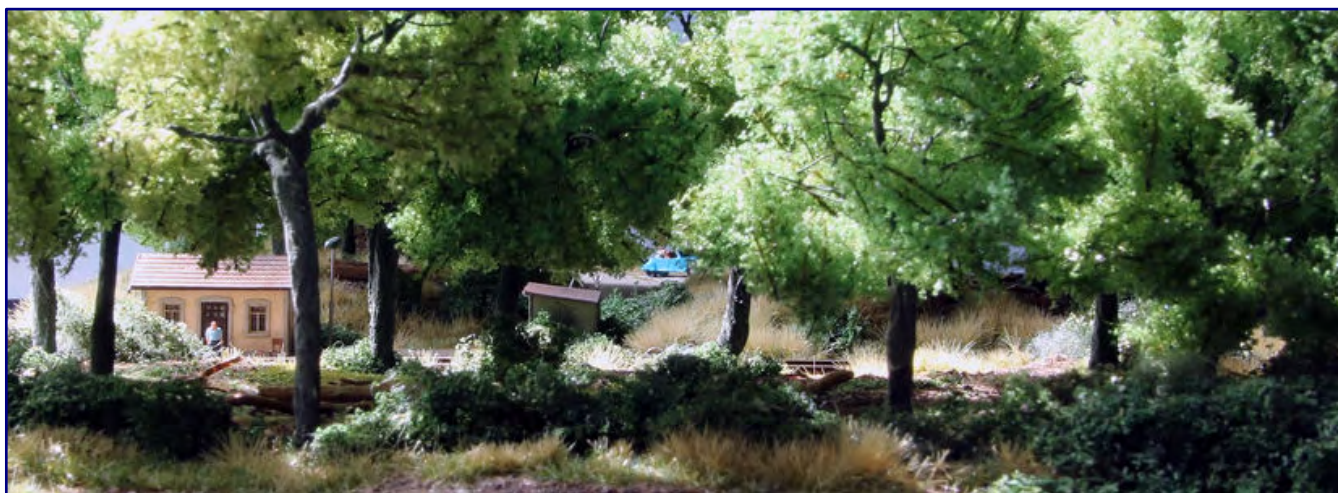




In the absence of human intervention, the natural weathering process will take its course. Layers of earth are worn away by the force of water, and, depending on the region, the subsoil will appear. Nevertheless, plants are once again asserting themselves. The scene shown can be seen on the "Tillmanns Loch" layout of the FdE Burscheid (model railway club).

continues on page 25





**Photo above:**

On the right an environment artificially formed by humans and a gradual transition to the left towards an increasingly chaotic and “naturally untidy” environment. Both scenes nevertheless harmonize well between each other.

**Photo middle:**

At the edge of a forest, it is the previously mentioned variation in the ground cover and low growing vegetation which fills the layout with life (see also photos on page 19).

**Photo below:**

The concepts described here apply to all modelling scales, as this shot from a non-Z-scale layout illustrates.



With all the design suggestions, the basic rule is quickly established: Approach the design of your landscape in a completely unorthodox way, if possible by following your gut feeling. If you leave the ruler or set square in the drawer, it will work out with modelling an authentic looking piece of nature.



**Further evidence:** Even at larger scales, the obsession with detail in the design of chaotic nature, can flourish, or simply just look good.

Take your time for the basic ground design, as the later addition of greenery is simply the completion of the landscape. Forest areas should not be hinted at in a rudimentary way, but require a lot of trees and suitable ground cover for arriving at a convincing result.

To avoid “design accidents” during the first serious attempts at taking up such an approach, it is advisable to first test compositions on small wooden boards. This could directly involve also involve some training sessions for the use of a static grass applicator.

Of course, there are wonderful ready made grass mats with the most diverse vegetation available today. They offer a real alternative to electric grassing or even speed up the work. Many of the products on offer from the H0 gauge range can also be used for the 1:220 scale without further modification.

Even the 4 mm high grass fibres serve a useful purpose. Look in late summer at a non-cultivated edge of a meadow, the height of growth is sometimes quite astonishing.

Finally, I would like to give you a simple but effective piece of advice, which I have been following myself for many years: Repeated test photos taken of the evolving layout at the approximate height of a Preiser figure and further photos taken from a maximum of 10 cm above the layout surface will give a good indication of whether or not the design is going into the right direction.



Last, but not least: Nature shows us the different colours of grass and thus a welcome visual variation of pure meadow landscapes (left). Transposed to the model layout, this variety of colours speaks its own language.

In doing so, avoid to focus on the track and don't even include any rolling stock in order to avoid distraction from the landscape. Include a train only later in the final photos, and you'll see that the entire scenery looks absolutely coherent.

References of our author:  
<https://www.helenensiel.com>



Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

## Bildbearbeitung mit GIMP 2.10 **Sicher zu guten Bildern**

***Ratgeber haben wir schon viele gesehen und gelesen. Doch nur wenige erfüllen tatsächlich den Zweck, einem unbedarften Menschen zum sicheren Umgang mit dem vorgestellten Thema zu verhelfen. Ein Buch, das sich genau diese Lorbeeren verdient, stellen wir heute vor. Das einzig Außergewöhnliche an ihm ist sein Inhalt: Üben und Erlernen von Bildbearbeitung mit GIMP 2.10.***

Robert Klaßen  
GIMP 2.10  
Der praktische Einstieg

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Bonn 2018

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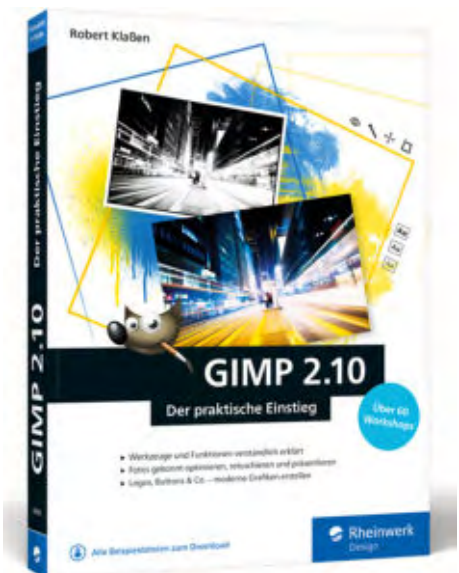
Keine Frage, diese Rezension, die Sie nun lesen, ist in einem Modellbahnmagazin nicht ohne weiteres zu erwarten. Doch besteht ein nicht unwesentlicher Teil unseres Hobbys auch darin, die eigenen Werke gekonnt auf Bildern zu präsentieren und Dritte daran teilhaben zu lassen.

Die selbstgefertigten Aufnahmen sollen dann mindestens genauso spannend und packend wirken, wie es das fotografierte Objekt selbst im persönlichen Betrachten auch tut. Und wenn eine solche Anlage oder Diorama dann in einem Magazin wie unserem portraitiert werden soll, stellen sich auch schon höhere Ansprüche an die Aufnahmen.

Vielleicht aber denkt der eine oder andere Leser aber auch daran, einen eigenen Internetauftritt zu installieren und sich auf diese Weise bekannt zu machen. Dann sind bald Banner und Schaltflächen, bisweilen auch weitere Grafiken gefragt.

Für alle diese Belange gibt es mit GIMP ein leistungsfähiges und frei verfügbares Programm. Es zu verstehen und zu beherrschen, ist dann der Schlüssel zum Erfolg. Diesen verspricht das vorliegende Buch und wir möchten natürlich wissen und teilen, wie gut es diesen Zweck erfüllt.

Zum Autor Robert Klaßen können wir keine eigenen Erfahrungen beitragen, doch seine Referenzen stehen außer Frage: Er versteht sein Handwerk und ist fachlich dafür bestens geeignet. Doch wird er auch ebenso gut und verständlich schreiben können, um sein umfangreiches Wissen weitergeben zu können?



Die Antwort nehmen wir gleich vorweg: Die Aufgaben wurden mit Bravour bewältigt. Vor uns liegt ein Ratgeber, der wie kaum ein anderer den Leser in die Lage versetzt, GIMP 2.10 erfolgreich anzuwenden, den Aufbau des Programms zu verstehen und Erlerntes auch auf neue Projekte zu übertragen, die andere Werkzeuge oder Inhalte erfordern.

Dieses Druckwerk besitzt die Grundfunktion einer guten Bedienungsanleitung, sinnvoll angereichert mit vielen Tipps und Ratschlägen und so eng wie eben möglich am praktischen Arbeiten mit GIMP 2.10 ausgerichtet. Es lässt einfach keine Wünsche offen und ist aus unserer Sicht als Glückstreffer zu bezeichnen.

Schritt für Schritt lernt der Leser die Welt von GIMP kennen und bekommt verständlich wie auch kurzweilig vermittelt, wie die vielen Werkzeuge und Funktionen gekonnt und zielgerichtet eingesetzt werden können.

Dabei arbeitet der Autor viel mit Bildern oder Grafiken und verschafft seinen Lesern mit über 60 Workshops, in denen die im Buch abgebildeten Ergebnisse (angeleitet und erklärt) selbst herbeigeführt werden, einen eigenen Erfahrungsschatz. Nur dieser verspricht auch Erfolge, wenn das Buch erst einmal beiseitegelegt wurde.

Inhaltlich umfasst die Lektüre die Bildkorrektur und -retusche, Fotomontagen, Schaltflächen und Logos für das Internet und noch einiges mehr. Bevor es mit dem Anwenden losgeht, stehen aber auch die Installation des Programms und individuelle Einstellmöglichkeiten der Benutzeroberfläche auf der Agenda.

Sämtliche Werkzeuge und Funktionen werden thematisiert und vorgestellt, damit beim späteren Anwenden ein wichtiges Grundverständnis bereits vorhanden ist. Hier zeigt sich mit der guten und klaren Gliederung eine weitere Stärke dieses Titels.

Typische Aufgaben wie das Korrigieren und Nachschärfen unscharfer Fotos, der Umgang mit Text in Bildern, das Freistellen von Motiven oder auch das Optimieren von Kontrasten, Intensivieren von Farben oder auch das Retuschieren werden hier vermittelt.

Plötzlich erscheint ein leistungsfähiges und deshalb auch komplexes Programm geradezu leicht. Dank der leicht anzuwendenden Tipps und Tricks wird nahezu jeder Käufer in die Lage versetzt, mit GIMP 2.10 Bilder zu erstellen, die ihre Betrachter verblüffen und meist wohl auch faszinieren.

Für vergleichbare gute Programme wie Adobe Photoshop Elements oder gar die Vollversion Adobe Photoshop muss teilweise tief in die Tasche gegriffen werden. Hier profitiert der Nutzer von einer ehrenamtlich aktiven Programmierergemeinschaft.

Alles, was dann noch fehlt, ist ein Buch wie dieses. Auch der Preis erscheint sehr moderat, besonders aber auch im Rückblick unter Einbeziehen der hohen Qualität. Diese schließt auch die Wiedergabe im Druck, eine gute Lesbarkeit und die Reproduktion der Fotos und Grafiken mit ein.

Einem Kritiker, der stets bemüht ist, auch in einem noch so guten Produkt noch ein wenig Verbesserungspotenzial aufzuzeigen, fällt es schwer zuzugeben, dass er nichts dergleichen gefunden hat. Schließlich gibt es auf dieser Welt nun mal nichts Perfektes. Aber alle eigenen Erwartungen, überprüft an der fokussierten Zielgruppe, vollständig erfüllt zu sehen, ist dann das größte Lob, das hier bedenkenlos auch ausgesprochen werden darf.



Filmportrait zur Baureihe 03

## Ein verkannter Renner

*Hören wir den Begriff „Schnellzugdampflok“, am besten zusammen mit „Pacific“, dann denken wir unweigerlich an die Baureihe 01. Die betriebliche Bedeutung ihrer kleineren Schwester war jedoch etwas größer und doch blieb sie stets in deren Schatten. Die Bundesbahn trennte sich sogar ein Jahr früher von ihr. Im Märklin-Programm spielte sie seit den siebziger Jahren hingegen eine wichtige Rolle. Ein Filmportrait macht die Vorbilder unvergessen.*

Rio Grande Video  
Die Stars der Schiene – Folge 85  
Die Baureihe 03 (DB)

VGB Verlagsgruppe Bahn GmbH  
Fürstenfeldbruck 2017

DVD-Video  
Bildformat 4:3  
Tonformat Dolby-Digital 4.0  
Sprache deutsch  
Laufzeit ca. 80 Min.

Best.-Nr. 6385  
Preis 9,99 EUR (im Abverkauf)

Erhältlich direkt ab Verlag  
oder im Fach- und Buchhandel

Während ihrer Dienstzeit stand die Baureihe 03, etwas despektierlich auch als „Salondampfer“ bezeichnet, stets im Schatten der größeren und populäreren Schwester der Baureihe 01. Entsprechend blieben auch weniger Exemplare von ihr erhalten und keine einzige Lok aus dem früheren DB-Bestand ist heute betriebsfähig.



Und so blieb auch diese DVD lange außerhalb unserer Wahrnehmung und erwies sich im nun laufenden Abverkauf als unverhoffter, aber glücklicher Fund. Interessant ist die Schnellzugdampflok der Baureihe 03 doch allemal, denn immerhin gehörte sie im Maßstab 1:220 gleich zum Startprogramm des Premierenjahres.

Als die Mini-Club 1972 auf der Spielwarenmesse als kleinste elektrische Serieneisenbahn der Welt vorgestellt wurde, rollten fast zeitgleich die allerletzten Vorbildmaschinen aufs Abstellgleis. Unsere Modellbahn hat dieser Dampflok also lückenlos ein Andenken bewahrt.

Deshalb glauben wir, dass sich viele unserer Leser auch für die Einsätze dieser Maschinen interessieren, um das stimmig auf ihrer Anlage wiederzugeben. Alles, was Sie dann dazu brauchen, ist passendes Rollmaterial und dieser Film.

Gemessen an ihrer Gesamtzahl von 298 gebauten Exemplaren war ihre betriebliche Bedeutung sicher höher als die der größeren 01. Immerhin 145 Lokomotiven dieser Baureihe gelangten nach dem Krieg in den Bestand der Bundesbahn. Um die verbliebenen Vertreter des letzten Einsatzjahrzehnts geht es in diesem Film.

Fast vollständig basiert das Gezeigte auf historischen Aufnahmen, die auch das klassische Filmformat erklären. Nur wenige Einspieler und die Kulisse für zwei Interviews mit früheren Lokführer zu deren Betriebserfahrungen und Erinnerungen zeigen zwei erhaltene Maschinen.

Einen deutlichen, thematischen Schwerpunkt bilden die von Hamburg-Altona und Ulm ausgehenden Einsätze, waren es doch zwei der letzten Hochburgen dieser Baureihe. In Ulm endet auch die Betriebsgeschichte, denn dieses Betriebswerk wurde als Auslauf-Bw bestimmt.

Blieben die kleinen Pacific-Dampflokomotiven einst gern mit Dampfangel liegen, bewährten sie sich in den letzten zehn Jahren voll und ganz. Zurückzuführen war das auf die umgebaute Saugzuganlage und auch das Abwandern in niedere Dienste, wo sie vor allem die Baureihe 38<sup>10-40</sup> ersetzte. Höchstleistungen waren fortan nur noch selten gefragt.

Dies wird ebenso erläutert und mit interessanten Informationen unterlegt wie auch ihre Rolle als Ablöser der Baureihe 18<sup>6</sup> auf der Strecke zwischen Friedrichshafen und Ulm im Jahre 1962. So wird der Zuschauer in die Lage versetzt, die behandelten Baureihe ins Betriebsgeschehen und auch leistungsmäßig einzuordnen.

Wie von Rio Grande gewöhnt, wurde all das gekonnt auf Zelluloid festgehalten und nachvertont. Freilich sehen wir den historischen Aufnahmen deren Alter von inzwischen bis zu sechzig Jahren an; alles andere wäre aber auch nicht authentisch. Das gezeigte Material wurde so gut aufbereitet, dass es inhaltlich nicht an Wert verliert und wir uns daran vollkommen erfreuen können.

Ein paar weitere Informationen zu den Inhalten möchten wir Kaufinteressenten noch mitgeben: Die Einsätze, die von Ulm aus erfolgten und mit einem Höchstbestand von 17 Maschinen (1965) einhergehen, machen rund 25 % des Gesamtfilms aus.

Aber auch Einsätze im Rheintal und im Umfeld von Wiesbaden fanden ebenso Eingang wie einige wenige Sequenzen von der Emslandstrecke, auf der die 03 einst eine wirklich wichtige Rolle spielte. Auch Bremen und eine Episode in Husum konnten hier untergebracht werden.

Aufmerksamkeit verdient auch das Bw Köln-Deutzerfeld, wo bis 1968 so viele Maschinen dieses Typs aus verschiedensten Heimat-Betriebswerken zusammentrafen wie wohl nirgendwo sonst. So finden wir natürlich auch Verweise auf Langläufe dieser Baureihe. Schwermütig stimmen hingegen Aufnahmen von der Verschrottung.

Ins Bewusstsein gerufen wird durch einen Moderatorenkommentar, dass die Nummernschilder bei der Baureihe 03 auf der Rauchkammertür fast durchweg unterhalb des früheren Zentralverschlusses befestigt waren, während sie bei der 01 exakt auf dessen Stelle montiert wurden. Er fragt, ob das eine (gezielte) Erfindung des AW Braunschweig gewesen sein könnte, um die Baureihen leichter unterscheiden zu können?

Letzter wichtiger Themenschwerpunkt sind die Einsätze auf der Marschbahn Hamburg – Westerland. Garniert mit den Lokführererinnerungen sind die teils spektakulären Aufnahmen noch spannender anzusehen. Den Produzenten der DVD war dies offenbar so wichtig, dass hierzu noch ein separater Bonusfilm angehängt wurde, auf den die Hülle nicht mal hinweist.

Wer jetzt Feuer gefangen hat, sollte sich beeilen. Wie eingangs erwähnt, trennt sich VGB | Geramond vom verbliebenen Lagerbestand und hat diese DVD deutlich abgepreist. Die DVD erweist sich als Schnäppchen und Entdeckung zugleich.

Publishing pages:  
<https://www.vgbahn.shop>



Relaunch of Intermodellbau Dortmund

## The leading Modelling Exhibition returns

*In comparison to previous years, the number of visitors to Intermodellbau was disappointing, but after the pandemic it stands up very well against all other modelling exhibitions in Germany. A successful comeback is, therefore, definitely to be noted. And in this latest revival, Z gauge also played a noticeable role, even though some exhibits also appeared in places one would not expect.*

Like probably every other trade exhibition, the Intermodellbau in Dortmund had a hard time getting re-established after an autumn run and a completely cancelled exhibition. A visible sign of this was that exhibition space was considerably reduced. This leading trade exhibition for model building and model sports had lost a whole three halls at its most recent staging.

When all model building fans were invited back to Dortmund from 20 to 22 April 2023, the exhibition could look back on more than 40 years of tradition. At the same time, the 25th anniversary of the Model Railway Association in Germany (Moba) was also celebrated on Friday evening, which was still to be made up for with a colorful gala evening.



Already in the reception area "Messe Nord" it was noticeable that something was different at the Intermodellbau: Lego had made its debut here. The visitors had different opinions about this, but there is no doubt that other target groups can be addressed with it.

However, around 60,000 satisfied visitors did find their way to Dortmund, where they found a wide range of products from all the other model building sectors and scales – only in many parts the scope was significantly reduced.

Some well-known names were also missing among the commercial participants, but a total of 355 exhibitors from 17 countries presented their products in Halls 3 to 7. This most recent event was once again targeting experienced model builders, as well as newcomers and families.



**The model railway hall was unusually empty: But on Saturday, three hours before the exhibition closed, quite a few visitors, some of whom had taken very long journeys to get there, were already on their way home: Due to the nationwide rail strike, many had no other choice but to travel by car and had to tolerate traffic jams.**

In the section of railway modelling relevant to us, more than 30 layouts were on display in Halls 4 and 7. The usual program also included seminars and lectures, an interactive area for children, the “Junior College Europa” of the BDEF, free handicraft activities for the youngest and rides on a steam railway (in Hall 6).

Of course, this also raises the question of how well the children's offers were aligned with the target group? Past exhibitions brought to light both successful ideas and those that failed to appeal to children. Therefore, this topic, including barrier-free access, remained a special focus of research.

A broad and well-mixed range of activities in all areas of model making was positively evaluated by those in charge of the assessment. Many handicraft offers, usually offered free of charge, invited visitors from kindergartens and schools to gain their first model building experience and to get excited about this hobby.

In the RC exhibitors' area, too, there were many opportunities to try controlling a car or a ship yourself. Here, too, those responsible, who are confronted with the greatest worries about young people, have long since realised that they should choose a new form of approach and participation.

This is probably part of the fixed repertoire in the area of Lego, which had moved in as a new offer at Intermodellbau and does not really want to fit in with the other categories, but proved to be important and right for attracting further groups of visitors.



It was also noticeable that the majority of model railway layouts were presented at a height that did not prevent smaller children from seeing what was on display. Wheelchair users also benefited from this, as they were happy to confirm in our conversations.

On the other hand, it is a problem for them, when stairs are set up at greater heights. This was well received by parents and children, which is why it has been acknowledged that this trend now also seems to be increasing in Germany, but it is not beneficial to the participation of physically disabled people.



It is always worthwhile to look beyond one's own nose: we also know the Borgward light railcar as a Z gauge small series model. In Dortmund, it could be seen in 1 gauge.

In this respect, we have regularly seen a greater awareness in many neighbouring countries to design the environment in such a way that hurdles are not unintentionally created for such people. This is also confirmed to us by reports from visitors who had travelled to the exhibition with their companions in the car and were visiting Messe Dortmund for the first time after the renovation work had been completed.

Their biggest point of criticism was directed at the handicapped parking spaces: While they were used to purpose-built parking spaces being as close to the building as possible at other destinations, the location and distance, at the redesigned Messe Dortmund, remained very much open to question.

The available parking spaces also proved to be too small in terms of width and accessibility – after all, a wheelchair is supposed to be moved next to the fully opened car door, which would have been disastrous in the absence of a healthy accompanying person.

However, it could not be ruled out that there are also closer and more suitable parking spaces. Should this be the case, the organiser should definitely check his signposting critically and improve it, because in this case the “suitable” disabled parking spaces were simply not found.

There is still almost a whole year to discover and cure the remaining “teething troubles” of the trade exhibition reconstruction. Here the people we interviewed also proved to be very forgiving of the Westfalenhallen Dortmund, despite all their displeasure. By the way, the next iteration of the exhibition is planned for 18 to 21 April 2024.

In the organiser's visitor surveys, guests at the exhibition gave the event an overall rating of “good”. Therefore, the Managing Director of Messe Dortmund, Sabine Loos, drew her personal conclusion as follows: “The exhibition is a permanent institution in Dortmund. It presents a cross-generational junior and continuing education program and is unique worldwide in its diversity of exhibits.”



Actually, not a trade fair exhibit, but as a historic vehicle also interesting for the model railroader, if it is perhaps also set up on his layout.

She is certainly right about that, but in the future it will be important to keep our finger on the pulse of the times and to recognise the changing world of model building and its new generation, in order to draw the right conclusions at the right time. After all, the official number of visitors, compared to around 100,000 spectators about ten years ago, also shows how much not only Intermodellbau has suffered – the trend cannot be explained by the consequences of the Corona pandemic, alone.

## Our exhibition tour

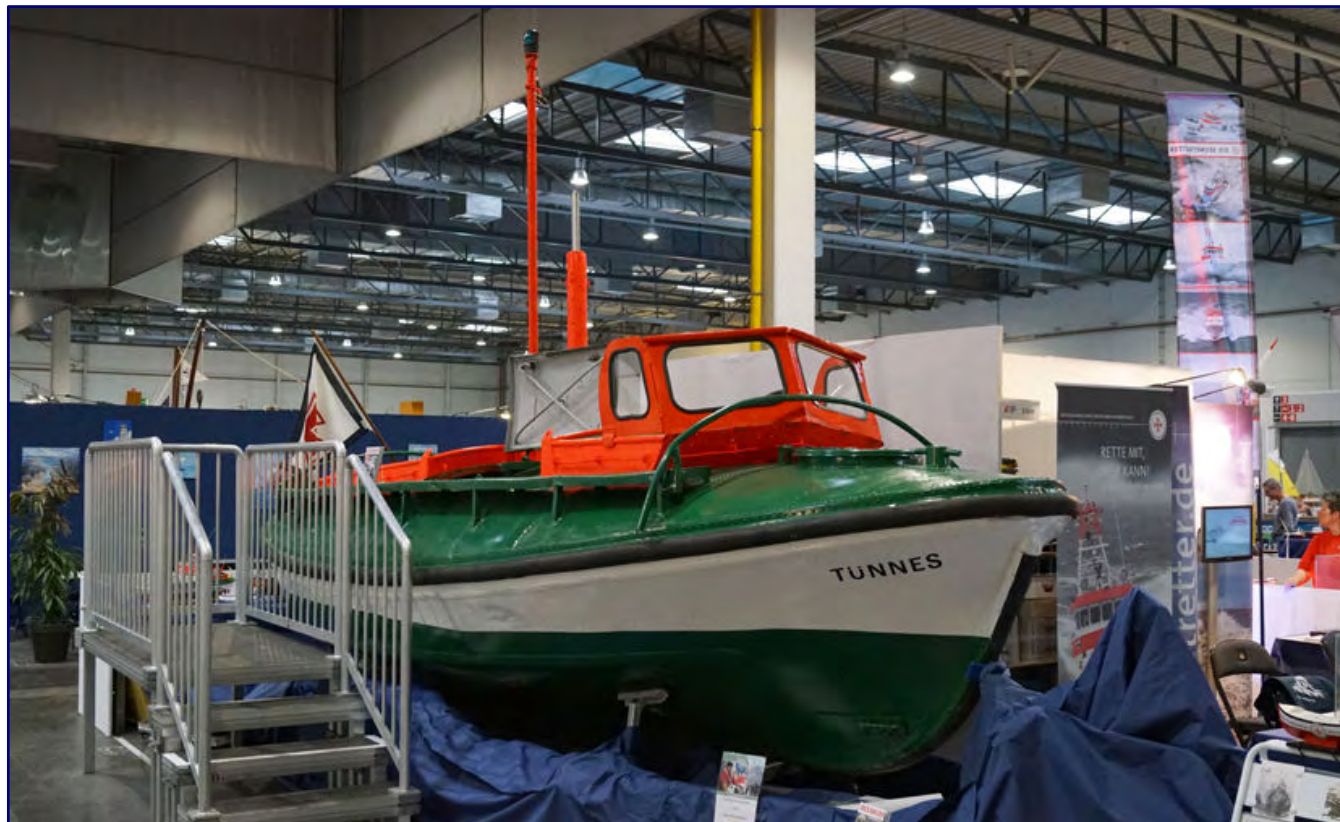
We start our tour with a look at the other model-making sections. Many of the exhibitors represented there are regulars at the exhibition, which is why individual exhibits and especially the different display areas are repeated every year.

Nevertheless, there are always models to see, sometimes more, sometimes less spectacular. We were impressed by the dinghy "Tünnes" presented by the DGzRS from its museum collection. It clearly pointed out the tasks of the society and indirectly also made clear the dangers to which its rescuers expose themselves in many missions.

The original created a fitting scenery for RC ship models of the "John T. Essberger", once the largest sea rescue cruiser with a marine position in the Fehmarnsund and today a museum magnet on land. The "Hermann Marwede", today's flagship, was also represented, as a model.



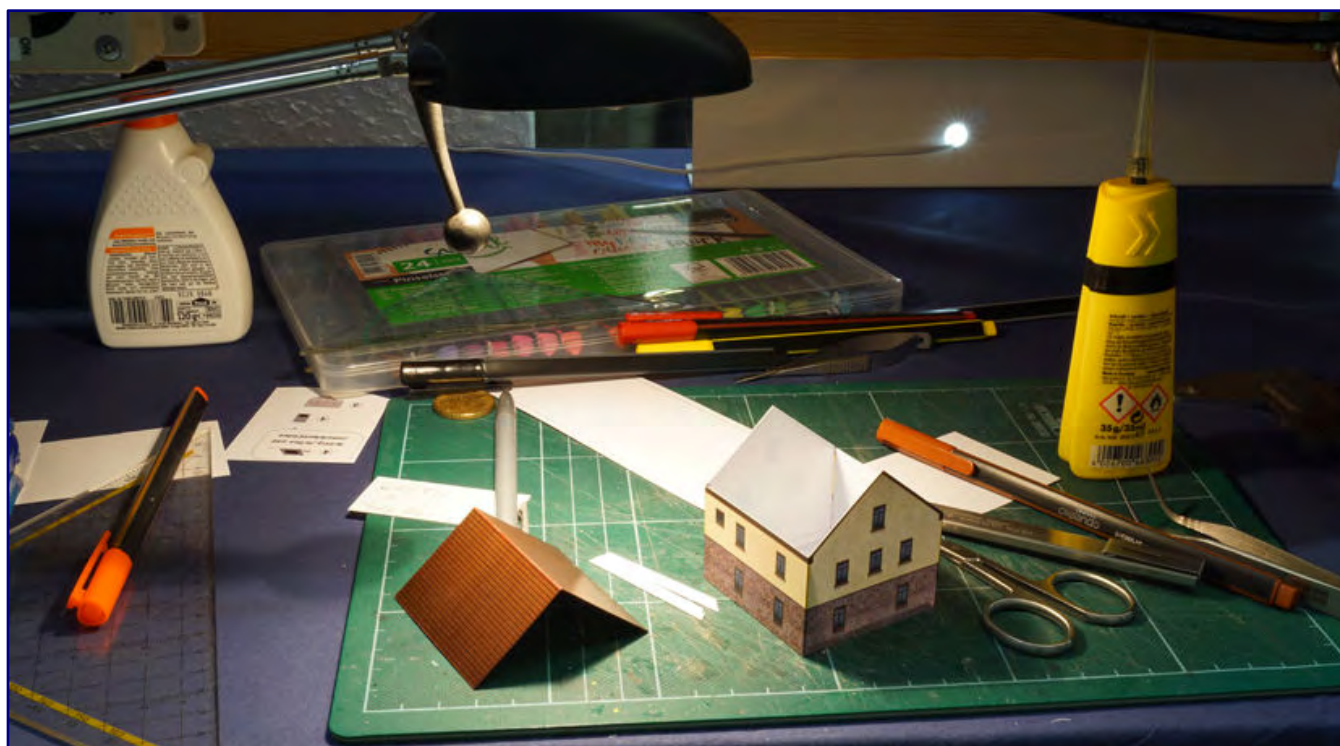
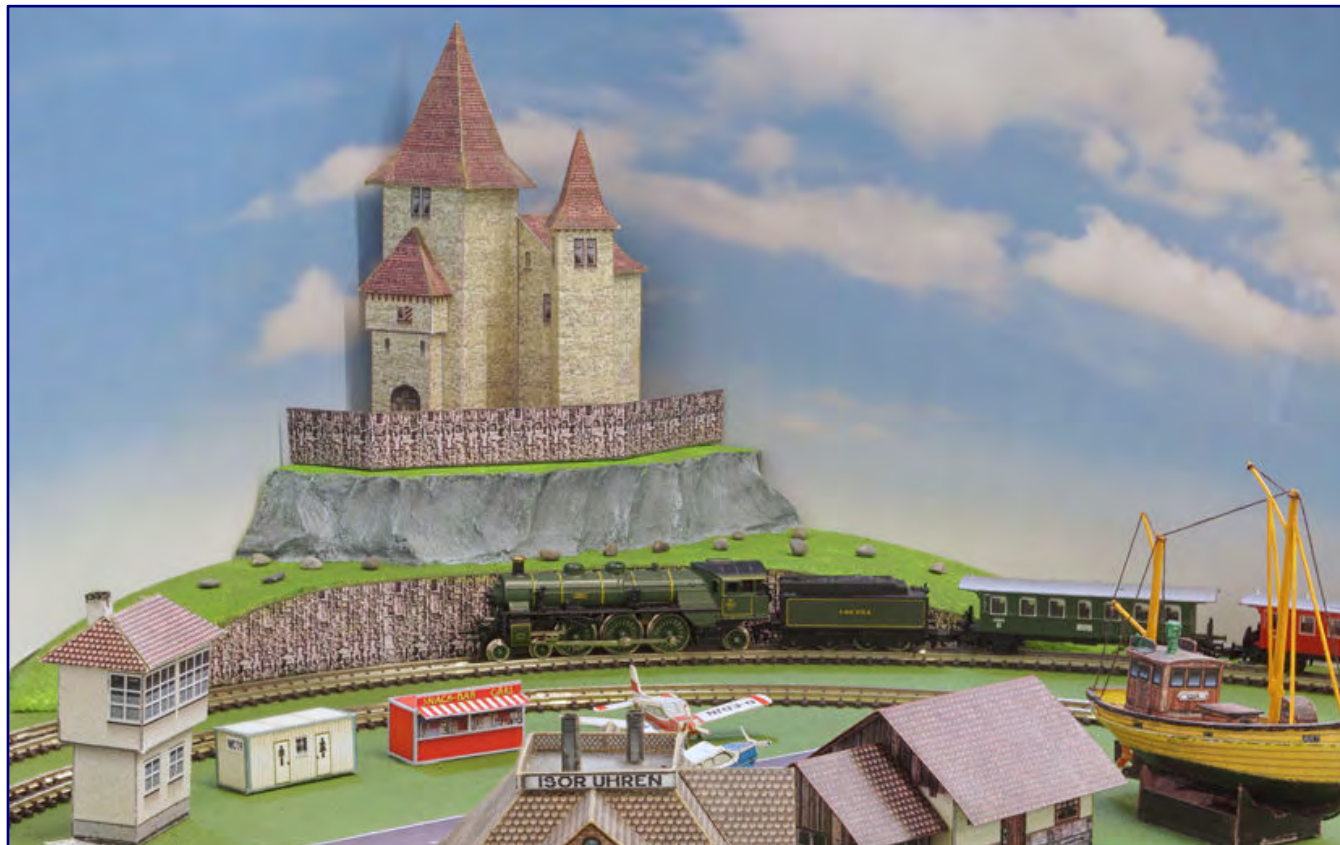
In the hall lobby, a gigantic model of the "Titanic", which sank in 1912, welcomed the visitors and proved to be a popular photo opportunity. Functional modelling created a fitting scenario by placing an all-terrain MAN 4x4 truck at the centre of one of the areas allocated to it. In this way, it attracted visitors to follow the smaller models as they made their way through unpaved terrain.



Prototype and model: The dinghy "Tünnes" (photo above) advertises the DGzRS, while the "John T. Essberger" (photo below) is representative of the fascination of models.



In this hall we also found Burkhard Merz, an exhibitor of Z gauge, who, however, had come for paper modelling. His small layout with two ovals inside each other showed a simple landscape, but with many paper buildings.



Burkhard Merz had participated in paper model making on a scale of 1:220 and placed his buildings and vehicles in a depicted railway landscape (photo above). But he also demonstrated to interested visitors how such beautiful models are created from print sheets (photo below).



He had built these with great sensitivity from print templates that he had either found as freely accessible files on the internet or was allowed to scale down further to 1:220 scale with permission. All looked carefully coordinated and captivated with their photo-realistic print surface.

Not far from his booth we found Minichamps and Imprintium. Thorsten Zwarg took a lot of time for us to explain the new approach of his brand Imprintium. According to his explanations, the successful designs of Minichamps could easily be transferred to smaller scales, including Z gauge, and thus be transformed into cars for model railways via 3D printing.

The initial program that emerged from this idea is immense and is set to grow steadily. Three product lines mark the beginning. In the "Military" segment, the ISU-122 tank, the Tiger I armored combat vehicle, the Marder infantry combat vehicle and the Leopard 2A6 are already being offered. They are joined by the Jeep W with spare wheel and fold-down windscreen. They are each available in a raw version, painted and completely decorated.



The Mercedes-Benz G-model in short version illustrates the delivery state as a unpainted model (left) and a finished model based on it (right). It is offered from a cooperation between Minichamps and Imprintium.

The civilian motor cars are only available in raw form for designing, but their number is all the greater for that: Alfa Romeo GTA 1300 Junior from 1971, Audi Quattro (1980), Chevrolet Camaro (1967), Ford Escort RS Cosworth (1992), Mercedes-Benz G-Model Short (W460 from 1980), Lancia Stratos (1974), Porsche 911 (1992) Carrera 4S Cabriolet and Coupé (2019), Porsche 911 Turbo (1977), Porsche Cayenne (2017), Volvo 240 GL (1986), and the Honda Civic Del Sol (1993).



This year's Märklin exhibition wagon (item no. 80731) is printed differently on both sides due to its flap numbers.

This list is supplemented by a model of the MAN SL200 bus from 1973 as well as single-axle trailers with a flatbed or open rear loading area.

They can be attached to trailer couplings, which have actually been functionally reproduced on some of the car models.

A link between the military templates and those for street racing are above all some products from the accessories line, which is also only available as raw models: Wooden

crate cargo set (6 parts), wood pile set (15 parts), dustbins (12 parts), sandbags and barricades (7 parts), transformer house (5 parts), Euro-pallets (20 parts), and oil drums (10 or 50 parts). Two different types of fences, each with 20 parts, complete this part of the starter program.

When we stop by Märklin, the first thing we look for at an Intermodellbau is, of course, the annual exhibition train car. For this year a self-unloading freight car Fals (80731) of the Eisenbahn und Häfen GmbH was selected. This model was made for Era V and was sold with a load of "real coal".



Märklin's new exhibition layout for Z gauge offers appealing motifs. It is worth it to stop and "travel around" with the eye.

The reddish-brown livery still shows a condition before the repainting to the company's own factory colours. For collectors, it is particularly interesting that this exhibition train car is printed differently on both sides.

Also worth seeing was the still quite new exhibition layout for our scale, on which several harmoniously arranged trains made their rounds. So, it caught our eyes, and we also took a look at the scenes depicted on it.

In the showcases there were, as usual for the market leader from Göppingen, samples of the current new products to see. Among them we found a model of the Era III version of the "Hasenkasten", which was delivered for the first time for Era IV a few weeks ago.

But before we turn to other Z-gauge exhibitors, we would like to take a look at three selected layouts that we particularly liked. We want to mention "De blauwe trein" ("The Blue Train") by Loek Bronkhorst, a former On traXS exhibit that works with enormous depth effect through a perspective representation.



What is also special about this exhibit is that all means of transport, i.e., not only trains, but also trams, cars, ships, and even cyclists, are shown in motion.



On the layout "De blauwe trein" (photo above) almost everything moves, controlled as if by magic. On the small layout "Waldeslust" (photo below), at least the track gauge of 6.5 mm is correct from our point of view, but not the implementation scale.

From our point of view, at least the gauge of Bertram Kauert's H0f field railway "Waldeslust" was right, if not the scale. The builder, who did not seem to be averse to Z gauge, took this hint with the hoped-for humour.





The “Châtenoy le Royal” layout by Rail Modelisme Chatenoyen from France was one of the biggest eye-catchers at Intermodellbau 2023.

The H0 scale layout “Châtenoy le Royal”, presented by Rail Modelisme Chatenoyen from France, was an ultimate highlight of the Intermodellbau. This particularly scenic and perfectly designed layout showed motifs from the home country of the club members and was exhibited in Germany for the first time.

## Z gauge at the exhibition

Contrary to isolated and first impressions, there was a lot to see at Intermodellbau 2023. The already mentioned layout by Burkhard Merz should be mentioned here, as an example. For the manufacturer's, this is mostly reflected in cross-gauge programs.



The remaining examples of the 181<sup>2</sup> series held back by Noch were sold at the Intermodellbau at an unmatched, rock-bottom price. Accordingly, these last examples were sold quickly, including this oriental red version.



Viessmann, with its Kibri and Vollmer brands, may be considered proof of this, as this manufacturer is also active in the field of lighting and electronics. They also had samples of the new Polyplate kits in their baggage, which were not presented in showcases, however, but were explained to guests at the booth in conversation.

In relation to our Z gauge, this would certainly have been a visitor magnet for some uninformed people to get personal impressions, because the Vollmer kit of the “small workshop” is already sold out in the first edition at the factory – which speaks for a good demand.

The Faller booth made it probably easier for the exhibition visitors. Here all the new products were on display in dioramas and so they also got an impression of how the “old spinning mill” will look in the layout environment. This building will certainly be used credibly in other functions in the city environment, for example as an administrative building or “old grammar school”.



At their booth Faller showed the “old spinning mill”, their already announced main new product 2023 for Z gauge. On the fair diorama the manufacturer himself shows that this building can also be used in other functions.

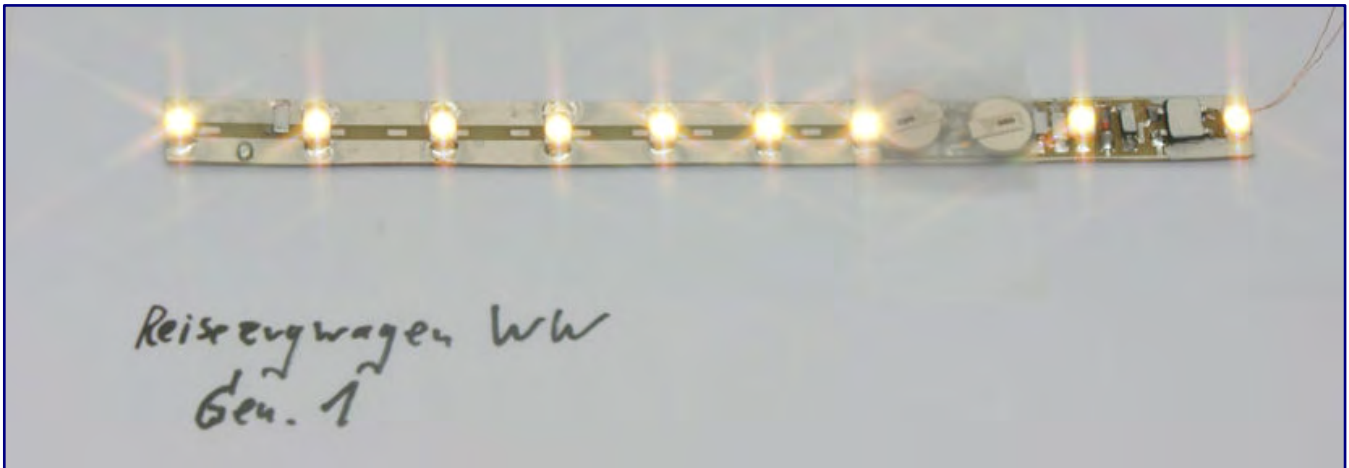
Those who favour railway travel will have found their way to the stand of Bahnreisen Sutter. As is standard there, a small suitcase layout with a train on a scale of 1:220 served as a symbol and eye-catcher. You can see it in our trade exhibition impressions from episode 13 of **Trainini TV**.

The new figures from Noch, which are produced with the help of the latest 3D printing technology, are also discussed in detail there. In the interview, Dr. Rainer Noch, as the internal idea generator, provided what he finds so special about them and what status they have in the Noch overall program. Use the contribution on our channel to be comprehensively informed about this.

But the booth was not only worth a personal visit because of these and other innovations. There was also a sale of remaining Rokuhan models of the 1812 series in all six design variants in moderate quantities.

Apparently, these were the models that had been held back in case of warranty claims. The rock-bottom price suggests that urgently needed storage space should finally be made available.

We were also happy to be guests at the Uhlenbrock booth. Ever since its release, the Intellibox from this company has been one of the favourite command stations of digital railway fans in the nominal size Z. The current generation of the IB 2neo follows on seamlessly and can now be adjusted to the appropriate track voltage by the customer. In addition, it is also fully up to date with integrated WLAN. We will, therefore, present it in more detail in one of the following issues.



Michael Boost displayed a selection of his lighting solutions in a showcase. Here, we have put his intensely buffered first-generation passenger coach lighting in warm white into focus. It can be individually shortened to three LEDs to be able to illuminate short carriages, as well.

Like Rüdiger Uhlenbrock, Jürgen Schirmer from Peter Post Werkzeuge was also our interview partner. We presented the new products of this tool specialist in the February 2023 edition. In the meantime, Microscale products have been added, which every model railroader will need and appreciate, sooner or later.

In our current episode of **Trainini TV**, the owner personally presents his new products, explains an also frequently requested tool, and speaks about what makes his sales so special – good tools are simply not the classic of electronic sales and he has adjusted to this appropriately.

Not to forget the manufacturers Artitec and Kuswa. The Dutch accessories specialist mainly had the distribution of its own assortment in mind, but was of course also available to answer questions about the new products for 2023.

With Gerd Kuswa, on the other hand, there are no off-the-peg products, because his business in our scale is individual vehicle lettering, which is placed and produced according to the customer's wishes.



Michael Boost's range also includes end-of-car lighting, demonstrated here on a funnel-flow tank car and the VB 141 rail bus trailer.



Kersten Tams had some not yet announced new products in his luggage. It is also worth knowing that the production of the popular Heißwolf speed controllers will continue under Tams. In the near future, however, the focus will be on updating the inner workings to available and affordable components, as the electronics market continues to change rapidly.

The power splitter was newly introduced as a circuit board or in the housing of the company's own central unit. It makes it possible to use the booster current even with small nominal sizes: This is divided into two or three sections with 2 or 3 A each.



Michael Boost has also looked at Märklin's party wagon (entertainment coach), and its interior lighting can now do a lot more than it did as initially delivered, including the continuous light captured here.

It can be used with all boosters, absolute synchronicity between the booster sections is guaranteed. Optional connections and setting options guarantee that individual wishes can be fulfilled. On the manufacturer's pages, all the necessary information is already available and can be found if the product designation is selected as the search name.

We will only briefly mention other hidden Z gauge exhibits here, because they can be seen in moving photos in our channel contribution already mentioned: There was a tiny layout under glass at the fire brigade's model construction groups in Hall 6. Also, to be seen at previous exhibitions was the 1:220 scale shuttle traffic on the trailer of a lorry at the RC Truck Car Club Dortmund.

## A grand finale

The most important contact point for Zetties was certainly Dietmar Allekotte's booth. This was also where appointments were made for meetings and talks, which meant an interesting exchange of information for us as well.

Meanwhile, the man from Mülheim in the Ruhr region dutifully steered the shuttle traffic to his new layout "Waterkant", designed as a showcase. As the name suggests, the motifs are again those of the North Sea coast. A small station at the jetty forms the focus and centre of attention, but the roaming view through the landscape promises many impressions of lovingly designed scenes that still remain hidden at first glance.



We conclude this report with a bit of a night-time atmosphere: On Dietmar Allekotte's "Waterkant" layout, a couchette coach has arrived prior to the first ray of sunlight, reaching its small destination station as a through coach. The extreme early risers, who seem to expect something from bathing in the North Sea at night, seem rather curious.

Much to Dietmar Allekotte's surprise, the booth was also equipped with a showcase. So it was also necessary to fill it attractively. Michael Boost took on this task and presented a miniature layout and a few diorama-shaped display pieces.

They served as a scenery for his own electronic developments and digital conversions. Worth mentioning here is the interior lighting of the Märklin entertainment wagon, which was able to reproduce both a continuous white light and lively light organ simulations. On the single-axle rail bus luggage trailer, once called VB 141 by the railway authorities, the red tail light glowed.

To get a full overview of his customer offering, you will find a link in the information box at the end of this article. Still without a date, it is also planned to present one of his wagon lighting systems through a conversion article in this magazine.

This edition of the exhibition was again complemented by the book offers of EK-Verlag and Geramond/VGB, as well as the specialist bookstore Stiletto, where the new publishing house Süd-Nord-Express was also to be found. The latter has meanwhile expanded its range to include books and DVDs. Worthwhile, for example, is the film series "Westphalian Steam Impressions" made from historical material by Ton Pruissen.

This concludes our tour of the exhibition. It remains impossible to summarise all the impressions and exhibits in a single report. Dortmund is always worth a visit, because nowhere else is it possible to look so far beyond one's own nose. This is also why we have compiled a parallel film report with other focal points to complement this summary: Episode 13 of **Trainini TV** is already waiting for you!

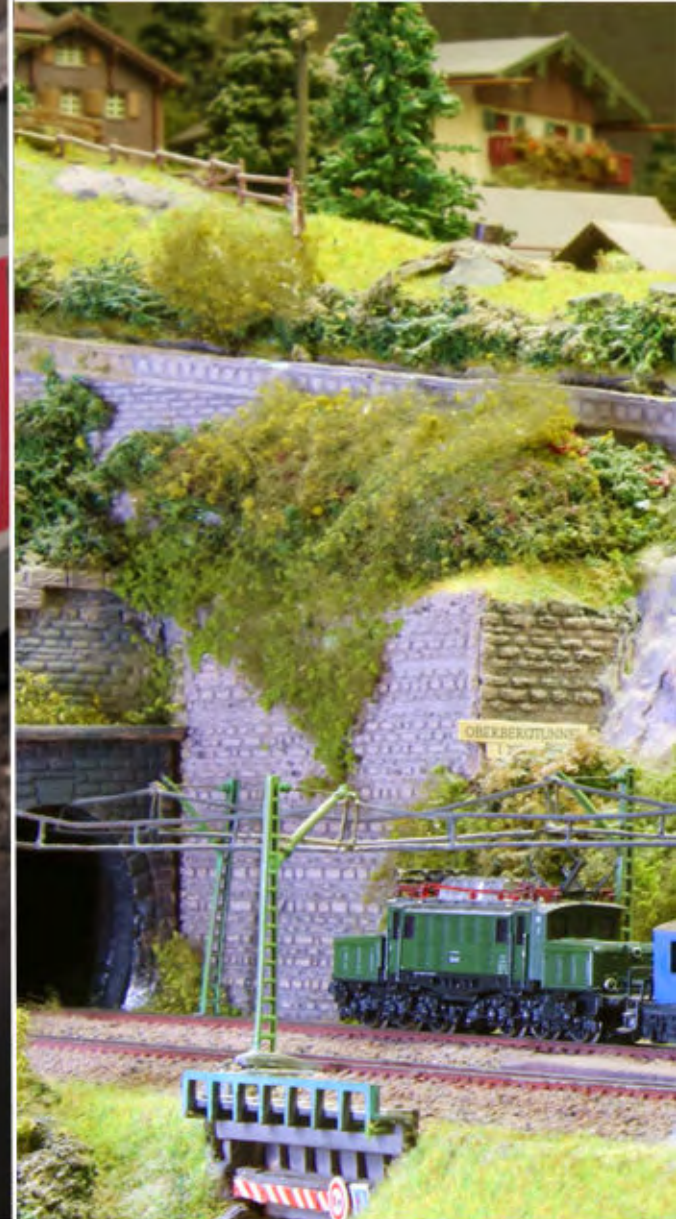
**Selection of Exhibitors with Z gauge relevance:**

- <https://artitec.nl>
  - <https://bahnbuch.de>
  - <https://www.busch-model.com>
  - <https://www.eisenbahn-kurier.de>
  - <https://www.faller.de>
  - <https://www.geramond.de>
  - <https://imprintium.de>
  - <https://www.kuswa.de>
  - <https://www.luetke-modellbahn.de>
  - <https://www.maerklin.de>
  - <http://www.michas-moba.de>
  - <https://www.noch.de>
  - <http://www.peter-post-werkzeuge.de>
  - <https://www.rokuhan.de>
  - <https://www.nordsuedexpress.de>
  - <https://tams-online.de>
  - <https://viessmann-modell.com>
  - <https://www.vgbahn.shop>
- Trainini TV Folge 13:**  
<https://www.youtube.com/TraininiTV>



# SONDERAUSSTELLUNG KLEIN TRIFFT GROSS

50 Jahre Baugröße Z, 1:220  
19. März 2023 bis 17. März 2024



**märklin**



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## Besuchsinformationen:

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78713 Schramberg





## The 11<sup>th</sup> construction phase at Miwula **On to the End of the World**

***On 3 May 2023, the eleventh construction phase was opened in the Miniatur Wunderland. Its construction began as early as 2019, when work on Rio de Janeiro had long been underway. An exciting journey now leads further across Patagonia to the Antarctic at the end of the world. We were there for the opening ceremony and report on what you can expect during a visit to Hamburg.***

It was less than a year and a half ago that we attended a ceremony in the Miniatur Wunderland Hamburg where the first part of the South America section was opened with Rio de Janeiro and lots of glitter and samba dancing. At that event, we could already see a floor area that was to be transformed into Patagonia and Antarctica by 3 May 2023.

Of course, the tinkerers and model builders don't allow themselves any rest, and, so, there is always something new to marvel at here at irregular intervals. But while the Formula 1 race in Monaco is still causing frowns, the South America section progressed more quickly. And then, suddenly, a mysterious letter arrived in our editorial office.



**Three, two, one, go! Together, Frederik Braun, Katharina Fegebank, Arved Fuchs and Gerrit Braun (from left to right) sound the foghorn to start the first thunderstorm in the Drake Passage, which opened the new Patagonia section.**

The sender was Base Primavera, a research station at Hope Bay in Antarctica. It reported on abandoned penguin colonies that could not be explained scientifically and asked to “hatch” the egg that had been collected and sent along. Should a little penguin be given the gift of life, and would probably provide a clue as to where its colony was so that it could be brought back?



The destination, which the Preiser figure of an emperor penguin revealed, was of course the Miniatur Wunderland Hamburg, the letter in the small package an invitation to the opening ceremony. The three of us gladly accepted and travelled to the port city to take a sufficient number of photos and to prepare another episode of **Trainini TV**.



**Call for help from Antarctica: The invitation itself was particularly creative. The first step was to “hatch” the enclosed penguin egg in order to obtain further information. In the first step, Miniatur Wunderland did not reveal itself as the sender.**

The German polar explorer Arved Fuchs was the guest of honour and we were literally glued to his lips as he talked about his journey to and through Antarctica in 1989. He was one of only a few people who managed to reach both the North and South Poles on foot within a year. What he saw and experienced there, he impressively arranged in the now eleventh construction section.

Before that, however, Katharina Fegebank, the second mayoress, took a look back, and told how the Braun brothers once spoke of their idea for this show installation over 23 years ago. Today, “Miwula” is Hamburg's most popular tourist attraction and is visited by guests from all over the world. That's why the people in charge come up with so many great ideas.

65 m<sup>2</sup> were added at the beginning of May, pampas and eternal ice are waiting in the South America section for the curious onlookers striding across the glass bridge. What the Martinez family from Buenos Aires built on site and then shipped to Germany, only to be assembled, fitted and further equipped on site, is once again without equal.

A completely new lighting control system and impressive projections skilfully set the scene for the impressive landscape. After all, the south of Argentina, the Drake Passage and Antarctica demand different attractions than the dazzling Rio de Janeiro. Where hardly any people live and endless expanses determine the photo, other highlights are needed to captivate and inspire visitors. Push-button actions are, of course, still part of it.



Within a few minutes, a terrible thunderstorm is about to hit. The swell in the Drake Passage is already getting much rougher, and the sky over the horizon darkens ominously – a technical tour de force and the highlight of the new section.

Frederik Braun told us about how he was lucky enough to travel through Patagonia himself 20 years ago. The photos have stayed with him ever since and also influenced the construction of the Pampa with its temperate and rainy climate. Ricardo Martinez and his family members, who are enthusiastic model builders, were responsible for ensuring that travellers from South America would also recognise their homeland as accurately recreated in Hamburg.

50,000 working hours went into the latest part of South America, and the construction costs of 2 million euros even exceeded the mark of the previous premiere section. What distinguishes Patagonia and Antarctica in the prototype and model becomes clear from the following figures: 200 hand-made figures add up to just 410 inhabitants together with the industrial ones. But they are matched by 2,000 animal figures.

### Familiar and yet foreign: Patagonia

The vegetation of Patagonia looks quite familiar to Europeans and is reproduced by 3,000 trees. Points of light in the expansive landscape are set by around 1,500 light-emitting diodes in the regular night simulation, which also uses indirect light to bathe the outlines of the mountains first in warm, then cold blue light. The beauty of the mountain and pampas scenes is thus beautifully accentuated.

A premiere is the lighting system with the described effects, which was installed for the first time in a section of the layout. It makes for quite noticeable differences to the previous installations, which were also very impressive. Patagonia and Antarctica have become a section whose special charm is largely due to elaborate technology.



This is easy to imagine, because we have already mentioned the sparse human settlement. And, so, the railway again plays a rather subordinate role in Patagonia, although it is indispensable as a binding link and for opening up the endless expanses for man. Of course, Antarctica, like the real southern polar region, has no rail transport at all.

In southern Argentina, 46 trains with 195 cars run on 353 m of track. That is surprisingly little by Miwula standards. This is also clear from the total number of only three signals that regulate rail traffic there.



In Ingeniero Jacobacci on the edge of the Pampa, the wide-gauge Tren Patagonico and the narrow-gauge “La Trochita” meet each other, even sharing the track in sections over a three-rail track. The new lighting control sets the mood even more impressively than we have long been used to in Hamburg.

However, it is definitely a highlight, because at the Ingeniero Jacobacci junction, broad gauge and 750 mm narrow gauge meet. For a few metres, both railways even leave the station on a three-rail track. Meanwhile, life in the small town is tranquil and unagitated.

And while trains of both gauges come and go almost every minute, we watch the work in the locomotive stations of both railways. On the Tren Patagonico it seems to be quieter, on the narrow-gauge “La Trochita” (“The Little Train”) we get a glimpse into a workshop hall and not far away we see decommissioned vehicles rusting.

The tourist magnets of Patagonia, which are known far beyond South America's borders, form a recognisable thematic focus. Fitz Roy, the highest spur of the Andes (3,406 m), is one of them and attracts climbers, in particular. In Hamburg, however, it attracts attention because of a spaceship crash, as above the snow line we can see the remains of a space ship that crashed into the rock.

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**Photo above:**

High up on Fitz Roy, a spaceship crashed, as brought into view by the glowing thrusters at the push of a button. As Patagonia's highest mountain in the Andean foothills, this mountain rises to well above the snow line.

**Photo below:**

Further down the barren valley, the Tren Patagonico passes a family of Nandos. Meanwhile, the South American ostriches are not impressed by the train. The large ratites are used to the train traffic, and are not startled by it.





The passenger train has moved on in the meantime and crosses the Rio Chico on a bridge (photo above). Below, anglers on a rubber dinghy are trying their luck. Close to the water, the vegetation is much denser. What kind of view might the mounted gauchos on the rock massif have over the wide river valley (photo below)? Further back we recognise the dinosaur excavation site. Barely visible is the large dog that has snatched an equally large bone.



Otherwise, few people and a herd of nandus determine life here. On Rio Chico, anglers wait for a catch, while some gauchos on horseback enjoy the view over the river valley to the highest mountain from a high rock massif.

Down in the valley, researchers are digging for the remains of dinosaurs, because this is the area where the world's largest species have been found. A dog is happy about the huge bone, which he must have been able to carry away unnoticed.



**The calving Perito Moreno Glacier is also a technical masterpiece – this applies to the model as well as the original. Tourists flock to the viewing platform to capture the typically blue ice masses advancing and breaking off in the photo. And it's hard to get any closer to this attraction in the Miniatur Wunderland!**

The technical highlight of Patagonia, however, is the Perito Moreno Glacier, whose inland ice pushes towards the sea over large areas with a striking blue colour, even in the model. Once there, they break off as icebergs with a loud thunder.

This “calving” of the glacier was realised with a lot of effort and tinkering. The impressive sinking of the broken-off part in the water is accompanied by the rising of countless air bubbles. The mini-visitors on a tourist platform can also experience this hustle and bustle, which the operators are still a little critical of.

Both Gerrit and Frederik Braun were basically satisfied with the regular demonstration, but they also stated that another development goal was to speed up the demolition process, and, thus, make it look more spectacular.



## Granite towers in the mountains

Our gaze now moves on to the Torres del Plaine (“Towers of the Blue Sky”), whose granite massifs also rise high above the edge of the complex. In this national park, the land is probably at its most pristine: a wilderness of Andean condors, free-roaming llamas and the puma.

The Mapuche people, the indigenous people of this region, also belong here. They are being pushed further and further back, and therefore deserve to be taken into account. The layout also shows their simple life and a small demonstration of this population group for their rights.



The striking granite massifs of the “Cuernos del Plaine” tower high up to the ceiling. The railway line and the road with the VW transporter seem tiny in comparison.

Above us, we recognise the mountain massif “Cuernos del Plaine” (horns). Indeed, they look like two horns in the middle of the mountains – no question, Patagonia has extraordinary charms to offer. Tourists are more likely to be drawn to a cave discovered in the late 1890s, where an extinct species of giant sloth used to live. At the push of a button, its spooky-looking shadow casts itself visibly towards the cave entrance, frightening visitors.

Not far away we discover a gaucho settlement, developed by “La Trochita” and economically determined by sheep breeding. The animals even populate the narrow-gauge tracks here. When a train approaches, the cattle have to be herded off the tracks – in Miwula this happens automatically.





**Photo above:**

Llamas belong to the “indigenous people” of Patagonia and are, consequently, also found on the layout. Camping in the wilderness here can be life-threatening, as the couple visible in the tent after dark are probably not sufficiently aware. Have you spotted the puma, well camouflaged by the colours of the landscape, lying in wait for prey not far away?

**Photo below:**

This photo contains a search game. The aim is to find the animal pyramid that some sheep, defying the threat of their last journey to the slaughterhouse, have formed in the fence.

No one is moping around here. Even the sheep, waiting in gates to be loaded into cattle wagons for their final journey, play tricks. They try the nerves of the gauchos with a high pyramid of animals they have formed. Children will recognise this scene from a popular series.



Within sight, the police are busy at the same time, recording a small rear-end collision on the edge of the gaucho settlement. In the process, it seems to escape her notice that bank and train robbers Butch Cassidy and the Sundance Kid are also up to mischief here on the outskirts of town. The two criminals, who have fled to South America, lie in wait for unsuspecting travellers on “La Trochita”.



Near the gaucho settlement at the foot of the “Torres del Plaine” mountains, the paths of the Tren Patagonico (above) and “La Trochita” (below), which has a station in the village, cross. Just outside the frame, the criminals Butch Cassidy and the Sundance Kid lie in wait for the train passengers.

Meanwhile, the gauchos in the village are celebrating their day's work beside the railway station. Only one of the gentlemen feels on the verge of despair: his donkey seems to have gone on strike and so he carries the grey animal piggyback to the stable at home.

The nature outside the village seems rather barren. Here we find a herd of llamas and gauchos at the campfire, who have obviously taken off their sombrero and poncho. The couple, visible in the lamplight in a tent at night time, seem unaware of the dangers of the wilderness.

We now make a big leap back to Ingeniero Jacobacci and the adjacent pampas. Arved Fuchs impressively explained how quickly and frequently the weather can change here, the seasons seem to change in a single day.

But this area now leads us to the Drake Passage, which connects the Pacific and Atlantic Oceans at Cape Horn and also separates Antarctica. If you want to get there, you can't avoid this sea route and it is famous among sailors for its storms and rough seas. In **Trainini TV** episode 14, we let the German polar explorer have his say on what the crossing means.

## Technical masterpiece

For the technicians of the Miniatur Wunderland, this Drake passage was an enormous engineering challenge. Gerrit Braun told us that the decisive idea came from Ricardo Martinez, the “head” of the family from Buenos Aries who helped build it.

The moving frame made of slats, covered with blue fabric and illuminated with elaborate projection technology, was then developed and produced ready for demonstration in Hamburg. One of the two largest machines in the world's largest model railway layout provides the necessary up and down of the wave crests and troughs, spray and water bubbles are projected onto the blue fabric.



**Looking through an iceberg at an emperor penguin colony: On our journey through Miniatur Wunderland, we have arrived in Antarctica.**

Gerrit Braun let us know that they wanted to have highly realistic-looking water without using real water. And, so, we don't get wet during the performance. In a rhythm of about seven minutes, a lightly undulating sea turns into a heavy thunderstorm that throws the ships through foaming and splashing waves. On two of them, a mobile phone may be jammed in to film and share the rough seas.

However, we would like to cross over to Antarctica, where there is guaranteed to be no railway. Arved Fuchs explained that it was by no means a white wasteland; on the contrary, it was well worth seeing. Of course, there are only penguin colonies in the areas near the coast.

He passionately told the audience about his crossing of the ice desert together with Reinhold Messner in 1989, during which they also reached the South Pole. They traversed the 2,800 km route with the help of skis. With maximum summer temperatures, we're talking just -25°C!



He described the environment there as threatened and fragile. His great concern is to create awareness for this indescribable beauty, which is worth protecting, and, therefore, counteracting global warming. As an indication of this, an ice-cream vendor in the model area distributes his refreshments to the obviously sweating emperor penguins.

If all the ice in Antarctica melted, sea levels would rise by 60 metres worldwide, Arved Fuchs gave those present something to think about. The evil that man is doing here is clearly demonstrated to us by a whaler who disguises his hunt as research. Fortunately, environmentalists thwarted his plan and saved the hunted from the harpoon.



After the ship is completely encased in ice and begins to sink, the crew of the “Endurance” starts a race against time in extremely hostile conditions.

A little away, we witnessed another dramatic struggle for survival: we saw the Endurance expedition there, which failed in 1914. The sailing ship is completely encased in ice and will soon sink. For the crew, who managed to save themselves on “solid ground”, the fight against extremely hostile conditions in the deep sub-zero temperatures begins.

Underneath the ship is a small ice cave in the system's subsoil. It is almost only accessible to the smallest visitors. Inside, they will find three display cases that show what is hidden under and in the eternal ice. Besides a frozen dinosaur, they discover the film hero “Scratch.” The brave, prehistoric squirrel clings stubbornly to his captured acorn.

continues on page 59





In the small ice cave (photo above) we become aware of what the eternal ice holds captive: We discover a frozen Tyrannosaurus Rex and Scratch, the prehistoric squirrel from the movie series. Further up, a whaler is on the move (photo below), whose mission conservation organisations want to thwart. An environmentalist raises his sign in warning: "Is this really research, or is it just commercial interests in disguise?"



There is also good news here at the end of the world. At the end of our Antarctic journey, we met Base Primavera, where the call for help came from that brought us to Hamburg. It is used to research the life of the emperor penguins, who seem to know no shyness here.

In the station they are up to tricks, on the floes in the sea they indulge in a happy life. We were happy because the little penguin that we were supposed to “hatch”, would find its home here among its fellow penguins.



The call for help that led us to Hamburg came from here, and it is here, at the end of the world, that our journey through Antarctica ends: at Base Primavera, not only is the life of the emperor penguins being researched, here, the clumsy fellows are also up to some mischief.

So the circle closes, and we look forward to what is still to come in Hamburg. The Andes, the Atacama Desert, which is already under construction, and, finally, the Caribbean invite each other to further journeys here from the end of 2024. We are looking forward...



The site of the world's largest model railway layout:  
<https://www.miniatur-wunderland.de>

Readers' letters and messages

## Zetties and Trainini in Dialogue

**Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.**

### Future Readers in France?

I am the webmaster of a French blog called <http://blog.ptitrain.com>. I am also a beginner in Z scale, after many years in HO.

I discovered your magazine and will see only English and German version is available for the moment. I can read English, but most of French model railroaders don't.

I tried to google translate your last issue. (...) Do you plan to publish a French version in the future? Do you need help to do the translation? I'll will be pleased to help you.

Christophe Franchini (France), per E-Mail

Editor's reply: A French version of Trainini® would certainly have its special appeal, because then we could cover all three most important languages in the united Europe. Unfortunately, however, we do not have any well-founded language knowledge in your field in the entire team, nor do we have any free resources. For the proposed idea, we would need an additional team of at least three translators to be able to produce a French version reliably and permanently.

Since we enjoy observing what has been developing in the French market for Z gauge in recent years, we alternatively suggest that you enter into firm correspondence with us and provide us with information and the state of development. By the way, our magazines are deliberately designed as PDF documents in such a way that anyone interested can copy the text passages into a translation programme themselves, in order to be able to absorb all the information in their native language.

### Enthusiasm for Ratimo products:

The new Trainini® is certainly already in the works. However, you should not miss to introduce the trailer trolleys for the train-platform trolley by Ratimo. The models are great and the highlight is the movable drawbar! If I can suggest something, then the trailers would be a clear candidate for me for a model of the year in the accessories category.

If I can suggest something, then the trailers would be a clear candidate for me for the best model of the year in the accessories category.

Rainer Hecker, Düsseldorf

Editor's reply: We are pleased about this letter and are sure that this new product deserves a detailed report. Unfortunately, we have not yet received any information from Rainer Tielke and have not yet had the product in our hands, a basic requirement for a personal evaluation that can lead to a nomination. Also, the manufacturer's pages were not active at the time of our review. We will be happy to fulfil with your request as soon as we have been able to inspect and test what you have described.





**Final clarification of a reader question:**

In the last issue we responded to the alleged error on Herpa's Douglas DC-6 of Pan Am, which Uwe Pfannenschmidt had reported to us. We were already able to report that the mirror-inverted representation of the US flag is prototypical.

Our translator Martin Stercken was finally able to explain this phenomenon: A flag is depicted on the aircraft. The use of such flags at the stern originates from the shipping industry, from which large parts of the light signals were once adopted. The close connection is also expressed in terms that are still used today: The best example is the airport.



**This prototype photo proves the correctness of the flag display on the right side of (not only the) Douglas DC-6 aircraft. Our translator provides the explanation as to why this is the case. Photo: Jon Proctor (GFDL-1.2)**

Therefore, we should imagine the flag in fabric form on a mast, as we know it from the stern of ships, in order to understand the display on aircraft. The non-transparent fabric is not printed differently on both sides, but shows the flag mirror-inverted on one of the two sides. This was, therefore, deliberately adopted for aircraft, as well.

**Current topic shift:**

For this issue we had planned a test report as well as a supplementary prototype article on the class 01.5 express steam locomotive from the former DR stocks. Due to the large number of topics in this issue and time constraints, we have had to postpone this at short notice.

The final trigger for this decision is that shortly before the editorial deadline, it became known that reworked models (item no. 88019) are now being delivered by Märklin because of a missing detail.

We would therefore also like to give the manufacturer the chance to include a corrected model in the test. This is to ensure that a useful judgement can be made for our readers and his customers alike. We will also include the Bahls Modelleisenbahnen set of accessories.

## An obituary for Klaus Hammesfahr:

On April 20, 2023, Klaus Hammesfahr (born on February 12, 1930), a veteran of Z gauge, left this world. We would like to summarise and pay tribute to his work in the age before the achievements of the internet. Together with Jochen Brüggemann and Alfred Geifes, two of his members from the ZMRR, we have compiled and summarised his commitment and achievements.

Klaus Hammesfahr was there from the very beginning when Märklin launched the Mini-Club. Visits to exhibitions and fairs, where Z gauge was still a marginal product, quickly brought him into contact with other enthusiasts of the new gauge.

His first layout in the size (1.80 m x 4.80 m) was a tour de force in design and control. When he moved to another house, it was lifted out of the attic flat on a crane and into the new flat again. His credo was "First came the landscape, then came the railway."

From about 1980 onwards, he began to actively search for like-minded people in order to create a permanent exchange of experiences and to promote the idea of a modular layout. The destinations of his trips were Münster, Bochum, or Wuppertal.

The following monthly meetings met with lively interest and were therefore well attended in the first years. They appealed to both beginners and experienced Z gauge railway enthusiasts alike, some of whom travelled from as far as southern Germany.

In 1993, his most important project, a modular layout in Solingen, bore fruit. An area of 7.20 x 0.50 m could be filled and operated, which meant the birth of the ZMRR (Z-Module Rhein-Ruhr). In the following years, the layout could be seen at exhibitions in Solingen, Dortmund, Bochum, Geseke, Göppingen, Cologne, Stuttgart, and also Genk (Belgium). Later, the layout grew to a total length of 14 metres.

This was the most active phase of the ZMRR, which Klaus Hammesfahr was instrumental in bringing to life, and during which today's **Trainini®** editor-in-chief was able to get to know and appreciate him. He passionately promoted Z gauge, collected experiences and passed them on to others.

With regard to the modules, Klaus dedicated himself to eliminating "teething troubles", making technical improvements, and, above all, designing a sophisticated landscape. The ZMRR and Klaus Hammesfahr were prominent figures of the Z gauge, known to the friends of this scale.



Klaus Hammesfahr am 15. September 2007 während der Märklintage in Göppingen.  
Foto: Alfred Geifes



From the turn of the millennium onwards, it became quieter. This was due to the lack of a place to set up the layout permanently, but also to a partial decline in interest among the model railway enthusiasts organised here as a result of more club meetings closer to home or of relocations. However, Klaus Hammesfahr remained true to his second passion, the original railway, with trips to interesting railway lines until the end.



The ZMRR visiting the Z-Stübchen Krefeld: Of course, Klaus Hammesfahr (3rd from left) was also there. Photo: Alfred Geifes

In the spring of 2012, the ZMRR module layout nevertheless reached its greatest state of expansion before the assembly site that had been found in the meantime unfortunately had to be abandoned. In 2014 it was sold and built up by the members for the buyer.

The ZMRR disbanded for reasons of age, and it also became quieter around the already elderly Klaus Hammesfahr, who contributed to the best of his ability until the end. The contacts of the former carpenters continued.

Apart from model railways, children were always particularly important to the deceased. As a former teacher and headmaster of a primary school, he continued to tutor pupils in reading and writing German after his retirement. He always knew how to combine his family, his job, and his hobbies over the years.

On 20 April 2023, his life candle sadly went out at the ripe old age of 93. We are sad that a pioneer of our model railway greatness, energetic Zettie, and socially committed person had to pass away.

Everyone involved in these lines will miss him, keep him in good memory and are grateful for the wonderful hours he spent with everyone. May angels escort him to paradise.

## Delivery momentum from Märklin:

With the merry month of May, Märklin deliveries are suddenly picking up speed again. Since the models had not passed the final quality control, the V 320 001 (art. no. 88320) still underwent changes before the Insider Club models 2022 now arriving at dealers one after the other.



The Insider Club model 2022 in the form of the V 320 001 (item no. 88320) recently began delivery. So far, however, only a small partial order quantity has arrived at the dealers.

The new products that have now been delivered have a convincing appearance and impress with many details, their high weight, and clean lacquering and printing. The detailed test report on this new mould is planned for the June issue.



The Pop car pack (87402) contains the rarest of all colour variations of this short-lived concept with the two couchette cars Bcüm 243.

After two years of waiting, the “bunny hutch” BDNf 738 in an MHI car pack (87189) has also begun delivery. It is complemented by two other silver pieces of the types ABnb 703 and Bnrzb 719. The new model is presented in detail at the beginning of this issue.

Meanwhile, the two Bcüm 243 couchette cars from the “Pop car” express train set (87402), which are intended to complement the MHI starter set from the 2022 anniversary year, provide a splash of colour. They carry the basic colours RAL 4005 blue lilac and RAL 4002 red violet from the pop colour concept and reproduce a very rare paint scheme that was only used on three (blue lilac) and two cars (red violet).



The very appealing and eye-catching duo is supplemented with a train run to Emden (Emsland line) by a 2nd class pop colour Büm 234 (cobalt blue/pebble grey).

**Additions to the Insider model:**

Almost traditionally, the 1zu220-Shop (<https://www.1zu220-shop.de>) adds a free gift to the Insider models ordered from them. Their value is always based on the price of the Insider model called by Märklin.



Additions of the 1zu220-Shop to the Insider model 2022 in Z gauge are the MAN 19.230 DHK (left) and the MAN 26.280 DFK (right), which were exclusively produced by Wespe Models. Both models cannot be ordered individually.

The customers who ordered the V 320 001 will receive two tipper truck models to be able to enhance their model layout streets with them. The blue models with red chassis were exclusively manufactured by Wespe Models. The MAN 19.230 DHK pontoon truck and its successor, the MAN 26.280 DFK front-steer truck from a cooperation with Saviem, served as models.

**Supply source for modular tables:**

Our editor Dirk Kuhlmann became aware of the pages of Modellbahnmodule of the owner Thomas Friedrich (<https://www.modellbahnmodule.de>). His range includes numerous standard, corner, and special modules that can also be used for Z gauge.

They are certainly suitable for those model railroaders who have difficulty with woodworking to size and do not want a fixed panel layout, but rather a configuration that can be assembled, as desired. The module boxes with flat surfaces for track and superstructure can be used to create module tables that can be assembled, as desired.

Thomas Friedrich from Lausitz describes himself as the market leader for this service.

**A lot of new items at AZL:**

American Z Line has reported many new deliveries in May and some very attractive models are once again among them. The ALCO PA1 of the Delaware & Hudson (item no. 64415-1 / -2), which has a blue Warbonnet design, has to be mentioned right away. It is joined by the EMD E8 of the Baltimore & Ohio (62618-1 / -2).



ALCO PA1 of the Delaware & Hudson (item no. 64415-1; photo left) and double pack for the Pacific Fruit Express (914830-2; photo right). Photos: AZL / Ztrack

R-70-20 refrigerator cars for the Pacific Fruit Express are available in two sets as reissues: two-piece (914830-2) and four-piece (904800-2 / -3). Still very young are the models of the three-piece Trinity RA column cars, each 53 feet long. They are intended for the transport of semi-trailers. As TTAX copies they now come with ABF trailers (905231-1), which can also be bought separately (954007-1).

The 13-car pack of the UP long-distance train "City of Los Angeles" (72400-1) will leave a gigantic impression on a layout. Three more cars can be added with an extension (72401-3). AZL marks all models as new releases, but due to a lack of detailed photos, we could not judge this more closely.

Therefore, we recommend a look at the manufacturer's website at <https://www.americanzline.com>.

#### Faller anniversary model delivered:

As an anniversary model "50 years of Z-gauge" Faller (<https://www.faller.de>) announced a three-part farmstead consisting of a house, a stable, and a barn. The three-sided farmstead (art. no. 282800) with different infills and roof coverings has now arrived at dealers.



Faller is currently delivering the three-sided farm (item no. 282800), which was announced as a new release for the Z gauge anniversary. A photo of the kit contents can be seen on the news of our website.

Also, included in this laser-cut kit are wall and picket fence elements that optionally connect the three free-standing buildings.

The 165 parts made of solid-coloured hard cardboard in nine different colours do not require any colour treatment, but can, of course, be patinated individually.

The kit also includes a window foil and a curtain mask, because, as usual with Faller, the installation of an interior lighting has already been thought of in the factory.

After unpacking, the individual parts have a clean finish and precise cuts, so they also promise the usual high accuracy of fit that we have always experienced with this manufacturer.



## Small series offer from Italy:

GMmodelli Torino is the name of a new small series manufacturer from Italy. The two namesakes, Guido and Mirko from Turin, have always been enthusiastic about trains and railways and are now launching a range in 1:220 scale. H0 and N gauges are also catered for.

The focus is on previously unavailable models of Italian prototypes, in Z gauge this is the prototype E444 001 FS locomotive, which is offered as a kit. Produced in high-resolution stereo lithography 3D printing, they are intended to be assembled by hand.



FS's prototype E444 001 locomotive is the new small-series manufacturer's first Z-gauge model: the finished model, shown here, uses the running gear of Märklin's old class 216 and pantographs from the same supplier. Photo: GMmodelli Torino

The customer then also applies a spray painting himself and equips his model with the help of supplied parts and decals. The delivery of the E 444 001 consists of the 3D-printed housing, buffers, coupling hooks of suitable length, and decals.

The kit has been constructed in such a way that it can be used without any modifications on the Märklin chassis of the model series 216 from 1972 (art. no. 8874 / 8875), which can be purchased cheaply second-hand, and similar ones. Other Italian Z gauge models are currently being tested.

Contact can be made via the e-mail address [GMmodelli\[at\]hotmail.com](mailto:GMmodelli[at]hotmail.com).

## Semi-relief houses via own brand:

Modellbahnunion (<https://www.modellbahnunion.com>) has introduced nine new relief houses for Z gauge in quick succession. They are all made of solid-coloured hardboard and are available immediately.

These new products follow the style of the Gründerzeit (1848/49 to 1873) and are intended for the rear edge of the layout at the transition to background scenery. Their models were once uniformly built houses that only differed from each other by differently designed facades.

A three-storey town house can be built from each of the different kits, the width of the buildings differs in some cases. The façades of the houses in the colours grey (art. no. MU-Z-H00191, MU-Z-H00192 & MU-

Z-H00195), white (MU-Z-H00196, MU-Z-H00197 & MU-Z-H00200) and sandstone (MU-Z-H00201, MU-Z-H00202 & MU-Z-H00205) are elaborately engraved.



We show one of nine Wilhelminian relief houses here. It is a version with a grey façade (art. no. MU-Z-H00191). Three other variants can be seen on our website in the related news item. Photo: Modellbahn Union

They show many beautiful details, which at the same time provide a varying streetscape. All the houses in the series can be connected to each other, as desired, to fill a complete streetscape, together.

#### Second part of the Busch new products:

The accessories manufacturer Busch (<https://www.busch-model.info>) announced more new products at the Intermodellbau. They primarily serve larger gauges, but we still found something for Z gauge.

For the company's 65th anniversary, for example, old trees are being reissued in large packs. "Old" here means the construction method as "bottle cleaners". Since, especially a few decades ago, trees were reproduced much too small, all the models shown can also be used without hesitation for scale 1:220.

These are then 65 firs (art. no. 9766), deciduous trees (9767), or other firs of the same number and smaller bushes (9768).



65 fir trees for N scale (Item no. 9768). Photo: Busch



This is rounded off by 65 deciduous trees and firs (9769) in partly flowering colours. All trees have heights from 30 to 135 mm, depending on the article.

**Continued from Micro-Trains:**

Micro-Trains continues the “War of the Worlds” series with the third wagon (art. no. 525 00 181). This time it is an older type of flat wagon on which an artillery gun is loaded.



This is how wagon number 3 (item no. 525 00 181) of the ongoing collector's series looks. Photo: Micro-Trains.

**Further new releases from Yellow Dwarf:**

The Czech small-series supplier Yellow Dwarf (<https://www.yellowdwarf.eu>) is expanding its range at a rapid pace. Since the last report, another six new products have been added, which are primarily intended for gardens and parks.



The “Pause in the Garden” (art. no. 60464; photo, top left) and the wooden pavilion (60471; photo, top right) promise cosiness on the grounds at home. The two statues (60475; photo, below left, and 60477; photo, below right) belong to a selection of four different objects. Photos: Yellow Dwarf

The “Pause in the garden” (art. no. 60464), which includes loungers, flower pots, and other equipment for a quiet afternoon in the sun, has been implemented. The wooden pavilion (60471) can provide shade, but also be a focal point in parks.

There, visitors are sure to come across the statues I (60475), II (60476), III (60477) and IV (60478). They offer different replicas based on Christian or ancient models.

**Summer new releases from Märklin announced:**

On 10 May 2023 Märklin announced its summer new releases 2023. Of course, Z gauge was also considered. The MHI “DB Netz” train set with the class 120 electric locomotive as the train horse (item no. 81528) was well received. In addition to the locomotive, this package for Era VI contains a RAILab 1 measuring wagon and a Dienstmz 307 driving trailer of Deutsche Bahn AG.



The MHI train set “DB Netz” (item no. 81528) with an electric locomotive and two monitoring wagons for Era VI immediately met with a good response. Photo: Märklin

A highlight of these new releases is also the electric locomotive E 41 374 (88355) of the Bundesbahn. It is equipped with five lamps, multiple ventilators (vertical lamellas) and a wraparound rain gutter. Its special colouring with beige frames and chrome oxide green body represented a colour experiment by the DB around 1966. The model thus belongs to epoch III.

With the train set “V 36 Railbouv Leerdam” (81771) Märklin turns to a foreign market. In addition to the yellow V 36 diesel locomotive, which is likely to represent a “Hippel” here, the pack also contains three turntable side unloading wagons.

In cooperation with AZL another variant of the diesel locomotive EMD E8A (88625) is released. It is assigned to the American company Amtrak for passenger train service and shows their early paint scheme variant.

Meanwhile, the manufacturers collector's series is being given a 7th part. The acid pot car with 12 pots (82310) has now been produced in bronze investment casting. As a contrast to the metal, they and the





The Bundesbahn's 50 2412 (88847) is already looking well ahead to Christmas with its replica chalk inscriptions. Photo: Märklin

brakeman's platform are made of black plastic, and the rest of the model (with the exception of the wheel sets) is made of unpainted bronze.

Already looking ahead to Christmas is probably the class 50 steam locomotive in the four-domed version with box tender (88847). The 50 2412 picks up the once common chalk messages, which were written and painted by persons on the wind deflectors and tender. The model is delivered in a real wooden box that plays the melody "Jingle Bells" when opened.

Classically represented in the summer new releases is the Märklin Magazine Annual Car (80833), whose design also received a good response. In the usual blue, the magazine's identifying colour, it is based on a KLV carrier wagon Sgns 691 of the Deutsche Bahn AG (DB AG), which is loaded with two 20-foot containers showing the inscriptions of the Märklin magazine. The wagon model is a 2023 mould new product.

#### **20 Year Anniversary for Modelleisenbahner Soest:**

On 13 August 2023, the Modelleisenbahner Soest (<http://www.mes03soest.de>) will celebrate their 20th anniversary at a summer party. After the founding of the club in 2003 and two necessary moves, the club's premises are now located in Bad Sassendorf in the Neuengeseke district.

More than 50 members operate a large H0 gauge layout, which is the heart of the association, as well as a digitally operated Z gauge module layout with 30 m of track. This is certainly worth a visit, which will undoubtedly lead to lively technical discussions.

From 11:00 a.m. to 5:00 p.m., visitors are welcome to Neuengeseke on 13 August in the hall at Neuengeseker Heide 1. We are also planning a birthday visit there.

## Attractive refrigerator car at Full Throttle:

In its series of reconstructed 34-foot food refrigerated trucks with wooden walls, WDW Full Throttle introduced a new model in attractive colours in May. It is a Grand Union version with striking green painted side walls. The car pack (item no. FT-9210-1) contains two examples with different road numbers and Bettendorf bogies.



The 34-foot “Grand Union” food refrigerator wagons from the new wagon pack (item no. FT-9210-1) look appealing. Photo: WDW Full Throttle

The new collector's pack “American Chemicals II” (FTPZ-COL63), which picks up two wagons with new road numbers that have long been sold off, is certainly also interesting. These are bulk goods wagons with cylindrical containers. One of each is on the road for Hooker Chemical Company (orange with black roof walkways) and U.S.I (black design).

In Germany, the models are available, for example, from Case Hobbies (<http://www.case-hobbies.de>).

## Mark your calendar for Märklintage 2023:

The 13th Märklintage in Göppingen, together with the 38th International Model Railway Exhibition (IMA), are once again to become a centre of attention for model railway enthusiasts from all over the world. From 15 to 17 September 2023, this unique event will once again take place in its usual setting.



15.-17. September 2023 in Göppingen

The exhibition venues will continue to be the Staufer Park, the EWS Arena, the Leonard-Weiss-

Areal, the Göppingen railway station, the Märklineum and the Märklin main factory. More than 100 well-known manufacturers from the model railway industry are expected to be there, as well as many attractive model railway layouts of different gauges, including, of course, the Z nominal scale.

An extensive entertainment programme for families, free parking spaces and free round-trip buses for visitors are all elements that the organisers have listed as a matter of course for this year's event, as well.



## 25th anniversary of a disaster:

On 3 June 1998, almost exactly 25 years before this issue appeared, the most serious railway accident in German post-war history occurred. When the ICE 884 “Wilhelm Conrad Röntgen” derailed in Eschede, a small town in the Lüneburg Heath, 101 people were killed.

Almost as many were seriously injured. Victims, relatives and helpers of this disaster often suffer from the consequences of this event to this day. That is why we would like to commemorate the 25th anniversary of the world's worst accident in high-speed transport.



Today, a memorial wall, with the names, dates of birth, and place of residence of all the deceased victims, commemorates the terrible disaster. It is surrounded by 101 cherry trees. Photo: Hajotthu (GFDL)

As a result of a broken wheel on the third axle of the first intermediate wagon, the defective bogie tore off a wheel guide of switch 2 of Eschede station and derailed. It then set turnout 3 to branch, which, together with the diversion and derailment of subsequent wagons, led to a collision with the bridge pillars, which caused the 200-ton bridge to collapse.

It buried parts of the ICE, while all the remaining carriages crossed and pushed together in front of the bridge debris. The rear power car hit the debris under emergency braking with the pantograph automatically detaching. Among the dead were two signal technicians who were standing under the bridge at the time of the accident.

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Board of Editors:  
Holger Späing (Editor-in-Chief)  
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English translation:  
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Publisher and Responsible in terms of German press law (V.i.S.d.P.) is Holger Späing, Am Rondell 119, 44319 Dortmund; Contact: 49 (0)231 9598 7867 or by e-mail to [redaktion\[at\]trainini.de](mailto:redaktion[at]trainini.de).

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# Trainini

German Magazine for Z Gauge



**Märklin's First Reko Steam Locomotive**

**The V 320 Test**  
**The diversity of Nature**

## Introduction

Dear Readers,

I am writing these lines on 21 June 2023: Summer solstice and, at the same time, the calendrical beginning of summer. Tropical temperatures, which give the feeling that spring was a long time ago, have already been with us for more than a week.

Today is the longest day of the year in the northern hemisphere, with well over 16 hours of daytime. This also fits in with my feeling that I have never experienced longer days, meaning, of course, everything that counts as work.

That makes the model railway even more important, as it creates that crucial feeling of free and self-determined time. However, you are doing, make the most of your time! This can happen in the cool basement doing handicrafts or on excursions from which photos and impressions are taken home.

That is what this edition of the magazine is about. A model railway layout should not be created at random or overloaded with possibly conflicting themes. Self-limitation is often the recipe for success. And to ensure that the landscape, whether created by nature or by man, has a harmonious effect on the viewer, we have put it in the spotlight today.

In our annual focus theme "Layout Details", we want to make everyday things conscious and specifically bring them into focus. For me to pass on my impressions to you, it was also necessary to go on excursions where I could capture my thoughts in photos.

But we are also focusing on train operation today because the last few weeks have been dominated by new series of models. Unfortunately, I would like to note this as my own impression, we have already asked for better model realisations for testing.

Certainly, the V 320 and the class 01<sup>5</sup> express steam locomotive are not bad, but both were delivered with serious deficiencies. And then it is also one of the duties of trade journalists to report honestly, comprehensively, and also factually.

Märklin has also reacted quickly and in one case has made improvements, in the other it has now started a recall. We hope that it will also be used to check and eliminate a defect that we have discovered.

However, a market leader does not have it easy either: he is very much in the customer focus and can only do it wrong. That is why we do not forget to praise the fact that we are always listened to in Göppingen, that we are informed and that we have already worked out many good solutions in dialogue. I say thank you, because I also see model railways as my hobby and contribution to the joy of life.

You can now look forward to a summer reading that is certainly not light fare. But it should be even more lasting, i.e., it should have an impact and not be forgotten straight away. This is rounded off, as usual, by two current reviews of works that have had an equally lasting effect on us. Enjoy!

Sin-Z-erely,

Holger Späing



**Holger Späing**  
Editor-in-chief



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We sincerely thank Collin and Markus Bauer and the Eisenbahnstiftung for their image support..

Date of publication of the German language version of this issue: 29 June 2023

### Cover photo:

01 519 has been caught on the Hindenburgdamm in front of a special steam train to Westerland. When the locomotive and its train set off on their return journey to Hamburg, the photographer caught sight of it a short time later.

DB V 320 001 for Z gauge

## The Colossus from Göppingen

*Most recently, Märklin focused its skills on small gauge Z models, which one would hardly have be-lieved to come into fruition. Then came the anniversary year 2022 and V 320 001 joined the celebration as an Insider Club model. It is, after all, the largest German diesel locomotive, and, when it appeared, it was also the most powerful single-frame diesel locomotive in the world. Now she has a large-series memorial in 1:220 scale.*

At the latest with the appearance of the tiny KlV 20, it seemed that Märklin wanted to test the limits of miniaturisation and motorisation in 1:220 scale anew. So, it came as a great surprise when for the 50th anniversary of the Mini-Club, of all things, the V 320 001 of the Bundesbahn (art. no. 88320) was announced as an Insider Club model 2022.



In the summer of 1964, V 320 001 is on the move with an express train. Since the “Silberlinge” cars were still high-quality material at that time, especially in comparison to old passenger coaches, they appear again and again in this type of train.

After all, when it appeared in 1962, it was the most powerful single-frame diesel locomotive in the world with 4 000 hp and remained the largest machine of this traction type ever to run on German tracks for decades. It was based at the Kempten depot, and later at the Hamm depot.

We reported on its prototype history and the Schmidt model, with which we will make some comparisons today, in **Trainini®** 6/2012. The file of this issue can be found in the licensed archives and at the German National Library. About three years after our portrait, “Wiebe 7” had to be parked due to axle bearing damage.



## Interesting facts about the prototype history of the V 320 001:

A V 320 was planned as a "double V 160" (diesel locomotive with two V 160 engine layouts) in the DB type programme. However, there was never a development and construction order from the DB in view of the rapidly advancing electrification.

So, Krauss-Maffei with the V 300 and Henschel with its V 320 sent their own designs into the race to win orders. While the DB took over the V 300, in the case of the V 320 there was only a longer-term lease with subsequent return to the manufacturer.

The machine proved itself well and was reliable. On some lines in the south of Germany, the class 218 had to be used in double traction; the V 320 would have been more economical on its own.

It had a power output of 4 000 hp (correction of our report from June 2012), which is also indicated by the manufacturer's designation DH 4000.

It was the first Bundesbahn locomotive to have disc brakes at the factory and the only DB diesel locomotive to also have a magnetic rail brake, which enabled it to reach a maximum permissible speed of 160 km/h.

It was and remained a superlative locomotive, and, therefore, enjoyed a lot of attention and popularity, and, thus, also earned its role as a model of choice for model railway enthusiasts of all sizes.

It remains to be seen whether Märklin's model will have an equally long service history - 53 years, after all. Today the model is still dewy-eyed and has to stand up to our test.

It should be briefly mentioned that in the meantime the Wiebe version has also been delivered as part of a construction train package (81320): The exclusive advantage for Club members therefore only lasted a few weeks.

As early as April 2022, a still unpainted model made of series parts could be seen in Altenbeken, which was very impressive and raised hopes that it could be delivered soon.

But this was delayed and Märklin made it clear to our editorial team that the final quality control had revealed required rework. In May of this year, however, the time had finally come.

## The first impression

Taken out of the box and "driven off" with the eyes, the new model immediately makes a good impression. Nothing spontaneously points to weaknesses, but we will find these, too. The proportions of the locomotive are correct, which is also impressively confirmed by the dimensions table.

Anything that does not correspond exactly to the converted prototype dimensions can undoubtedly be interpreted as a measurement tolerance. Only the slight height deviation matches the high-legged impression of the reduction.



The first impression given by V 320 001 (Art. No. 88320) is excellent: the locomotive looks coherent in its proportions and well detailed.

The wheels have turned out to be 0.2 mm too large, which increases the operational safety in the turnout area, but could not be detected by eye. Consequently, we do not consider this to be a defect, and they do not provide an explanation for the described impression.

Very noticeable on the model is a buffer beam that is clearly too high as soon as it is coupled with a wagon: The lower edge of the locomotive is roughly where that of the wagon ends. This is a mistake that we cannot explain and that has a very disturbing effect on the viewer. The impression that the entire locomotive housing sits too high on the chassis is also confirmed here.



In the side view, the locomotive reveals its long leggedness at first glance through the large gap between the upper end of the bogies and the locomotive frame. This is also underlined by the shining copper of the wheel sliders.

This is a pity, because the overall proportions have been reproduced very accurately and a real strength of this model is the successful printing. All the features of the prototype can be found and have been very attractively emphasised: Nothing seems to have been left out and nothing has been overly emphasised to stand out.

This also becomes very clear in comparison to the small series model by Schmidt: for us it was always a good and very accurate reproduction of the prototype. Nevertheless, in direct comparison it becomes clear how much time has passed since its appearance.

Some printing had to be done much sharper there and always looked believable. Now there is an opportunity for comparison and the same details seem almost obtrusive on the older model. This also becomes clear in the area of the cooling fans: Where only the cover grille was visible on Schmidt, we can also clearly see the rotor blades of the fans on Märklin.

We also notice differences in the area of the bogies. Both models have taken into account the different wheelbases, but show different details. Comparisons with prototype photos clearly speak for Märklin here – this is also a plus point that we honour. If you look closely here, you will discover fine with many details, including the Indusi locomotive magnet, which partially covers the magnetic rail brake.

What also suits V 320 001 in 1:220 scale very well are the separately applied, shiny chrome handle bars on the driver's cab ascents and on the locomotive fronts. We already know this from the E 41, which was also issued for Insider Club members, but we certainly don't want to simply declare it standard and take it for granted.





The separately attached grab rails around the driver's cab contribute positively to the overall impression. Inside we also find a plastic reproduction of the interior, which unfortunately can neither be seen nor photographed through the windows. As an example, for successful engravings, we mention the roof ventilators, in which even the rotor blades are visible under the cover.

This miniature also has inner values: The interior of the driver's cab, which we found, was not announced. It also follows the newer club models and we like it. However, it has been sprayed dark grey instead of light grey and can therefore neither be seen from the outside nor photographed without disassembly. Unfortunately, the effect is ineffective.

We took a close look at the engine room equipment and lighting, which were named as product features. As with all previous models, Märklin did not reproduce the engine equipment plastically, thus sacrificing weight in the area of the running gear block, but chose a black outline drawing on a light grey carrier.

We find this acceptable, but it would be much more effective in the form of a photo print. Unfortunately, the engine room lighting is rather amateurish because it does not include the engine room at all: The side windows remain dark.

The side windows remain dark. Light-emitting diodes on the main circuit board only illuminate the two overhead, skylight windows, with which the prototype locomotive was equipped, because only one side window per locomotive side would not have illuminated the interior nearly enough.

The solution chosen by Märklin, therefore, seems to us to be far from the prototype and implausible. We would like to compare it with the interior lighting of the Hamburg type express railcar: Half of the train remains dark, the other half receives light. How can this be explained on the basis of the prototype?



The chosen form of the “engine room lighting” seems senseless and stays away of the prototype: while bright light shines from the two skylight windows above the entrance doors, it remains dark in the central window on both sides of the engine room.

#### Product recall by Märklin:

On Wednesday, 14 June 2023, Märklin announced a product recall for both variants of the V 320 (Item Nos. 88320 & 81320) delivered to date.

Due to a possibly defective component, excessive heat can develop in the locomotive electronics, which can lead to consequential damage. Therefore, the models should not be put into operation and should be sent to the Märklin repair service for repair.

According to our research, burnt-out components will cause damage to the housing (and a fire hazard that cannot be ruled out) have so far only been found while using the Märklin Electronic08 power supply.

The recall no longer had any influence on our test because it had already been completed. We do not expect any significant changes in the measured values as a result of the reworking.

The clean, silk matt to slightly matt paintwork including sharp colour separating edges is excellent and impeccably executed. Here, Märklin's well-known skill is again shown at the highest level.

The printing is not quite up to this standard. It is complete on the locomotive body, but not in the usual high sharpness and readability with a magnifying glass. In the past, much more was possible.

For the sake of completeness, it should be mentioned that the prototype also had inscriptions in the area of the bogies and the chassis frame. These have not been implemented on the model, which is the manufacturer's practice in Z gauge. This is

acceptable, because in the prototype they were quickly covered by rust and dirt and are hardly visible in photos.



Well-engraved and detailed bogies are contrasted by unclean-looking printing, where even in the company nameplate the Henschel lettering, which is recognisable even without a magnifying glass, is not clearly legible in the largest font.



A buffer plate warning coating would have been desirable and exemplary. The prototype wore this at times, and this was the case in the operating condition shown.

The equivalent of the gauge H0 shows that Märklin was aware of this: It was equipped accordingly with identical printing.

## Technical evaluation

After we have found an externally successful locomotive with a few minor weaknesses, we would now like to know how this model runs technically. Constructively, it follows the still recent principles of Märklin.

### Dimensions and data for the V 320 001 of the DB:

|                            | Prototype           | 1:220    | ModII    | Difference |
|----------------------------|---------------------|----------|----------|------------|
| Length over buffers (LüP)  | 23.000 mm           | 104,5 mm | 104,8 mm | + 0,3 %    |
| Height above the rail head | 4.225 mm            | 19,2 mm  | 19,5 mm  | + 1,6 %    |
| Width                      | 2.995 mm            | 13,6 mm  | 13,6 mm  | 0 %        |
| Overall axis base          | 17.710 mm           | 80,5 mm  | 80,2 mm  | - 0,4 %    |
| Bogie axle base            | 4.350 mm            | 19,8 mm  | 19,6 mm  | - 1,0 %    |
| → 1. / 2. axles            | 2.550 mm            | 11,6 mm  | 11,4 mm  | - 1,7 %    |
| → 2. / 3. axles            | 1.800 mm            | 8,2 mm   | 8,2 mm   | 0 %        |
| Wheel diameter             | 1.100 mm            | 5,0 mm   | 5,2 mm   | + 4,0 %    |
| Service weight             | 128,0 t             | ---      | 47,7 g   |            |
| Axle form factor           | C' C'               |          |          |            |
| Power                      | 4.000 HP / 2.940 kW |          |          |            |
| Allowed highest speed      | 160 km/h            |          |          |            |
| Manufacturer               | Henschel            |          |          |            |
| Year built                 | 1962                |          |          |            |
| Quantity produced          | 1                   |          |          |            |

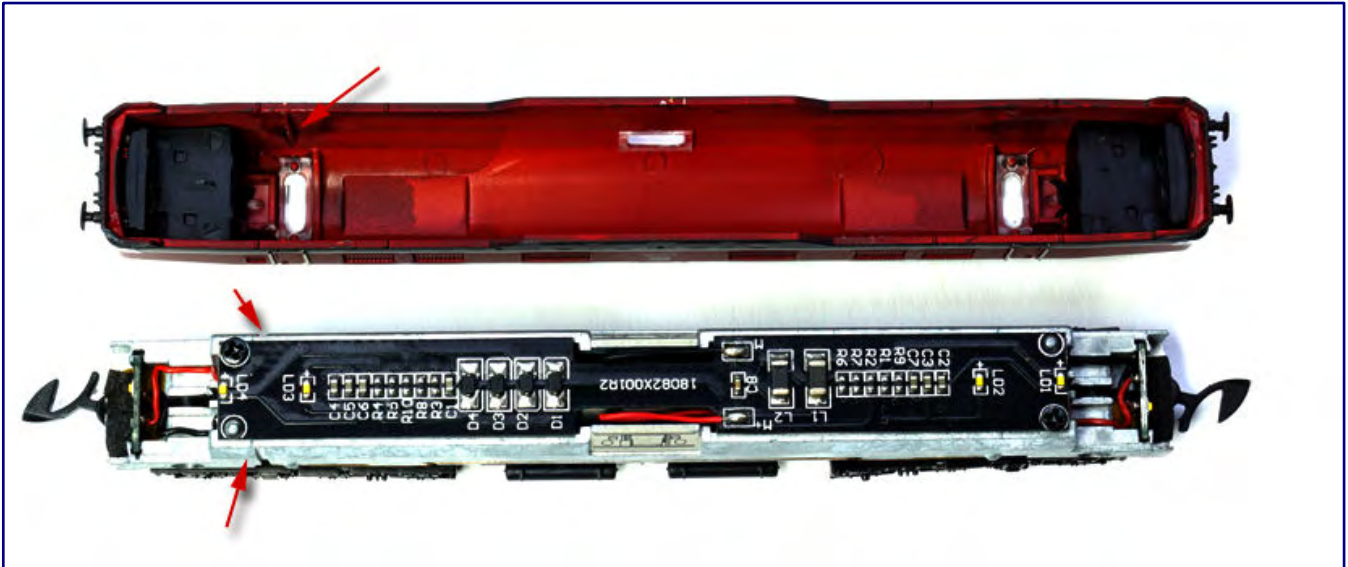
The running gear is divided lengthwise into two halves, which are separated according to the electric poles. The bell-shaped armature motor resting inside has two flywheel masses and transmits its power via cardan shafts, worm and gear wheels to the four outer axles of both bogies.



On our test model, the housing was glued to the chassis by means of double-sided adhesive tape and could not easily be removed according to the instructions. After completion of the removal, the longitudinally divided chassis becomes visible.

Good driving characteristics and a safe current consumption confirm the successful construction of the model. However, this only applies to a limited extent to the main board, which is coordinated with them: When starting up, the warm white LEDs under the skylights, which are part of the incomplete engine room lighting, light up first.

Only when the driving voltage is increased further does the warm white three-light headlight become visible. We have never noticed this phenomenon on an earlier Märklin model. By the way, the two red tail lights also come on before the headlights.



Groove and chamfer (see arrow markings) at one end of the locomotive ensure that the chassis and housing only come together in a defined position. The positions LD1 to LD4 indicate the position of the light emitting diodes for the light emission in the roof, at the centre of the vehicle the board does not show such components.

The driving characteristics are almost perfect: already at 0.3 volts track voltage, which our test transformer 67011 applies to the track, the locomotive starts up with the equivalent of 0.9 km/h. In order for it to drive safely over points, it needs 2.2 volts of track voltage to accelerate it to 25.9 km/h.

Certainly, it would be possible to go a little slower, but the unit used for our test trials is not that sensitive. Since the bell armature motor does not put any significant load on the transformer, it is also able to handle a voltage significantly above the nominal voltage of 10 volts, whereas this voltage drops with the classic iron core motors.

On the boxes of the new Märklin models, 12 volts is stated as the maximum voltage. We also use this as a guideline, but even with the transformer maximum voltage of about 15 volts we have not noticed any heating of the motor.

V 320 001 reaches its converted prototype speed of 160 km/h at 9.5 volts, at 10 volts nominal voltage it runs at 185 km/h, which is still within the NEM tolerance range. At 12 volts, however, it runs at a whopping 215 km/h, which even the eye can see is no longer consistent.

As already mentioned, the current consumption is again pleasantly low: At transformer position 100, the locomotive consumes 25 mA, at 150, 36.5 mA - this is also within the usual range of this motor generation. Under full load, it increases its "hunger" to 60 mA.

The weight of this model is impressive, despite its plastic housing: we measured a whopping 47.7 grams, beating even the Schmidt model with 45.3 grams, which always seemed very heavy and also set the tone for all models in terms of tractive effort until the appearance of Märklin's V 188.



The Märklin version can't outrun the much older small series model either, but this is recognisably due to the fact that it benefits from its traction tyres: The tractive force on the level, measured as a trailing load pulled over a pulley, is sufficient for 6.0 grams with Märklin, and even for a full 10.0 grams with Schmidt – these are considerable values in both cases.



We compared the much older Schmidt small series model (rear) with the new products: in terms of tractive effort, the Märklin model has to admit defeat despite good values, but in terms of detailing and lettering it is clearly ahead.

On a 3% gradient, the loss of tractive force is less with Märklin than with Schmidt: Here, we measure 5.25 grams to 8.0 grams. By the way, a trailing load in wagons cannot be derived from the weights. They are only useful in comparative series to determine the tractive effort of different locomotives in relation to each other.



The printing reflects the difference of about thirty years of development. The frames of the window frames of the driver's cab on the Schmidt model (right) have been treated with paint afterwards, otherwise the difference would be even greater. The big weakness on the Märklin side is also visible: The buffer beam is much too high and even the system coupling is not at standard height.

The decisive factor for model railway enthusiasts is, therefore, rather how many wagons I can attach to the V 320. Here we must clearly point out one last problem of the Märklin model, which affects many of the delivered models, but apparently not all.

The system coupling is too high on both versions of the V 320 with which we were able to carry out tests. As a result, it loses its traction at the latest when entering a curve. This urgently calls for an improvement within the framework of the recall that has already been announced, because the V 320 001 is simply not suitable for use on a layout.



On the other side of the locomotive the photo is even more blatant: the height of the completely wrongly adjusted coupling is obvious. Since the model loses attached wagons on both sides at the latest when cornering, we can unfortunately only attest to a lack of suitability for a layout. Märklin would be well advised to take this product defect seriously and to remedy it, because we have not come across an isolated case here!

It remains completely incomprehensible that such a grievance was not noticed within the framework of quality controls. From this we can only conclude that the test runs must have been limited to solo operations in a test track; a thought-provoking impulse for more practical test series seems to us urgently advisable in this case.

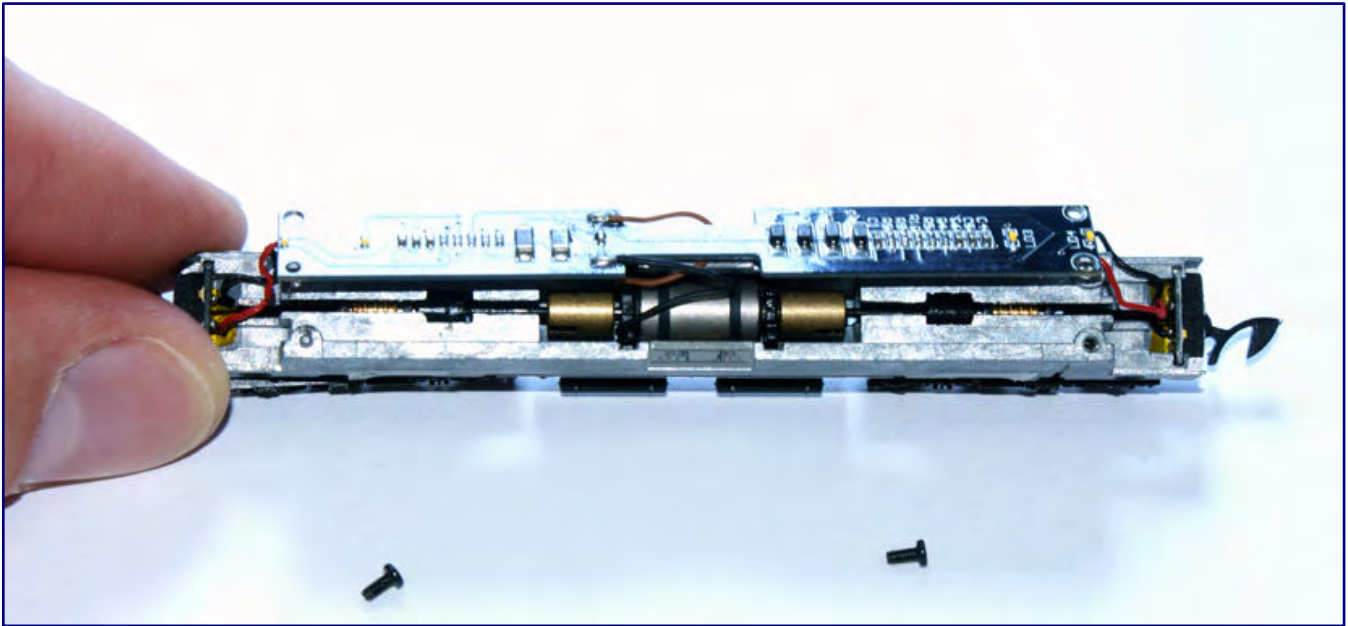
This is a great pity, because this locomotive offers enormous tractive forces, which predestine it, like its big role model, for long trains. Ten UIC-X passenger coaches should not be a problem on the level, and even with long goods trains, the length of the layout is more likely to set a limit than the tractive power of the model.

The wagon material that can be used is correspondingly colourful: in freight traffic, every contemporary wagon from large and small series can be used, which does not even have to be limited to the DB as railway administration.

From the Kempten depot, however, as the DB's fastest diesel locomotive, it was also preferably used in heavy passenger traffic. Due to the lack of suitable old-built coaches, also from the SBB, the choice here will probably fall on green UIC-X coaches, interspersed with blue 1st class examples.

Ocean blue ivory or the pop colours did not follow until the seventies, the locomotive bears markings from the years before 1968. However, if you want to bring a little more colour into play, you can fall back on





If the printed circuit board is unscrewed and put aside, the bell-shaped armature motor, two flywheel masses, cardan shafts and worms in the gear block become visible.

Silberlings. Since they were much more comfortable than most of the old coaches, the Bundesbahn also used them in express trains at that time, not always only as additional coaches.

## Summary

Not only functional tininess has its appeal on the smallest scale. Even a fireball like the V 320 001 knows how to inspire the customers for Mini Club. This is proven by the voices and comments that were heard when this model was announced and when the first sample made from series parts was exhibited in Altenbeken 2022.

There is therefore no question that this locomotive can be a much greater success than the prototype. However, Märklin must do its homework conscientiously, which in this case literally means detention.

It is downright embarrassing when a model has to be held back and reworked for many months because of a quality control finding and the buyer's anticipation turns into frustration because this locomotive loses every train after less than a metre. It is not reasonable to expect the owner to look for the cause and do something about it himself.

The consistent product recall because of a component that may be defective is commendable. We hope and wish that Märklin will also use this opportunity to correct the system couplings.

Because then, the great printing, free-standing handle bars, and the appealing overall impression will pay off, and this locomotive will find many admirers and viewers, justifying the pride of its owner.

We also openly question the pointlessness of incomplete engine room lighting to be seen in front of the tail and top signals, and would like to provide food for thought for future models.



There are plenty of applications for the V 320 001 at Kempten depot. Goods trains can be mixed, but the prototype was mostly used in express service there. Märklin's new passenger coaches can be used here as well as single "Silberlinge". What we are sorely missing in the product range at this point are, in addition to old-built passenger coaches, especially SBB passenger coaches for Era III.

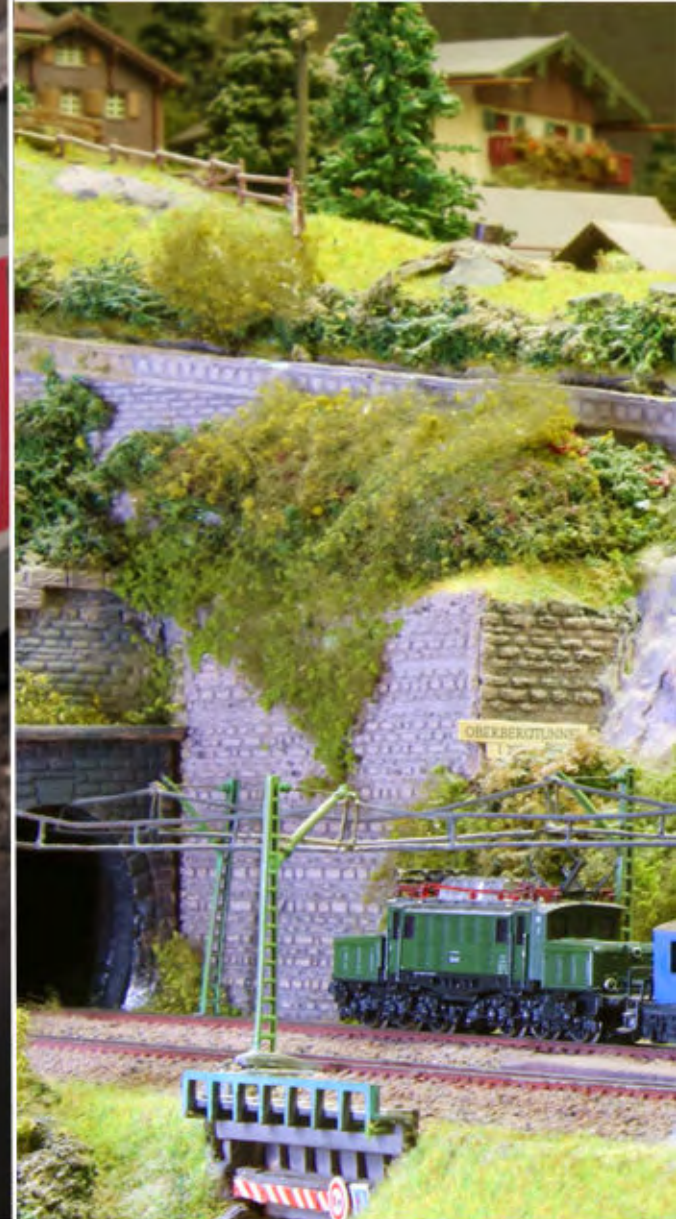
We trust Märklin's will and competence, which is why we are nominating V 320 001 (the early DB version) for the best new release of 2023 in the locomotive category. Once the homework is done, we are certainly dealing with a heavyweight in two respects!

Producer of the basic model:  
<https://www.maerklin.de>



# SONDERAUSSTELLUNG KLEIN TRIFFT GROSS

50 Jahre Baugröße Z, 1:220  
19. März 2023 bis 17. März 2024



**märklin**



**Schramberg**  
Schwarzwaldqualität erleben



## Besuchsinformationen:

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78713 Schramberg





A Z Gauge Class 01<sup>5</sup>

## Flops, Misfortunes and Mishaps

*The lovers of the German Reichsbahn in the former DDR are rather a minority in our scale. Most recent-ly, Märklin has tried to increase their numbers with models from the time before the fall of the Berlin Wall. One of their most desirable models may have been the former flagship class 015 steam locomotive. In celebration of last year's anniversary, this dream was to come true. We take a closer look at the new model.*

The class 01<sup>5</sup> express steam locomotive certainly has the potential to find customers in both East and West Germany: This type of steam locomotive was distinctive and well-known, and moreover the machines came with interzonal trains as far as the Federal Republic of Germany. There, they were popular photo objects.

This is certainly one reason why Märklin contributed financially to the refurbishment of the 01 519 and had its logo affixed to the tender. The museum locomotive of the Eisenbahnfreunde Zollernbahn is therefore also the model for the Z gauge model (item no. 88019) that we want to review today.



After the quick revision, the model of the 01 519 (item no. 88019) makes a mostly coherent impression on the layout.

According to its listed operating condition from 2016, the model belongs to Era VI. If we disregard the owner's lettering on the side walls of the driver's cab and the Märklin emblems on the tender, it could also be used as Reichsbahn service without any objections. This is exactly what Märklin surely has already planned for, and we are curious to see whether other features such as oil tender, side skirts and Boxpok wheels will then be varied.



But we don't want to lose ourselves in reveries now, but rather inspect and examine the delivered model. The tender, which we will come to later, and the running gear are existing parts that are not part of the new design. The running gear could thus be taken over from the 01 series as it appeared in 2012.

However, this is also the reason for the most serious dimensional deviation compared to the original: Although the wheelbases of the locomotive and tender are well matched, the overall wheelbase has the largest deviation (see table).



Because Märklin persistently continues with a 51-year-old museum relic, we are not spared impressions like this one: The distance between the locomotive and tender is not only much too large, but in view of the level of detail that has long since been achieved, it is completely out of date. In addition, this and many other steam locomotive models lack the typical coal box elevation.

The almost 4 mm are largely due to the (once again) terribly large locomotive-tender distance, but this alone cannot explain it, because the length over the buffers is not even 1 mm too long. How is that possible?

#### Dimensions and data of the express steam locomotive class 01<sup>5</sup> of the DR:

|                           | <u>Prototype</u>      | <u>1:220</u> | <u>Model</u> | <u>Abweichung</u> |
|---------------------------|-----------------------|--------------|--------------|-------------------|
| Length over buffers (LüP) | 24.350 mm             | 110,7 mm     | 111,4 mm     | + 0,6 %           |
| Height above rail head    | 4.550 mm              | 20,7 mm      | 21,1 mm      | + 1,9 %           |
| Width (Boiler)            | 3.100 mm              | 14,1 mm      | 14,2 mm*     | + 0,7 %           |
| Wheelbase locomotive      | 12.400 mm             | 56,4 mm      | 56,1 mm      | - 0,5 %           |
| Wheelbase Loc & Tender    | 20.320 mm             | 92,4 mm      | 96,0 mm      | + 3,9 %           |
| Wheel-Ø front             | 1.000 mm              | 4,5 mm       | 4,2 mm       | - 6,7 %           |
| Wheel-Ø rear              | 1.250 mm              | 5,7 mm       | 5,0 mm       | - 12,3 %          |
| Coupling wheel-Ø          | 2.000 mm              | 9,1 mm       | 8,7 mm       | - 4,4 %           |
| Service weight            | 183,8 t**             | ---          | 44,5 g       |                   |
| Axle form factor          | 2'C1' h2              |              |              |                   |
| Power                     | 2.450 Hp / 1.802 kW** |              |              |                   |
| Allowed highest speed     | 130 km/h              |              |              |                   |
| Years built               | from 1961             |              |              |                   |
| Retired                   | 1977 – 1982           |              |              |                   |
| Quantity produced         | 35                    |              |              |                   |

\* Width over cylinder is 16.5 mm

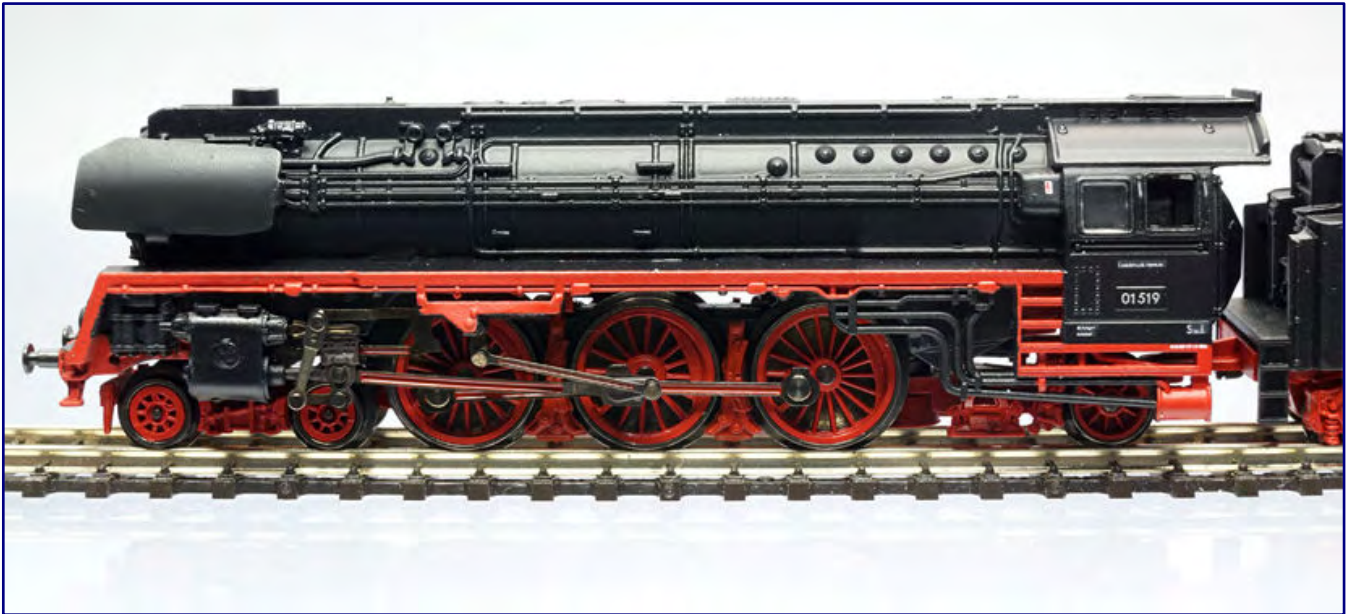
\*\* Coal version (mass indication with full supplies)

The conclusion to be drawn from both deviations, which do not easily fit together, is: The locomotive, not the tender, has been made too short.

The DR once had to add an extension to the front end of the frame, which increased the length compared to the donor locomotive, to be able to accommodate the pumps behind the skirt.

Märklin did not base its novelty on an adapted chassis, but constructed it around the existing one of the class 01.

The basic construction goes back to 1972 and class 03. In principle, this is not to be criticised if it is not annoying, but, in this case, it has not been done very well, as we will see later.



The power lines and pipelines below the service deck look well done and appealing. However, a closer look at the prototype also reveals inconsistencies: The controls rack is mounted noticeably higher, and the front cowling is too steep, which is probably due to the too short overall length of the locomotive.

Many details in the chassis area look appealing and are well represented. This also includes detached lines that are part of the housing, especially on the fireman's side below the driver's cab. Only the controls rack is noticeably lower on the prototype. Here either the boiler position is not correct, or the proportions of the locomotive have been distorted.

The spoked wheels in the tender are praiseworthy, whereas those on the locomotive leave some wishes unfulfilled, because they stand out mainly due to disturbing burrs, which were only insufficiently removed. The fact that the balance weights do not protrude outwards, but stand lower than the wheel tyre, we accept as with all other steam locomotive models.

This is due to the space required for the parts of the fully operable detailed control. It is a feature that Märklin can also use to appeal to new customers and is up to date, because the linkage brings the models considerably closer to the prototype.

## The outward appearance

The first thing you notice on the locomotive body are the clear inserts in the front side windows of the driver's cab. This is praiseworthy because Märklin has by no means made this the delivery standard for all steam locomotive models.

But that was almost it with the positive impressions. Certainly, this novelty is not a failure, but after a first good impression, some inconsistencies are unfortunately noticeable. For example, the driver's cab seems somehow too narrow compared to the original photos.

In fact, its width should be correct, because at the lower quarter there is a distinct edge, which does not exist on the prototype. The reason for this is not evident to us, but somehow it looks as if the designers have "added" a bit of cab to the scale construction without a need.



From our point of view, this can only be explained if distorted proportions are to be largely camouflaged this way.

We had already suspected a possible incorrect boiler position, which could also have left recognisable deviations there.

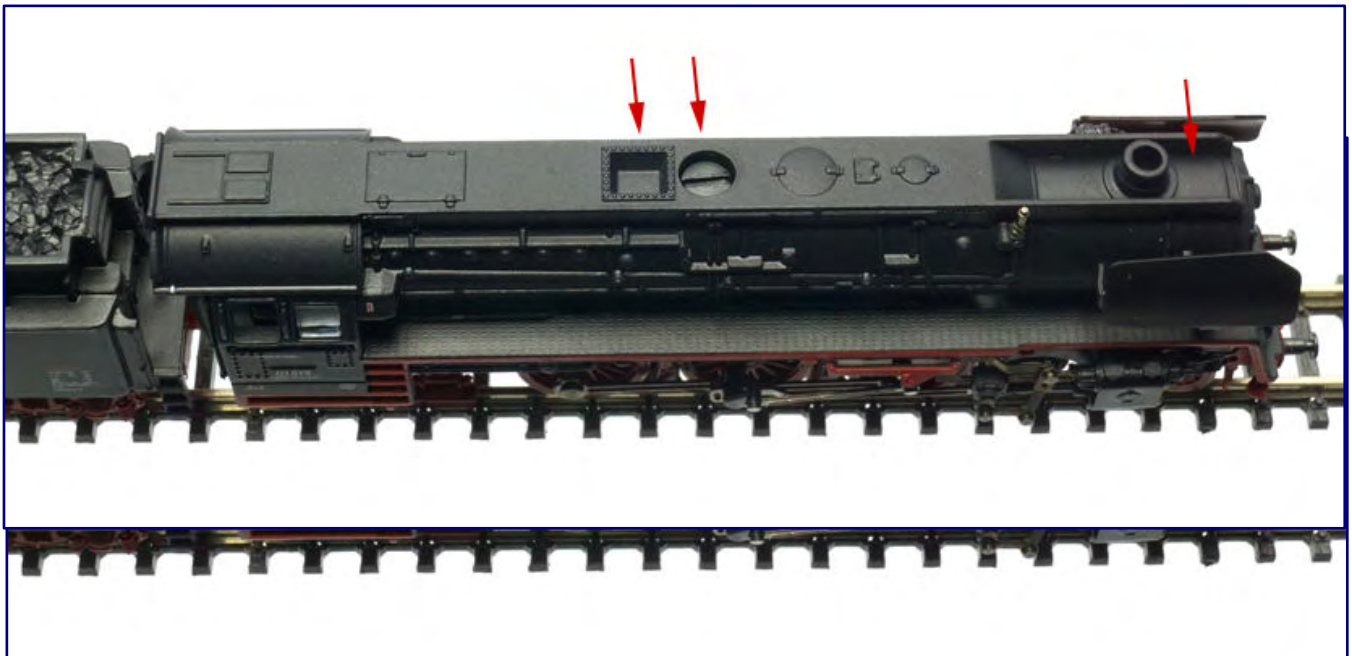
The boiler printing is correct from our point of view, showing wash hatches, adjusting rods and pipes, as well as typical handlebars. As usual in our scale, these are not detached.

But it must be mentioned that they are not as good as Märklin's best models, which is especially obvious at the shut-off valves.



In the prototype, the class 01<sup>5</sup> received a new, welded driver's cab, in Märklin a fantasy conversion: The ratio of the length to the height is not correct, the edge marked with arrows completely contradicts the prototype impression.

Most disturbing is the look at the top of the model: The lockable skylight window is engraved, but its characteristic can only with imagination be recognised as such. This is a pity, because these windows are one of the typical features of DR Reko steam locomotives.



The view from above doesn't satisfy us: The biggest mistake that can be made on a model of a Reko steam locomotive is probably the forgotten mixer preheater box (arrow on the far right); Märklin has since touched this up. What remains are the disturbing housing screw in an uncovered hole (middle arrow) and the missing safety valves in the cut-out of the dome covering (left arrow). For the requested purchase price, customers should expect more.

If we look further forward, there is an almost square hole in the closed top of the boiler with the side plates, and in front of it another hole in which, deeply sunk, the housing screw can be seen. The rectangular opening is prototypical, but there would have to be three safety valves in it, which Märklin was able to reproduce even on much older models.

The uncovered housing screw is also not up to date, because Märklin already knew how to solve this better in 1994 with the West German class 10 with a cover cap. To put it kindly: This model, which is so important for DR admirers, and which can also be used on DB tracks, seems to have been implemented in a somehow careless way.



The front of the locomotive shows a distant resemblance to the prototype: The upper top light is placed too low, the semaphores do not look coherent and due to a clearly too short apron the lower lanterns sit almost on the buffer beam instead of in its middle, to make matters worse also too close together.

arrived at the locomotive front and here nothing is right at all. The locomotive looks attractive, but not correct. It was not only the missing mixing preheater.

The proportions of the smoke box door are also wrong, which is why the sash locks look strange compared to the prototype photos. The upper top light sits too low and the two lower ones a bit too far inside.

The distinctive cowl of the DR locomotive is too short, which is why the lower head lights are clearly too close to the buffer beam. The DR prototype expert needs a bit of imagination to be able to identify the class 01<sup>5</sup> with certainty.

So, it is difficult for us to formulate the positive impressions equally clearly. One of them is definitely the matt-looking plastic colour, which does not immediately betray its material compared to the metal parts. The paintwork is also clean and sharply defined throughout the model.

Bahls Modelleisenbahnen can cure these serious weaknesses, because the small-series manufacturer has reacted immediately and designed and produced a 3D-printed set of accessories. This is sold via the 1zu220 shop.

The kit contains an insert with the missing boiler valves and two dome covers (1 x spare) to cover the screw.

Also included is an attachment part for the most embarrassing mistake that a manufacturer can make on a Reko locomotive: Märklin completely forgot the angular storage box of the mixing preheater in front of the vent!

In the meantime, this has been rectified with a separate add-on part, but such a faux pas, which also went undetected in the factory, unfortunately speaks volumes.

This is the climax in a series that unfortunately only deserves the name "flops, misfortunes and mishaps", but unfortunately not yet its final point. We have now



The quality of the printing does not reach the usual Märklin level, because gaps can be seen in the owner's lettering on the driver's cab in the macro shot and there is a visible offset between the signs for the homesite and their imprint.



Märklin also falls short of its possibilities when it comes to pad printing: We criticise a lateral offset of the imprints (left detail magnification) and above all, gaps in the printing on the driver's cab (right detail magnification), which make these service inscriptions illegible.

The 51-year-old tender is and remains a real imposition, and Märklin has still not moved away from it, although we have often complained about this.

As with every delivery that must make do with this memento of a long company history, it has the well-known defects that customers simply can no longer be expected to accept for the price demanded: giant lanterns at the back and lumps of coal at the top that would not fit through a fire door.

**Photo right:**

There are boulders in the coal box of the antiquated tender and even the remaining burr on the water box does not lead to a scrap. Price and performance are simply not right with this model.



Even an elaborate printing, which includes the reflectors of the lanterns as well as two-coloured lightning warning arrows, only partially turns the tide in this case. On the tender side wall, by the way, the inscriptions show significantly more printing sharpness than on the locomotive.



The rear view does not cause any excitement for us either, despite the more elaborate printing with lightning warning signs: The missing coal box elevation also affects the overall impression, but much worse are the giant tin plates with which Märklin has been trying to imitate the lanterns for years.

The far too large and uncorrectable locomotive-tender distance of the museum-aged standard tender might exceed that between two passenger coaches by now. And the coal box elevation of its prototype (in the indicated operating condition 2016) is also missing.

To top it off, our test model also has a surface defect between the water tank covers that cannot be overlooked, except apparently for quality control. However, it does not affect the entire series.

## Honourable technology?

Since the exterior does not cause us to cheer, we turn our attention to the technology. The model is driven by a bell-shaped anchor motor in the driver's cab via worm and gear wheels. The gearbox works on all three coupled axles.

At the front, the steam locomotive has a top light that shines warm white from the three lanterns depending on the direction of travel. Maintenance-free light-emitting diodes are responsible for this, which draw attention even at low voltage on the track.

For the driving test, we first ran the model in for 1.5 hours and then operated it with Märklin transformer 67011. We should not expect miracles of tractive power from a steam locomotive model in Z gauge, but the class 01<sup>5</sup> is certainly on the move with respectable express trains.

What the museum locomotive in the prototype must move, the miniature can do as well. On the pulley it pulled 4.5 grams on the level, on a gradient of 3 % still 3 grams. This is somewhat above the Mikado models and on a comparable level with other Pacific express locomotives from Märklin.





Even if good driving characteristics and sufficient tractive power compensate for a lot: We don't really want to enjoy this model — the list of faults is simply too large.

At full load, its wheels spin, which protects the motor. Then it draws 40 mA of current. At transformer position 100 it is only 17 mA, which increases to 24 mA at position 150. This is also within the expected range for this type of motor and speaks for a well running gearbox.

The small locomotive starts smoothly at 0.3 volts track voltage. Then it moves at the equivalent of 1.4 km/h. To get safely over any turnout track, we had to increase the track voltage to 2.2 volts. Then 01 519 moves with 31 km/h, also an acceptable value.

Since a bell-shaped armature motor does not sufficiently stress the selected test transformer, it is able to apply far more than the nominal 10 volts to the track. According to the packaging box, however, the locomotive may even be operated with 12 volts. We determined the converted speed for both voltages: They were 186 and 214 km/h. With the prototypical top speed of 130 km/h, the miniature runs at 7.8 volts.

To fulfil a wish of our readers it means to make train formation suggestions at this point. So far, there is no suitable DR car material for express train traffic in Z gauge. This calls for self-building, perhaps 3D printed constructions can be found.

However, it is certainly a stroke of luck that a museum locomotive has been reproduced and that we can classify it in era VI. Therefore, DB coaches can be used without hesitation. This can be classically green, but certainly also follow the ocean blue-ivory colour concept.

If you want to reproduce a steam-powered standard schedule trains, "redlings" are also suitable, i.e., former silberling cars in the traffic-red livery of DB Regio. If you remove or cover the Märklin lettering on the tender, you almost have a DR locomotive in sight that has its markings before 1970.

Then the miniature can also be used ahead of an interzonal train made of DB material. However, it should be considered that in those days, cars were still painted in chrome oxide green or steel blue. Only silberling cars offer some variety here, because these cars, as very modern representatives, were once also frequently used in long-distance trains, sometimes at least as reinforcement cars in D trains.

With this, we finally summarise our test impressions. Technically, this Märklin model is on a par with the younger steam locomotives from the same company. A prototypical use is possible without any problems and this DR series plays an important role in the line-up.



Due to the lack of DR passenger coaches, only DB coaches remain for the train formation, but 01 519 is also a museum locomotive in this operating condition. Express train cars and silberlinge are probably typical sets that she has on the hook for special trips and steam-powered standard schedule trains.

The external faults that we found seem all the more serious. This locomotive was obviously constructed on an existing chassis that did not fit properly. Some proportions are distorted and especially the front of the locomotive is not correct. To forget the mixing preheater of a Reko locomotive raises many questions.

But also in the further realisation, meaning boiler printing and the printing on the locomotive, Märklin clearly falls short of its own capabilities. Especially prototype connoisseurs will be annoyed that they have to incur further costs to bring this model closer to their desires. And, even with that, it is only approximately attainable.

**Base model manufacturer:**

<https://www.maerklin.de>

**Required accessories:**

<http://www.bahls-modelleisenbahnen.de>

<https://www.1zu220-shop.de>



Rekolok Class 01<sup>5</sup>

## The Powerhouse of the DR

*Like the DB in the West, the DR (East) could not do without steam locomotives for a long time. In addition, it was particularly affected by a lack of high-quality express locomotives. So, the class 01 underwent an extensive reconstruction, which made it one of the most powerful steam locomotives in Germany. Reclassified as the 01<sup>5</sup> series, 28 of the 35 locomotives were also fitted with oil firing.*

After the end of the Second World War and the division of Germany, a total of 70 class 01 locomotives remained in the East. They entered the stock of the Deutsche Reichsbahn on the territory of the later DDR.

Due to severe war damage, however, five units (01 026, 030, 035, 110 and 214) had to be withdrawn from service. This meant that the DR could only fall back on 65 machines for heavy express train service. In addition, not a single locomotive of the three-cylinder type 0110 remained on its territory.

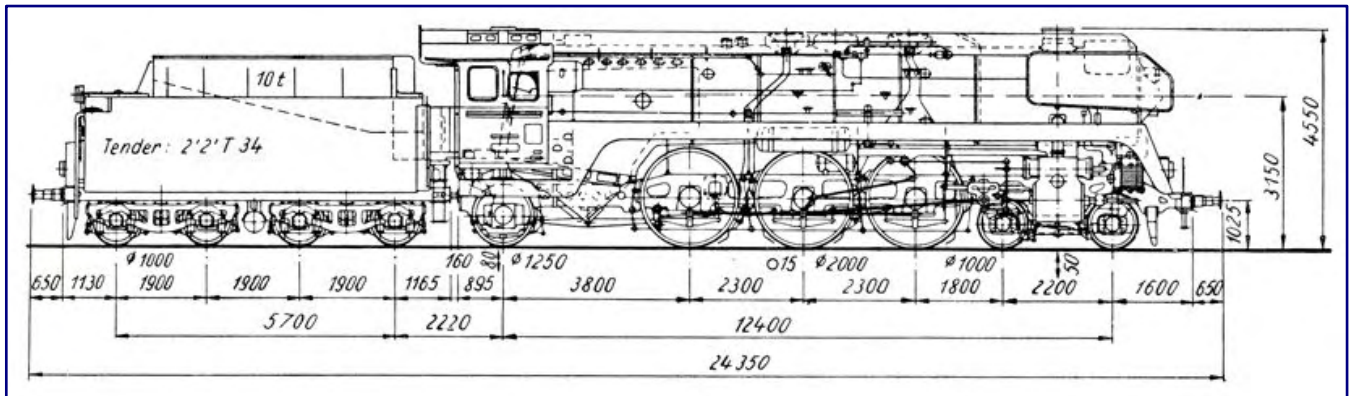


For many years, the class 015 was a mainstay of passenger train traffic at the DR. However, when oil-fired 01 0513 passes barrier post 45 just before Könitz on 16 October 1977 with E 802 to Leipzig, its star is already sinking. Photo: Wolfgang Bügel, Sammlung Eisenbahnstiftung.

Consequently, the Reichsbahn was faced with several tasks: Repairing the most urgent war and wear damage as well as upgrading the machines for the heaviest train services, because they would still form the backbone of the express train service for years to come.

The process to achieve this goal began with the so-called “locomotive reconditioning:” depending on the condition and needs of the locomotive, it received welded replacement cylinders, welded bogies (with 1,000 mm running wheels), large balancing levers and brakes on the trailing axle.

These measures also ensured further standardisation of the 01 series, as hardly any of the steam locomotives had all these features beforehand at the factory. Many also needed new standing boilers. Nevertheless, the representatives of Germany's best-known unitary locomotive foreseeably reached the limit of their useful life.



In order to emphasise the elegance of the locomotives, on which the Boxpok wheels had a negative effect, the machines of the 015 series initially wore circulation skirts, as the DR also recorded in the first drawing. Illustration: Instruction book for traction units 939 Tr, Trainini® collection.

By the end of the fifties, some of the locomotives were already thirty years old and were ready for “retirement.” Since the DR could not continue to do without them, they needed to be comprehensively modernised and upgraded for further years of service.

In February 1959, this was the hour for inclusion in the reconstruction programme, as the comprehensive general overhaul was called in DDR parlance. Originally, all 65 existing locomotives were to be included. However, because the Meiningen works raised objections due to the high cost of reconstruction, the focus was reduced to the road numbers from 01 102 onwards.

In November 1963, HvM finally made a U-turn: in view of the costs of more than 400,000 Marks per locomotive, the decision was made to reconstruct only 35 of the existing locomotives.

However, this was done in a remarkable way: The newly installed combustion chamber boiler “01E” had the same mass as the previous standard boiler, and with its high load capacity and steam output it even surpassed the new DB boiler for the 01 and 0110 series.

With a specific heating surface load of 75 kg/m<sup>2</sup>h, it could deliver 16.8 t/h of steam, which exceeded the value of the Bundesbahn counterpart by a whopping 2 t/h. This Reko boiler was thus the same mass as the previous unit boiler. This Reko boiler was thus the most powerful boiler ever installed on the class 01. It helped the DR machines to become one of the best German steam locomotive types, even advancing into the performance range of the three-cylinder machines of the 0110 series.

The order to work out the reconstruction went to the FVA Halle (Saale), later VES-M Halle, and its director Max Baumberg. He and his staff attached great importance to the external appearance. The 01 series was to be a modern and imposing machine that corresponded to the style of its time.





On 5 September 1976, the coal-fired 01 1518-8 has the E 314 "Gedania" to Gdynia (Gdingen/Gotenhafen) on the hook in Berlin-Lichtenberg. The photo perspective allows us to check for ourselves our criticisms of the Märklin model regarding the position of the control carrier (cf. page 18) and the prototypical driver's cab (cf. page 19 above). Photo: Dr. Uwe Knoblauch, Sammlung Eisenbahnstiftung.

The boiler position was raised by using the vehicle perimeter line II, so that the boiler centre was 3,150 mm above the top of the rails (SO). This also promised an advantage for the later installation of an oil main firing system. The converted steam locomotives were fitted with IfS-type mixing preheaters, the tanks of which were placed in front of the chimney above the smoke chamber.

The boiler superstructures received a continuous cladding from the chimney to the driver's cab, which resulted in a uniform and level upper edge. The driver's cabs were also exchanged for welded new cabs according to the dimensions of the standard design, additionally equipped with sliding skylight windows.

All the rebuilds, with the exception of 01 501 and 01 503, received new bogies in welded construction, while only two machines (01 501 and 01 520) received new cast steel cylinders. The pointed smoke box doors, which together with the typical Witte wind deflectors gave the locomotives the nickname "bat", were also a distinctive feature of the locomotives.

## Disaster with Boxpok wheels

Already during the reconstruction of the class 01, broken spokes began to accumulate in the dome axles of the locomotives. On a trial basis, eight of the Rekoloks - in total there were even twelve in the end - were therefore equipped with so-called Boxpok wheels. This remained a unique development in German locomotive construction.

They were disc wheels made of cast steel parts in which cone-shaped openings were made to save weight, creating a spoke-like impression on the wheel surfaces. They were first used on the New York Central in 1933.

In addition to the USA, they were also used in the USSR and in China, where axle loads much higher than 20 tonnes were common. Boxpok wheels offer the advantage of lower manufacturing costs, are comparatively light and can also be easily balanced.



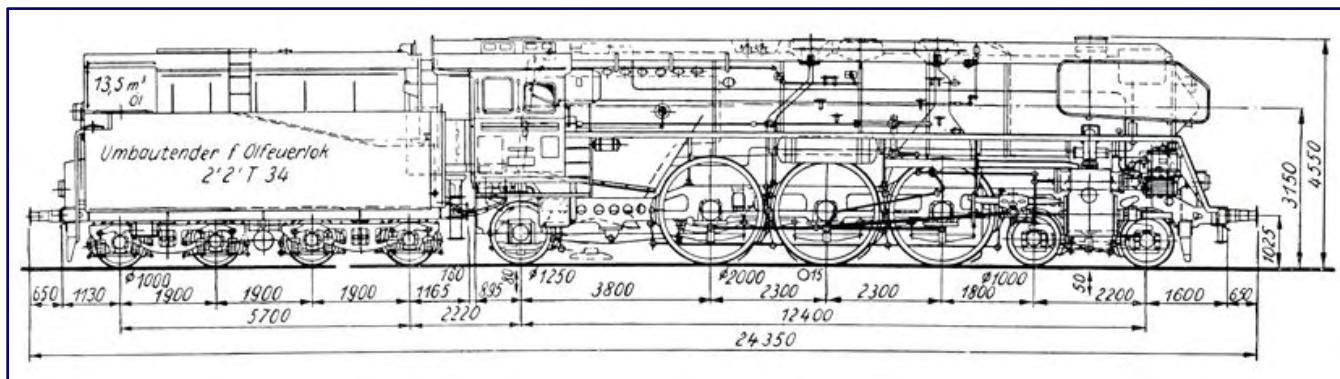
In 1972, the Reko locomotives also came to the territory of the DB in transit traffic. This is how 01 0507-2, still equipped with Boxpok wheels, could be photographed in Bebra. Photo: 8474tim (CC-BY-SA-3.0)

But the DDR failed with these wheels. The production capacities for steel castings were limited, but a production was pushed through for the Deutsche Reichsbahn. 01 504 was the first to receive this type of wheel and with it a side skirt under the circulation plate to soften the bulky impression of the wheels.

But soon the complaints of the locomotive crews increased because of unsteady running above 100 km/h, the suspicion quickly fell on the new wheels, especially as these also eventually tended to crack. The conversion of further locomotives to disc wheels continued regardless of this, until the total number of twelve was reached.

In the mid-sixties, the conversion to newly constructed, reinforced spoked wheels finally began, and the circulation aprons were also removed. 01 503 was the last to be in service with disc wheels until July 1976. They show what had gone wrong and also how it could have been solved.





Die spätere Zeichnung seitens der DR zeigt die Baureihe 01<sup>5</sup> in der wohl vertrautesten Form immer noch mit spitzer Rauchkammertür, aber nun ohne Umlaufschürze und mit Öltender. Abbildung: Merkbuch für Triebfahrzeuge 939 Tr, Sammlung Trainini®

The cause of the running difficulties were manufacturing errors. The foundries had arbitrarily deviated from the drawings of the VES-M hall. The imbalances were mainly caused by uneven wall thicknesses, and the wheels exceeded the pre-calculated total mass by about ten percent! Subsequently procured wheels, where these sloppiness's had been eliminated, therefore ran perfectly. The replacement of 01 503 was then only carried out for reasons of standardisation.

In retrospect, however, the choice of Boxpok wheels seems incomprehensible and remains an unusual episode in German locomotive history. In the United States, only machines with more maintenance-friendly frames received this type of wheel, in the Soviet Union it was only machines with far more than 20 tonnes axle pressure. A requirement on the part of the DR does not seem conclusive.



Today's museum locomotive 01 519, now long since converted back to coal firing, was the first locomotive of its class to leave the Meiningen works with oil firing. On 26 May 2019, it is on its way with a special train of eleven passenger coaches from Oldenburg to Düsseldorf, photographed near Ratingen West. Photo: Marcus Henschel, Sammlung Eisenbahnstiftung



The following experiment did not bring any further increase in performance: 01 504 was temporarily fitted with a Giesl flat ejector ("Quetschesse"). As a result of the excellent boiler design, however, no further advantages could be achieved with this.

The situation was different with the oil main firing, which also relieved the stoker noticeably of physical work: From the conversion year 1964 it was part of the factory conversion. The 01 519 chosen by Märklin as the prototype was the first 01<sup>5</sup> to leave the Meiningen works in this form. All the others up to and including 01 535 followed in the same way. With the exception of seven express locomotives, this conversion was carried out subsequently in the years 1965/66.

## Operational use

Immediately after the conversion, the class 01<sup>5</sup> express locomotives were used in the heaviest and most demanding services. Thus, the Bw Erfurt-P became their first home. When the oil-fired locomotives also followed, their stock was divided somewhat equally between Erfurt-P and Wittenberge, while the seven coal-fired locomotives that were still in service were brought together at Berlin-Ostbahnhof.

While they were only used within the DDR from Berlin, those from the Erfurt-P and Wittenberge depots also reached Bebra and Hamburg. In this way, railway enthusiasts in the West also got their own view of the impressive steam locomotives and they also became a popular photographic subject, including in the Wende-Bw Hamburg-Altona with the two turntables protruding into each other.



During the passage of special train DPE 52288, 01 519 shows what the face of class 01<sup>5</sup> should look like properly at Leverkusen-Schlebusch on 2 June 2018. Photo: Joachim Bügel, Sammlung Eisenbahnstiftung.

In 1972, the star of the class 015 began to sink. The interzonal trains to Bebra were now hauled by the new class 132 diesel locomotives. Erfurt was thus able to hand over its locomotives to Saalfeld, Wittenberge, and Pasewalk.

continues on page 32





Photo above:  
In 1968, the oil-fired and Boxpok-wheeled 01 508 turns at the Hamburg-Altona depot. Photo: Heiko Hamm, Sammlung Eisenbahnstiftung.

Photo below:  
On 5 June 1980, coal-fired 01 1511-3 arrived at Leipzig Hbf with E 800 from Saalfeld. The exterior of the locomotive has changed considerably: Instead of the pointed smokebox door, it again carries a flat one with central locking. The farewell of the class 01<sup>5</sup> is already in progress. Photo: Wolfgang Bügel, Sammlung Eisenbahnstiftung





Three months before the boiler explosion in Bitterfeld, the Berlin 01 1516-2 was still in front of the photographer's lens. Photo: Leinwand (CC-BY-SA-3.0)

In 1977, Wittenberge also had to disband its stock of oil-fired engines and passed them on to Saalfeld. On 27 November of that year, as a result of water damage, a German steam locomotive suffered its last, but devastating, boiler explosion.

Nine people, including the driver and stoker, were killed and 45 others injured.

The accident happened to the Berliner 01 1516-2 (before 1970: 01 516) in Bitterfeld station, which had to jump in before the delayed D 567 Berlin-Schöneeweide - Leipzig Hbf. The staff still took coal, but no water, so the disaster took its course.

Finally, at the decisive moment, the fusible plugs, which are supposed to dampen or, in the best case, extinguish the fire on the grate when the firebox overheats, failed. They were so heavily clogged with scale, that they could not blow themselves out.



On 24 June 2018, 01 519 of the Eisenbahnfreunde Zollernbahn (EFZ) is transferred back to Rottweil. On the way, it picked up the frame, wheels and cylinders of 50 3565 from the insolvent estate of Dampftradition Oberhausen (DTO) in Oberhausen-Osterfeld and brought this to Hanau. Photo: Marcus Henschel, Sammlung Eisenbahnstiftung



This defect remains a mystery because 01 1516-2 had just completed a general inspection at the Meiningen repair works a few days earlier. As a result of the severe damage, the locomotive involved in the accident was the first of its class to be taken out of service.

For the rest, there was still a future of several years. In 1981, however, more than a dozen were taken out of service. As a result of the lack of foreign currency, however, the DR had to severely restrict its crude oil imports and so it was decreed that all steam locomotives with oil firing were to be decommissioned.



**With 01 519 the circle closes:** The original of the Märklin model survived as a heating locomotive in the form of a torso before it was completed and refurbished again. In the after shot on 24 June 2018, the tender view proves the coal box elevation missing from the model. Photo: Marcus Henschel, Sammlung Eisenbahnstiftung

They were converted into heating locomotives and steam dispensers or passed on to industrial companies. Coal-fired locomotives experienced a brief renaissance; they were passed on from Berlin to Saalfeld to take over the most urgent train services there. But the increased use of diesel traction was soon to take its toll on them from 1982 onwards.

With 01 511 and 01 512, the last two machines of this series were taken out of service in Berlin on 18 December 1985. At least five of them were preserved, one of them in Austria. The history of the 01 519, which Märklin chose as a model, is particularly interesting. It was rather a torso as a heating locomotive before it was refurbished and completed again.

Evaluation page for refurbishment data and base:  
<https://revisionsdaten.de/tfzdatenbank/suche.php?art=1>



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Layout details (Part 3)

## Of Colours and Shapes

***A random arrangement of any vegetation beyond geometric shapes was a theme of the last issue: nature means chaos, order on the other hand is human. But our environment has many more things to offer and which we should take into account: This includes especially colours and shapes.***

You may be surprised today, because this article on designing landscapes will hardly say anything about modelling. Our focus is once again on nature, because we can and must learn from it if we want to arrive at an authentic and coherent looking layout.

Besides the randomness that creates chaos and the look of nature, it is also nature's colours and shapes that provide an individual appearance. This can vary from region to region, but we should take the time to immerse ourselves in what we want to depict and to perceive it consciously.



**In order for a scene to appear as coherent as this one, many things have to be taken into account and consciously observed in the prototypical environment: Shapes, colours, and also the transitions between individual patterns and structures.**

Every photograph would be wasted if we did not penetrate it mentally, sometimes even emotionally. Often, our skill does not depend on our craftsmanship, but on our perceptiveness. Our focus theme this year wants to take you there, to understand these preparatory processes, to experience and skilfully integrate them into your own project.

How often has it happened to you that you stand in front of a layout and think: "That's quite a nice build, but it doesn't look realistic in any way." One of the aims of this article in our annual series is to avoid that happening with your layout.



If what is presented on the layout appears coherent, the vehicles blend in like extras in front of the modelled nature. This way, photos remain longer in the memory and sometimes we have to ask ourselves whether we are looking at a prototype or a model. A skilful interaction between light and shade does the rest.

We spend a lot of money on our rolling stock, but when it comes to investing it in landscaping many modelers are less generous. Unfortunately, however, our trains simply work best as "extras" in a credibly arranged and passionately designed environment. So, maybe it is time for some to rethink the allocation of their financial resources and, in addition, to also allow for a larger time budget to the landscaping part of their layout.

But this is also and in a much greater way a call to be more conscious in your approach to modelling nature, to keenly observe specific details, bring the unconscious into consciousness, to include it into the planning process from the outset, and to just pay attention to details. Many model railway layouts show spring or summer themes and because of the current season, which invites excursions into nature, we are ignoring autumn and winter themes in this article.

## Shades of green

Green is just one colour? Far from it, as becomes clear even at a first glance at the landscaping materials of manufacturers like Noch. The names of the grass fibre variations of some companies already give clear indications of the season for which they are intended.

But the choice is not limited to summer, autumn or even May green. There is as much colourful variety with respect to producers as there is within their product ranges. Materials from different manufacturers intended for the same season can complement each other perfectly or be a poor match. Individual comparisons or test pieces bring clarity.

As a general rule, freshly sprouting greenery in spring is bright, with a lush-looking tone. This applies to





Let your eyes wander and see how many different shades of green you can make out in this landscape photo and how they interact with each other. Towards the horizon, the amount of blue we perceive increases, while the colours fade into the haze of the distance.

grasses and flowers as well as bushes and trees. Over the following weeks, the greens then become stronger and visibly darker.

This can be seen particularly clearly in the denser foliage of the trees. But it also applies to the grass in wild meadows, only there it is less noticeable. Here, drying panicles and withered stalks provide alternating colours and make the overall area appear brighter.

But if you look closely, you will also notice that a meadow covered with tall grass is by no means a monotonous area. We recognise many patches of very different tones in it, which indicate other plants that push back the grass or benefit from water layers near the surface.

Also, such patches are by no means as dry as those that are in shade or partial shade during large parts of the day. In biological terms, these are highly diverse habitats. Each plant will prefer a location whose conditions are best suited for its individual needs.

And it does not even require a deeper knowledge of nature to see such differences. All it takes is an attentive eye, a documenting photo and perhaps some notes. Once we determine the directions of the sky, we can go through the course of the sun in our mind and recognise how the light will change at the observed spot over different times of the day.

While on a meadow, as just described, many shades of green and in summer also yellow alternate, we must not make the mistake of measuring different shades of green in the foliage of the trees. Except for fresh leaves that grow again and disappear in the mass, they all have almost the same shade of green.

This is monotonous, but can be broken up by flowers or fruits. These should then be clearly perceived as such, because they should also attract the attention of pollinating insects such as bumblebees and bees.



**There is no place for monotony where nature conquers its terrain: plants proliferate and fight for the light they need for photosynthesis. This, too, leaves differences in the colours and, above all, the different heights of growth.**

Now allow yourself the time to critically examine your stash of materials and check how realistic it would look when used on a layout. Red hues really have no place in grass fibre mixtures, but they are readily bought and therefore offered in large numbers. This shows all too clearly how little people look at nature and simply buy what has been on dealers' shelves for decades.

Equally disastrous have been, for some time now, these bright green and very obtrusive-looking microfibres, which are offered on the internet as direct imports from the Far East. They do not cost much, but are also not inexpensive. If we look critically at their purpose and effect, they are cheap in the truest sense of the word.

Nothing could be further from a realistic look than a train passing such a meadow in uniform poisonous green growing at the exact same height at all points. It has more semblance with a bad science fiction B-movie. But for those who might be into exactly that, this year's spring novelties from Silhouette feature orange, red and blue grass fibres in contrasting shades...

## Nature is colourful

Nature presents us with many colours, but they are not all equally represented quantitatively. Many summer flowers are white or yellow in their basic colour. Delicate pink or violet can also be found from time to time. Blue, on the other hand, is rather rare.





A flowering rape field can also become an eye-catcher on a layout with a spring theme. Further in the background, a bush shows its white blossoms amidst the first tender leaves. In this photo, almost all colours of the spectrum are harmoniously united. Photo: Collin Bauer

We should also take this into account when we design our model railway. The blue of a cornflower catches our eye immediately because it stands out clearly from the familiar colours. The same applies to the orange-red of the corn poppy. With just a few colourful accents, the attention of the viewer can be directed and an authentic-looking environment created.



White and yellow are typical flower colours. When we encounter them by the wayside, our gaze inevitably lingers on the few specks of colour, so, why not in the model as well?



But, of course, there are also exceptions, such as seasonal seas of flowers that appeal to people's aesthetic sensibilities. A look at nature reveals which colours can be usefully combined here and what proportions they should occupy.

This can be the blossom of the blue gentian in alpine regions as well as a typical summer meadow full of blossoms - created by humans, these would be butterfly meadows, which farmers are, often and for some years now, legally required to let grow along the edges of their fields.

Used in a targeted manner, some visual stimuli can also be created deliberately to attract the viewer's attention. The season to be depicted on the layout must be determined in advance by the model railroader. A golden yellow wheat field creates contrasts and can be found in July, for example.



**Man-made, the strict colour separation reveals. But such a field of tulips will certainly cause eye rubbing on the model railway layout. A sign could be put up to explain which flowers are grown here: "Tulips for self-picking".**

There is definitely no mature corn next to it, because this will only follow from September onwards. The still young plants have not yet formed any fruit clusters. Neither does a rapeseed field correspond to that season, because their bright yellow blossoms are a classic spring feature.

If you like it more colourful, you can look to one of Germany's neighbouring countries. Holland is especially known for its tulips. In April, the flower fields therefore adorn the landscape as colourful stripes.

Since we are not talking about the wild form of a flower here, the colours in the field are neither monotonous nor mixed. They are selectively and separately sown in the seed rows so that they can later be harvested individually.

If we look at this from a bird's eye perspective, it will seem unnatural to us because this view is foreign to us. However, it can be justified, attracts everyone's attention and only requires a little explanation. This can also be done, for example, in the form of a sign at the edge of the field: "Tulips for self-picking". And every visitor to the field knows about it.

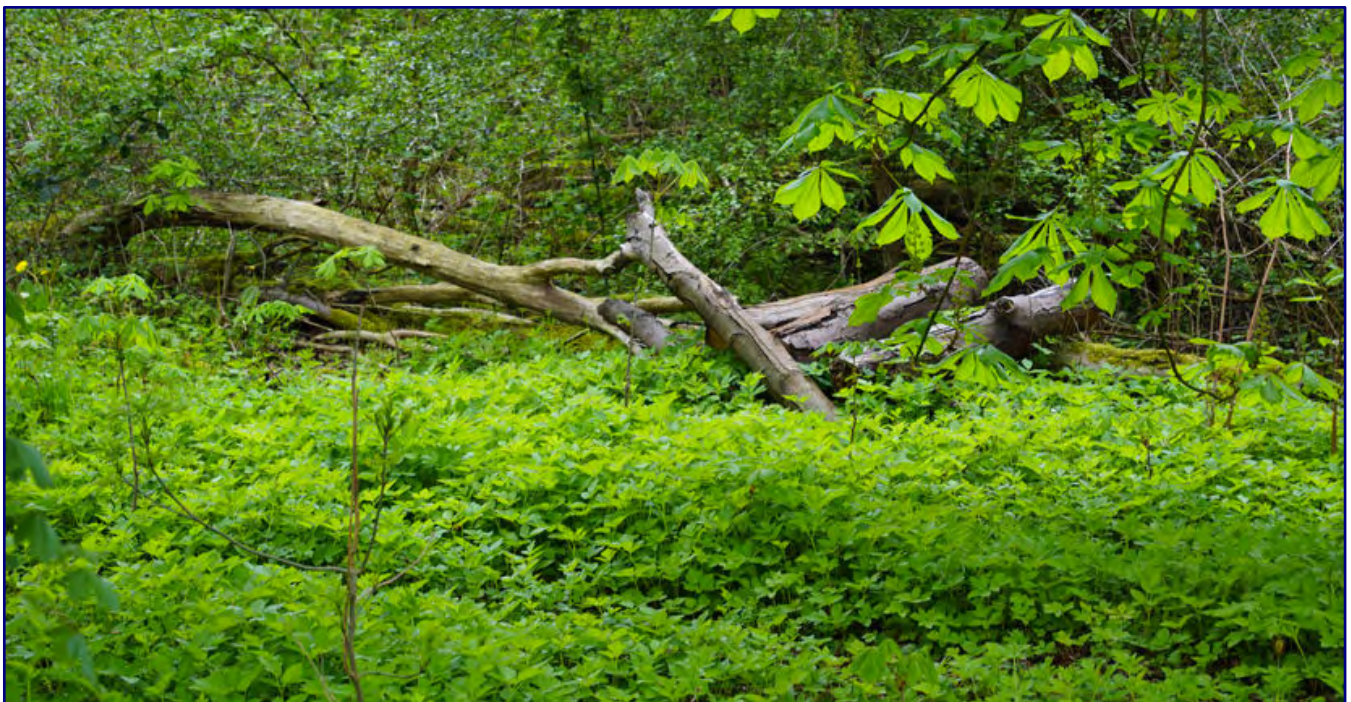


## Nature on the move

The length and abundance of annual or recurrent plants must also be taken into account for an individual appearance. It is not for nothing that we speak of a growing season. When the days become longer and warmer again in spring, nature changes within weeks, if only it has (already) rained enough.



Nature is on the march in spring. And the dandelion, in particular, is able to take hold and thrive in almost any place.



What could better express the term “growing season” than this impression from a nature reserve? The sprouting of the plant shoots can almost be felt. Even on deadwood, mosses are conquering their habitat.





**Shapes and nuances of spring: The ring fern shoots up and creates particularly beautiful impressions in the morning backlight (left). The typical image of its curled leaves (right) is also worth reproducing with the help of fern plants from the MBZ or Noch product ranges. Both photos: Markus Bauer**

This phenomenon is easy to understand when looking at cultured lawns. Just think how often you mow it in spring, summer or autumn? Warmth must be “seen”, especially on a layout.

Subconsciously, we read a layout’s season from the way and abundance in which the vegetation is displayed. As long as the soil receives sufficient moisture, sunlight and warmth do their work.

Grasses literally shoot up and form clearly different growth heights (as well as colours). Sunny spots and those with water veins can now be easily identified. The wild image of nature shows its face most clearly in late spring.

As we approach the height of summer, we often long for a cleansing thunderstorm that brings water and cooling. It is no different in nature. If the sun burns on the earth for up to 16 hours a day, the soils dry out and plant stems burn in the light.

Summer drought is now visible in the landscape. Straw-coloured yellows are spreading by the day whilst green is retreating. Where there is still intact foliage, we mostly encounter dark green colour nuances.





At the beginning of September, i.e., in late summer, the landscape has changed noticeably: The shades of green seem darker, and have been joined by yellow nuances of dried vegetation. In the clearing, however, it is also worth taking a look at the ground along the path, which is also being conquered by nature, at least in part. On the model railway layout, unfortunately, we find such scene usually only depicted through monochrome turf materials.

But also barren and emaciated places now become visible, as well as harvested cornfields. The dry soil that remain there are usually a light brown. Of course, this also depends on the respective soil type, which is unique for the individual regions and can be easily researched thanks to the internet.

## Typical growth forms

We have not yet looked at the trees. On model railways, they are regularly depicted too small, because from a human perspective, the height of their growth cannot usually be correctly grasped. But a difference between a shrub like the hazelnut and a tree should also be perceptible in the model.

In addition to botany pages on the internet, own research also offers good clues. For example, tree heights can be well calculated or estimated the scene includes objects for comparison. Ideally, these are houses that are at the same height as the viewer and whose storey height can serve as a measure of comparison.

For the implementation of the model, it is also important to bear in mind that solitary trees, i.e., single specimens, regularly grow higher than those in forests or larger groups. The reason for this is that most trees compete for sunlight and nutrients, so that each individual tree does not find optimal conditions.





Wind swept trees are typical near or at the coasts (see pines in the background). This photo was taken in autumn 2022 on Fischland-Darß (a Baltic Sea peninsula in Mecklenburg-Vorpommern).

Unfortunately, it is often forgotten how much the shape of a tree determines its appearance. Distinguishing a large poplar from a lime or a plane tree will not be too difficult for most people. But when it comes to trees that have a broad crown and do not strive so much for height, the task becomes more challenging.

Some crowns are more round, others umbrella-shaped. There are also forms that end in a (slight) point at the top, such as the Norway maple. Or just think of the familiar image of an old, gnarled oak. It clearly makes its presence known in an unmistakable way.

For an effective and authentic look, it is better to avoid mixing and distributing trees arbitrarily. Rather, they require some thought and a targeted selection. Here, too, a cheap green Chinese tree will hardly deliver convincing results.



Two trees of life frame a memorial (sign) and convey the impression of a rural idyll. They stand out clearly from the growth forms of the other trees. The light colour of the dry dirt road soil is also worth noting.

continues on page 46





**Photo above:**

With its deep red leaves, the copper beech is sure to attract attention on a layout. However, there should be enough greenery around it so that the viewer does not get the impression of an autumn tree.

**Photo below:**

A particularly striking tree is also the English oak. Old, gnarled specimens show a typical growth which makes them easily recognisable. Photo: Avantgarde Modellbau



This is especially true because each tree species has its own characteristic leaf colour. This cannot be seen on a single tree, but as a dense crown they certainly convey different nuances. This, too, is partly due to “deeper” reasons, because among trees there are shallow and deep-rooted plants. This also influences how well they can access water during dry periods.

The blossoming of trees in spring can also be played with. In many species, (white or pale pink) flowers appear before the first leaves. If we use this in the model, grasses, shrubs and surrounding bushes in bright green ensure help to avoid the impression of that one wanted to imitate the look of hoarfrost.



Even from a distance, a horse chestnut can be easily recognised during its flowering season. The tree is then already fully leafy, but its blossoms, which rise up like candles, determine its appearance to a large extent. By the way, this type of tree is available as a model from Avantgarde Modellbau.

Among the trees with a very distinctive and typical appearance that invites imitation is the horse chestnut, which is found in many parks, along the edge of wide roads or even in parkways.

Its white blossoms gather in very conspicuous candles that are striking and can be seen from afar. They also form an attractive contrast to the foliage that the tree already bears at flowering time. Here we leave the keywords early and late bloomers as a distinguishing feature for the selection on the plant.



## Summary

Convincingly and correctly translating nature into any scale is not witchcraft. Like other challenges of model making, it only requires paying attention to two important points:

- Success needs a plan.  
Everything that is personally perceived as beautiful or is on one's own wish list does not necessarily fit together. Simply building on it is therefore perhaps a recipe for a game plan, but one that will not inspire anyone in the long term because it is neither coherent nor realistic.



It makes sense to deliberately limit yourself to a fixed plant theme. Rural scenes often offer many possibilities for reproducing attractive natural scenes. This can be, for example, a tree-lined country road, as here near Westheim. Photo: Hendrik J. Späing

If you want to enjoy your layout for a long time, if you want to inspire and captivate visitors at exhibitions and fairs, you should consider beforehand which themes (e.g. “unspoilt landscape” or “agriculture” or “urban environment”) and which seasons you want to reproduce.

The final choice of vegetation and its arrangement should be based exclusively on this. If personal wishes remain unfulfilled and cannot be integrated, a diorama with a different content can be used as a supplement or alternative.

- Skills come with practice.  
Experienced model railway professionals who build good layouts for customers are not magicians. They too have learned their craft, acquired new skills and perfected them. Reading guides or articles is a beginning, but not the end.





A key to success is to have an eye for the little details by the wayside, such as this dead tree in a forest clearing. Discovering such a prototypical scene and to transfer it onto a railway layout can make all the difference. But, such an approach does require trials and practice for finding and mastering the right techniques.

Previously absorbed and stored knowledge needs to be put into practice. Small and manageable practice pieces are perfect for developing your own techniques, perfecting them or simply trying out something new. The critical eye reveals whether the way you have found is right or whether you need to find a new one.

- Good things take time.

A new locomotive is bought, it is supposed to run. Fingers are itching to snap away and to immortalise the good piece on the camera's memory card. Now a layout has to be built quickly... Well, that's the point to say "stop" as it can't end well.

A key message we wanted to convey with this article is that a beautiful model deserves an equally beautiful environment. So let's take the time to build a beautiful landscape that complements our new acquisition without taking centre stage. For a single locomotive, an area of 20 x 10 cm is sufficient to create a backdrop.

This is something we can practise on and we can approach a layout project with much more peace of mind. Even though model railway layouts are known to never be finished, we should at least try our best to hide this from the viewer, shouldn't we?

The author of these lines has evolved as a modeler along the path described here. Hopefully, this article will stimulate your thoughts and make you walk through nature "with your eyes wide open". And don't forget to take your camera (phone) with you.





Creating convincing looking landscapes like this one here are possible with developing a keen eye for nature and disciplined practice.

The next modelling season is already closer than it may appear. And when it arrives, we can all get to work even more joyfully.

Historical editions for reference:  
<https://www.trainini.eu/z-world/trainini-archives>

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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

## Lebenswerk von Ludwig Rotthowe **Eine posthume Würdigung**

**Ludwig Rotthowe gehört zu den bekannten Eisenbahnfotografen der fünfziger bis siebziger Jahre. Aber auch darüber hinaus war er noch Jahrzehnte mit der Kamera aktiv. Dass Eisenbahnfreunden sein Name nicht gleich als erstes einfällt, liegt wohl am ehesten an seinem bescheidenen Auftreten. Im EK-Verlag wurde nun sein Schaffen zusammengefasst und ausführlich geehrt.**



**Burkhard Beyer**  
**Alte Meister der Eisenbahn-Photographie: Ludwig Rotthowe**  
**Der bekannte Fotograf aus Westfalen**

**EK-Verlag GmbH**  
**Freiburg 2023**

**Gebundenes Buch**  
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Eigentlich sollte an dieser Stelle eine andere Rezension stehen: Gedacht war an Ludwig Rotthowes zweites Buch „Dampflokotiven 1956 – 1976“, das im selben Verlag wie das nun vorgestellte erschien. Insgesamt fünf sollten es bis 2018 werden.

Dieses sechste ist das erste, das posthum ohne sein Mitwirken erschienen ist. Und doch scheint es, als träfe es das Wesen dieses bekannten Menschen am besten. Und dabei ist sich der Rezensent, der ihn leider nicht persönlich kennen lernen durfte, auch sehr sicher.

Nur wenige Aufnahmen aus dem genannten Frühwerk haben es auch in dieses Buch geschafft. Doch das allein ist es nicht, was es so anders macht und eine klare Kaufempfehlung rechtfertigt. Waren es zunächst Bände für Dampfliebhaber, weitete sich der Fokus auf die beiden anderen Traktionsarten.

Doch immer stand die Eisenbahn, oft nach Baureihen geordnet, im Vordergrund. Nun ist es die Person Ludwig Rotthowe, der ein Andenken geschaffen wird, das auch anhand einer Auswahl seiner gelungensten Werke erzählt wird.

So kommt es, dass die Bahn in diesem Buch bisweilen in den Hintergrund rückt und eher mal eine Funktion als Statist einnimmt. Viele der ausgewählten Aufnahmen kommen sogar ganz ohne Lokomotiven und Wagen aus. Und wenn sie vertreten sind, dann bilden sie einen repräsentativen Querschnitt des Bundesbahn-Fuhrparks ab.



Schön ist auch, dass die zeitliche Spanne ebenso groß angelegt ist. Von den fünfziger Jahren – aus der Zeit der Ausbildung des Ludwig Rotthowe – bis in die frühen Neunziger reicht die Bandbreite hier. Herausgeber Burkhard Beyer erläutert sehr ausführlich das Spektrum des verfügbaren Materials, der daraus noch vom Fotografen selbst getroffenen Auswahlen und seine Entscheidung, was in dieses Buch gehörte und wie es inhaltlich zu strukturieren war.

Herausgekommen ist ein erfrischend anderes Werk, das wir im Folgenden nun auch ins Leben seines Urhebers einordnen möchten, um es unseren Lesern gleichermaßen nachvollziehbar aufzubereiten. Nahtlos fügt sich der vorliegende Titel in die EK-Buchreihe „Alte Meister der Eisenbahn-Photographie“ ein und zeigt trotzdem eine besondere Eigenständigkeit.

Ludwig Rotthowe ist am 18. Oktober 2017 im Alter von 80 Jahren verstorben. Bei seinem Ableben zählte er zu den wichtigsten und bedeutendsten Eisenbahn-Lichtbildnern in Deutschland. Mit der Eisenbahn kam er direkt vor der Tür seines Elternhauses in Telgte (nahe Münster in Westfalen), das er zeitlebens bewohnte, in Kontakt. Sie übte durchaus einen Einfluss auf ihn aus, ohne dass er wie viele andere seiner Generation zum „Dampflokjäger“ wurde.

Mit 15 Jahren begann am Heimatort eine Lehre zum Fotografen, noch im gleichen Jahr entstanden seine ersten Eisenbahnfotos – teilweise auch mit geliehenen Kameras seines Meisters. Dieser war zuvor im Ruhrgebiet in der Industriefotografie tätig und gab sein Wissen an den kreativen Rotthowe weiter. Auch die Portrait-, Architektur- und Fotografie in der Landschaft gehörten zu den Ausbildungsinhalten.

So mit dem handwerklichen Können gerüstet, konnte er seinen kreativen Geist auf die ihm eigene Art entfalten. Beeinflusst wurde er nach eigenen Angaben auch durch die Eisenbahn-Landschaftsfotografie von Toni Schneiders und Jean-Michael Hartmann.

All das sollte er in seine eigene Handschrift einbauen, die von anderen zeitgenössischen Fotografen abhebt. Gerne integrierte er auch Menschen, darunter seine Freundin und spätere Ehefrau oder auch Bekannte „wie zufällig“ in seine Bilder. Dasselbe gilt auch für so banale Alltagsdinge wie Schrankenanlagen an Bahnübergängen oder Formsignale, häufig schön aufgereiht. Mit Licht und Schatten wusste er ebenso geschickt zu agieren wie mit Statik und Dynamik.

Den Rezensenten hat einst zunächst besonders angesprochen, dass Ludwig Rotthowe gern den Eisenbahnbetrieb in seiner unmittelbaren Umgebung interessiert hat. So hielt er Motive von der Emslandstrecke, der Rollbahn, aus Rheine oder Münster fest, die sonst beinahe vergessen wären, weil das Direktionsarchiv der früheren BD Münster seit Jahrzehnten als verschollen gilt.

Doch er war gern und teilweise auch weit entfernt unterwegs. Beispiele sind München (nicht in diesem Band), Altenbeken, Bullay, Dortmund, Hamm (Westfalen) Emden, Norddeich oder auch Hamburg. Zu seinen Favoriten gehörten auch die Obere Ruhrtalbahn, die Rhein-Sieg-Strecke sowie die Nebenbahnen des Sauerlands.

Diese Fülle an Aufnahmen in bester Qualität und ebenso hervorragender Reproduktion macht dieses Buch so einmalig und wertvoll für Eisenbahn- und Modellbahnfreunde. Gleich, ob jemand in Erinnerungen schwelgen, Fotos genießen oder Anregungen für die authentische Modellbahngestaltung finden möchte: Alle Lesergruppen werden hier gleichermaßen gut bedient.

Abschließend gehen wir noch einmal auf die Struktur dieses Bands ein: Die Bilder wurden nicht nach Baureihen ausgewählt und angeordnet, sondern nach Themen. Diese widmen sich der unmittelbaren Umgebung des Wohnorts, dem Hauptbahnhof von Münster, dem Spiel mit Telebrennweiten oder anderen Sachgebieten wie Brücken, Tunnel, Viadukte oder Berge. Eigene Kapitel sind auch der Nacht, dem Gegenlicht, dem Winter oder Personen und Signalen gewidmet.

**Publishing pages:**  
<https://www.eisenbahn-kurier.de>  
<https://www.ekshop.de>

Filmschätze von Ton Pruissen

## Wertvolle Zeitdokumente

**Die Emslandstrecke ist ein klassisches und beliebtes Thema, das schon viele DVD gefüllt hat. Es scheint bisweilen unvorstellbar, dass mit diesem Fokus noch etwas wirklich Neues kommt. Doch Menschen irren sich und so geriet die an dieser Stelle vorgestellte Datenscheibe für uns zur Überraschung des Jahres.**

Ton Pruissen

So war sie damals, die DB - Teil 4

Die Emslandstrecke, Emden und Oldenburg

Nord Süd Express GmbH

Gröbenzell 2022

DVD-Video

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Tonformat Dolby-Digital 4.0

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Vielleicht stutzen Sie zu Beginn dieser Besprechung, weil Ihnen der Verlagsname Nord-Süd-Express (noch) nichts sagt. Doch das wird sich auf dieser und der nächsten Seite gewiss ändern. Und Sie werden einen ausgesprochen positiven Eindruck mitnehmen!

Hinter dem noch jungen Verlag mit großer Erfahrung steckt vor allem Geschäftsführer Thomas Hilge, der zuvor in gleicher Funktion beinahe 20 Jahre bei der VGB Verlagsgruppe Bahn tätig war, bevor diese an einen Wettbewerber verkauft wurde. Das breite und hochwertige Verlagsprogramm, das seine Handschrift trug, veranlasste ihn, ein solches Angebot auf eigene Rechnung ins Leben zu rufen.

Es geht also auf jeden Fall weiter! Neben der neuen Zeitschrift Eisenbahn-Klassik, von der wir in Ausgabe 7/2021 bereits ein Heft vorgestellt hatten, folgten in der Zwischenzeit erste Bücher und nun auch ein eigenes DVD-Angebot. Inhaltlich dafür verantwortlich zeichnet der niederländische Eisenbahnfreund Ton Pruissen, der sowohl sein eigenes Material auswertete, als auch auf Aufnahmen weiterer Begeisterter zurückgriff.

Und der Produzent des heute besprochenen wie auch weiterer Titel verspricht nicht zu viel, wenn er bekundet, dass es kein Neuschnitt bereits veröffentlichter Streifen ist. Keine einzige Aufnahme ist uns als schon bekannt ins Bewusstsein getreten, wenngleich natürlich einige Kameraperspektiven ebenfalls zu den bevorzugten anderer Filmfreunde gehörten.

Breit ist das Spektrum des gezeigten. Es reicht, wie dem Titel zu entnehmen ist, von der Emslandstrecke, bis nach Ostfriesland und Oldenburg. Dabei arbeitet sich Ton Pruissen geographisch und nicht chronologisch voran.

Die Reise beginnt im Bahnhof Rheine und führt in Richtung Norden über den Block Bentlage samt Bahnübergang und Salzbergen weiter bis nach Leer (Ostfriesland) und Emden sowie schließlich



Norddeich Mole. Quantitativ nimmt dieser Teil deutlich den größten Umfang ein, was sich auch mit der „Magnetfunktion“ dieses letzten Dampflokdomizils erklärt.

Allerdings mischt der Produzent und Filmer hierbei die unterschiedlichsten Aufnahmen. Sie entstanden ab Mitte der sechziger Jahre und reichen bis zu einem Zeitpunkt rund zwei Wochen vor dem endgültigen Aus der Dampflok. Sogar Führerstandsmitfahrten sind auf der DVD zu filmen, teilweise erleben wir sogar Mehrfachperspektiven derselben Szenen.

Gut verstanden hat es der Niederländer also auch, neben einem „roten Faden“ auch einen spannenden Schnitt zu finden. Zu seinem anerkennenswerten Können gehört es aber auch, Schwarz-Weiß- und Farbsequenzen zu miteinander zu verbinden, dass der Zuschauer nicht mal gleich den Zeitsprung dazwischen bemerkt.

So hat er einzelne Szenen am jeweiligen Schnitt nachträglich koloriert, was dann als Blendenfunktion genutzt wird, um die Bildfolgen ansprechend sowie schlüssig und flüssig zu verbinden. In einem Eisenbahnfilm war uns dieser Kunstgriff bislang unbekannt, er tut allerdings auch hier dem Gezeigten sehr gut.

Kurz ergänzen möchten wir noch einen weiteren Überblick über die Themenfülle: Auf der bereits aufgezeigten Reise entlang der Strecke bereichern auch einige Unterwegsmotive die tollen Bilder. Die markante Emsbrücke in Hanekenfähr und sogar ein Besuch im AW Lingen bereichern diesen Film enorm. Besonders der Blick auf die Hauptuntersuchungen der Dampflokomotiven offenbart, dass Ton Pruissen die Emslandstrecke deutlich früher als andere Enthusiasten besucht hat.

So hebt sich diese DVD überraschend deutlich von vielen anderen ab, die wir bereits gesehen (und teilweise auch besprochen) haben. In bewegten Bildern sind Einsätze der Neubaudampflok Baureihe 82 vor Personenzügen von Emden West nach Emden-Außenhafen sonst selten oder gar nicht zu sehen.

Sogar die einst in Emden beheimatete Baureihe 23 ist hier wiederholt im Einsatz zu erleben und erinnert uns gleich an das Bahls-Modell für unseren Maßstab, dessen Vorlage aus einer dort beheimateten Serie stammt. Auch die ehemalige P 8, die sie ablösen sollte, steht im Film noch im täglichen Einsatz.

Für Zetties aktuell von Interesse sind bestimmt die vielen Silberlinge, die in vielen Zügen aller Kategorien zu sehen sind: Wie hoch reichte das geschliffene Pfauenaugenmuster? Wie waren sie beschriftet? Und welche Darstellungsform zeigt der Ege-Keks auf ihren Längsseiten? Impressionen für den Anlageneinsatz gibt es hier zuhauf.

Längst vergessen schienen auch die Einsätze der Baureihe 01 (mit Neubaukessel wie 01 133) oder auch der Baureihe 03 auf der Emslandstrecke, die einst die Standardschnellzuglok im Verkehr an die Nordsee war. Selbst die Baureihe 01<sup>10</sup> Kohle, ab 1968 Baureihe 011, taucht sonst selten auf Filmaufnahmen auf, weil das Material teuer war und das Bw Rheine mit seinen Maschinen noch nicht in den breiten Fokus gerückt war.

Diese DVD zeigt eine ungekannte Fülle an Eindrücken und besticht zudem durch den großen Aufwand, der für das Digitalisieren, Reinigen, weitere Aufbereiten und professionelle Nachvertönen getrieben wurde. Wir kennen schlicht nichts vergleichbar Gutes!

**Publishing pages:**  
<https://www.nordsuedexpress.de>

**Explaining digital film processing:**  
<https://www.youtube.com/watch?v=f3i-PYjvaMg>

Readers' letters and messages

## Zetties and Trainini in Dialogue

**Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.**

**Note on the train set "V 36 Railbouw Leerdam" (81771):**

However, the model (Märklin 81771 from the summer new products; editor's note) is not a Hippel! An alternative from Märklin, but it is a V 36 true to the prototype! This was: Deutz V6M 536 R (make no. 55824, built in 1955).

Leon N. Polderman, Moerkapelle (Niederlande)

Editor's reply: We also received the same information (with explanations and photo evidence without publication rights) from other readers. They are entirely justified and correct. We would therefore like to thank our readers, in the Netherlands, in particular, who noticed this immediately and helped us to correct the error in the report in the last issue.

**Märklin deliveries and recall in June:**

Right at the turn of the month May / June Märklin delivered the two-piece Hupac semi-trailer wagon pack (item no. 82273). The two, grey painted Sdkmms type wagons each carry a truck semi-trailer of the Swiss freight companies "Planzer" and "Schöni."



The Hupac wagon pack with trailers from Planzer and Schöni (item no. 82273) has now arrived at the dealers. Photo: Jörg Erkel, 1zu220-Shop

Shortly before, the Vectron class EU-46 / class 370 of PKP Cargo (88237) in Era VI condition appeared on the market. The model with four pantographs technically follows its predecessors and is also painted grey. The fronts and the front roof are red, on the sides there is a large owner's lettering in white.



The DB's 50 1019 (88846) with cabin tender is a welcome addition. The Era III model is the first reproduction of a three-domed version of this class in Z gauge. In order to be able to implement this as a mould variant, Märklin resorted to brass investment casting from lost moulds.



Variety is offered by the class 50 freight steam locomotive (88846), offered for the first time in a three-dome version, which knows how to convince and inspire us.

This can be seen in some places on the painted and fully labelled model: Pumps and steam bell have not been applied separately here, but are part of the body. That is why the bell is also painted in the same colour as the housing instead of having a bright galvanic coating and why the openings through which the attachments are usually passed are not present on the circuits.

The model is illuminated with warm white LEDs, powered by a bell-shaped armature motor and has full detail controls including reproduction of brakes, rail clearers, and sand pipes. The printed window frames of the tender cabin are also very appealing.

The train set "Wiebe with 320 001-1" (81320), in which the first V-320 variant of the regular catalogue programme is presented, has also arrived in the shops. It is offered here together with two Fcs side unloading wagons and two Res low side wagons as a construction train.

But as soon as they hit the shops, the joy is over: Märklin issued a recall for this variant, as well as for the Insider locomotive (see also the information box at the test report). The models should not be put into



The joy about the delivery of the Wiebe construction train package with 320 001-1 (81320) only lasted for a short time, then a product recall already followed. To make matters worse, our review sample also has optical and other technical defects.

operation because excessive heat can develop in the locomotive electronics due to a possibly defective component. Models should therefore be sent to the Märklin repair service for reworking.

The following impressions can be given about the contents of the package: The wagons appear with Wiebe logos, but otherwise look largely familiar in their presentation. The locomotive model we received makes a good overall impression, but the paint on one of the front handle bars was chipped, which should have prevented it from being delivered to the customer anyway.

The DB crew car (87002) based on a Württemberg passenger car and the KkIm 431 stake car (82103) loaded with three grey oil tank containers have also been delivered.

**Faller has also delivered a new product:**

With the "Old Spinning Mill" (art. no. 282742) Faller has delivered a very interesting hard cardboard architectural kit. The scope of delivery includes 131 individual parts in four colours, which are assembled into the building with window foil and curtain mask. Illustrated instructions are included for success.



The basic dimensions of the house are 102 x 37 x 57 mm and ensure that the quite functional, yet somewhat monumental-looking house can also be integrated into the cityscape in different functions. We can imagine it as a grammar school building, but also as a public administration building or on a company site.

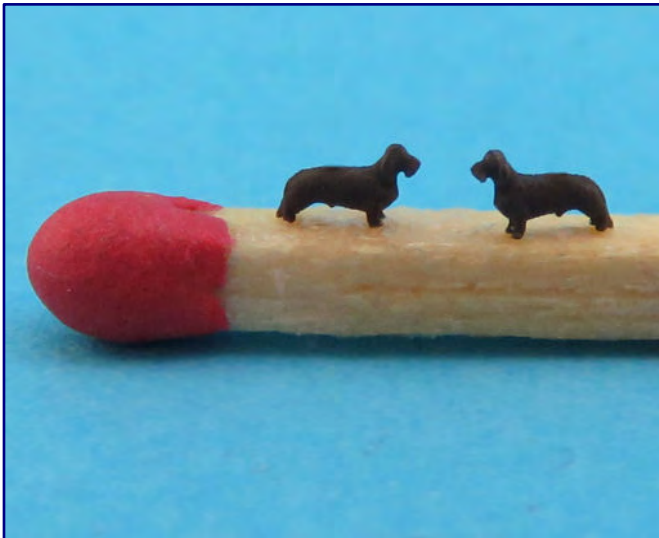
**Photo on the left:**  
The "Old Spinning Mill" is now available as an architectural kit by Faller.



**Klingenhöfer Miniatures enhances the animal world:**

New at Klingenhöfer Miniaturen (<https://www.klingenhoefer.com>) are new figures of dogs from the 3D printer: With Dachshund, West Highland Terrier, and Newfoundland dogs, three of the most popular breeds, can be used as models on the layout.

With the dachshund, of course, you can also go hunting for the fox's den, because Meister Reinecke has also been in the range for several years. If you want to recreate such a place at home, you may not be able to avoid the new owls, which will watch motionlessly from the treetops.



Two of the new dog breeds from the Klingenhöfer novelties: Dachshund (left) and Newfoundland (right). Photos: Klingenhöfer Miniaturen



This bottlenose dolphin, colloquially known simply as a dolphin, is likely to meet with warmth. Photo: Klingenhöfer Miniaturen

The ravens, which are also included in the programme, are more intended for the open field and usually search a harvested field in flocks. As crop followers, they also appear in small groups in the vicinity of residential buildings.

The highly intelligent dolphins are popular because they are considered good-natured. The marine mammals can be accommodated in a dolphinarium of the model zoo as well as used to populate a North Sea or Mediterranean section of the facility.

#### JSS-Elektronik returns:

JSS-Elektronik (<https://www.jss-elektronik.de>) has been quiet for a long time, but now Jörg Seitz has announced some new products. They are now offering decoder adapter boards for Z gauge. With these, the supplier wants to achieve that, as little as possible, should be changed on the locomotive.

The original circuit board is exchanged for the product and a separate decoder is applied. That is the idea and the concept of this development in a nutshell. Unfortunately, the manufacturer's pages were under maintenance at the time of our research. Readers may therefore have to be patient for a while.

Something new is also planned for the control area. A WLAN CAN interface is already finished internally and is undergoing practical tests. It is simply connected to a digital track box with the Mobile Station (Märklin) and connected to the WLAN. This allows the control of small layouts with the PC, possible are WinDigiPet or Rocrail.

#### And here are the AZL summer deliveries:

In June AZL will deliver the ALCO PA1 in the bright blood orange-black livery of the New Haven. This is the 3rd version, which is available with a choice of two operating numbers (art. no. 64425-1 / -2).

The R-70-20 refrigerated wagons are back in the programme in a new BNFE version (Burlington Northern Fruit Express): this time, the roof of the orange-coloured wagons is also painted in the same colour as the wagon body. Available are a double (914843-1) and a four-pack (904843-1).



The heavy half luggage trolley (item no. 74007-1) of the New York Central is almost a novelty. Photo: AZL / Ztrack

The Trinity RAF pillar wagons with a length of 53 feet are now preceded by semi-trailers from Ryder. The three-rein unit (905238-1) is set to TTAX and carries their old logo. Two trailers (954014-1) can be purchased separately again. Last novelty are the heavy half luggage cars (74007-1 to -3) of the New York Central in dark green. The still very new cars run on two-axle bogies.

#### Full Speed at Yellow Dwarf:

Yellow Dwarf (<https://www.yellowdwarf.eu>) continues to deliver new products at a rapid pace. The latest additions are modern flower pots (art. no. 60459), more classical specimens (60461), ornamental hedges with fountains (60473), and decorative hedges with a labyrinth (60474). They are all suitable for decorating a park or larger garden, such as that found at a castle.





Ornamental hedges with fountain (art. no. 60473; photo left) and decorative hedges with labyrinth (60474; photo right) are the names of these two new products. Photos: Yellow Dwarf

For the spontaneous traveller, the “Tents II” (60502) now complement the existing range. All accessories are supplied as unassembled and unpainted 3D printed parts. They can be coloured with model building paints and, if necessary, assembled with superglue.

#### Modellbahn Union expands house series:

In the new series of relief houses, Modellbahn Union (<https://www.modellbahnunion.com>) has now also introduced three corner town houses matching the previous series. The 90° angle house front follows the style of the rest of the series.



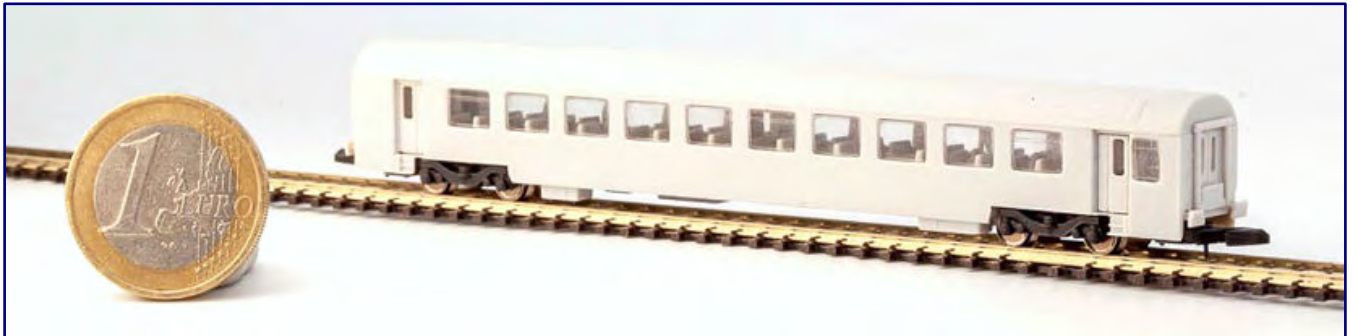
The new corner townhouses in relief construction are available in the colours grey (art. no. MU-Z-H00193; photo left) and sandstone (MU-Z-H00203; photo right), among others. Photos: Modellbahn Union

Once again, solid-coloured hard cardboard is used, which does not require any colour finishing. A grey (art. no. MU-Z-H00193), white (MU-Z-H00198) and sandstone-coloured version (MU-Z-H00203) of the Gründerzeit are offered.

#### New project status at Azar Models:

Azar Models (<https://azar-models.com>) has sent us some new information about the ongoing projects of French Z-gauge models. The first parts of the Corail-vtu coaches have been delivered from the moulds

and have been assembled for testing. The necessary corrections are currently being implemented; the first series of these internationally operating passenger coaches is planned for autumn 2023.



One of Azar Models' two well-advanced projects is the SNCF Corail vtu passenger coach that was used for international traffic. Photo: Azar Models

In the case of the electric locomotive CC72000, the corrections to the shape of the body have now been completed. The fineness of the grilles has been revised once again with a high-quality result. The mechanics and bogies are also finished. By the way, the chassis will be made of metal to ensure sufficient weight for good traction.

The manufacturer expects the first painted pre-series samples in July 2023, and production will then probably begin in September. The German sales partner for this supplier is 1zu220-Shop (<https://www.1zu220-shop.de>).

#### Series 3 from WDW Full Throttle:

Ten years ago, WDW Full Throttle (<http://www.wdwfullthrottle.com>) started with the first of three reefers series. These were the rail series (9000 article number group), followed by the food series for meat, poultry, dairy products and fruit and vegetables (9200).



The Miller brand launches the new series of American beer wagons based on the wooden modernized reefers. Photo: WDW Full Throttle



Now the third series of 34-foot-long wooden wall reefers is to be launched before the end of June. This is the beer wagon series (9400). The first limited edition (item no. FTB9405) is dedicated to the well-known Miller Brewery of Milwaukee.

They have brown end walls and roofs, while the side walls shine strikingly yellow and are printed with the well-known Miller logo in black as well as company inscriptions. The models are supplied in double packs.

WDW products are distributed in Germany by Case Hobbies (<https://case-hobbies.de>), among others.

#### Herpa announcements for midsummer:

For the high summer months of July and August, Herpa announces the following new Wings models in 1:200 scale:

Lufthansa Cargo Airbus A321 "Hello Europe" (Art.-Nr. 572439),  
Maersk Air De Havilland Canada DHC-7 (572637), and  
LOT Polish Airlines Tupolev TU-154M (572712).



The Tupolev TU-154M of LOT Polish Airlines (Item No. 572712) is announced as a novelty in 1:200 scale. Photo: Herpa

According to the military model, the following aircraft has been re-issued with modifications (such as the identification number):

German Air Force Airbus A400M "Atlas", LTG 62, Wunstorf Air Base (557207-004).

In the Snapfit model range, there are two 1:200 scale versions as well as an equally suitable 1:250 scale version for hanging in the model railway sky:

Air France Concorde, 1:250 (605816-001),  
Edelweiss Air Airbus A320 "Help Alliance" (613712), and  
TAP Air Portugal Airbus A321LR "Amália Rodrigues" (613835).

#### Märklin participation at the International Toy Fair 2024:

Märklin will also be taking part in the Spielwarenmesse 2024. This will take place from 30 January to 3 February in Nürnberg (Nuremberg). A consumer day is again planned for the Saturday of the fair (3 February) in the model making hall.

Märklin explained to the dealers the reason for their renewed participation in the fair: “We were also able to take many suggestions from the discussions into our daily work. A valuable experience that we missed in the two years of the pandemic. The dealers present made intensive use of the opportunity to exchange ideas with each other.”

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# Trainini

German Magazine for Z Gauge



Pop Cars in Z Gauge

Human Traces  
The Intellibox 2neo

## Introduction

Dear Readers,

This could be a summer after my own heart: So far, extremely hot and dry days have followed each other only for a manageable period of time. The following days were usually much cooler and also brought rain to the garden.

For excursions as well as photo and film work, this means that there is often diffuse light outside. Thanks to clouds, the light is distributed more evenly and does not cast harsh shadows. Of course, these are part of life and also of the model railway, but they are rarely desired for documentaries.

And that brings me to this issue: "Layout Details" is the name of our annual theme for 2023, which would like to bring you closer to the many small things in everyday life that are usually considered marginal. In addition to tinkering tasks, a high level of observation is required here.

Going outside and looking at the prototype is recommended these days and is also used by me actively, because accompanying our reports, an episode for our video channel **Trainini TV** is also planned. The time of year seems perfect, as the vast majority of model railways are also dedicated to summer themes.

This month, my colleague and friend Dirk Kuhlmann continues our series. He is now looking at all the things that people leave lying around or put down somewhere, and then finally tend to forget. Such legacies characterise gardens, yards and company premises until the area around them is conquered by nature. That is why they should not be missing on the model railway, although they should be used skilfully and in a targeted manner.

Judging by the number of pages, however, the pop cars of the Bundesbahn dominate this month's issue. This was not originally planned at this point in time, but two construction projects could not be completed in time and therefore have to be re-adjusted in our issue planning – they should fit better anyway, when you, dear readers, are also drawn to tinkering in the basement again.

Apparently, the 3D printed figures from Noch, with which we already had very precise ideas about for upcoming articles, have also been delayed. So, a large-scale swap of contributions and magazine themes was recommended, which brought one of my personal favourites into the limelight in a big way. I hope that the large-scale colour experiment of the Bundesbahn from 1970 to 1974 will also inspire you in prototype and model alike.

Last, but not least, the Intellibox 2neo from Uhlenbrock should not be missed in this issue. It has been on the market since the end of 2022 and is also arousing the interest of digital railway enthusiasts in Z gauge. In order to give you useful help with upcoming purchase decisions, we have taken a look at it and with today's product portrait are catching up on an article that has also been planned for some time. Supplemented by our literature tips and current news, I now hope you enjoy reading **Trainini®** 7/2023!

Sin-Z-erely,

Holger Späing



**Holger Späing**  
Editor-in-chief



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### Cover photo:

110 250-8 has belonged to the Stuttgart depot since October 1963 and only lost its original livery in May 1978. In the seventies, the locomotive was used on sections of the Dortmund - München (Munich), route in front of pop-coloured trains.

## The Era of the Pop Colours of the DB **The Country needs new Colours**

*The pop cars attracted attention on the German Federal Railways, are still popular today, and are an attractive splash of colour on model railway layouts from Era IV. However, they were not able to establish themselves in the prototype. Instead, their popularity with people reverberates in the model to this day. We take a look at their short-lived history and some models.*

The term pop car is probably familiar to every model railway enthusiast, but this, like the terms pop colour scheme or pop colour concept, are not terms that the Deutsche Bundesbahn itself has ever used.

Nevertheless, what was later understood by it does not seem to have been named so arbitrarily. A reference to Pop Art, which already emerged in the fifties and reached its peak in the sixties, cannot be overlooked. It was characterised by a rather trivial appearance and mostly pure primary colours.



Only a few trains were made up exclusively of pop cars. And even those that were scheduled to do so more often saw passenger coaches in the classic colours in their ranks. Nevertheless, this shot from 1974 is unusual, because the only two deviants at the end of the train already wear the DB colour concept that had just been introduced and was final. Photo: Volker Credé

The conspicuousness thus achieved also applied to the passenger carriages that DB included in its concept, a total of 145 passenger carriages (not including prototype and S-Bahn carriages). It followed the zeitgeist of the late sixties, and DB had its finger on the pulse of the times when its ideas to replace the classic and rather dark appearance of passenger trains took shape.



A look back into the past makes it clear why DB was looking for a modern and friendly appearance: the majority of vehicles were painted monochrome in muted shades. 1<sup>st</sup> class passenger coaches were painted steel and later cobalt blue, those of the 2<sup>nd</sup> class chrome oxide green and dining and sleeping cars purple. Only the TEE and the Silberlinge provided variety on the rails.



Sleeping cars in pop colours with the identification colour crimson red for the DSG were among the rarer examples of this colour experiment. In 1975, the year the photo was taken, two such carriages are shown here at the end of the train. Photo: Volker Credé

The locomotives and multiple units were no different. The still numerous steam locomotives presented themselves jet black, the electric traction chrome oxide green or steel blue, while diesel locomotives and railcars were crimson red on the road.

There were also comparable considerations abroad, which led to the appearance of lighter-coloured vehicles in isolated cases. In Japan, the new Shinkansen trains of the series 0 entered service in 1964, which, like the Rh 4010 (Transalpin) of the ÖBB, which entered service only one year later, appeared in a blue and white design. As early as 1952, the Italian FS chose a grey livery with a green window band for its three ETR 300 “Settebello” express trains.

In this context of international models and the TEE trains also running in Germany, a two-colour design concept was probably obvious. In the case of the pop colours, however, the division between the lighter and the darker colour was swapped compared to the Trans-Europ-Express trains.

## Assignment to the Design-Center

Heinz Maria Oeftering and the Bundesbahn board had set themselves the goal of “turning DB towards the market”. One measure to achieve this was the Design Centre set up in 1969, which was placed under the control of the Bundesbahn Central Office in Munich (BZA München). The task was to express the declared goal in the appearance of the state railway as well.



The 420 140-6, still very young on 17 March 1974, shows on its departure from Düsseldorf Hbf on line S 6 to Langenfeld the pop livery intended for the S-Bahn Rhein-Ruhr, which became the standard for the AC network nationwide. Photo: Prof. Dr. Willi Hager, Eisenbahnstiftung

Brands such as the Intercity, established early after its establishment, or the product colours of 1986, which included orient red locomotives with white bibs ("contrast surface"), were created by this department.

As early as the end of 1969, the new DB organisational unit presented its first result on the three-car prototype trains of the class 420/421 for S-Bahn traffic in the alternating current network, which were delivered by the beginning of 1970. They were painted in three different identification colours, which at the same time marked the start of the pop colour concept:

- RAL 2004 pure orange on 420 001-0 / 421 001-9 / 420 501-9, provided for the S-Bahn Rhine-Ruhr,
- RAL 5001 green blue on 420 002-8 / 421 002-7 / 420 502-7, provided for the S-Bahn Munich and
- RAL 3002 carmine red on 420 003-6 / 421 003-5 / 420 503-5, provided for the S-Bahn Rhine-Main

In addition to the window strip, narrow decorative strips below the roof edge and at the level of the car floor also bore the identifying colour. The belly band of all three trains was painted uniformly in RAL 7032 pebble grey, the class numbers were also in pebble grey, while the DB emblem remained black.

The roofs wore RAL 7022 Umbra Grey, frames or underframes RAL 9005 Deep Black. The 1st class stripe was highlighted in RAL 1004 golden yellow. This basic paint scheme was to be used for the entire test concept. However, while it did not become established for the express train carriages, it finally became the standard for the suburban trains with the identification colour pure orange until 1986.

continues on page 8





The 420 503-5 / 420 003-6 (photo above), delivered to Munich on 30 April 1970 by MAN / SSW and photographed on 7 September 1972 at the Hackerbrücke, was the third prototype of the series with which the pop colours appeared. It was actually intended for the Frankfurt S-Bahn with the identification colour RAL 3002 carmine red. On the same day and at the same location, the sibling train 420 014-3 / 420 514-2 (photo below) also comes into view. It wears the RAL 5001 green-blue colour, which was intended for München (Munich) from the beginning and was also introduced there. Both photos: Dieter Junker, Eisenbahnstiftung



The first series of class 614/914 diesel multiple units, which were procured from 1971 onwards and were based in the Nuremberg area, were also painted according to this colour concept in RAL 2002 blood orange and remained in use in this way until well after the turn of the millennium.

The prototypes procured in the late seventies, as well as the series of Type x coaches procured on their basis, which were used by the Rhine-Ruhr S-Bahn and from 1988 also Nuremberg, were also given the pure orange-pebble grey livery that had proved successful on the class 420.



614 033-9 belongs to the first series of the diesel multiple unit that was painted according to the pop colour trial concept. On 25 May 1985, it leaves Hersbruck station on the right bank of the Pegnitz river as an Nt in its original colours, heading for its home in Nürnberg (Nuremberg). Photo: Peter Schiffer, Eisenbahnstiftung

If it is usually stated in the literature that locomotives were never procured according to the pop colour scheme, then this is only true to a limited extent for express train service. The Bundesbahn finally decided to use the class 111 in the Rhine-Ruhr area for the S-Bahn service.

Starting with 111 111-1, a total of 78 machines of this class were painted in RAL 2004 pure orange and RAL 7032 pebble grey to match the coaches. In 1984, when the time of the experimental concept had already passed ten years, the DB even transferred the colour combination and layout to the City-Bahn Cologne - Gummersbach.

For the newly introduced train type CB, it modernised 25 Silberlinge, from which seven ABnrz 400, ten Bnrz 430, one bistro coach Bnrzk 490, and seven driving trailers BDnrzf 460 were created. Ten class 218 diesel locomotives were also given a matching coat of paint.

## Sample trains for passenger coaches

In the spring of 1970, the Design Centre was commissioned to develop new design ideas for express train carriages as well. This was triggered by the upcoming delivery of new air-conditioned long-distance coaches, which were to be made of stainless steel.



Like the silver pieces, they did not need to be painted, but they were still to be given a coloured window band for an appealing appearance.

Although only two prototypes of the types Bwüzmz 237 and ABwüzmz 227 were delivered by Linke-Hofmann-Busch (LHB) in 1972, they provided the decisive impulse in advance: The metallic basic colour on the belly band should harmonise as well as possible with other passenger coaches. Thus, besides silver, only a light grey was available for the existing stock.

Following the positive response to the S-Bahn multiple units in customer surveys and votes, the department simply transferred their colour scheme to the 26.4-metre passenger coaches of the UIC-X type to be designed, but omitted their upper decorative strip. However, the classic passenger coach colours for the window band were initially retained.



The Büm 239 in RAL 6020 chrome oxide green shown here in the AW Frankfurt/M.-Nied belonged to the second sample train of 1970 and remained a one-off like the Büm 232 in the same colour, which also served Märklin as an example. Behind it is the centre entry car ABym 411 covered with foil. While the lighter blue and green shown on it was discarded, the RAL 4002 red violet was applied to three couchette cars. Photo: Joachim Claus, Eisenbahnstiftung.

The first test object was an ABym 411 mid-entry car, which was not painted but only covered with foil. It was to demonstrate four colour variants at the same time. It did not enter regular passenger service in its new design.

Two colour sample trains were put together for the official presentation on 23 June 1970 at the headquarters of the Deutsche Bundesbahn (HVB) in Frankfurt on the Main:

- one in RAL 5013 cobalt blue / RAL 7032 Pebble grey with row from Aüm 203, Büm 232, Büm 234 and BDüms 273 for the D 538/539, and
- the others in RAL 6020 chrome oxide green / RAL 7032 Pebble grey with row from Aüm 202, ABüm 223, Büm 232 and Büm 239 for the D 670/671 "Senator".

The darker colour of the closed window band gave the passenger coaches a stretched and modern appearance, but neither of these design variants caused any jubilation, neither within the company, nor among the passengers.

Although the pebble grey did not fail to have the desired effect, brighter, and thus friendlier colours, were desired for the window bands. Thus, in the summer of 1970, the concept was modified, resulting in a third model train.



This photo was also taken in 1970 at the Frankfurt/Nied depot. On the right, a Büm 234 in the final identification colour protrudes into the photo, to the left of it stands a ABüm 223, which also wears the final colour RAL 2002 Blood Orange. The couchette car Bcümk 255 in RAL 5013 cobalt blue with a red lower trim stripe behind it remained a loner, as did the WRümh 132 (far back), which burnt out early on and introduced the identification colour RAL 3004 crimson red for the DSG at this presentation. Photo: Joachim Claus, Eisenbahnstiftung

In contrast to the first two, it no longer relied on a uniform identification colour for the unit of the entire train, but assigned this to the wagon class and the purpose of use of a passenger coach. The train, which was already presented on 14 August 1970 in the Frankfurt (Main)-Nied repair works, consequently showed a more colourful photo.

It was composed of an ABüm 223, a Büm 234, two Bcüm 243 and a Düm 902 in the already final colours. Only for the couchettes was a final decision to be made only after practical experience. In addition to the already familiar RAL 5013 cobalt blue, RAL 4002 red violet, and RAL 4005 blue lilac were also available.

The third new colour of the 3rd sample train was RAL 2002 Blood Orange for 1st class seating cars and those with 1st and 2nd class. The last missing colour was RAL 3004 Crimson, which was the corporate colour of the Deutsche Schlafwagen- und Speisewagengesellschaft (DSG) and was specified for the coaches supplied by them.



Thus, at the end of the tests, there were six characteristic colours, which were assigned to the different wagon functions as follows:

| Wagon type / intended use  | Specified identification colour(s)                                 |
|--|--|
| Passenger wagon 1 <sup>st</sup> class<br>Passenger wagon 1 <sup>st</sup> / 2 <sup>nd</sup> class | RAL 2002 blood orange  |
| Passenger wagon 2 <sup>nd</sup> class<br>Half luggage wagon                                      | RAL 5013 cobalt blue   |
| Full luggage wagon   | RAL 6020 chrome oxide green  |
| Couchette coach 2 <sup>nd</sup> class  | RAL 4002 red violet<br>RAL 4005 blue lilac<br>RAL 5013 cobalt blue |
| Sleeping wagon<br>Dining wagon<br>Buffet wagon   | RAL 3004 purple red  |

In the end, 17 coaches were to wear the purple of the DSG, including six sleeping coaches and five buffet coaches. The remaining six were dining cars, but only one of these was a new-build car WRmh 132. This was only rarely used in round trips as a reserve car and burnt out in Brussels on 25 March 1974.



As many as five apron dining cars WRüge 152 took part in the colour test. Here, in 1974, next to one of only three couchette cars Bcüm 243 in RAL 4002 red-violet, the comparison shows the essential difference of an additional decorative stripe below the window band. Photo: Volker Credé

The remaining five dining cars were apron cars of the pre-war design, type WRüge 152, which were already more than 30 years old when they changed colour. Their design differed slightly from the other pop cars, as they also carried a second decorative strip below the window band. The fact that they were still included was due to the desire for uniform trains.



In 1977, three colour concepts are shown next to each other: The Bm 234 cut in on the left wears ocean blue / ivory, initially still with a black frame and without a separating stripe; on the far right, three Am 203 can be seen, still travelling in the classic colours. The link between them is one of the three red-violet-pebble-grey painted couchette coaches Bcm 243. Photo: Volker Credé

## Expanding the trials

After the colourful and generally friendly appearance was well received by passengers, the Bundesbahn expanded its experiment. The 13 colour sample carriages, not including the different centre entry carriages, were initially to be joined by three complete sets of carriages for long-distance traffic.



One of the two 2nd class coaches with chrome oxide green identification colour, each a unique design, is already shown in 1975 with a faded roof: the original umber grey can hardly be guessed at anymore. Photo: Volker Credé



After the DB head office issued the corresponding order on 31 August 1970, the trains were quickly relocated, which meant that practical testing could be extended as early as late autumn. The train pairs chosen were D 512/513 (Munich - Dortmund), D 576/577 (Basel SBB - Hamburg-Altona) and D 610/611 (Munich - Dortmund).

Between December 1970 and May 1971, the largest series of pop cars was delivered to the Bundesbahn. The final conclusion of the trial was the LHB prototypes Bwüzmz 237 and ABwüzmz 227, delivered in June and October 1972 and already mentioned at the beginning.

If we exclude the middle entry coach and those built later for S-Bahn and City-Bahn traffic, the colour trial comprised 145 long-distance passenger coaches, which did not exist at the same time due to the early retirement of the new-build dining car and a Büm 234 (accident near Rheinweiler on 21 July 1971).



The most common pop-coloured car by far in terms of numbers was the Büm 234 with the identification colour cobalt blue. Here you can see one of the 68 representatives in Kassel Hbf in 1974. Photo: Volker Credé

As a result of the three sample trains and subsequent wagons, not all of the specimens were assigned to the final identification colours according to the purpose of use; there remained deviating individual specimens. The total number of wagons included was distributed among the individual identification colours as follows:

- 86 wagons in RAL 5013 cobalt blue,
- 26 wagons in RAL 2002 blood orange,

- 17 wagons in RAL 3004 purple red,
- 12 wagons in RAL 6020 chrome oxide green,
- 3 wagons in RAL 4002 red violet, and
- 2 wagons in RAL 4005 blue lilac.

However, the experimental concept did not catch on and the wagons, which were poorly represented in the DB's overall stock, remained rare splashes of colour. Only in S-Bahn traffic did it last longer, while otherwise the old colour scheme and this experiment were equally replaced in 1974.



The 1975 Düm 902 with a cobalt blue window band (road number 51 80 92-40 064-8) remained a one-off because chrome oxide green was specified as the identifying colour of the luggage wagons. Photo: Volker Credé

With 218 217-8 and 218 218-6, the DB tested another paint scheme in spring 1974, from which the promptly introduced RAL 5020 Ocean Blue / RAL 1014 Ivory was to emerge, which followed the TEE colour concept in its division, but finally swapped the purple for the blue. Prior to this decision, there had been two smaller colour trials.

In 1973, at least the light-dark distribution on the sides of the class 403 / 404 express railcars was still reminiscent of the pop colours. The Eurofima prototypes of the class ABvmz 227 in 1974, on the other hand, already followed the Eurofima C1 livery with white contrasting stripes, but wore Olympic blue instead of orange.

The decision against the pop colours was probably due to economic reasons: With the small number of wagons, it was only partially possible to put together trains that were true to the concept, although this would soon have changed with a different decision.

However, the comparatively high number of four to six identification colours, depending on the decision regarding the couchettes, also led to higher costs, because they all had to be kept and procured in all AWs that were responsible for maintaining the passenger coaches.



It also proved to be rather unfortunate that the light pebble grey was painted on the lower part of the car, which was the most heavily soiled during operation. This reduced the advertising effect in the long run and also increased the costs due to shorter washing intervals. In this context, it is astonishing that the product colours of 1986 were again based on a light-coloured belly band (light grey).



In spite of the typical and final identification colour, this Düm 902 with the road number 51 80 92-40 049-9 also has a special feature: Its Ege biscuit has been applied in negative representation, contrary to the labelling specifications, as was the case with many silver pieces at the time. Photo: Volker Credé

A shortcoming of the experiment was probably also that no locomotives or railway mail wagons were repainted to match the long-distance wagons, and both in themselves broke up the desired appearance.

## Outreach and museum operations

The more colourful wagons in the DB fleet were mainly used on routes from Bremen, Osnabrück, Norddeich, and Dortmund to Munich and between Basel SBB and Hamburg-Altona (D 576/577).

In 1973, the Bundesbahn established its Intercity supplementary network and introduced the new DC train category. It proved unsuccessful and was discontinued as early as 1978. In **Trainini**® 9/2021 we presented its brief history and explained the reasons for its failure. In the scarcely five years, the Bundesbahn designated the DC as the main area of use for the pop carriages.

The following train pairs and trains were thus made up exclusively or, as a rule, predominantly of pop cars:

- D 516/517
- D 576/577
- D 610/611

- DC 910/915 “Emsland”
- DC 912/917 “Ostfriesland”
- DC 913/914 “Münsterland”
- DC 918/919 “Westfalenland”
  
- DC 930 “Eggeland”
- DC 935 “Lippeland”



This set of wagons in the stabling group with a class 112 electric locomotive at the head probably belongs to a DC, because this train type for the intercity supplementary network was still in existence in the year the photo was taken, 1975. Photo: Volker Credé

The already small number of pop cars soon declined again. Since 1982 they were only used from Lindau, Munich-Pasing and Saarbrücken. They were still to be found in the train pairs D 360/361, D 362/363, D 364/365, D 894/895, D 896/897, D 950/951 and D 1284/1285.

By May 1985, the total number of coaches had been reduced to five, all of which were of the Bm 234 type. Three of these coaches were based in Munich, the remaining two in Lindau. They were found in D 762/763, D 1284/1285 and in the express train pair E 2713/2798, because there was a transfer to and from D 798/799, which took them to Kassel.

The last pop car was converted into an Interregio car and disappeared in 1988. Before that, it was still part of the Munich stock. This meant that this colour experiment was finally history for the Bundesbahn.

However, they were to be given a memento in museum and charter traffic. In 2001, the Stuttgart-based rail transport company (SVG) designed three express train carriages for charter transport based on the historic prototype, but they were only on the road in these colours for just under four years.

The pop livery of a Bm 238 of the Stoom Stichting Nederland (SSN), which was applied in 2017, has lasted longer. However, like the three SVG coaches, it was not included in the paint test during the time of the Bundesbahn, which is why it is not historically correct for it.





D 577 (Hamburg-Altona - Basel SBB) was one of the (predominantly) purely coloured trains, here at the head end in Kassel Hbf in 1974. It can be seen here that the DB's wish was not always realisable in operation. The blood orange Aüm 203 as coach 10 stands next to a purple WRbumz 138, to its right a Büm 234 joins it as coach 9. Photo: Volker Credé

## Pop wagons as models

For model railway enthusiasts, the colourful carriages always seem to have played a special role, as they also provide welcome splashes of colour on the layout. This is true across almost all gauges, but in the nominal size Z already for the moment of their market launch.

The colours, which were highly topical at the time, stood for a modern state railway and consequently could not be missing from the Mini-Club's starting assortment. When Märklin filled the 1:220 scale with life and planned its opening programme, the identification colours for seat, baggage and dining cars were finally fixed. The new construction of carriages and also the repainting were still in progress.



The WRümh 132 dining car (item no. 8723), whose prototype was only in service for a short time, was part of the Mini-Club launch programme in 1972 in the highly topical livery. It was also a part of the first starter pack.

No one could have guessed that the WRümh 132 dining car would remain a loner and be the rarest of all, and that it would also prove to be extremely short-lived. Märklin sent an Aüm 203 (item no. 8720), a Büm 234 (8721), a Düm 902 (8722) and this dining car (8723) into the race for buyers' favour until 1977. Only the dining car, which had long since been lost as a prototype, remained in the range for about ten years longer.



Probably, also as a model, the Büm 234 in single edition (8721) reached the highest number of pieces of all pop colour cars. It, too, was already in the programme from 1972.

This meant that the inventor of Z gauge had no less than four identification colours to choose from and was able to reproduce the test concept almost completely. The pop cars were also offered in a rather incomplete starter set (8901), in which they were accompanied by a purple class 216 diesel locomotive. Not included were the baggage car and a transformer.

In the anniversary year 2022 Märklin took up its beginnings again in 1:220 scale and reissued this train according to the current status (81873), now including a baggage car instead of the dining car. The class 216 has long since come from new moulds, is powered by a bell-shaped armature motor and illuminated with LEDs. But the pop cars have also gained significantly in appearance due to the quality of painting and printing achieved today.



To compare the quality of painting and printing achieved over the past decades at Märklin, we have compared the Büm 234 from the first years of production (8721; left) and an Aüm 203 from 2012 (from 87401; right).



If we compare the original models and the anniversary edition, the age of the 1972 pop cars cannot be overlooked. All of them were printed with the respective identification colour on the pebble-grey plastic, but the end walls were not integrated. Above all, their blurred inscriptions always remained an exception in the Märklin programme.



The 2004 Olympic year express train pack (81435) was a good idea and also attractively put together, especially since it also included one of the colour loners (centre back; cf. photos on pages 9 and 12). However, the wrong roof colour of the baggage car (rear right) led to some annoyance.

But that didn't bother anyone, which underlines how popular the colourful passenger coaches may have been. If we disregard the WRümh 132 dining car, they were nevertheless rare for a long time - for more than 25 years there was no offer from Göppingen!

It was not until 2004 that the product management remembered this short era of the dawning Bundesbahn and announced in the spring new products one of the few trains as a one-off series that were formed "purebred" at that time. This was the D 611 of 1972 mentioned in the prototype section, which took spectators from Dortmund to the Summer Olympics in Munich.

Five cars and a blue class 110 box electric locomotive were included in the express train pack (81435), which was well received. In addition to the expected models of Aüm 203, Büm 234 and Düm 902 in the identification colours of the final concept, Märklin also gave its customers two surprises: For example, another 2<sup>nd</sup> class car with a chrome oxide green window band was included, which was supposed to pick up the single piece of the Büm 232 from the second colour sample train.

With regard to the transition and access doors, steps and toilet windows, customers had to accept some compromises here, but certainly this colour variant enhanced the existing range.

The second surprise of the set was the included dining car. In the meantime, the apron cars were also available in the mould kit and so Märklin was able to give the buyers what was typical in the train set at this point. The additional decorative line, which the five apron dining cars had in pop livery, also adorned the model that was included.

We have not mentioned a third surprise that was not part of the announcement because it also does not correspond to the prototype: While the product illustration also showed a prototypical umber grey roof on the luggage van, the delivery here was made with a pebble grey example.

This broke up the appearance of the train in an unpleasant way and would not have been conceivable with the original model because of the expected dirt. But Märklin did not react to customer objections in this case, because at that time the production of this pack had long since been outsourced to the Far East and could not be remedied in a timely manner anyway. Self-help remained in demand.

Eight more years went by before the colourful cars returned to the programme. It was the 40th anniversary of the Mini-Club and Märklin wanted to launch them, incidentally equipped with train route signs for a route from Braunschweig to Aachen, as matching cars for the new class 001 express steam locomotive.



The most elaborately printed pop cars to date were contained in this set (87401), which was issued as a one-off series for the 2012 anniversary year: The identification colour extended around the corners of the cars for the first time and prints were even applied to the end faces on door handles and grip bars.

This required a lot of imagination and did not fit at all to this steam locomotive, with which the detail control was introduced, but did not stand in the way of the success of the wagon pack (87401) at all. After all, these were lavishly printed and cleanly painted models that were unique in this form to date.

For the first time, the paintwork and even the printing also included the front sides, where even the tail lights were laid out in red. But the side panels were also beyond reproach and even received colour-contrasting door handles and grip bars. There were no repeated faults and no defects to be found: Top mark for Märklin.

At the latest now it was time to send the old vehicles from the launch year of the Mini-Club to their old age and to replace them with this edition. With three cobalt blue Büm 234, a blood orange Aüm 203 and the chrome oxide green Düm 902, the one-time series was also well equipped.





It was not always possible to put together trains of pure colours. We have deliberately taken up the Quick Pick self-service dining car WRbumz 138 (photo above), captured in 1974 and placed in D 577, between the 2nd class car 255 (left) and the 1st class car 10 (right). A model to recreate this scene was once supplied by Heckl Kleinserien (photo below), where the finely etched window frames just don't stand out as much from this perspective. Photo (above): Volker Credé

At this point, a wagon should be inserted that never wore pop colours in the prototype, but nevertheless fits well with the wagons, as was already seen in the prototype part of this article: the prototype of the WRbumz 138 Quick-Pick self-service dining wagons (part of the "Heckl 2/97" package).

Since the DB only (still) had five dining cars in pop colours, but high-quality D trains were usually equipped with them, the prototype, for which the Bundesbahn had high expectations, also appeared in the few “purebred” trains and attracted attention in them.

Heckl Kleinserien took on this model in 1997 and realised the car in its original purple colour with the striking white inscriptions on a Märklin basis. In addition to the bogies, the roof and the end walls also came from Märklin. The correct window arrangement of the prototype could be realised thanks to etched sheets.

This means that we are now making another ten-year leap in time when we focus again on the Märklin programme. We will then be in the next round anniversary year of 2022, for which we have already mentioned the retro starter pack.

But Märklin did not want to leave it at that, because two colours of the pop colour experiment had never been found in the Mini Club assortment before: Red violet and blue lilac as the two rarest, but probably also most distinctive colours.



Intended as a supplementary pack (87402) to the anniversary starter pack also for earlier compositions, the latest pop colour car edition is a good idea. With a small compromise (1 compartment window per car side too many) Märklin has now also included the two rarest colour variants of the couchette cars in red violet (rear left) and blue lilac (front).

Customers should receive these as part of an MHI supplementary wagon pack (87402), with which the trains from item numbers 81435 and 87401 are also to be optionally extended, although different train runs are printed on them. Many customers will certainly use them, which is why the decision is to be welcomed.

But then again, this is a compromise that cannot be avoided without new moulds: The replica couchette coaches Bcüm 243 in the previously missing identification colours had eleven compartments in the prototype, which is why neither the Aüm 203 (10 compartments) nor the Büm 234 (12 compartments) fit exactly.

Märklin decided on the 2nd class car as the form, because it reproduces the prototype impression quite coherently and will not immediately reveal its compromise without the possibility of counting in passing. Supplemented by a “real” Büm 234 in cobalt blue, the car pack is now complete and is now also on the road on home layouts.



Regarding the quality of the last package delivered, it should be noted that it largely follows the high standard of today. Compared to the direct predecessor pack 87401, however, the new carriages do without the printed door handles and climbing handle bars and also have no inscriptions on the front sides.

However, the tail lights have been coloured more precisely, and all models of the production year 2022/23 also show reproductions of the door seals and the black separating strip at the edge of the roof. In addition, they are now equipped with close couplers, just like the cars of the retro starter set. Thus, all in all, they are unchanged at the highest level.



With the exit from the station, where the eyes of father and son as well as ours fall on the two couchettes at the end of the train, the story of the pop cars in the Märklin programme is told for the time being.

Our model history is now complete for the time being, although this experimental paint scheme will certainly play a role at Märklin in the future. Small-series suppliers have not yet adopted this colour concept, presumably because of the higher painting effort compared to other colour concepts. However, the loners in the prototype stock could be appealing models for them.

## Lone wolf by self build

Based on these thoughts, we also had the idea of reproducing a wagon ourselves that had a loner status in the prototype. For decades, the Märklin assortment has been lacking not only half luggage cars but also mixed-class passenger coaches from the DB's new construction programme.

This gap has so far only been filled by Heckl Kleinserien, where the type ABm 225 in two colour versions was once part of the delivery programme and the BDms 273 half-luggage wagon was also delivered to customers with the aforementioned Quick-Pick wagon.

As BDüms 273, four coaches were once on the road in the cobalt blue pop design, which would certainly be worthy of a replica. However, such a Märklin-based car remains only a project idea for the time being and thus “dreams of the future”.

Thus, the mixed-class ABüm 223 and ABüm 225 came into focus. As many as 20 examples from these two types wore blood orange as their identification colour. The more modern type 225 already had revolving doors and was a UIC-X standard car, which can be easily derived from Märklin conversions.



The only ABm 223 in chrome oxide green had the road number 51 80 31-40 199-6 and became our conversion template, with compromises. The photographer was able to capture this car, which unlike other pop cars did not have a decorative stripe in the identification colour directly above the edge of the frame, in a photo in 1978. Photo: Volker Credé

Far more interesting, however, appeared to be the only ABüm that had received chrome oxide green as a decal colour because it was part of the second sample train. As a further special feature, it was also missing the lower decorative stripe in the identification colour near the frame.

So, this example with the road number 51 80 31-40 199-6 was supposed to be the model. Unfortunately, it belonged to the type ABüm 223 and still had the large revolving doors. Further differences concern the size of the toilet windows and the type of transition doors (folding instead of sliding doors).

It would be very difficult to take these differences into account in the model: at least the door area would have to be sanded, the door window would have to be enlarged and its transparent insert would have to be suitably replaced. Further alterations and replacement requirements would concern the adjoining toilet windows.

For us, the expected effect was not in a justifiable proportion to the effort described. Therefore, it was quickly clear that the exterior of our own construction would follow the successor type ABüm 225, especially since only a few model railway enthusiasts would know the loner and would be able to classify it cleanly.





We also painted and lettered our mixed-class example with a similar effort as the more recent Märklin editions.

A conversation with Dieter Heckl, with whom this idea was discussed at an exhibition, proved to be a stroke of luck. He said that he still had a remaining stock of machined blanks from which he would no longer produce his small-series models. We were now able to fall back on one of them.

He had already cut the donor wagons (Märklin 8710 and 8711) in the middle and joined them together, and he had also filled and cleaned the cut edges. So, his blanks were already prepared for a coat of paint.

All that remained to be done was to remove the roof, take out the window inserts and then mask the black lower frame with masking tape from Tamiya. We have had the best experience so far with this product in terms of the interplay between perfect hold and protection of the parts and later removal without damage to the paint.

For the paints we used water-based acrylic paints from Oesling Modellbau. The pebble-grey base coat was the first pass, and the roof was sprayed in umbra-grey on the same day. The chosen gloss level silk matt was the finishing touch for this one at the same time.

After drying, we had to mask the windows carefully, because the window band had to be painted chrome-oxide green, but towards the roof it had to keep a narrow decorative stripe in the light base colour. It was also important to us to bring the paint cleanly around the corners of the wagons, as the more recent Märklin editions show.

After this challenge was also mastered, a new masking pass followed. The golden yellow 1st class stripe (RAL 1004) was also to be applied by spray painting. In principle, this would also be possible with a decal, but it remained unclear whether the desired colour tone would have to be screened for printing; we should deal with this unavoidable issue anyway.

Except for the black colouring of the rubber bulges and the transition tread plates and the red colouring of the tail lights with a fine brush, the colouring work was completed at this point. We ordered the decals for the model from Andreas Nothafft.



Only good connoisseurs of prototypes will notice that the shape of our model reproduces an ABm 225, but is based on an ABm 223 (photo above). In any case, it closes a gap in the Z-gauge range in an appealing way. Despite the screening of the pebble grey and chrome oxide green inscriptions, this unpleasant effect is no longer noticeable on the model, even from a short distance (photo below).

He told us that the pebble grey class numbers and the chrome oxide green DB emblems and wagon numbers would be screened for printing. Smoking and non-smoking panels, on the other hand, should not pose any problems.

This seemed worth a try; if it failed, we would have had to choose a different, but matching ink that would be as close as possible to the desired ones and printable without a screen. But the result turned out to be usable: Even though the sharpness of the print is very low up close, the exact colour from a normal viewing distance proved to be the better alternative.



After being soaked in water and sprinkled with softener, all the decals found a place and a firm hold on the surface of the car, which had been primed with adhesive. Precisely aligned, excess liquid removed with a cotton swab and then dried thoroughly, a sprayed, silky matt clear varnish ensured a firm grip.



A prototype photo inspired us to create this train: Behind 181 201-5, the first car to run is the lone ABm 223 and hardly anyone will have noticed it particularly, as its livery is reminiscent of that of the contemporary SNCF passenger coaches with which the dual-system locomotive was so often on the road.

At the same time, he has also levelled out the edges of the decals, which are hardly visible anyway, so that the origin and technology of the lettering are no longer recognisable. With this, we can release our self-built locomotive into service.

It continues the history of pop colours in its own way and complements many trains in an appealing as well as eye-catching way even without its concept siblings. And so, we remain excited and hopeful about how this unusual and short-lived experiment in 1:220 scale may continue in the future. Attentive **Trainini®** readers will surely remember that we are not the only ones who like these cars.

**Model manufacturer:**

<http://hecklkleinserien.de>  
<https://www.maerklin.de>

**Materials used:**

<https://www.modellbahndecals.de>  
<http://www.oesling-modellbau.com>  
<https://www.tamiya.de>

**Statistics as a model:**

<https://www.bahnstatistik.de/Popfarben.htm>



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Layout details (part 4)

## Abandoned and forgotten

*In what is now the fourth part of our annual focus theme series on “Layout Details”, Dirk Kuhlmann looks at the many relics that people carelessly leave behind. The world is full of objects that were once left in a quiet corner and have been waiting for their revival, usually without success. Many things are forgotten, and the ravages of time soon gnaw away at their substance.*

Recently, I have been going through and organising many photos and articles on the subject of model railways. I was pleased to notice that the landscaping side of the hobby has experienced a noticeable upswing in the last ten years. Beautiful meadow landscapes alternate with well executed wire trees and the overall visual impact of many layouts are generally impressive nowadays.



The MKO Museum-Bw in Norden looks very tidy on the surface. On closer inspection, however, the objects that have been here for a long time stand out. Go ahead and search!

There is only one type of detail that often is still missing: abandoned objects. I am not talking about ordinary trash, but about more or less carelessly discarded objects that are even overgrown over time. In our understanding, a normal garden behind one's home, for example, appears tidy and naturally inviting.

A well-tended lawn and flowering plants make for a pleasant sight. The self-built small wooden shed is the pride and joy of the hobby gardener and is always presented to guests. The view behind the shed is wisely blocked with a privacy fence.

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For almost two years, many parts of the signalling technology lay around rusting in the Opladen station area, which had not yet been converted (photo above). Leverkusen-Schlebusch station used to have a number of sidings. After the dismantling, the remnant of a track closure lay here in the gravel bed for years (photo below).



But now we get to the interesting part: An old bench accommodating discarded flower pots, a rusty metal barrel no longer being able to collect rainwater, but at least now a convenient place for storing wood boards – after all, there is always a use for something.



Nowadays, this scene with the discarded car parts is no longer visible. However, even after reforestation, many rather hidden marginal phenomena can be discovered on the large “Werdersheim” layout.

Most of the time, these “collector's corners” fall into oblivion and replicating them on a layout can add some very special charm to a model railway. And it gets worse! In earlier times, cars covered with tarpaulins were a frequent sight in many backyards.

Sometimes, an old VW bus ekes out a shadowy existence there, but at least the owner has been for years thinking about recommissioning it, and the wheelbarrow in front of it, which has lost its wheel, is actually also more of a thing for a scrap metal collector.

If we look a little further in the direction of the railway tracks, we see similar occurrences. Forgotten sleepers are almost overgrown by weeds and an old signal board lies next to it. Most of the time, these scenes occur after construction works on the tracks. Time and again, something is left lying around and is thus forgotten here as well.

The art of implementing such scenes on a layout lies in exercising a deliberate restraint with said objects. Railway layouts up to era IV may actually still look quite “clean”. Only layouts in a more modern era would call for a somewhat wilder look in order to be prototypical.

An exception would be the construction of a US layout. Here, downright chaos was and is often to be found to the left and right of the tracks at all times. Once again as a reminder: This does not mean trash.



We do not know whether the draisine is still being moved. Otherwise, the scene looks like a still life. Every item has had its place here for years!

## A few tips

Of course, there are some manufacturers who offer ready-made “leftovers”, but do-it-yourself solutions should always be considered. Depending on the scale, remnants of veneer wood can serve as the basis for wooden boards. After a bath in a wash of black and brown emulsion paint for several days and the



Some construction machines eke out their existence in the countryside, especially if a repair causes high costs and the repair is put on the back burner.





A typical backyard atmosphere shows up in rural Westheim. The owner is fully convinced by the accumulation of his utensils. Who knows what else it can be used for.

subsequent drying, the boards look rotten as desired. This could be used to depict old willow fences, stacks of boards and many other scenes.

Furthermore, old watches are a real treasure trove for modeling various piles of metal. The gear wheels in particular fit in well with our scale. Most recently, I even used leftover parts from H0 scale truck kits in a layout for our 1:220 scale.

Engine parts and exhaust systems, wonderfully painted with rusty texture colours from Vallejo, look like abandoned parts from a larger ship. Even the spare parts box which most of us keep should not be safe from scavenging for interesting objects – just a bit of colour some colour to a specific piece might transform it into a unique detail.

## Further experiences

Most recently, I paid a visit to the smaller Leverkusen-Manfort station to photograph the now deforested areas of the former sidings. Ten years ago, unused tracks were removed and left to mother nature.

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The absolute classic: a more or less well-veiled Volkswagen has been waiting for its revival for years. It doesn't matter whether it's on the east coast of the USA (photo above) or at the "Westerende" stop in East Friesland (photo below), the scenes are similar.





**Photo above:**  
It's not junk or rubbish. The American fish company Miller only stores its older working materials here, and they are often forgotten.

**Photo below:**  
This American diesel locomotive has been here for a while. Will the machine be set in motion again? In the meantime, the pitting from the rust is working its way forward.



Now it turns out that for about fifty metres several rail tracks are still there, with only the running surface of the rails visible. Such a scenery looks quite good on a small corner of a layout, perhaps in combination with an old goods wagon, or even a collapsed shelter for the shunters of the time. The now emerging trend of discovering "lost places" would be aptly represented here.



Over time, some materials that were no longer required have accumulated on the side of the small locomotive shed. Otherwise, the small depot of the "SVG." looks very tidy.

Next to the station is a bicycle shop. Here, in a corner facing the former railway tracks that is not visible to customers, countless dilapidated bikes are lying around. All of them motorless and apparently no longer up to date. This scene could be easily modeled by taking, for example, Artitec bicycle models, covering them with some rust paint and placing them appropriately on the layout.

Let's move on to more urban environments. Nowadays, model railroaders also like to recreate city centres and their streets. As with the prototype, the buildings facing the street usually look vivid and tidy. The many businesses and shop windows are intended to attract potential customers.

A short walk through the courtyard entrance, however, reveals a completely different scenery. In addition to the items already described above, discarded electrical appliances and pieces of furniture such as refrigerators, sofas, tables, etc. would be typical sight here.

Actually destined for collection, the order for removal was maybe forgotten or the date is simply still pending. Some old shelves from a shop eke out a rusty existence here, paired with many bicycles of the same colour, as repeated in the photo.

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**Photo above:**  
Disfigured with various parts from old clocks and lighters, the American "Helper Station" looks very lively. The boards leaning against the wall of the house have also been here since ancient times.

**Photo below:**  
Picking up items that are no longer required does not stop at even the most remote place. Maybe the old keeper's house at Catharinentief will just be renovated, soon?



Only this time the bikes are parked perfectly correctly in stands. A little further on, the first smaller buildings begin. In the courtyard of the two-family house, construction machinery and materials have been standing around for years; the renovation or redevelopment is just dragging on a bit. The stories behind such scenes can be manifold.

Of course, I am aware that time for such exploration is rare in my professional life, but at least one's own holiday is an opportunity. I found many a legacy in an old Deutsche Bundesbahn depot in the town of Norden (East Frisia).



In 1:87 scale, many of the legacies seem even more real, an old radiator is lying around here, some of the rusty barrels are leaking. I wonder if the rest is still usable?

My old railway spare parts bin contains many wagons and items that are no longer usable. Members of a museum railway often accumulate a wide variety of materials and old passenger and freight wagons often sit around here for years, while they wait to be restored or cannibalised.

An old track bumper, placed in the middle of a meadow, was supposed to be installed in the railway station, but somehow, it was forgotten. A few kilometres further on, I was able to observe a similar scene, but this time it involved maritime objects.

Probably even the fans of epochs V and VI will not use an extreme version of the representation. Here, however, the rubbish that has been left out so far would be an issue. Especially in urban areas, our modern throwaway society leaves quite a bit behind.

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Going up a notch, track 0 is all about the right subtleties (photo above). The old window frame from the block site was actually supposed to be repaired (photo below). On a scale of 1:45, the dirty and broken glass panes are a must.



Here, overfilled rubbish containers in a housing estate could be depicted in a quite expressive and extremely realistic way. This is garnished and supplemented with two old mattresses, a three-legged armchair, and, not to be forgotten, a clothes horse that is often discarded. Actually, this is not nice to look at, but the model is far from an ideal world.



All sorts of flotsam draped artfully and untidy looks good even on a scale of 1:220; is it partly rubbish? A clean-up of the beach would be advisable, or everything may be forgotten, or the winter storm with the corresponding flooding will sweep everything away again.

After so many impressions, the model railway implementation should not be too difficult for us. Only one aspect would have to be carefully considered: the well-dosed accumulation of the old things. A layout quickly tips over into a toy-like character if, as with many other projects, the presentation is exaggerated.

The desired effect is then quickly lost. The details at the edge must be there and should be perceptible, but they must not impose themselves on the viewer or even literally beat him to death.

The author's web pages:  
<https://www.helenensiel.com>



Uhlenbrock Intellibox 2neo

## The Success Story continues

***25 years ago, a success story began with the Intellibox. As the IB 2neo, it is now on the market as a new variant of the Intellibox II. And even if it does not claim to embody a third generation, it is equipped with modern technologies and ready for the future.***

25 years ago, the Bottrop-based digital specialist launched its first digital command station under the product name Intellibox. Twelve years ago, a successor followed under the name Intellibox 2. Both were also suitable for Z gauge and enjoyed corresponding popularity.

But it always required an adjustment ex-works to generate a track voltage suitable for our scale. Now that the second generation of command stations is getting on in years, Uhlenbrock released the Intellibox 2neo a few months ago.



Fresh out of the box, the Intellibox 2neo from Uhlenbrock presents itself in its usual design and comes straight from the factory with a matching plug-in power supply unit, which is included in the delivery.

Curious as to why it should not be seen as a third generation, we had a conversation with Rüdiger Uhlenbrock at the Intermodellbau in Dortmund (see [Trainini TV](#) episode 13).

As a product developer, the managing director gave a clear answer to this: “It is a further development of the Intellibox II, it is not really a completely new device. It has been modernised internally and not much has changed externally.”

The exciting question for us was what changes had been brought about by the internal measures. It seemed particularly important to us to clarify whether it is still a good alternative for our size and whether it brings additional gains.

Before we look at the “inner values” of the new products, let's first note what has remained constant and what Rüdiger Uhlenbrock therefore also emphasised as an argument for classifying the unit with 2neo.



The equipment with pushbuttons and controls as well as their night lighting and the well-known display with its good resolution remained unchanged.

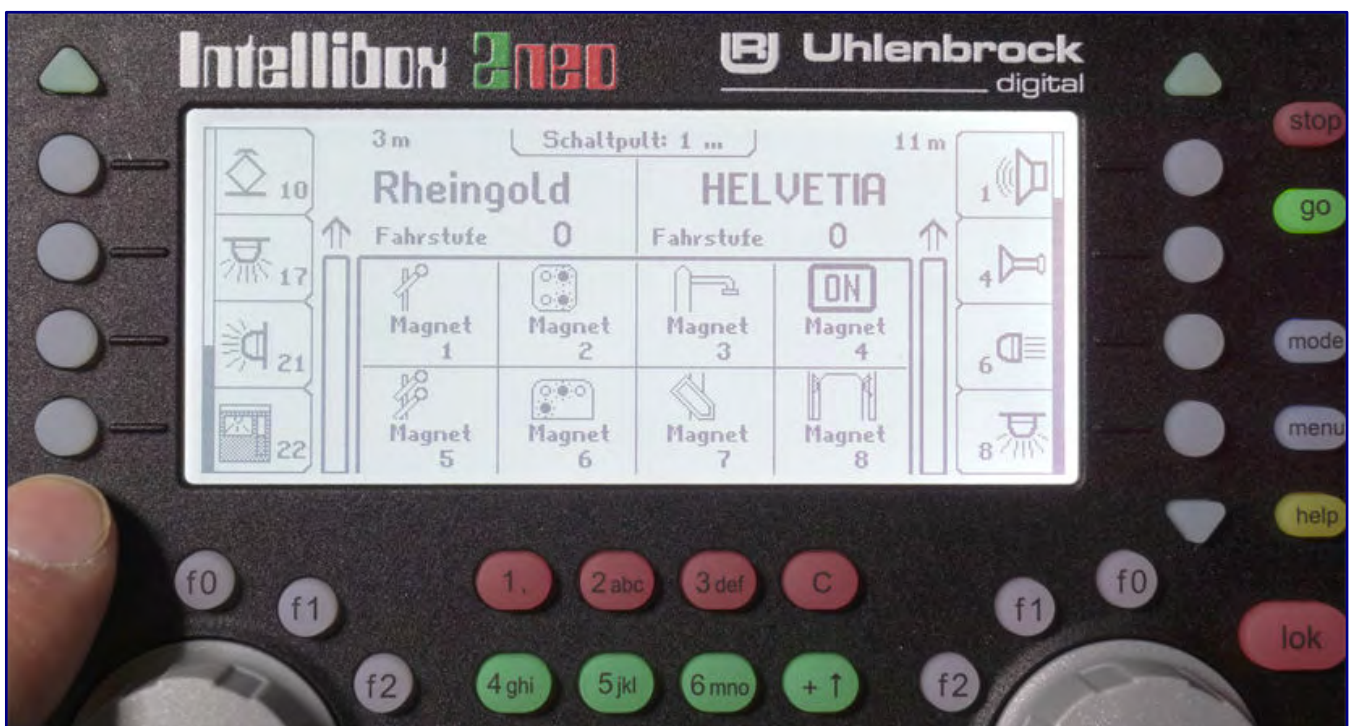
The shape and size of the housing, including the external design, seem familiar. The backlit display (size 98 x 42 mm, visible diagonal 105 mm) and the illuminated control buttons have also remained. The size of the display instrument and its resolution again make it stand out as a feature.

Since the previous, comprehensible operating concept has also been retained, there are no changes in the area of proven ergonomics. Thus, previous users of the predecessors will be able to cope with this control unit without having to get used to it. But even newcomers should quickly get used to it. We will discuss new functions later.

By the way, the parameters are displayed in plain text as well as with function icons. The display is detailed and easy to read even from the side. The keys directly to the left and right of the display are assigned contextually and are thus suitable for quick access to menu items and functions.

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Photos on page 43

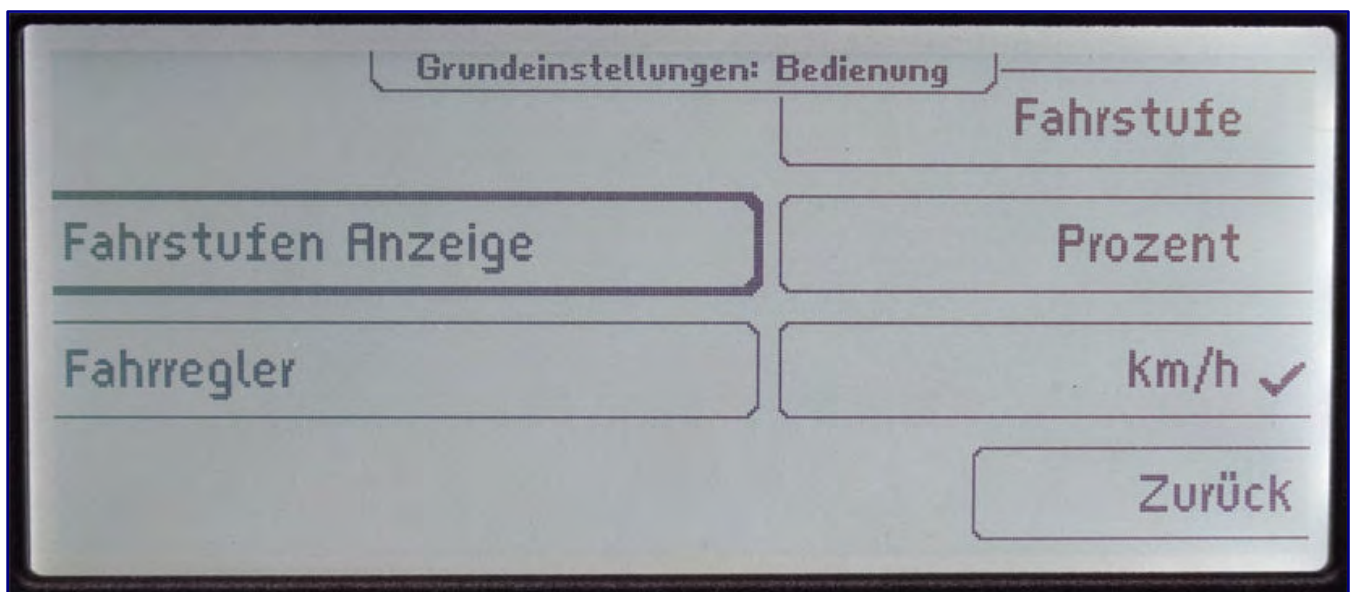
As we are used to from the previous model, we find two controllers on the user interface together with the function keys f0 to f4 surrounding them. All other functions can be accessed via the variably assigned keys above, together with the associated arrow keys for scrolling up or down (see image below). On the far right are the emergency stop and release buttons as well as access to the mode, menu and help function. In the middle between the controllers is the so-called keyboard.

## Functions and connections

Since we have not presented the Intellibox in this magazine so far, we do not want to go straight into the observations that have arisen with this product update. We think it makes sense to give our readers an overview of what both the predecessor and this new product can do, and how the device is equipped.

After all, if you are interested, you will have to invest several hundred euros, which is why it should be clearly emphasised that this is a fully-fledged control centre that controls the locomotives, triggers their functions and also handles the switching tasks that arise. And even this is not the end of the description of the test candidate.

It can probably be expressed more accurately as follows: With the Intellibox 2neo, the model railway enthusiast is fully equipped for digital operation. It supports up to 128 speed levels and 9,999 decoder addresses, which means that it will not fail, even on large layouts.



The speed level display can be displayed in speed levels, percent (of maximum speed) and as a value in km/h. The desired display is selected using the multifunction button (see images on the previous page) next to the selection to be made. The display can also be adjusted in terms of brightness and contrast to one's own viewing habits.

Locomotive and function decoders can be addressed simultaneously in all three data formats (Motorola, DCC and Selectrix) used in Z gauge. In the area of turnout and switch decoders, these are the Motorola and DCC formats of the different manufacturers.

The unit provides the following components of a digital system: Central unit, booster, two controller, keyboard, programmer, interface, feedback monitor, route control, model time clock and the LISSY/MARCo mode with display of the current locomotive position on the layout and the DirectDrive function. An integrated infrared receiver enables the direct use of the IRIS infrared remote control.



The locomotive database holds up to 500 vehicle entries with function symbols up to function F28. Address, name, data format, train category, special locomotive options and the setting for the maximum speed in km/h can be assigned. The locomotive database can also be transferred to other Uhlenbrock devices via the LocoNet.

The speed display can be shown in speed steps, as a percentage of the maximum speed and also in km/h. Up to 32,767 special functions per locomotive are (theoretically) switchable.



On the back of the device, we find the following connection sockets (from left to right): s88-N feedback modules (Märklin), Märklin Motorola booster, LocoNet (B and T), DC booster (CDE), driving and programming track (1 to 4), power supply and the USB interface.

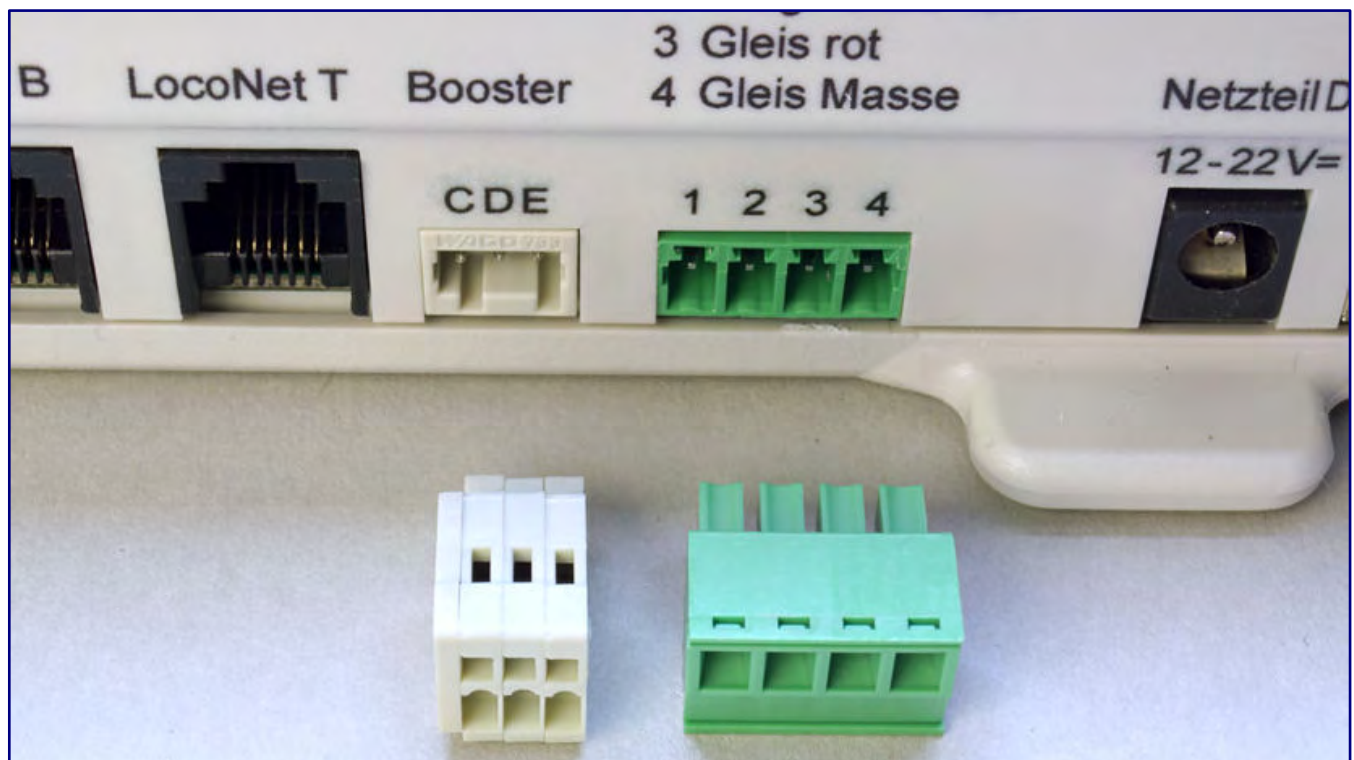
A helpful option is decoder programming in plain text, because for the standard CV in DCC format this eliminates the need to search for locomotive decoder instruction manuals for reference.

A detailed help function (at the simple touch of a button), a model time clock (for operating according to a timetable), booster mode and computer connection via the USB port complete the essence of the Intellibox 2neo.

Friends of automated operation will not want to do without a connection to the computer, in particular. However, it is also used to back up data on a computer and to import future programme updates.

ABC braking according to the system developed by Lenz is possible with the Intellibox 2neo if the other data formats are switched off and only DCC is output. The new command station can also be operated as an additional device on other LocoNet command stations (examples: other Intelliboxes or DAISY II).

A high level of compatibility not only with Uhlenbrock products is also evident on the rear of the unit through the many connection options: in addition to the sockets for the power supply, driving and programming track, we find others here for DC as well as Märklin Motorola boosters, LocoNet, s88-N feedback modules (Märklin) and the already mentioned USB interface.



The necessary plugs for connecting DC boosters and tracks for operating and programming are included with the Intellibox 2neo.

## Adjustable operating voltage

The most important change to the Neo version for Z gauge users is the power supply: All previous Intelliboxes were supplied with alternating current. For a use in the nominal size Z desired by the customer, Uhlenbrock therefore made modifications that suitably reduced the track voltage output.

The reason for the earlier power supply was the design of the internal booster output stage. As with the Märklin digital system, there was a common ground rail that also had a connection to the bus systems. Despite advantages, such as compatibility with the Märklin digital system, this also had disadvantages.

**One device - all gauges**  
For use in Z gauge, the Intellibox 2neo no longer requires a conversion by Uhlenbrock.  
It can be used in parallel for all sizes of a model railway, also independent of the power system and protocol. This means that even the use of a two-wire layout (Mini-Club) and a three-wire system (H0 gauge), which is to be expected from Märklin customers, does not present a hurdle.

With the Intellibox 2neo, a changeover to direct current supply via a switching power supply unit, which is part of the scope of delivery, has now taken place. By means of a slide switch, 22 (large-scale railways), 18 (H0 gauge), 16 (TT and N gauges), and 12 volts (Z gauge) can be selected.

This eliminates the need for manufacturer modifications and an investment in the Intellibox 2neo also has the advantage that the same device can be used for all nominal scales in the owner's inventory. For this reason, in particular, we will also discuss new features here that have no relevance for the Z gauge itself.

The power supply unit supplied by Uhlenbrock delivers up to 3.5 amps, which also corresponds to the maximum output current. The programming track, which is also intended for test runs, is supplied with a maximum of 1 A, the LocoNet components are limited to 0.5 A on both connections. This is sufficient for many hand controllers and other components.





A powerful power pack is already included with the Intellibox 2neo, on which the required output voltage is passed on to the track. For Z gauge, 12 volts are preselected, as can be seen here. Thanks to the implementation of all common data formats, this control centre can also be used at home for different scales and adapted to their needs at any time.

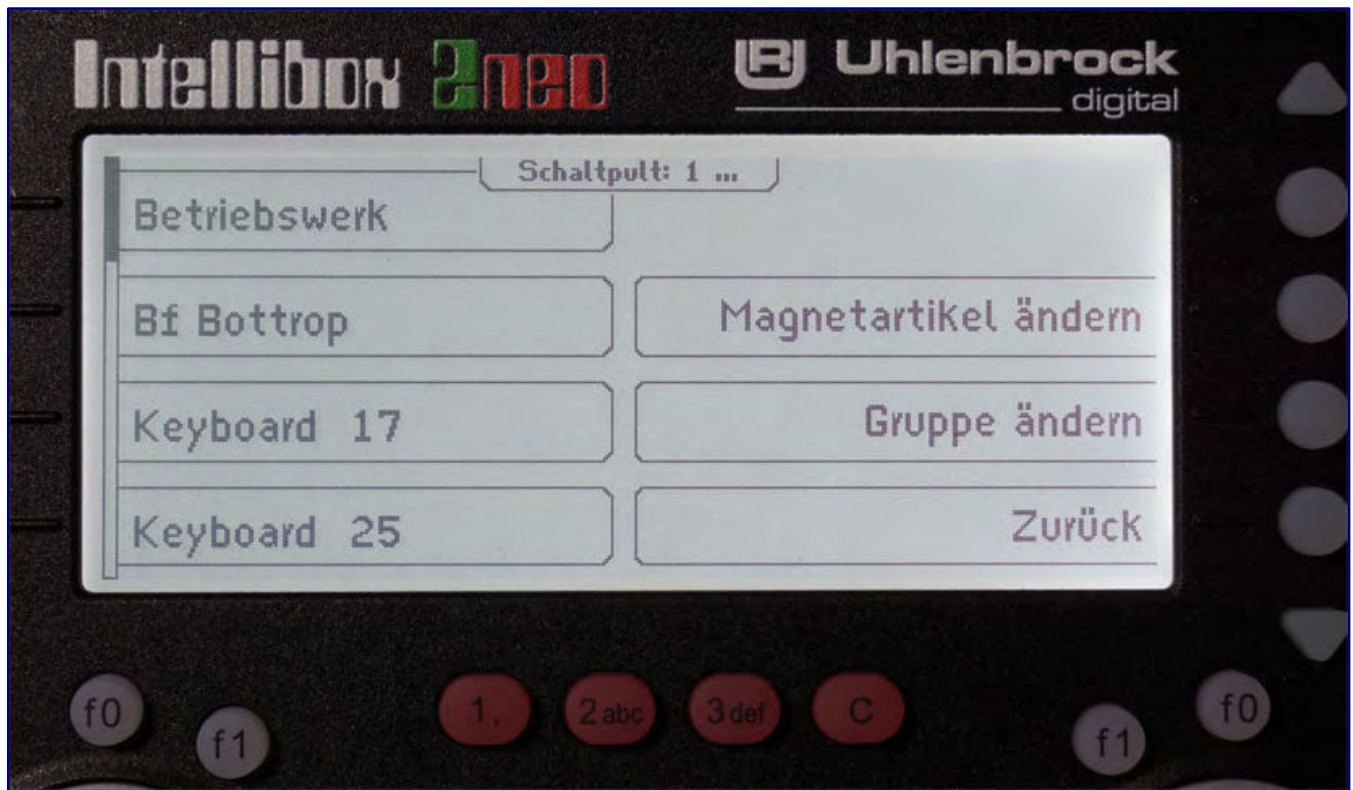
A big step forward is also the integrated WLAN equipment. When operating as an access point, the IB 2neo does not need an external router. Its network can be found under IB2neo\_XXXXX (individual, seven-digit number sequence) and the IP address 192.168.4.1. It is an unencrypted WLAN that is also not secured by a password.

However, if the central unit is switched in mode via an LNCV, it is sufficient to press the WPS button on the router to integrate it into its network. However, this requires that the router at home is WPS-capable, which should be a matter of course with newer devices.

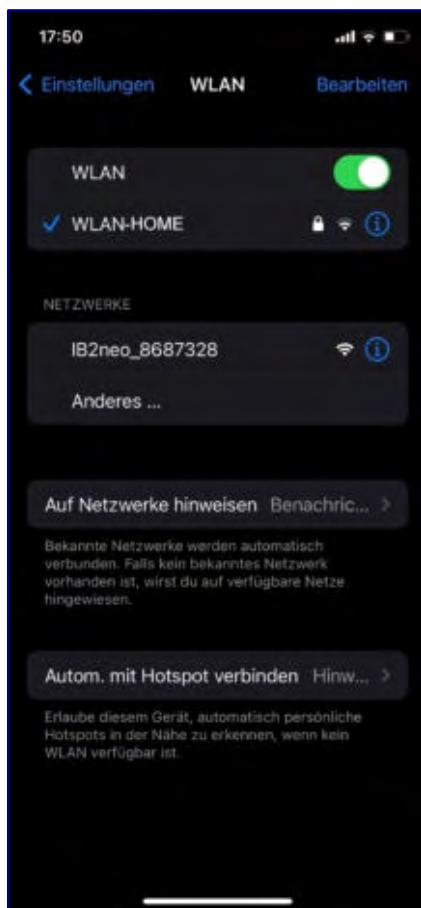
### Other features

There has also been a change to the s88 feedback bus used in the Märklin digital system. The previous five-pole flat plug as a classic has now been replaced by an eight-pole Western plug. Although this makes no functional difference, this step is to be welcomed. Network cables that are easily and almost everywhere available, are now used, which are more interference-proof and also less expensive.

In the booster menu of the Intellibox 2neo there is now the option to monitor and also remotely control external Uhlenbrock boosters of the latest generation. In this way, selected areas can also be switched on or off from the central unit. The current load and temperature can also be queried in the monitoring functions.



All functions offered by the Intellibox 2neo to its owner can be quickly accessed via the menu selection and are arranged and displayed in an understandable and intuitive way.



It is convenient to assign names to the boosters that help with identification, such as “shadow station.” They are switched on and off via a solenoid address with automatic assignment. Manual restarting after a short circuit can also be specified, if the booster configuration prevents automatic restarting.

There has also been an expansion in the data formats with which the locomotives can be controlled. The previous Motorola, DCC and Selectrix from various manufacturers have now been joined by MFX. As with DCC via the RailCom functions, the independent registration of the locomotive at the control centre is therefore also possible in the format used today by Märklin.

According to Uhlenbrock's own understanding, it is a matter of course that previous devices can continue to be used in conjunction with the Intellibox 2neo. The Daisy with WLAN was also adapted to the IB 2neo as part of product maintenance.

With the mentioned own WLAN, control is also possible via a mobile phone or tablet. When combined with LISSY or MARCo from the Uhlenbrock range, the position of a train can also be displayed on the layout.

**Photo left:**  
The IB 2neo sets up its own WLAN, which makes it possible to connect to an end device that can also be used to control the system.



So, despite many innovations, Uhlenbrock is sticking to tried and tested things. This is of particular benefit to model railway enthusiasts who already use components from the Uhlenbrock range or other compatible devices. The Intellibox 2neo can therefore be easily integrated into existing configurations and reduces the financial requirements for new purchases.



All functions offered by the Intellibox 2neo to its owner can be quickly accessed via the menu selection and are arranged and displayed in an understandable and intuitive way.

This central unit is also equipped for further developments, because thanks to the USB connection, future programme updates or extensions can be easily installed. It is also praiseworthy that a functional complete set of equipment is supplied and that the actual price is not concealed by the fact that, for example, the powerful power supply unit or connection plugs have to be purchased separately.

The Intellibox 2neo may not be a completely new digital centre, but, all in all, it is more than just a revision. Twelve years after the release of the second generation, it is now at the current state of development with this very extensive product update. And so, it can easily hold its own against alternatives from other manufacturers.

Manufacturer's webpages:  
<https://www.uhlenbrock.de>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

## Endgültiger Abschied der Dampflokomotive **Schwanengesang im Emsland**

*Viele Eisenbahnfreunde hofften wieder und wieder, dass sich das Ende der Dampflokomotive doch noch weiter hinauszögern möge. Keine Maschine nach ihr wirkte auf ihre Betrachter so lebendig. Das Bw Rheine was Auslauf-Bw für gleich drei Dampflokomotivebaureihen, eine davon sowohl gar in kohle- wie ölgefeuerter Ausführung. Der EK-Verlag erinnert mit einer neuen DVD an die letzten Jahre.*

CFT Video Berlin  
Bw Rheine  
Die letzte Dampflokomotivehochburg der Bundesbahn

EK-Verlag GmbH  
Freiburg 2023

DVD-Video  
Bildformat 16:9  
Tonformat Dolby-Digital 2.0  
Sprache deutsch  
Laufzeit ca. 58 Min.

Best.-Nr. 8636  
Preis 22,80 EUR (Deutschland)

Erhältlich direkt ab Verlag  
oder im Fach- und Buchhandel

Erst in der letzten Ausgabe haben wir eine DVD vorgestellt, die auch, aber nicht ausschließlich Rheine mit dem damaligen Dampflokomotivebetriebswerk zum Thema hatte und in vielerlei Hinsicht einen großen Bogen spannt. Das ist hier etwas anders, aber nicht minder interessant.

Während die Zeit der Maschinen unübersehbar zu Ende ging, schien dort die Welt noch weitgehend in Ordnung. Rheine wurde zum Auslauf-BW für die Schnellzugrenner der Baureihe 011 / 012 sowie der Baureihen 042 und 043 bestimmt.

Das bedeutete, dass auf deren letzten Etappen die leistungsfähigsten Vertreter des Schnellzug- wie auch Güterzugdienstes hier zusammengezogen wurden. Entsprechend viel Foto- und Filmmaterial wurde in diesen letzten Jahren gefertigt und meist auch veröffentlicht.

Immerhin sind fast fünfzig Jahre vergangen, seitdem die Dampflokomotive bei der Bundesbahn ihren Lebensatem ausgehaucht hat. Ihre letzten Zeitzeugen, zumindest jene mit einer Kamera im damaligen Besitz, haben fast durch die Reihe das Rentenalter erreicht.

Besonders sie, aber auch jüngere Enthusiasten, die der Dampflokomotive auf andere Weise begegnet sind, lechzen immer wieder nach Filmdokumenten, mit denen sie in der Vergangenheit schwelgen und ihre Erinnerungen wachhalten können. Dies wird auch den EK-Verlag motiviert haben, diese DVD-Produktion in Berlin in Auftrag gegeben zu haben.





Wir haben sie uns angeschaut und möchten unsere Eindrücke an dieser Stelle mit Ihnen teilen. Zentrales Thema des Films ist der Schwanengesang der Dampflokomotive im Emsland, ausgehend vom genannten Bw Rheine.

Das gezeigte Material präsentiert sich überraschend bunt und gut vertont, offenbart aber auch sein wahres Alter durch die einst üblichen Bildstörungen des Filmmaterials. Es zeigt die in Rheine beheimateten Schnellzuglokomotiven im schweren D-Zug-Dienst.

Ebenso sehenswert sind viele Sequenzen, in denen die leichtere Mehrzwecklokomotive der Baureihe 042 vor Personenzügen auf der Emslandstrecke im Einsatz ist. Solches Material taucht sonst seltener in vergleichbaren Produktionen auf. Es liefert viele Inspirationen für die Modellbahn.

Ansonsten ist diese Baureihe zusammen mit der 043 im Güterzugdienst anzutreffen, wo sie fallweise auch Vorspann vor schweren Erzzügen leisten musste. Nicht ausgelassen werden auch die Bauzugeinsätze, melancholisch mit Musik untermalt, mit denen die Dampflokomotive ihr eigenes Ende beschleunigen musste.

Neben einigen Führerstandsmitfahrten sind auch wichtige Orte und Bahnhöfe der Einsatzstrecke wiederholt in den Aufnahmen zu sehen. Dazu gehören neben Emden und Lingen (mit Ausbesserungswerk) vor allem die Emsbrücke bei Hanekenfähr und der Streckenabschnitt bei Bentlage, immer wieder aber auch der Bahnhof Rheine selbst, wo auch die anderen Traktionen vor der Kamera auftauchen.

Ungewöhnlich viele Aufnahmen konnten im Betriebswerk Rheine gedreht werden. Sie vermitteln sehr gut einen Eindruck davon, wie es für das Personal damals auf der Dampflokomotive zugeht. Wer bewusst hinschaut, entdeckt dabei auch viele Anreize fürs Ausgestalten seines Areals auf der heimischen Anlage.

Dennoch ließen sich die 58 Minuten Laufzeit nicht allein mit historischem Material füllen. Viele Aufnahmen wurden in der heutigen Zeit entlang der Strecke nachgedreht und zeigen moderne Fahrzeuge der Traxx-Familie, die Baureihe 101 und auch 103 sowie die Triebzüge der Westfalenbahn. Das alles eignet sich gut, um einen Kontrast zu schaffen und die vielen Veränderungen wahrnehmbar zu machen, aber zum Ende hin wirkt dies dann doch wie etwas zu viel Füllmaterial.

Die Produzenten bekommen so gerade noch die Kurve, um zu Sonderfahrten der alten Veteranen auf ihrer letzten Stammstrecke umzuschwenken und so an die Begeisterung für alte Technik zu erinnern, die heute wichtige Einnahmequelle für viele Museen und Museumseisenbahnen sind. Damit schlagen sie im Film eine wichtige Brücke.

Was ist sonst noch aus der Endzeit der Dampflokomotive geblieben? Gezeigt wird auch ausgiebig, wie das Abschiedsfest für die Dampflokomotive im Bahnhof und Betriebswerk Rheine ablief und welche Stimmung dort vorherrschte. Auf offiziellen Abschiedsfahrten zu sehen ist ebenso 043 196-5 – die letzte planmäßig eingesetzte Dampflokomotive der DB.

Es bedarf keiner großen Spurensuche, um sie heute unter ihrem überdachten Unterstand am Bahnhof Salzbergen zu finden, von wo aus der Blick auch leicht auf den heutigen Streckenbetrieb fallen kann. Die Überreste des alten Lokschuppen und der Lokleitung werden durch das Begehen des Ortes im Stadtteil Hauenhorst erkundet und dokumentiert.

Als hilfreich dabei erweisen sich Drohnenaufnahmen – mit dem nun erfolgten Veröffentlichen der DVD ist aber auch vieles davon längst wieder Geschichte und der Kenner merkt, dass dieses Projekt wohl einige Jahre im Voraus vorbereitet wurde. So entfaltet dieser Film eher ungewollt noch einen weiteren dokumentarischen Wert.

**Verlagsseiten:**  
<https://www.eisenbahn-kurier.de>  
<https://www.ekshop.de>

Farben wie vom Winde verweht

## Beeindruckend oder hilfreich?

*Ein neues Buch zum gesprühten Farbauftrag im Bereich der Modellbahn möchte ein Ratgeber sein, schränkt seinen Nutzen aber gewollt ein, weil erforderliches Grundwissen vorausgesetzt wird. Einige vermeidbare Schwachpunkte liefern uns auch Grund zur Kritik, obwohl hier in Summe ein gutes Buch vor uns liegt.*

Mathias Faber  
Praxishandbuch Airbrush  
Modellbahnanlagen farblich gestalten

VGB | Geramond  
München 2023

Klappenbroschüre mit Fadenbindung  
Format 22,5 x 26,5 cm  
168 Seiten mit ca. 330 überwiegend farbigen Abbildungen

ISBN 978-3-96453-603-7  
Preis 29,99 EUR (Deutschland)

Erhältlich direkt ab Verlag  
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Vom Autor Mathias Faber aus Hamburg haben wir in diesem Magazin bereits einige Bücher vorgestellt. Sein jüngstes, das nun (wieder) bei inzwischen zur zu Geramond gehörenden Verlagsgruppe Bahn erscheint, ist Gegenstand dieser heutigen Rezension.

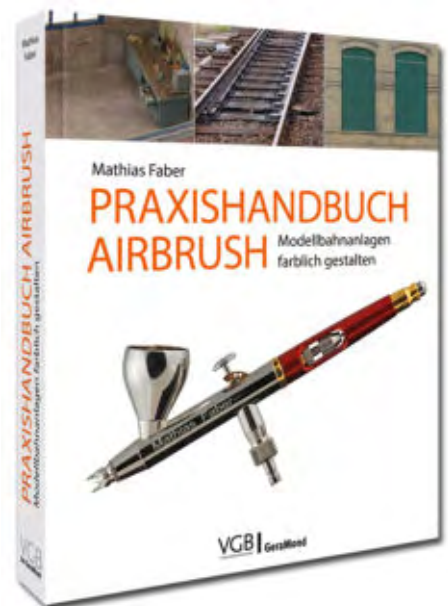
Mathias Faber ist bezeichnet sich als vom anspruchsvollen Modellbau und der Modelleisenbahn begeistert. Dies bringt er auch durch seine Fremo-Mitgliedschaft gern zum Ausdruck. Beruflich hat er eine Ausbildung zum Maler und Grafiker vorzuweisen. Seit den achtziger Jahren veröffentlicht er auch Bücher.

Dies erscheint auch vor dem Hintergrund als Hinweis wichtig, dass der vorliegende Titel Anfängern im Umgang mit den Techniken eines Spritzapparats („Airbrush“) keine Hilfe leisten wird. Er wird sie neugierig machen und beeindrucken, aber vom Nachahmen oder Anwenden des Dargestellten blieben sie weit entfernt.

Schon im Vorwort stellt der Autor klar, dass er grundlegende Fertigkeiten sowohl im Modellbau als auch bei Spritzlackierungen als vorhanden voraussetzt. Insofern handelt es sich hier nicht um einen klassischen Ratgeber oder ein Übungsbuch, sondern eher einen Folgeband.

Das erforderliche Einstiegswerk ist nach seinem Bekunden der eigene Titel „Airbrush im Modellbau“, der vor acht Jahren erstmals bei Geramond erschienen ist. Wir hatten die seinerzeitige Auflage in **Trainini®** 7/2018 vorgestellt.

Darin waren grundlegende Fertigkeiten, Vorgehensweise und wichtige Techniken sehr anschaulich und nachvollziehbar beschrieben und dokumentiert. Das neue Buch wendet sich in diesem Zusammenhang an alle diejenigen Leser, die daran Gefallen gefunden haben und sich mit dieser im Modellbau eigentlich unverzichtbaren Farbauftragstechnik auseinandergesetzt haben.





Gestiegen sind nun die Ansprüche, denn es werden weitaus komplexere Projekte behandelt, die viele Schritte und sinnvolle Kombinationen erfordern. Doch es geht auch nicht allein um den Farbauftrag, sondern oft auch um geeignete Vorbereitungen beim Bau von Modellen.

Auch das Modifizieren von Bausätzen und das Fertigen eigener Objekte spielen nicht nur eine Nebenrolle. Bindendes Glied aller behandelten Projekte sind das Beobachten von Vorbildsituationen und ein genaues Studium der Farben in der realen Welt. Das war und ist zu erwarten, denn der Titel stellt ja Farben und deren Auftrag nicht zufällig in den Mittelpunkt.

Nicht folgen können wir aber dem Verlagsfazit „Airbrush einfach erklärt“. Natürlich spielt dieses Thema oft eine zentrale Rolle im Modellbau und auf jeden Fall im Buch, aber detaillierte Erklärungen sind in diesem Werk eher Mangelware, weil ja auf den Vorgänger verwiesen wurde.

Die dargestellten Arbeitsabläufe sind keinesfalls so detailliert, wie in der Titelbeschreibung behauptet. Im Aufbau handelt es sich eher um einen Bildband, der durch verschiedene Projekte mit vielen Teilschritten führt, die jeweils mit wenigen Sätzen beschrieben werden. Dabei geht es meistens darum, was gemacht wurde und nicht, wie dies zu erfolgen hat.

Auswahl wie Qualität der Bilder sowie deren Reproduktion sind durchweg als gut und hochwertig zu bezeichnen, was hier ein ausgesprochener Pluspunkt ist, weil ja auch eine möglichst treffende Farbwiedergabe zu den besonderen Herausforderungen für den Verlag gehörte.

Das Spektrum der Themen reicht in drei wichtigen Kapiteln vom Gleisbau über die Architektur – diese mit dem weitaus größten Seitenanteil – bis hin zum Gestalten der Umgebung rechts und links der Gleise. Die Ausführungen zu Farben an Straßenfahrzeugen am Ende des dritten Kapitels stellen jedoch einen inhaltlichen Bruch zu den übrigen Inhalten jenes Abschnitts dar.

Ein paar Kritikpunkte möchten wir hinterlassen: Ein wiederholtes „Product Placement“ für MKB und Harder & Steenbeck / Hansa, beide wie der Autor in Hamburg ansässig, empfinden wir als aufdringlich, zumal die Buchinhalte – wie auch an anderen Stellen – hier auch eine Neutralität erlauben. So etwas wird im Sprachgebrauch nicht grundlos als Schleichwerbung gebrandmarkt, heute aber leider gern von „Influencern“ verschleiert.

Sofern diese Hersteller über ein gewöhnliches Maß hinaus unterstützen und vielleicht sogar Einfluss auf die Inhalte und Darstellungsweisen nehmen, erfordert das einen expliziten Hinweis, um glaubwürdig wie authentisch zu bleiben. Dies bleibt immer eine Frage guten Stils, im Pressewesen unterliegt es sogar einem Ehrenkodex.

Unsere Kritik haben wir bewusst mit einem Anglizismus begonnen, denn damit streifen wir einen weiteren Bereich, der uns das eine oder andere Mal aufgestoßen ist. Viele Begriffe aus dem Englischen haben sich im Modellbau und besonders auch rund um den Sprühauftrag von Farben eingebürgert.

Trotzdem bevorzugen wir Deutsch, solange es passende Ausdrücke gibt. Als Beispiel mag die Polystyrolplatte an Stelle eines „Plastic Sheets“ dienen. Das mindert auch die Gefahr, ältere Buchinteressenten mit hohen handwerklichen Begabungen nicht vom Verständnis auszuschließen und sich dem Leser womöglich abgehoben und arrogant zu präsentieren.

Wer die erforderlichen Grundfertigkeiten besitzt und die Vorzüge dieses Buches zu nutzen weiß, der findet hier eine Begleitung, mit der sich die eigene Kreativität sicher noch beflügeln lässt und neue Herausforderungen geweckt werden.

Verlagsseiten:  
<https://www.vgbahn.shop>

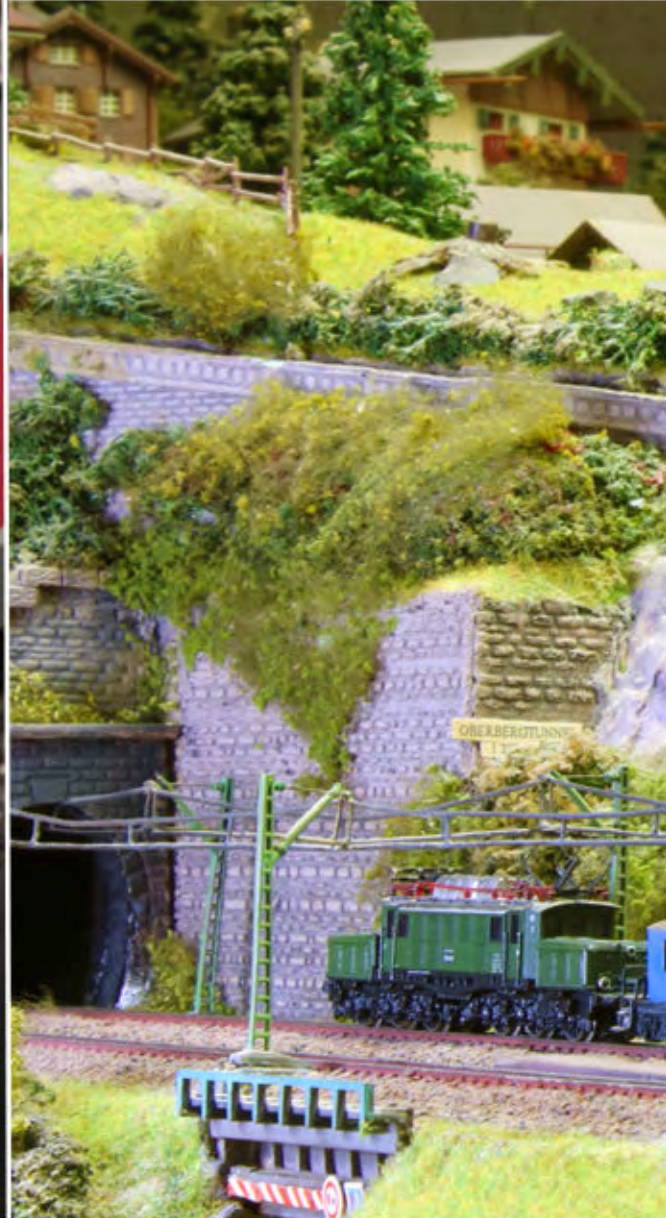


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**Trainini**  
Praxismagazin für Spurweite Z



Model railways in Sindelfingen

## The Meeting of the Regions

*In the summer months, dealing with model railways is certainly not at the top of the list of topics. Nevertheless, the clubs from Baden-Württemberg and Bavaria met again for a joint summer club meeting in Sindelfingen on 16 and 17 June 2023.*

By Ralf Junius. Early on Saturday morning, I took the ICE from Düsseldorf via Stuttgart to Sindelfingen. There, Eberhard Krug had opened his business premises once again and held a club meeting.

Because of the space available there, it was possible to set up some larger exhibits in addition to smaller layouts, unlike at the regular club meetings. So, there was not only a lot to exchange, but also to see and experience.



The book "Mini-Club-Praxis" covered the creation of this segment layout by Karl Albrecht, which was subsequently acquired by Eberhard Krug and shown in Sindelfingen.

In addition to the showpieces of old-established Z gauge railroaders, the models of two young Zetties also caught my eye. It's great that both of them were allowed and able to attend this meeting.





Also on display were two small offspring layouts that are still under construction (photo above).

The original Albrecht system and a replica (demonstrated by Klaus Moser) stood side by side on site for direct comparison (photo left). Photos: Ralf Junius



A large part of the area was taken up by two modular layouts. The first of them is the layout built by Karl Albrecht and once documented in the book "Mini-Club-Praxis" for replicas. Today, it is owned by Eberhard Krug.

The other was set up exactly opposite and shows one of the replicas that the author once intended. This provided direct opportunities for comparison.

I was very taken with a small box on site because it depicted a scene from Africa. Although there was no train there, there were many interesting impressions of the continent. Among other things, the Dornier Do-27 with the registration D-ENTE was shown there.

This is the machine that was used for the filming of "Serengeti darf nicht sterben" ("The Serengeti must not die") by Prof. Dr. Bernhard Grzimek, but in which his son Michael then crashed and died over the African steppe.

An older layout showing a short section of the first ICE high-speed lines (valley crossing between two tunnels) was also shown again after several years. Although this showpiece by Rolf-Dieter Wörz was once shown at some trade fairs, this exhibit was previously unknown to me.

Last year in Altenbeken I saw the "Junction City" layout for the first time. It was also shown in Sindelfingen. So, now I was able to marvel at everything in detail and take photos of it.





**Photo above:**  
The steppe landscape of Africa appears authentic, as great care has also been taken to depict the correct vegetation. At the bottom left, the off-road vehicle and the Dornier Do-27 aircraft with the registration D-ENTE of the two Grzimek animal researchers can be seen, clearly locating what is shown in the Serengeti.

**Photo below:**  
Rolf-Dieter Wörz found the template for his new ICE line with its alternations between tunnel and bridge sections almost on his doorstep. At the time of construction, the high-speed line was under construction between Stuttgart and Mannheim. Photos: Ralf Junius

This list covers only a few of the exhibits that were on display. The day went by so quickly, and, soon, it was time for me to return home.

continues on page 59





**Photo above:**  
The track crossing at the station is the namesake of the "Junction City" layout, which was demonstrated for the first time in Altenbeken in May 2022.

**Photo below:**  
The atmosphere of US railway lines seems to have been captured very well here. In any case, the many scenes appeal not only to the author of this article. Photos: Ralf Junius





From the early days of Z gauge comes this advertising sign in the then modern pop art style. You could also see this historic motif in the big Mini Club guidebook "Alles über die Spur Z" for the 50th anniversary last year.

Despite the long journey there and back and the many kilometres, the visit was really worthwhile for me. Apart from "driving around" the beautiful exhibits with my eyes, I was also able to have many interesting conversations with the Zetties present on site. The well-known concern of small get-togethers with club members was, therefore, not neglected, either.

Club webpages:

<https://z-stammtisch-bayern.de>  
<https://www.zclub92stuttgart.com>

Readers' letters and messages

## Zetties and Trainini in Dialogue

**Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.**

**Good news about crZ Christian Ribatzky:**

In the April issue 2023 you had published my letter from March. Thank you very much. Today I have to add the following positive things:

I then tried to reach Mr Ribatzky again in June via the crZ portal contact request, with success! Mr Ribatzky answered me and also called me back. We had a nice chat and he also told me about an accident-related outage. However, the crZ company is still on the market with its decoders, including sound decoders.

My question was about a decoder for the BR 143 (Märklin 88431). Since Mr Ribatzky does not yet have a decoder for this model in his programme, he suggested that I make my model available to him for decoder development. I get the model back digitised and only have to pay the pure costs for the decoder.

I think that is a good service to the customer. I want to say that I cannot share the bad experiences that you described in your answer (...), except for the lack of feedback on contact requests, which was more likely due to the accident at work.

Peter Taubert, Schwerin

**Running problems on Rokuhan switches:**

I have problems with Rokuhan turnouts R040. A small tender locomotive, Märklin 8895, derails regularly. When I asked Rokuhan, I received a disappointing answer (...).

Is this limited functionality known in Zetties circles? Is it mentioned in the product descriptions of the Noch company?

Karl-Jürgen Heine, Boniswil (Switzerland)

Editor's answer: If the problems affect the same type of points several times, but always the same vehicle, while others can pass without problems, the Märklin locomotive should also be considered as the cause. Single-axle leading points on steam locomotives are known to us as sensitive parts. A possible cause could be a deviation of the flange inside dimension or also a too little contact pressure of the largely unloaded leader. An indication of this could also be provided by targeted driving tests with detailed observations: driving over the points both forwards and backwards as well as alternately over the straight and deviating track.

**Embedding Trainini TV on the portal page:**

This morning, while browsing the Trainini website, I noticed that a reference to **Trainini TV** is missing.

Hans Helbach, Bonn







**Trainini**  
Praxismagazin für Spurweite Z

Aktuelles   Magazin   Trainini TV   Rekorde   Team

Spur-Z-Welt   DE   EN

## Trainini TV

### Alles, was Fotos nicht sagen können...

Mit unserem Magazin stoßen wir überall an Grenzen, wo Texte und Fotos nicht zum Ausdruck bringen können, worum es in einem Bericht geht. So können wir darüber keinen Eindruck vermitteln, wie ein digitales Sound-Modell klingt oder wie eine aufwändige Lichtsteuerung arbeitet. Auch die auf Messen und Ausstellungen dominierende Stimmung lässt sich mit Worten allein nur sehr eingeschränkt vermitteln.

Editor's reply: Indeed, we have already given a lot of thought to embedding our channel in our own pages in a legally secure and convenient way. The fact that this is also requested by readers confirms us and pleases us in equal measure. As a result of the letter, it has moved up on our agenda and has now been embedded in the portal via a separate tab with page and link selection option. We sincerely hope that **Trainini TV** will see a significant increase in the number of hits, as it still lags far behind our magazine.

#### US bridge kit from Raildig:

On the pages of the Ztrack Centre (<https://ztrackcenter.com>) a new bridge kit by Raildig is offered. It is titled as "Catfish Creek Bridge" (art. no. RD-170) and is offered without the diorama in which the bridge was embedded for illustrative purposes.



The assembled kit "Catfish Creek Bridge" (art.-no. RD-170) has been inserted into a diorama here for demonstration purposes. Photo: Raildig / Ztrack

#### Azar Models clarifies:

In response to an enquiry from our editorial team, Azar Models (<https://azar-models.com>) has clearly stated its position in the interest of its customers. We had enquired whether injection-moulded models of the G4 boxcar had actually been delivered without a base coat.

This was obviously the case, but met with acceptance in France, which had not exactly been spoilt with Z gauge so far, especially as some customers had patinated the models and liked the appearance thus achieved.

In Germany, on the other hand, where many model railroaders shy away from their own colour treatments, this caused some resentment. Azar Models pointed out that this was the first time they had gained



This trial spray shows the CC7200's body on the heavy metal running gear as it will receive the production delivery of this SNCF locomotive. Photo: Azar Models

experience in injection moulding and that they had learned here to always completely paint models from this production technique for larger quantities in the future.

The industrious manufacturer has also shared with us the first shots of an assembled hand sample of the French CC72000 diesel locomotive with a metal running gear that now gives it strong tractive power. As soon as a painted and lettered sample is also ready, we may further inform our readers!

#### Summer deliveries from Noch:

While the eagerly awaited 3D printed figures for Z gauge seem to be delayed a bit, Noch (<https://www.noch.de>) reports the delivery of further new products. The tree XL packs from the "Classic" series are now available in various combinations.

Suitable for Z-gauge are the following sets with five 8 - 10 cm tall deciduous trees and eight 8 - 12 cm tall fir trees: Fruit trees (item no. 25610), flowering fruit trees (25615), deciduous trees (25620), autumn trees (25625), and fir trees (25640).

The 41 x 26 cm sea foil (60852) can also be ordered in a transparent version and the Knitterfelsen (crumpled rocks) XL, which come from their inventor Andreas Dietrich and are now offered in an even larger format of 61 x 34.5 cm. The variants "Wildspitze" (60307), "Großvenediger" (60309) and "Seiser Alm" (60311) are offered.

#### New deliveries from AZL:

Even if AZL is not able to present any new models this month, there are definitely exciting models for various customer interests.

The EMD E8, for example, comes in the neat design of the Chesapeake & Ohio. The A-unit is offered with two operating numbers (art. no. 62617-1 / -2).



The EMD E8A (item no. 62617-1) looks good in the Chesapeake & Ohio colour scheme. Photo: AZL / Ztrack



There are also new variants of multi-part column carriers for truck trailers. The three-part units, each with a prototype length of 53 feet, bear the old TTAX logo and are loaded with Xtra trailers, in use for the US Postal Service (905230-1). Two of each of these trailers are also available separately (954006-1).



The heavy half luggage car runs in the UP version (74008-1) on three-axle bogies.  
Photo: AZL / Ztrack

The R-70-20 refrigerated trucks are now returning to the range in ATSF orange. There is a choice of a two-pack (914833-2) and a four-pack (914803-2). The last delivery is the heavy half luggage car on three-axle bogies. With no less than three service numbers, it is continuously in service with Union Pacific (74008-1 to 74008-3).

#### Deliveries by Märklin since the last editorial deadline:

The tiny small car KlV 20 from the Wolff company in Walsrode (item no. 88026) has found its way to the dealers. The VW T1 combination vehicle from the former DB stock experienced its last period of service there and is to be classified in epoch IV. Unchanged, the model is equipped with a small bell-shaped armature motor that works via a belt drive. The housing is made of metal-filled plastic.



The current new edition of the class 138 (item no. 88386) stands out from its predecessor technically due to its drive and visually due to its fine printing, but not in the colour scheme chosen by Märklin.

The SBB class Re 460 electric locomotive (88468) is now available for Swiss railway enthusiasts. This model belongs to Era VI, because Märklin indicates an operating condition around 2018. Like the later editions of this locomotive, it does without an electric overhead line function, but offers the customer equipment with a bell-shaped armature motor and warm white LEDs for the head and tail lights.

We were very impressed by the electric locomotive class 139 of the Deutsche Bundesbahn (88386). The body is equipped with six panel vents and a central engine room window. It seems to be a firm rule at Märklin that this class and an ocean-blue-ivory-coloured one belong together, where one or the other customer might have wished for a chrome-oxide-green sister locomotive to the older model from the eighties.

Particularly appealing is the clean paint job, which also sets off the roof superstructure in umber grey and the pantograph drive in two colours. The printing is also impeccable, highlighting details such as rubber strips, handrails, door handles, chrome rings on the lanterns or the rubber seal of the engine room window.

The indicated operating condition around 1978 results in a classification in epoch IV. Our impressions are rounded off by excellent driving characteristics thanks to the installed bell-shaped armature motor. Warm white LEDs ensure direction-dependent peak lighting.



The Prussian express luggage coach Pw4 (87566) reproduces the early Bundesbahn period without the Ege biscuit printed on it. It also ran in this design together with the four-axle compartment coaches that are still awaiting delivery.

Initially still without its “car escort,” the express train luggage car Pw4 (87566) in the condition of the early Bundesbahn has set off on its journey. The Prussian design with train driver's cab has a chassis with sprinkler system and reproduction of the floor details, attached running boards, access ladders and handle bars.



Many customers have long awaited the reissue of this model. This time Märklin presents the condition before the Ege biscuit was attached, which was also the final service condition for the still missing compartment cars due to the expected retirement in the near future.

The last new delivery before the editorial deadline was the passenger train steam locomotive of class 38 (88997) in an early DB version. It does not yet have an Ege biscuit, but still has the Prussian box tender and large smoke deflectors.

**Exhibition space reopened:**

As we reported in **Trainini TV** episode 15, HRT GmbH in Marsberg-Westheim, better known under its brand 1zu220-Shop (<https://www.1zu220-shop.de>), has reopened its showroom for its customers since 8 July 2023.

In it, the highest art of model making is shown on various layouts and dioramas, and the models, which are also available in the company's own shop, are demonstrated in digital operation. Customers are asked to book in advance for Saturdays, as the visit to the demonstration room is preceded by a short city walk, during which templates of some realised scenes and buildings from the company's own series are shown.



In the meantime, managing director Jörg Erkel no longer devotes himself solely to Z gauge. In the actual sales rooms, a 0-gauge driving diorama also awaits his customers, and the H0 scale is soon to be appropriately honoured in the rooms as well.

A further journey is also worthwhile, especially as there is also a Ritzenhoff factory outlet on the premises, which sells high-quality glass articles, as well as the Westheimer beer specialities produced on site.

The temporary closure of the room was partly due to the Corona restrictions, but also to the expansion of the large segment installation, because a gravel loading facility based on a regional model was added, which had to be set up and embedded in the existing control configuration. This was and is not yet on public display outside Westheim!

#### Back to Spur Z:

The Panzer-Shop.nl (<https://www.panzer-shop.nl>) is again offering articles for Z-gauge. He justifies this with e-mails from customers who wanted the continuance of such offerings. The articles have now been reintegrated into the range.

We, too, can only profess to be enthusiastic about the detailing of the vehicles and had already been preparing an article when suddenly the end was announced and we had postponed our report due to lack of availability. However, the range also includes various figure sets.

We very much hope that the operator's decision will pay off and that the nominal scale Z will find its place there permanently.

#### Lack of clarity at Micro-Trains:

The current status of new products at Micro-Trains (<https://www.micro-trains.com>) is not immediately clear, because some models are still listed in the delivery list, which are supposed to have been on the market for a few months. The manufacturer also seems to want to enter into a preview with others.

According to the status information, two models of the US diesel locomotive EMD F7 for the Norfolk & Western (art. no. 980 01 341 / 342) should be on the market or currently on their way to the dealers, but in our opinion they should have a hard time holding their own against the much younger designs of the direct competitor with their old drive and without window inserts.



Reissued in this Norfolk & Western livery is the EMD F7 (Item No. 980 01 342) from Micro-Trains. Photo: Micro-Trains

The "War of the Worlds" series, based on a literary novel, can also be continued with another boxcar (518 00 843). This is wagon number 5 of a series that deals with the attack by Martians described in the books and the successful defence by mankind.

#### Faszination Modellbau in Friedrichshafen again:

From 3 to 5 November 2023, the 21st Faszination Modellbau (<https://www.faszination-modellbau.de>) will take place in the exhibition halls of Friedrichshafen in the border triangle. The exhibition will be held together with a real steam hall meeting and a Lego exhibition. We do not yet have any information about the participation of Z gauge. Z-Freunde International e.V. will be in charge of organising the event.



## Further new items from Yellow Dwarf:

It is hard to keep up with the high delivery speed of Yellow Dwarf (<https://www.yellowdwarf.eu>), because month after month we are allowed to report here. There is always a feeling for gaps and exclusivity that can give a layout its special and individual charm.



Yellow Dwarf fills re-emerging gaps with the advertising pillars (art. no. 60041; photo left), while completely new territory is entered with the "Fun at the Pool" (60467; photo right). Photos: Yellow Dwarf

The series of small watercraft is currently being continued with kayaks (art. no. 60102). Since the withdrawal of Modelplant, advertising pillars (60041) have also been a gap in the range, which, like "public waste bins III" (60045), are now supplied from the Czech Republic.

Rather more unusual scenes at castles, market places, and, in the latter case, gardens will decorate the fountains (60303) and "Fun at the Pool" (60467).

## Recent changes in the Electrical and Electronic Equipment Act:

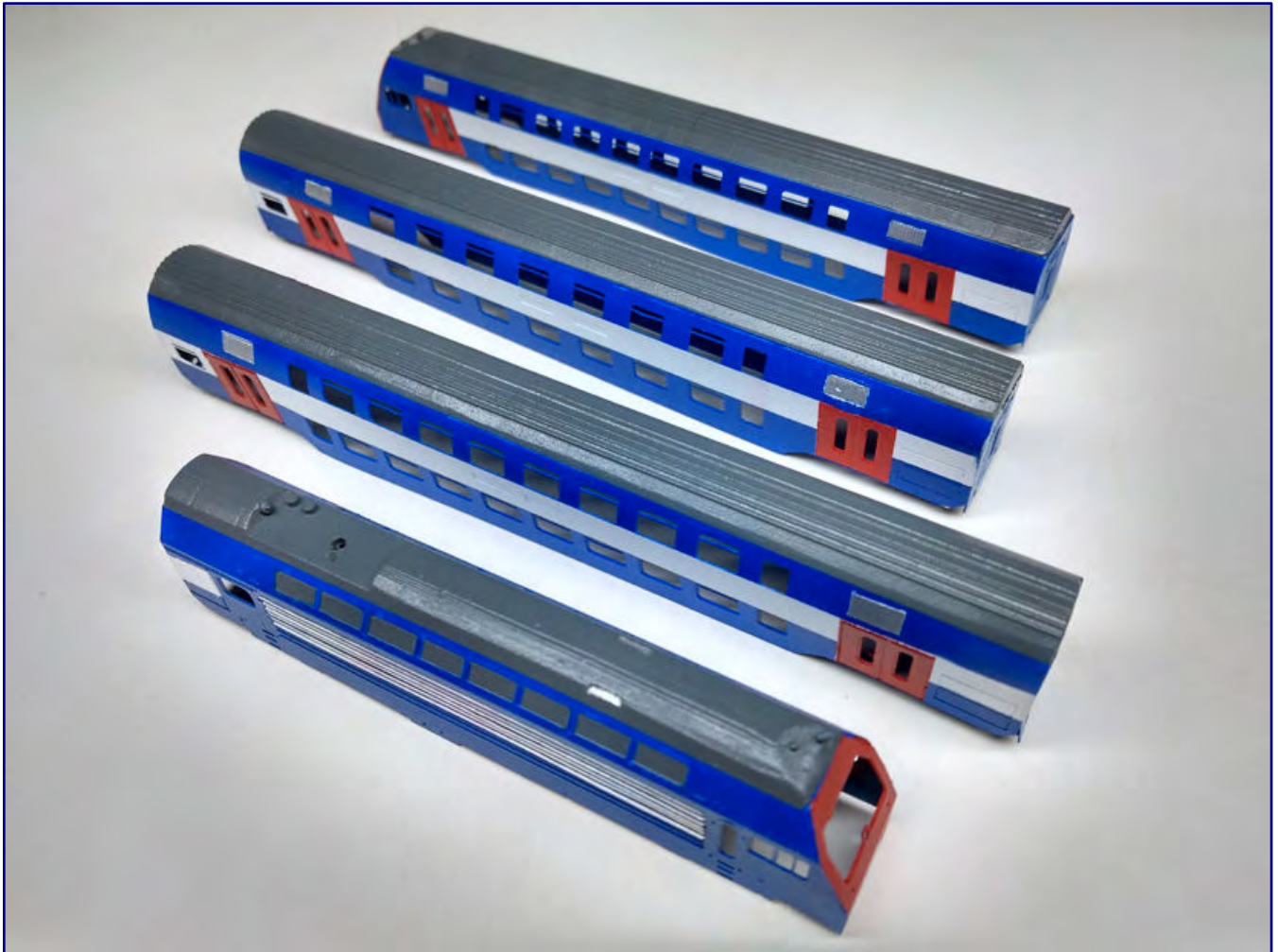
Märklin has informed its affiliated dealers about changes in the Electrical and Electronic Equipment Act that came into force on 1 July 2023. Since then, operators of electronic marketplaces such as Amazon or eBay, as well as dealers with their own sales via the Internet, have had to comply with a new inspection obligation with regard to ElektroG.

This applies if electrical and electronic items are offered or placed on the market within Germany or into Germany. Märklin fulfils this obligation for all three brands, whereas it can quickly become a major and serious problem for dealers' own offers or dealer platforms. After all, the affected equipment now also includes "passive electrical and electronic items" such as tracks or cables with plugs.

In case of doubt, such providers are well advised to seek legal advice from an official body such as the Chamber of Commerce and Industry (if the provider is a member there) or an expert lawyer in order to implement and correctly fulfil all obligations arising from the amendment to the law.

## New products with a view at Z-Otti:

Hans-Jörg "Otti" Ottinger (<https://zotti.lena-johannson.de/shop>) has also been active. New in his range are high-quality nickel silver etched parts kits for the Swiss Re 450 (locomotive, 2 double-decker intermediate cars and the driving trailer) and the double locomotive Ae 8/8. They are suitable for ambitious modellers, not for beginners without experience with etched kits.



The Re 450 double-decker multiple unit is both a novelty and a highlight in the Z-Otti programme. By means of a Märklin running gear, the four-part set can also be easily made to run. Photo: Z-Otti (Hans-Jörg Ottinger)

The kits for the Re 450 are supplied with numerous pressure elements, including the running gear mounts and a complete interior. All parts for completing the kits are included except for the motors, scissor pantographs and wheel sets. The set can be easily motorised by a Märklin locomotive (item no. 88441 and identical in construction).

For the Ae 8/8 a professional motor and dummy set is planned for 2024 in cooperation with Harald Freudenreich. Based on the entries made so far, the edition size will be 20 pieces, but could be increased if necessary. A motorisation by a Z-Shorty is just as easily possible, parts for the corresponding conversion are included.

Another good idea are certainly the Märklin compatible catenary masts with tension weights. They are created completely with the help of 3D printing.

#### July new products at WDW Full Throttle:

William Dean Wright continues its food refrigerator series. The latest version is dedicated to the Clicquot Club Company ("Kleek-O"), a once very popular beverage manufacturer from the USA that was active since the end of the 19th century.



# Trainini® *International Edition*

German Magazine for Z Gauge

He transported high-quality, sometimes exotic ingredients to his numerous factories to produce different flavours of lemonade. The latest two-pack 1 (art. no. FTB9208) suggests a continuation due to its name and is dedicated to the design of this company, which is characterised by large letters. WDW products are distributed in Germany by Case Hobbies (<https://case-hobbies.de>), among others distributed.



The refrigerated trolleys for the Clicquot Club Company (item no. FTB9208) are part of the supplier's food series. Photo: WDW Full Throttle

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Board of Editors:  
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Dirk Kuhlmann  
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Correspondent North America:  
Robert J. Kluz

English translation:  
Alexander Hock, Christoph Maier, Oleksiy Mark, Martin Stercken

Further voluntary work: Stephan Fuchs, Ralf Junius (**Trainini TV**), Torsten Schubert

Licensed **Trainini Discussion Group** (<https://www.facebook.com/groups/1597746057122056/>): Michael Etz (**Trainini Locomotive Doctor**)

Publisher and Responsible in terms of German press law (V.i.S.d.P.) is Holger Späing, Am Rondell 119, 44319 Dortmund; Contact: 49 (0)231 9598 7867 or by e-mail to [redaktion\[at\]trainini.de](mailto:redaktion[at]trainini.de).

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# Trainini

German Magazine for Z Gauge



**Lots of Traffic in the Sixties**

**Greatest Era of the Steam Crane  
A Water Tower Project**

## Introduction

Dear Readers,

For over 50 years, the 1:220 scale has been firmly anchored in our minds. In every decade, with the simultaneously growing range of products, extraordinary and highly attractive Z-gauge layouts have been created.

Some have even survived the times and have been given a place of honour in our very personal charts. In other gauges, the press likes to talk about super layouts, and this is exactly where "Werdersheim" belongs, which we will portray in detail today.

The choice of motifs and materials, suitable models and a large portion of individuality have created a very special overall result that leaves lasting impressions. The railway traffic shows itself with a versatile touch, whereby even the admirer of long trains is accounted for.

Very rare is the strict reduction to a certain year, which in the model can be classified in the middle of era III. On the following pages, dive with us into a world that seems to represent a hitherto completely unknown town in the Bergisches Land and experience everyday life there amongst the local population.

After leaving the fantasy world and arriving back in reality, a drop of melancholy remains: "Werdersheim", as a non-portable residential layout, will unfortunately never be shown at an exhibition. But it would certainly have been an extraordinary eye-catcher there.

What we can do to counter this insurmountable gap is a presentation on **Trainini TV**: Ralf Junius made, edited and moderated video recordings for us. As episode 16, we were able to make it available on our channel shortly before the publication of this edition.

But we have more topics to offer in this edition. And, if I have already mentioned that elsewhere superlatives are often used to stand out from the crowd, we Zetties may not be too impressed: our community seems to be full of them and this is also reflected in this edition.

Jochen Brüggemann has ventured on a self-build project that may seem easy only at a first glance. As we all know, the devil is in the detail, and he demonstrates this very clearly with his water tower project. Mathematics and model railways are two threads that rarely meet at this level.

On the manufacturer's side, too, we find offers that fit perfectly into what we have described so far. Not without reason we had awarded the Ardelt steam crane 57 to. by NoBa-Modelle, which was published last year. Today, we underline this with a construction report and some prototype history in a way that will hopefully draw your attention also!

On that note, I now hope you enjoy reading and realising your own ideas!

Sin-Z-erely,

Dirk Kuhlmann



Dirk Kuhlmann  
Editor



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Currently no items

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We thank Jochen Brüggemann for his guest article and the Eisenbahnstiftung and Walter Schepperle (Eisenbahnfreunde Wehratal e.V.) for historical photo material.

Date of publication of the German language version of this issue: 31 August 2023

### Cover photo:

V 60 563 has only completed a short period of service with DB and still has a much longer one ahead of it. In Werdersheim, it earns its living by shunting the many freight wagons that want to be moved along the loading road and goods shed. Meanwhile, the increasing car traffic has to wait at a closed barrier.

The Ardelt-Crane 57 to. from NoBa-Modelle

## Powerful Help for heavy Loads

*Rail service cars are something special. The editors of Trainini® know this at the latest since we have focused on them and service wagons in our reports: The success of these issues proves us right. We were all the more pleased that NoBa-Modelle recognised these signs and filled a gap in the market with the 57-ton steam crane from Ardelt. When using the layout, the model also provides a lot of tinkering fun and also offers challenges.*

In **Trainini®** 1/2023 we have published our decision to select the Krupp Ardelt rail crane 57 to. (art. no. 5319R) from NoBa-Modelle as the new release of the year 2022 in the technology category.

Consciously, we advised that we chose the resin kit instead of a finished model (5319RF). However, the simultaneously announced article on building this model was delayed, which was also done with respect to this year's summer break of this manufacturer.



The crane train from NoBa-Modelle in layout use: The way from the kit to here, poses some challenges, and at the same time offers plenty of tinkering fun.

But now the time has come: We show how the model is gradually built from the resin and metal parts plus the application of paint and decals. On **Trainini TV**, too, we will be covering the construction project in moving pictures in episode 16, which will be released soon, and will also discuss a few special features, challenges and tricks.

If one or two readers who didn't get their hands on it last year now still have an appetite for this giant on Z gauge tracks: NoBa-Modelle resumes production with the start of the tinkering season in October. Our report will at least shorten the waiting time a bit.



Before we present the construction of the model, we would like to take a look at its prototype. This should help to correctly classify the tasks of this rail crane and its importance. After all, there were many types of rail cranes with very different tasks and loads, which was already a topic in our magazine.

## In the age of steam cranes

No sooner was the Second World War over than there was (also) a lot of cleaning up to do in the western zones. The railway had played an important role in the logistics of the war, and was, therefore, also the focus of Allied bombing raids. During their retreat, the Wehrmacht had desperately tried to stop their advance by blowing up bridges: 3320 railway bridges were affected.

To accomplish this task, a sufficient number of heavy railway cranes were also needed. One of the pre-war manufacturers of such equipment was the Ardelt company in Eberswalde. In spring 1945, the owner's family fled to Lower Saxony to escape the approaching Red Army. In 1946, they ventured a new start in Osnabrück and Wilhelmshaven with Ardeltwerke GmbH.

In 1948, an order from the Deutsche Reichsbahn of the Bi-Zone for the construction of four steam cranes with a lifting capacity of 57 tonnes went to Osnabrück. They were to be delivered as early as 1949, which was only possible if they could be based on designs that had already been built in the pre-war period. In fact, Ardelt's 90-tonne steam crane was used as a basis.



The Ardelt 57 t "Mainz 6600" steam crane is on the way to a job site on June 22, 1968 near Schwäbisch Hall-Hessental. Its jib rests on the "Mainz 6650" crane tender. Photo: Heinz Hangarter, Collection Eisenbahnstiftung

The tonne figure, which is crooked and seems arbitrary compared to the other steam cranes, is easy to explain: Only with this load capacity limit was it possible to limit the wheelset load to 18 tonnes on average, so that the counterweights could remain mounted on the crane superstructure (i.e., on the slewing cab) and there was no need for a counterweight wagon.

For the DR, which was in dire financial straits and had placed the order, economy was the highest priority at the time. And, so, it was also an advantage that the entire crane unit consisted of only five wagons: crane and protection wagons, plus water wagons, equipment wagons, and a caravan – four of which NoBa-Modelle took up for its implementation.

By the time the new cranes could be put into service, the “DR Brit-US-Zone” had already become the Deutsche Bundesbahn. It classified the four new additions as Essen 6600, Mainz 6600, Munich 6664 and Wuppertal 6602, which also indicates a focus of use in the particularly hard-hit Ruhr area.



On 13 July 1968, the 57 to. crane “München 6664” stands in Gaildorf-West station to replace bridge parts on the Kocher Bridge. The photo also shows the supplying tender 2'2' T 34 and the staging car for the crane personnel – a former “Hechtwagen”. Photo: Wolf Schneider, Collection Eisenbahnstiftung

The main tasks were to put vehicles back on the track, bridge construction sites, and the reloading of other heavy loads, also in the service of private railway customers. For particularly heavy tasks, two crane units were used together, which could then also lift heavy locomotives. Such operations lasted until 1978/79.

The last of the four cranes to be taken out of service was the former Mainz 6600, which was later assigned to the Ludwigshafen depot as Karlsruhe 6601. It ended its service in 1979 under the UIC number 30 80 974 0 035-9, under which UIC number it also became the model for the miniature of NoBa-Modelle. It is the only one that has been preserved for posterity at the Technik Museum Sinsheim.

continues on page 8





In 1957, Ardel's 57-tonne cranes were still among the most modern lifting equipment the Bundesbahn had to offer. Thus, during a trade fair demonstration, the performance of the apparently golden-yellow painted rail crane is demonstrated on a tender. This was done with a low outreach but maximum support. Interesting for the model to be presented today is the layering of the construction sleepers on which the stabilisers of the supports rest. Photo: Slg. Eisenbahnstiftung

The outrigger width was 6 metres. With a total weight of 106 tonnes, the steam crane of 1949 could lift a maximum load of 57 tonnes with an outreach of between 5.5 and 7.5 metres. The maximum possible outreach of the rigid jib was 15 metres, which still allowed 15 tonnes of suspended load on the auxiliary hook all around. Unsupported, it could lift 23 tonnes in the direction of the track, but only a modest 3 tonnes in all-round operation.

But in order to be able to operate in this way, a rail crane first had to get to its place of use. In order not to exceed the clearance gauge of the tracks, the chimney top was designed to be removable and was lashed behind the crane house above the weights.

The image is a technical drawing titled "Kranwagen 57 t Tragfähigkeit mit Dampfantrieb". It consists of three views: a side view of the crane with its jib extended, a side view of the crane with its jib retracted, and a top-down view of the crane on the rail wagon. The drawing includes various dimensions in millimeters. The side view with the jib extended shows a total width of 6000 mm and a height of 12100 mm. The side view with the jib retracted shows a total width of 3000 mm and a height of 12100 mm. The top-down view shows a total length of 10900 mm and a width of 6000 mm. The drawing also shows the crane's base, wheels, and the rail wagon it sits on.

When the cranes were new, the trade publications were also devoted to them. In June 1950, Glaser's Annalen showed drawings of the crane both in the transport position and supported with the jib extended. In addition, a third drawing provided the dimensions for the highest and widest design. Drawing: Glaser's Annalen, Collection Walter Schepperle

Some prototype photos show the four cranes in use without the attached chimney, which presumably was not a problem if there was enough wind to light the fire. The boom was placed on the bearing block of a protection wagon during the approach, so that it could not swing out. It, created from a stake wagon, and the crane wagon together, had a length over buffers of 22.6 m.

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www.trainini.eu



If the team had to be transported over longer distances, it could also be put on goods trains, as it was allowed to travel at a cruising speed of 80 km/h. However, the cranes belonged to the railway service cars and consequently did not serve public transport.

The maximum permissible speed of the 57-ton steam crane represented the maximum among all rail cranes and made it a frequently requested tool on the tracks of the Bundesbahn. They were also frequently on their way to the site of operation as a special trip.

Its steam engine may seem small in size, but its power was actually only required for the strokes. When the load was hanging on the hook, its power was not required; only the mechanics of the crane kept the load suspended. Thus, the 27-ton crane was content with a twin steam engine of 75 hp, which had an upright heating tube boiler in the rear part of the crane house.

## Steam crane in operation

About two hours before the operation, it had to be heated up to reach the boiler pressure required for the work. If operations were requested at short notice, these preparations were already done during the journey (without chimney top). The set-up time on site was about 25 to 30 minutes.

Four men together made up the regular crew that was on duty here. These were always trained and experienced personnel from the respective home service depot of a crane. This was necessary to ensure the required routine and also to cope with difficult operational situations.

**Helpful reading on the great role models:**  
The exciting subject of the various service goods and railway service cars is to be systematically dealt with in a new series of books by Stefan Carstens. Volume 1 is presented in this issue. One of the planned follow-up volumes will also be dedicated to the various rail cranes and will fill this extremely productive complex of topics comprehensively and with background information helpful to model railway enthusiasts. Until then, the EK photo volume by Udo Kandler presented in **Trainini®** 3/2022 provides some prototype impressions and basic framework information.

These included over height curves (up to 30 mm can be easily compensated for), confined spaces, or the obstructions on electrified lines (switching off and earthing the overhead line). The weights to be lifted had to be calculated in advance in order to be able to request the appropriate crane.

The crane was used in accordance with the DV 933 lifting regulations (DV = Dienstvorschrift; Service Regulation), to which the head of the crane unit was responsible for adhering. If the requested activities were not compatible with these, he had to refuse the assignment.

If an operation in accordance with the rules was to be expected, the transport to the immediate place of operation proceeded as follows: First, the escort wagons, except for the protection wagon, were separated from the crane train and parked. The necessary accessories such as tools, crane ropes and support timbers made of sleepers were placed on the protection wagon.

The work train locomotive then pushed the crane and protection wagon to the place of use, where, after lifting the boom, the protection wagon was then also uncoupled and pulled off again. Now the upper carriage could turn 180° and thus take up its working position.

If the loads to be lifted required full support, the swivel arms were swung out and underlaid with up to eight layers of construction sleepers. In the first layer, they were laid close together and occupied an area of about 2.60 x 2.60 metres.

Up to the top edge of the sleepers, they were then stacked as a cross pile and, if necessary, levelled with so-called support timbers up to the stabilisers. Spindles on the swivelling supports, with which the punches were moved, allowed fine adjustment.

If the crane crews encountered soil with insufficient load-bearing capacity, it had to be compacted before stacking could begin. It was important that the working crane was exactly horizontal and could not sink into the ground.



Another time we see the Ardelt 57 t steam crane "München 6664" on 13 July 1968 at Gaildorf West station. It still has to be heated up before it can be used, but the break in the station gave us the opportunity to look closely at its undercarriage and to study the signs with the operating instructions on the superstructure. The handbrake wheel and the tilt scale at the side of the crane cab, which we will also put down on the model, are also clearly visible. Photo: Wolf Schneider, Slg. Eisenbahnstiftung

Anyone who has looked at prototype photos of rail cranes for a longer time will have noticed that the tender, which is usually carried along, cannot be seen at all or only at a considerable distance from the crane. The transport path for the coal is recognisably too long for constant supply, nor is a hose connection with the water tank to be seen.

The reason for this is the relatively small steam engine, which also proved to be very frugal due to its small dimensions. During an operation, it was usually simply not necessary to supply the steam crane.

Only when the crane was in operation for an above-average length of time or had to make an exceptionally large number of strokes, its consumption increased to such an extent that the water supply had to be replenished in between.

Then it was also quite possible that the grate had to be cleaned and fresh coal had to be laid on. For such cases, the tender was at least ready nearby. From our point of view, this opens up a welcome opportunity not to reproduce the tender 2'2" T 34 supplied by NoBa-Modelle filled to the brim with coal, but to make its coal chute visible and to reproduce it only with a very manageable fuel residue.



## Long awaited start of construction

Without any exaggeration, the creation of the 57-ton steam crane from Ardelt, in 1:220 scale by NoBa-Modelle, can be described as a personal project of the heart. As in the case with many other model railway enthusiasts, a rail crane unfolds very special charms, when in use.

It is eye-catching, arouses interest and is simply an attention getter that catches the eye even at a fleeting glance. This is the only way we can explain the extraordinary success of the small-series model, which will (hopefully) further fuel this report.



**The parts for the crane and the protection trolley are delivered separately according to assembly groups: One bag each contains the parts for the superstructure and undercarriage, movable attachments (lintels and deflection pulleys), the crane protection trolley and decals. This ensures sufficient overview.**

In unison, all manufacturers report in personal conversations that almost all types of construction and auxiliary trains promise success as models. If we are honest, this does not surprise us. After all, they stand out clearly and extremely pleasantly from the rest of the freight traffic, both in terms of colour and appearance. It is astonishing how long relevant types have been neglected by all manufacturers across all scales!

Fortunately, at least NoBa-Modelle has recognised this gap and responded appropriately. To build the crane train, the crane and protection wagon (item no. 5319R) and the tender 2'2' T 34 with equipment wagon G 10 Kassel (5323R) are required. The only thing missing is the staff car, but this can easily be provided from the Märklin range in the form of a chrome oxide green "Blunderbuss" ("Donnerbüchse") car.

The two kits consist of many grey resin printed parts, brass blocks, pins, metal round profiles, screws, nuts, neodymium magnets, axles, enamelled copper wire and water-sliding decals. Already at first glance it becomes clear that there is a lot of work waiting here, because the crane is designed to be just as mobile as its large prototype.

The work starts with a bath of all printed parts in isopropyl alcohol, regardless of this pre-treatment by the manufacturer. We also brush all surfaces afterwards with an old toothbrush. We want to make sure that no material residues, chips or skin grease remain on the surfaces to be painted.



**Absolutely grease-free parts are important for the subsequent colour application. Therefore, we subject the residual prints to a separate bath in isopropyl alcohol.**

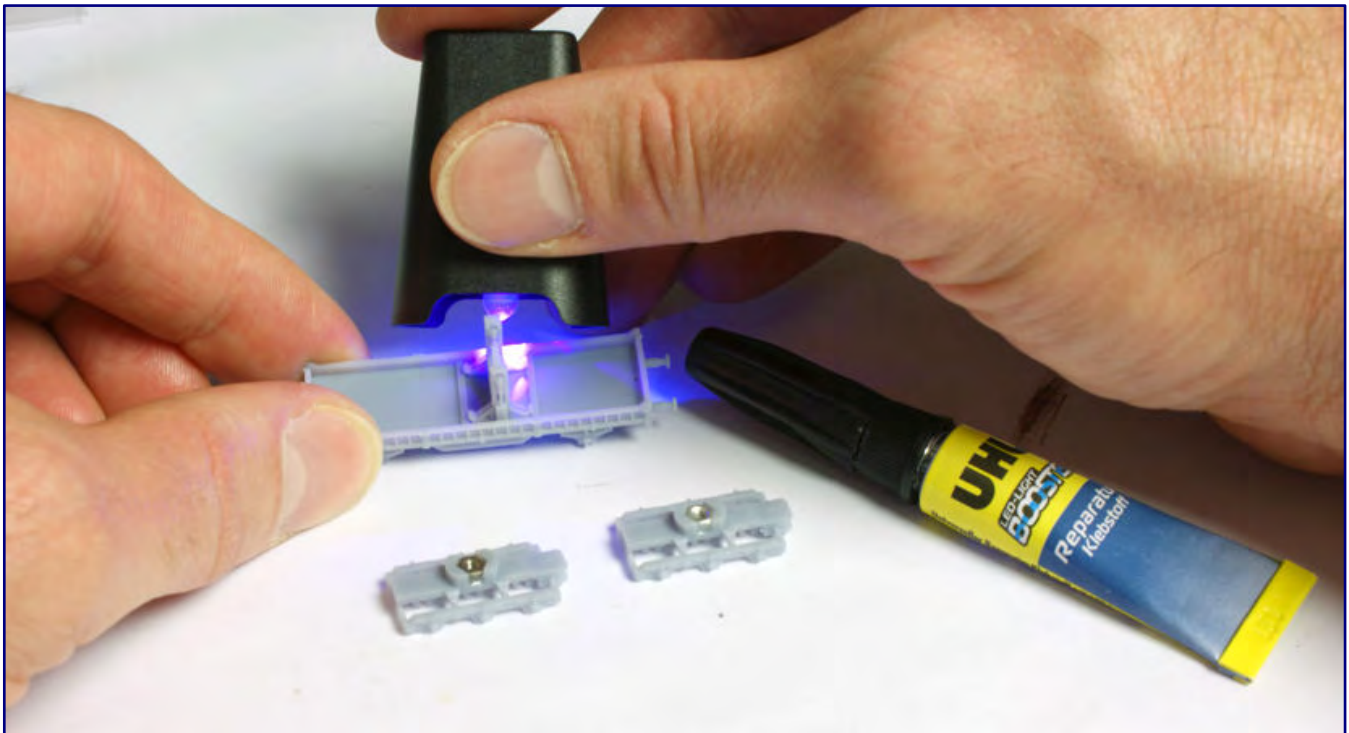
The first preparations can be made in the next step after drying. So, we glue the bearing block with Uhu LED-Light Booster onto the low side car and in the same way the M2 nuts into the bogies of the crane undercarriage and tender. The brass plates supplied are glued invisibly to the wagons, i.e., inside the superstructures and, in the case of the barrier wagon, to the running gear from below.

After that we can start the painting work, which seems to be easy because of the clearly defined areas, but which will be a challenge for us. For example, some masking work is needed to protect areas that have already been coloured with Vallejo masking tape (sold by Faller). Only in this way the roof of the equipment wagon can be set off in umber grey and the undercarriage of the protective wagon in deep black.

Contrary to Märklin's usual design, the body and the running gear consist of a single part. Masking off is not easy in this case because the platform gate and running gear have many structures.

Before applying the top colours, however, we almost forgot to prime all resin parts. For the tender and all undercarriages we used "Black Primer" from Badger (SNR-403) and for all other parts grey primer from Vallejo (74601; distributed by Faller). Both are based on water and polyurethane (PU), the light grey provides more luminosity and forms the best base for yellow paint application.





All necessary bonding is preferably done with the LED Light Booster from Uhu (photo above) because it hardens within five seconds of activation using ultraviolet light and provides the best adhesive strength of all adhesives tested, so far. Before applying the top coats, all resin parts are primed (photo below). Except for the later black parts, a light grey primer is shown throughout.

This was done with the spray gun during priming as well as the subsequent top coat in order to achieve a thin but covering application that does not cover the fine engravings. The water-based paints for this step also came from Oesling Modellbau. The following colours are required for the construction of the Ardel crane and the additional wagons:

| <u>Art.-Nr.</u> | <u>Colour number / designation</u>       | <u>Application area</u>                       |
|-----------------|--|---|
| 81001004        | RAL 1004 Golden yellow satin finish      | Crane hook and auxiliary hooks                |
| 81003000        | RAL 3000 Fire red satin finish           | Handbrake wheels on crane undercarriage       |
| 81006020        | RAL 6020 Chrome oxide green satin finish | Crane superstructure, jib, and car bodies     |
| 81007022        | RAL 7022 Umbra grey satin finish         | Roof of equipment car (base G 10)             |
| 81009002        | RAL 9002 Grey white satin finish         | Tilt scale on crane house and tender lanterns |
| 81009005        | RAL 9005 Deep black satin finish         | Chassis parts, crane undercarriage and tender |

The spray-painting is the step that visibly conveys the later work of the crane to the viewer, because more and more the kit parts now take on their prototype appearance and also reveal their details.



In the meantime, all parts have received their basic colour according to the layout and have been laid out here in order to be able to understand their position on the rail crane. The chimney top has been glued lying behind the crane house, which is also already assembled from a total of four parts (base with slewing ring, superstructure, chimney insert and openable roof), before the paint applications.

Brush and pen work is only required on the tender lanterns (reflector insert), the tilt scale on the sides of the crane cab, and on the handbrake wheel of the crane undercarriage. At the very end we will put a few final dabs on selected correction points with the help of a micro brush from the dental field.

### Personal refinements

We are not completely chronological in our descriptions, because we always remember smaller steps that were completed long before the spray gun was used. For example, there is the chimney top that we stuck into the holder above the counterweight.

This was still guided by the original idea of being able to transport the crane on the layout in the train formation, in any case, because the movements of the bogies and the boom in the bearing block certainly catch the eye. Similarly, a crane construction site also has a special attraction.



An error in our thinking led to the temporary assumption that the jib could still be raised at any time. However, in contrast to the very simplified rope guidance of the small Märklin switch crane, the carrying ropes of the jib and those of the hooks are separate. Since they cannot be rolled up, they would then hang slack.



For the upcoming assembly steps, the metal pins are assigned to the individual parts (of the boom) according to length. The jib supports and punches are inserted and connected with the help of the pins, because the crane is reproduced in working position.

The reproduction in the upright operating condition resulted from the fact that a decision had already been made in favour of mounted supports on the swing-out stabilisers – during the journey they were removed from the prototype and were therefore also printed as separate parts.

This is because they hinder the swinging out of the bogies and cornering when mounted on the stabilisers. In order to comply with the clearance gauge, the stabilisers must be folded onto the undercarriage during transport.

So, it was clear by now at the latest that our crane would be shown in action. NoBa-Modelle has reproduced the supports on swivels; they are attached separately and held by a pin to be shortened, which is inserted from above and glued to the undercarriage.

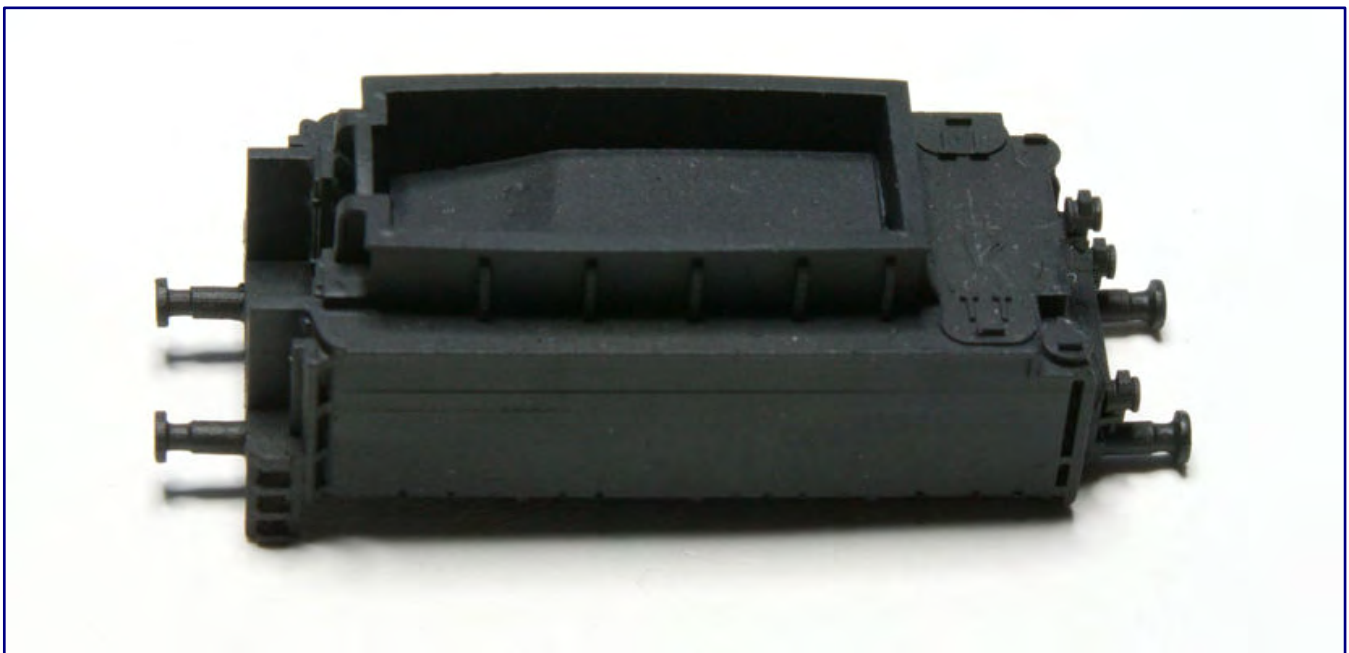
This is helpful when the crane is placed in changing locations and aligned according to the terrain structure. However, this method of construction does not help when building in transport position, because the stamps are then to be left out anyway and loaded separately. The pins are then shortened accordingly. But what does this mean for our project?

The decision was quickly made to simply purchase a second model: NoBa-Modelle finally makes it easy for its customers by offering the crane hoist disassembled into two components. Thus, only one additional crane and protection wagon is needed, the other two can also be used together with the crane in transport position.

This puts us in a uniquely comfortable position: exchanging the crane truck keeps us flexible and ensures double the fun, and it would also be possible to take up an operation with two cranes in the model: one of the two cranes would then be ready for use while the other is just being pushed to the site. Ingenious!

In the meantime, our construction report is limited to the first model we bought. We continue with the attachment of the opening roof, the boom and the pulley unit at its upper end. The manufacturer has included metal pins of different lengths for these. The position of each one is quickly clarified when a test fit is carried out.

This also indicates that, if necessary, the hole should be widened again slightly with a pin block and HSS twist drill if the paint application inhibits the feed. NoBa-Modelle itself recommends drilling out all holes with a 0.8 mm bit before assembly.



The brass weight has disappeared under a black PS plate according to my own idea, which makes it possible to reproduce the slope of the coal chute in the front area. This makes it easier to reproduce a cone of repose when the real coal is placed on top.

After all, it should move smoothly so that the printed parts are not damaged. Once everything is in place, a drop of Uhu's UV-curing adhesive helps to secure the pin against slippage and falling out.

Up to this point, the construction corresponds to what the manufacturer intended. For the tender, which carries supplies of coal and water for the steam crane, the freestyle takes the form of an individual approach. That is why we do not simply fill the brass block, which is glued into the coal container to weigh it down, with coal.

There are several reasons for this: On the one hand, it would need reworking here anyway to make a cone of repose recognisable. Secondly, we don't like the structure and colour of the coal supplied, which NoBa models obtained from the Faller range. The brass block, correctly positioned, proves to be the perfect support surface for the coal chute inside the tender.

So, we measure the interior and cut out a suitable strip from a black Evergreen sheet (9113; thickness 0.5 mm). We scribe it in the direction of the coal outlet so that we can make the last part of the slide steeper. As soon as the fitting test gives the expected result, the self-made part is glued in place.



## Construction progresses

Now is the right time to apply the decals on the individual models. The superstructure of the equipment wagon, the tender box in the central position, the chassis of the protection wagon and the crane undercarriage are given white company markings.



The crane protection wagon and tender (photo above) will be provided with white company inscriptions by means of (water sliding) decals. Two address plates and a coat of arms are to be applied to each of the crane superstructures (photo below). To ensure success, adhesive primer (Mr. Mark Setter) and softener (Mr. Mark Softer) are also used.

On both sides of the crane superstructure, white fields with black company inscriptions (for operation and load capacity) as well as the Ludwigshafen city coat of arms are applied. They are all cut out as accurately as possible, exactly along the print edges, and applied according to the same scheme.

The surface on the model is prepared with an adhesive primer, the decal is soaked in a water bath for about ten seconds, and placed on the lid of the jar. Here we let a drop of softener soak in to make the decal supple.

The transfer to the model is then done with the help of tweezers and a soft hair brush, which can also be used to correct the position of the decal. If it does not float sufficiently, a drop of softener will help. When it is finally in position, a cotton swab placed at the edge removes excess liquid, and the brush can be used to smooth out the image towards the edges.

Sufficient drying time must be allowed each time, especially if several fonts are to be applied to one side or if it is the turn of the opposite side and the model has to be placed on the possibly still damp side. Then, there is always the risk of unwanted changes in position or damage to the print image due to the substrate.



The number inscription (barely visible in the photo under the guide block) is sealed with Bergswerk clear varnish. The Vallejo masking tape is used again to protect the parts that are not to be sprayed.

To prevent this from happening permanently, we spray on a protective lacquer. For this purpose, we use a silk-matt clear acrylic lacquer from Bergswerk (83213). If desired, the deep matt finish can also be chosen, always following personal taste. It is only important that the base colour is silk matt or even glossy so that the wet sliders can bond with the base.

The next step is the tender. At the coal extraction point, plug-in boards are to be used, because NoBa-Modelle has reproduced their holding guides perfectly without reproducing the boards themselves. So that the coal cannot fall out freely here, the timbers must not be missing.





The pegboards at the coal extraction are made from the remainder of a wooden arch left over from the construction of a building. For gluing the material to plastic / synthetic resin, Pattex Special, which has been in the Henkel range for some time, proves to be perfect.

They look best when they are also made from the original material in the model – this was certainly the reason for the manufacturer's decision. A suitably thin layer, from which they can be separated with the help of a cutting ruler and a Mozart knife, is provided by the remnants of an architectural kit that contained wooden parts as well as hard cardboard.

With the help of the calliper gauge, the width and required height of the three boards are quickly determined and cut almost as quickly. What is needed now is a waterproof glue that bonds just as well with the resin as with the wood.

That's where Pattex Special, an adhesive for model making, comes in handy. Thanks to the dispensing needle, a tiny drop can be applied precisely to the glue spot. The three pegboards are glued in no time. Now fine coal is added. As we said at the beginning, only a very small amount should be visible in the tender, which will still be enough for the rest of the day's operation with the small boiler of the steam crane.

The finely and to scale grained material we have chosen comes from the Belgian supplier Jeweha Modelbouw (0400.5) and is made of real hard coal. Therefore, it shows matching fracture edges and different degrees of gloss, which are determined in each case by the position and direction of the fossils.

This extra-fine stone charcoal is sprinkled generously into an adhesive layer of Uhu Kraft Alleskleber (all-purpose glue). Light pressure with a finger or the stump of a brush handle ensures perfect adhesion in the adhesive. It is important that its traces are nowhere to be seen after drying.

Adhesive threads are therefore carefully removed with tweezers, the coal is poured off and reapplied in places and pressed on if there are still traces. With this conscientious, but certainly not laborious procedure, we achieve a realistic appearance that will also give our crane train a very realistic appearance.



We have always found the most suitable coal for the scale 1:220 in the product range of Jeweha Modelbouw. It is also used here and finds sufficient hold in the tender thanks to Uhu Kraft Alleskleber.

There is still a preparatory step to be taken on the crane superstructure, because otherwise access through the jib will be blocked in the subsequent construction steps: The glass reproductions in the windows of the room for the steam engine and rope pulleys, but above all of the crane operator's cabin, are still missing.

They are a case for the Micro Kristal Klear (MI-9) from Microscale. An extremely fine brush, dot glue tools and the drawing pen are required here to hit the sometimes very tiny openings safely. The adhesive varnish is always placed in one corner and then applied as a film in an oblique direction to the opposite corner so that it wets all four sides of the frame.

After drying, the originally white liquid is solid, transparent and shows a slight glass sheen. The lenticulation that can often be observed with such products is also only very slight here and is completely negligible.

## Sisyphus work on the boom

The construction of our crane and its accompanying wagons has now entered a decisive phase. Enthusiastic about the photos of the prototype, which are particularly impressive because of the extensive rope work, there is no way around transferring this to the model, as much as possible.





As here when setting down the tilt scale on the driver's cab, Standard graph drawing nibs and nib holders are also used when glazing the cab windows. They prove to be valuable tools for every finest and precise colour application!

In the tiny space that remains for working, it is probably not possible to pull in all the ropes and it will have to remain with a compromise. Nevertheless, the overall picture should be reproduced, which conveys the impression of large masses and heavy loads on load-bearing steel cables to the viewer.

This then also means increasing the effort at this point compared to the manufacturer's finished model. It is a benefit that a kit inevitably brings with it, because it leaves room for own ideas. In this case, the work involved takes a back seat because it represents a fun factor, and is not to be paid for by the customer – the present model would then also explode in price, which must not be concealed.

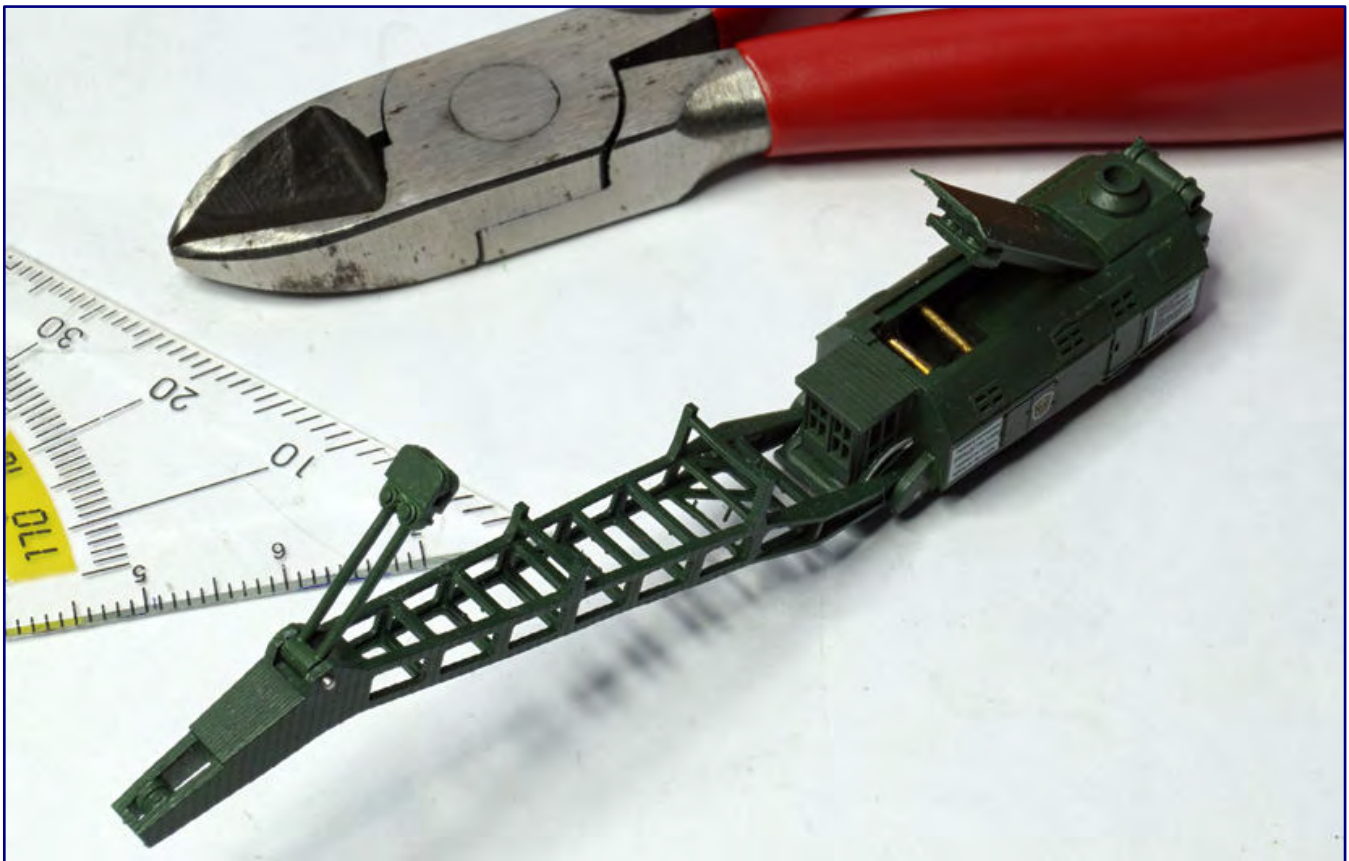
In order to be able to realise one's own ideas, additional installations are required inside the crane house: If the main and auxiliary hooks are not only attached to the jib, but their rope replicas are guided in its "machine house" in a prototypical manner, an attachment possibility is required there, i.e., replicas of the prototypical rope pulleys.

These must be sufficiently stable so that they do not break during later tensioning, must be securely fastened so that the glued joints do not represent the decisive weak point and at the same time must also be able to be used through the narrow roof opening. The material of choice for both reel replicas are therefore brass round rods from the model building accessories.

The calliper can be used to determine the width of the opening, to which sufficient mark-up is then added for the gluing points. The first profile, cut off with a suitable side cutter, is used to test the fit: can it be passed lengthwise through the roof opening, turned by 90° and placed in the position determined by the prototype drawing?

If all three questions can be safely answered with yes, the piece serves as a template for the second roll. With the help of a toothpick tip, we now place a drop of the UV glue (Uhu) at the front of the two positions, which has already proven its usefulness. Then the first roll is inserted as explained and placed in the prepared bed of glue.

The adhesion force is sufficiently high so that it does not fall off. This gives us time to put the tweezers aside and grab the UV LED unit to exposed both glue spots to a blue light glow, which activates the glue and cures it within five seconds. Next, attach the second roll in the same way, which now requires a little more dexterity as the space to turn the profile piece has now become smaller.



We place our rope pulley replicas at about the same height as the prototype drawing. They are held securely in place by UV adhesive, which has proven to be enormously resistant to both tension and pressure.

The metallic sheen of the pulleys does not bother us for the time being, this is remedied at the very end. Instead, the steel rope replicas from the crane house to the jib and up to the crane hooks now demand full concentration.

A total of four “ropes”, which are still considerably fewer than on the prototype, are to carry the boom and two lead to each of the two hooks. With eight replicas we come sufficiently close to the prototype impression. However, we do not use the enamelled copper wire supplied by NoBa-Modelle, but rather twisted thread.

The wire is more rigid and stable than the thread, but it will immediately reveal unavoidable kinks in macro shots and interfere with the prototypical effect. A thread, on the other hand, is easier to tension, but must then, subsequently, solidify in its final position.



The thought process is triggered by the fact that four of them have to pass through the narrow head of the boom end, in which the pulleys are reproduced. Due to the curve, these cannot simply be drilled out and widened in this way.

Guiding aids and even two auxiliary holes now become necessary. The thread can only be guided with a curved surgeon's needle. A classic darning needle from grandmother's sewing box is much too large in diameter. The only thing that helps here is to ask a doctor you know, because medical equipment is expensive and, in this case, cannot be ordered individually.



The elaborate retraction of the ropes, simulated by black twisted thread, into the crane boom is done with the help of surgeon's needles and enamelled copper wire as a guide. Not shown in this photo is the microsurgery tool that is crucial at the end. The auxiliary hole described in the text can be seen at the top of the angle of the boom.

However, the first attempt is not without setbacks. While we manage quite well in the area of the crane superstructure and manage the 360° turns with the help of clamping tweezers and guide wire, we initially fail in the upper area of the jib, which is angled.

There is about 270° to go around here. The surgical needle is too thick to be passed through the narrow channel. Even an auxiliary hole on the top of the boom, which splits the total angle into two approximately equal partial angles, does not yet lead to success.

This only came about when we were given a much smaller needle with an identical curvature from the field of microsurgery. Many thanks at this point to the doctor we trust, who does not wish to be mentioned by name here!

Now a straight surgeon's needle guides the twine through the boom mast, where the fine tool takes over. With the help of the hole, the project succeeds, the loop protruding at the top can be pulled back in at the end, close the hole with the LED Light Booster from Uhu, because it can also fill gaps.

Before this, however, the ropes must be passed through the pulleys at the roof opening and the opposite end of the cantilever, tensioned and knotted in a defined length that is always the same. The glued joints are permanently secured with the UV adhesive against unintentional loosening. Only now does the apparent chaos of many thread ends begin to clear.



At the very end, all the ropes are brought to tension, starting with the support ropes of the boom, and knotted. Loose ends can be cut off, the knots secured with a drop of glue. As soon as the threads are also hardened, they seem to hang down tightly like steel cables and the crane hooks also remain permanently in the desired position.

After that, the final length of the ropes for the hooks can also be determined, fixed with knots and secured in the same way. Overhanging thread ends are cut off before the glue is applied and brought to invisible places by pulling. At the main hook (double hook) a horizontal hole through the pulley is also needed in the course of the process, because obviously paint or hardened resin.

The conclusion of this most difficult construction step is the wetting of all ropes with a liquid superglue ("Uhu superglue precision"), partly spread with a toothpick. When put under tension, the glue hardens quickly and leaves the desired image without slack ropes. Colour corrections by small dabs on the auxiliary holes make them disappear for good.



## Final assembly and completion

What can be described here in comparatively few words drags on over several days during the construction of the crane. The parallel work on a video report for **Trainini TV** which is to follow in September, shows the dilemma again and again: extremely limited space where precise work is required. The targeted illumination of the “construction sites” also proves to be difficult again and again.

In addition, there is extreme concentration and quietness when working, which results in high body tension and requires additional rest breaks. Accepting this and taking it to heart is a key to success. A good result cannot be forced.

But once the crane superstructure with its ropes and golden hooks is in front of us, the sight compensates for all the efforts that have been necessary up to this point. The remaining activities prove to be manageable and much easier to accomplish.



The screws of the bogies engage from above through the undercarriage into the glued-in nuts. The undercarriage floor, on the other hand, is screwed from below with a hexagon socket screw in the melted-in sleeve of the tender body

Now the wheel sets are pressed in, the couplings are inserted and the superstructures are bolted to the running gears. The individual wagons are connected to each other with magnetic couplings and only have the classic “lobster claw” on the outside (of the equipment and protection wagons). This must be taken into account with regard to the different coupling shafts, but also with regard to the polarity of the magnets.

The four carriages are to be mounted in the desired order and position and then fitted with the neodymium magnets, taking into account the polarity. Glue them in with Uhu-Alleskleber Super. As two couplings are glued at the same time, the two carriages are not separated until full adhesive strength is guaranteed.



NoBa-Modelle advises to first screw one bogie into the undercarriage, then to mount the upper carriage and only then the second bogie. If, as shown here, the superstructure is bolted first, this is also possible: if the boom is turned 90°, it gives access to both bogie bolts.



There is a small hint for the assembly of chassis and superstructure on the crane and on the tender. Since both vehicles have bogies and these make access to the central housing screw more difficult, a targeted approach is required here.

On the tender, the bogies can be turned 90° to gain sufficient access. However, we should bear one thing in mind: the front bogie is restricted in its rotation by the access ladder on the right and left and should not damage the painted surfaces.

For the crane undercarriage, the manufacturer specifies a sequence in its assembly instructions. It says: "Connect bogies and chassis with countersunk screws. Before mounting the 2nd bogie, insert the Allen screw from below into the chassis, then mount the 2nd bogie".



As a result of all the work described, here we have the finished crane train from two kits. If you prefer to get the equipment wagon and tender from a Märklin model, you can stick with the kit for the two vehicles on the right. However, at least one Märklin vehicle would then have to be converted to the magnetic coupling of the small series manufacturer.

The screw holes for the bogies are easily accessible because the superstructure can be turned without any problems and thus both remain easily accessible when superstructure and bogie are connected. Because of this visibility, the two screw heads should also be covered with a brush coat of black primer so that they do not enter the picture in a disturbing way.

The same applies to our pulleys, which can best be reached with the micro brush as a substitute for a brush without smearing parts of the superstructure unintentionally. Equipped like this, it's time for the final photo shoot: A Kö 1 has derailed during the switch run and has to be lifted back onto the track.

The standing model, which the model crane can also lift without any problems, comes from Z-Doktor Modellbau (ZD-220-01001-1). When instructing the crane operator and subsequently guiding the wheel flanges when setting down the hanging locomotive, the "workers in protective clothing" from Preiser (88537) help.

Our crane with 57 tonnes lifting capacity is certainly not over dimensioned here, because it has to work with a strong outreach, at times, and, thus, loses a considerable part of its lifting capacity. It doesn't always have to be a bridge construction site, because we don't yet have a second crane that could come to the rescue.



The Ardelt 57-to steam crane in action: prototypically, the stabilisers of the outrigger supports rest on two layers of construction sleepers to recover the derailed Kö 1 and put it back on the rails. This prototypical scene was created with a locomotive model from Z-Doktor Modellbau and Preiser figures. It proves how impressive a crane looks on the layout, especially when it is shown in action.

**Model manufacturer:**

<https://www.noba-modelle.de>

**Materials and tools used:**

<https://www.bergswerk.de>

<https://www.faller.de>

<https://www.fohrmann.com>

<https://www.oesling-modellbau.com>

<https://www.pattex.de>

<http://www.peter-post-werkzeuge.de>

<https://www.standardgraph.com>

<https://www.uhu.de>

**Accessories in the scene photos:**

<https://www.preiserfiguren.de>

<https://www.z-doktor.de>

**Pages with prototype information:**

<https://www.eisenbahndienstfahrzeuge.de>

<http://www.wehrtalbahn.de>



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## Welcome to Werdersheim

*16 years of planning and building, but much more Mini-Club experience, have gone into Dirk Rohwerder's large, private, Z scale layout. It is unprecedented in its basic dimensions, control system, the attention to detail and the craftsmanship that has gone into it. Dirk Kuhlmann reports for us on a true masterpiece by his namesake – an engineer by profession, and nothing is too hard for an engineer, as we all know.*

Welcome to Werdersheim, a small town in the Bergisches Land region of Germany. Not far from Wuppertal, the town nestles in a green landscape and is a home to happy citizens. We are in the early autumn of 1960 and are now taking a leisurly stroll through its busy streets.

The postwar economic boom has also arrived here and is clearly noticeable; business is good and many people can afford their first motor vehicle. The triangle of Remscheid-Solingen-Wuppertal is a good place to live, especially since the textile industry and the stell blade manufacturers guarantee a secure livelihood.



The access road to the DB depot is somewhat hidden. This is where the tender locomotives and the auxiliary train are usually parked.

Many workers are already eying the latest TV sets in the local electronics shop, with the newest Kuba Imperial model being eagerly awaited. The cinema at the end of town is, of course, screening the film of the year: "La dolce vita," which means "the sweet life".

If we continue downhill along the main road, we reach Werdersheim station, which is a little out of the way from the village. The black steamers still dominate the scene and especially on this important main line they offer the railway enthusiast a varied picture. Keen observers of depots should know that the best place to watch the operations is in the small-town park opposite the hotel "Zur Post".





At lunchtime, the roundhouse is only partly occupied and business is largely quiet. In 1960, most of the shops in the suburb also only reopen at 3 p.m. after a lunch break.

Have we sparked your interest? Do you need a map of how to get there? I'm sorry to disappoint you, the village of Werdersheim is purely fictional, but at least it is a model of the typical life and hustle and bustle of the Bergisches Land.

## Track plan, substructure, and track laying

Around 2007, Dirk Rohwerder, the owner of this four-metre-long layout, started planning his project together with a good friend. Experiences from earlier projects, whether positive or negative, were immediately incorporated here.



The mighty-looking 03 1001 from Hagen-Eckesey, which is not far away, will soon leave the depot again and take over a D-train in the direction of its hometown.

The theme was to consist of a small suburban station with two main and three passing sidings, a level crossing, a goods dispatch depot, and a small maintenance depot – quite a normal and prototypical setting at that time.

In the concealed area, a 14-track staging yard on a level approximately 90 cm below the scenery would allow the rolling stock to travel over a double-track helix to a higher level (main plate at the 105 cm level).



The large staging yard is located at a height of about 70 cm behind the actual layout and is connected to it via a double-track spiral. The rolling stock waiting here to run leaves nothing to be desired.

In a junction it is then decided whether the train enters the station from the right or the left. The areas around the maintenance and freight handling depots should be manually operated, but the majority of the tracks should be equipped with an automatic system.

The choice fell on the MpC control system from Gahler & Ringstmeier. With this, the existing and future rolling stock should be immediately usable without a digital conversion.

Work on the layout began in 2010 with the construction of angle irons and anchors in the wall. The very distinctive helix is constructed from laser-cut, bolted aluminium quarter sheets and threaded rods.

This makes it very torsionally stiff and even minimally adjustable in clearance height. The centre of the track guide in the circular arc had to remain open and accessible and thus leaves enough room for cleaning the tracks.



With the laying of rails in the later concealed area, the construction and installation of the Gahler & Ringstmeier components began at the same time. The circuit boards and their power supply are located in a self-built 19-inch electrical cabinet.

A PC set up for this control programme is the heart of all processes; its monitor shows the status of all train runs. An integrated track diagram also allows each train to be specifically selected and controlled with a mouse click.

Three years after starting to lay these technical foundations, Dirk Rohwerder finally reached the top level: from here on we are in the visible area of the layout. And here, too, the ambitions for the landscaping were at least as high as they were for the technical aspects of the layout.

For the track work in the hidden part of the layout, the use of Märklin track was justified for economic and operational safety reasons. However, the ambition was to be much less compromising in view of achieving as much realism as possible with the track, points, and wide sweeping curves in the visible area, and, therefore, required a different type of material.

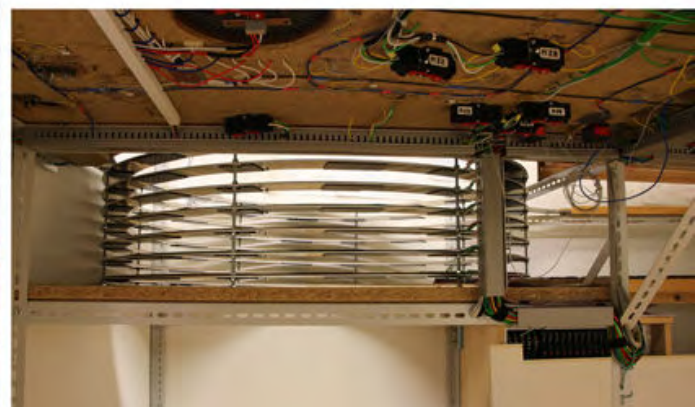
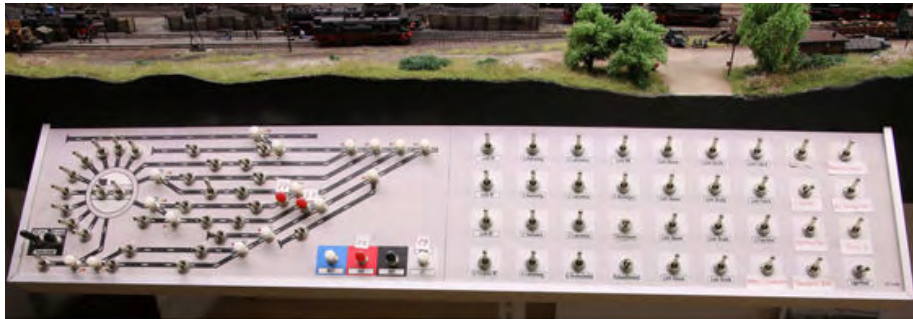


With a slightly backlight shot, the filigree König track can be seen very well. Paired with the 1:9 turnouts, it is the best there is in 1:220 scale.

So the owner swiftly decided to go with the König (“Weichenlaterne”) system with its wonderful Code 40 rails and 1:9 turnouts, which strictly follow the German Federal Railway prototype. Due to the fact that it is produced manually, Dirk Rohwerder had to allow for a sufficient waiting time for the fabrication and delivery of the track.

All sections of the tracks then had to be individually treated and soldered; track separations (0.15 mm) are so fine that a visually cohesive and coherent scene is created with standard points and original sleeper spacing.

All electrical connections are invisibly led downwards and all turnouts are equipped with an underfloor motor. With this, the builder achieved the highest degree of realism that is currently possible in Z scale.



Various views show main elements of the control system that is based on Gahler + Ringstmeier components: classic track diagram control panel (top left), plug-in card cabinet for the circuit boards (top right), computer for intervention and screen monitoring (bottom left) and the track helix (bottom right), via which all trains are sent onto the layout.

## Dirk & Dirk as a Duo

Dirk Rohwerder already had concrete ideas for the landscape he wanted to create, many of which drawing on his childhood memories. His aunt's house with a backyard, the ubiquitous Aral petrol station or the often visited railway crossing on the edge of the small town are just a few examples.

Knowing fully well that landscape design is not necessarily his forte and that mistakes from his previous layout builds should not be repeated, he sought experienced and proven help. Fortunately, he found a (completely crazy; editor's note) modeller in his circle of friends, namely the author of these lines.

I quickly agreed to join in on the project, especially since we do not live too far from each other and the "personal chemistry" for working together was right. Now, we had a lot of planning work ahead of us over the next few months: looking at photos, consulting specialist literature and making lots of sketches.

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**Photo above:**  
In the evening hours, there is still hustle and bustle at the station; many commuters do not want to miss their train home.

**Photo below:**  
Later in the evening, slowly but surely all the locomotives arrive back at their depot. The night shift takes care of the arriving engines, which includes keeping each steam locomotive at rest, ready to go back into service early in the morning, or to be kept in reserve.





The houses with scaffolding and façade advertising positioned at the front edge of the layout prove to be attention getters (top) and effectively frame the level crossing. Here, the proud owner transposed one (of several) childhood memories into the layout.

All the vehicles have been selected to match the year 1960, and the forest of signs also corresponds to the German road traffic specifications of that time (bottom). But many other details also enhance the harmonious effect. The tanker truck is just turning in at the Aral petrol station to supply it with more fuel.



We had to throw some of my namesake's wonderful ideas overboard, so that the later representation of his "perfect world" would not appear overloaded. To give our readers a better idea, it should be mentioned here that at that time there was nothing but a 4-metre long, flat board with very beautiful tracks in front of us.

A modern model railway always shows an "up and down" in its landscape. Incorporating a hill or a wall would be easily possible on the flat board, but putting holes into the base plate for deeper landscape parts was almost impossible because of the very extensive electrical components already installed or planned.

So we agreed on adding a small river in the foreground, because this required only some minimal carving of the 22 mm particle board. And in order to avoid an overly obvious appearance of a classic model railway, a topographically shaped front edge added to the layout. In this way, everything was ready to create the hoped-for scenery of the Bergisches Land.

In addition, the originally conceived layout depth of 120 cm was trimmed to a maximum of 70 cm on the left-hand side. The layout simply looks better this way and it ensures the accessibility of all its parts.

Unfortunately, this meant that the already completed staging yard behind the scene could not be integrated as originally foreseen. With increasing age, it becomes more and more difficult to slide down on one's knees. But wait, isn't there a small room next door? Who knows, maybe there is hope to find an alternative solution.



One half of the duo Dirk & Dirk: This one goes by the surname Kuhlmann and models on the later river before pandemic regulations required a forced break.

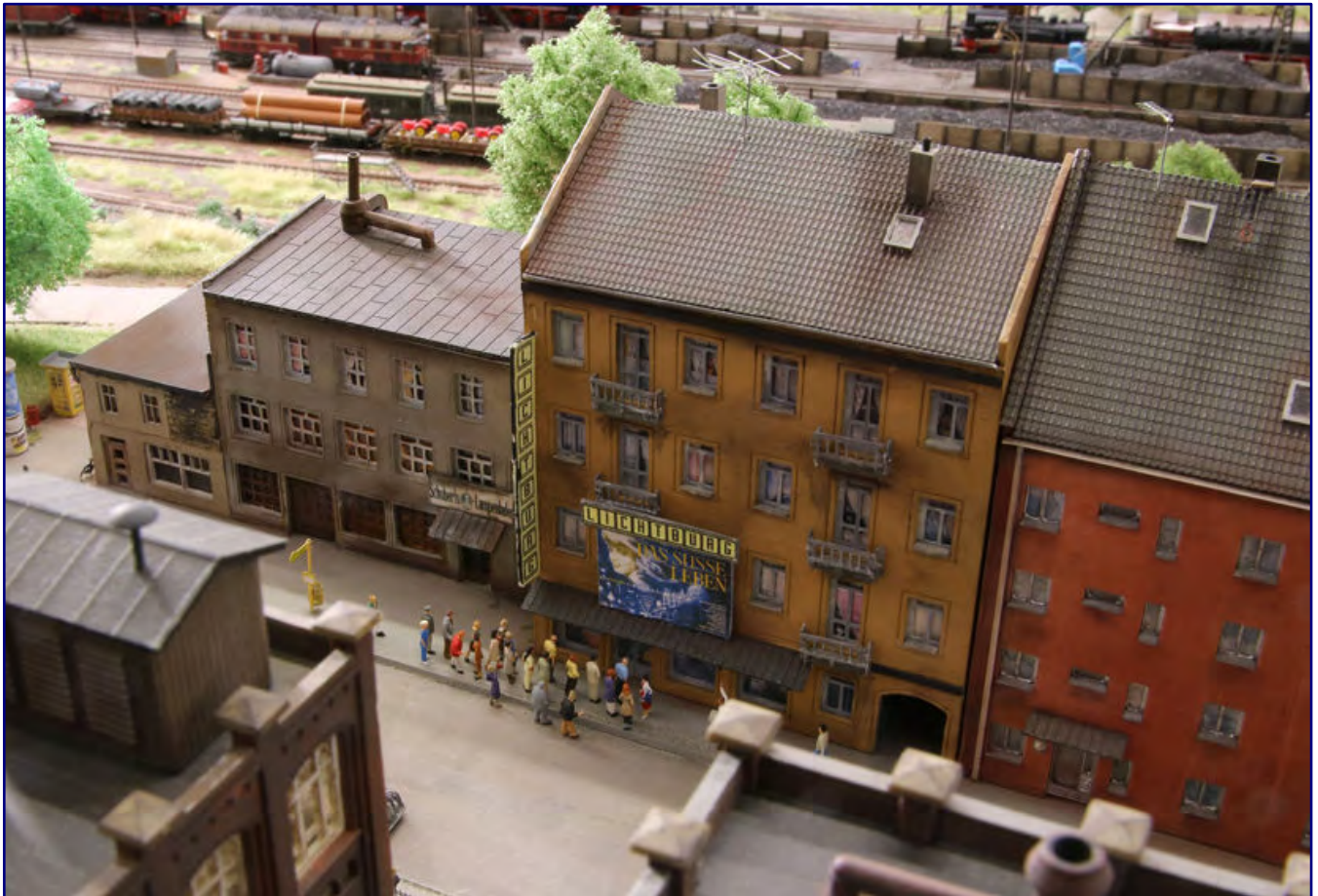
With a final plan agreed upon, we distributed our areas of responsibility. Dirk Rohwerder continued to lay the remaining track and perfected "Werdersheim" in terms of operational technology.

In the meantime, I built a large number of houses and other buildings in my small workshop, several pure landscape parts in their raw form and countless small parts. The complete suburb of "Werdersheim" had found its home arranged on a suitable separate base and could later be fitted exactly into the layout.

## Finishing touches to the landscape

The multi-storey townhouse models from Lütke Modellbahn come with a shop on the groundfloor. So, of course, I equipped them all with an interior and also installed interior lighting. To do this, I had to go through countless photos from the early sixties.

Small paper backdrops depicting interiors even create an illusion of some depth in the shops. I was even able to incorporate some backcourt atmosphere with many buildings. With only 1 mm high Evergreen strips, even the kerbs of the streets of Werdersheim are correctly reproduced.



Normal visitors to the layout will get these views; Schubert's lamp shop and the cinema can only be seen from the courtyard side. The movie of the year called "La Dolce Vita" is playing in the cinema. Werdersheim has detailing down to the last corner, which also includes the interiors of various shops.

The next major construction site then dealt with the depot. The roundhouse comes from Archistories and is, of course, also equipped with interior fittings. The coal bins, on the other hand, are completely scratch-built and faithful to prototype dimensions.

Fortunately, Dirk Rohwerder had accumulated many useful materials and small parts over the decades, which we could now use. This collector's mentality is typical for Z-scale enthusiasts: "Parts for Z gauge are bought when they are available, not when you need them."

Many products came or come from small series producers and are only available for a limited time. Over the years, a lot of "stuff" accumulates. And, so, the desired Aral petrol station could also be made very realistically from combining existing parts with a lot of modifications. Once the buildings and structures were finished, shaping the terrain was next in line.

To do this, we glued (more) layers of 20 mm thick rigid foam boards to the layout until the desired height was reached. With sharp, toothed knives and various rasps, the foam board was shaped in order to give the landscape its basic contours.





An effective detail from the large-scale construction site of the depot: the coal bins consist of tiny boards and double-T beams made of brass, which, with matching colours, make for a very authentic looking model.

Soon, retaining walls and other structures were installed in their intended places. Over time, my constant work on many layouts has given me both a relaxed attitude to model making and an eye for the essentials. There are no quick solutions for a layout of the high standard envisaged here.

For example, we all too often see modellers applying static grass and ground cover directly on top of a brown painted surface. However, this can be done much better, but admittedly requires a more deft technique! Armed with various putty spatulas, a rich coat of Molto wood repair filler was applied over the foam board terrain.

Although this material is not exactly inexpensive in the quantities processed here, the excellent result and the achievable properties for the final appearance justify this investment. The compound dries to a sand-like consistency and, with its rough surface, makes for a very good adhesive base for the subsequent application of static grass.

Afterwards, the terrain was painted with an earth-like colour to match the region and the desired weather conditions. An irregular application is quite desirable here, nature always provides us with the best template.

The last action is to sieve out the finest topsoil, which in this case was collected in Sprockhövel (near the target region), some stones, and various types of sand. The material is either scattered on the glued base or washed in with thin gravel glue.

After that, we jointly turned to weathering the tracks, followed by adding and glueing the ballast. And finally we were ready to move on from this watery and sticky phase of the build to the next steps.

So far, of course, the result looked rather bleak, almost like a desert. But this was soon to change for the positive. The subsequent technique of modelling of water with emulsion paints and boat paint has been known to many **Trainini®** readers for years.

If depth is feigned with a little illusion painting and everything is sealed at the end with a few layers of very high-gloss varnish, the impression is remarkable for such a small river when viewed from a right angle.



Werdersheim presents itself in the state shortly before adding vegetation. The plan can be seen in the background. The river in the foreground is still visibly marked by the Molto filler, which is the preferred material of our editorial team for modelling landscapes.

There is only one thing that should be taken into account: The layout must be covered and left to rest for a few days; the paint dries slowly during this time and needs to be protected from dust during this period.

Before the addition of first layers of vegetation, the layout is carefully vacuumed and any visible defects will be repaired before work continues.

Different, finely sieved types of turf from Woodland Scenic always form the first green base. The first layer of grass fibres (Mininatur, 2 mm) was then applied to a 20 x 20 cm section.

The grass glue was not applied over the whole surface, but dabbed on irregularly. This was done section by section, in order to maintain as much control over detail, as possible. Later locations of trees and bushes are to be excluded from the treatment.



After two more applications of grass, the ground cover reached its desired look. Dense, more or less tall grass, weeds, and lichens alternate with bare soil. I always prefer some length with the ground cover part of my layout builds, but it is definitely worth the effort.

Now, it was the turn of the trees to be built and there were hundreds! As always, they are only my own creations, made with the typical tools, such as wire and self-mixed “wood trunk paint.” The foliage is from Polak and/or Mininatur. In the end, it's always your own taste that decides.



Werdersheim is truly not a big city, in most cases it is still tranquil. Only the postman, who was later transferred to Westheim in Sauerland and became a civil servant, has to take the bend very rapidly in the usual manner.

There are, of course, excellent and specialised tree manufacturers, but I find that self-built trees tend to give more control for achieving overall visual harmony of a layout. This way everything comes from one source and it's also more fun!

One exception were five large oak trees which were sourced from Silhouette. When around 50 to 60 trees were finished, it was time to go back to the layout to “plant” them. This took several months, during which Dirk Rohwerder had already built more small parts and further developed his new passion for painting with an airbrush. Nothing stood now in the way of weathering the rolling stock.

### Second green phase and a forced break

Let's jump back a little in the last construction steps described: Before the prepared trees and bushes completed the scenery, small accessories and details were to be placed in exposed, and later, no longer easily accessible places.

This was true even if some objects (almost) disappeared into the forest or thicket in the future. The countless logs and timbers were also given their place. Here, of course, my handwriting came through (once) again and so a few old boards had to be placed against various house walls.

I don't see any direct sense in it, but it looks good and makes for real life scenes. Left and forgotten – see our contribution to the annual focus theme in the last issue of the magazine.



Just outside the city limits, one finds only forest and meadows. We experience a scenery that is rarely found anymore in modern times.

By the way, this time I also chose the fine “Naturex” material from Polak for the bushes. A small colour comparison on site was the deciding factor.

Unfortunately, our joint work was then abruptly interrupted in spring 2020: The Covid 19 pandemic took its long course. Our regular meetings were cancelled, remained very rare in the course of time and could only be carried out with greater caution.



This is not going well! At least the guitar case is about to end up on the track. The frozen suspense of the scene allows us a glimpse of two of the homemade semaphore signals.



At least, we realised that a large swathe of trees was still needed. We divided up the rest of the work and Dirk Rohwerder concentrated on further detailing his layout. For him, for example, the prototypical placement of signals is an important cherry on the cake of realism, but, in the end, he had no choice but to make the signals himself.



One of the famous F trains rushes through Werdersheim station in the direction of Cologne. It could be the F 24 "Schwabenpfeil", which runs from Dortmund to München (Munich) - but usually not with so many coaches. In the photo are again some specimens of the scratch-built signals.

The ready-made products available on the market did not meet his expectations. Using cast brass pieces from non-functional signals (Schmidt / Bahls) and by means of precision turned and milled parts, functional 8 meter form signals with a narrow mast were built.

The decisive factor was to be the scale, which also included the signal lanterns, so lighting was dispensed with in favour of appearance. Thanks to the "Miba-Report" special editions on signals (Volumes 17 and 18), he was able to determine the appropriate prototype dimensions and to successfully overcome also this hurdle in his quest for authenticity.

## Final work and review

Anyone who has been around Z gauge for a while will know that my namesake has been a known quantity for decades when it comes to locomotives and wagons. Modifications and super-detailing have accompanied him for half his life. There is no need for further words, because the pictures already say it all.

One could only guess that we are looking at a Z scale layout, if it weren't for the clunky couplers. Only the best is good enough! The last big step towards perfection started at the same time as the landscaping of this layout. After some hesitant first steps, the project also resulted in an extensive effort of weathering the rolling stock. (Editors note: The editor-in-chief of this magazine will not forget how Dirk Rohwerder came to him with small-series material to have a patina applied, and to learn how to use an air brush).

This enhanced the coherent overall picture of “Werdersheim” even more. By the way, the prototypes of most of the locomotives on the layout were based in the Wuppertal depot in 1960.



The class 50 locomotive is still being supplied with coal, while the BR 78 is moving out again, after maintenance. The coaling facilities are authentic, which is still a great challenge even in Z scale. Many parts are self-made.

Of course, a few strangers are also allowed to run here, but even 85 007 had its raison d'être here in 1961: For a short time, the former Höllental locomotive pushed heavy trains towards Wuppertal on the steep line in Erkrath. Actually, a second part of this report would be needed at this point, as the topic of locomotives and wagons alone is far too extensive to be dealt with conclusively here.

Werdersheim is a layout based on the basic principle of the “peep-box”, but with a stationary, non-transportable design. It is, therefore, also a classic home layout, but with a fixed operator perspective. The closed system shows only the intended scenes and also protects the layout.

A panoramic backdrop with typical scenes from the Bergisches Land creates a fine and very bright stage set. The realistic illumination is provided by several daylight spectrum LED strips, which can be switched to two brightness levels. It should also be noted that due to the many green and yellow tones in the landscape, a light colour of 4000 K is used to reduce the notorious colour cast on model railways.

From the early summer of 2022, we have ramped up the joint activity again. Many, many figures and other small details are now breathing real life into “Werdersheim”. They may only be trackside details, but, without these decorations, something would be missing, and the first visitors were particularly enthusiastic about the many little stories that are told right into the last corner of this layout.



Our cooperation worked out very well and shows once again that even model railroaders with different areas of interest can cooperate very well, and that the results speak for themselves in the end.

With a deliberate break of several months, I photographed the layout for the last time and let it have a new effect on me. It only took a few minutes, and I was immersed in the scenes. They created the emotions I had hoped for: This is what it must really have been like at the beginning of the sixties.



Our final photo shows two superlatives: the beautifully wide curves of the track with slender points and the effect of a perfectly matching scenic backdrop.

People lived and worked here, perhaps with a little more joie de vivre in their little free time, compared to the digitalised life of present times. Dirk Rohwerder and I were privileged to experience these times as small children, and they have influenced who we are. We've created a part of our perfect world.

**Suppliers mentioned in the report:**

<http://www.bahls-modelleisenbahnen.de>

<https://www.luetke-modellbahn.de>

<https://www.maerklin.de>

<https://www.mininatur.de>

<https://www.molto.de>

<http://www.mpc-modellbahnsteuerung.de>

<http://www.polakmodel.com>

<http://www.weichenlaterne.de>

<https://woodlandscenics.woodlandscenics.co>



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## Build your own structures (part 3) **Mathematics in Modelling**

*In the first two parts of a small continuing series, the construction of a locomotive control tower and a signal box were described. Now follows a water tower that is not characterised by straight wall and roof surfaces but has barrel and cone-shaped elements. Our readers can examine the result for themselves now.*

By Jochen Brüggemann. Since I have started building my current layout more than 20 years ago, I have been looking for a water tower kit for the planned railway depot modelled on the Ottbergen example: a round tower, built up on bricks, and crowned by a cylindrical sheet steel water tank with a conical roof.



Due to the lack of commercial offers, the only way to build a water tower based on the Ottbergen model was to build it completely from scratch. How this came about will be explained in detail in two parts. Photo: Jochen Brüggemann

The longer I looked and waited for a model, the more I thought about building it myself. Again, only the dining table came into question as a building site, and to avoid a break in style, the materials and tools used in parts 1 and 2 were to be used again to a large extent.

I wanted to make the brick parts of the tower out of polystyrene (PS) with a surface structure, the water tank, and the roof out of smooth PS panels. In addition, I still had to develop a practicable process for creating conical and barrel-shaped tower parts from flat PS wall die plates



End of shift at the Ottbergen depot on 15 May 1976: After the arrival of Ng 64444, 044 149-3 moved into the Ottbergen depot for restoration shortly after 18:00. On the right is 044 195-6 (ex 44 1202), on the left 052 262-3 of the Lehrte depot, which is waiting to be scrapped and was last used as a heating locomotive in the Göttingen depot. And all this takes place against the backdrop of the striking water tower. Photo: Joachim Schmidt, Railway Foundation

The tower base consisted of a lower part with a conical shape (truncated cone) and an upper section that was round with a constant diameter (barrel shape), as can be seen in the prototype photo.

But before I could start building, I had to plan it. I used full-scale plans and photos of the model from several books as a basis. My goal was a (slightly simplified) model of the tower.

Among other things, I did without the “cut-off” foundation on one side, which was necessary in the original to keep the necessary clearance for a neighbouring track. A further simplification is not visible for a viewer later: On the rear side of the building, which cannot be seen, I have omitted some windows.

Also, the access to the circular platform was only hinted at. I also simplified its supports in their shape and reduced their number - as well as the railing supports - from 16 to 12.

Under these conditions, I created a side view of the Ottbergen water tower that was as exact as possible, and almost to scale. I did not yet consider windows, doors, and other details, as I could determine their size, shape, and position at any time, if necessary. I made the necessary sketches and notes separately, as required.

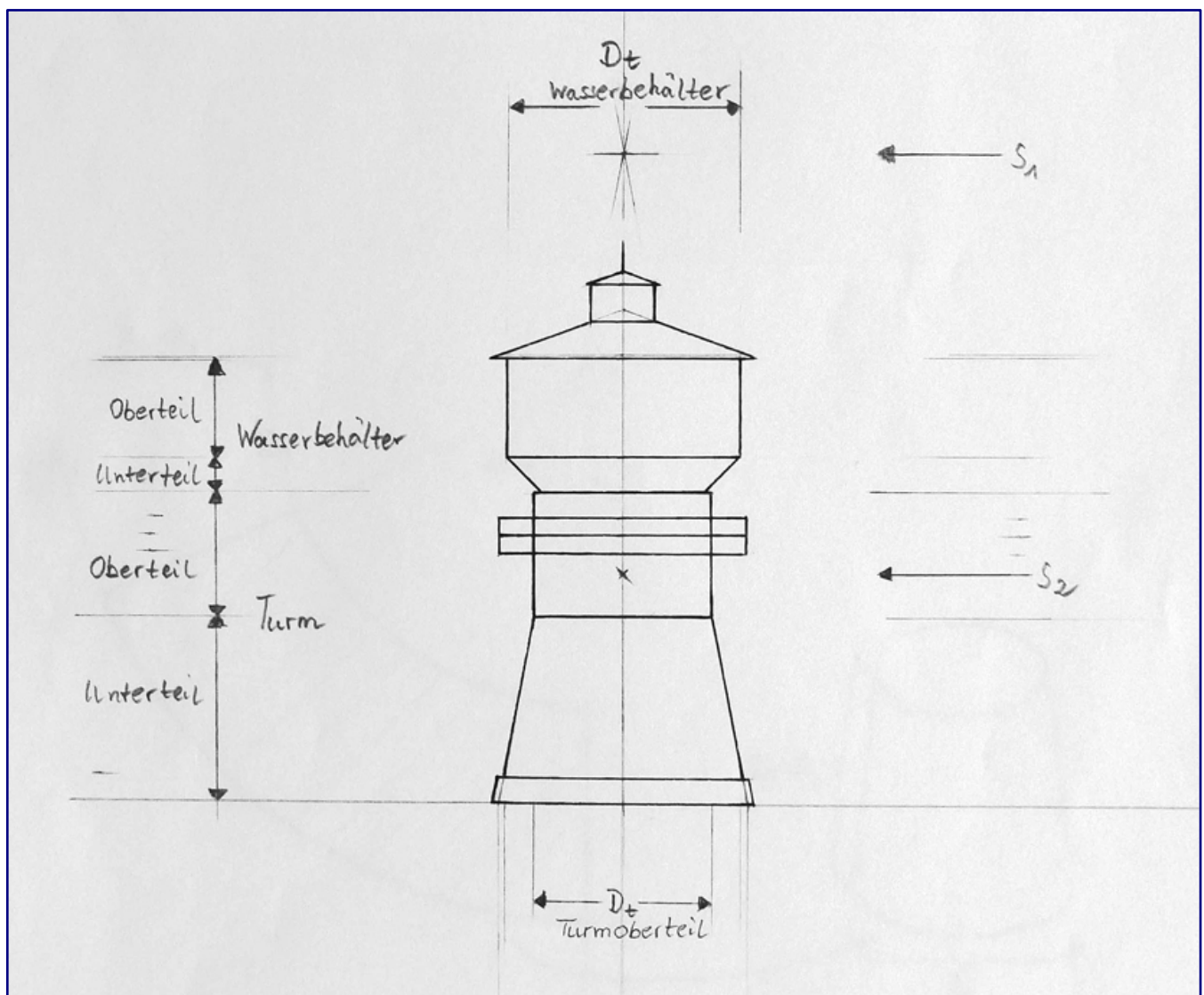


## Determining dimensions

If we do not want to leave the later result to chance, we cannot get around mathematical calculations and remembering what we learned at school. Only with precise calculations is it possible to produce all parts exactly, and to reproduce the prototype impression correctly.

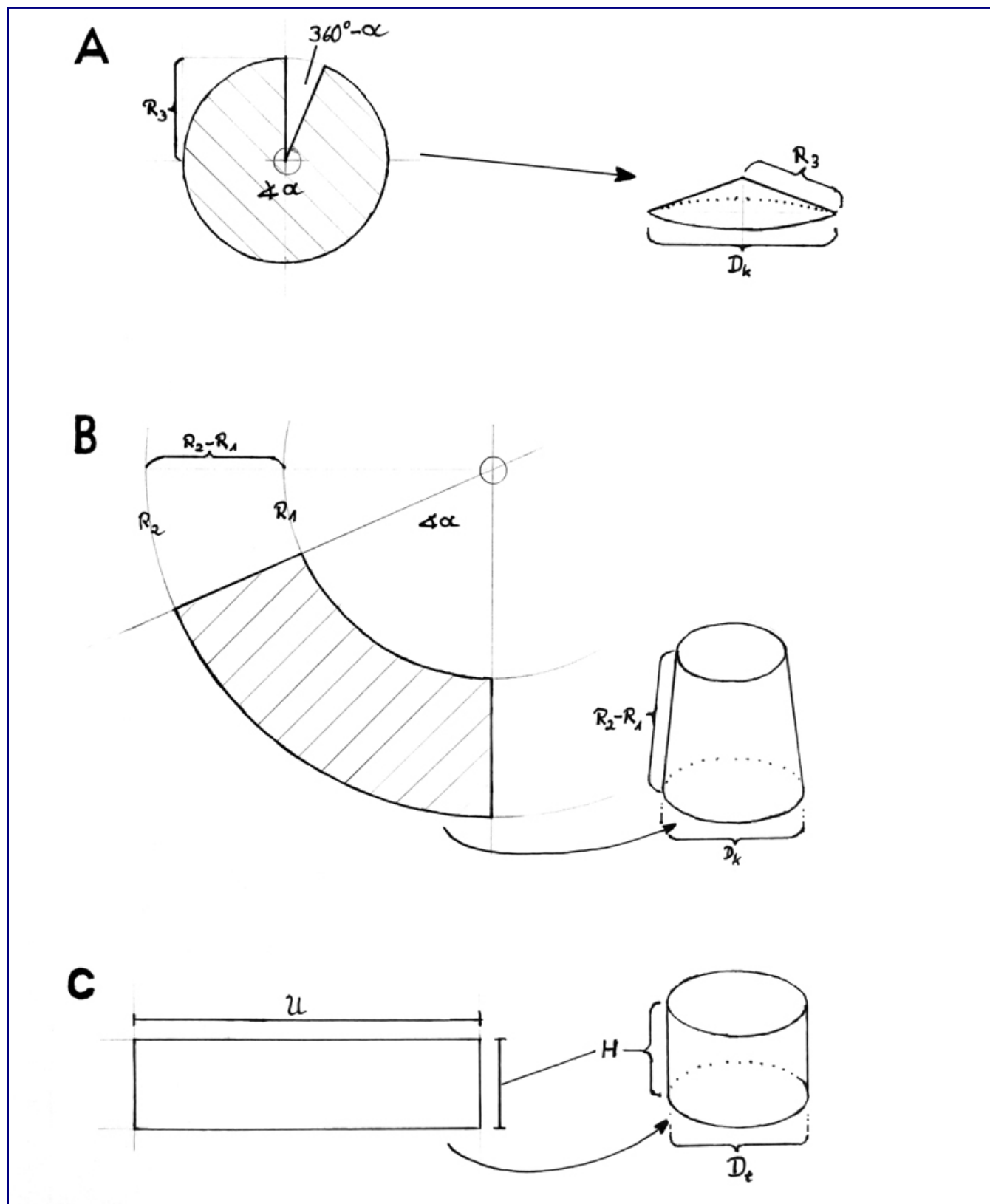
For the theoretical part that follows, I recommend looking up the shapes and abbreviations in the two sketches to be able to follow the explanations. The second sketch contains the three parts A to C, which graphically explain the conical roof, the truncated cones, and barrel-shaped parts.

The side profile, provided with additional, useful construction lines, now served as an important basis for the calculations that now had to be made. It was particularly helpful to draw the lines of the cone mantle edges of the two truncated cone-shaped elements beyond the vertical centre line of the side elevation to determine their respective point of intersection ( $S_1$  and  $S_2$  in the drawing).



Almost to scale side profile of the Ottbergen water tower. Drawing: Jochen Brüggemann

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Sketches of the procedure for determining the dimensions of components in the shape of a cone, a truncated cone, or a barrel-shaped body (A - dimensions for a cone-shaped component / B - dimensions for a truncated cone-shaped component / C - dimensions for a barrel-shaped component). Drawings: Jochen Brüggemann



To calculate circular figures and objects, the geometric constant  $\pi$  is required. For this project, an accuracy of  $\pi = 3.15$  (or 3.2) was sufficient. A circular cone is created from a circular plate from which a segment is cut out. The inclined surface of the cone is called the cone mantle.

On a flat plate (e.g., paper), a circle with a radius  $R_3$  is created around a point with the help of a compass. The angle at the centre point is  $360^\circ$ . If a segment is cut out of this circle towards the centre (pie slice), a circle segment with an angle  $\alpha$  (smaller than  $360^\circ$ ) remains, which can be shaped into a cone.

The following applies: The smaller the remaining circle segment or the smaller the angle  $\alpha$ , the steeper the cone and the smaller its diameter at the bottom. Determining the dimensions for a cone is done in several steps:

1. Circumference of the conical roof  
The radius  $R_3$  of the cone mantle can be taken from the side plan. The circumference  $U_3$  of the circle with radius  $R_3$ , from which the cone is to be formed, is given by the following formula:  
$$U_3 = 2 * R_3 * \pi$$
2. Circumference of the cone at the lower edge  
The diameter  $D_k$  of the base of the cone at its lower edge can be taken from the side profile. The circumference  $U_k$  of the cone bottom is therefore given by the formula:  
$$U_k = D_k * \pi$$
3. Angle  $\alpha$  of the desired circle segment  
$$\alpha = (U_k * 360^\circ) / U_3$$
  
From this ring segment (with the angle  $\alpha$ ), the desired cone is created by even bending, with the cone radius  $R_3$  and the diameter  $D_k$  at the cone bottom.

If we cut off a part of a circular cone parallel to its base, we get a truncated cone at the bottom and the residual cone at the top, also called a supplementary cone. In both bodies, the mantle (the inclined outer surfaces) retains the same angle of inclination.

At this point we are interested in the truncated cone, and how it can be made from a flat plate. For this consideration, the truncated cone is assumed to be standing on its base (top surface above). On the side elevation of the water tower, only the diameters of the base and top surfaces are visible, as well as the lines of the mantle edges (with the slope of the mantle) and their point of intersection.

Starting from the point of intersection, two distances are to be determined:

1.  $R_1$  = distance from the point of intersection to the upper edge of the truncated cone.
2.  $R_2$  = distance from the intersection point to the lower edge of the truncated cone.

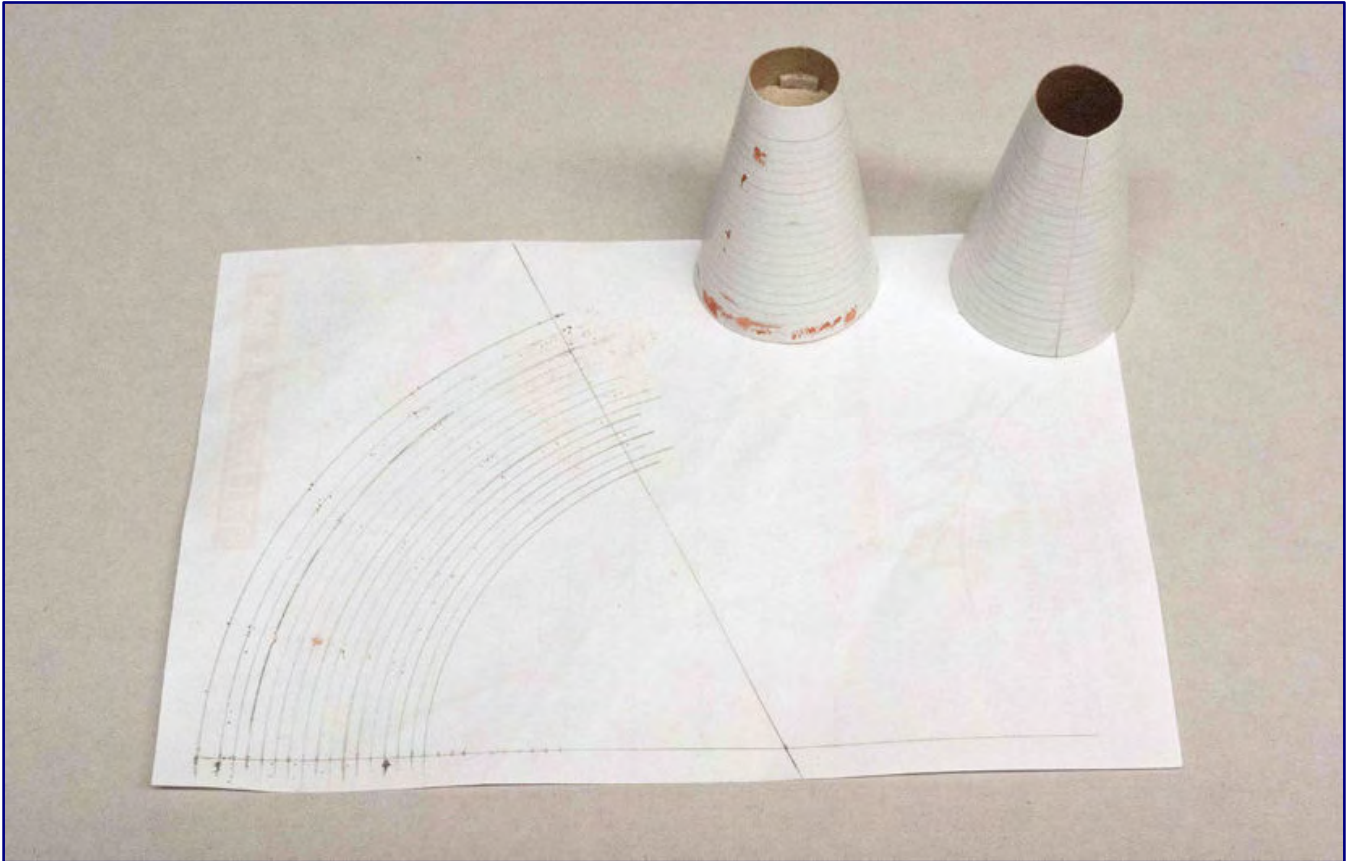
On a piece of paper, two circles with the radii  $R_1$  and  $R_2$  are drawn around a point with a compass. If a segment is cut out of the larger circle (with radius  $R_2$ ) in a wedge shape, a segment of a circle with an angle  $\alpha$  remains.

If we now only consider the area between the two circles (with radii  $R_1$  and  $R_2$ ) for the truncated cone, the result is a flat, circular ring segment that can be shaped into a truncated cone. Determining the required dimensions for the truncated cone is done in several steps.

1. The circumference  $U_2$  of the full circle with the radius  $R_2$ , from which the truncated cone is to be formed, results from the formula  $U_2 = 2 * R_2 * \pi$ .
2. The diameter  $D_k$  of the truncated cone on its bottom side can be taken from the side profile. The circumference  $U_k$  of the truncated cone is given by the formula  $U_k = D_k * \pi$ .

3. The angle  $\alpha$  of the desired ring segment results from  $\alpha / 360^\circ = U_k / U_2$ , in other words  $\alpha = (U_k * 360^\circ) / U_2$ . The truncated cone is formed from this ring segment with the angle  $\alpha$  by evenly bending.

A barrel-shaped structure with a circular cross-section is created from a rectangle with a height  $H$  and a width  $U$  that corresponds to the later circumference of the barrel. Only the diameter of the barrel  $D_t$  and the height  $H$  are to be determined from the side profile.



The supports for the construction of the tower base are two truncated cone-shaped gauges. Below is the drawn truncated cone mantle segment on which the brick wall strips were mounted in the second trial. Photo: Jochen Brüggemann

This is sufficient to calculate the circumference  $U$  of the circle with diameter  $D_t$  from which the barrel is to be created, according to the formula  $U = D_t * \pi$ . The dimensions  $U$  and  $H$  result in the rectangle from which the barrel-shaped component is created by bending it evenly.

## Water tower substructure

After the theoretical part, the handicraft work could begin. Besides my “household remedies” presented in part 1 (**Trainini**® 12/2022) and 2 (**Trainini**® 1/2023), I used a pair of compasses, a mini electric drill, and plastic clamps.

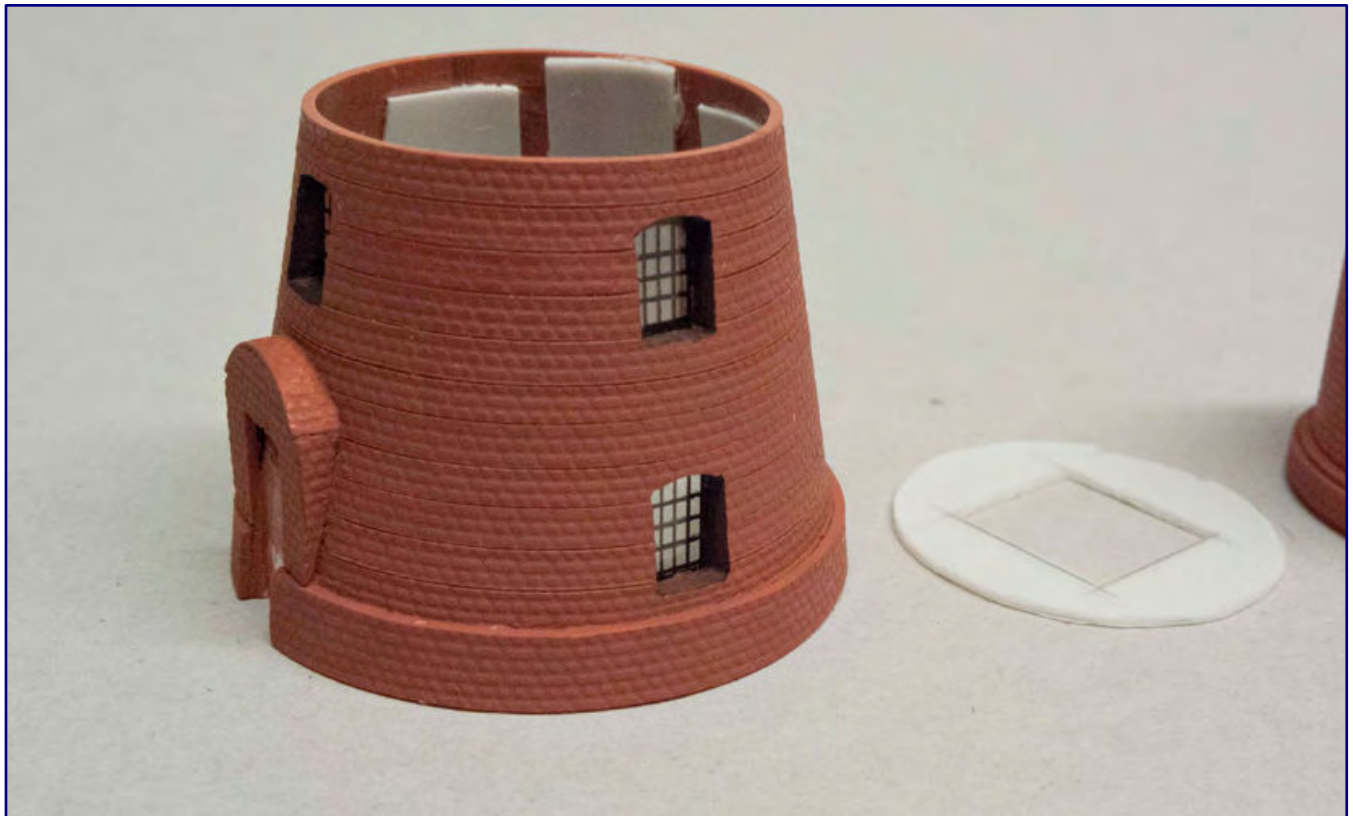


For various equipment details I resorted to Evergreen profiles, 0.5 mm thick wire, remnants of etched ladders from the former small-series manufacturer Beier Modellbahn-Technik and high-gloss photo paper remnants. My window building methods are described in a digression of part 2.

The construction started with the truncated cone-shaped tower base. First, I prepared the elevation of the truncated cone shell. I determined the dimensions from my side profile; the reference point here was the intersection point  $S_1$  above the top of the tower.

The finished water tower should approximately comply with the specified dimensions and proportions. For this purpose, the material thickness of the PS embossing plates had to be considered. Therefore, I drew a simplified side profile for the truncated cone, in which the diameters of the bottom of the truncated cone and the top surface were each reduced by twice the material thickness (1.5 mm). The angle of inclination of the frustum sides remained unchanged.

This reduced the radii  $R_1$  and  $R_2$  equally by about 3 mm compared to the side profile. Using the method described above, I calculated the angle  $\alpha$  for the desired segment, i.e., the truncated cone shell.



The lower part of the tower is already in an advanced state of construction. The outer, ring-shaped wall reinforcement and the attached tower entrance are already in place. A brick-coloured coat of paint has also been applied and the windows inserted. To the right of the cone lies the upper stabilising plate of the tower base, which will occupy us in the next episode.

With these corrected measurements, I made a truncated cone out of medium-thin, smooth cardboard as a gauge for the lower part of the water tower. When I drew it on the cardboard, I made the top of the truncated cone taller by reducing  $R_1$  to make it easier to use as a handle.



Indispensable tools in the construction steps covered today are a brush handle and low-viscosity plastic glue and superglue, which are applied precisely to the most delicate gluing points with the help of pins.

I also drew parallel lines at 2 mm intervals on the circle segment with the compass; these marks were intended to make it easier to keep the brick layers horizontal. My first plan: around this gauge, wall panel elements glued together in a ring shape, each consisting of three brick layers, were to be stacked on top of each other.

Unfortunately, the PS panel material could not be bent well enough, it continuously stretched back a little into its original shape, and the glued joint at the ring seam could not cope with the great tension. So, this trial was a failure. I kept the two cardboard gauges – I had made two to be on the safe side – and continued to use them.

For a new attempt I drew the outline of the truncated cone together with the parallel concentric lines on paper. I attached this sheet with pins to a small plywood board as an assembly aid.

I marked the position of the lower and upper edges of the truncated cone with arrows. Along the lower edge I placed very sharp and hard pins at intervals of about 1 cm, slightly inclined towards the upper edge. These pins served as supports to fix the curved brick wall strips exactly.



I now very carefully cut strips of three brick layers each from the brick embossing plate. The cut was made from the top of the panel, as centrally as possible, in a joint. As a cutting tool I used a scalpel with a fresh, sharp, and thin blade.

The sharper and thinner the blade, the more precise the cut and the finer the remaining cut joints later. The wall strips were measured in length so that after the ring-shaped bending they extended slightly beyond the side boundary of the drawn truncated cone.

I placed the first (and at the same time longest) brick strip on a cutting pad and bent it with my hands into an even circular shape that roughly corresponded to the first line marked by the pins.

Then, I placed the strip with the embossed side down on the mounting aid, pressed it with its outer curve against the row of needles and fixed it on the free side with pins so that it lay with its embossed side flat on the paper of the assembly aid.

In the same way, I bent the next brick strip, placed it next to the first and moved the pins so that both brick strips were practically without gaps next to each other and fixed in the correct arc. I made sure that the brick strips were parallel to each other, based on the concentric strips on the paper backing.

The gluing was done with thin plastic glue, which was applied carefully and in small amounts to the back of the strips facing upwards. This step required great care because excess glue could easily run through remaining gaps or soften the PS strips. I left the glue seam to set for several hours.

The following brick strips were bent, pressed on, and fixed in the same way. Where necessary, I also used plastic clamps to ensure that the embossed side was evenly positioned on the assembly aid.

In such cases, I first limited the gluing to the freely accessible seam sections; only after these glued areas had dried, I carefully moved needles or plastic clamps, so that I could continue with the gluing of the seam pieces that had now become free. During this work I could add a maximum of two brick strips per day.

After the last one had been added and the glue had dried, I carefully cut off the lateral protrusions with a scalpel and removed the resulting, still flat truncated cone shell from the assembly aid for checking. However, I fixed the component back onto it to correct any imperfections.



**This photograph, which shows the water tower after the water tank has been assembled but without further details, is intended to illustrate the geometric figures. It had to be disassembled into such figures and manufactured step by step with high precision so that in the end a coherent building would be standing in front of the constructor. Photo: Jochen Brüggemann**

With my hands and the help of a sturdy wooden brush handle, I now bent the structure into a tapered truncated cone. I immediately repaired any cracks that appeared; only after they had dried, I continued the bending work.



**Preview of the next edition:** The finished water tower has been positioned at its intended location for its installation test. It becomes apparent that the distance to the retaining wall should be increased a little before it is glued down, and its surroundings designed. Photo: Jochen Brüggemann

Once the desired shape was nearly achieved, I provided the inside of the component along the glue seam with a strap made from a piece of PS board that I had pre-bent evenly to match the inside at this point.

After the glue had set, the free side of the strap could be coated with plastic glue and both sides of the shell could be joined together to fit.

First, I held the component with both hands to be able to fix any problems immediately. Then I put one of the two gauges over the fresh truncated cone shell from above and pressed the second gauge against it from below (inside). Holding it in shape like this, I let the glue joints in the tower base set overnight.

In the model of my water tower, the wall of the cone-shaped lower part was significantly reinforced at the base. I reproduced this with a six-brick wide and sufficiently long strip of embossed plate. This I bent with my hands as described above and then joined to the outside of the lower part of the tower flush with the lower edge but protruding slightly.

The gluing was done gradually, starting at the seam of the lower part, and fixing it with plastic clamps. After the plastic glue had hardened, I applied thin super glue to the remaining gap. With a pin, it could be finely dosed and applied with pinpoint accuracy to enforce the gluing surface.

In the following part 4 of this small series we will continue with the construction of the barrel-shaped middle section. We have already mastered the most difficult steps so far.

**Supply Sources for Materials:**  
• <https://www.faller.de>  
• <https://www.fohrmann.com>  
• <http://www.peter-post-werkzeuge.de>



Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

## Bahndienst- und Dienstgüterwagen **Neues Standardwerk etabliert**

***Hinter den Kulissen waren Bahndienst- und Dienstgüterwagen unverzichtbar, um den geordneten Eisenbahnbetrieb zu gewährleisten und aufrechtzuerhalten. Doch im besonderen Fokus von Fotografen, Dokumentaren und Modelleisenbahnern standen sie nie. Ein Autorenduo um Stefan Carstens hat sich dem gestellt und erstmals systematisch den Dschungel der Helfer im Hintergrund gelichtet.***

Stefan Carstens / Wolfgang Henn  
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Eisenbahn-Dokumentation  
Hamburg 2023

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Der Name Stefan Carstens hat im Bereich der Fachbuchautoren Gewicht: Seit rund drei Jahrzehnten beschäftigt er sich, intensiv wie niemand sonst, mit den Güterwagen der deutschen Eisenbahnen. Seine Buchreihe zu diesem Themenbereich hat längst die Funktion eines Standardwerks.

Doch trotzdem blieb im April 2023, als das hier vorzustellende Buch erschien, ungewiss, ob auch dieses Werk ein Erfolg werden könne. Unsere Rezension soll dem nachgehen und darauf eine Antwort finden. Unbestritten ist hingegen, dass dieses Werk bestens zum Baubericht in dieser Ausgabe passt, den wir als thematischen Kontext vorgesehen hatten.

Stefan Carstens und Wolfgang Henn betreten mit der vorliegenden Lektüre völliges Neuland: Nie zuvor sind Bahndienst- und Dienstgüterwagen von DRG, DB und DR so umfassend, ausführlich und systematisch aufgearbeitet worden. Da muss die Frage erlaubt sein, ob diese Wagen nur ein Randthema darstellen, das kaum einen Eisenbahnfreund interessiert oder ob sie eine echte Marktlücke entdeckt haben.

Unsere Antwort nehmen wir gleich vorweg, um sie im Anschluss noch ausführlich zu begründen: Es ist eine Marktlücke, deren Potenzial erst jetzt richtig deutlich wird und sicher einige erst auf den Plan rufen wird. Deshalb haben wir keinen Zweifel am Erfolg dieses Buches, denn es aber auch brauchen wird – das soll ebenso deutlich mitgegeben werden.

Beide Autoren haben fleißig recherchiert und mussten ihre Ergebnisse früh auf mehrere Bände aufteilen, weil die Themenfülle unmöglich in einem einzigen unterzubringen war. Der Leser wird zugleich aber auch



feststellen, dass die Präsentationsweise und -tiefe sich von den bisherigen Güterwagenbänden unterscheidet und nicht zufällig außerhalb dieser läuft.

Anders als bei Reisezug- und Güterwagen sind nur wenige Originaldokumente erhalten geblieben, was die Vorarbeiten zusätzlich erschwerte. Außerdem handelt es sich ja um rein dienstlich genutzte Fahrzeuge, die oft nur in relativ geringen Stückzahlen erforderlich waren und insofern auch einen höheren Spezialisierungsgrad aufwiesen. Erhaltene Unterlagen erlauben deshalb kaum repräsentative Rückschlüsse auf das Aussehen.

In diesem Zusammenhang ist es umso beeindruckender, dass es den Autoren gelungen ist, einen vollständig erscheinenden Überblick ab etwa 1930 bis zur privatisierten Bahn zu geben, in deren Geschäftsmodell eigene Wagen an dieser Stelle nicht mehr passen wollten.

Der vorliegende Teil 1 erläutert Grundlegendes zum Einordnen eigener Fahrzeuge als Bahndienst- oder Dienstgüterwagen, den grundsätzlichen Unterschied zwischen diesen Typen sowie die angewandten Nummernsysteme. Anschließend aufgearbeitet werden dann die Dienstgüterwagen für bahnübliche Transport- und Sonderaufgaben wie das Befördern von Bau- und Betriebsstoffen, Abfällen, Radsätzen oder ganzer Fahrzeuge.

Bahndienstwagen für den Betrieb und die Instandhaltung schließen sich in Folgekapiteln an. Fahrbare Tankanlagen, die viele Modellbahner stets sehr interessiert haben, Schienenschleifzüge oder auch Sprengzüge sind hier zu finden – erfreulicherweise sogar mit Originalzeichnungen und Faksimiles.

Mögen die hier behandelten Wagen auf den ersten Blick noch weniger spektakulär gewirkt haben, so räumt das Buch schnell mit diesem mögliche, aber eben voreiligen Schluss auf. Noch spannender wird es freilich werden, wenn in den Folgebänden die Bahndienstwagen und Sonderfahrzeuge wie Schienenkrane, denen wir ja ein Modellthema dieses Hefts gewidmet haben, aufzuarbeiten sind.

Am wirtschaftlichen Erfolg von Band 1, der uns für ein Fortsetzen der Reihe unabdingbar erscheint, haben wir nun freilich keinen Zweifel mehr. Da bestätigen uns auch die bekannten, stets als sehr gut zu beurteilenden Rahmenbedingungen, die für Lesespaß und gutes Verständnis sorgen: gut gewähltes und aussagekräftiges Bebildern aller Textpassagen.

Annähernd 750 Fotos (von beinahe 800 Abbildungen) stammen von namhaften Eisenbahn-Fotografen wie Joachim Claus, Dr. Rudo von Cosel, Peter Driesch, Jörg van Essen, Günter Meyer, Reinhard Todt, Benno Wiesmüller oder auch Fritz Wilke (rund 200 Aufnahmen). Hier wird die fleißige Archivarbeit von Stefan Carstens ebenso deutlich wie sein wertvolles Netzwerk.

Fassen wir unsere Eindrücke zusammen: Schon nach wenigen Seiten wird deutlich, wie umfassend und aufregend das Themenfeld dieser Sonderfahrzeuge ist und welches Potenzial sie auch für die Modellbahn offenbaren. Bau- und Hilfszugwagen sollten sich doch überall im Bestand finden und standen auf irgendeinem Bahnhofsgleis herum. Gigantisch scheinen persönliche Wissenslücken, die dem Leser bei der Lektüre bewusst werden.

Um das zu erreichen, hatte das Autorenduo eine Herkulesaufgabe zu absolvieren, die sie mit Bravour gemeistert haben. Die wenigen Originaldokumente, fehlende, gezielte Bilddokumentation und eine Vielfalt beinahe individueller Konstruktionen wurden hier in beeindruckender Weise als Spannungsfeld aufgelöst. Geglückt ist das in der bekannten Qualität der Güterwagen-Reihe.

So sehen wir beste Voraussetzungen für einen Erfolg dieser neuen Reihe. Das scheint uns auch insofern wünschenswert, als dass wir die Folgebände kaum erwarten können. Den vorliegenden Band 1 nominieren wir deshalb auch in der Konsequenz der beschriebenen Eindrücke für die Neuerscheinungen des Jahres 2023 in der Kategorie Literatur.

**Publishing pages:**  
<https://www.stefancarstens.de>  
<https://www.modellbahnunion.com>



## Zehn Jahre Dampflok im Zeitraffer Rundreise durch Westfalen

**Wieder liegt ein Filmstreifen von Ton Pruissen vor uns, der mit großem Aufwand in ein digitales Format überführt wurde und beinahe vergessen lässt, wie alt das gezeigte Material inzwischen ist. Wehmütig tauchen wir in die Eisenbahngeschichte ein, wie sie unwiederbringlich ist. Wie scheinbar selbstverständlich begegnen wir dabei auch seltenen Vorbildern, die meistens nicht oder allenfalls als Einzelstücke erhalten blieben.**

Ton Pruissen

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Die vorliegende, neue DVD von Ton Pruissen wurde wieder mit großem Aufwand digital aufbereitet und nachgearbeitet. Das Ergebnis lässt beinahe vergessen, dass die digitalisierten Filmstreifen zwischen fünfzig und sechzig Jahre alte sind.

Doch die bewegten Bilder haben ihren Charme und authentische Wirkung nicht verloren. Dampflokomotiven ohne Ruckeln in Bewegung zu sehen und nicht von übermäßigem Flimmern abgelenkt zu werden, hat einfach etwas Besonderes. Mag die klassische DVD längst als digitales Auslaufmodell wahrgenommen werden, so hat sie längst noch nicht ausgedient!

Wer einen Blick in diesen 51-minütigen Streifen wirft, der wird uns zweifelsfrei zustimmen. Auch dieses Werk basiert nämlich auf Material, das zwischen 1963 und 1970 vom holländischen Eisenbahn-Filmpionier Ton Pruissen aufgenommen wurde oder aus dem Bestand befreundeter Filmer zu ihm gelangt ist.

Dafür war er etliche Male mit einer 16-mm-Filmkamera in Westfalen und im Weserbergland unterwegs. Während die Inseln des Dampfbetriebs bei der Bundesbahn immer weniger wurden, konnte er noch ungeahnte Schätze sichern, die nur wenige Jahren später endgültig von den Gleisen verschwunden waren und heute entsprechend selten in vergleichbaren Produktionen zu finden sind.

Beispielhaft genannt sei die Baureihe 10, die als „Schwarzer Schwan“ wegen ihres eleganten Erscheinungsbilds bei Fotografen und Filmern sehr beliebt war, aber angesichts von nur zwei gebauten Exemplaren auch selten vor die Linse zu bekommen war. Ihre Dienstzeit war derweil so kurz wie bei keiner anderen Dampflok: nur gut zehn Jahre stand sie im Betriebsdienst.

Die filmische Reise beginnt mit einem ersten Höhepunkt im Bahnhof Hamm (Westfalen). Hier erleben wir zunächst 10 001 bei der Ausfahrt vor einem schweren Reisezug. Der Streifzug setzt sich fort zum dortigen Betriebswerk und zeigt dem Zuschauer weitere Raritäten: eine Neubaukessel-41 mit Kohlefeuerung (wie aktuell von Märklin als Modell angekündigt), Maschinen der Baureihe 50<sup>40</sup> (Franco-Crosti) oder auch eine vierfach gekuppelte Zechendampflok „Henschel D 600“ auf Überführungsfahrt.

Die Rundreise führt weiter nach Lengerich zur Teutoburger Wald-Eisenbahn und von dort zur Georgsmarienhütten-Eisenbahn mit weiteren Schmankerln, die nur selten für Videowiedergaben festgehalten wurden. So finden hier auch besondere Privatbahnen ihren Platz, die auch einen nicht unerheblichen Teil der Gesamtlaufzeit füllen.

Weiter geht es nach Lübbecke, Löhne, Altenbeken (mit Viaduktaufnahmen aus dem Führerstand), Langeland, Beringhausen und Bestwig – also an höchst unterschiedliche Bahnstrecken, die aber gleichermaßen viel Atmosphäre bieten. Im Gedächtnis bleiben die letzten Einsätze der Baureihe 03<sup>10</sup>, der mit neuem Kessel keine lange Dienstzeit mehr vergönnt war.

Zu den wertvollsten Aufnahmen gehört wohl ein unerwarteter Abstecher ins oberhessische Kassel, denn der Titel richtet den Fokus ja schließlich auf den Dampfbetrieb in Westfalen. So finden auch einige in Farbe aufgezeichneten Sequenzen Eingang in den Film, mit 10 002 auch die zweite Maschine ihrer Baureihe. Kein Weg vorbei führte dort an den Schnellzuglokomotiven der Baureihe 0110, mit denen sie im gemeinsamen Plan liefen.

Zurück auf der Oberen Ruhrtalbahn erleben wir ein weiteres Mal die Baureihe 03<sup>10</sup> im Einsatz, wobei der filmische Ablauf auch schnell die Ablösung mit der Baureihe 23 ins Bewusstsein rückt. Aber auch diese Neubaulok gehört zu den Favoriten der Spurweite Z und wird im Vorbild entsprechend dankbar aufgenommen.

Folgen wir dem Streckenkartenverlauf, landen wir schließlich in Schwerte (Ruhr), wo die Ruhrtalbahn aus der Strecke Hagen – Dortmund ausfädelt. Zur Dampfzeit wäre es töricht gewesen, das dort ansässige Ausbesserungswerk auszulassen.

Es bildet im Film deshalb den End- und Schlusspunkt der Reise, zumal dort auch das Ende vieler Dampflokomotiven besiegelt wurde, während engagierte Bundesbahner den vermeintlichen Schrott als technische Denkmäler zu erhalten versuchten. Darauf gründete sich die Beliebtheit bei Fotografen und Filmdokumentaren wie Ton Pruisen maßgeblich.

Und so werden wir in Schwerte Zeuge der letzten Dampfloklüte wegen einer anziehenden Konjunktur und gleichzeitig ihres Niedergangs, der auch das Ende dieses Ausbesserungswerks im letzten Quartal 1967 mit sich brachte. Historischen Wert haben Aufnahmen der Schwerter Werkslokomotiven: Mit Lok 2 tat hier nämlich auch eine echte preußische T 3 hier noch ihren Dienst!

Blicken wir zurück, bleibt Wehmut über ein vergangenes Kapitel Eisenbahngeschichte zurück. Schon viele Filme haben sich der Dampfzeit gewidmet. Dabei haben die Ersteller alle als Dampflokaradiere bezeichneten Orte und Strecken besucht.

Doch immer wieder gelangen Aufnahmen in die Öffentlichkeit, die für sich besonders und im Gesamtbild abwechslungsreich sind, weshalb das Thema beinahe unerschöpflich scheint. Bei Ton Pruisen kommt noch hinzu, dass er seine Filmdokumente aufwändig nachbearbeitet und vertont wurden.

**Publishing pages:**

<https://www.nordsuedexpress.de>

**Explaining digital film processing:**

<https://www.youtube.com/watch?v=f3i-PYjvaMg>



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Readers' letters and messages

## Zetties and Trainini in Dialogue

*Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.*

### One magazine for all scales:

I wrote "Trainini - Praxismagazin für alle Spurweiten, nicht nur Spur Z", as subject. Each issue contains articles on the main model, tips on design, building layouts, presentations of new products, e.g., now the Uhlenbrock Intellibox 2neo, book reviews.

A treasure trove of knowledge, not only for fans of Z gauge, but also for the larger gauges.

It would certainly be interesting to find out who else reads Trainini, apart from Z followers. Perhaps there will also be a new subtitle soon.

Hans Helbach, Bonn

Editor's reply: There will be no new subtitle, because our focus remains on Z gauge, which is our passion. Nevertheless, we reach model railway enthusiasts far beyond this size with our publication. The monthly access numbers to the issues are about 14 times higher than our core target group would expect.

Incoming enquiries, calls and also trade fair discussions prove that we have a loyal readership in all nominal scales up to gauge 2, without exception. We are proud of this and it motivates us to give our best month after month.



### Reader search for small prefabricated plants:

Is it possible to buy a Z-gauge layout, complete with base, station and buildings that fit on a coffee table? Do you know anyone who sells prefabricated layouts or a marketplace where they are offered for sale?

Simon O'Keeffe (Irland), by E-Mail

Editor's reply: Of course, professional layout builders such as the Spur-Z-Atelier (linked from our pages) are a possibility, but this question is probably more directed towards pre-formed layouts that can be equipped with tracks and buildings and wired with little effort. Then, it is worth taking a look at the range from Noch (also linked by us), which has a wide range from small case format to medium-sized layouts.

### A round club anniversary:

Because the Model Railroaders Soest e.V. (MES 03) celebrated a round anniversary on 8 March 2023, they invited to a big summer party on 13 August 2023. At the club's home in Neuengeseker Heide, a district of the spa town of Bad Sassendorf in the Soest region, they exhibited various layouts of (almost) all gauges on an exhibition area of 400 m<sup>2</sup>.





On the occasion of the 20th anniversary of the MES 03 Soest e.V., there was not only an ordinary driving day, but also plenty of professional exchange over coffee and cake as well as sausages from the grill. Meanwhile, the youngest visitors could let off steam on a bouncy castle, as the weather also played along.

These are the permanently built exhibits of the club, whose main focus is the nominal size H0, but which also has a small and very active Z gauge faction. The more than 10-metre-long 1:220 scale layout, built in a U-shape, has been the subject of much tinkering since last year. As an extension, the reconstruction of an older bridge and a new station entrance are planned, in order to create an efficient staging yard behind the visible scenery, which can provide even more varied operation.



With a large, digitally operated module layout in U-shape, there will also be plenty on offer for Zetties who want to visit the club's exhibition days.

For the time being, however, the conversion to digital operation is still being pushed forward, which in the meantime also includes the control of points and signals, in addition to operating. The z21 system from



Roco, which works with the DCC format, is in use here and enables operation via mobile phones and portable computers or tablets.



The layout is controlled by a z21 digital control unit from Roco, which also allows the convenient use of mobile devices. Meanwhile, the association's Z-gauge group still has many ideas for expanding, modernising, and further developing its layout.

The anniversary event was well attended, and some of the visitors stayed enthusiastically for several hours. This was also due to the accompanying offer, because with coffee and cake as well as sausages from the grill, sufficient provision was also made for this. Small visitors could also let off steam on a bouncy castle outside, as the weather also played along.

So, we also succumbed to the attraction of talking shop with like-minded people about the hobby of model railways, talking about digital technology and spinning ideas for future projects.

Managing Director Dieter Lichtenberger was particularly pleased in conversation with the editors that the association also has an energetic youth group. This is certainly no longer a matter of course, but all the more important for the future of the association and the model railway.

If you are interested in this association or would like to visit one of the next exhibitions, you will find all the necessary information on its own pages: <http://www.mes03soest.de>.

#### **Project status of Azar Models:**

Moïse Rogez from Azar Models (<https://azar-models.com>) advised us about the progress of the current wagon projects. For example, the four-axle sliding tarpaulin wagon of the SNCF Fret with the generic designation Shimms is also being injection moulded.





This photo shows the current project status of the sliding tarpaulin wagon Shimms, also with regard to painting and printing of the series models. Photo: Azar Models

A first pre-series sample is currently available, which has also been test-painted and lettered. All new models are manufactured and delivered according to this quality standard.

#### **Trainini® meeting in midsummer:**

Summer time is holiday time, and this also applies to all volunteers on the **Trainini®** staff. They made good use of this time to get to know each other better and to promote togetherness. Because the work on the magazine and video channel is completely digitalised and personal appointments are presented via video conferencing, there is always a great interest in meeting in person.



Alexander Hock (left) and Christoph Maier (right) used their meeting to visit the Belgian railway museum. Photo: Christoph Maier

Because of the great distances between the helpers, sometimes even national borders and oceans lie between them, this is not always so easy to do. The summer of 2023 turned out to be a real stroke of luck.

So our translators Christoph Maier and Alexander Hock met in Brussels to visit the railway museum together and promote personal exchange. A few days later, editor-in-chief Holger Späing had the chance to introduce Alexander Hock to the editorial office in Brussels.



A "Tour de Ruhr" took editor-in-chief Holger Späing (left) and Alexander Hock (right) to various sites of industrial culture: sites of a bygone age with a commemorative and memorial function, always also closely linked to railway history. Photo: Simone Hock

On a joint tour of the city, they delved deep into the history of industrial culture, which is closely interwoven with the railway, and with the modern logistics and technology location, they drew the bow to the present, in which the railway now plays a changed role.

Meanwhile, the personal exchange between translator Oleksiy Mark and Holger Späing is ongoing, because in this case the distance from home is the shortest. A timely meeting between editorial colleague Dirk Kuhlmann and Holger Späing is currently pending, in which material and work pieces on reports are also to be exchanged.

All those involved in the staff agree to continue and live personal encounters. After all, the personal and friendly atmosphere is a central element of everyone's motivation.



## Rhine-Neckar Club Car 2023:

Once again, this year, the Rhine-Neckar regulars' table has presented itself with its own wagon pack. Produced by FR Freudenreich Feinwerktechnik in all-metal construction, it consists of two covered new-build freight wagons Gms 54 of the Bundesbahn (item no. 49.343.02) on the theme of agriculture.

Under the motto "From Fertiliser to Potatoes", the two wagons were equipped as follows: The first one without handbrake shows a glued-on label "Pfälzer Frühkartoffeln" (Palatinate early potatoes) with the transport destination Eberbach (chalk address), the second one has a handbrake platform and bears an imprint "Kali" (potash) on a blue background.



The two goods wagons of the Club special pack 2023 (art. no. 49.343.02) differ not only with regard to details in the colouring.

The roof colour was not painted in RAL 9006 white aluminium according to DB colour specifications, but in two slightly different shades of light grey in order to come closer to the typical operational impression. The ventilator flaps are also painted in different shades of reddish brown and white aluminium. On one of the two wagons, the slip box is also set off in black.

The edition of the Club car pack 2023 is 46 copies, which were individually numbered by client Volker Töpfer. The remaining stock that was not sold via the Club was passed on to the 1zu220 shop, but was already sold out by the editorial deadline.

## No summer slump in sight at AZL:

There is no sign of a summer slump at American Z Line, where monthly deliveries continue unchanged. In August, the EMD E8A in the Brunswick green of the Pennsylvania (art. no. 62605-5 / -6) rolls onto the dealers' shelves.



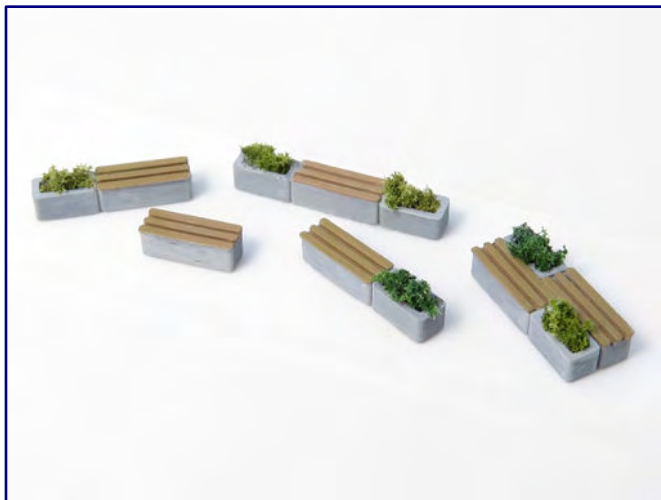
PRR's EMD E8A (item no. 62605-5; picture on the left) and UPFE's double pack R-70-20 refrigerated wagon (914831-2; picture on the right) are current novelties from American Z Line. Photos: AZL / Ztrack

This version of the diesel locomotive is also particularly striking due to the antennas running parallel to the roof line in the longitudinal direction, which were once very typical for the PRR.

Back in the programme are now the R-70-20 refrigerator cars of the UPFE (Union Pacific Fruit Express<sup>9</sup>, which certainly cannot be overlooked on the layout as a yellow snake of cars. They are offered as double (914831-2) and quad pack (914801-2).

#### No Pause at Yellow Dwarf:

3D printing supplier Yellow Dwarf (<https://www.yellowdwarf.eu>) does not take a break. Month after month, new items for Z gauge are also derived from the existing products. In August, new “benches II” (art. no. 60048) for the modern cityscape were added to the range.



Three ongoing novelties from the Czech Republic: benches II (art. no. 60048; photo above left), concrete bollards (60072; photo above right) and traffic safety traffic cones (60238; photo left). Photos: Yellow Dwarf

The concrete bollards (60072) have a guiding or blocking function in a permanent capacity or the traffic safety traffic cones (60238) have a temporary function, which will find just as many possible uses on the installation.

#### New source of supply for Minichamps:

As part of our coverage of spring new products and the Intermodellbau in Dortmund (incl. **Trainini TV** episode 13), we presented the very detailed and

finely structured 3D models by Imprintium, which are produced by this supplier according to Minichamps' designs.

Meanwhile, some of our readers reported that there was great interest in the vehicles and accessories, but that the calculated shipping costs were not economically viable in relation to the model prices.

In the meantime, a viable solution seems to have been found for everyone, as the 1zu220-Shop (<https://www.1zu220-shop.de>) from Westheim now reports that it has added the Minichamps brand to its range. Customers can now benefit from the retailer's services and shipping offers, which can be combined with all items in the range.



The range of the dealer from the eastern Sauerland includes both unpainted raw models and painted finished models. Not all of them are immediately available from stock (as of the editorial deadline).

**Only a few Märklin deliveries:**

The only new wagon arrived at the dealers in August was a heavy-duty transport wagon pack (item no. 82228), but it was delivered complete at the same time. This set consists of four heavy-duty wagons of the type Rlmp 700, former SSy 45.

The wagons are labelled according to a transitional scheme that was used when the Bundesbahn converted its wagon fleet to UIC-standardised class symbols. Thus, three of the four wagons also bear the former SSy 45 in addition to the new generic designation Rlmp 700. This was once intended to make it easier for the staff to find their way around.



The four-axle heavy-duty tracked vehicles now also carry lettering matching the Leopard 1A1 battle tanks in terms of time as the type Rlmp 700 (art. no. 82228). All four tanks bear different turret numbers.

In the news item on our portal pages, there are two more individual photos showing both lettering schemes of the four-axle bogie wagons from this pack, in detail. They are again loaded with a Leopard 1A1 main battle tank each, which have been printed quite elaborately.

The box also contains stanchions for the cars and four pieces of plasticine for temporarily fixing the load. At the same time a three-part pack (89025) of further tank models with different numbers on the turrets was delivered.



Here we show the back of the Wiesen semi-detached house (art. no. Z7701), which has an unusual effect and attracts attention. The picture below shows the front of the Berlin Hotel Excelsior (Z7401)  
Photo: Modellbau Laffont

#### Swiss surprises from Modellbau Laffont:

Modellbau Laffont (<https://modellbau-laffont.com>) surprises the Zetties with two new architectural building kits. They are made of solid-coloured hardboard, and, therefore, do not require any colour finishing.

The Restaurant Wiesen (Item No. Z6901) certainly does not have to stand on the tracks of the Rhaetian Railway, and also impresses at any other alpine location with fine, two-coloured window frames, an outside terrace with fence and finest tile engravings on the roof. The smart Swiss restaurant can be used from epoch II onwards.

Matching this, i.e., originating from the same region, is the semi-detached house Wiesen (Z7001), also usable from Era II. Here, too, the finest base and roof tile engravings are joined by the equally fine, two-coloured window frames.





At home in pre-war Berlin was the Grand Hotel Excelsior (Z7401). This exclusive hotel could be found on Askanischer Platz and was opposite the Anhalter Bahnhof, which was offered as a polystyrene kit by Märklin many years ago. At that time, it was the largest hotel in Europe, the front part of which has now been compressed.

The finest engravings are again a matter of course. However, this kit is only available directly from the manufacturer and from Stadt im Modell (<https://www.stadtmodell.de>).

#### Good ideas from Schrax:

Schrax (<https://www.schrax.com>) now has a model of a free-standing cast iron bathtub in its range. Such tubs were once widespread and are nowadays partly in a time use also as cattle watering troughs or water reservoirs for irrigation water in large gardens, as they are simply indestructible.



A bathtub and a hand-operated swing pump are two welcome accessories in the garden or even in the pasture. Photo: Schrax.

The openings for the drain and overflow, as well as the small feet, are also reproduced. If there is not enough precipitation to fill the tub, a hand-operated swivel pump appears to be a suitable aid. This is also new in the range in a reliable, simple form.

As these pumps proved to be very robust, they can still be found in rural areas today, often, also, at cemetery wells. In the fifties and sixties, they were still part of everyday life.

#### Timely deliveries of Micro-Trains:

MTL continues the War of the Worlds series with wagon number 6 (art. no. 518 00 844). Once again, this is a boxcar with a historical illustration of the literature for this special series.

We have also newly discovered light passenger coaches of the business class with boarding platforms at the ends of the coaches. They run on three-axle bogies and are lettered for the Pennsylvania (556 00 021), Union Pacific (556 00 061) and Norfolk & Western (556 00 240). All three are scheduled for delivery in the near future.

#### In-house adhesive from Busch:

Busch has now delivered a new laser-cut glue (art. no. 7594) in professional quality. It is said to reliably glue wood, cardboard, paper, polystyrene, and many other materials. Like comparable products from other suppliers, it dries transparently.

The solvent-free glue can also be used instead of white glue in model making, with its extra fine dosing tip. It can also be applied to larger gluing surfaces with a brush or spatula.

# Trainini® *International Edition*

German Magazine for Z Gauge



A current collector's pack (item no. FTCOL64) contains these two bulk goods wagons. Photo: WDW Full Throttle

#### Selected new products at WDW Full Throttle:

The latest WDW Full Throttle (<http://www.wdwfullthrottle.com>) new product is a two-piece collector's pack (item no. FTCOL64). It contains two covered ACF Centre Flow bulk freight cars, one of which is lettered for the Southern Pacific and one for the Cotton Belt.

William Dean Wright announces a new refrigerator car, in limited quantity, with advertising for the US beer brand Atlas (9403). The former brewery started production in Chicago in 1896 and became very popular, but had to close down as a result of Prohibition.



This red and white Atlas beer wagon (9403) continues the currently running refrigerated wagon series with a traditional brand. Photo: WDW Full Throttle

Later, the brand name returned in association with other breweries. At the turn of the 20th century, the brewery modernised many of its wooden refrigerated wagons and fitted them with steel bogies with Bettendorf bogies and new braking systems. The prototype of such a 34-foot wagon was in use until the early diesel era.

Full Throttle is sold in Germany by Case Hobbies, among others (<https://case-hobbies.de>).

#### The Herpa late summer new products:

Herpa has announced its aircraft new products for the months of September and October 2023. As usual, we are limiting ourselves to 1:200 scale models in a layout-suitable format up to a length of 30 cm. Aircraft models based on Russian models are not included, as they are currently not allowed to use European airspace anyway due to international sanctions and thus do not fit European-style layouts.

We have put together the following models for you under these criteria:



Air Baltic Airbus A220-300, re-issue (Art.-Nr. 571487-001),  
Interflug Ilyushin IL-18 (572873),  
Jersey European Airways British Aerospace BAe 146-300 (572828), and  
Eurowings Airbus A320 "BVB Fanairbus" (572750).



With the Airbus A320 "BVB Fanairbus" of Eurowings, Herpa once again provides a splash of colour and delights the friends of successful Dortmund professional football. Photo: Herpa

The aircraft in the design for Borussia Dortmund can also be purchased as a simplified snapfit model in flight display (613927). This series also includes a Lufthansa Airbus A320neo in the "Lovehansa" livery (613880).

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Board of Editors:  
Holger Späing (Editor-in-Chief)  
Dirk Kuhlmann  
Joachim Ritter

Correspondent North America:  
Robert J. Kluz

English translation:  
Alexander Hock, Christoph Maier, Oleksiy Mark, Martin Stercken

Further voluntary work: Stephan Fuchs, Ralf Junius (**Trainini TV**), Torsten Schubert

Licensed **Trainini Discussion Group** (<https://www.facebook.com/groups/1597746057122056/>): Michael Etz (**Trainini Locomotive Doctor**)

Publisher and Responsible in terms of German press law (V.i.S.d.P.) is Holger Späing, Am Rondell 119, 44319 Dortmund; Contact: 49 (0)231 9598 7867 or by e-mail to [redaktion\[at\]trainini.de](mailto:redaktion[at]trainini.de).

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# Trainini

German Magazine for Z Gauge



Small meets large in the Black Forest

Finale for the Water Tower  
Variety for the Tender

## Introduction

Dear Readers,

Today is a debut for me: my first foreword for **Trainini®**. Somewhat unfamiliar, but I'm going to do it. In this edition you will also find a news item about me as a new member of the editorial team.

September 2023, outside temperatures around 24 degrees Celsius. Not exactly the time to hole up in the basement train hobby room. But, nevertheless, we want to give you lots of ideas for the active crafting season that will come soon.

One must have ideas! This is what the small series manufacturer NoBa-Modelle has thought and has taken up the topic of the "tender," which has been unsatisfactory for many years. With a new tender construction, we want to show how a great unitary tender from DRG times can be constructed. Maybe the mass-production manufacturer will jump on this subject?

With our building construction series by Jochen Brüggemann we have now arrived at the last episode. In our opinion, it was a very successful project, and we wish all of you a lot of success with the reconstruction and, of course, fun with the crafting.

With the book on the history of the Prussian G 10 we present a new publication from the "blue series" of the Eisenbahn-Kurier. The author Hans-Jürgen Wenzel shows us the history of this class on a high professional level. Volume 1 is presented this month. Volume 2, which continues the history of the locomotive type from 1945 onwards, will also follow in a later edition.

As promised, we would also like to present the second part of the book on Diesel railcars from VGB | Geramond, which was published one and a half years ago. With the title "Verbrennungstriebwagen der Deutschen Reichsbahn – Triebwagen in Leichtbauweise von 1932 bis 1945" (Combustion Railcars of the German Reichsbahn – Railcars in Lightweight Construction from 1932 to 1945), the editors received a very factful and high-quality book that can be helpful in many ways.

With a visit to the Eisenbahnmuseum Schwarzwald (Black Forest Railway Museum) in Schramberg and this year's special exhibition "Small meets big", we have documented one of the highlights of this year in photos and text for you. Our Z gauge is very well staged there with Eckard Jehle's home layout. Fortunately, it is permanently available to this museum as an exhibit. In any case, we think that the museum, which is run by dedicated model railway enthusiasts, is always worth a visit.

Of course, the seasonal new products of our Z gauge suppliers must not be missing, which we will of course also present in detail in this edition. More will certainly follow in the next few weeks, as the exciting period of our hobby is only just beginning.

Finally, I had to note with a wink that our editor-in-chief did not present an aircraft model this time, or hid it so well that even I did not find it. On that note, I wish you a lot of fun reading, always joy and humour as well as a steady hand when modelling.

Sin-Z-erely,

Harald Fried



Harald Fried  
Editor



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We thank Jochen Brüggemann for his contribution and the Eisenbahnstiftung for their photo support.

Date of publication of the German language version of this issue: 29 September 2023

### Cover photo:

The 1:32 scale model of the 232 TC 401, a Prussian T 18 once in service with the SNCF, meets selected Z gauge vehicles from almost three decades in Schramberg. The contrasts provide the special contrast of the exhibition.

## Tinkering for version diversity **You have to have Ideas!**

*In Märklin's Z gauge programme, among many tenders, there is also an extremely aged 2'2' T 34, which has been the standard for unit locomotives in the range for 51 years. However, its prototype only appeared in 1939 together with the class 41 and is therefore not at all suitable for the period of the Reichsbahn from 1925 until the outbreak of war. We want to address this shortcoming with a tinkering idea.*

It seems to make one's hair stand on end: "Thinking in terms of trains" is a frequently emphasised guiding principle of our regular supplier Märklin. However, if we look at the new products brochures that appear several times a year, the total number of new locomotives and wagons seems to be almost identical.

These striking ratios leave no doubt about the basic level of margins in the individual product sectors that a commercial enterprise needs to cover its costs and generate a reasonable profit.



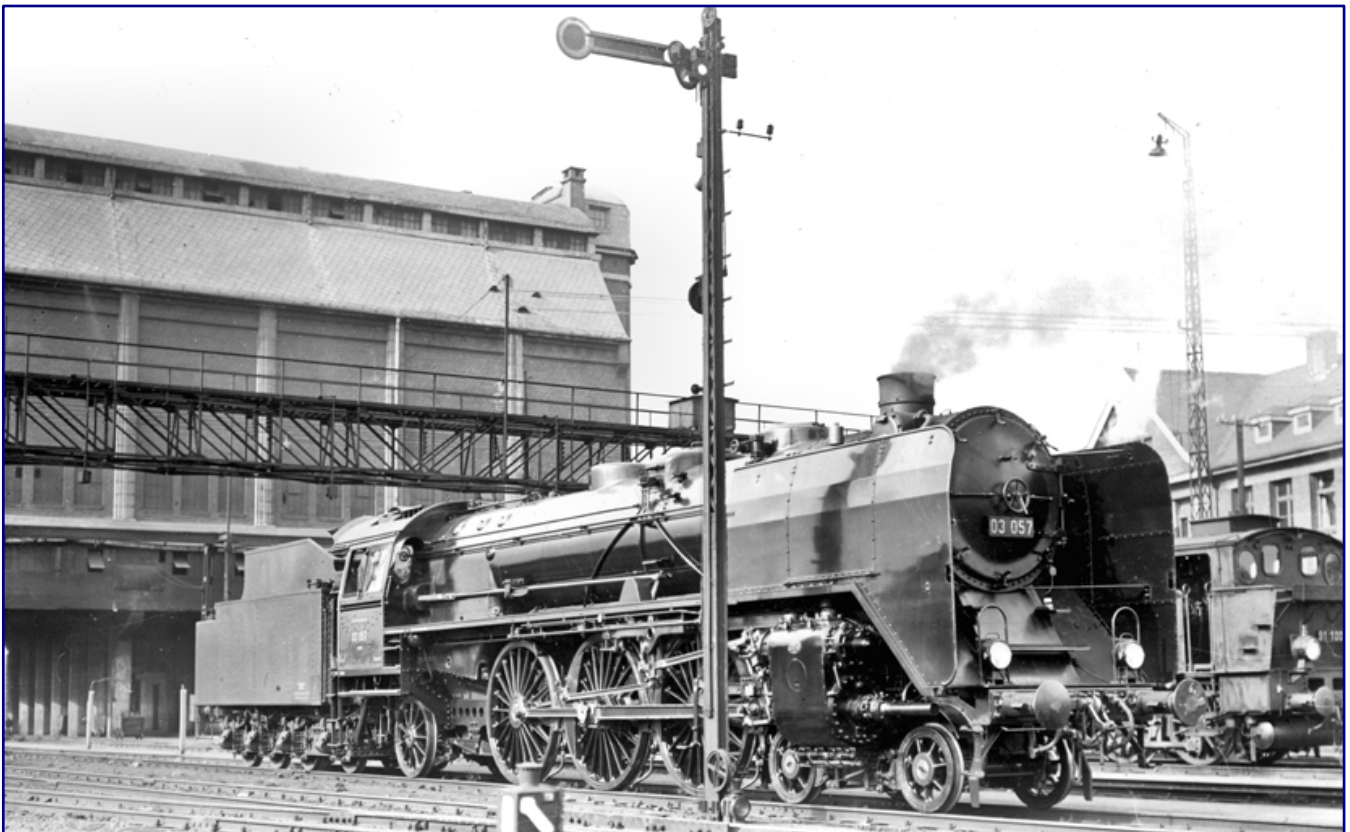
The photo of box tenders on DRG unit locomotives is far more colourful than manufacturer Märklin would have us believe: here 41 032 (with old construction boiler) from the Rheine depot was photographed in Münster (Westf.) Gbf on 11 April 1968. Coupled to it is a riveted 2'2' T 32, which it only received after 1945. Photo: Bernd Kappel, Eisenbahnstiftung

In view of this incalculable importance of locomotives in the range, it seems particularly incomprehensible that a 51-year-old tender model that has not been reworked or repaired to its advantage is still being dragged through the programme today and repeatedly becomes part of steam locomotive novelties.



We are talking about the unit tender 2'2' T 34, which first saw the light of day in 1972 as part of class 003 (item no. 8885). It lacks the coal box elevation that is often mounted on the prototype and, after reworking also fine lanterns. The inlay, which was battered almost two decades ago and insufficiently repaired, is more reminiscent of roughly hewn pieces of rock than of lumps of hard coal.

Equally serious, however, is the fact that the prototype for this tender did not see the light of day until 1939. With the exception of the three-axle, riveted tender for the "Steppenferd" (class 24), Märklin still does not have a single one in its construction kit that can be coupled with a standard steam locomotive for previous years.



The DRG's first new development was still strongly oriented towards Bavarian tenders. The 03 057 of the Dortmunderfeld depot, photographed in front of the high bunker of the Cologne depot, was coupled with the 2'2' T 30 on 21 March 1936. It received the tender with only one bogie and two rigid axles later, because in its home depot only a 20 m turntable was available. Photo: DLA Darmstadt (Bellingrodt), Eisenbahnstiftung

And this gap in the programme is not small: the history of the unit locomotive began in 1925 with the 01 and 02 series, which makes 14 years that cannot be represented. The dilemma also reveals itself when looking at the number of series and total numbers of units, because it affects all locomotives with trailing tenders up to the appearance of the class 41, which was converted in the current procurement process.

Only for the class 50 (and not offered 23), which also dates to 1939, is there again an offer, which was designated as 2'2' T 26. In a nutshell, this means the missing possibility to offer the class 01 parade locomotive, its lighter sister class 03 and the heavy class 44 triplet locomotive prototypically with "Deutsche Reichsbahn" plates.

Even if the number of Epoch II enthusiasts who are directly affected by the gap in the range as model railway enthusiasts may be continuously decreasing and may not justify an injection moulding new product on its own, we see a need for action here.

The expected solution is a tender design that stands out from the museum-ready box tender at first glance. At the same time, it could use an existing running gear and save development costs here as well as take up shape variants for individual design differences with a well thought-out construction that relies on exchangeable sliders.



In front of the FD 23 (Hamburg - Berlin), the photographer had probably expected a class 05 locomotive, but instead it was 03 038 from the Bw Hamburg-Altona that led him on 2 August 1938. Taken between Zernitz and Neustadt (Dosse), its tender 2'2' T 32 of 1926 design, which we are making a model subject today, is also well featured. Photo: RVM-Filmstelle Berlin (Bellingrodt), Eisenbahnstiftung

But all this remains a pious appeal, which has not been heard in Göppingen so far, and in parallel must also come up against an existing budget. Until then, only self-help can offer a way out, which we would like to demonstrate here today with the help of Märklin and small series parts.

First, however, we would like to briefly touch on the development history of the DRG's four-axle box tenders up to 1939 in order to make our explanations both clear and comprehensible.

## Larger box tenders

Important construction principles of box tenders should be explained in advance in order to be able to clearly distinguish this type of construction. The central construction element is a rectangular water tank, the sheet thickness of which was mainly oriented towards sufficient corrosion resistance. On the inside, they were reinforced with transverse trusses.

Perforated plates on them also served as surge plates, i.e., they dampened the water moving when starting and braking, but allowed for equalisation and thus an identical level in the entire tank. When the

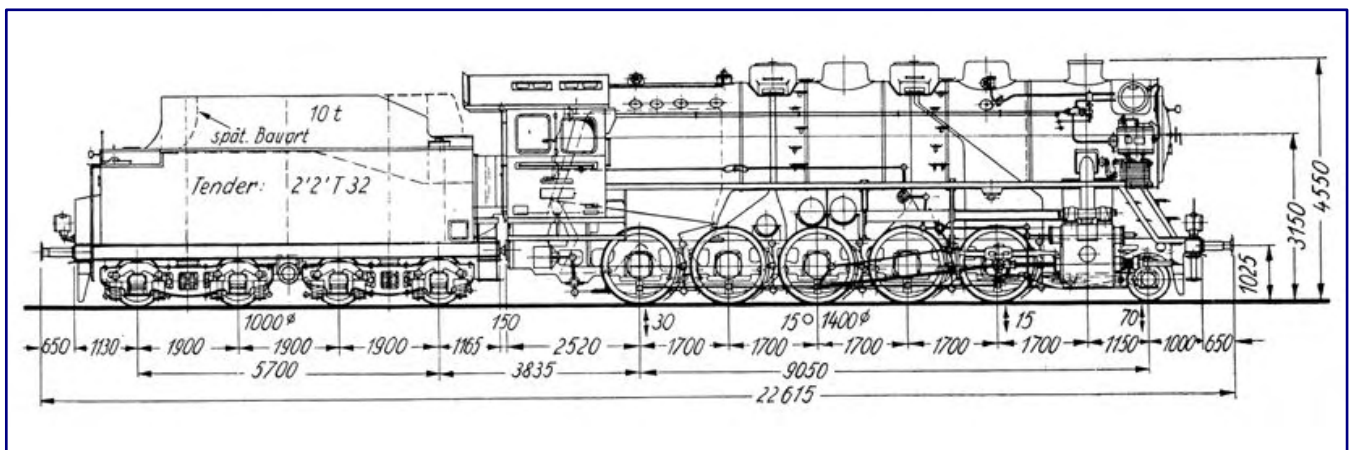




41 010 (Mönchengladbach depot) restored in 1950 at the Duisburg Hbf depot, which was closed in 1966. In the meantime, the steam locomotive has also given up its tender and received a 2'2' T 32 in exchange. 38 3772 and 42 1507 can be seen in the background. Photo: Willi Marotz, Eisenbahnstiftung

tenders, the term, by the way, comes from the English language, became larger, they were also fitted with longitudinal undercarriages to provide the necessary stability.

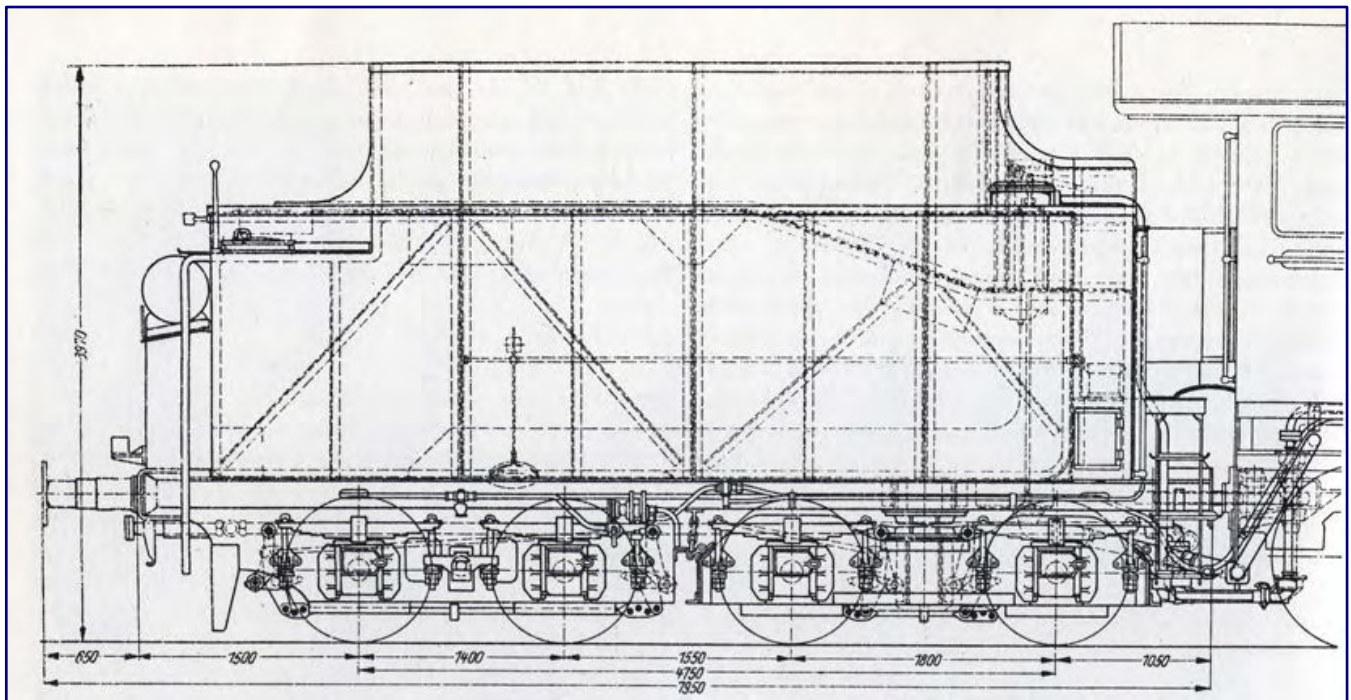
The ceiling of the water box also forms the floor of the coal box, which was built on top of it. At the front of the tender was the coal extraction point, called the shovel plate and placed 500 to 600 mm above the driver's cab floor. An uncontrolled falling of the coal was prevented by sluice doors, typical especially for Bavarian designs, or facing boards.



The class 43 was also coupled with the tender 2'2' T 32. The drawing in the Merkbuch für Trariefahrzeuge even marks the outline of the two last designs of this tender. Drawing: Deutsche Reichsbahn, DV 939 Tr (Ausgabe März 1962)

The water inlets were usually located at the end of a tender. The position (in the middle or on the outside) changed in the course of time and development. The height of the DRG's standard tenders was 3,000 mm above the top of the rails. With regard to water cranes of old national designs, additional emergency inlets were required at a height of 2,750 mm above the top of the rails.

The water tank with the mounted coal box rested on a stable frame of long girders, which was made torsion-resistant by the buffer beam and cross girders. The cross members were located in the centre of the tender and at the level of the pivots, if a bogie was to be found there. Otherwise, there were axle bearing cut-outs in the frame cheek.



The first four-axle unit tender, first designated as the 4 T 30 and later as the 2'2 T 30, still borrowed heavily from Bavarian designs. This is clear from the box bogie with an axle base of only 1,800 mm, which is clearly different from the subsequent unit bogie, and especially from the rear, fixed axles with only 1,400 mm distance from each other. The short construction and the low water inlet at the rear were due to the still common turntables and water cranes from the Länderbahn era. Drawing: Deutsche Reichsbahn-Gesellschaft (RZA)

Decisive demands on the running gear were above all the maximum limitation of the total axle base of a locomotive (due to the turntable length), the maximum wheelset load (due to superstructure limitations), sufficiently large supplies for the chosen boiler of the locomotive, the required load braking and running smoothness with regard to the intended maximum speed.

It becomes clear at this point that the development of suitable tenders, which tended to be larger, also had to keep pace with that of the DRG unit steam locomotive. Nevertheless, the Special Locomotive Committee paid little attention to them. The locomotive factories based their designs very much on the Bavarian state railway engines.

Their primary focus was to ensure that the large standard locomotives could also be accommodated on the 20-metre turntables that were common at the time. But the focus was not limited to the Bavarian wheelset arrangement alone. The lock doors also became the standard for the time being, although not unanimously. Likewise, the Bavarian template prevailed in the arrangement of the clothing and tool lockers.



Contrary to all the principles customary there, the Prussian constructions were alleged to lack order and structure in the storage of clothes and tools. Tools in particular were left lying around in disarray and often required a complete clearing out when searching for them.

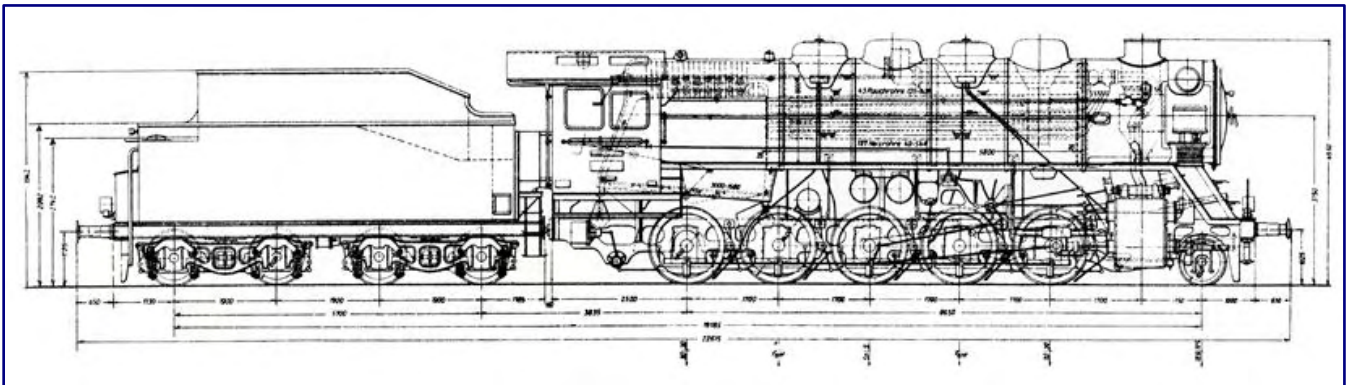
The first result of the consultations was the unit tender 2'2 T 30, which was delivered with the ten types of the 01 and 02 series from 1925. As typical Bavarian features, it had the aforementioned lock doors and only one bogie, the rear two were rigidly mounted in the frame. It was only a transitional solution, chosen with a view to the 20 m turntables.

## Bogies and welding technology

Already in the same year, another tender followed, which now had two bogies. It also held 10 t of coal, but now already 32 m<sup>3</sup> of water. Subsequently, called the 2'2' T 32, it belongs to the type of construction that we have made our model topic today.

Unchanged from its short-lived predecessor were the rivet construction and plain bearings of the axles. However, its axle base had increased by 950 mm to 5,700 mm, which initially limited its use. Initially, it was only coupled with the model locomotives of the 43 and 44 series, which were shorter than the two express train series and could therefore still be turned on a 20 m turntable.

The appearance of the coal box in the 1925 design still corresponded to the 2'2 T 30: The ends of its side walls were rounded towards the driver's cab side. This already changed with a first revision in 1926 (1926 design). The side walls of the coal box now ran in a slant towards the driver's cab.



Since the 43 and 44 series were shorter than the standard express locomotives, they could be equipped with the longer tender 2'2' T 32 from the beginning. With the older 1926 design shown here, template for NoBa models, the machines were delivered starting with 44 013. Drawing: Deutsche Reichsbahn-Gesellschaft (RZA)

While the axle stands and overall length remained identical and the lock doors, which were unpopular with the personnel, were retained, a saving of 1.9 t could be made in the weight. With this 2nd design of the 2'2' T 32, which was the template for NoBa models, the locomotives 01 012 to 101, 03 001 to 162 as well as series machines of the classes 43 (from 43 011) and 44 (from 44 013) were put into service.

Another revision of the tender, which sometimes leads to irritation in the technical literature because of its designation "new design 1926" in the instruction books, took place in 1934. Instead of "2nd design 1926" it would therefore have been more accurate to designate it as "design 1934". This tender was also riveted and equipped with sliding axle bearings.

continues on page 12

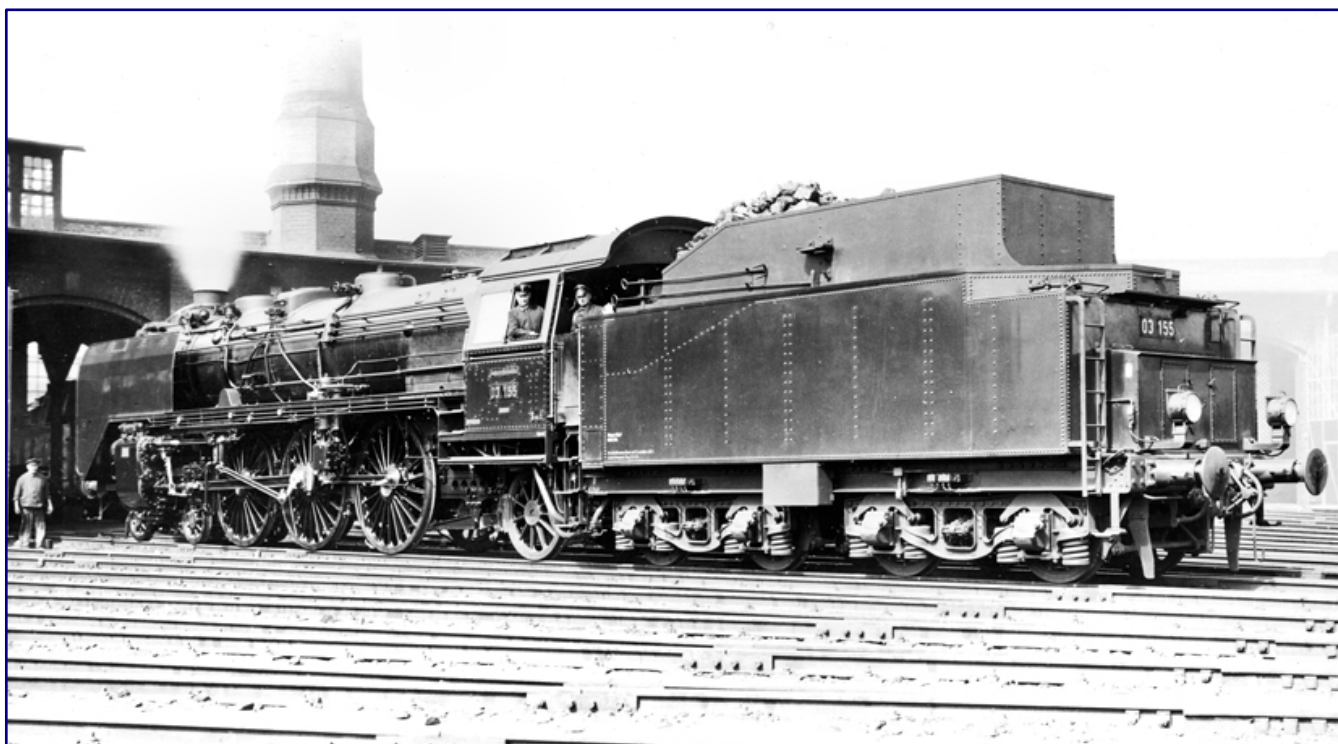


Photo above:

01 089 (Bw Hamm P) is a guest at Bw Köln Bbf on 9 August 1931. Besides the missing surface preheater, an indication for a steam injector of the Friedmann company, the tender 2'2' T 32 in design 1926 of an older version is noticeable. Photo: DLA Darmstadt (Bellingrodt), Eisenbahnstiftung

Photo below:

03 155 is also coupled unchanged with this unit tender when photographed at Leipzig Hbf West depot on 2 October 1935. Photo: DLA Darmstadt (Hubert), Eisenbahnstiftung



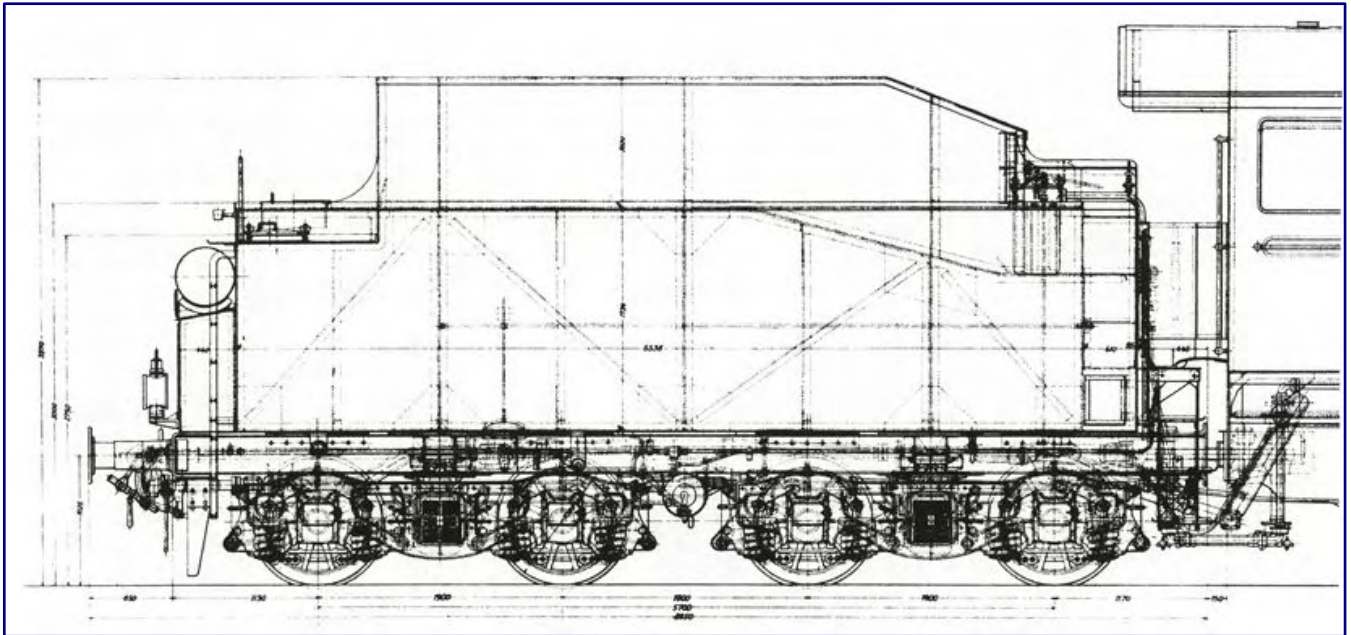


**Photo above:**  
On the Cornberg ramp we see 44 1392 (Bw Göttingen Vbf) from DB stock, still equipped with a 2'2" T 32 type tender on 21 April 1961. The open water box lid illustrates the central position, the opening lower down the side is an emergency inlet. Photo: Joachim Claus, Eisenbahnstiftung

**Photo below:**  
03 293 rushes through Königsmoor station on the Bremen - Hamburg line as a replacement locomotive on 23 June 1954 with F 33 "Gambrinus". Her tender at 44 1392 is a 2'2" T 32 of second design 1926. Photo: Walter Hollnagel, Eisenbahnstiftung

An external difference was again the coal box. Its side walls were higher and angled inwards at the upper edges. Towards the driver's cab, they were pulled through to the front edge. It was coupled for the first time with the locomotives 01 102 and 03 163, and was also frequently encountered after the Second World War.

In 1936, the 2'2' T 32 underwent its last development leap. In all dimensions and its exterior, it still corresponded to the design of 1934, but now it was a welded construction instead of a riveted one.



The RZA drawing illustrates the features of the unit tender 2'2' T 32 in the older 1926 design. The forward sloping side wall of the coal box, like the curved line of the 1925 design and the 2'2' T 30, was due more to an expression of dynamism in the taste of the time than to function. As it caused air turbulence and dirty cabs during fast journeys, another modification followed. Drawing: Deutsche Reichsbahn-Gesellschaft (RZA)

This resulted in a weight saving of 2.4 t, which reduced its unladen mass to 30.2 t. With the same axle pressure, its water tank capacity could therefore be increased from 32 to 34 m<sup>3</sup>, which cannot be read from the official designation.

Both the 1934 and 1936 designs had new boards instead of sluice doors at the coal tapping point. This was the RZA's reaction to unauthorised conversions by some depots that had simply built such boards between the permanently open sluice doors. Only fourteen steam locomotives were delivered with the last design of 1936: 03 254 to 261 and 03 270 to 275.

The reason may have been that a new design benefited from the advantages of the welded construction: the 2'2' T 34, which was first seen on a locomotive in December 1938 and which formed Märklin's template for its tender established in 1972. This was the 41 003, which entered service with this tender in 1939. It was followed by the 44 066, soon followed by many hundreds of other machines with this tender.

continues on page 14





**Photo above:**  
Unit tenders 2'2' T 32 were not that rare on the German Federal Railway. Also 03 054 (Bw Köln-Deutzerfeld) is shown in April 1955 in Oberhausen Hbf. Photo: Reinhard Todt, Eisenbahnstiftung

**Photo below:**  
Even the 41 293, fitted with new-build boilers, ran with a riveted tender of the 2'2' T 32 type, as recorded on 1 June 1968. Although all Class 41 production engines were originally supplied with the 2'2' T 34, they soon had to give it up in exchange to Classes 01 and 03. Photo: Wolfgang Bügel, Eisenbahnstiftung

In the peacetime version it can be recognised by the openwork coal box struts, in wartime deliveries the struts were without the cut-out. The front boards were now also standard. The Reichsbahn, on the other hand, soon abandoned the sliding axle bearings and opted for roller axle bearings.

A decisive reason for this was that this tender, because of its large water supply, was soon coupled with the 01 and 44 series, for whose large boilers it was the first choice. They ensured extensive freedom from maintenance and lower running resistance.



Also, because the unit tenders 2'2' T 34 were almost universally equipped with roller axle bearings, the Reichsbahn liked to provide them with the express steam locomotives, because this finally eliminated the danger of hot running during high-speed runs. 01 066, seen here in Göppingen during Märklin Days 2023, also owes its existence to these circumstances.

With the class 01 parade locomotive, it was also more suitable from a running point of view, as the maximum permissible speed of the more recent deliveries had been raised to 130 km/h. In the course of the war, the Reichsbahn converted both the first deliveries of this tender and the 2'2' T 32 accordingly.

## The conversion project

NoBa-Modelle's tender body fits very well overall with the Märklin running gear, which can easily be removed from a 2'2' T 34. In view of already 51 years of production, it is easy to get hold of a donor model on the second-hand market.



The fact that we do not simply use the bogies of the small series manufacturer is due to the fact that these are designed for the in-house magnetic coupling. They therefore do not fit on a steam locomotive model and also do not open up any coupling possibilities for non-converted wagons from Märklin.

A first test reveals some need for adjustment, but this can be judged as manageable. It turns out to be just perfect that the Märklin running gear has a circular opening in the middle that is exactly aligned with the threaded sleeve in the small series body. The undercarriage and superstructure can be screwed together just as easily later as NoBa-Modelle has thought to do with its own complete model.



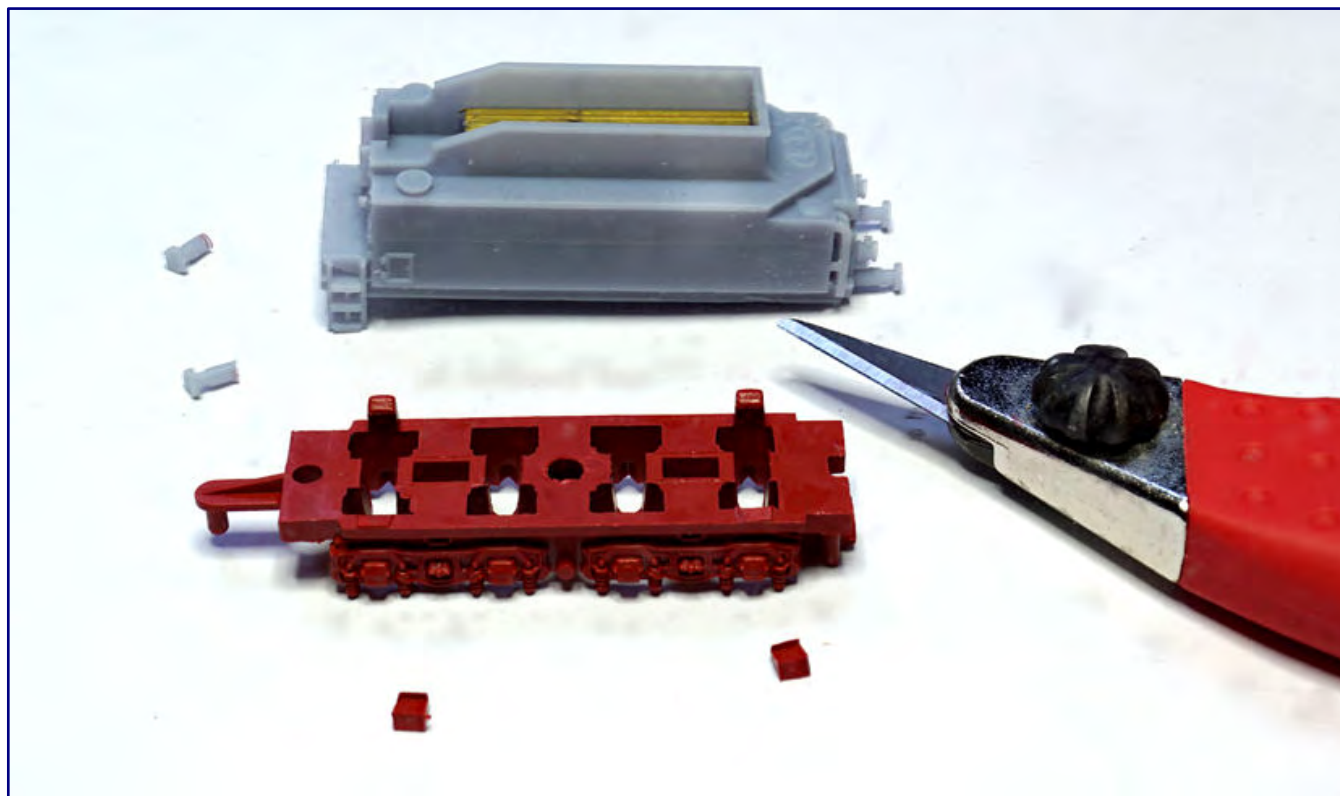
The first step is to glue the brass weight into the coal box. In order to be able to build a prototypical dumping cone on it, a suitable structure is built in front of it and on its surface with the help of the LED-Light Booster from Uhu.

So, our conversion begins with gluing the brass weight in the coal box. For this we use Uhu All Purpose Glue Super, which is a mixture of conventional glue and cyanoacrylate. After gluing, the weight forms the substructure of the coal insert.

In order for this to be applied credibly later, however, the pouring cone still needs to be refined somewhat. This is done with the help of the “LED Light-Booster” from Uhu, which is also suitable for filling gaps. Here, we place some of it in front of the weight in the direction of the removal point and harden this by means of UV light.

If we work our way forward layer by layer, nothing will flow, because after the light treatment the mass is crystal clear and hard. We also apply some of it to the top. In this way we gradually remove the appearance of a rectangular block. The cured UV adhesive is paintable and needs no further treatment for the following steps.

continues on page 17



**Photo above:**  
The catches on the running gear for the weight used by Märklin have to give way, as well as the front buffers on the 3D printed housing are removed. The working surface of the heater must also be shortened a little with the file.

**Photo below:**  
Some material also has to be removed from the side so that the tender undercarriage is aligned with the new housing. The standard rail clearers on the undercarriage have already been dismantled.



Now it's time to adapt the running gear and the superstructure so that both parts can be connected without any problems. The front buffers have to be removed from the body of the tender, because the prototype only had these when it was converted into a service freight car. On the locomotive side it originally had butt plates as well as main and auxiliary coupling irons, which represented a close coupling.

On the Märklin undercarriage, we cut off the four retaining lugs at the top with a sharp tinkering knife, which are intended to hold a weight with this manufacturer. The long sides also require material removal with the file until the undercarriage fits into the new superstructure.

Too much of a good thing are also the round replicas of the track clearers at the rear end, while the system coupling shaft can remain, as it does not interfere and will have to accommodate the coupling again later – it just cannot then accommodate a fusible pin from the earlier superstructure. This will therefore have to be replaced, as well as the track spaces that find their support on the underside of the superstructure.

As soon as this adjustment work is finished, we put the two parts together provisionally, reinsert the four axles and do a positioning test with a suitable steam locomotive. The intermediate result proves to be good, but the tender cannot swing out freely in curves. This changes when we grind back the tread for the staff a little. Repeated tests show when enough material has been removed.



Evergreen round bars are used to create new track scrapers, which are mounted directly on the tender box. A rectangular profile for the coupling guide in the shaft is already prepared and only needs to be sanded. After painting, the final assembly and shortening of the track scrapers can take place.

Then the new construction of track clearers takes place. For this we use a round bar made of polystyrene with a diameter of 1 mm (Evergreen, art. no. 500211; distributed by Faller). In the longitudinal line of the wheel treads and buffers we create the necessary receptacles at the tender end behind the buffer plank. An HSS twist drill in the pin block helps us to do this.

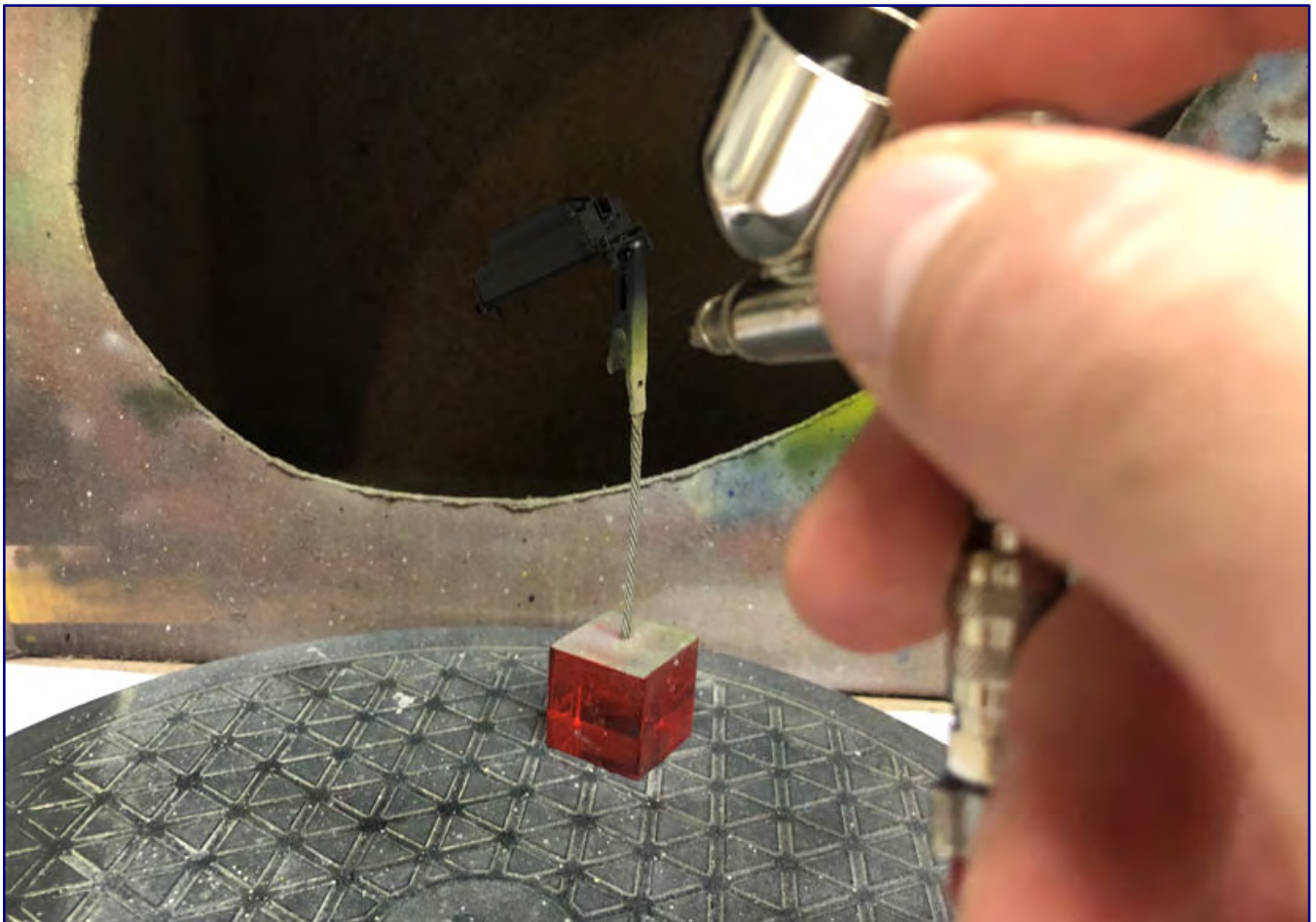
We cut the round rod generously so that we can shorten it to the correct size later. Since we now have the quiver with our PS profiles at hand anyway, we immediately look for a suitable rod to hold the coupling guide, the piece of which we first cut off just as generously.

A square profile with an edge length of 1.5 mm each (Evergreen 500153) is suitable. It fits unmachined but not yet into the opening of the coupling shaft, because one side has to be sanded oval first, so that the coupling body can turn in the curve later. Prepared accordingly, we put this part aside for the very last step on our tender.

## Fine tuning?

What follows can certainly not be described as fine work, but it is decisive for the overall appearance in a special way. Our attachment parts are still lacking colour, as is the 3D printed superstructure. The latter gets a deep black overall appearance with the injection pen (RAL 9005).

For this we use a PU primer ("Black Primer" SNR-403) from Badger, which also produces the final Photo here. After drying, we paint the rail edges with a brush in satin fire red (Oesling Modellbau 81003000).



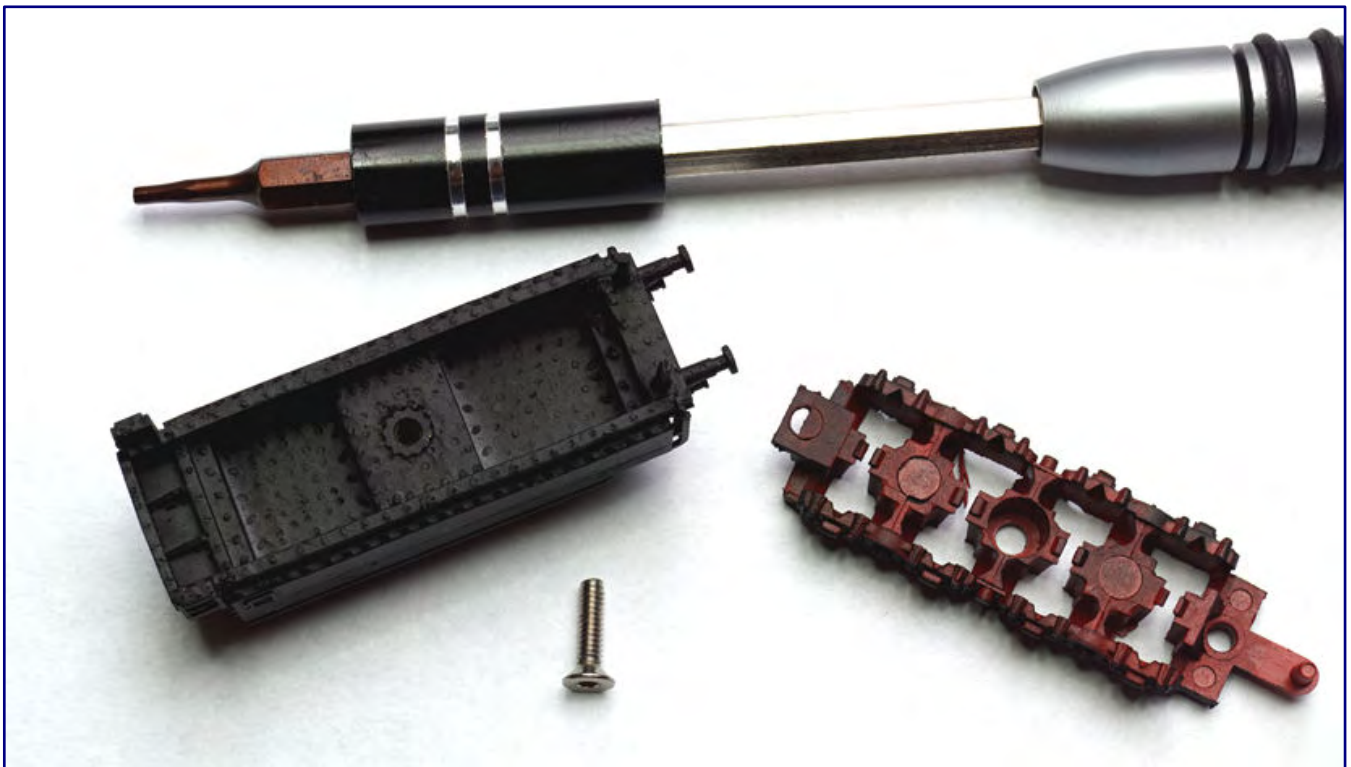
We already achieve the black colour of the tender superstructure with a primer that is applied by spraying. We use "Black Primer" (art. no. SNR-403) from Badger.



If you wish – we have refrained from doing this for the time being – you can now apply wet decals with new tender inscriptions that you have created yourself or that have been printed by one of the relevant suppliers. The same applies to the operating number of the intended locomotive, which can also be selected by printing or as an etched plate, depending on the time depicted.

In our case, it has not yet been decided which series the converted tender will be permanently coupled to. The final step in the colouring process is the clear varnish sealing, for which we recommend the products from Bergswerk.

The degree of gloss should also be adjusted to the locomotive model, which can vary greatly depending on the article number and year of production at Märklin. Consequently, this is also a step that remains open for the time being in our conversion.



After the described work, the small series superstructure and the Märklin chassis fit together well. Thanks to the opening in the chassis, the screw connection from NoBa-Modelle can be used for a permanent hold.

There are also several alternatives for the running wheels of the tender: light or black nickel-plated disc wheels, also matched to the locomotive model in front. The prototypes had spoked wheels, which are also available in the Märklin assortment, but unfortunately have a high price and are hardly available second-hand.

Because there is little to see of them in the chassis, there is definitely an option to continue using existing disc wheels and modify them a little. To do this, take a piece of polystyrene plate, drill a hole in the diameter of the wheel tread and use it as a template for spray painting.

Since the Oesling lacquer (and also alternative products) have little hold on metal, a suitable primer is indicated. A solvent-based primer is the first choice here. In the Weinert range you will find such a primer as nitro-acrylic metal primer (2698). After all solvent residues have dried and evaporated, we can continue working with the red Oesling lacquer.



To help our readers understand the constructional differences to the Märklin tender, this top view should help with the comparison. You can also see how the dump cone above the brass weight could be reproduced: In the prototype, the rear bottom of the coal box was horizontal, only the front one had a slope that hindered the sliding of the coal and was not very popular with the personnel because the stoker sometimes had to crawl there.

At the modeler's request, the undercarriage gains even more depth if it is also painted black and the protruding surfaces are then again highlighted in red with a fine brush-over. At the same time this creates the impression of light dirt, because a steam locomotive undercarriage does not stay clean for long.

At the end of these finishing touches, we put real coal in the tender's box superstructure. As with the crane in the last issue, it comes from Jeweha Modelbouw in extra fine grain. This Belgian supplier has various distribution partners, but is mainly present in person at larger trade fairs.

Now the chassis and superstructure can be connected with the screw included in the NoBa model kits and we are approaching the final step. Now only the clutch and the pressure spring have to be inserted into the shaft and fixed with our PS profile piece.

To prevent it from falling out, we fix it to the bottom of the coupling shaft with a drop of glue. This should be thick so that it does not run into the interior and stick to the entire coupling.

The result of our little tinkering is a unit tender from DRG times, which looks considerably better than the Märklin fossil and at the same time offers a welcome change behind some unit machines.

Again, we can transfer a piece of railway history to the small scale in an individual way. Who knows when the large-scale manufacturer will recognise the potential here and expand its mould construction kit for steam locomotives?





Inspired by the prototype photo on page 6, we have had a class 03 steam locomotive pull up with an express train for a photo session. This scene, which was not uncommon in DRG times, can still be depicted after 1945: Some express locomotives that did not have this tender coupled ex-works even received it later, as can be seen from pictorial evidence in the literature.

**Manufacturer of the donor models:**

<https://www.maerklin.de>  
<https://www.noba-modelle.de>

**Working materials and tools:**

<https://www.bergswerk.de>  
<https://www.faller.de>  
<https://www.oesling-modellbau.com>  
<http://www.peter-post-werkzeuge.de>  
<https://www.uhu.de>  
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Build your own structures (part 4)

## Up to the Water Tank

*In Part 3 of this series, Jochen Brüggemann described the preliminary calculations and drawings for a precise construction of his water tower, before he then turned to the most difficult construction stage with the conical base. Today, we continue step by step upwards until the finished model, based on the Ottbergen original, is standing in front of us.*

By Jochen Brüggemann. In the last episode I described the construction of the truncated cone for the base of the water tower. The other elements will follow in this fourth part, but their construction is not the end of the story. The already finished part as well as the other parts have “flaws” that should not be noticeable. I also had to consider this.

Since all the components in the shapes of a truncated cone, a barrel or a cone would not be seamless to make, the plan from the very start was to align all these components during assembly so that their seams lay in a vertical line.



Today, the water tower is growing upwards according to the Ottbergen model, until it looks like this picture and is ready to decorate the layout.

I carried out all further steps in such a way that this seam line came to lie with a slight lateral offset on the later rear side of the tower, which would not be visible to the viewer.

In the prototype, the entry door and the windows of all floors were arranged in four vertical rows, each offset by 90°. To simplify matters, I only considered the row above the entrance door and an adjacent row of windows.

The wall areas of the designated window and door rows as well as the opposite ones were reinforced on the inside with suitably pre-bent PS pieces (PS = polystyrene) that reached just below the upper edge of the tower base. This reduced the risk of fracture when making the window and door openings.



In the fitting check of the tower base and top, we get a view of the back with the seam lines aligned vertically as mentioned in the report.

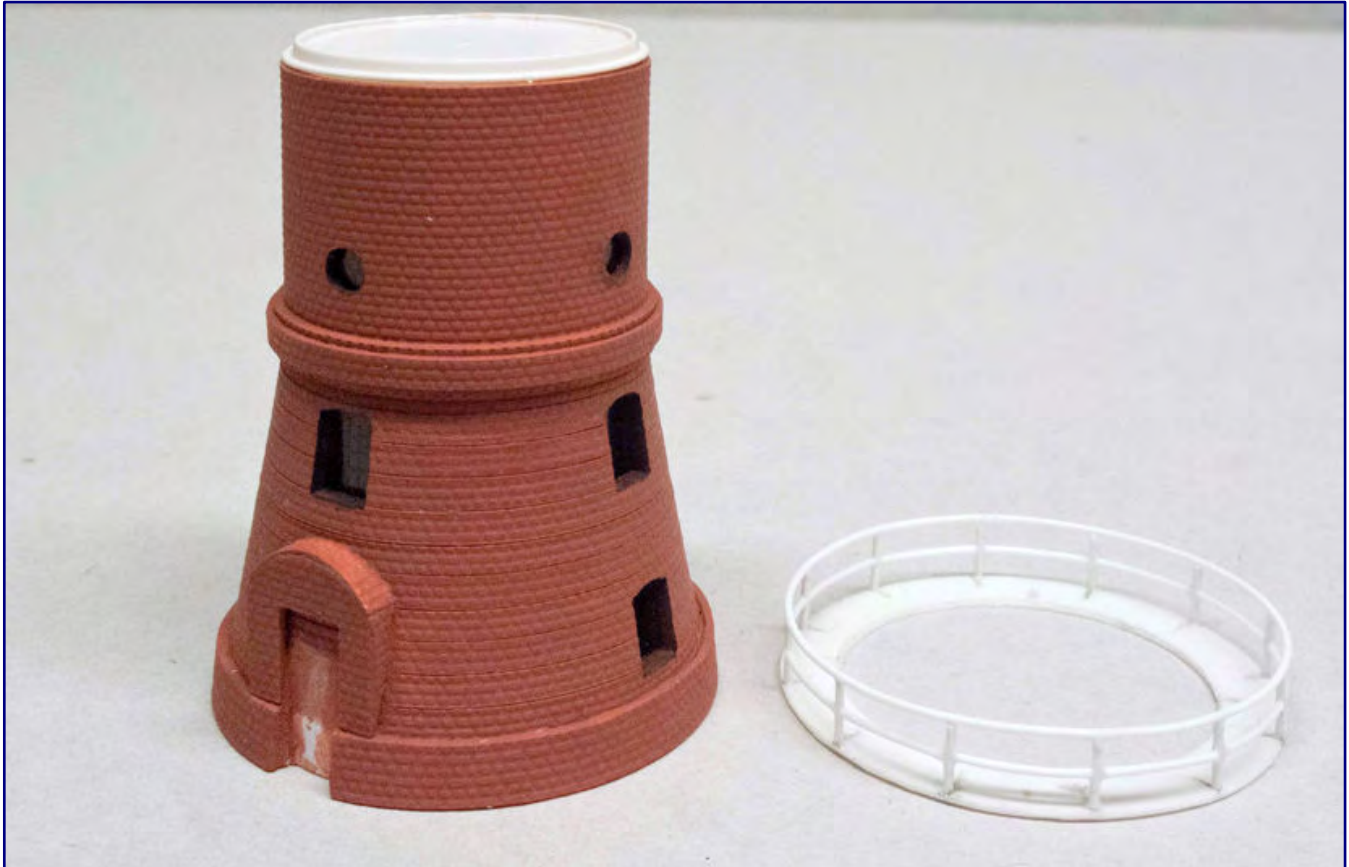
I also made two round plates from plain PS material to support the circular shape. To do this, I scratched circles of the right sizes with a pair of dividers that can be fixed at a desired radius.

The diameters of these stabilising plates were chosen so that one plate formed exactly a base cap (flush with the bottom edge of the wall), while the other could be positioned just below the top edge of the truncated cone. I matched both plates to the glue tab of the seam and provided them with a central opening for better handling. Then they were inserted but not glued yet.

The lower edge of the tower base I sanded on fine sandpaper. A check measurement with a calliper gauge showed that the outer diameter of the base at its upper edge corresponded almost exactly to the dimension provided in the side plan (see drawings and preliminary calculations in part 3).



For the cylindrical upper part of the tower, I determined the measurements for the height  $H$  and the diameter  $D_t$  from the side view. For both tower parts to fit on top of each other later, the diameter had to be reduced again by twice the material thickness (1.5 mm) of the PS brick embossing plate.



The cover plate resting on the top of the tower was later discarded. The pre-assembled circular platform (right) has been prepared and is waiting to be installed. It was to be placed above the row of round windows.

With this measurement I calculated the required circumference  $U$  of the tower top. Considering the alignment of the brick layers, I applied the measurements  $H$  and  $U$  to a PS brick masonry embossing plate, cut it out with a sharp scalpel and rolled it into a cylinder.

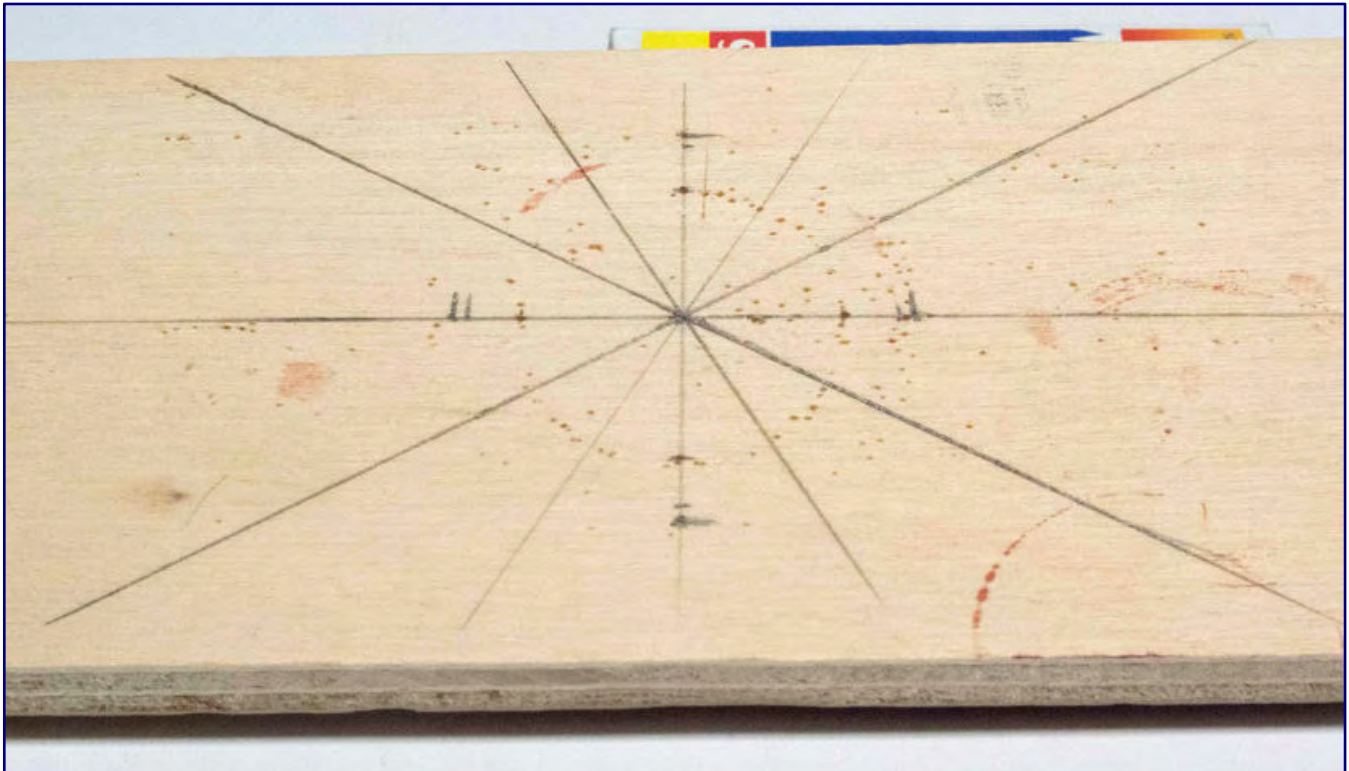
I pre-bent a suitable piece of plate as a glue strap and glued half of it to the inside of the wall seam with finely dosed thin-bodied plastic glue. After the glue had dried, I coated the other half of the lug with glue in the same way and carefully closed the seam, making sure that the brick wall joints fitted exactly.

As with the pedestal, I also made two circular stabilising plates for the upper part, with a central opening and recess for the seam glue strap. These plates were also initially only inserted in their intended positions (just above the lower edge or just below the upper edge of the part), but not yet glued. A check showed that now the upper part of the tower fitted exactly on the lower part of the tower.

The Ottbergen water tower was reinforced with several layers of bricks in the transition from the conical lower to the cylindrical upper part. I also copied this brick ring in a simplified form. To do this, I cut two strips out of a brick plate: one with three and one with five brick layers, a few millimetres longer than the circumference of the upper part of the tower.

First, I bent the wider of the two strips into a ring, put it around the bottom of the top of the tower and adjusted its length. Spreading a thin layer of plastic glue on the back, I glued this strip in the shape of a ring like a cuff to the bottom of the outside of the cylinder, starting at the seam, so that the lower half of the ring extended evenly over the bottom edge of the tower part.

While the glue was drying, the ring was secured in its correct position with plastic clips. Then I bent the narrower brick strip in the same way, adjusted its length as well and glued it in the middle of the wider strip so that a layer of brick remained visible all around the top and bottom of the wider strip.



This assembly board with guiding lines is used to determine the positions for the twelve circular railing supports as well as the drill holes for the four actual load-bearing replicas.

After drying, I fixed the lower of the two stabilising plates at the intended place of the tower top with glue and expanded the lower edge of the ring slightly conical with fine sandpaper, so that afterwards the tower top came to stand evenly on the substructure.

## Door and windows

In the next step, I marked the positions of the entry door and the windows on both tower parts with a soft pencil. For the windows in the conical base, which were rounded at the lintel, I drilled small holes with a mini drill and inserted conical cutter, which I widened with a file and scalpel and brought into the intended window niche shape.

These, I then lined on the inside of the wall with thin polystyrene strips, which I sanded vertically upwards with file and sandpaper until a vertical window position was achieved. For the circular rosette windows in





This oblique view of the tower with the water tank removed allows a view of two of the four supporting struts (see arrow markings). The windowpanes with the drawn frame are also partially visible.

the upper part of the tower I drilled small holes with the cone cutter, which I then gradually enlarged with larger drills to the intended diameter of 2.5 mm. This was followed by a window outline on paper.

The bottom part inside the entrance door area was reinforced with several layers of PS boards. On the outside I marked the position of the entrance door and engraved and sanded with scalpel and file a flat, vertical surface in the door format, because the entrance door had to stand vertically and was not supposed to protrude over the reinforced tower foundation.

The entrance porch of the model got made from several wall panel sections. This created the rounded front at the top (with the actual door opening), two sides tapered at the bottom (so that the front became vertical) and a rounded cover made of brick panel.

After assembling and mounting, I painted both wall elements of the water tower and also the window niches with slightly diluted acrylic paint. Afterwards I covered the niches with a non-opaque anthracite glaze.

With the help of the window outline I drew the window frame edges and the glazing bars according to the method described in the excursus of part 2 (see **Trainini®** 1/2023). In this case, black pigment ink was used. The installation also followed the pattern familiar to readers.

A rectangular piece in the shape and size of the entrance door was cut from a very thin scrap of board, painted it medium grey and glued it into the prepared tower entrance. This marked another milestone in my building project.

I continued with the circular stabilising plates in the base, which were now permanently fixed with plastic glue. Beforehand, I had widened the central opening in the base plate to an octagonal shape and made a matching octagonal “negative” out of cardboard, which would later serve as a placeholder when positioning it on the layout. A centre line with an arrow marks the position of the entrance door.

In the next step, I determined the outer diameter of the circular platform from my side plan. I measured the inner radius for the circular platform so that after assembly a gap of about 0.5 mm would remain between the tower wall and the circular platform.



The door leading into the base was created in the manner described in the article. We have already explained the design of the windows in a previous article.

The ring-shaped platform I drew on a 0.5 mm thick PS plate and cut it out with a pair of dividers. For the railing, a mark for a railing support was made every 30° along the outer edge of the platform. The railing was made of 0.5 mm thick round profile.

From a round profile bar, I cut 12 supports to fix them at marked positions on the outer rim of the platform with plastic glue. I used the same material to make a ring with the same outer diameter as a handrail, which I glued to the top of the supports.

The handrail railing at half the height of the handrail was made of slightly bent round profile pieces cut to length and glued between two handrail supports.

A small rectangular recess the same width as the access door was made on the inside edge of the platform at the point that would later be opposite the access door.

Finally completed, it was to be held up by 12 simplified rests underneath the railing supports. Four of them should actually have a load-bearing function, the other eight are only dummies.

The load-bearing parts were made from 0.5 mm thick pins, for which I drilled precisely fitting holes in the walls of the upper part of the tower.

These holes had to be offset by 90° each and were not allowed to be exactly above a window but were offset 15° laterally from their vertical centre.



Then I applied plastic glue to the lower edge of the outer wall of the upper part of the tower, inside the sleeve, and placed it on the pedestal, checked its position and alignment and pressed it on.

Finally, I covered the brick walls of the entire tower with the charcoal glaze. In the upper part I now also fixed the upper circular stabilising plate with glue. The upper edge of the tower and the edge area of the upper stabilising plate were then given a covering charcoal-coloured coat of paint before the work on the water tank was to follow.

## The water tank

The construction of the water tank began with the production of its lower part, which tapers downwards in the side view. I used a smooth 0.3 mm thick PS sheet as material. I took the dimensions for the radii  $R_1$  and  $R_2$  from the side view; the reference point here was the intersection point  $S_2$ .

Due to the low material thickness of the PS plate, I did not correct the radii  $R_1$  and  $R_2$  by the plate thickness. Using the method described above, I also calculated the angle  $\alpha$  for the desired segment.



This oblique view from the later rear side also shows the seam lines. The water tank is almost ready for mounting except for its roof.

The determined values were then transferred to the plate and the drawn ring segment was separated by scoring with a pair of dividers and cutting with a scalpel. Carefully and evenly bent into a conical shape



The water tank has also received a ventilation assembly on the roof. Without the tower pedestal, however, this component would hardly reveal its function right away.

by hand, this ring segment became a truncated cone whose seam I could glue together on the inside with a strap.

Inside, I stabilised the truncated cone at its narrower end, the later water tank base, by gluing in a precisely fitting circular plate and sanded it flat with fine sandpaper to create a level bearing seating surface.

The barrel-shaped upper part of the water tank was also made of 0.3 mm thick PS material. The diameter  $D_t$  of the upper part was measured and taken from the diameter of the lower part at its later upper edge.



From the side elevation I determined the height  $H$  of the upper part of the tank, whereby I added a small amount to consider that the upper edge of the tank would later be covered by the roof overhang.

From the 0.3 mm thick polystyrene sheet I cut out a rectangle with the dimensions  $H * U$  and carefully bent it with my hands into a barrel-shaped hollow body. I closed the seam on the inside of the shell with a glue strap.

From the panel I cut two equal, circular plates whose diameter corresponded to the inner diameter of the barrel. One of these plates I glued into the lower end of the shell to stabilise it. The other plate I put aside for the roof construction.

Originally, I wanted to fix the water tank with an angle profile on the tower. But as this would have changed the proportions of the structure to its disadvantage, I decided against it and placed it directly on the stabilising plate of the tower cylinder. A test confirmed the horizontal position of the upper edge of the water tank and the vertical course of the shell.

To finish this step, I painted the outside of the water tank with a medium grey acrylic paint and imitated with dark brown paint rust stains and rust flows in some spots. Finally, I also covered the water tank with the charcoal-coloured glaze.

## The roof as a capstone

The cone-shaped roof of the water tower was also made from the 0.3 m thick building panel. The necessary dimensions  $R_3$  (radius of the shell) and  $D_k$  (diameter of the cone bottom) could again be taken from the side plan.

This enabled me to calculate the circumference of the cone's surface  $U_3$  and the circumference of the cone at the lower edge  $U_k$  and then also to determine the angle  $\alpha$  of the desired circular segment.



**This is how the water tower looks after the assembly of the water tank. Other parts are still missing.**

Since the edge of the roof was not to be flush with the upper edge of the water tank, but had to protrude evenly, an overhang of 2 mm had to be added to the shell radius R3 when drawing the circle on the PS plate. The angle  $\alpha$  was also drawn into the circle.

Again, I scribed out the surface with a recess compass and drilled a hole exactly in the centre with a 1 mm thin drill bit, which made it possible to reduce unwanted stresses and cracks later when bending the cone shell.

Then I cut out the wedge-shaped segment ( $360^\circ - \alpha$ ) on the drawn lines with a sharp scalpel. I shaped the remaining part of the circle into a cone and fixed the seam with a glue strap, leaving the later roof overhang free. Here the seam was only closed with glue.



On the finished water tower, which had also already received its patina, the door on the platform can also be seen from this viewpoint, from which the maintenance staff exited after using the stairwell in the base.

A small central hole was drilled in the previously prefabricated circular plate (from the previous step in the construction of the water tank). Then I glued it into the roof construction from below to stabilise it so that the roof overlapped evenly. The test on the water tank revealed that the roof was lying straight.

Next, using 1.0 mm wide Evergreen profiles with a semicircular cross-section. I pre-bent a circular piece on a cutting plate, so it was slightly longer than the circumference of the roof. Finally, I glued it on as a gutter, starting at the seam.

Before fixing its end, I shortened the profile to the appropriate length so that a flush gluing was possible. Finally, the roof and the round side of the gutter got a coat of black and dark grey acrylic paint, respectively.

I simplified the ventilation construction on the conical roof a bit compared to the prototype. I took the height and the diameter of the octagonal structure from my side plan. On a 0.3 mm thick piece of board, I drew a strip, the width of which corresponded to the height of the superstructure without the roof.

Then I carefully carved parallel stripes into this strip with a scalpel to indicate the slats. I handled the diameter of the superstructure as if it were the diameter of a circle, calculated the circumference of the circle and divided it by 8. I rounded off the result to half a millimetre and used it as the width of a single side surface of the superstructure.

At the distance thus determined, I scored the strip vertically, cut it out and shortened it to the total length of the eight side faces. The vertical scores were my kinks for the octagon, which was closed at the seam with plastic glue.



For the roof, I scribed a circle with a pair of dividers of the same material, with a slightly larger diameter than the octagon of the tower bonnet. In the centre of the circle, I drilled a hole with the diameter of a pin and squeezed the surface of the circle to form a flat cone.

A wooden block with a hole smaller than the diameter of the circle and a slightly conical brush handle served as stamp to apply pressure. I stuck a suitable pinpoint into the formed cone from below, secured it with some plastic glue and glued the roof onto the ventilation unit. After drying, I mounted the ventilation unit on the roof of the water tank. Now the ventilation unit got a coat of black acrylic paint as well.

## Setting up platform and roof access ladder

The next step was to assemble the circular platform. Into each of the pre-drilled holes in the top of the tower I inserted a pin cut to length so that they protruded horizontally and at exactly the same distance (4.5 mm) on the outside and secured them with plastic glue on the inside of the tower.

From a 0.5 mm thick round profile bar I cut eight dummy pieces of the same length (4.5 mm). I coated the protruding ends of the pins with plastic glue and placed the finished circular platform on it from above.



From this perspective, other parts described in the construction report can be seen: the downpipe, the access ladder to the roof and the water level indicator.

I aligned it so that the seam of the railing faced backwards (the direction of the seams of all tower parts), the gap between tower and circular platform was even everywhere and each of the pins came to rest exactly under a railing post.

After the glue had dried, I turned the tower upside down and reinforced the four glued joints with super glue. After this had also set, I glued the round profile pieces that don't carry weight with plastic glue to the underside of the circular platform, with one end touching the tower wall and the other end pointing towards a railing post. Finally, the complete circular platform with railing and supports also received a dark grey acrylic coating.

From a very thin piece of leftover panel, I made a small door that closes the access from the upper part of the tower to the circular platform. It was painted in medium grey and glued to the outside of the tower, opposite the recess in the circular platform. From the small remainder of an etched ladder, I formed an access ladder, which I glued as a connection from the lower edge of the access door to the recess in the circular platform.



The water tower is located on the layout and has been integrated into its surroundings.

Next, I coated the bottom of the water tank with Noch grass glue and glued it, again with the seam side facing backwards, to the centre of the stabilising plate of the upper part of the tower.

In the original, a ladder made it possible to climb from the circular platform up to the roof of the water tank. This ladder was supported by two metal rails arranged in a ring. The upper rail was attached to the water tank below the roof, the lower rail to the upper part of the tower at the height of the circular platform. In this manner the ladder could be rotated almost all around the water tank.

In a simplified way, I made the ladder out of 0.5 mm wire scraps and an etched ladder by making the lateral supporting elements out of wire and fixing the ladder in between with superglue. The finished ladder was attached to the tower with BUSCH grass glue.

From the gutter a downpipe ran vertically along the water tank, which then disappeared diagonally at the top of the tower through the outer wall into the interior of the tower. I copied this with a 0.5 mm thick round profile, painted it dark grey and placed it according to the original.

The water tower in Ottbergen had, as usual in the steam locomotive era, a water level indicator that could be read from the outside. This was controlled by a float on the water surface, which was connected to the indicator via a rope through a small opening in the roof and via several guide rollers. This allowed it to move vertically in front of the large scale.



Unfortunately, a scale reproduction of this rope guide would have turned out too clumsy, which is why I refrained from doing so. Therefore, I limited myself to the eye-catching display scale. From glossy photo printing paper, I cut out a long rectangular piece and drew in the scale and the horizontal pointer with a very fine pigment ink pen. After painting the back in medium grey, it was attached to the railing of the circular platform.

This completed the work on the model. Before placing it on the layout, I glued the placeholder in its intended position with white glue. To do this, I aligned the octagonal piece of cardboard with the help of its centre line and pointer so that the tower entrance later pointed in the desired direction.



The eyes wander over the likewise self-built locomotive administration, the water crane and coal barn to the water tower as the highest building of the depot. This part of the layout appears harmonious and coherent, which is largely influenced by the matching buildings.

After designing the surroundings, I put a few drops of NOCH grass glue to the underside of the base plate, lowered the water tower from above onto the placeholder, looked for the right position by turning it slightly, and then pressed the tower on lightly. Since then, it has performed its role in the depot to my complete satisfaction.

All Photos: Jochen Brüggemann

Material supply sources:  
<https://www.faller.de>  
<https://www.noch.de>





# Faszination Modellbau

**3. – 5. NOVEMBER 2023**

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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

## Die Geschichte der preußischen G 10 **Leistungsfähiges Leichtgewicht**

**Die G 10 war dem leichten Oberbau vieler Länderbahnstrecken geschuldet. Überall, wo die G-8-Reihe zu schwer für einen Einsatz war, bedurfte es der fünfachsigen Schwester. Preußisch sparsam und wirtschaftlich konzipiert, erreichte diese Baureihe ein beachtliches Alter und war schließlich auch auf Nebenstrecken anzutreffen. Der EK-Verlag spürt ihr (mit zwei Bänden) nach.**

Hans-Jürgen Wenzel  
Die Baureihe 57<sup>10-35</sup>  
Band 1: Entstehung, Technik, Einsätze bis 1945

EK-Verlag GmbH  
Freiburg 2022

Gebundenes Buch  
Format 21,0 x 29,7 cm  
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Zu einem neuen Buch der „blauen Reihe“ aus dem Hause des Eisenbahn-Kuriere, so könnte man meinen, ist nicht allzu viel zu sagen. Gewohnt hoch ist das fachliche Niveau, bislang gab es davon keine einzige Ausnahme.

Doch weit gefehlt! Zwar haben wir auch hier keinen Ausreißer gefunden, aber einfach nur ein Prädikat zu vergeben, erschien uns dann doch zu einfach. Die Inhalte dieses Titels, dessen zweiter Band vor wenigen Tagen folgte, möchten wir schon zusammenfassen und vorstellen.

Oft heißt es, lange zu warten, bis ein neu angekündigtes Buch dann endlich ausgeliefert wird und sehnsüchtig wird es dann verschlungen. Auch hier macht das Lokportrait der preußischen G 10 keine Ausnahme. Etwas aus der Reihe fällt es allenfalls dadurch, dass ihre Geschichte eben in zwei Bänden zu präsentieren ist und dafür gibt es gute Gründe, die wir noch herausarbeiten wollen.

Mit Hans-Jürgen Wenzel greift der Verlag auf einen seiner bewährten und bekannten Autoren zurück, der die Dampflok noch aus eigenem Erleben kennt und fundierter Kenner der Materie ist – gut vernetzt und mit den Archiven vertraut, die es zu durchstöbern gilt.

Alles, was das heute vorgestellte Werk, das dabei herausgekommen ist, dokumentiert und vermittelt, ist wieder einmal von einer exzellenten Bildauswahl begleitet. Die historischen Aufnahmen sind ebenso gut aufbereitet und reproduziert worden.



Bedingt durch deren Alter, nicht selten mehr als einhundert Jahre, sind sie ausnahmslos schwarz-weiß, kein einziges Farbbild konnte hier Einzug halten. Doch das ist zu verschmerzen, denn für Band 1 wurde ein harter Strich mit dem Ende des Zweiten Weltkriegs 1945 gezogen.

Die Geschichte der preußischen G 10, die ab 1925 als Baureihe 57<sup>10-35</sup> geführt worden war, begann Anfang des 20. Jahrhunderts. Erfolgreich etabliert war die Gattung G 8 in ihren unterschiedlichen Erscheinungsformen. Doch deren Achsdruck war einst längst noch nicht für alle Strecken tauglich. Die G 10 kam ebenfalls ohne Laufachsen aus, erhielt aber eine fünfte Kuppelachse, dank derer ihr Achsdruck auf 15,3 Tonnen begrenzt blieb.

Der Weg zur Konstruktion, ihre technische Beschreibung und Indienststellung sind ausführlich in der vorliegenden Lektüre dargestellt. Gebaut ab 1910, erlebte die G 10 die Wirren des Ersten und Zweiten Weltkriegs, die beide viel Einfluss auf ihre Betriebsgeschichte nehmen sollten und den ersten Band so prall mit Inhalten füllen.

Bis 1924 wurden immerhin mehr als 2.500 Exemplare von insgesamt neun Lokomotivherstellern gebaut, auch aus dem Ausland gab es Nachfrage nach dieser Konstruktion. Auch in die Fänge der Alliierten gerieten sie als Waffenstillstandslieferung, Militärtransporte brachten sie zuvor (und später im Zweiten Weltkrieg) aber zusätzlich außerhalb der Reichsgrenzen.

So waren sie beinahe in ganz Europa verstreut und bei verschiedensten Staatsbahnen im Einsatz. Der Autor spürt ihnen allen nach und konnte dafür neue Erkenntnisse und weitere Archive nutzen, die über Jahrzehnte nicht zugänglich waren.

Dieser Hinweis ist insofern wichtig, als dass es sich um eine überarbeitete und deutlich erweiterte Neuauflage eines Titels aus dem Jahr 1979 handelt. Der über vierzig Jahre alte Vorgänger ist mit diesem Buch endgültig reif fürs Altenteil.

Grund dafür sind die eingearbeiteten Informationen, die bereits genannt wurden, und den Inhalt nach Seitenzahlen glatt verdoppelten. Das war nicht gleich zu erwarten, machte das Aufteilen auf nun zwei Bände denn auch erst erforderlich.

Die aufgrund ihrer geringen Achslast, robusten Bauweise und Leistungsstärke so vielseitig einsetzbare und langlebige Dampflok hat das aber auch verdient. Sie ist bis heute bekannt und auch bei Modellbahnern geschätzt – in der Spurweite Z ist sie bislang aber allenfalls über einen Eigenbau zu haben.

Die geeignete Basis lässt sich ebenfalls aus den Inhalten des Buches ableiten, denn ausführlich wird dort auch behandelt, worauf die Konstrukteure zurückgreifen konnten und aus preußischer Sparsamkeit heraus auch wollten.

Grenzen wir abschließend noch kurz den Inhalt gegen den in einer Folgeausgabe zu besprechenden Band 2 ab: Vollständig aufgelistet werden im Band 1 die in Deutschland gebauten Fahrzeuge. Die beschriebene Einsatzgeschichte umfasst die Länderbahnzeit und Ära der Deutschen Reichsbahn bis zum Ende des Zweiten Weltkriegs. Kriegseinsätze und Auslandsverbleib bis 1945 gehören freilich auch dazu.

Band 2 wird dann 1945 ansetzen und vor allem die DB und DR im Osten Deutschlands behandeln. Auch Auslandseinsätze nach 1945 sind dorthin gewandert. In Summe liegt damit ein gelungenes und ausführliches Werk vor, das für sich noch unvollkommen bleibt und Lust auf seine Fortsetzung macht. Und genau dem werden wir uns nun auch widmen!

**Publishing pages:**  
<https://www.eisenbahn-kurier.de>  
<https://www.ekshop.de>



## Zweiter Teil zu DRG-Dieseltriebzügen **Bindeglieder zu Schnelltriebwagen**

*Der vor anderthalb Jahren erschienene, erste Band einer VGB-Buchreihe zu den Dieseltriebwagen der Deutschen Reichsbahn hatte uns angesprochen. Maßgeblich dafür war auch einer der beiden Autoren, der uns aus eigenem Erleben bekannt war. Mit Spannung warteten wir auf die Fortsetzung, die das Bindeglied zu den Schnelltriebwagen darstellt, mit denen die Vorkriegsentwicklung dann später abzuschließen sein wird.*

Dirk Winkler | Günther Dietz  
Verbrennungstriebwagen der Deutschen Reichsbahn  
Band 2 – Triebwagen in Leichtbauweise von 1932 bis 1945

VGB | Geramond Media GmbH  
München 2023

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Im Januar 2022 hatten wir Band 1 einer neuen VGB-Reihe über Verbrennungstriebwagen der Deutschen Reichsbahn vorgestellt und dies als Start für neue Standardwerke bestätigt. Inzwischen liegt auch der zweite Band vor, den wir heute besprechen möchte.

Zunächst fällt leider auf, dass das vorliegende Buch rund dreißig Seiten weniger als das erste Buch der Reihe aufweist und auch mit etwa vierzig Bildern weniger auskommen muss. Gleichzeitig ist der Preis, den der Verlag verlangt, um satte zehn Euro gestiegen.

Das möchten wir nicht als K.o.-Kriterium missverstanden wissen, aber auch nicht einfach mit „allgemeiner Preissteigerung abtun“. Für einen um 20 % gestiegenen Preis müssen Verlag und Autoren dem Käufer aber auch etwas bieten. Genau dem werden wir nun auf den Grund gehen.

In Erscheinung treten wieder dieselben beiden Autoren wie schon beim Band 1, was aber letztmalig so sein wird. Auffällig ist schon, dass sie auf dem Buchdeckel ihre Reihenfolge getauscht haben. War diese zuvor alphabetisch, so steht nun Dirk Winkler vorn.

Der Anlass dazu wird (leider) schon im Vorwort deutlich: Der auch von uns sehr geschätzte Autor und Triebwagen-Kenner Günther Dietz ist während der Arbeiten an dieser Fortsetzung kurz vor Erreichen seines 84. Lebensjahres verstorben. Von seinen tiefen Kenntnissen weit über das heimische Gebiet der Deutschen Reichsbahn in der DDR hinaus hatten auch wir uns aus anderen Publikationen und durch persönliche Hilfe bei Recherchen vor rund dreizehn Jahren persönlich überzeugen können.

Erfreulich ist, dass sein Wissen hier noch maßgeblich einfließen konnte und sicher auch im weiteren Verlauf der Buchreihe spürbar bleiben wird. Für den künftigen Alleinautor wird die Herkulesaufgabe gewiss nicht leichter. Mitgewirkt haben aber auch weitere Kenner der Materie wie Rolf Löttgers aus Siegen, denen im Vorwort gedankt wird. Und so liegt unsere Messlatte an dieses Buch sehr hoch.



Band 1 behandelte Trieb-, Bei- und Steuerwagen, die bei den Länderbahnen und der DRG bis 1930 entstanden waren. Oft hatten sie noch einen experimentellen Charakter und waren für Nebenstrecken konzipiert.

Der nun vorliegende, zweite Band knüpft nahtlos daran an, auch wenn das an der Jahreszahl 1932 nicht gleich deutlich wird. Dass dies keine zeitliche Lücke darstellt, wird am ergänzenden Hinweis zur Leichtbauweise deutlich.

Mehr und mehr verschiebt sich damit das Wesen der Fahrzeuge zu vierachsigen Drehgestellfahrzeugen für Hauptbahnen, auch wenn Zweiachser und Nebenbahnkonstruktionen weiter zu finden sind. Doch auch sie folgen zunehmend neuen Erkenntnissen und Fertigkeiten jenes Jahrzehnts bis zum Kriegsausbruch.

Wie zu erwarten, blieb das Präsentationskonzept mit historischen Fotos, abgrenzenden und erläuternden Typenskizzen und ergänzenden Abbildungen von beispielsweise Motoren sowie ergänzender Tabellen erhalten. So bedarf es keines Umgewöhnens beim Suchen und Nachschlagen in nun zwei Enzyklopädiën.

Die Bild- und Wiedergabequalität entspricht unseren Ausführungen zum ersten Teil. Was nicht perfekt ist, ist dem Alter geschuldet und historisch einfach nicht verzichtbar. So fanden auch viele zuvor unveröffentlichte Aufnahmen Eingang in dieses Werk, wofür den Autoren unser Dank gilt.

Vermisst haben wir bei den behandelten Fahrzeugen nichts, allerdings sind die Schnelltriebwagen der DRG nicht in diesem Band enthalten! Das mag ein Käufer angesichts des zeitlichen Rahmens vielleicht anders erwarten, aber die fliegenden Züge gehören technisch nicht zu den hier behandelten Fahrzeugen.

Anders sieht das bei Aussichtstriebwagen und Güterschlepptriebwagen aus, zwei Sonderbauarten, die auch schon das Eigenbauinteresse von Spur-Z-Freunden geweckt haben. Auch Schmalspurfahrzeuge sind Bestandteil dieses Werkes.

So sind nach unserer Kenntnis tatsächlich alle Typen in Wort und Bild vorgestellt, die zum Titel dieser Lektüre passen. Ein dritter Band ist damit allerdings nicht nur wahrscheinlich, sondern auch zwingend erforderlich.

Die portraitierten und beschriebenen Bauarten dokumentieren den Weg vom Nebenbahn- bis hin zum mehrteiligen Hauptbahntriebwagen, die den bereits angesprochenen „Fliegenden Zügen“ erst den Weg ebneten.

Ein weiteres, ausgesprochen spannendes Kapitel deutscher Eisenbahngeschichte ist hier Thema eines Werkes, das wir im Umfang zwar etwas größer erwartet hatten, aber dennoch keinen Wunsch offen lässt und schon gar nicht Lücken offenbart.

Insofern kommen wir zu den Überlegungen am Anfang dieser Rezension zurück: Der Preis lässt sich durchaus als happig wahrnehmen, steht aber im Verhältnis zu Quantität und vor allem Qualität des Inhalts in einem ausgewogenen Verhältnis.

Deshalb freuen wir uns, dass der Verlag die Reihe gelungen fortsetzt und sicher ebenso würdig abschließen wird. Bleiben wir gespannt, ob dies bereits mit einem dritten Band erfolgen wird oder doch noch einen vierten Teil erfordern wird.

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<https://www.vgbahn.shop>



Black Forest Railway Museum

## Very small meets very big

*This year's special exhibition "Small meets big" at the Eisenbahnmuseum Schwarzwald (Black Forest Railway Museum) has been advertised in our magazine, and Trainini® was happy to join in as a supporter. We were naturally tempted, therefore, to visit Schramberg, in person, to talk to the people involved and to report back to our readers on the experience.*

Schramberg, located in the Württemberg part of the Black Forest, is not exactly a household name. And yet it has a special history, because it is home to the famous watch manufacturer Junghans, which was once the world's largest manufacturer of timepieces.

Former company buildings in the H.A.U. industrial park, that were no longer needed for production, later became the property of the municipality and now house various museums. Among them, in building 22, is the Black Forest Railway Museum, whose name can be misleading. After all, everything there revolves around model railways and not the 1:1 scale prototype.



In the the Eisenbahnmuseum Schwarzwald (Black Forest Railway Museum), (almost) everything revolves around gauge 2, and with it primarily around the complete self-made construction of locomotive and wagon models as well as their individual components. The unmistakable favourite of the active members is the DB's class V 200<sup>0</sup>.

The "Interessengemeinschaft Spur II, Regionalgruppe Süd", is the operator of the 742 m<sup>2</sup> exhibition area. The core of their exhibition is the world's largest collection of 2 Gauge models.



When it comes to this scale (1:22.5), The only commercially available narrow-gauge vehicles for this scale (1:22.5) are from the LGB brand, while no comparable products exist for standard-gauge rolling stock, at least for authentic looking ones.

Thus, the 170 metres of glass showcases comprise around 900 such self-made constructions. Each of them consists of up to 10,000 individual parts, which have also been produced in-house over several years of work. A 400 m<sup>2</sup> layout shows some of them in operation. There they make for a fascinating sight with their sheer their size and smooth running properties.



LGB is also represented in the exhibition, among others with this fanciful steam train based on Wild West themes.

The museum has been in operation for eleven years already, with the beginning of this courageous commitment (which also involves a significant financial risk for the association) dating back to 26 October 2012 - exactly 35 years to the day after the end of steam locomotive era in West Germany.

And steam traction does indeed find a worthy place in the rooms of the exhibition: we look into the former Braunschweig repair works and experience a trailing steam locomotive during supply runs in the depot. Museum director Michael Herberger emphasises that seeing trains in active operation is a key part of the museums philosophy. After all, a model railway is always about wheels in motion.

Other objects also include by the somewhat smaller "royal" gauge 1 (scale 1:32), which is manufactured industrially and is represented here, for example, by models from Kiss, KM1, and Märklin. And the latter manufacturer was then also the cue for a new experiment, which, as it turned out, has been successful.





The museum houses 170 metres of showcases which are filled to the brim with models in 1 and 2 scale. And 900 of these models were built from scratch.

Expanding to Gauge 1 allows for a comparison to the sometimes huge-looking (and usually also very heavy) models of the core exhibition. This creates the opportunity for visitors to understand and appreciate the exceptional achievement of scratch building in such a scale.

But it was the arrival of the Z gauge in the form of the Märklin Mini-Club that completed the picture. The occasion was the 50th anniversary of Z scale last year. The special exhibition that runs for a year (after which it changes again) was therefore also dedicated to it, and our magazine has been promoting it, repeatedly.

The fact that 2 gauge is almost ten times larger than 1:220 scale creates the hoped-for visual contrasts that promise even more excitement. This was made possible by two key persons: Firstly, Eckard Jehle was looking for a permanent home for his home layout, which we had presented in **Trainini®** 8/2020.

He is a native of Schramberg and our layout portrait proved to be a decisive reference, as he told us. It was through this article that he and the people in charge came into contact. In the end, the layout was moved from Karlsruhe to the Black Forest Railway Museum. Together with incoming donations of rolling stock, this provided a basis for the expansion of the museum exhibition.

On the other hand, there was still a lack of a person who is familiar and experienced with Z gauge. This person should take on the task of maintaining and, if necessary, repairing the rolling stock, but also be able to demonstrate the layout and instruct the museum staff sufficiently in its operation.



Bringing Z scale into the exhibitions is attractive because of the deliberate contrast with the large models. The differences in size as well as the advantages of both scales are thus best perceived by the visitor.

The person they were looking for was Klaus Sieber, since they had already made the acquaintance of museum director Michael Herberger. So, the basic conditions were fulfilled and nothing stood in the way of the special exhibition.

That this sounds easier in retrospect than it actually was to prepare and organize, however, becomes clear from one fact: The desired, one-year special exhibition on the 50th anniversary of the Mini Club did not begin until March 19, 2023, and thus after the end of the anniversary year. We must also not forget here that 2022 was still characterized by contact restrictions.

And, so, we are all the more pleased that everything has come to a happy conclusion and that the juxtaposition of the two extremes in model railway scales got off to a good start. To whet your appetite for your own visit, we therefore briefly summarize what will be on offer on site.

On the one hand, there are showcase presentations with selected items from 50 years of model railroad history. In the case of rolling stock, the Z-gauge models, which document progress in painting, printing, and detail reproduction, are always juxtaposed with a larger gauge, usually gauge 1.

In this way, the visitor is well aware of the advantages that distinguish the respective scale: on the one hand, uncompromising attention to detail and impressive size, on the other hand, unabridged passenger cars that line up to form equally impressive long trains.



But interesting and sometimes rarely seen accessories are also part of the museum's exhibition. These include, for example, the Fallerhof in the Upper Black Forest, which is hardly known to most readers as a miniature, from the regional television series of the same name.

However, the best advertisement for the small scale is certainly the already mentioned model railroad layout by Eckard Jehle, which has found a new home here. As the central object of the special exhibition, it demonstrates the advantages of our size particularly impressively.



**The Alpine layout by Eckard Jehle is also surrounded by 1 gauge models. With its extraordinary and impressive rock and mountain design, it is a demonstration of what would not be possible in any of the other scales on a home layout.**

Visitors entering the room of this special exhibition, will be immediately presented with the long side of the layout, i.e., its full extent. Since it is not exactly small for a private layout, it is likely to create a lasting impression on the viewer.

But that's not all, because the layout's height is also cleverly designed. The authentic look of an alpine landscape can only be achieved in 1:220 scale. In any other scale, such a layout would probably have to do with cute little hills with barren rocks that cannot really convey the crossing of the tree line.

An excellent scenic backdrop and the realistic design of the rocks and mountain peaks, garnished by a cable car that leads up from the valley and halves the distance hikers have to walk, also contribute to the success of this layout.

Of course, many structures on the layout are not quite adapted to our scale and many modelers today would resort to other kits.

But here, too, a special touch is evident: After all, the layout is about thirty years old and, despite being in near-perfect condition, is thus itself a part of the history depicted. It illustrates what was once possible and what is still possible today with some creativity and talent.

continues on page 48





Photos on pages 46 & 47:

The alpine backdrop of the layout is unique and evokes memories of mountain holidays. The builder skilfully managed to create the impression of high mountains despite the limitations of a model railroad layout, even at this scale, and to create an authentic looking illusion. And in the middle of this landscape, we see prototypically long express trains with true to scale passenger cars.







By the way, family members traveling along without an interest in model railroads will not be bored either. They, too, will be spellbound by the layout operation, which of course also includes the larger side room with tracks 2, 2m and 1. For the younger visitors, there is even a short trolley track where they can test their muscle power.

And if all this is still not enough, there is still the option to visit to the Museum of Inventions, Watches and Automobiles across the street, because these four exhibitions are part of a presentation network that can be visited on a single ticket.



Tunnels and galleries to protect against debris avalanches naturally also belong on an Alpine layout. It was designed according to the school of Bernhard Stein, which was the measure of all things in the nineties.

Things will continue to be exciting at the museum even after the end of the current special exhibition: Museum director Michael Herberger revealed to us that there is always one that changes annually. And at least the next one, in response to the extraordinarily good reception, will probably also cover Z gauge again.

So, we can't go back to business as usual either. Although we have been let in on the plans and are also deliberately keeping quiet in order to maintain the suspense, our fingers are still itching (to operate the shutter release of the camera).

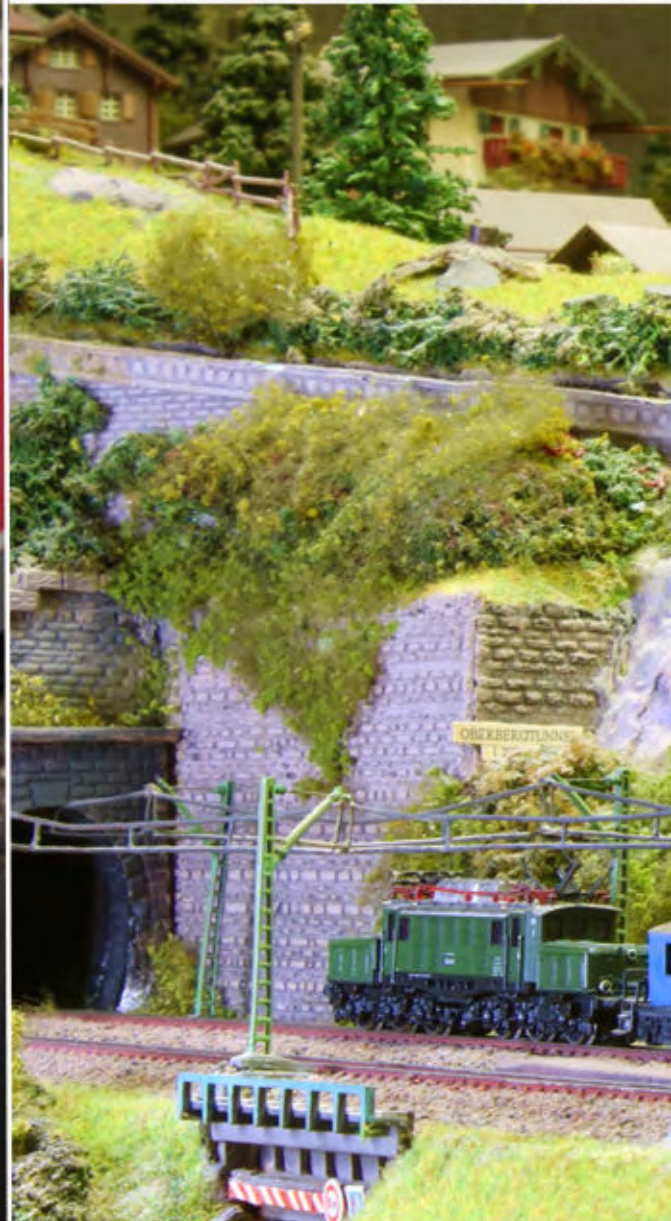
So, we wait and see if all plans can be realized as thought and quickly. And when the time comes, we probably won't be able to avoid another visit. The trip to the Black Forest has been worthwhile for us without any doubt!

All information for a visit:  
<https://eisenbahnmuseum-schwarzwald.de>



# SONDERAUSSTELLUNG KLEIN TRIFFT GROSS

50 Jahre Baugröße Z, 1:220  
19. März 2023 bis 17. März 2024



**märklin**



**Schramberg**  
Schwarzwaldqualität erleben



## Besuchsinformationen:

[www.eisenbahnmuseum-schwarzwald.de](http://www.eisenbahnmuseum-schwarzwald.de)  
Infotelefon +49 (0) 74 22 / 29 - 3 00

Gewerbepark H.A.U. 22  
78713 Schramberg





Readers' letters and messages

## Zetties and Trainini in Dialogue

*Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.*



A thank you and pleasure with our report:

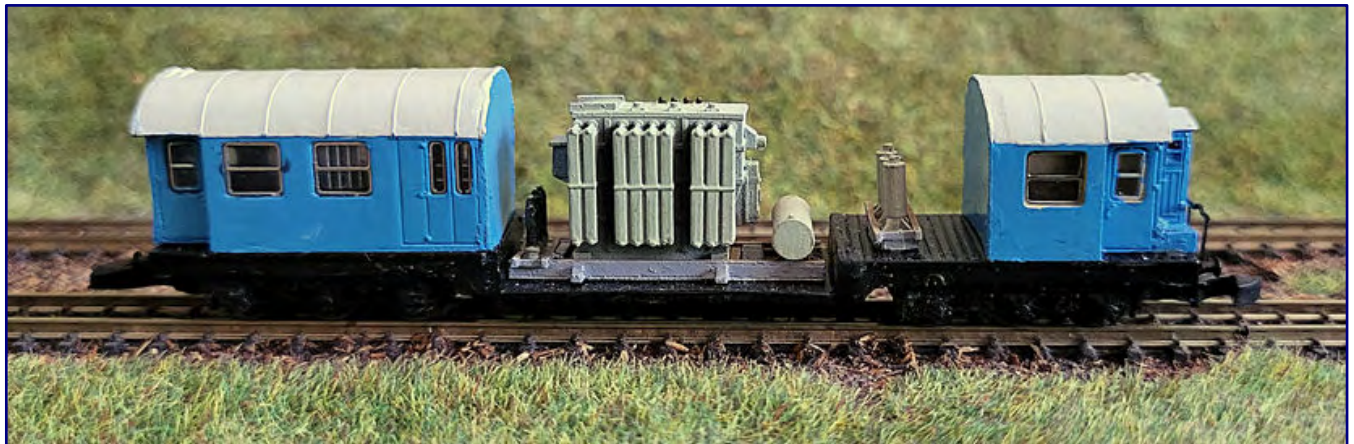
I would like to thank you very much for this great report about "Werdersheim". And then also a cover photo. Super.

But the other topics are also a pleasure to read. Trainini is and remains a great magazine. Many thanks.

Dirk Rohwerder, Sprockhövel

Inspired by "Railway Service and Service Freight Cars (Part 1)":

Inspired by the above-mentioned book by Stefan Carsten and Wolfgang Henn, I had the idea to realize such a mobile undercarriage shortly after purchasing and looking through the book. I got an SSym 46 and an AEG transformer from Artitec, then I started: Split the SSym undercarriage, built a suitable flatbed out of plastic.



A mobile undercarriage on the basis of a low loader wagon provided the template for this self-built model of our reader. Photo: Bernd Jablonski

Housing for the superstructure was quickly found - 3-axle conversion car; housing machined, new walls created from thin plastic, the whole thing then painted in a "faded" traffic blue and mounted on the car. Test runs were successful and the car can now be used in a construction train.

Bernd Jablonski, Bochum



Free and without suitcase:

Please find attached a photo of my new layout: 80 x 56 cm. Not finished yet, motto: "It doesn't always have to be a suitcase layout."

Stephan Viragh (Hungary), by E-Mail



80 x 56 cm measures the new layout of our reader from Hungary. It was deliberately not built in a suitcase. Photo: Stephan Viragh

Editor's reply: Please keep us informed if there are any news! We are grateful for news or even a suggestion for the portrait of the finished layout.



Harald Fried is the new man on the editorial team.

#### Personnel reinforcement for Trainini®:

The editorial staff situation has been enormously strained for several months. Necessary changes in the issue planning due to delivery delays of the manufacturers lead to stronger time demands, which at the same time meet with scarce free time and a decimated group.

For the past five months, there has been a shortfall, and at times the deserved vacations of members of the editorial team could not be fully reconciled with the tasks they had to perform.

For a long time, we have been looking for a solution internally, including the entire team of editors, translators and Trainini TV, without any loss of quality. Finally, we are pleased to announce that we have found a suitable reinforcement.

Initially, on a temporary basis, Harald Fried, whose name may be familiar to some readers from the Märklin 2022 anniversary book, is supporting our editorial team and noticeably strengthening it. With this month's issue, he is taking over proofreading, and would also like to continue to look around with us and contribute wherever he can and enjoys doing so.

If it suits everyone involved, he has the option of joining the editorial team on a permanent basis, if he wishes, and contributing his knowledge and skills for our readers. From his professional life, he brings years of relevant experience from the fields of marketing and public relations.

We would like to take this opportunity to extend a warm welcome to Harald Fried and say "auf gutes Gelingen!"

#### Seasonal New Products at Noch:

"City, country, river" is the annual motto of the accessories manufacturer Noch from Wangen in Allgäu. Now, it has presented matching Seasonal New Products, which also include products for Z gauge.



The pine tree with 15 cm growth height (Item No. 20140) from the Master series is one of the seasonal new products from Wangen. Photo: Noch

The first thing that catches the eye are the trees of the Master series, a large part of which can also be used in 1:220 scale, because the vegetation in the larger gauges is reproduced almost through the series for space reasons considerably too small.

For example, we consider the ash as an extremely tall solitary tree (Item No. 20100, 20 cm), the oak (20110, 15 cm), the poplar (20130, 18 cm), pines (20140, 15 cm / 20141, 18 cm), the copper beech (20150, 15 cm), which in our view is incorrectly declared as a "copper beech", and spruces in three lengths (20190, 19 cm / 20191, 22 cm / 20192, 25 cm) to be suitable. If you have any doubts about this, you are welcome to consult the technical literature for these trees.

Then there are four new varieties of grass tufts to be added: multicoloured flocked they are in the brightly coloured variety "flowering" (06900), while in late summer they appear as "dry grass" (06910). Obviously, we looked into the bush program for the two varieties "stony" (06920 / 06921). In each of these compilations are 25 tufts with 6 mm fibre length.

Noch has now also developed its own building lighting system and offers this as the starter pack "micro-rooms LED building lighting set" (51250). This contains four different LED boards and eight interior rooms.

These "interiors" are light boxes of different sizes made of cardboard, which are folded and glued. They are then further processed similar to the Viessmann products. Also included are instructions, a diffusion foil and seven decorative sheets for designing the floors and walls.

If you need more lamps, you will find four or single LEDs (51270 / 51275) or double LEDs (51280 / 51285) per package in the light colours cool white and warm white (article numbers are listed in this order). Eight light units of these versions are also available (51271 / 51276 / 51281 / 51286).





The package "micro-rooms LED building lighting kit" (51250) contains everything needed for the new system. Photo: Noch

New LED light chains with 10 (51240), 20 (51244) or 30 (51248) light sources each have a length of 10, 20 or 30 cm and are intended as a lantern garland for festivals or as stall lighting for a Christmas market. Further ideas for use are up to the creativity of the customer.

#### Märklin deliveries since the last issue:

Märklin delivered the "compartment car" car pack (Item No. 87565) during the reporting period. This now supplements the baggage car of Prussian origin (87566) that has already been available since July. For the first time since 2005, the four-axle compartment cars with DB markings (Era III) found their way back into the program.

The main difference between these cars and the earlier model is that the Bundesbahn inscriptions do not have an Ege cookie imprinted on them. This is the only thing that corresponds to the historical prototype, because the cars with wooden bodies were no longer allowed to be used in passenger trains after 1962 at the latest.

Their early retirement was thus already a foregone conclusion, which is why the DB decided not to affix its logo (as it did with other types and series). In contrast to their three-axle siblings, the long compartment cars disappeared from the rails much earlier and supplied parts for the young German Federal Railroad's conversion car program.



In addition to the now missing Ege cookie, the new compartment cars are especially impressive with their fine printing and frame embossing, here the 1st/2nd class car AB4 pr 04 (from item no. 87655).

So, almost 20 years after its first edition, Märklin is now showing the courage to consistently follow the historical templates and to trust in sufficient prototype knowledge of its customers.

The car set consists of one example of the type AB4 pr 04 with 1st/2nd class and brakeman's cab and three B4 pr 04 (2nd class), one of which also has a brakeman's cab. All four cars are painted in bottle green (RAL 6007). The bogies correspond to the prototypical Prussian standard design. The much finer reproduction of the frames on the trap windows is pleasing to the eye.



**Photo above:**  
The new “compartment car” set (87655) comes in four pieces, and only the corresponding pack car is sold separately.

**Photo right:**  
A first tranche has also become available of the Mobile Station WLAN (60667) together with its complementary receiver box WLAN (60117).



About half of the Insider annual cars 2023 (80333) have arrived at the dealers. The model was once again a covered freight car of the GI Dresden type, used on the German Federal Railroad around 1972 (Era IV). It carries an advertising inscription “SABA” and has neither a brakeman's cab nor a platform.

The delivery of the Mobile Station WLAN (Art. No. 60667) has also begun. If the device is not operated with a Central Station, the WLAN receiver box (60117) is still required for operation. So far, only a first tranche of both is available; currently, they are not available ex-works. So, it will probably take a few more weeks until all customers will have access to them.

In addition to the wireless MS, the WLAN receiver box also enables the start-up infrared control units to be coupled and integrated for layout operation. In total, it can even couple up to four MS WLAN.

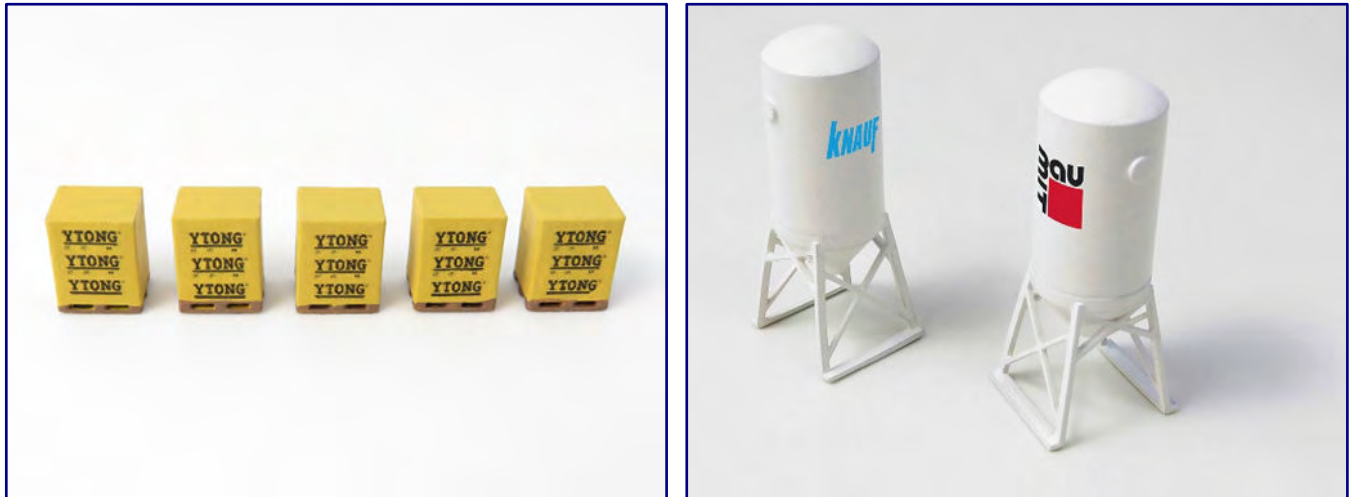
Configuring the devices should be easy via a web application or Central Station. We will test the devices extensively and report on our experiences with regard to Z gauge requirements.



## Useful things at Yellow Dwarf:

Yellow Dwarf (<https://www.yellowdwarf.eu>) has reported four new items that are very useful on the layout: Tree grids (item no. 60070) can probably be found in every city around trees planted in squares or other highly frequented areas.

But construction sites also offer their charm when they are loaded with the necessary construction material such as the pallets with Ytong blocks (60201) or lever blocks (60202). But they can also be used as loads on road and rail vehicles or in reloading scenes at the freight shed.



Among other things, pallets with Ytong blocks (Item No. 60201; photo left) and stand-up silos (60225; photo right) help equip Z-gauge construction sites. Photos: Yellow Dwarf

Back on the construction site, it is advisable to set up pedestal silos (60225), which form the fourth and last new product of this month.

## In search of personnel:

Modellbahn Union is looking for new workers for two positions at its Issum production site. Those who would like to turn their hobby into a profession can apply there as model railway technicians (N gauge) or as diorama builders. The latter position is to take care of the construction of new developments and their presentation.

Two short films about the job description at this provider are posted at the following links:

- Modelleisenbahntechniker (model railway technician) ([https://youtu.be/yI03wSnxcgk?si=\\_dyEgZZ5dgp7rwCb](https://youtu.be/yI03wSnxcgk?si=_dyEgZZ5dgp7rwCb))
- Dioramenbauer (diorama builder) (<https://youtu.be/y3EETNbiQAq?si=GT2W9m9EhLfIDoz4>)

You can reach the Model Railway Union at <https://www.modellbahnunion.com>.

Two new brochures by Stefan Carstens on open freight cars are announced as new items, available probably from October 16, 2023 (Part 1) and spring 2024 (Part 2).

## Faszination Modellbau at Bodensee (Lake Constance):

The Faszination Modellbau trade show will again be held in Friedrichshafen together with the Echtdampf-Hallentreffen (live steam meeting) from November 3 to 5, 2023. 22 model railroad layouts await visitors there, including a 24.5 x 8 metre module layout in Z gauge.

The “US Z-Lines Europe” are responsible for this giant exhibit based on models from North America and thus help to make this exhibition a visitor magnet for our scale. Trains up to 11 meters long with multiple diesel locomotives will run on this layout and clearly show the fans of larger scales what they lack the space for.



At Faszination Modellbau, US Z-Lines Europe want to wow visitors with seemingly endless trains. The layout on display also has record-breaking dimensions. Photo: Stephan Fuchs

Visitors will also not have to do without such popular steam locomotives as the “Challenger” or the “Big Boy”. In total, they show, as well as the separately built joint stand of the Z-Freunde International e.V. with other layouts and exhibits of various participants, what is feasible in our nominal scale.

For information on tickets, prices and opening hours required for a visit, please visit <https://www.faszination-modellbau.de>.

#### **Exciting from AZL:**

The National Train Show in the United States of America is the most important model railroad exhibition of the year. Traditionally, new products are also announced and presented there. American Z Lines makes no exception.

They are presented in more detail in a film report at the following web address: <https://youtu.be/dkPqhoBwjk0?si=gsgUKsIR5X75nlkC>. Topics in it are the EMD diesel locomotives SD50, SD60 and SD60M, the long-distance train “Coast Daylight” of the Southern Pacific including the legendary steam locomotive GS4, bulk freight cars “Ortner” and covered, modernized 40-foot AAR freight cars with single doors.



Out for delivery this month is FRISCO's striking EMD E8 with two different car numbers and locomotive names (Item Nos. 62621-1 / -2). The Trinity RAF column cars now carry 53-foot FedEx Multi-modal trailers (905202-1). Two such semi-trailers can also be purchased separately (954002-1).



Completing this roundup are the heavy Pennsylvania semi baggage cars (74003-1 to -3) on three-axle bogies. Returning to the program are the R-70-20 refrigerator cars of the SPFE as a two (914836-2) and four pack (904806-2).

FRISCO's EMD E8 (Item No. 62621-2) is certainly not overlooked on a layout. Photo: AZL / Ztrack

The same goes for the wide-view freight train escort car (921020-1 / -2), which we haven't seen for a few years. The first of several reissues is for the Alaska Railroad.

#### NoBa models fresh back:

Shortly before returning from the summer break, NoBa models has told us the autumn new products 2023. Equally new, however, is the significantly refreshed company logo, which now conveys the dynamism and innovative power of this supplier much better.



The Maypole with colourful decoration (item no. 10104) stands out from the other new products and the previous program. Photo (and logo top right): NoBa-Modelle

The Zetties can look forward to a table tennis table with players (item no. 10615R), two power distribution boxes for the roadside (11021R), a maypole with colorful decorations (10104), and a forklift suitable for eras III and IV (6513R).

As soon as production starts again, all four new products can be purchased directly from the manufacturer. Until then, the message "sold out" will appear in some cases, if no stock is available. The sales pages can be reached at <https://www.noba-modelle.de>.

#### New from Uhlenbrock:

Luisa is not a new employee at the Bottrop-based digital supplier Uhlenbrock. Instead, it is another creative abbreviation for a new product. The name stands for "LocoNet universal isolator, power supply and display" (Item No. 62 280).

This new LocoNet repeater with galvanic isolation via optocouplers creates a new LocoNet branch and refreshes the signals. It allows the connection of all Uhlenbrock LocoNet devices such as feedbacks and LocoNet boosters to all LocoNet-capable command stations with a non-grounded end stage.

For the second LocoNet branch Luisa also provides a power supply with 12 V and 500 mA for the “fresh” LocoNet connections. A suitable plug-in power supply is included.

Another autumn innovation is the power distributor with power supply connection socket (20 300). It was developed to transfer a previous task of a transformer to modern switching power supplies. Thus, it is not necessary to use a separate power supply for each device, but to supply several devices via this new product.

The power distributor has a DC socket for the power supply unit and 10 terminal pairs (+/-) for connecting the devices. The current carrying capacity is max. 5 A. For example, servo decoders or DC-operated lighting items can be connected. It can also be used to distribute traction current.

#### **New from Ratimo-Z:**

Even though electronic distribution at Rainer-Tielke-Modellbau is shut down for maintenance at the time of going to press, this supplier is by no means idle. For example, it has taken on the 3D printing of a concrete switching box for level crossings, which came out of ideas in the ZFI forum.



The switch houses for level crossings prints Ratimo-Z as a service for interested customers. The origin of the model construction is in the forum of Z-Friends International e.V.

Also, own ideas do not go out. For example, a letter to the editor already referred to the yellow luggage trailer (Item No. 50078) for the cart of the usual DB design already in the range. Interesting is also a white painted refrigerated trailer (50079), as it is used in the present time gladly at the edge of events for the storage of perishable goods, in order to be able to offer constantly fresh culinary specialties.

Our readers may not yet be familiar with a truck engine block (50081), which is used on forwarding agents' premises with their own workshop, but which can also be loaded onto the flatbed of a truck (or a freight car) just as effectively.

After completion of the maintenance work, Ratimo-Z can be reached again as usual at <https://www.rainer-tielke-modellbau.com>.





The yellow luggage trailer (item no. 50078; photo left) and the refrigerated trailer (50079; photo right), which is also offered as a finished model, are also current new items from the Ratimo-Z program that we have not yet presented here.

#### Azar Models works its way through:

In the truest sense of the word, French Z Gauge supplier Azar Models is working its way through: After locomotives and cars, the small-series manufacturer has now arrived at the end of the train. Working tail lights based on the SNCF example are to become available in the near future.

They are mounted on two versions of the covered UIC standard freight car G4. A contact is triggered in the roof of the model by a magnet (included) that can be used to turn the tail light on and off. The power supply for the two LEDs is provided by two included button cells (type LR48), which are hidden in the car body and should last for several days. In this way, the models are suitable for both analogue and digital operation.



The tail lights based on the SNCF model are a good idea because they make sense in layout operation. The chosen method of battery supply and contactless on/off switching by magnet also seems well thought out.

This new product is offered via the following car versions made of brown coloured plastic: G 4 freight car with brown standard ventilation flaps (Item no. W02-STX) and with bare metal aluminium ventilation flaps (W02-STAX). The tail lights are mounted on the car body approximately at the buffer distance above

them. The sales partner in Germany is the 1zu220-Shop. The manufacturer's pages with its information can be found at the following address: <https://azar-models.com>.

#### Latest information from Japan:

Z gauge is also still present in the land of the rising sun. Over the years, there have been many manufacturers there who have brought interesting products and innovations to the market. Some, however, left it at short market appearances or treat themselves to longer breaks – not so at Rokuhan.

This high-volume supplier has long since established itself as the number one on its home market for 1:220 scale and has been present there for more than ten years without interruption with its catalog goods. This also regularly includes new products, most recently in the area of shorties, which now also take up American and German models.



Rokuhan was well received by customers at the Model Train Show (JAM) in Tokyo in mid-August. The first complete sample of the Shinkansen Series 0, forefather of all high-speed trains, was also shown here. Photo: Rokuhan

The Shinkansen Series 0, however, promises to be a cracker, as it was the world's first high-speed train in 1964, making its debut for the Tokyo Olympics. Rokuhan has taken it up and implemented it in its original version, which is characterized by a different window size in addition to its blue-ivory paint scheme.

At the time of going to press, the manufacturer was expecting the first production models to be available. The train will probably also be offered in Germany as a complete twelve-piece set (Item No. T020-1). This is indicated by the good response to the announcement and distribution of information via digital media.



Among other things, Rokuhan received good feedback at the Model Train Show (JAM) in Tokyo, which took place from August 18 to 20, 2023. There, the Japanese market leader was represented with its own booth, where a complete sample of the forefather of all high-speed trains was also on display. However, it was explicitly emphasized that this response also came from foreign customers.

Other new products were also shown at this trade fair, which we will also discuss in our magazine, at least in part. In September and October, Rokuhan will also be present at exhibitions and trade fairs in its home market, where they will be able to get in touch and exchange information with end customers.

#### Autumn new products 2023 at Märklin:

Märklin's new 2023 fall items, which were announced just in time for the event in Göppingen, are also quite richly equipped for Z scale. The "Pantone Color of the Year 2023" car (Item No. 82163), which looks unconventional as well as colourful and lively, was immediately well received.

To be expected was the 2023 Christmas car (80633), which is traditionally introduced with this last instalment of new products. This year it is a Pwi blunderbuss baggage car that completes the last two predecessors to a Christmas train.



This year's Christmas car (Item No. 80633) competes for the customers' favour in gingerbread colour. The baggage car completes the blunderbusses of the two previous years to form a Christmas train. Photo: Märklin

It is base coated in brown colour and supplemented with Christmas prints. It is packed in a clear house, one half of which is colourless and the back is grey. With this the wagon can also be hung as a tree decoration.

The class 221 diesel locomotive (88208) holds a small surprise. With ocean blue ivory, it reproduces the most unpopular colour version on this locomotive type. But, this is also a part of railroad history and the contemporary successor of the earlier model is technically up to date, and, for the first time, now also gets functional red tail lights, presumably the interior of the V 2000 is used for this to a large extent.

Although the locomotive's prototype was used primarily in coal and steel traffic at its Oberhausen-Osterfeld out-of-service depot, as is also shown in the prototype photo in the new items brochure, Märklin has provided a set for container traffic (82664) as a matching car set. Four Lgjs 598 gondolas and an Sgs 693 are loaded with various 20- and 40-foot ISO containers.



The class 221 (88208; photo left) now gets a light change for the first time that also includes red tail lights. A five-piece container car pack with containers also from DB-TFG and DR (82664) is intended to complement it. Photos: Märklin

In addition to typical representatives of the eighties, they also show one each of the DB and the DR. Diesel locomotive and five-car packs are one-off MHI series, which also makes it clear how the special series for these dealers will be completed in 2023.



The package "Bauzug EfW" (81372) with four red-brown side-dump cars Fas 126 and one EfW diesel locomotive of the class 212 should now be available at dealers. Photo: Märklin



A German Federal Railroad "vacation train" set (81304) apparently has a guest worker train as a model. It consists of a chrome oxide green class 140 electric locomotive and three pairs of By3ge conversion cars in the condition around 1972. The cars have Era IV markings, we find the locomotive somewhat unimaginative because, as with the class 139 in ocean blue ivory, there is no variation and for almost 40 years the class designation seems to be firmly tied to one colour.

To a small series Aral tank cars makes another new product (82304) two previous predecessors. Once again, a two-axle old tank car with an attached adjusting shield appears. This time, it again wears a light grey colour and has a brakeman's cab. It is also set at the DB.

The MRCE Dispolok is the prototype of the black ES 64 electric locomotive from Siemens (88588). For the first time, this class enters the Mini Club assortment with the latest technology in a version with three single-arm pantographs.

The official end of the listing is the "Bauzug EfW" (81372) package. The four red-brown Fas 126 side dump cars from the DB AG stock, loaded with gravel, are led by a class 212 diesel locomotive.

True to Era VI, however, the purple locomotive painted according to the DB scheme no longer belongs to the former state railroad, but to the private company EfW (Verkehrsgesellschaft Eisenbahnfreunde Westerwald). By the way, more photos of the autumn new products can be found on our website in a separate message.

Not mentioned in the fall new products is another message car that Märklin will present on October 5, 2023. Consequently, we are not yet allowed or able to make any statements about the details of the offer at this point. Those interested in the model should therefore keep an eye on what information the manufacturer reveals on the due date.



A new "Message Wagon" will be unveiled on October 5, 2023. As of today, the only thing known is that it will be dedicated for a second time to the Dutch artist Vincent van Gogh.

## **New miniature car from Turin:**

After our report, Guido and Mirko, whose initial letters characterize their own brand GMmodelli Torino, have also received encouragement beyond the borders of Italy, and the two are continuing their work.

While the first Gauge Z model of an FS electric locomotive was certainly, due to the lack of foreign suitability of the prototype, rather exotic for the majority of the Z Gauge community, the situation is completely different with a new car model.

To our surprise, however, it was not a model from the local manufacturer Fiat (with its Lancia brand) that was chosen, but a classic from the Alfa Romeo factory halls in the north of Milan. The Alfa Romeo 75R, built from 1975 - 1983, is an icon of the brand, which bears the city emblem of the northern Italian metropolis in its logo.



With the Alfa Romeo 75R from the 1975 to 1983 model years, GMmodelli is producing an icon of recent automobile construction in Italy on a scale of 1:220. Photo: GMmodelli Torino

Supplied in a transparent plastic box, the tiny vehicle is finely handcrafted to an exact scale. A transparent glass replica allows a view of the replicated interior, while the exterior of the sporty runabout impresses with a red paint finish applied by spraying. Other colours are also planned for the future.

The distribution takes place in Italy via model railway stores, the manufacturer probably therefore does not have its own website. Interested parties can contact the two authors via GMmodelli[at]hotmail.com. We plan to present the new car model with our own photos in the magazine.

## **Atlas Model Railroad announces its own cars:**

Atlas Model Railroad announces a total of twelve variants from six different railroads, each with two operating numbers, for its rolling stock debut. Yes, you read that right: Having once said it would only enter the field of track material, freight cars based on US models now follow.

The 53-foot covered Evans car (box car) with double sliding doors (DPD) was selected. The prototypes were built in the sixties and seventies and were widely used. They often transported wood and paper products, but were also suitable for foodstuffs.



Sample images or technical descriptions are not yet available. We therefore show a manufacturer's drawing and refer to its pages for ongoing information and the variants offered, which can now be pre-ordered: <https://shop.atlasrr.com/c-1700-z01.aspx>.

Distribution in Germany is handled by, among others, Case Hobbies (<https://case-hobbies.de>).

#### Exhibitions in November and December:

It will also be an interesting fall for Z gauge outside of the larger trade shows. We have collected three exhibitions for you at which our size is represented or even plays the main role.

The first event will be the EHEH Model Railroad Days in the Hochdahl locomotive shed at the Hochdahl-Erkrath steep ramp, located between Düsseldorf and Wuppertal. They will take place on November 1, 4 and 5, 2023. The opening hours can be found on the organizer pages (<https://www.lokschuppen-hochdahl.de/wordpress/veranstaltungen/>).

A four-member delegation from **Trainini®** will also be taking part in this exhibition, presenting some exciting exhibits, holding technical discussions and, above all, offering free handicrafts for children.

The event continues with the Model Railroad Day in Duisburg. In the inland waterway museum, the enthusiasts around Markus Schiavo (<https://www.spur-n.com>) expect the visitors to the 6th hobby show, which has to offer besides railroad also ship model construction and other sections. On December 2 and 3, 2023, the exhibition will be open from 10:00 to 17:00.

In addition to Ralf Junius, Volker Bastek will also be present with a new layout for Z scale. According to our information, his brother André Kammels will again accompany him with his own exhibit. Torsten Schubert (Z-Lights) will show his illuminated model cars and give soldering demonstrations. This event is also supported by the Modellbahn Union.



We will certainly get to see the "Kistrath" layout of Ralf Junius this fall, as he is participating in all three events. This leaves Hochdahl, Duisburg, and Zell an der Mosel, as possible meeting places...

The highlight and end of the year for our Z scale will again be the Advent meeting in Zell (Mosel). Detailed information will be given by the Z-Freunde International e.V., as organizer.



A Trainini® handcraft and driving programme for children is to be offered again in Hochdahl and probably also in Zell an der Mosel.

On the 2nd weekend of Advent, Zetties will again meet for a glass of wine and a meal together in the little Moselle town. The meeting will start on Friday evening, 8 December 2023, depending on the arrival time of the participants, with a relaxed get-together at the old train station in Zell.

Saturday will be the set-up day for the exhibiting participants in the Zeller-Schwarze-Katz-Halle, near the entrance to the town on the main road. For the evening, we expect to have a meal together, for which we do not yet have any information. However, this has been a tradition for many years, as the focus is clearly on conviviality and being together.

Sunday (10 December 2023) is then the official exhibition day in the municipal hall from 10:00 a.m., entertaining and delighting visitors from near and far. Many Zetties who do not exhibit themselves also appear here to get up-to-date information and have nice conversations.

We have suggested that this event should also be listed under the Model Railway Day, registered on its campaign page and that its logo should be added to the event poster in order to further increase its reach and awareness outside the local area and the Z gauge community.



**Two deliveries by MTL:**

The seventh wagon of the "War of the Worlds" series is now rolling in at Micro-Trains. It is an olive-green flat wagon (art. no. 525 00 182) loaded with a tank model that roughly reflects the time of the First World War.



The flat wagon loaded with a tank is number 7 of the "War of the Worlds" series (item no. 525 00 182; Photo left). In keeping with the season, another Halloween wagon appears (507 00 730; Photo on the right). Photos: Micro-Trains

In keeping with the season, a special Halloween model is also being released. Once again, a covered 40-foot freight wagon was chosen, which is printed with a matching motif around the company's own Micro Mouse (507 00 730).

**Latest news on JSS Electronics:**

Newly designed pages and an expanded range of products are promised by JSS-Elektronik. Owner Jörg Seitz let us know that the new sales pages are now available and that he is adding new products to them every day. The new range of services also includes, for example, decoder installation for customers.

So, it's worth browsing this resurrected offer at <https://jss-elektronik.de>.

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Board of Editors:  
Holger Späing (Editor-in-Chief)  
Harald Fried  
Dirk Kuhlmann  
Joachim Ritter

Correspondent North America:  
Robert J. Kluz

English translation:  
Alexander Hock, Christoph Maier, Oleksiy Mark, Martin Stercken

Further voluntary work: Stephan Fuchs, Ralf Junius (**Trainini TV**), Torsten Schubert

Licensed **Trainini Discussion Group** (<https://www.facebook.com/groups/1597746057122056/>): Michael Etz (**Trainini Locomotive Doctor**)

Publisher and Responsible in terms of German press law (V.i.S.d.P.) is Holger Späing, Am Rondell 119, 44319 Dortmund; Contact: 49 (0)231 9598 7867 or by e-mail to [redaktion\[at\]trainini.de](mailto:redaktion[at]trainini.de).

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# Trainini

German Magazine for Z Gauge



Radio-controlled digital Operation

Märklin Days in Göppingen  
Abandoned Places

## Introduction

Dear Readers,

Before we know where we are, summer is gone. Rain, fog, and falling leaves are images that we now deal with every day. What we are just as familiar with, without attributing any particular significance to them, are abandoned places.

No building decays from one day to the next, no track field becomes a “botanical garden” overnight. Such processes proceed over years, and we are rarely aware of them.

And yet they have special attractions that have triggered a trend, make people curious and somehow challenge them. This is exactly what we can do on the model railway if we do it consciously and thoughtfully.

This is what my editorial colleague Dirk Kuhlmann explains to us today. The ability to make models is something everyone should have, so it is rather the personal perspective and a little courage to approach such a topic. That is why this topic is a perfect continuation of our theme of the year, “layout details”.

A complete contradiction to this were the Märklin Days in September: lively, colourful, well attended, and full of exhibits, as prototypes or layouts, in excellent shape. Once again, Z gauge proved how vibrant and multifaceted it is.

And this also found favour in the trade press outside of our scale. That is praise and recognition at the same time. That is why we would like to treat you with a detailed report. If you have not been there, you can experience three exciting days, at least virtually. This will, of course, also include a new episode of **Trainini TV**, which our colleague Ralf Junius is currently preparing.

Questions will certainly have been addressed to Märklin in Göppingen about the V 320. Our test models were also there for repair. The contact partners provided us with detailed information that we do not want to withhold from our readers.

We also have a new product from Märklin. The Mobile Station WLAN was not designed for our scale, but it is suitable. It could prove this in our test, and its results have also been included in this edition.

As (almost) always at this point, I will conclude my tour of the topics with a look at the news and new product reports that we have placed in the dialogue together with the letters we have received from readers.

Not to be forgotten are the two literature reviews, which are also a long-standing tradition. They are exciting and helpful as well, but this time they are not directly related to another report in the current edition. Nevertheless, they provide ideas for new models! Therefore, my colleagues and I hope you enjoy reading them.

Sin-Z-erely,

Holger Späing



**Holger Späing**  
Editor-in-chief



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We thank our reader Jan Szymkowiak for his impressive photo support.

Date of publication of the German language version of this issue: 26 October 2023

### Cover photo:

In October, the 78 235, which is capable of reversible traction, is allowed to support the test of Märklin's new Mobile Station 2. This only succeeds after a few pitfalls have been avoided, but then it reveals a considerable fun factor. Our conclusion is clear.

Reworking the V 320

## Successfully modified

*When the V 320 was announced by Märklin, it was well received and met with great enthusiasm. Our test was correspondingly sobering, revealing some shortcomings and accompanied by a product recall. Four months later, it is time to revise our first impression in important points, because the manufacturer has heard the concerns of its customers.*

In **Trainini**® 6/2023 we tested the newly released V 320 001 (item no. 88320). The criticism we clearly expressed also applied in the same way to the Wiebe version (from 81320), which was delivered almost at the same time. Nevertheless, we liked the locomotive overall and have nominated it for the new releases of the year 2023 in the category locomotives.

We explicitly described this as a leap of faith and made it clear that we trust in Märklin's competence and expertise when it comes to the points of criticism. The company has also responded to this and has spoken to us about what is currently possible and about prospects. The reworking of our models coincided with a product recall for safety reasons.



320 001-1, the former V 320 001 in service with Wiebe, is back from rework and now presents itself again with an intact handlebar below the front windows of driver's cab 1.

Therefore, today we also want to report on the results achieved, present corrections and also give some help in case one of the problems observed with us should temporarily show up with our readers, as well.

We start our results report with the Wiebe locomotive 320 001-1: On the review sample we noticed that on the front of driver's cab 1, the handle bar running horizontally under the windows had paint damage. It should be painted in the yellow of the locomotive body, but, in this case, it was partially bare.



It was clearly visible that the applied paint had sheared off over a large area, but not completely. The paint application was originally present, but had suffered clearly visible damage on the way from the factory to us.

Märklin has remedied this by completely replacing the locomotive housing. Whether the faulty one can be reworked by hand and reused, is beyond our knowledge. We assume that other affected customers will be helped in a similar way.

The manufacturer also looked into the cause of this paint damage and found that this does not occur on the other model, where the same bar is painted purple. On the Wiebe model, however, it does not seem to have been an isolated case.



The cause of the paint damage was found to be the deep-drawing insert, which was a bit too tight in the area of the handlebar. The red version of the locomotive was not affected by this.

The deep mould insert was identified as the cause of the damage, and a new one was manufactured. The model was returned to us accordingly with a new packing insert. Hard to see, but in the absence of new damage, it is pleasing to note that this rod was given minimally more space in the critical area.

This means that the model is no longer exposed to any contact with the thermoformed insert at this critical point when it is inserted (even at the factory) and each time it is removed, and it also corresponds to the Insider model, where this phenomenon did not occur. There were no indications of faulty surface treatment or reduced paint adhesion.

## Security risk eliminated

The product recall for both delivered variants took place due to identical electronic equipment inside. Heat development with destruction of the model or even a fire hazard could not be ruled out.

In the meantime, the editorial staff also had photos of a model where heat-related warping in the roof area above the affected parts was clearly visible. According to our research, the problem only occurred when using the brown transformer “electronic08” (6701).

To our knowledge, this was the only one from Märklin to use pulse width modulation (PWM). The “electronic05” unit, which was only sold in starter sets at the same time, differs from this, even though it looks identical on the outside.

With the help of PWM technology, it was possible to achieve a smoother start-up of the old generation of engines. Thus, the catalogues of the eighties already promised a “smooth slow speed.” “Extremely smooth start-up” was also highlighted as a quality feature only on this model.

The bell-type armature motors installed today no longer need this electronic starting aid due to the lack of cogging torque. They heat up more when a mixed voltage is applied than with pure direct current, which is why such traction devices are usually not recommended.



In order to eliminate the source of danger that led to the product recall, a varistor has now been soldered onto the circuit board between diodes D2 and D3, as shown in the enlarged section.

In the case of the V 320, however, the heat generation apparently affected not only the motor, but much more strongly components on the main circuit board. The constructional weak point could be localized in the area of four diodes in series.

This is exactly where Märklin's rework has now started. According to our comparison, no fundamental changes have been made to the PCB. Instead, an additional component was added to the existing circuit boards. We now find this as an electrical connection between the diodes D2 and D3.



This is a varistor: The term was created by joining the English words “variable resistor” and refers to a variable, voltage-dependent resistor. Such a resistor is often abbreviated as VDR (Voltage Dependent Resistor).

This electrical component has the characteristic that its resistance depends on the applied voltage, but in contrast to diodes not on their polarity and also not on the current intensity. Above a defined threshold voltage – here, when reaching a largely pure DC voltage at the “electronic08” – the differential resistance abruptly becomes smaller.

This is a very simple and easy to install remedy, which at the same time effectively eliminates the cause of the heat source. It seems to us to be sustainably suitable for permanently eliminating the originally observed problem and its dangers.

## Self help on the coupling

No changes can and will be made to the two versions already available with regard to the phenomenon of the buffer pads being too high. Future editions will be examined to see whether design changes are possible so that the housing slides lower on the chassis and remedies this shortcoming, which is visible from the side

We have attested the Märklin new product a largely good and very exact adherence to the prototype dimensions. We noted that the wheel diameters were 0.2 mm too large, which is not visible to the naked eye anyway, and that the overall height was 0.3 mm too large.

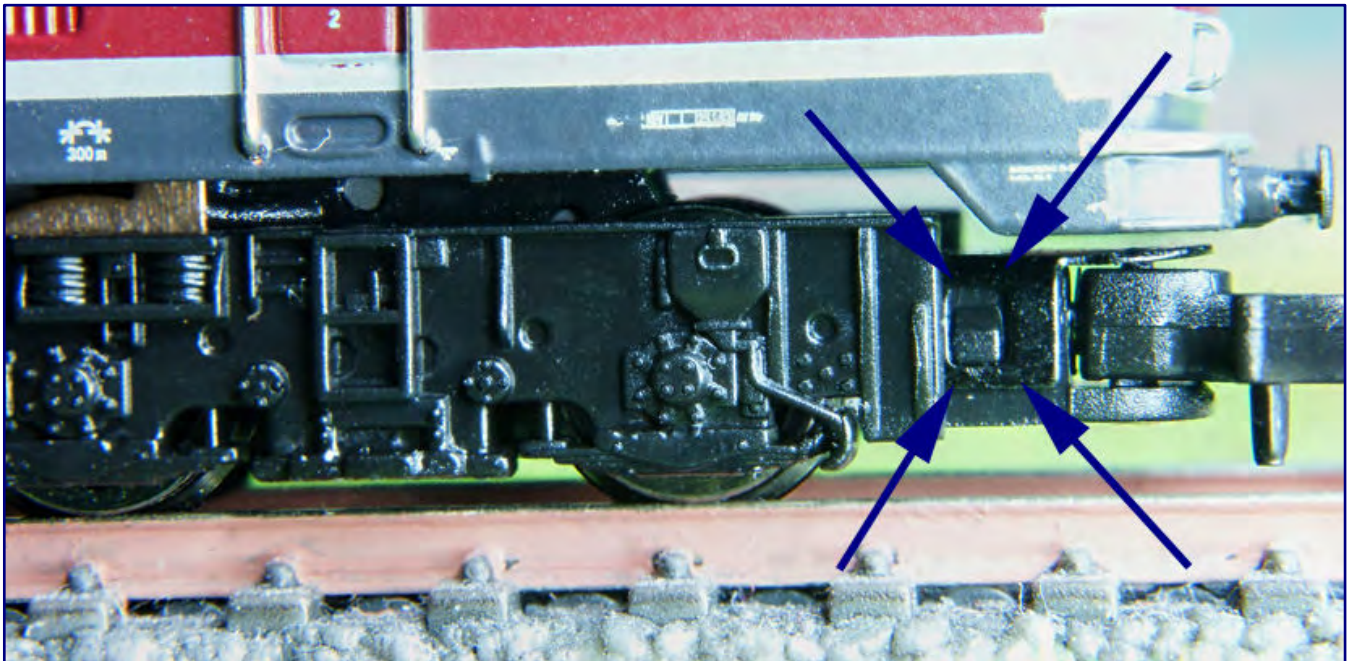


On V 320 001, the side of cab 2 was particularly affected by a coupling that was too high (see [Trainini® 6/2023](#)). This problem has now also been eliminated as the new photo proves. What remains, however, is a buffer beam that is also too high.

They alone cannot explain to us the excessively high level of the buffer beam, because the deviation should be somewhat greater here, otherwise, it would not be so noticeable. It should be all the more difficult for Märklin to achieve an improvement here.

Should it be found, it will probably also fix the model's high-legged appearance, which is made so obvious especially by the high-leading ridges of the shiny metal wheel sliders.

Easy to fix, even by the model railroader himself, is the problem of a coupling that does not grip correctly. On our test models, three of four couplers were so clearly too high that the locomotives lost their attached train after only a short distance. This observation had led us to the conclusion that the V 320 is not suitable for layouts without reworking.



The incorrectly “set” coupling height is related to a new design of the bogie frame: The coupling shaft is bounded at the top and bottom by separate parts. They only keep the system coupling in balance when the detent between them fully engages (see arrow marking). Accordingly, this can also be rectified by the customer himself if this should ever become necessary, for example after disassembly for maintenance and cleaning.

After the models have been returned, there is no longer any sign of this, as we will show by way of comparison using the example of the previously most severely affected system coupling. The cause here was not a design fault, but an assembly error, which was rectified by the repair service.

Unlike earlier coupling shafts, the two parts around the coupling and pressure spring interlock here, which is why we were not aware of any self-help. The phenomenon we observed can be remedied by carefully pressing the top and bottom of the coupling shaft together.

Suitable aids here are tweezers with a wide tip, e.g., classic stamp (collector’s) tweezers, or tweezer pliers. If the parts have been successfully pressed together, this will be evident from the reduced vertical play and lack of gap under the coupling.

However, the lateral latching lug is used primarily to check the correct fit, which can then be seen clearly and completely in the intended rectangular opening on the shaft. We also want to illustrate this with a macro shot.





After returning from the "AW Göppingen", V 320 001 provides exactly the joy in layout operation that we had promised ourselves from the beginning with this model.

Our overall conclusion of the new model, which is nominated for our new product award, has thus improved sustainably. The essential and decisive points of criticism have been eliminated. What remains are possible options for the future in the height of the buffer plank, which is not so disturbingly perceived in passing traffic, and the pointless illumination of the skylight side windows without the engine room side windows.

Manufacturer of the base model:  
<https://www.maerklin.de>





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Layout details (part 5)

## The Charm of abandoned Places

*After a two-month break, we continue today with our annual focus topic. Dirk Kuhlmann follows a trend and tracks down abandoned and forgotten places, whose special and morbid charm also make for attractive scenes on a model railroad layout.*

In this episode of our annual focus topic on “Layout details”, we want to discover abandoned places and show how to integrate them effectively into a layout. In recent years, various manufacturers have been increasingly catering to this theme and are now offering corresponding kits.

From buildings in need of renovation to ruins, everything is included to reflect the trend of so-called “Abandoned Places”. What ten years ago was more a matter for model specialists and rather frowned upon by the general community, as it departed from the norm of depicting an ideal world, has become a widespread practice today.



This is how Hall 1 of the former railway maintenance depot Opladen looked two years after it closed. Everything that could be used was shipped to East Germany. Fortunately, the hall was preserved.

Of course, Z scale hobbyists must always be aware that commercial brands mostly cater only to the H0 scale market with trending products. But I don't see this as a shortcoming at all. After all, the term model making is made up of two words, the second of which should be given a greater emphasis anyway.

continues on page 13





In former times, there was a large industrial area in Cologne-Mülheim, and the sidings all ended here near the Federal Railway's tracks. Long since cut off, this area has fallen into disrepair since the 1990s (photo above). The Leverkusen-Morsbroich signal box (photo below) has also been out of service for decades and is deteriorating. Apparently, there are plans to preserve it.



And, so, in addition to modifying standard buildings, scratch-building has become a fine art, especially in the Z gauge sector, and is no longer foreign to us. More than 50 years with far fewer off-the-shelf kits than the other scales have sharpened our skills, right? And the growing availability of laser-cut products and 3D printing solutions have inspired us all the more.

The choice of a abandoned places scene should be well matched to the era and the location of the action. While run-down and deserted streets would work well on a US-themed layout, a similar scene would look strange in a German setting; perhaps the mass shop closures in many inner cities could still be effectively depicted in Era VI, but who would want to design such a depressive theme?



In 2004, the author of these lines was allowed to visit and photograph the area and the former Hirschsprung railway station with the permission of the property owner. How long has this door been closed?

Therefore, there is once again only one effective method to explore a worthwhile scene: on-site research within the bounds of the possible. Most of the time we find that it is often only individual buildings or smaller complexes that have been abandoned for years and are slowly being reclaimed by nature.

Modelling abandoned industrial areas in their entirety is hardly feasible due to the vast footprint this would require even on a scale of 1:220. I was able to observe this very well 20 years ago at the abandoned maintenance depot of the German Federal Railway in Opladen.

Covering even just the most important buildings, a replica in Z scale would require an area of 120 x 60 cm! If it was still an active depot with trains running on it, it might perhaps be a great idea to model such a facility, but filling this amount of space with an abandoned site would be, realistically speaking, more the kind of project for a large club layout.

## Targeting scenes which are feasible to build

Therefore, today we will concentrate on individual houses or smaller building complexes. Just around the corner from me is an old signal box from the 1920s. Until the 1960s, there was a level crossing of Bahnstraße (today Gustav-Heinemann-Straße) across the railway line.

The “Lmf” (signal box abbreviation for “Leverkusen-Morsbroich Fahrdienstleiter”) even had an in-house signalman, as indicated by the “f.” It can be seen on page 12 below.

This structure has already been lying dormant for decades and features the typical characteristics of an abandoned place. Therefore, it makes a good example of the features to include in the design of similar scenes:

- desolate and partly leaking roof areas,
- sloping gutters,
- dirty (and obscured) window panes, partially also broken,
- rotting wood,
- dirty facades,
- depending on the season, partially overgrown by nature, and
- chipped paint in various places.

Only a few kilometres away stood the signal box Of (“Opladen Fahrdienstleiter”) until its demolition in 2015. Only built in 1968, it also showed the typical signs of decay after its decommissioning in 2012.



Dieringhausen has often been described as a typical railway village. Unfortunately, this photo only bears witness to the decay of the old infrastructure in the Oberberg railway system.

Meanwhile, the buildings of the former track engineering yard immediately behind showed a very high degree of decay. I have to admit at this point that, apart from landscape photography, the impressions of abandoned railway facilities have been very appealing to me for decades.



Along the way, I also discovered that many people are attracted by the charm of the dilapidated. Curiosity and wondering about what used to happen in that house, factory or area, as well as the people that used to work there and their everyday life, motivated me to go on a journey of discovery.

On my many trips I have taken plenty of photos of a very similar nature. Only very special buildings bear witness to a certain region and the model should absolutely follow this. The typical façade of the former Hirschsprung station (see page 13), for example, only really comes into its own on a Black Forest layout.

## Left to decay

Shifting up a gear, we now look at the buildings that are almost beyond renovation. Here, the ravages of time have gnawed at the substance to such an extent that immediate demolition would make more sense. Commonly, it could also be considered a ruin – a truly abandoned place.



This small and boarded-up building belonged for decades to the steam locomotive depot at the terminal station “Helenensiel.” A piece of track and the buffer stop still exist.

Here, the hobbyist has to dig deeper into his bag of tricks. Partially collapsed roof areas, for example, expose rooms in apartment buildings. This would call for a little interior design, right down to supposed wallpaper remnants on the walls or the toilet bowl that has already fallen one storey below.

On the outside, the former colour of the walls can only be guessed at and bare wall areas are emerging. In rural areas, we still find wooden barns whose roof has already fallen through to the ground. Here the battens, rafters, or ridge purlins should be visible, but, of course, in a complete mess.



Meanwhile, the buildings of the former track construction yard immediately behind showed a very high level of decay. I have to admit at this point that, apart from landscape photography, impressions of abandoned railway facilities have been very appealing to me for decades.

Within the railway theme I found further objects that actually no longer serve any purpose and were left to decay. The trackless platforms in the Dieringhausen or Pattscheid stations serve as prime examples. Abandoned goods ramps and sheds also are a typical consequence of the “modern railway”. Similar to the practice of the Deutsche Bahn AG, the withdrawal from servicing large areas was also a feature of the former Federal Railway – abandoned, lost, and deserted.

But sometimes a building only looks like an abandoned place in photos. In 2003, I was by chance at what was then the main station in Solingen city centre: I have never seen such a large station in such a decrepit state.

And this station remained in operation like this until 2006! I could only capture this atmosphere when no traveller crossed my lens. Later, the building was restored and became a monument, but it has not been used as a railway station since.

continues on page 19





Photo above and following page above:  
This looks like an abandoned building but isn't one! It is the former Solingen main station, which was actually still in operation when this picture was taken. It is a perfect showpiece for our abandoned places topic, even if this was probably not the intention of those responsible for the station at the time.



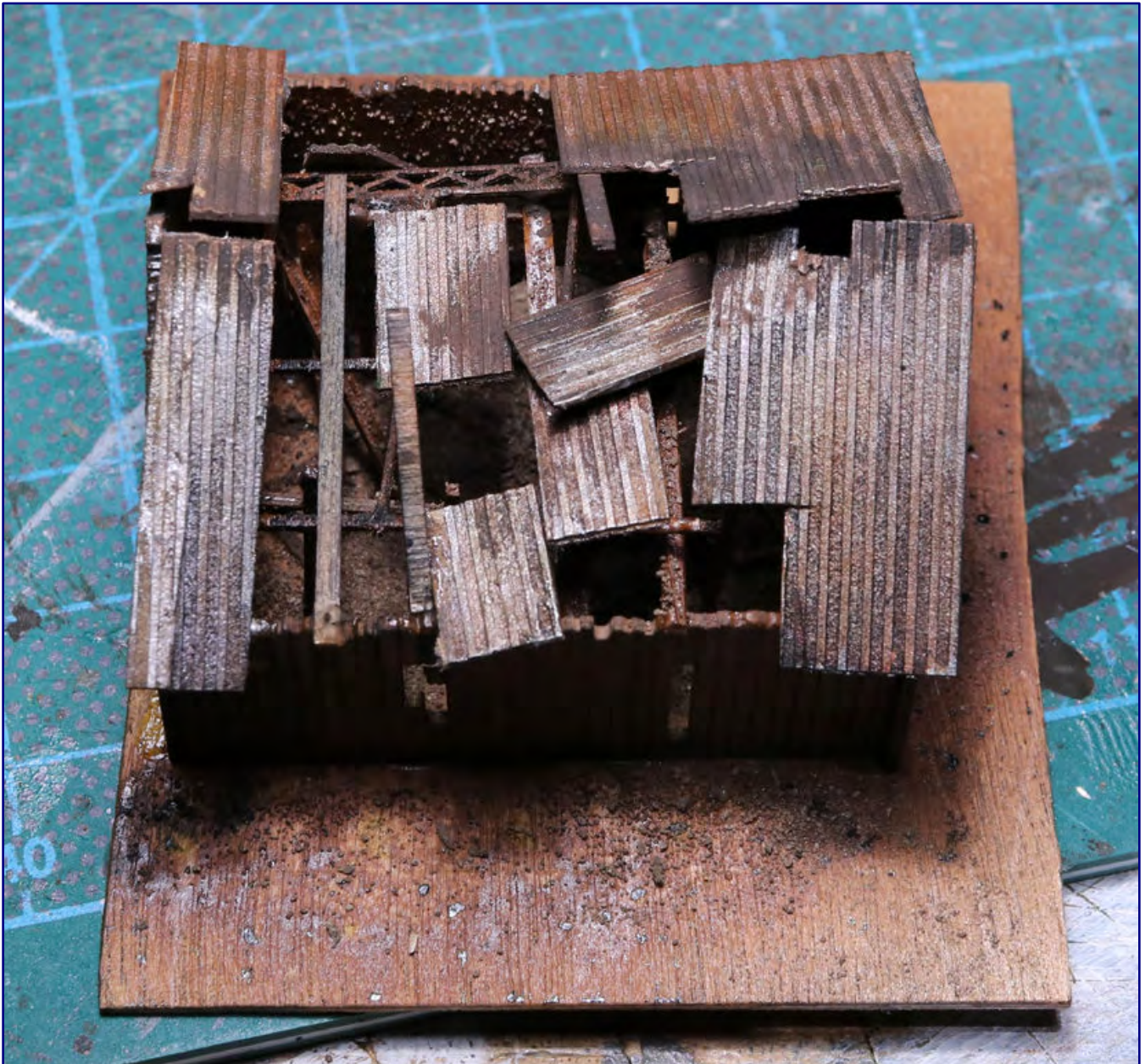


**Photo below:**  
When constructing your own dilapidated buildings, it is always worth it to pay attention also to their “interior design”.



After so many impressions and information, we then come to the model railway department. There are often bare areas on almost finished layouts. Mostly it is the empty corners on oval track plans that make for potentially interesting real estate. Instead of covering them with a featureless grassy field, try occupying them with a more unique eye-catcher.

An abandoned house or an overgrown ruin, also unfold the described charms in the model, and will make for a visually interesting trackside detail.



We usually look at the model railway from above first. Therefore, we will also show how a dilapidated building can captivate the eye from a helicopter perspective.

When planning a new model railway layout, it may be worth including an abandoned place scene right from the beginning. For example, an abandoned farm would look good on a rural layout. The possibilities are manifold, as you can see.



On my layout “Helenensiel” I placed a small abandoned locomotive station next to the reception building at that time. On my “Kniephaven” layout, the small railway stop in the dyke foreland is an abandoned place. In both cases the buildings are boarded up with veneer wood cut to size of the windows and doors.

An even more extreme approach was taken on the US themed “Old Port Shop” layout. It includes a scene with abandoned tracks cut-off from the main line and in parts hopelessly overgrown with vegetation. The entire scene is complemented by a decommissioned coaling station and a decrepit diesel engine.



Even a single wagon can become a deserted place if the weathering is right! However, the rubbish to the side behind it should also be noted here, which further enhances this intentional setting.

This is a real peripheral feature, as it is placed at the back of the layout. It is designed to be noticed only at second sight. But once recognised, the scene will impress and captivate the viewer for a long time.

It is precisely this kind of playfulness with many different buildings, whether active or abandoned, that makes viewers linger for a long time at a layout, with their eyes guaranteed to explore every centimetre.

Give these abandoned places a try! On the internet you will find a multitude of photos and impressions, because their fascination is simply too great not to be shared it with others worldwide.

Author's webpages:  
<https://www.helenensiel.com>



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**Z**

## Mobile Station WLAN All in one Hand

*A wireless control unit for the Märklin control centers was a noticeable gap in the digital portfolio of this manufacturer. At the toy fair in spring, the people from Göppingen finally presented their idea of how to close this gap. The developers also focused on one-hand operation. Now the device called "Mobile Station WLAN" is on the market, and we have tested it for you with a focus on Z gauge value.*

In 1984 Märklin was the first large-scale manufacturer to herald the start of the digital age in model railways. The number of models offered, control components and matching accessories grew quickly. Semiconductor technology and microelectronics developed rapidly, and the sequence of component generations was correspondingly fast.



Here we see the delivery scope of the Mobile Station WLAN (art. no. 60667) on the left in the photo. It comes with a round handle (bottom) to stick on for one-handed operation. The WLAN receiver box (60117; on the right in the photo) comes with two wood screws for mounting on the layout.

Since the technology was still expensive at the beginning and for many model railway manufacturers rather a marginal topic of their work, digital technology remained a business field for small, new and aspiring companies, especially in its early days – “start-up” is what we call it in today's use of language. Märklin, too, could not get by for many years without such “outside help”.



The years passed, a “Control Unit” of the early days became the “Central Station” (CS), the communication format Märklin-Motorola (MM) finally became MFX with automatic registration. Before that, with the takeover of Trix in 1997, the digital format Selectrix (SCX) had also come into the company, until DCC finally prevailed worldwide in the DC sector.

This is where Märklin's activities for the H0 and 1 gauges meet as well as the concepts still pursued by Trix for H0 and above all N with the Z gauge. For Selectrix there were the first decoders of such small size that locomotives of the nominal size Z could also be equipped with them. The requirements for a sufficiently high track voltage and electrical power were sufficiently close to each other for the two small scales.

To this day, Selectrix also found its way to Z gauge without being offered in large series, until the more modern DCC technology overtook it. Components developed for Minitrix could mostly also be used for the Mini-Club, because Märklin's former Nürnberg (Nuremberg) brand also made the turn to DCC digital format.



The two devices are offered as separate products. The reason is simple: For stand-alone operation or on an existing configuration based on the wired Mobile Station, the receiver box is interposed, while the Central Station can establish a WLAN connection using a router.

In addition, since the first generation (the “green version”), the small control unit “Mobile Station” (MS) was also a popular control unit for digital operators from our model railway segment. This has not changed until today, whereby the second generation is still in use, in contrast to the large CS3. Only the housing colour was changed from light grey to black.

If the MS2, for which the number indicates the product generation, is supplied with a mini club power supply unit, it delivers a track voltage of 12 volts, which is usable for our purposes; for sound decoders, up to 14 volts are useful to ensure non-interrupted operation.

Under these certainly timelapsd general conditions, it becomes clear that the new offer of a “Mobile Station WLAN” (art. no. 60667) is also worth a look and a test run for our scale.



**Everything under control: The Mobile Station WLAN is based on the MS2 in its outer design and proven operating concept. Modifications such as the two recessed grips on the back qualify it for wireless operation**

The “receiver box WLAN” (60117) is also included, which is required for the sole operation on the layout (then necessarily together with a track box) or as a supplement to an existing MS2 (with connection via track box).

It is sold separately because it is not required if the Mobile Station WLAN is integrated into a CS configuration as an additional device. We will not test this operation option because the Central Station (2nd or 3rd generation) without special precautions applies a higher digital voltage to the track than is advisable for our gauge.



## Theoretical background of the new product

In one respect almost all digital suppliers seemed to be years ahead of the manufacturer Märklin: wireless hand controllers had long been established and were available. At Uhlenbrock, one of the pioneering companies from Bottrop in the Ruhr area, for example, it was and is the “Daisy” device.

At Märklin, the Mobile Station as a small control centre and hand controller (also for the Central Station) always kept a wired connection to the main unit or via the so-called “track box” to the conducting rails.

Because of the WLAN capability of the newer CS generations, Märklin finally reacted by integrating the clever mobile phone or a tablet computer, which communicate with the central station via the router. But a separate wireless handset was not launched.



The Mobile Station WLAN does not have a charging connection to charge rechargeable batteries. Instead, it can be operated with four A batteries or rechargeable batteries. They are inserted into the screwed in battery compartment on the back. The type of energy source chosen by the user must then be entered in the device menu.

Brand-loyal Märklin railway fans had to be willing to suffer while the desire for a wireless control unit was repeatedly put to the manufacturer. In September, the result was available to the first customers, and a first, albeit small, batch went on sale. Our test unit is also from this batch.

From the outside, there are hardly any differences to the well-known MS2. Those who are familiar with this device will therefore hardly have to adapt. Of course, the connecting cable to the CS or track box has been omitted.

The rotary control knob also differs somewhat and no longer has smooth surface, but has ray-shaped elevated lines, which provide a better feel and increase the ease of use. Here, you can quickly see that Märklin had one-handed operation in mind.

Two recessed handles on the back of the device are also suitable, which are intended for the four remaining fingers when the thumb is in use at the front. Younger model railroaders will probably find the new product much easier to hold if the handle which is included is glued to the centre of the back.

Those who fill their native language with Anglicisms, call this handle a “pop socket”. Young users are already familiar with this tool from their mobile phones. It enjoys great popularity there, which is why it certainly did not escape the attention of Märklin's developers.

The rest is largely the same as the familiar cable device: the backlit graphic display is only two-coloured instead of multicoloured and works with pictograms instead of pictures. Those who own many vehicles or locomotives with more than 16 digital functions will also want a larger and finer range of suitable symbols to distinguish between them.



The user interface of the two MS sisters is almost completely identical. The only difference is the rotary handle on the WLAN version (right), which has been fitted with ray-shaped elevated lines to make it easier to operate with one hand.

Overall, this means easy and familiar operation if the user is accustomed to the original device. After all, with the programme updates of the last few years, modifications have also been made at this point.

If Märklin itself emphasises this with the words “even without looking”, then the market leader from Göppingen is also concealing something that will become relevant in our test: The MS WLAN does not have a locomotive card slot like its wired counterpart.

But it apparently does have a gyroscope because it can detect changes in position like a modern mobile phone. At least, it is held vertically with a glued handle on the back for one-handed operation, so that the thumb can reach the rotary control.

The usual screen display would then be inconvenient because the head would have to be turned constantly to read the display. The MS WLAN recognises this position automatically and rotates the display by 90°. Incidentally, this works for both right-handed and left-handed people; the only thing it cannot do is rotate the display by 180°.





Whether left- or right-handed, the display can rotate 90° in both directions, but not upside down. The operator can specify in the unit settings if the display should rotate or remain fixed.

## Unit in stand alone operation

Connecting the unit for operation is simple. The new product and the Central Station can connect per radio link. The procedure is explained in the enclosed instructions.

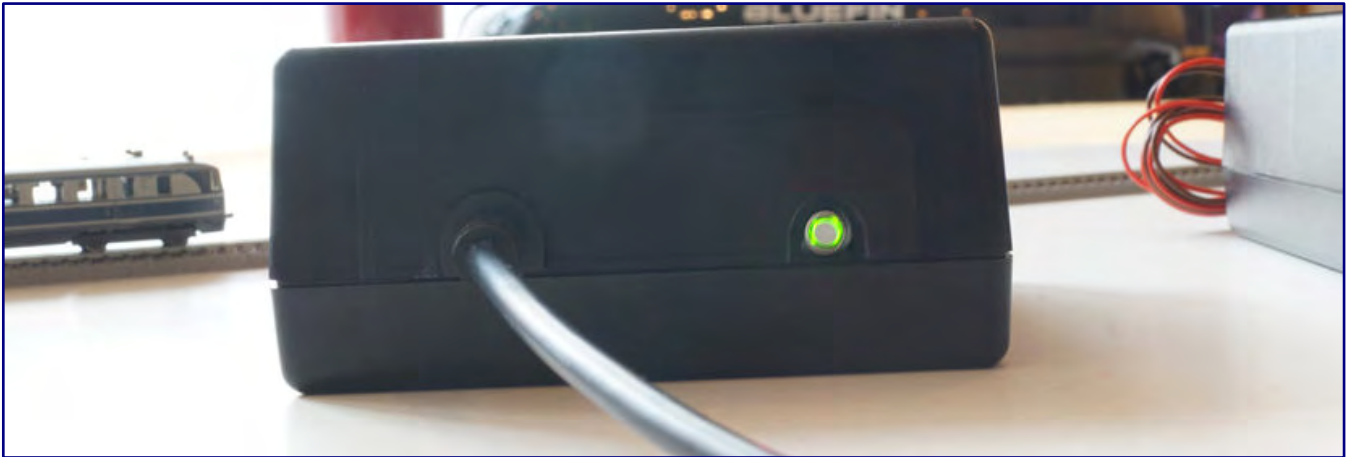
For stand-alone operation or together with an existing Mobile Station, the track box is required. The receiver box is connected to this like an MS2.

When first used, it updates the control programme of the track box and then sets up its own WLAN network. Its availability for operation is signalled by a green LED display in a switch.

For its part, the Mobile Station WLAN is switched on by pressing the stop button for more than one second after inserting suitable batteries. After booting up, it displays all WLAN networks found. The receiver box is then selected from these, which leads to a password prompt.



The unit is connected to the WLAN of the receiver box via the menu selection. It is then necessary to enter a password that is stored in the instructions.



A green LED on the switch of the receiver box indicates readiness for operation (photo above). In the mode that we are now testing, we cannot access a locomotive database. For new locomotives, we can only select “Create manually” or “Recognise” (photo below). If a new locomotive is to be recognised automatically, it may be useful to switch off the unused digital protocols in the Mobile Station WLAN.

This password is provided in the instructions. Unfortunately, it contains an error that experienced users of an MS2 may not even notice: The password entry is not only confirmed by pushing the rotary knob, but the shift key must also be pushed at the same time.

Now our first configuration is ready for testing. Märklin promises to be able to switch up to 32 locomotive functions and 320 electromagnetic items. This corresponds to the scope and operation of the MS2 in the current programme version, which is why we don't want to go into detail about it.



The most exciting thing for us Zetties will be the configuration of a locomotive that we want to operate digitally: The integrated Märklin digital locomotive database is also available and selectable on this unit. However, it is of no use to us because Märklin does not offer its Z gauge vehicles with digital operation. The situation is similar for model railroaders of larger gauges who also use other makes.

It has proved to be a convenient aid to use a locomotive card for each model, on which the address and all functions are stored by the MS2 and can be easily imported on any Märklin device.



Above we show the minimum configuration for the stand-alone operation of the Mobile Station WLAN: Both the receiver box WLAN (centre) and a track box (right), which generates the digital signal and puts it on the track, are respectively required. The two latter devices use an identical housing but can be distinguished by their imprints and the switch shown at the top of page 21.

This often eliminates the need for time-consuming manual creation because all stored functions are also stored on the card with the assigned function key, symbol, and function type. However, this is one of the few differences between our test candidate and its wired sibling: the card slot is missing.

The only remaining possible function is the automatic decoder recognition, which assigns the selected memory location in the database to the detected locomotive – in this case the first memory location due to the lack of selection.

The storage of a representative locomotive name, a locomotive symbol and all functions now remains manual work, which takes a lot of time, especially due to the lack of a keyboard on a touch screen. The larger the digital vehicle stock, the more annoying this effort is. In our view, the concept of the Mobile Station in both versions is not user-friendly.



If we use the readout function of the Mobile Station WLAN, it finds one that corresponds to the (long) address 604 in DCC format. The following procedure, which is identical for the wired device, is not very comfortable: Enter a locomotive name, select a suitable locomotive symbol (from a limited selection) and store all available functions on the respective F-key.

Once all this has been accomplished, we experience familiar operation and enjoy the lack of cables and the freedom of movement gained as a result. In particular on larger layouts, where the operator also wants to move around and follow his controlled model, this is of noticeable benefit.

Operating the Mobile Station WLAN as a stand-alone device or in a purely wireless combination is possible in the way described, but, for us, it results in a noticeable loss of comfort. It makes more sense to operate it purely with self-registering decoders, or Märklin digital products that are stored in the database.

Consequently, we now want to focus on combined operation with a Mobile Station 2, which then also provides the card reading function. As soon as two devices are in use, one must function as the main device ("master"), all others are subordinate ("slave").

In principle, one Central Station always assumes the role of master; in the case described, it is the Mobile Station with the smallest serial number, which for logical reasons should correspond to the oldest device in one's inventory.

This also applies to the Mobile Stations WLAN among each other, but according to the instructions not in relation to the Mobile Station 2: In this configuration, the latter always becomes the main device, a WLAN device is subordinate. This also seems to make sense, because the wired device should so far be the older device, but always locally bound and thus firmly assigned to the system. The mobile device, on the other hand, can more easily and flexibly change the centre with which it is to cooperate.

But here, theory and practice still diverge at the beginning of the test. Our test device has the serial number 717, which is far below that of the mobile station deliveries. The programme version according to the manual and the device is 4.0.0.1.



And, it is precisely this programme version that still has a serious error, as Märklin itself quickly recognises: The allocation of roles is based exclusively on the comparison of the serial numbers, the WLAN version wants to be the main device, which (so far) cannot be altered.



The message “No locomotive!” can be extremely nerve-wracking on both devices, because the larger your digital vehicle stock, the greater the amount of manual input required in this case. Time can be saved by using the locomotive cards from Modellbahn-Union, on which a model can be permanently stored with the selected settings and assignments. However, with programme version 4.0.0.1, the Mobile Station WLAN unfortunately still knows how to prevent the reading of the card.

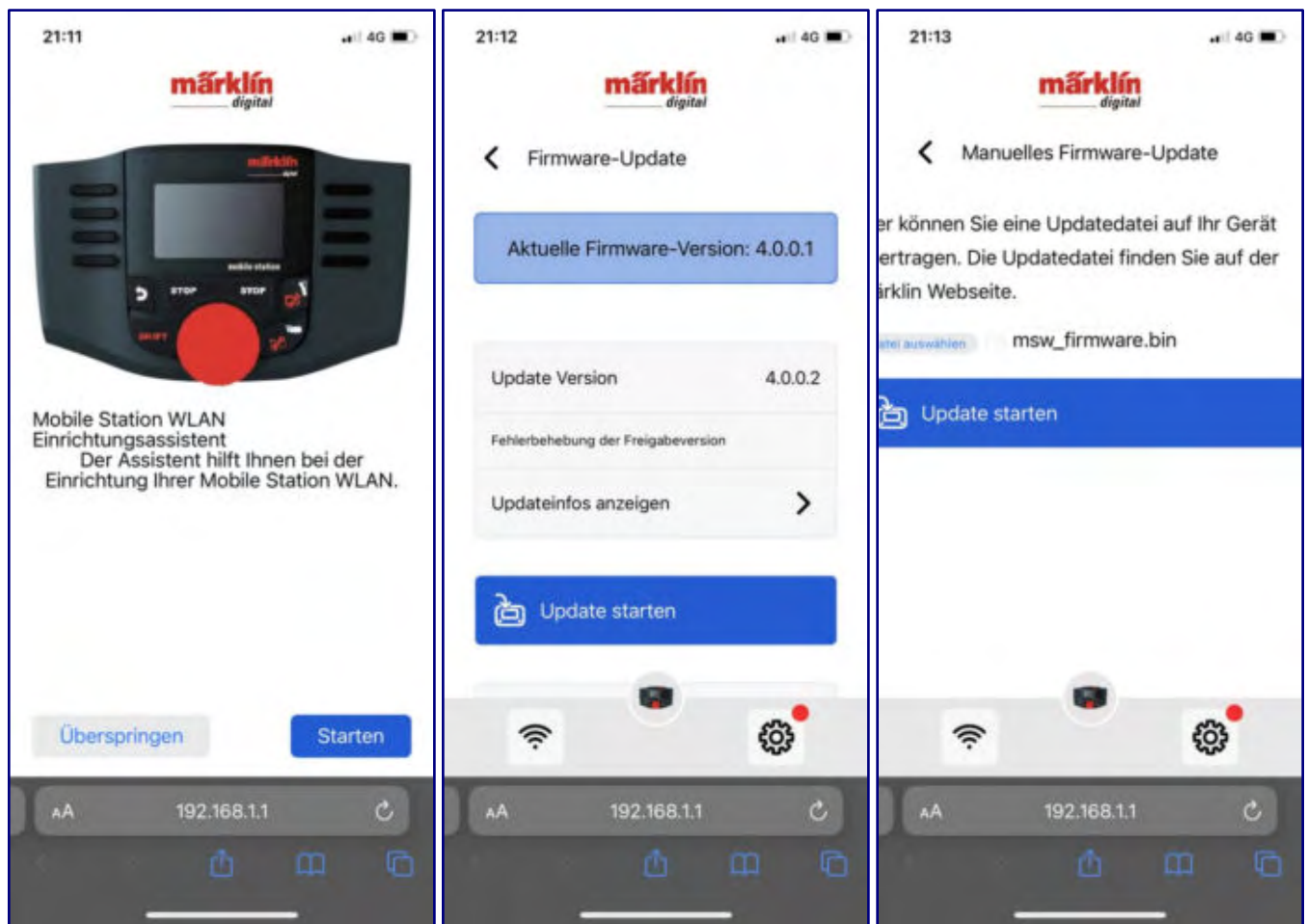
There is no access to the already existing, own database and no new locomotive can be added via the card reader, because the registration is reserved for the master. We have to interrupt our test for the time being, but fortunately Märklin provides an update to version 4.0.0.2.

## Annoying programme pitfalls

Mistakes happen, Märklin has already reacted, so no need to worry. But in our test, updates are not as simple as the instructions describe. The reasons for this are probably security measures on routers and in various browser programmes.

But a certain “sneakiness” in the input screens also plays a role here, as we only realise later: If the password entry for the receiver box is to be confirmed on the last digit, i.e., with the last entry, this is not the case with the password for a home network.

Here, the unit first expects a jump to the following digit, which is no longer filled with a field content. This is not described in the instructions and is, therefore, probably, not intended by the developers. In this case, it can be considered a small programme error (“bug”).



For set-up, the Mobile Station WLAN allows access from an external device if both are connected via WLAN. It can then be accessed under the IP address 192.168.1.1 (photo left). We need this access to be able to update the programme (centre photo). After the manual update by means of the downloaded file “msw firmware.bin” failed, we decided to use the automatic version. It is noticeable that some screens do not yet adapt to the display size (photo on the right).

If you don't realise this, you could quickly get lost and get desperate. In our case, two dealers who offer free updates for Märklin digital devices failed to do so. Only later did we receive the decisive hint from a third party.

Back home, we tried our “luck” a fourth time – without changing the procedure from attempt number 1. We found the instructions for the installation on the Märklin website (see information box at the end of this article), we saved the file to be processed on the mobile phone beforehand.

Finally, the connection between phone and Mobile Station WLAN worked, the promised interface appeared on the display of the mobile phone. We selected “manual update” and referred to the location where the file was stored, as Märklin instructed. Again, the result was sobering: “Update image invalid,” moreover the display did not even adapt to the width of the screen.

What now? Our call for help to Märklin has so far gone unheard. One last time we tried the option of an automatic update which meant to use the manufacturer's suggested path. Surprisingly, the device reported that the update was complete.

It switched off, restarted, and now displayed the programme status 4.0.0.2. “Press here and wait for a miracle” could describe what we experienced. Finally, the test could continue. By the way, the success



is not visible on the phone's display, because here the device is only read out again on explicit command. So here, too, the user may quickly be led astray.



The developers really did not make it easy for us to successfully complete the update. In the end, however, we managed to do it the way described and the joint test with Mobile Station 2 could finally take place.

## Usefully supplemented

Now we have finally reached the point where our test unit is supposed to work as promised. Consequently, the Mobile Station 2 is connected to the track box, then again, the receiver box to the second slot and the power supply is restored.

The Mobile Station WLAN searches for networks and also lists the receiver box from a series of suggestions. It does not want to connect automatically. After the manual selection, however, it uses the password already stored and does not ask for a new entry.

Now we have access to our locomotive database, can delete locomotives, register new ones, and also use the card slot of the wired control unit. Finally, all the advantages of the new product become apparent.

The menu offers some settings that we would like to mention here: We have already explained that the display can adapt to how the operator holds the unit by rotating it by 90°. This function can also be switched off if desired, which will probably rarely be used.

Still quite new, but not specific to the WLAN variant, is the display option “Tachotyp” (speedometer type). This allows you to switch from the usual bar display to a numerical display of the selected driving level and thus to adapt it to your own preferences.

If the Mobile Station WLAN is not operated for a predefined time, the display dims, meaning it is illuminated less intensively in order to save battery capacity. The selected battery type, meaning the choice between a rechargeable battery and a non-rechargeable battery, is also a setting that probably has an effect on the behaviour due to differences in voltage and capacity.



After updating to programme version to version 4.0.0.2, the WLAN device subordinates itself to the Mobile Station 2, and we can access our existing database and also use the locomotive card reader, as desired.

Incidentally, rechargeable batteries must be extracted for reloading. Regarding the option of batteries, Märklin's innovation does not have a charging connection via USB-C (PD / “Power Device”). If the display is too bright or dark for active operation, this can also be changed individually. The optimum luminosity may also depend on the lighting conditions in the layout room.

Finally, it should be noted that the housings of the track box and the receiver box have identical external dimensions, and each have two screw holes in identical positions. Both can, thus, be mounted out of sight in a suitable place on the layout.

The housings of the Mobile Station 2 and WLAN are not identical despite having the same top face. The two recessed grips on the back with the missing card slot are not the only difference: the housing of the newly introduced handset is slightly thicker, which can almost only be seen in a direct comparison.



We are reaching the end of this article, and it is time for a conclusion. A wireless control unit has long been on the wish list of Märklin digital railway fans, and this does not exclude Z gauge.

Many customers want everything from a single source if possible and remain loyal to Märklin in digital operation wherever possible. Due to the lack of Railcom capability of many decoders, self-registration at the command station is not yet as widespread in 1:220 scale as in other nominal sizes. The locomotive card function of the Märklin digital command stations is, therefore, particularly helpful.



The direct comparison between the Mobile Station 2 (left) and the Mobile Station WLAN (right) shows further differences in the view of the top side: only the wired device has the card reader, while its slot is missing on the right. Due to the battery compartment, the WLAN version is also somewhat thicker. The fold-out round handle, which we use to hold the right-hand unit, is not a criterion, however: it is included with the unit and is only attached if the user wants it and it accommodates his or her habits.

In our test, the Mobile Station WLAN proved to be just as operational as its wired counterpart. After updating the programme, its locomotive card function can also be accessed again. This makes the new product an option to be considered for initial purchase as well as extensions.

Which device and which manufacturer will be preferred in the end is a matter of personal preferences and demands. In any case, Märklin is up to date again and only a modern graphic screen, like the one on the large Central Station, remains on the wish list.

We would like to disregard minor program errors here. It may be annoying at times and give the impression that an immature product has been put into production, but on the other hand these are only small things that have a big effect. Moreover, Märklin has reacted quickly and will continue to provide updates in the future.



For use in Z gauge, we recommend the minimum configuration shown here, in which the new Mobile Station WLAN is only an extension as a sole hand controller. Only in this way can we control two locomotives at the same time and also use the comfort of the locomotive cards.

Anyone who is overwhelmed by the situation can always go to their dealer, because they have taken on and continue to take on an important service provider function. And, so we are happy about this new product and think that it deserves to be nominated for the best new releases of the year 2023 in the category technology.

**Provider webpages:**

<https://www.maerklin.de>

**Purchase of locomotive cards (house brand or Märklin):**

<https://www.modellbahnunion.com>

**Product update page and URL of the file:**

<https://www.maerklin.de/de/service/downloads/msw-updates>  
[msupdate.maerklin.com/msw/4.0.0.2/msw\\_firmware.bin](https://msupdate.maerklin.com/msw/4.0.0.2/msw_firmware.bin)



Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

## Moderne Bahn in Text und Bild **Aktuelle Triebwagenübersicht**

**Die moderne Bahn ist in Bewegung. Besonders mit Ausschreibungen für Nahverkehrsleistungen gehen oft neue Triebwagen und Triebzüge einher, weil sie von den Aufgabenträgern verlangt werden. Aber auch im Fernverkehr hat sich seit Erscheinen der letzten Dokumentation wieder einiges getan. Es ist Zeit für ein aktualisiertes Buch, das dies zusammenfasst.**

Michael Dostal  
Typenatlas Triebwagen und Triebzüge  
Deutsche Bahn und Privatbahnen

VGB | Geramond  
München 2023

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Rund fünf Jahre liegt die letzte Auflage des Typenatlas zurück, der nun in überarbeiteter Form wieder vor uns liegt. Da Fahrzeuge hinzugekommen sind, die es bei Erscheinen des Vorgängerbands noch nicht gab, war dies dem Verlag auch eine neue ISBN wert – zu groß erschienen die Unterschiede.

Aktuelle Triebwagen im Vergleich lautet die Herausforderung und Messlatte aber auch dieses Mal. Deutlich wird am reinen Zahlenvergleich zum Vorgänger ein nicht unerheblicher Zuwachs an Bildern. Das nehmen wir erfreut zur Kenntnis, haben wir beim Durchblättern aber nicht mal bewusst wahrgenommen.

Aber seien wir ehrlich: Niemand wird alt und neu gegeneinander legen und Seite für Seite abgleichen. Das wird kaum geeignet sein, um den Nutzen und Mehrwert für den Käufer herauszuarbeiten. Vielmehr stellt sich schließlich die Frage, ob das alte Buch noch die eigenen Ansprüche erfüllen kann.

Da wir hier von recht aktuellen Aufstellungen sprechen, wird die Antwort in den meisten Fällen Nein lauten. Und daraus resultiert ja auch für Verlag und Autor die Motivation, ein Werk zu aktualisieren und fallweise zu erweitern.

Verraten sei, dass sich die Seitenzahl nicht erhöht hat. Immerhin fallen alte Fahrzeuge ja auch mal weg und dann stellt sich die Frage, ob sie hier oder an anderer Stelle historisch zu dokumentieren sind. Dann können auch beide Bände nebeneinander im Regal einen Sinn ergeben.



Geblieben sind Autor und Struktur des Werks, formal geändert hat sich der Verlag: Geramond hat den Titel ohne Namensänderung zur VGB verschoben, die sie vor einigen Jahren von der Mediengruppe Funke erworben hat.

Die aktuellen deutschen Triebwagen werden mit einfachen Beschreibungen, kurzem Datenspiegel und mindestens einem Bild präsentiert. Einfache Orientierung und schneller Technikvergleich bleiben so weiterhin möglich. Bedingt durch den Umfang jedes Eintrags gehen die Texte natürlich nicht so sehr in die Tiefe, wie ein Baureihenportrait auf gleicher oder größerer Seitenzahl.

Genau das ist für die Zielgruppe, die der Verlag hier in den Fokus nimmt, aber auch ein wichtiges Kaufargument, denn ausschweifende Technikbeschreibungen jedes Bauteils hängen viele Nichttechniker auch schnell ab.

Als Buch aus einer Reihe ist der vorliegende Band als Ergänzung zum Typenatlas „Deutsche Lokomotiven“ gedacht, mit dem es dann ein fast geschlossenes Werk bildet, wenn wir Reisezug- wie Güterwagen an dieser Stelle mal außen vor lassen.

Eingang gefunden haben wieder unterschiedliche Traktionsarten. Neben Diesel- und reinen Elektrofahrzeugen ist die Fülle durch Akku-, Hybrid- und Wasserstofftriebzüge in den letzten Jahren weiter gestiegen.

Die Baureihe 612 mit Neigetechnik gehört hier fast zu den Auslaufmodellen, der Desiro HC von Siemens fand schon in der letzten Auflage knapp Eingang, aber die Entwicklung ging weiter. Bei S-Bahnen hat sich wieder einiges getan, ebenso auch bei der ICE-Flotte der Deutschen Bahn.

Entfallen sind nun einige Nostalgiker wie die letzten Schienenbusse, die Platz für neue Fahrzeuge machen mussten. Sonst hätte sich der gesetzte Seitenrahmen nicht einhalten lassen. Einen Hinweis verdient, auch wenn es sich um eine Wiederholung im Vergleich zur Rezension des Vorgängers handelt, das Einbinden von Bahndienstfahrzeugen.

Diese Züge der „gelben Flotte“ würden wir hier schließlich keineswegs erwarten, zumal sie ja eher abseits der öffentlichen Wahrnehmung ihren Dienst bei Fahrwegmessungen oder Instandsetzungsarbeiten verrichten.

Und so verbleibt auch dieses Mal ein runder Eindruck in unseren Köpfen. Die vorliegende Lektüre ist ausgewogen wie umfassend, informativ geschrieben, leicht zu lesen und zu verstehen. Sie gibt einen guten Überblick über fast alles, was heute auf Deutschlands Schienen unterwegs ist und beschränkt sich dabei auch nicht auf einzelne Eisenbahnunternehmen.

Der Umfang jedes Eintrags ist dem jeweiligen Fahrzeug angemessen, was seine Bedeutung und Erscheinungsvielfalt betreffen. Auch die Bildauswahl und -wiedergabe hat keine Wünsche offen gelassen.

So macht das Durchblättern unverändert Spaß und mit Blick auf das noch handliche Format lässt sich das Buch auch gut in Tasche oder Rucksack stecken, um unterwegs auf einem Fotoausflug mal zu einem der beschriebenen Fahrzeuge nachzuschlagen.

Publishing pages:  
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## Liegewagen der DB für Turnus- und Regelverkehr **Neue, schnell vergessene Wagenklasse**

*Liegewagen als preisgünstige Alternative für eine Nachtreise mit Ruhemöglichkeit sind im Wesentlichen eine Nachkriegserfindung. Die DB stand dieser Idee kritisch gegenüber und wagte sich nur unter Druck und mit finanzieller Drittbeteiligung an die Bc-Wagen. Entsprechend vernachlässigt erschien ihr Angebot seit den Siebzigern. Im Vorbild fast vergessen, sind die Wagen bei Modellbahnern beliebt – wenigstens ein Buch stillt deren Informationsbedarf.*

Armin Gärtner  
Die Verkehrsgeschichte der DB-Liegewagen  
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Das Thema Liegewagen bildet nur ein Randthema des deutschen Bahnverkehrs, wenn wir die einstige Stückzahl der Wagen mit dem Gesamtbestand an Reisezugwagen vergleichen. Zudem verkehrten solche Wagen im Nachtverkehr, als normale Sitzwagen im reinen Tagesverkehr waren sie kaum im Einsatz.



Trotzdem sind die Bauarten der Bc-Wagen Modellbahnern bestens bekannt, denn wegen der auffallenden Schriftzüge und Schilder, die sie trugen, fielen sie auf. Touropa, Scharnow oder Hummel-Reisen stand unübersehbar an ihren Längsseiten, wenn sie Urlauber im Nachtsprung in beliebte Urlaubsorte fuhren oder von dort abholten.

Auch andere Turnuszüge wie der Alpen-See-Express haben diese Art zu reisen bekannt gemacht. Und mancher Eisenbahnfreund von heute wird sich selbst an die Familienreise mit der Bahn erinnern. Darüber hinaus setzte die Bundesbahn auch eigene Liegewagen im Nachtverkehr ein.

Doch diese blieben unauffällig: Trugen sie nach dem ursprünglichen Farbkonzept überwiegend noch das Blau der 1.-Klasse-Wagen aus dem F-Zug-Dienst, verschwanden sie später im elfenbein-ozeanblauen Einerlei und waren nur an Piktogrammen unter der Klassenziffer erkennbar. Ihrer Popularität tut das keinen Abbruch und diese Wagen stehen auf den Wunschzetteln vieler Modellbahner. In der Spurweite Z blieben diese Wünsche bislang fast völlig unerhört.

Für unsere Redaktion ist es daher keine Frage, dass hier noch ein großer Informationsbedarf besteht. Der Bestand an Fachlektüre zu Liegewagen tendierte bislang gen Null. Erkannt hat das bisher nur Autor Armin Gärtner.

Er war in seiner Studienzeit als Liegewagenbetreuer tätig und reiste mit diesen Wagen im Auftrag der ehemaligen DSG (Deutsche Schlafwagen- und Speisewagengesellschaft) quer durch Europa. Von

seinen Eindrücken und der Arbeitswelt jener Tage berichtete er in einem ersten Buch, das so großen Anklang fand, dass zwei Folgebücher folgten.

Dieses zweite haben wir bereits in **Trainini®** 2/2023 vorgestellt und ist inzwischen ausverkauft. Das dritte Buch zum Themenbereich liegt uns nun vor: Es behandelt den aus Sicht von Vorbildfreunden und Modellbahnern wohl spannendsten Teil zur Übersicht der eingesetzten Wagen.

Liegewagen waren bei der DB zwischen 1953 und 2016 im Einsatz und berühren daher gleich vier Epochen der Modelleisenbahn – kaum jemand kommt deshalb an ihnen vorbei. Als „Voitures couchettes“ war die Bauart eine französische Erfindung vom Ende des 19. Jahrhunderts, die in Westdeutschland erst nach dem Zweiten Weltkrieg aufgegriffen wurde.

Dies beschreibt der Autor in einem geschichtlichen Abriss, der erläutert, wie die Bundesbahn entgegen ihrer Erwartungen und Überzeugungen überhaupt dazu gebracht wurde, Bcm-Wagen zu konstruieren und zu beschaffen.

Deren Erfolg und internationale Vereinbarungen führten schließlich auch zum Beschaffen von Liegewagen für den Regelverkehr im Nachtzugdienst. Jenen für die Reisebüro-Sonderdienste bezeichnete die DB als Turnusverkehr. Die eingesetzten Wagen waren getrennt eingesetzt.

Weil sie folglich viele Jahre das Erscheinungsbild der Urlaubs- und Nachtzüge maßgeblich mitbestimmten, griff der Autor auch dieses in den vorherigen zwei Bänden nicht bearbeitete Thema auf: Überblick über die Entwicklung und die eingesetzten Bauarten sowie deren Einsatz und erfolgte Umbauten.

Mehr als 60 Jahre Eisenbahngeschichte fanden auf diese Weise Eingang in die vorliegende Lektüre. Sie wurde gewissenhaft, ausführlich und tiefgreifend recherchiert und analysiert, um sie in diesem Werk aufbereitet wie verständlich zusammenzufassen.

Ein großes Problem stellte das Bebildern vieler Passagen dar. Wagen stehen eh selten gezielt im Fokus von Bahnaufnahmen, aus dem Inneren fehlen Fotografien meist völlig. Aber auch diese Klippe umschiffte Armin Gärtner in für uns anerkennenswerter Weise.

Zugleich entschuldigt er sich im Vorwort für die geringere Qualität vieler eigener Aufnahmen aus seiner Dienstzeit als Liegewagenbetreuer. Neben dem Alter der Bilder ist das vor allem der eingesetzten Kamera zuzuschreiben, die ja auch in das Budget eines Studenten passen musste, der mit einer Nebentätigkeit zu seinem Lebensunterhalt beiträgt.

Genau das macht dieses Buch aber auch so authentisch. Keinesfalls wirkt es „abstrakt“ und von einem Unbeteiligten niedergeschrieben. Auch nach fast fünfzig vergangenen Jahren sind viele Besonderheiten noch in Erinnerung, die hier miteingebaut wurden und einen besonderen Reiz ausmachen.

Auffallend ist, dass die DB so zögerlich war, modernere Bauarten zu entwickeln, als das Geschäftsmodell zu kippen begann. Ein Kapitel zu Projekten und Entwürfen enthüllt, wie viele Ideen und sogar Pläne es zu klimatisierten Liegewagen gab.

Nur die TUI hatte den Mut, solche Wagen nach zeitgemäßen Anforderungen der ausgehenden Siebziger zu bauen und auf eigene Rechnung zu beschaffen wie einzusetzen. Sie waren ein Erfolg, kamen aber um Jahre zu spät.

Wir wollen nicht zu viel verraten. Dieses Buch ist eine klare Kaufempfehlung und verpasst eine Nominierung für die Neuerscheinungen des Jahres nur, weil es bereits im Vorjahr aufgelegt worden ist.

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## Märklin Days in Göppingen **Life in the Booth again**

*After an enforced break due to the pandemic, Märklin once again called its customers and friends to the fair at its home. Peter, too, is apparently a model railway enthusiast and ensured the best weather – quite helpful for an event that is also held outdoors to a large extent. And, so, it was full for Märklin Days, which also offered visitors a lot to see and experience.*

When Märklin invites us to Göppingen every two years, tens of thousands follow suit to enjoy the Märklin Days with its colourful programme. Between 15 and 17 September 2023 it was that time again, the break to the last edition was unintentionally, but unfortunately four years. The need to catch up seemed correspondingly great.

As with the previous exhibitions, the event was once again allowed to adorn itself with the attribute “International Model Railway Exhibition,” and this time for the 38<sup>th</sup> edition. Of course, this also awakens our ambition to take a closer look at how international the meeting really is on the exhibitor side.



44 1315 again played a central role: in its shelter, reminiscent of a Märklin packaging from an elevated perspective, it formed a backdrop for the many table-based sets displayed on the factory premises.

At least we perceive that after the unfortunate, perhaps deliberate end of the biennial fair in Cologne, this brand has simply moved on to Friedrichshafen without any conceptual change on the spot.

However, no one is going to make a multi-day trip just because of a colourful logo in order to be there and find out whether a trade fair can meet their personal expectations. When this flagship was first used in Göppingen, the meeting there still had the character of an in-house exhibition, because other large-scale manufacturers of rolling stock were completely absent and probably not invited either.



This was different four years ago, but every exhibition has to overcome start-up difficulties after a forced break of several years. And, so, this report should also serve to answer the questions that arise for our readers after we have critically, but constructively, analysed what was presented.

As with prior such events, the 13<sup>th</sup> Märklin Days were held simultaneously at different venues in the city. On the one hand, there is a lack of a suitable exhibition hall to unite all the exhibitors and sections, and on the other hand, the Märklin factory, and Märklineum cannot be moved, as part of the whole. With the prototype part taking place at the station and at the Leonhard Weiss track construction company, a special feature also has to be considered.



The calm after the storm: One of the three days of the fair is over, the Stauerplatz has emptied. In the background you can see the round tent with play areas and a stage where studio talks were held. On the right is the Werfthalle, a former helicopter hangar, which offered Märklin and the large accessory manufacturers a presentation area and had also once again accommodated the Z Gauge Convention.

As far as visitors are concerned, the event was well attended overall, but the three days were very different: Friday was by far the busiest day in our opinion, and, at times, there was no way through at the layouts.

The two weekend days were also good, but significantly less frequented than the “record day”. If we are basing this solely on our impressions, there is a good reason for this: the organiser did not provide us with any final information on the official number of visitors.

And since we can only be at one place at a time, we cannot make a valid statement for the others. However, the “rumour” circulated through the halls was that the visitor record of 2019 had not been met, but that the 13<sup>th</sup> Märklin Days is the second best attended edition. Incidentally, around 60,000 people were expected over the three days.

## World record and great exhibits

In view of the focus of this magazine, our report should concentrate on what was shown in 1:220 scale, but at the same time not completely ignore what happened and was shown outside our scale.

And so we begin our report with a successful world record attempt, which was reported on at an evening event in front of invited guests on Friday evening by Märklin managing director Wolfram Bächle, who also invited RhB director Dr. Renato Fasciati to the lectern to provide context.



Hold your breath, will the world record work? That's what the three actors surrounded by cameras must have thought before the order for the LGB train to depart: at the lectern (from left to right) Wolfram Bächle, Dr Renato Fasciati, and Florian Sieber. Photo: Stephan Fuchs

As is well known, the Rhaetian Railway set a world record with the longest narrow-gauge train in the world on 29 October 2022. To do so, it coupled all 25 existing Capricorn multiple units together and ran them on the Albula line. The total length of all four-part units was 1,910 metres.

This sounds very simple to a layman, but the director of the railway company talked very vividly about the many pitfalls that had to be navigated around: all 25 trains had to be supplied with electricity at the same time, the couplings are exposed to enormous loads and must not break. Special precautions, coordination, continuous communication and absolutely synchronous manoeuvres are required.

At times, those involved could not believe that it had worked out in the end. In his own company, the railway boss was initially met only with astonished faces and shaking heads; only from the media did he receive signals that he was on board. When he asked for the reason, he was told: "It's simple: your attempt can also fail."



A personal conversation at the buffet – the editor-in-chief of this magazine was also involved in two unchanged world records – took place with mutual understanding and respect for the enormous efforts. Media pressure can also be a burden at times, but at the same time it can also be a challenge.



The train of 25 Capricorn units is on the move! Accompanied by the cameras, the suspense remains as to whether it will also make the specified distance without breaking couplings or encountering electrical and mechanical problems. A few minutes later we know it has succeeded! Photo: Stephan Fuchs

In Göppingen the same now applied to Märklin, and Dr. Fasciati provided the same assistance to the two Märklin managing directors that he himself had received from Göppingen a year earlier. An identically assembled LGB train was now to run in the EWS Arena and also would break this world record for model railways. After all, that's about 85 metres of train length still on the scale of 1:22.5. Many of the problems that the RhB had to solve, also had to be overcome by the house of Märklin.

It worked with a large number of witnesses, which our reporter Stephan Fuchs also wanted to make sure of. However, it was not reproducible on the following days, because the many couplings did not want to take part for more than three days. Of course, they hardly corresponded to the series standard anyway, but the project succeeded thanks to thorough preparation.

While the action took place in the stands of the handball hall and there were also traders in the gallery, the main attraction for the visitors was the playing field. Here, various 2m gauge layouts were set up and captivated with great motifs, including a Big Boy that ran over a hand-built wooden truss bridge ("Trestle Bridge") again and again.

On the premises of the construction company Leonhard Weiss (LW), track-laying machines and locomotives could be seen, some of them from below the maintenance pit, if desired. Bouncy castle and trolley rides delighted the youngest visitors there.

A new product 2024 for the H0 scale was already announced there. This did not happen by chance, because LW presented its prototype there in a premiere to the visitors. We are talking about the dual-mode Vectron 248 040, which looks extremely good in the company yellow. Subsequently, we were frequently asked for our own assessment of whether Märklin could also plan this model in Z gauge?



Leonhard Weiss used the exhibition to present its new Vectron Dual-Mode 248 080. At the same time Märklin announced a gauge H0 model as a new product for 2024.

In the meantime, it has become a tradition of the Märklin Days that the Leonhard Weiss area is given a bit of model railway, so that there is always a direct link to Märklin. The regulars' table MIST 7 presents itself there with an approximately 45-metre-long modular layout on which the Karwendelbahn trains are handed over from block to block to a new "dispatcher".

The tour continued by rail bus transfer to Göppingen station. Each time, we find more modern and historic prototype vehicles here. Because of their great attraction, steam locomotives are the main focus of attention.

Some of them also invite visitors to take a ride in the driver's cab. In 2023, there were significantly fewer vehicles than in previous years, and at the same time there were no vehicles that do not (yet) know a Märklin implementation and used to be a hidden announcement of new products.

Nevertheless, it was exciting to see and hear old acquaintances puffing away. And with 151 001-5, there was also a pre-series locomotive on which connoisseurs wanted to study the differences to the following series-production specimens. A 140 can also look really good if it is given a metallic paint that looks like the delivery colour from a distance and shows its true characteristics up close.

Göppingen station is the only venue (apart from the factory tour) that can be entered without a ticket. We mention this because the prototype area, as a publicly accessible site, is not locked, even at night.





Always besieged were above all the steam engines, whose running gears, cylinders and linkages fascinated many visitors in particular (photo above). A special guest this year was pre-series locomotive 151 001-5 (photo below), on which it is worth comparing its differences to the series machines.

Once the visitors had disappeared at the end of an exhibition day and the fire in the steam locomotives was extinguished, a heavenly calm returns with the onset of summer twilight. Free from the crowds of visitors, there are then completely undisturbed views of the great prototypes.

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Hp00 shows the signal in front of 194 192-1, as she is to be exhibited during the entire event. At blue hour, the German Crocodile “cheats” its way into the photo in a particularly striking way.

Photo on page 48:

Almost a regular guest at the Märklin Days is 38 3199, returned from Romania, in Prussian livery, but with a Reichsbahn number, reflecting the operating condition in the early 1920s. Both photos: Jan Szymkowiak

The machines are then bathed in a very special light from the last sunlight and later from the many lights. Our reader Jan Szymkowiak from Kiel made skilful use of the “blue hour” to take particularly moody photos of the crocodile and steam traction. Enthusiastic about his motifs, he immediately agreed to share them here with all other readers.

## Factory and Märklineum

Märklin's main factory was also always well attended. That is not new either. However, it was important to note that factory tours were only possible on Friday and Saturday between 9:00 am (start of the event) and 3:00 pm. Sunday, as a protected day, must not be used for work.

We have already reported several times on the production processes, of which zinc die casting, brass investment casting, electroplating and also the turning of very small parts are reserved for the Göppingen plant. We do not want to repeat that here and refer to a **Trainini TV** episode that will be published soon and will also include a tour of this section.

However, it is still worth mentioning how effective Märklin's training workshop was in terms of publicity. Not only were contact persons available to talk about training courses at the company and to answer questions. The apprentices and dual students also showed how they put the skills they had acquired into use.

Visitors were allowed to take away a yellow-transparent drinking cup as a souvenir, which was a teaching piece from the injection moulding area. So, even at home, it will not be forgotten that Märklin can be a



At the entrance, to the factory tour, Märklin had set up the driver's cab of 110 144-3 - an exhibit from the days of the "Märklin Experience World," whose whereabouts after its abandonment were unknown to us until now.

good alternative for young people who are enthusiastic about model railways in view of the increasingly lamented shortage of skilled workers.



With open eyes during the tour, we discover turned parts for Z gauge wheels on the lathes of the Märklin factory in Göppingen.

Once outside again, hunger and thirst soon made themselves felt.

In the shade of the LGB open-air layout and the 44 1315 "packed" like a Märklin model, it was possible to fortify oneself at the tables around the food stands.

Not all of those who lingered there will have noticed the new truck trailer with Märklin design that was standing nearby in the company yard.

This may also have been due to the long queues that formed at the special sale tent. The treasure trove in the Märklinium was closed for the fair and so many sensed attractive special offers here. Unfortunately, we cannot say whether this was true.





Many guests at the Märklintage did not miss the opportunity to take a walk through the Märklineum after the factory tour. There, the large show layout also impressed with its clever lighting and projection effects on the background.

As not all areas were able to cope with the large crowds this weekend and the Märklineum, was scheduled as an optional additional tour after the factory tour, two interactive areas remained separated with flutter tape and were closed off. As we know from last year's opening ceremony, it is precisely these interactive areas that set the Märklineum apart from other attractions.

However, we did not perceive any criticism of the way visitors were led past the two points. After all, the overwhelming majority had paid the discounted entrance fee primarily to experience the large display facility on the lowest floor up close.

The illustrated reports in the Märklin magazine will have contributed significantly to this. With the elaborate weather and night projection on the background, also accompanied by thunder during thunderstorms, it has an almost unique attraction.

And even those who couldn't afford an admission ticket were in good hands in the museum shop area and the display cases with current Märklin products. These showpieces behind glass are, after all, special treasures for every model railway enthusiast who wanted to have a good look at them in peace and quiet.



Märklin had photogenically set up its new semi-trailer from Hungary in the factory courtyard, but only a few visitors seem to have noticed and photographed it.

In addition, the special atmosphere that gives the impression of being in a round shed with an indoor turntable is also nothing ordinary. Somehow everything seems “typically Märklin” and yet unusual as well as new. Many visitors took a personal look at the factory museum for the first time and were enthusiastic about how classic and modern railways are brought together here in the model.

Now, at the latest, we were probably not the only ones who wanted to see what model railways had to offer elsewhere. We therefore continued to the Stauer Park with the shipyard hall, where helicopters were once maintained and repaired, and a tent city. In the tents, N, H0 and 1 scale layouts were again on display, some of which were particularly appealing.

The larger suppliers such as Busch, Faller, Noch or Viessmann (with Kibri and Vollmer) were represented in the hall and presented autumn novelties or innovations from the spring. Trade fair sales were also a central component.

Also present were publishers such as Eisenbahn-Kurier or VGB | Geramond as well as the figure manufacturer Preiser. Here, as at every major exhibition, a demonstration was given of how a posing model is carved in wax by the artist. Some of you may have wondered whether this was a show or everyday life, when suppliers such as Faller or Noch have long since produced human replicas of at least the same quality on computer screens.



Many other suppliers were represented, some with the familiar programme and often without new products, which is why they are not highlighted separately here. Märklin speaks of more than 100 exhibitors, of which some came from outside Germany.



At least in Z gauge, no new products were presented by the accessory manufacturers exhibiting in the Werfthalle. However, a visit to the stands was well worthwhile for advice and trade fair offers.

## Hidden and innovations

In the information box at the end of this article, we have (hopefully) listed in full those who run or make Z gauge offerings. Some were not easy to find and were off the beaten track. For example, Klingenhöfer Miniaturen, a figurine manufacturer for our scale, was quite hidden in the back corner of tent 1.

In view of the short time available for preparing the report, we did not even pass by there. So, we can only hope that at least the customers found the stand and perhaps searched for it specifically using the stand directory.

This is also where a quiet criticism of the event comes in: Tents are certainly not ideal places for model railway exhibitions. A large Z gauge layout that had not been shown for a long time was also withdrawn because it was to be allocated a place in the tents. On the bending wooden floor, however, a safe operation of the trains cannot be guaranteed.

Then there are the enormous climate fluctuations depending on the weather. Ten years ago, heavy rain showers poured down on the festival grounds and the water seeped through the tent seams and dripped onto the exhibits. This year, the sun burned down on the tent shell for hours and made it unbearably hot inside most of the time.

It is also unfortunate, as in the case of Klingenhöfer, when Z gauge is concentrated in one place and a single vendor sets up his stall far away from it. It will not be possible to attract walk-in customers with chance finds and occasional purchases in this way.

Now, we would like to let some figures speak for themselves: The total area of the Stauferpark is about 17,000 m<sup>2</sup>. Of this, the outdoor area takes up about 8,000 m<sup>2</sup>, which was occupied by play areas for young and old, as well as seating with catering. This leaves 9,000 m<sup>2</sup> for exhibitors in the Werfthalle and the tents.



We show this photo of the King's track 1st photo as a representative of the facilities worth seeing and what else is on offer in the tents. Photo: **Trainini®** / Hendrik Späing

Part of the covered area was reserved for a world of games and wagon painting in the separately standing round tent. There was also a stage where various interviews were conducted. We recorded parts of one of them with Managing Director Wolfram Bächle and Sales Manager Gunther Schneider and will show it in our upcoming series of contributions from **Trainini TV**.

Before we get to the presentation of the Z gauge, we must now take a brief look at Märklin's own stand parts. The usual range included showcases with the latest innovations and an area for assembling your own H0 gauge locomotive. Contact persons were also available at all times to answer all questions from visitors and customers.

Smaller and larger show layouts for gauges 1, H0, and Z are also part of the self-presentation. The Märklin Insider Club celebrated its 30th anniversary, inviting its members to a glass of champagne and giving them a useful souvenir in the form of a small plastic bandage box.

Repairs could also be submitted in Göppingen or collected again in individual cases, provided this had been agreed beforehand. The presentation of Märklin Engineering, which dealt with prototypes, new technology perspectives and prototype work, was densely attended.



The aim was to show how ideas are implemented in order to test their usefulness, economic feasibility and future viability. Michael Zauner, for example, showed us how measuring injection castings have been further developed in the company since the first locomotive (series 80) was built using this technology.



Two examples of what Märklin offered in the Werfthalle: The popular telephone boxes made of brass injection casting were offered again, and Märklin Engineering had now also created a draw well as a demonstration object (picture above). At the MHI stand, the Württemberg passenger car (art. no. 98181) found a buyer after the forced break.

A large black box turned out not to be the proverbial “black box” but a state-of-the-art 3D printer inside which crystal clear and different coloured resins were assembled into vases or “glass blocks” with what appeared to be a locomotive cast in them.



Special wagon for the Märklin Days 2023 (80923; photo on the left) and the current one of the MHI (82032; photo on the right) with a metallic paint job.

This technology, which could be viewed here and was also explained on request, is also used to create the 3D printed figures announced by Noch, which are now to be delivered soon. Märklin also showed prints that corresponded to an electric locomotive in about the size of the Zf scale. However, the new technology is intended for the H0 scale and selected spare parts.

Collectors were also catered for. Four special wagons were up for grabs on site. As each time, a G 10 car was chosen to commemorate the Märklin Days 2023 with a historical motif (item no. 80923).



Since the beginning of the Z Gauge Convention South Germany at the Märklin Days, there has always been a special commemorative wagon. In 2023 it was this yellow Fals side unloading wagon (80520).

Since the Z Gauge Convention in southern Germany was also held at the same time, a bright yellow Fals side unloading wagon (80520) was dedicated to this part of the event. MHI jumped on the bandwagon many years ago, this time offering a metallic blue painted old tank car with historic Märklin inscriptions (82032). The noble-looking colour scheme really suited the model.

A fourth and last special model was actually already three years old, but could not be put into circulation as planned because of the pandemic. This was made up for there and so a dark green-beige painted Württemberger passenger car (98181) also went over the counter. Its nominal occasion was “30 years MHI - 15<sup>th</sup> general meeting 2020” – printed on both sides with the collectors' community in mind.

## The Z Gauge Convention

The side room, which was reserved for Z gauge and was the location of the “convention,” was not only in our opinion the one with the most sophisticated and beautiful layouts. Our community can pin this praise on itself as a lasting impressive group achievement and should not forget that.

We add that it was also the only area where not only the visitors fully met the demand for internationality. The exhibitors came not only from Germany, but also from Italy, Poland, and Austria. On the visitor side, of course, there were many other nations as well.



What was shown in the Z gauge could easily be walked in one round, provided that the occasional great narrowness in an approximately three-metre-wide foot corridor did not prevent this. The layout “L'Isle-sur-Sorgue / Provence”, the last one Wolf-Ullrich Malm built before his death, remained largely unaffected.

It was demonstrated by Rainer Tielke and Thomas Stobbe, who were familiar with the control programme and the rolling stock, most of which they had built themselves. And so many visitors were able to enjoy the lovingly designed motifs of the sunny region of France. The TGV, which regularly frequented the track, also caused astonished looks. In contrast to the other long-distance and local trains, it did not stop at the station.



L'Isle-sur-Sorgue / Provence” was Wolf-Ullrich Malm's last major layout. Unfortunately, he was no longer able to exhibit it personally. But it found its way to Göppingen – including a self-built TGV with 3D printed parts.

The direct stand neighbour was Aurelius Maier (Design Modellbahn). He exhibited one of his Plexiglas layouts, which are intended as sophisticated living objects with a model railway connection. Their effect resulted from the combination of transparency, movement of a train, coloured scattered lights as well as plays of light and shadow. With photos it is hardly possible to show how everything together affects the viewer.

From here, we started the announced counter-clockwise tour and first headed for the ZFI stand. Here, Sylvester Schmidt ruled with the best overview of all exhibits and exhibitors, as he had played a central organisational role in the process.

In addition to a Märklin display case with showpieces of previous ZFI annual wagons and special editions, the glass cabinets also deliberately contained a cross-section of various small-series programmes in order to show guests from outside the track world what they will not find at specialist dealers and yet significantly enhances the range.





The design objects by Aurelius Maier play with light and shadow (top photo). Small-series novelties shown at the ZFI stand included the trike driver by Trafofuchs (photo below left) and the oil boom crane by HOS-Modellbahntechnik (photo below right).

HOS-Modellbahntechnik, for example, demonstrated its oil boom crane there, which is part of the depot equipment also on display. There was a colourful selection of figures from Trafofuchs, from which for us a well-known television steam locomotive with axle sequence B from a puppet show.

To make sure that this “reception area” did not look static, the “50 Year Mini-Club” and “Dead End City” box sets were also waiting here to be activated at the push of a button by the visitors and to let a train run across the oval.

The oval as the basic shape of the routes was much larger for Jürgen Wagner. “Along the Rhosel” he led the spectators in epoch IV and inspired them with motifs of vineyards along the Rhine valley and the Moselle, which together give the rectangular layout its name.

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The Wiener Z-Stammtisch promoted its layout idea and ÖBB train operation (photo above). The popular theme of water was depicted by Gotthard Schmidt with his harbour scene (photo below).





A marvel of craftsmanship, creativity and technology is Raffaele Picollo's Wuppertal layout (pictured above), which also made the convention a bit more international. A classic, on the other hand, is the "Bridge on the River Kwai" diorama exhibited by Railex, which was once based on a Faller kit from H0 scale.



The Wiener Z-Stammtisch (Vienna Z Club) was also present: as in Altenbeken, for example, its active members had made it their task to demonstrate smaller case layouts and their club layouts that had been put together to form a modular layout, in order to show how trains running in a small space can be viewed in several different scenes at meetings.

Gotthard Schmidt, as the neighbouring stand, again demonstrated the revised harbour layout that he has taken over. It also seems to have been inspired by the port of Hamburg, because here too we find a musical theatre in the immediate vicinity. In any case, the railway takes a back seat as cargo ships, museum ships and a sea rescue cruiser dominate the scene.

Raffaele Picollo had travelled from Genoa and had once again reached into his bag of tricks. He had continued to build on his Wuppertal layout and further enhanced it. The suspension railway as the real eye-catcher almost takes a back seat, because in the background a circus draws attention to itself on the fairgrounds.



Alois Forstner has incorporated various motifs from Munich on his case layout. The rolling stock used also deserves attention, as many vehicles were or are equally typical of the city and region.

Meanwhile, the miniature golfers on the opposite side were actually rolling the ball. And those who looked closely also spotted a trip by Mr Bean with his mini packed to the brim, which banished him to the roof itself.

Roland Kimmich and Christine Specht recalled the now defunct Railex brand by means of models of various kinds that they brought along and the “Bridge on the River Kwai” diorama, which had also made it to 1:220 scale in Stuttgart.

Alois Forstner's passion for football could not be overlooked on his wide suitcase layout, as he had immortalised the Munich stadium in the left section of the layout and was also able to accommodate a miniature of the Olympic Tower. But the camera train that ran along the tracks and showed this layout from the locomotive driver's perspective caused a lot of excitement.

One of the longest journeys was made by Oliver Kessler, who lives in Poland near the Ukrainian border and runs his company there. This company is responsible for the Z-Car system, which has been vigorously developed in recent years.

In addition to the self-driving buses and trucks, many other vehicles have been able to join them because chassis of different lengths and designs have made their use more flexible. However, Oliver showed how several car models are steered on intersecting roads so that they stop at intersections or buses pull over to the right at the bus stop.

## Other highlights

The modular layout "Ratekau and surroundings" of the Eisenbahnfreunde Schwartau with various motifs from Ostholstein was also worth a longer stay. The fieldstone church was installed the other way round this time and was easier to view, but Jan Tappenbeck and Hans-Jörg Ottinger had also installed very personal motifs in the meantime.



Torsten Schubert always attracts attention with his many flashing light exhibits. His creations were there right from the start and have continued to grow over the years.



So, some dogs were also on the road as models with their owners and in the case of “Otti” we could even take a look at his house and those of his neighbours. But his own developments, which he sells as kits and which were ready to be used as models here, also caused enthusiasm.



The fieldstone church from Ratekau is a central point of the module layout from Bad Schwartau. Around it there is also bus traffic with vehicles from Z-Car-System.

Personal enthusiasm prevailed because of the announced rectangular site “Hirschsprung” by Jürgen Walther. It could be seen a few years ago in Altenbeken, but at that time it was not yet fully developed—especially the trees on the slopes were missing.

In the meantime, Jürgen had finished all the construction work and was already at shows with her again. Only a personal photo session was not yet possible. Finally, we were able to make up for it and when we drove around the layout with our eyes, we discovered many details worth seeing and exciting perspectives that do not open up with simple supervision.

Gerhard Maurer also travelled to Göppingen, but he did not demonstrate any of his layouts, instead he showed Manfred Forst's showcase with Hunt's large coaling system, which Märklin had produced a few years ago and which is particularly effective in this depot diorama.

Texas' landscape and its oil fields form the backdrop for the “Red Rock Canyon” layout with which Birgit and Volker Brock (Trafofuchs) had travelled. This object had been extensively overhauled by Birgit and equipped with new figures.

continues on page 66





Photo above:  
On the Ratekau site, Hans-Jörg Ottinger has also recreated his own street with his house and those of his neighbours. Family dogs, which were only handed over on site, are now also used there as figures. In the background, one of his self-built locomotives is in action.

Photo below:  
Figures also play a special role on Trafofuchs' Texas layout: Who recognises the two celebrities sitting on the wall of the tank?





**Photo above:**  
The rock to the left at the edge of the picture gives its name to Jürgen Walther's new installation and also found its way into the depiction because of its famous legend of how the forest animal escaped from its hunter.

**Photo below:**  
Hirschsprung station is depicted in its historical state, i.e., in the days when trains still stopped and crossed there. It has often been used as a model for various layouts, but the buildings were always a compromise with the PS kits of the big accessory suppliers. This time everything is right!



Besides railway photographers taking photos of the passing goods trains near a level crossing with a flurry of flashes, we also spotted a well-known TV couple from Sesame Street sitting on a wall.

Our man for **Trainini TV**, Ralf Junius, showed his first work “Kistrath”. This was a showcase with a curved single-track main line from which a siding led into a small company area. The railway operation is characterised accordingly by trains passing through and service trips by the siding operator, whose loading ramp became accessible after opening a gate.

Torsten Schubert (Z-Lights) had set up his Z gauge fair and showed many illuminated models, which only partially follow the 1:220 scale. An important contribution from his side were again explained soldering demonstrations, which are supposed to take away model railway enthusiasts' shyness about this technique and to teach tricks. Birgit Foken-Brock also gladly resorted to the workstation to show visitors how she colours her 3D printed figure blanks.



Winzlingen is in Swabia, says builder Thomas Heß. But, in any case, the operation on the small suitcase system is digital and is a good demonstration piece for this control technology.

Wilfried Pflugbeil from Chemnitz announced his appearance with “Wiesenthal” as his last exhibition. We have already presented this sophisticated layout with many moving scenes in detail and awarded it Exhibit of the Year in 2020.

His announced withdrawal as an exhibitor was received with great regret, but seemed understandable to the astonished interlocutors when Willi pointed out his age, now 80, which hardly anyone had guessed who did not know him better. He assured us, however, that he still wanted to appear at exhibitions as a visitor and then enjoy them himself. We look forward to hopefully many personal meetings and wish him good health for the future!



For some, the “Dingenskirchen” console railway by Matthias Vogel may take some getting used to. As the only showpiece on site, it deliberately refrained from landscaping and only hinted at an environment around the tracks with individual buildings and a locomotive shed.

This exhibit is intended to emphasise the warm colours of wood and to stage the building material in such a way that it knows how to fit into the domestic environment like a piece of furniture. A new demonstration installation in the usual manner with greenery and paths is already under construction – but nothing is to change in the future with regard to the console track, which was scheduled for display for the last time.

The name said it all for Thomas Heß, who knew how to fit “Tiny Things” into a suitcase. The circuits of this small layout were operated digitally, which was probably only noticed by those who took the time to take a closer look. Then it could also be noticed that individual locomotive models made operating noises.



**A branch line idyll and a time that is only rarely depicted form the recipe for success of “Draxlried.” A special feature of Hagen Sroka, however, are the many Era I vehicles that he has modified or built completely himself.**

Equally controlled was “Draxlried” by Hagen Sroka. He had lovingly collected Bavarian motifs and recreated the romance of a branch line on the K.Bay.Sts.B. Hand-built, Bavarian shaped signals were just as much a part of it as a loading crane or the station's reception building.

But the rolling stock didn't come off the peg either, because Hagen is also an enthusiastic and restless hobbyist who prepares one model after the other for his layouts according to his personal taste and makes it ready to run. So, it made a lot of sense to crouch down and let the light trains with tiny locomotives slowly pass by in order to catch all their details with the eyes.

With this end to our tour, we sum up that we were able to see some familiar exhibits again, although they had not yet been exhibited in this region. Some were passionately built on and given new finishing touches that invited us to linger again.

At the same time, there were also great installations to see that had not yet been widely exhibited and were actually celebrating their premiere here. They were complemented by some classics, some of which were also given a new look.

All of them together resulted in an appealing and colourful mixture with which our Z gauge could be presented in almost full width and in great variety. More clearly than before, we also heard appreciative praise from model railroaders outside our own scene. And that's what makes us happy and proud at the same time.

That's why we should make a note of the next industry get-together in Göppingen in our calendars. It will take place from 19 to 21 September 2025. We can be curious to see what things worth seeing will be presented at the same place again!



**Relevant Z Gauge exhibitors:**

- <https://artitec.nl>
- <https://www.busch-model.com>
- <https://www.design-modellbahn.de>
- <https://www.eisenbahn-kurier.de>
- <https://www.faller.de>
- <https://www.germond.de>
- <https://www.hack-bruecken.de>
- <http://www.hos-modellbahntechnik.de>
- <https://www.klingenhoefer.com>
- <https://www.maerklin.de>
- <http://www.mazero.de>
- <https://www.modellbahnunion.com>
- <https://www.modellplan.de>
- <http://www.mrztrax.com/Zcatalogue.html>
- <https://www.noch.de>
- <http://www.peter-post-wekzeuge.de>
- <https://www.preiserfiguren.de>
- <https://www.rainer-tielke-modellbau.com>
- <https://scenery.shop>
- <https://www.sommerfeldt.de>
- <https://tams-online.de>
- <http://www.trafofuchs.de>
- <https://viessmann-modell.com>
- <https://www.vgbahn.shop>
- <https://www.weichen-walter.de>
- <http://z-car-system.de>
- <http://www.z-lights.de>
- <https://zotti.lena-johannson.de/shop>
- <https://www.z-freunde-international.de>
- <https://z-stammtisch-bayern.de>

**Trainini TV – Folge 18:**

- <https://www.youtube.com/TraininiTV>



# TAG DER MODELL- EISENBAHN

INKL. SCHIFFS- & MODELLBAU



Museum  
der Deutschen Binnenschifffahrt  
Duisburg-Ruhrort

## HOBBY-SHOW

MUSEUM DER  
DEUTSCHEN  
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1. ADVENT // SA & SO

02. & 03.12.2023 // 10-17 UHR



Weitere Informationen finden  
Sie unter [www.spur-n.com](http://www.spur-n.com)

Readers' letters and messages

## Zetties and Trainini in Dialogue

**Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.**

**Great pleasure about the exhibition report:**

The pre-announcement of the September issue of your magazine with the corresponding cover photo already made me very curious. This morning I opened my laptop and read the report about your visit to the museum in Schramberg with great anticipation.

I would therefore like to praise you for your successful reportage and the illustrative photos.

I am very pleased that you are doing an excellent job of promoting the Schramberg Railway Museum with your comprehensive report and that "my" Z layout in particular, I believe, is well received there. My special thanks for that. Now we are looking forward to Trainini TV!  
Eckard Jehle, Karlsruhe



Answer from the editors: The article on **Trainini TV** will unfortunately have to wait a while, but will of course follow as promised. At the moment, our focus is on creating one for the Märklinage. Due to the large scale of this exhibition, this is connected with a large time commitment.

**Our tender building project is motivating:**

Enclosed are two photos of my tender. The artificial coal was removed and the tender was filled with styrofoam. Since styrofoam is made up of many balls glued together, a knife was used to create an almost flat surface. Sifted bird sand was used as coal. To get the black colour, black paint (Revell) was dribbled on the bird sand.

After drying, a coal-like structure is created. Tinkering tip from Märklin magazine from 1997 or 1998. (...) What I will still change are the ladders. How do you get up such a ladder? The starting point were the ladders from Laffont made of plastic or from Klingenhöfer made of brass.

I then decided on the brass ladders from Klingenhöfer because the ladders are almost the same width. I then cut the ladders to length, primed them and then painted them with black paint (RAL 9005), but did not attach them to the tender yet. The existing ladders on the tender will be completely removed, and then replaced by the painted brass ladders.

In the photo you can see the differences, but you can also imagine the result. Even though the new ladder protrudes a little closer to the rails, I am confident that smooth traffic is possible.

Hartmut Schnittjer, by E-Mail





With coloured bird sand our reader knew how to recreate hard coal in prototypical effect, which he would like to show us on this tender. Photo: Hartmut Schnittjer

Editor's reply: We are pleased about this feedback, because it shows us that construction contributions motivate, spur on and challenge our readership. At the same time, we have identified a need for this type of tender and addressed it to NoBa models. If we may make one more suggestion for optimisation: The coal protrudes a little on the outer sides, which does not suit a pouring cone. As long as the glue has not set, the carbon insert can still be carefully pressed down at the edges with a toothpick.

**Note on a new accessories manufacturer:**

I had contact with a French manufacturer of model scenery. He produced a typical bridge in H0 and N. I asked him if he was prepared to scale the bridge to Z. He was very enthusiastic and rescaled the bridge to Z scale for me.

From now on, there is a new Z scale scenery manufacturing in France. It is no problem to contact him in English. Communication is excellent. This is the link to the Z items: <https://cites-miniatures.com/categorie-produit/z-1-220/>.

Pieter Willems (Belgien), by E-Mail

Editor's reply: We are pleased with this tip and will pass it on to our readers straight away!

**Continued at the 1zu220-Shop:**

Even if the item number 98202 falls out of the series of the first wagon series (98160 to 98168), the Westheimer wagon series of the 1zu220 Shop (<https://www.1zu220-shop.de>) now finds a continuation. A new heat protection wagon in white paint and with a roof in white aluminium is the tenth in the series. It advertises the "Alte Wildsau" beer brand, which also comes from the local brewery at the company's location.



The Märklin “Alte Wildsau” heat protection car (item no. 98202) continues the exclusive Westheimer beer car series at the 1to220 shop.



Soldering and work station with magnifying lamp (art. no. 170556). Photo: Faller

As our photo shows, the model intentionally conveys the convincing impression of a heat protection car on a G-10 basis, as they once ran in large numbers for breweries, hired by the Bundesbahn. This model is again a wagon produced by Märklin and designed with tampon and digital printing.

#### Faller autumn new products 2023:

Shortly after the editorial deadline, we received access to the product photos of Faller's autumn novelties. Their new products brochure is 32 pages long. And in it are not just two new kits for our scale.

The “Langwies” station (item no. 282702) is a replica of the original station found on the picturesque Arosa Railway (Rhaetian Railway) in the Swiss canton of Graubünden.

Soon, it will also be able to decorate 1:220 scale layouts in its alpine and wooden style. Since its appearance is not necessarily reminiscent of a reception building, a different use is also conceivable. As with the “Langwies” Alpine House (282772), it is also a laser-cut kit.





After a long time, Faller once again focuses on Swiss Alpine buildings: railway station (282702; photo left) and Alpine house “Langwies” (282772; photo right). Photos: Faller.

Other new products include useful aids and tools: a dedicated figure glue (170485), a pick & place tool (170557) for precise placement of the smallest parts, and non-magnetic tweezers (170558). The soldering and work station with magnifying lamp (170556) can be helpful for many projects, offering many possibilities to hold, fix or bring objects to be worked on into the appropriate work position.

#### Two new models from EtchIT-Modellbau:

“It’s already rusting in the catalogue” was a contemporary comment when the Renault R4 was mentioned. Nevertheless, the small car was popular and found buyers in Germany, as well. In its classic red colour, it is still very familiar to the author of this report.

EtchIT-Modellbau (<http://etchit.de>) is now creating a memorial to him with a new product in 1:220 scale. The kit made of the usual fine 3D-printed parts (art. no. ET030a\_Z) has been available for order since last month.



The Renault R16 (photo left) and the smaller Renault R4 (photo right) are shown here as N-gauge samples from the accessory manufacturer, but identical to the Z-gauge miniatures. Photos: EtchIT-Modellbau

The larger R16 with hatchback, which appears as the second new product (ET036\_Z), came from the same prototype manufacturer. It too is based on a French vehicle. Both car models consist of two parts (body and chassis), which can be painted separately and thus do not require any masking work.

#### Beer wagon at Full Throttle:

Friends of the FT refrigerated wagon series should be pleased with the new product for October 2023. Full Throttle continues a journey along the Great Lakes and found the fitting template for its beer wagon series (article group 9400) at the Erie-Lackawanna RR.



The appealing-looking "Beck's Beer" refrigerator wagon (art. no. FT/B 9408) continues a new series of wagons. Photo: WDW Full Throttle

It also leased older, wooden refrigerated trucks to the Magnus Beck Brewery, which served its market with its "Beck's Beer" brand, not to be confused with the similar-sounding German beer brand, from Buffalo until 1956.

At the turn of the 20th century, the refrigerator cars were refurbished and received a steel substructure, Bettendorf bogies and a new brake system. In this form, the models (art. no. FT/B 9408) will soon be on Z gauge layouts.

#### More Märklin deliveries again:

After a temporary standstill, Märklin is continuing more extensively again in October, and a few more new products have been announced for delivery for the Christmas business. The class 86 tank steam locomotive (item no. 88693) has now arrived in stores in its entirety. This model is technically up to date and is lettered for the Deutsche Reichsbahn-Gesellschaft (Era II).

Also completely arrived at the dealers is the electric locomotive of the series Re 4/4 II of the SBB in green (88593). It also corresponds to the modern state and shows a Swiss light change. Feature of the model for epoch IV are the lower top lights in round shape, i.e., before the later modernisations.



A real surprise is the train set "Bauzug EfW" (8172), which we only presented as an autumn new product in the last issue. A few days later it was already available. For this reason, we are only showing one photo of it today and refer to our descriptions in the previous issue.



The EfW construction train (item no. 81372), which Märklin announced only a few weeks ago as an autumn novelty 2023, has already been delivered.

The second message car for Z gauge (82932) was only vaguely announced. Unveiled on 5 October, it could be ordered one week later. As a result of the high demand, not all interested parties were able to place their orders and their purchase requests were cancelled during the validation and payment process.

A GI Dresden without brakeman's cab was again chosen as the basic model and printed with a Van Gogh motif for the second time. The theme this time was the artist's love of nature, which he expressed in the portrait of a blossoming almond tree. The design implemented by Märklin is correspondingly lively and harmonious.

#### High release rate at Yellow Dwarf:

Not a month goes by without the kit supplier Yellow Dwarf (<https://www.yellowdwarf.eu>), which uses 3D printing to close many programme gaps and make ideas tangible, introducing new products. In October, many different topics will be covered at the same time.

The graves (art. no. 60411), which also include a single coffin, create a final and dignified resting place for the dead on a scale of 1:220. After the collapse of the Eastern Bloc and in view of current events, the Red Army Monument (60026) seems somewhat out of time.

The four fishing boats (60103) bear a somewhat misleading product designation, as they are rowing boats for anglers. The occupants, who may have been waiting for these new products, will consequently cast their fishing rods from there and not nets.



The graves (art. no. 60411) are certainly not a desirable topic, but they belong to our everyday life, and, thus, also to the model railway. The new product has been lovingly prepared with all the important features. Photo: Yellow Dwarf

Commonplace for us and yet not represented on the model railway so far is the DHL packing station (60090). The Boy Scout Camp II (60505) will probably bring back memories of their youth for some buyers. However, the tents built on a wooden base frame seem unusual for German regions.

#### Quarterly information from Ladegut Küpper:

In autumn, the leaves do not fall from the trees, but plenty of trees are felled and processed. At least that's what the duo from Spur Z Ladegut Josephine Küpper from Aachen.

Therefore, a reminder was given of the suitable loads for low-sided and high-sided wagons on the subject of timber transport: logs (Art. No. Z-Wood-1), stacks of boards (Z-60-long / Z-60-D / Z-60-H), firewood (Z-96), wooden boards (Z-158), and tied-up wooden elements (Z-159).



The new ballast load shown here is now available for Märklin's Eaos 106 open wagons. Photo: Spur Z Ladegut Josephine Küpper

A gap in the range for the Eaos 106 model high-sided wagons (Märklin 86950 and identical) has also been identified: There is now a new ballast load with two separate ballast cones on an accurately fitting



carrier board with intermediate web. The supplier pages can be reached as follows: <https://spur-z-ladegut.de>.

**The forefather of high-speed transport:**

Rokuhan delivered its Shinkansen series 0 at the beginning of October. The twelve-part inaugural train "Hikari" (Item no. T020-1 / Still 7297942) is reproduced, which heralded the high-speed era worldwide with its ceremonial journey from Tokyo to Shin Osaka on 1 October 1964.

The models make an extremely successful impression and show the typical features of the first trains of this series, which were built until 1986. Characteristic for the first vehicles are the large side windows, the original paint scheme with the blue window stripe and the slightly translucent ball of the early "bullet trains".



When the tail light is activated, the prototypical translucent ball at the tip of the Shinkansen series 0 (item no. T020-1) from Rokuhan in its original design is clearly visible.

Rokuhan has also largely reproduced the interior and cabs of the cars. There is no room for a figure, but a torso behind the windows should be quite effective after installing the factory's own interior lighting.

Electricity is drawn from all axles via the tip bearings, which is almost frictionless. However, there is no electrical connection between the train sections. Good driving characteristics are also typical for this manufacturer, thanks to the flat bell-shaped armature motors in two of the intermediate carriages.

For Japan, the trains and the Shinkansen system became a symbol for the recovery of the economy after the Second World War, because they were a flagship of high technology. Also because of this significance, we want to look at the new product in **Trainini®** 11/2023 in more detail.



Illustration: EK-Verlag

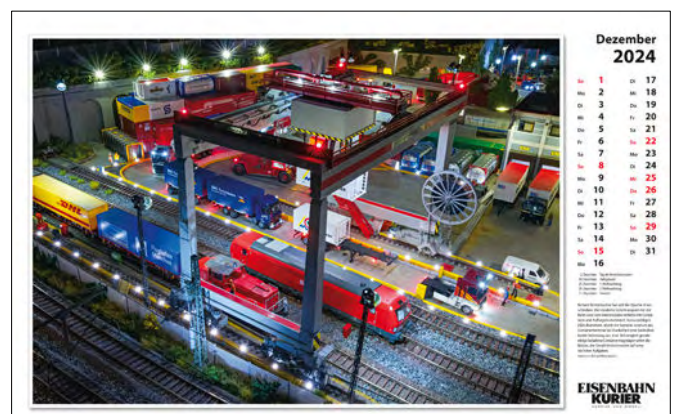
**Calendar offers for 2024:**

As autumn approaches, publishers announce their photo calendars for next year. They traditionally feature steam locomotive and construction series themes, but also appear with motifs of trams and historical vehicles or agricultural machinery, but do not forget the model railway.

This applies to both VGB | Geramond, where we miss the well-known Miba-MEB photo calendar for the first time and only find one with photos of new Brandl layouts, and Eisenbahn-Kurier. We have selected the 2024 model railway calendar from their programme for a brief review.

Even the appealing cover photo with the popular VT 115 arouses curiosity and captivates the eye. That is why we also took a closer look at the twelve-monthly motifs. Although the EK editorial team was so fascinated by the abundance and quality of the Z gauge layouts on display at the Märklintage, none of the motifs in our scale made it into this calendar.

Nevertheless, it is worth buying and hanging up even for model railway enthusiasts of our scale. No less than ten motifs impress with their generous landscapes, which attractively highlight the photographed trains with harmonious colours.



We have selected the motifs for February and December 2024 as examples to underline our impressions. Illustrations: EK-Verlag

A crane terminal motif at night and an industrial theme that fits well with the article “Abandoned Places” in this issue complete the selection. Although the scales shown are limited to 1:87 and 1:45, and Era III is conspicuously dominant, what is shown seems anything but one-sided.



The selected motifs stay in the mind and provide many ideas and inspirations for your own building projects in the smaller representation of our nominal size.

Those who still want to wait for a calendar tailored to Z gauge, or to complement it, will not have to wait much longer: Around mid-November we will present the **Trainini Photo Calendar 2024** and make it available for download and self-printing.

**First car model delivered:**

After the first offering of an FS electric locomotive for Z gauge, the manufacturer GMmodelli Torino (GMmodelli[at]hotmail.com), which has been established in Italy for years, has now also delivered its first car model in 1:220 scale.

The car in question is the Alfa-Romeo 75, built between 1985 and 1993. The vehicle, which was the last design of the tradition-rich manufacturer from Milan before it was taken over by FIAT, now enjoys cult status. It was powered by engines ranging from 1.6 litres to a V6 with 3.0 litres of displacement on the rear axle.

The model has now been delivered in red paint (art. no. ME-0008/1), but more will follow in the future. Designed on the computer, the model parts are created in the 3D printer before they are elaborately coloured by hand.



The Alfa-Romeo 75 also has what it takes to become a classic as a model (item no. ME-0008/1), not only in Italy. A photo showing this vehicle also dismantled with a view of the recreated interior can be found in the news on our portal page.

The windows are transparent and allow a view of the interior equipment, which is also reproduced. The appealing model, which can also be used outside of Italy in many different ways, whets the appetite for many more new products, because passenger car models in particular currently form a large gap in the Z scale programme.

The manufacturer uses a dealer distribution network in Italy, and, in Germany, the 1zu220 Shop also took on the role of a distribution partner a few weeks ago.

**October at AZL:**

Two new versions of the EMD E8A mark the October deliveries at American Z Lines. Both belong to the Southern Pacific. As a "black widow" it appears with two road numbers (item numbers 62617-1 / -2) as well as with a "bloody nose" (62616-5 / -6), for which the road numbers have now been corrected.

The heavy half luggage car with three-axle bogies bears the colours and inscriptions of D&RGW (74025-1 / -2) in the new delivery. The three-part column wagons are also again among the current deliveries. The TTX vehicles are this time equipped with trailers of the forwarder Schneider (905233-1). Two trailers each are also available again for purchase individually (954009-1).



The term "black widow" is used to describe the paint scheme with which the EMD E8A of the Southern Pacific (item no. 62617-1; photo left) is delivered. The command car of a fire engine is replicated by the former BNSF goods train escort car (921022; photo right). Photos: AZL / Ztrack

The former goods train escort wagon with wide-vision cabin (921022) is on the road as a command wagon in a fire-fighting train of the BSNF, which also has a red paint scheme for this reason. A clear contrast to this is provided by the Tropicana refrigerator cars with new service numbers. They are available as double (914842-2) and quad pack (904812-2).

Last but not least, AZL has exchange bogies on offer. For passenger coaches of the "City of Los Angeles" these are 26 (72400-V) or 6 pieces (72401-V), for the shunting locomotive SW1500 there are single parts (AZLSW1500AAR-BLACK).

The manufacturer's webpages with photos of all deliveries can be reached at <https://www.americanzline.com>.

#### Traffic regulation in Z gauge:

After a successful return of the supplier JSS-Elektronik or its pages, we received a tip about a particularly popular product.

In fact, it serves a topic that is not exactly lavishly provided for on our scale: traffic lights.

A small version, i.e., a mast without an arm with a second unit hanging higher, is included in this manufacturer's range together with electronics for a proper traffic light circuit. After painting the mast and screen, it also provides the best protection for crossings on Z gauge layouts.



With traffic lights and an electronic module, regulating traffic is easy even in Z gauge. Photo: JSS-Elektronik

Both products are explained under the following webpage addresses:

<https://jss-elektronik.de/produkt/verkehrsampel-spur-z/>

<https://jss-elektronik.de/produkt/10-fach-led-decoderampelsteuerung/>



**New products from Herpa for the Christmas business:**

As usual, we list here the new aircraft from the Herpa range for the scale 1:200. We focus on models that can be found at European airports, do not exceed a total length of 30 cm and cannot be attributed to a warring country.

These are for the “Wings” product range:

ITA Airways Airbus A319 “Pietro Mennea” (Art.-Nr. 572798),  
Maersk Air Fokker 50 (572804),  
Embraer E195-E2 - Corporate livery (572842), and  
Turkish Airlines Vickers Viscount 700 (572866).

In the Snapfit series with simplified plug-in models in flight representation will appear:

Icelandair Boeing 757-200 “Hekla Aurora” (610735-001) as a reissue, and  
Eurowings Airbus A320neo (613910).

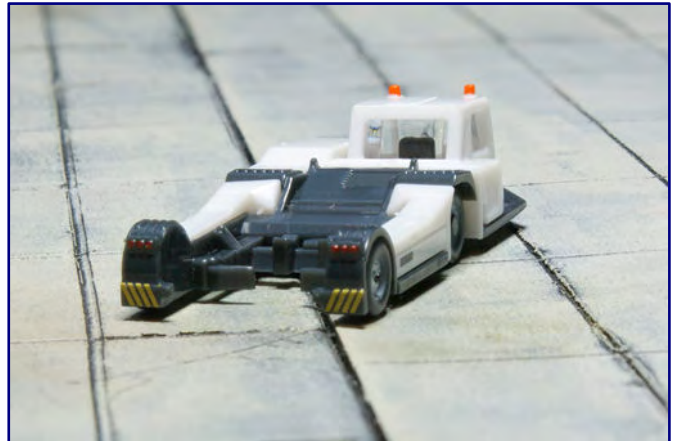
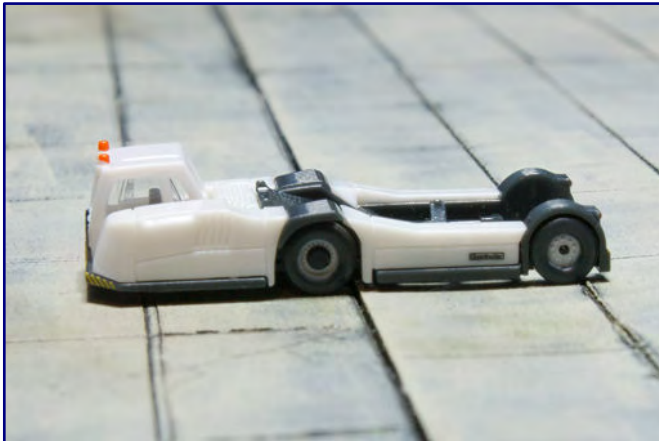
All the new products listed are planned for delivery in November and December 2023.

The delivery of the Goldhofer AST-2 aircraft tractor (572088), which already took place in February / March, completely passed us by. The white vehicle has been realised quite elaborately: It has a glass insert in the driver's cab, colour-contrasting rims and hazard warning lights as well as printed warning stripes.



Here, the new Goldhofer AST-2 aircraft tractor (art. no. 572088) from Herpa is successfully in use on an airport diorama.

The headlights are also individually and sharply separated, as are the tail lights at the other end. The imprints also include the manufacturer's brand Goldhofer. With this scope of design and good engravings, the new product fits perfectly into the Wings series 1:200.



Side and oblique views from the rear reveal a few more details that make Herpa's new product in 1:220 scale a successful and eye-catching model.

There it is also urgently needed by the airport scene designers, because an aircraft tractor can be found on every apron and it is impossible to imagine the spectators' gaze without it. And this vehicle is probably a typical representative of a so-called "push-back vehicle".

So, it was a real gap in Herpa's programme. The "Tow Bear" vehicle that used to be in the programme, which pulled instead of pushed on the nose landing gear, is hardly to be found at international airports today. So, it seemed about time that the accessories manufacturer followed this step in the model as well.

#### Glass train in a shortened form:

Rokuhan has now realised the DB class 491 ("Glass Train") in two versions as a shorty. The choice fell on the Olympic blue design of the seventies (art. no. ST015-1) and the subsequent paint scheme with gentian blue as the basic colour (ST015-2), with which the prototype finally crashed in December 1995.



The Olympic blue version of the class 491 (art. no. ST015-1) shows us the finely and very prototypically designed scissor pantograph DBS54.

The extreme shortening of the shortys, which are so popular with Japanese model railroaders because they take up as little space as possible, suits this model much better than many other vehicles whose prototypes are also significantly longer.

The effort that the manufacturer has put into this type of entry-level model is striking: The housings were moulded, painted and printed in crystal-clear plastic. They allow a free view, only interrupted by the chassis, which can be exchanged for a motorised version. Thus, they come quite close to the prototype impression at this point.



But, what we find most impressive are the movable pantographs, which have been completely redesigned. According to the prototype, both models have a single-arm pantograph and a scissor-type pantograph. The single-arm pantograph SBS 65 has been shortened compared to the model of the 181.2 series, i.e., it has not been taken over from it.



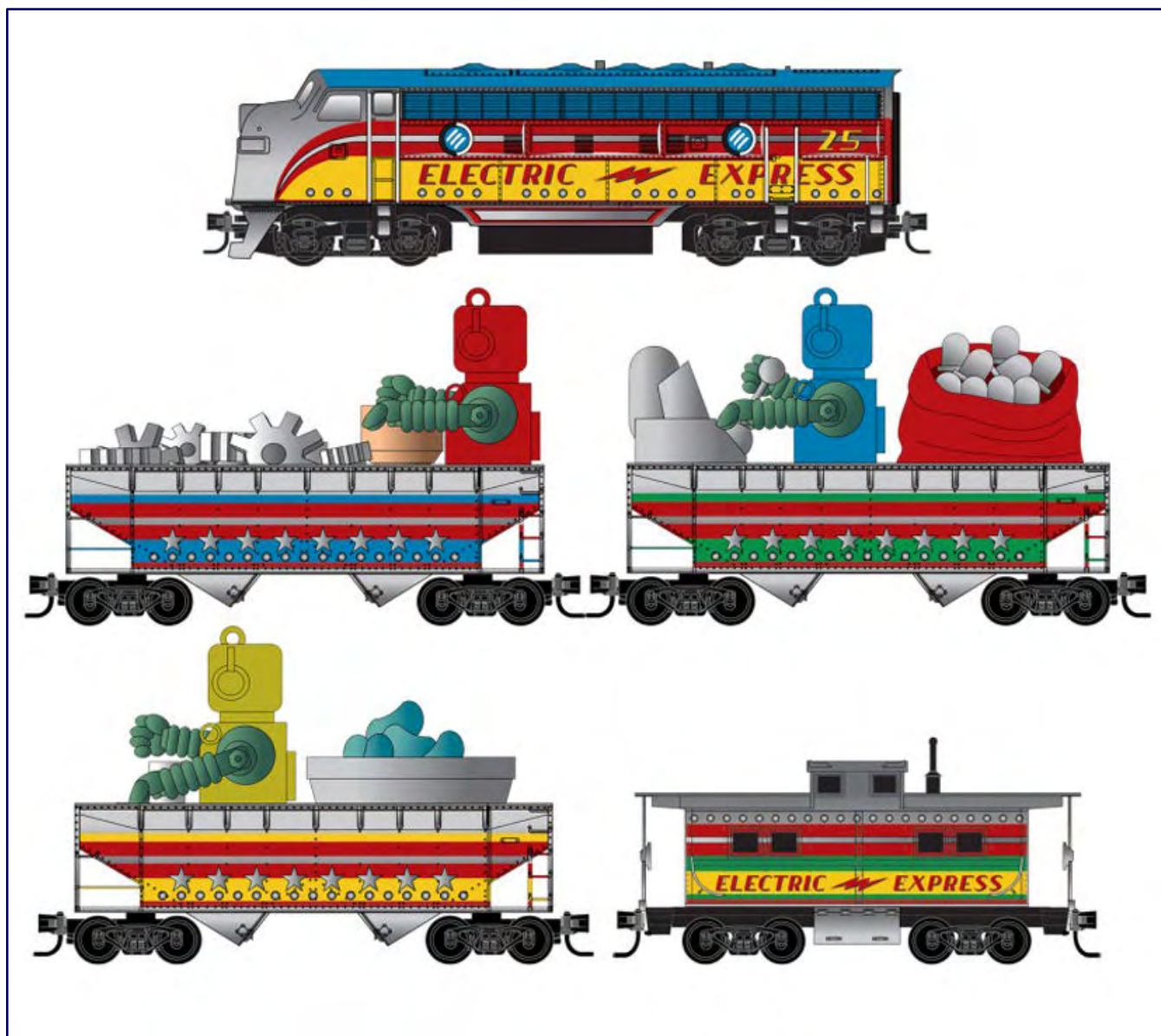
From this perspective, the gentian blue version of the “glass train” (ST015-2) clearly reveals the free view through the interior that was so characteristic of the prototype.

The DBS 54 pantograph, which is also new in shape, is significantly smaller than those from Märklin and also shows the typical diagonal struts in the upper pantograph, which Märklin's conversions have lacked for 49 years. The collector shoe, which stands out somewhat dominantly here, is about the same size as the one on Märklin's counterpart. This illustrates the filigree of the basic construction.

This suggests that Rokuhan is the first Z gauge manufacturer to create a scale version of one of several designs of the DBS 54 scissor pantograph! With a finer collector shoe and a carrier that fits on the roofs of Märklin models, it could become the perfect exchange part to bring their roof area closer to the large prototype.

#### **Christmas is not too far away:**

In view of Advent and the Christmas season, the “Robot Christmas Train Set” (art. no. 994 21 100) will soon be delivered by Micro-Trains (<https://www.micro-trains.com>). The train set consists of an EMD F7A diesel locomotive, three bulk goods wagons, and a goods train escort wagon all in a colourful Christmas design. The bulk goods wagons carry robotic decorative inserts that give the festivities a very modern character.



The “Robot Christmas Train Set” (art. no. 994 21 100) from Micro-Trains has an unusual look, which obviously suits the American taste for colourful Christmas decorations very well. Illustration: Micro-Trains

In parallel, the “War of the Worlds” literature model series is continued with wagon number 8 (518 00 845). It is again a boxcar with the basic colour of this series and book motifs from the American literary classic.



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Publisher and Responsible in terms of German press law (V.i.S.d.P.) is Holger Späing, Am Rondell 119, 44319 Dortmund; Contact: 49 (0)231 9598 7867 or by e-mail to [redaktion\[at\]trainini.de](mailto:redaktion[at]trainini.de).

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*International Edition*

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for railroad enthusiasts  
in the scale 1:220  
and Prototype

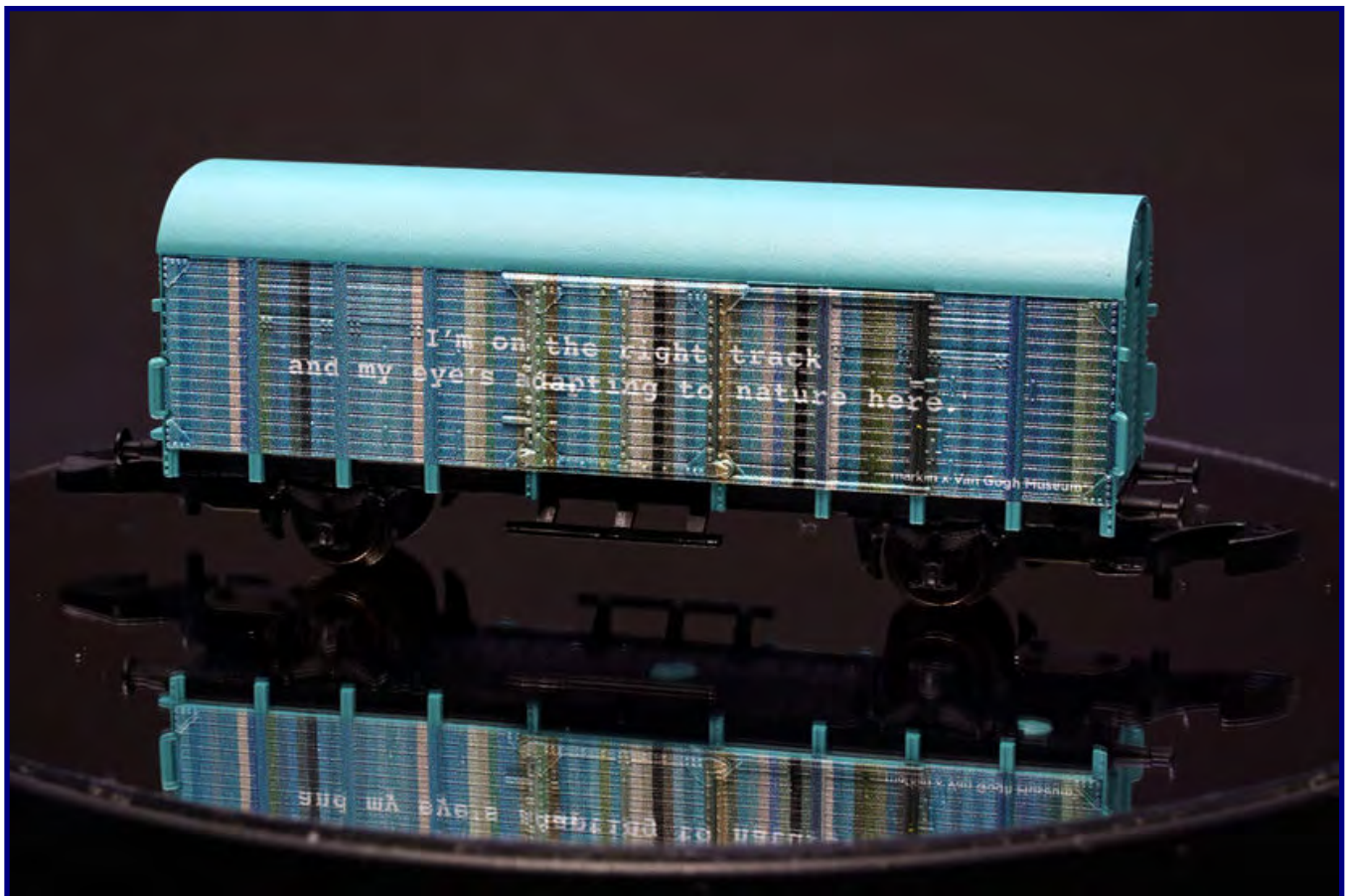
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# Trainini

German Magazine for Z Gauge



Model Railways and Art

Trailblazers on Rails  
Inspiring People



## Introduction

Dear Readers,

I admit that collector's topics are rarely to be found in our magazine. We have clearly dedicated ourselves to model railway construction close to the prototype and to those models that have a prototype and try to come as close to it as possible.

But in the Rhineland, where I work, people keep reminding me how different we all are: "Every joker is different!" Tolerance also means respecting those for whom collecting is the greatest thing or who seek their salvation in completely different forms of expression.

And so, our editorial team had been wondering for a while how we could put together an article for those readers who are just outside of what drives most of our readers.

Märklin made it easy for us, because their still young "Message Wagon" series is aimed at loyal Märklin collectors, but also tries to send a message by combining art with the model railway. Looking more closely at creation is more topical than ever these days.

And Vincent van Gogh is a famous artist who felt a close connection to nature and drew his strength from experiencing it. Let's not forget about it, let's put it at the centre of our activities!

In this edition, we would like to focus on the artist, his reproduced work, and an excerpt from his life. We hope you enjoy this unusual approach! If it is also an inspiration for reflection, then he has achieved a great deal.

But we have much more to offer. Then there is the 0 Series Shinkansen, which we are looking at in the prototype and whose Rokuhan model we are testing today. Not just any train was chosen: it is the "Hikari No. 1," which set off from Tokyo at 6:00 a.m. on 1 October 1964 to inaugurate the era of high-speed transport.

This particular train had some special features that can also be found on the model. Rokuhan has placed great emphasis on authenticity, which we will explore in the test report. A "little brother" in the form of a 500 series Shorty could also be found at the Model Railway Days in Erkrath-Hochdahl.

**Trainini®** and **Trainini TV** were exhibitors there. We are reporting on this rather modest exhibition today because the Model Railway Day is coming up soon and our idea is hopefully worthy of imitation.

We didn't exhibit for seasoned professionals at the highest level but focussed specifically on children and consequentially offered a large tinkering booth. There is even a new episode on **Trainini TV**. I hope you enjoy reading and watching!

Sin-Z-erely,

Holger Späing



**Holger Späing**  
Editor-in-chief

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We thank the Van Gogh Museum for permission to reproduce the artwork.

Date of publication of the German language version of this issue: 27 November 2023

### Cover photo:

Model railways and art – do they go together? This is probably a matter of great debate, but many people have already appropriated the model railway artistically. Märklin now wants to send messages about them. We think this is an exciting and new idea!



Märklin enters the second round

## A Message on a Railway Car

***Art and model railway enter a liaison, the result is a message. What sounds like a simple formula is the idea behind the still young "Message Wagon" model series. The messages sent are international, and are therefore written in English. For the second time, it now also includes 1:220 scale and honours the world's most famous painter.***

Vincent van Gogh is indisputably not only one of the most prestigious and best-known artists since the 19th century, but also one of the best. No other person's life and work seem to be as closely linked as in this painter's case.

Countless biographies, books and films deal with the stages of his life and their influence on his artistic legacy. He was almost always tempted to take his easel out into the open air, where he reproduced his impressions on canvas.



To highlight the effect of the new model in the "Message Wagons" series (item no. 82932) in the best possible way, we have implemented it in a black and white backdrop. For the second time, the inspiration for the motif is Vincent van Gogh.

Which other artists fascinated him and influenced his development in their own way can be recognised from his admiration, his works of art and meetings during his active period. Like other contemporaries, he increasingly moved away from Impressionism towards the later Expressionism. This transitional period is often categorised as post-Impressionism.

However, anyone who has ever seen a real van Gogh will be equally impressed by his painting technique, which gives the picture a completely unique and typical structure. Vincent van Gogh is still fascinating today, but he was only discovered late. He only achieved fame after his early death.

Today, it is hard to imagine that only a few of his works were sold during his lifetime. Today, they realise top prices and people seem to be aware of his artistic legacy.



This legacy also includes the active painter's connection to nature, which had been close to his heart since childhood. Again and again, he was drawn out with his easel and created portraits of what he saw in the places where he stayed – in the most important phase of his work in France.

1889 was a fateful year for him. His illness, characterised by seizures, prevented him from working during their occurrences. At the age of 36, he voluntarily went to the remote sanatorium and nursing home for the mentally ill in Saint-Rémy-de-Provence.

He was unable to leave his room for long periods. Dark corridors and barred windows are unlikely to have had a positive effect on his mind. At least, he was often allowed to leave the asylum with a guard and paint outdoors in the neighbourhood. He saw this as beneficial and conducive to his health.

His time at the clinic changed his paintings in many ways: the colours he chose, the meaning of forms and the selection of motif elements. The artist and his brushwork also changed in recognisable ways.

What always remained, however, was the desire to work outdoors with a special affinity to nature. "I am on the right path and my eye is adapting to nature here", Vincent wrote at this time to his brother and patron Theo van Gogh, who was living in the art centre of Paris at the time.

Märklin printed this as an English-language message on the second "Message Wagon:" "I'm on the right track and my eye's adapting to nature here." This not only fits perfectly with the motif chosen for the second wagon in the Z gauge series, but the picture selected is also a very special one among the approximately 900 paintings that the artist left behind.



Vincent van Gogh painted “Almond Blossom” in February 1890 during his stay in Saint-Rémy. The occasion was the birth of his nephew and namesake Vincent Willem. It was intended as a gift for his brother Theo and his sister-in-law Johanna Bonger, who was the person that would later make the painter widely known.

At the end of April 1890, the artist sent the painting from Saint-Rémy-de-Provence to his brother Theo van Gogh in Paris. It was meant as a present to his nephew Vincent Willem van Gogh who later founded the Van Gogh Museum.

So, it is not at all surprising that it was this work of art that was closest to the Van Gogh family's heart. According to the museum's information, the original format of this artwork is 73.3 x 92.4 cm.

The Van Gogh Museum says the following about the motif chosen for the Märklin wagon: “Large flowering branches like this one against a blue sky were one of Van Gogh's favourite motifs.”



The artistic model for the second “Message Wagon” in Z gauge is “Almond Blossom” from April 1890. Illustration: Van Gogh Museum, Amsterdam (Vincent van Gogh Foundation)

In fact, this is probably the brightest and clearest blue sky of all Van Gogh's masterpieces, and its intensity has an immense effect on the viewer.

It also forms a striking contrast to the white almond blossoms, which appear early in spring and ahead of the leaves. They are a symbol of new life. The special distribution of light in this painting also emphasises the painter's intention.

Some may consider the smooth areas of colour surrounded by contours in the blossoming almond tree branches to be “atypical” for Vincent van Gogh. In fact, however, like the position of the tree in the pictorial plane, they follow a Japanese style that was considered both modern and popular and was also frequently adopted by Vincent van Gogh.

## Art on a railway carriage

After the “Cornfield with Crows”, Märklin has taken up a message and a painting by Vincent van Gogh for the second time. After several larger gauge predecessors, they also form the start of the “Message Wagon” series for Z gauge.

The wagon presented now (item no. 82932) could hardly have been more different from the first edition: Colours, techniques, motifs, and intentions are far apart, but also demonstrating the artist's versatility.

His love of and respect for nature are unique and indeed embody a message that is more urgent today than ever. If we want to preserve all of this in its beauty, great efforts and perseverance will be required. Nature is life and we should cherish and preserve both.

Vincent van Gogh recognised this and conveyed it to his viewers in his paintings. It is up to us to recognise this, to understand it and to transform it into responsible behaviour. Märklin had the idea of transporting a message that is more than 130 years old into the present.

Stylistically, this seems to have succeeded: The main colour of the painting determines the image of the railway carriage. Matching colour hatching on the side walls also embodies modernity. The motif of the artwork has not been reproduced with photographic accuracy, but in its outline as if drawn with a white pencil.



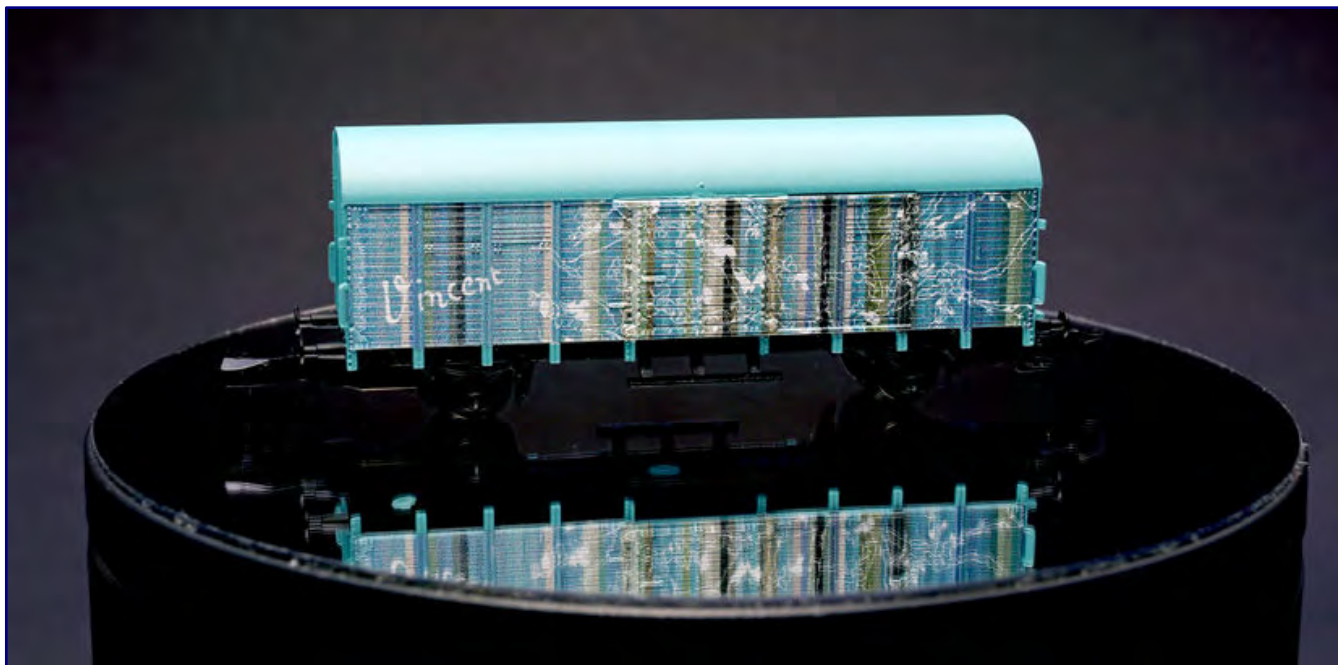
The dominant contrasts thus retained their effect without overloading the side walls of the model in terms of motif or colour. The message from a Van Gogh letter, on the other hand, is not coincidentally addressed in English, the international business language.

The approach of combining model railways and art in a special way is new and also unique. This also requires a certain framework to emphasise meaning and value: The packaging is elaborate and must deliberately stand out from an ordinary catalogue product, but also from that of other special railway carriages.

This is also illustrated by the photographs that accompany this article. Märklin is addressing a further target group, because in addition to model railway fans, art lovers are certainly also the addressees here. In the same way, it is about the messages sent, which can also appeal to people outside the two target groups already mentioned.



Certainly, it may take a while for the message to reach and penetrate its recipients. But in the meantime, the idea has found favour, at least with us, and has met with acceptance. After all, it is just as difficult to comprehend on the scale of model railways as it is on the scale of collecting. It touches on both but opens new doors and calls for the aforementioned aspects to be included in addition.



The blossoming almond tree branches on Märklin's new art car look like a drawing in white ink and leave plenty of space for the colours, which the artist had once chosen with care, to take effect. The message that is intended to be sent is on the opposite side and can be read on the cover.

And so we hope for curiosity, interest and a good response. We wish the idea success and look forward to seeing what messages will be formulated and prepared to continue this extraordinary series of railway cars.

**Model manufacturer:**  
<https://www.maerklin.de>

**Van-Gogh-Museum Amsterdam:**  
<https://www.vangoghmuseum.nl>

**Trainini TV – Episode 20:**  
<https://www.youtube.com/TraininiTV>

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## Rokuhan's magnificent new model **Premier of the Shinkansen**

*The Shinkansen trains have enthusiastic fans all over the world, since they were the first high-speed trains in scheduled service and therefore quickly became famous. JNR broke new ground with them in 1964 and gave them an unmistakable design. Rokuhan has successfully created a Z scale model of the first version from the time of the Olympic Games in Tokyo.*

It does not happen often that a 1.40 metre long multiple unit train rushes past you in Z gauge. What is even more surprising, however, is that, in this case, both the prototype and the model come from Japan.

Large series manufacturer Rokuhan has recently taken on the 0 Series Shinkansen (item no. T020-1) of the Japanese National Railways as a complete, twelve-part unit. Given the general lack of space in the densely populated island country, there will probably only be a few model railway layouts large enough to run the entire train.



With the 0 Series Shinkansen in the "Hikari No. 1" version (item no. T020-1), Rokuhan is now also opening up high-speed transport on rails at a scale of 1:220. Photo: Christoph Maier

Nevertheless, this creamy white and blue painted train will find its buyers there, because this is not just any old Shinkansen unit.

The model presented here was based on the very first train, "Hikari No. 1", which left Tokyo for Shin-Osaka at 6:00 a.m. on 1 October 1964 and marked the beginning of high-speed train services worldwide with an initial top speed of 200 km/h.

Markings on the side of the model, which were later removed from the prototype, also bear witness to the special journey. As just mentioned, the inaugural train was of the "Hikari" type (Japanese for "light"), which Rokuhan also included in the product designation.

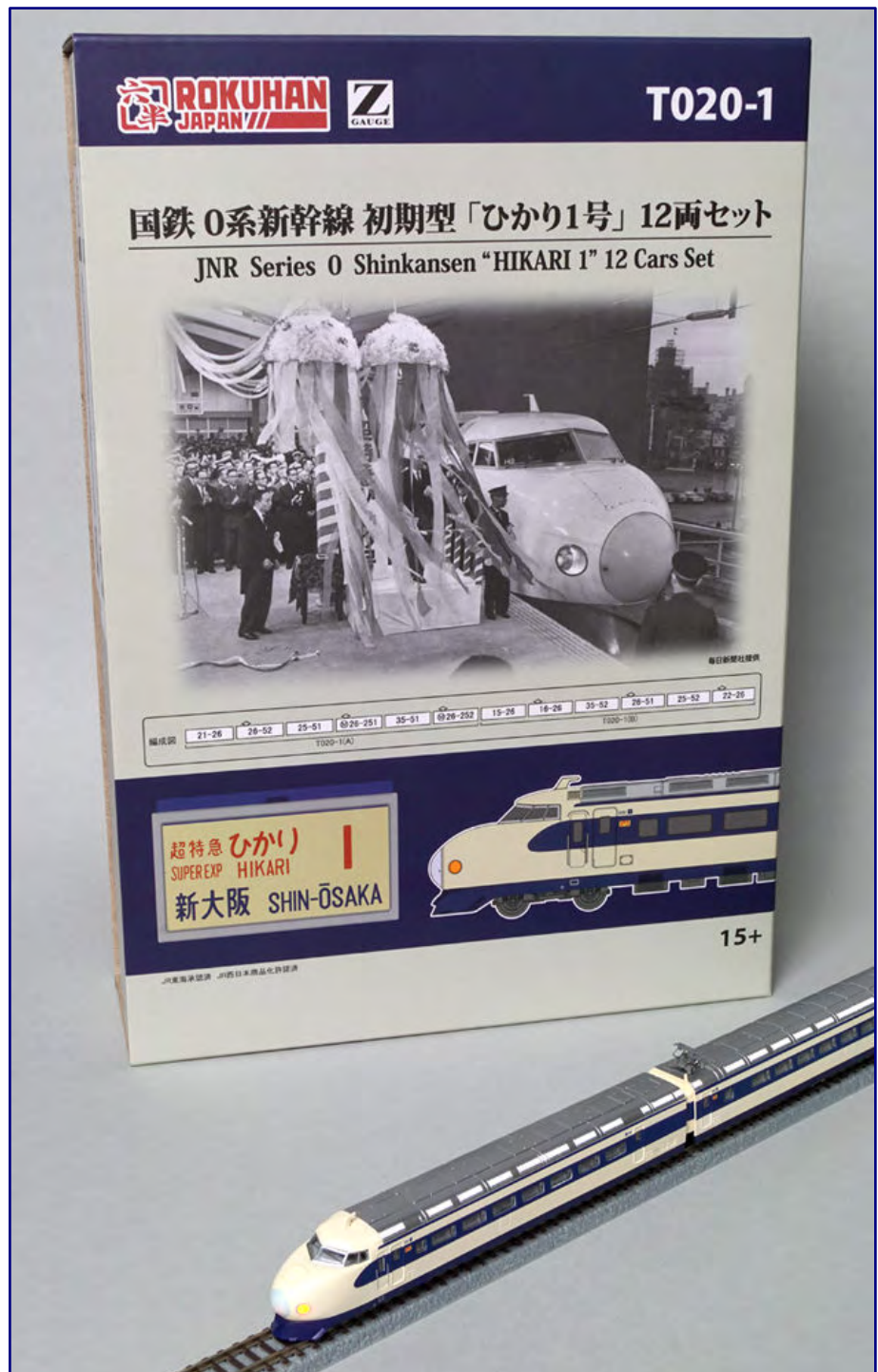
In contrast to the Kodama ("echo") trains, which served every station on the new line, the "Hikari" only made two stops en route.

Most of the 360 wagons ordered for 30 twelve-car trains were already ready at the start of operations, but Rokuhan made a point of researching and reproducing the running numbers from the very first official journey.

The journey time for the 515 km route was later reduced to 3 hours and 10 minutes, and the trains travelled up to 400,000 km a year. These are impressive figures and explain why Rokuhan wanted to recreate the ancestor of all Shinkansen trains.

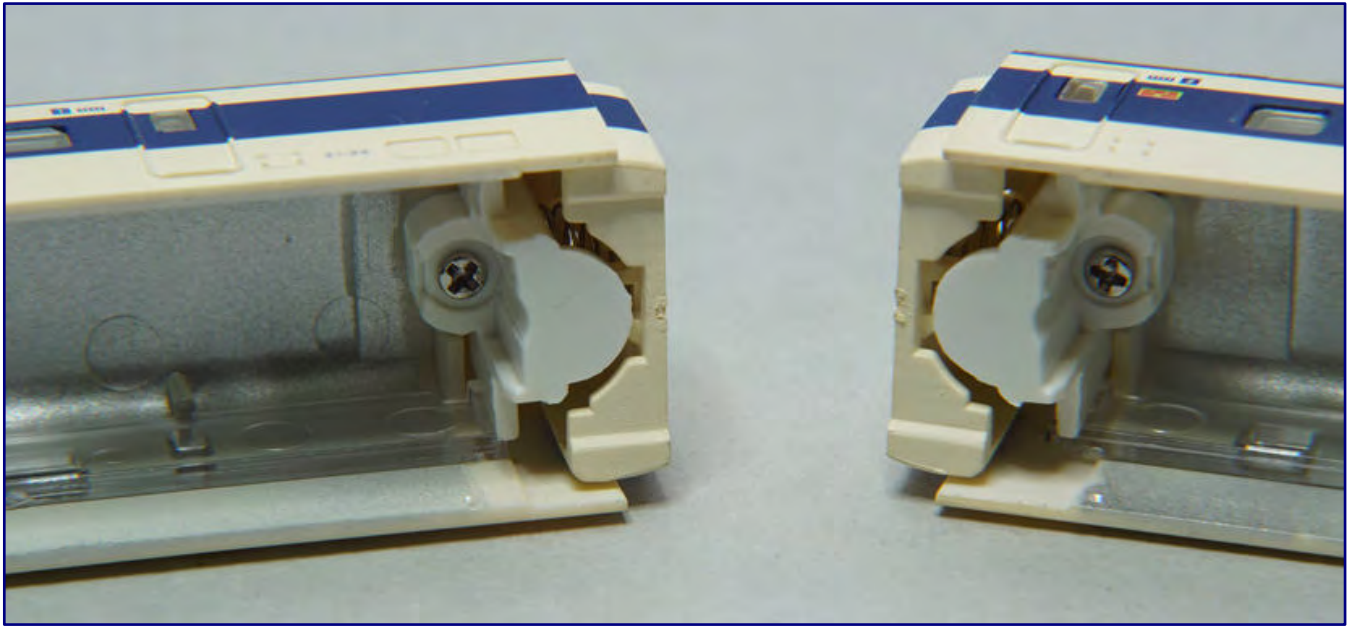
And we believe that it has been a successful project indeed, as the models make a good and high-quality impression, with the colours also matching those seen in contemporary photographs. A special feature of this Rokuhan's Shinkansen model are the car transitions, which ensure a cohesive train appearance.

As with some previous models, the end pieces of the transitions also move around a vertical axis and are spring-mounted, which is why no disturbing gaps are visible between carriages, even in curves. The mechanical connection has also been realised in the usual simple way with eyelets and hooks, but they couple firmly and securely.



The twelve-part train is packaged in two book cassettes, which are enclosed in a cardboard slipcase. This is printed with a historical motif of the inaugural journey and also provides information on the formation of that train.





Once again, the manufacturer has attached great importance to a closed train appearance. To achieve this, the side panelling protruding beyond the ends of each carriage have been upgraded to movable transitions supported by springs.

They also protect the wagons from unintentional twisting and ensure that they are always correctly positioned in the train formation in line with the appearance of the prototype. There is also no electrical connection between the individual wagons, as each individual wagon draws its own power from all the axles with the help of the centre bearings.

Rokuhan managed to engineer this in a way that the train runs exceptionally smooth, with each individual carriages exhibiting hardly any rolling resistance when pushed along the track. These are the best prerequisites for optionally equipping the train with Rokuhan's proprietary interior lights type A (A009; 2 x required) and type C (A031; 10 x required).



Under the shell we find interior fittings that reproduce the original 3 + 2 seating of the seating carriages. However, these do not provide legroom for figures! The pins shown in the photo are used for mounting the optional interior lighting.

After removing the shells, they are quick and easy to fit: simply place them on two guide pins, screw them tight on one side and hook in two tension springs to transfer the current from the contact plates protruding into the interior of the carriage to the circuit board – done.

We will also do this once all the measurements and test drives have been completed. This allows the replica interior to be shown off to its best advantage in the truest sense of the word. This is also standard at Rokuhan: all twelve coaches have a light grey replica interior, albeit with some unavoidable compromises in terms of authenticity.



Two short (A009) and ten long lighting sets (A031) are required, if the complete train is to be fitted with interior lighting. As all carriages are prepared with wheel current collectors and current guide plates in the interior, this can be done quickly.

This is, in particular, true for the motorized coaches where space is more limited than in the unmotorised carriage. The remaining cars have a complete reproduction of the rows of seats: 2 + 3 seating in 2nd class, 2 + 2 seating in 1st class and a different interior with a bar in the dining car and driver's cabs in the end cars.

Putting figures in the carriages is certainly a good way to add some extra detail and life to the model. Given that Shinkansen trains are generally well occupied, one would need to add quite a few passengers. However, simple paint schemes on the figures should suffice – or rather on the upper parts of the body, as there is no space for the legs in the interior, something which is not visible through the windows.



The two restaurant carriages are equipped with authentic looking interiors. Here, for example, the bar area.



## Illuminated noses

Talking about side windows: they make for another unique feature of the early series of this train and also define one of the differences between this model and the earlier 0 Series models of the F-Toys brand, for which matching bogies with drive were available from Eisen Platz.

The first 0 series trains had large side windows, which are replicated on the Rokuhan model. In the mid-seventies, JNR switched to smaller windows in order to reduce replacement costs. As there was no noticeable success and the smaller windows met with a poor response from customers, the national railway finally returned to the triple-glazed, larger windows.



This comparison between the Rokuhan model (rear) and two versions from the F-Toys brand (centre and front) clearly shows the different sizes of the side windows. The livery variant in the front is from the later years of operation could also be a future option for Rokuhan.

Another special feature of the chosen prototype was an important aim for Rokuhan when developing the model, as the Japanese manufacturer's website explicitly points out: The parabolic nose of the train was not only reminiscent of a rifle bullet, hence the name "bullet train", but was also made of acrylic and partially translucent.

During the journey, it was dimly lit on the first and second train in the first days of operation. This was also implemented in the model and is paired to the two-light head signal and on the other side to the end signal. The lights work depending on the direction of travel and are equipped with yellow and red LEDs.

By the way: The 0 Series Shinkansen did not have regular couplers, which is why no double units could run. There was only an emergency coupling under the "front bowl" to be able to tow a broken-down train. The coupling cover without handles chosen by the manufacturer corresponds to the early days of operation. A faint light shines through it.

# Trainini® *International Edition*

German Magazine for Z Gauge

As already mentioned, the original livery in two silky matt shades has been applied neatly and with crisp separations on the model. Only the parabolic nose is slightly shiny to imitate the prototype material and its properties. The coach roofs are painted white aluminium. The driver's cab windows have chrome-coloured embossed frames, and the headlights are also framed in this colour. None of the windows on the sides have frames visible on the outside.



A special feature of the first two trains was the dimly backlit nose, which was therefore not painted in the colour of the car. Rokuhan also reproduced this exactly in line with the prototype), as can best be seen in the photo with the taillight switched on (photo above). Another special feature is more apparent in direct comparison with the F-Toys counterparts (centre and rear on the photo below): the Rokuhan model of the original version does not yet have the side handles that the two rear models have on their noses.



**Dimensions and data for the JNR O Series Shinkansen:**

|                               | <u>Prototype</u>   | <u>1:220</u> | <u>Model</u> | <u>Deviation</u> |
|-------------------------------|--|--------------|--------------|------------------|
| Length (end carriage)         | 24.900 mm  | 113,2 mm     | 113,1 mm     | - 0,1 %          |
| Length (centre carriage)      | 24.500 mm  | 111,4 mm     | 111,3 mm     | - 0,1 %          |
| Height above rail head (roof) | 3.975 mm   | 18,1 mm      | 17,8 mm      | - 1,7 %          |
| Width (car body)              | 3.380 mm   | 15,4 mm      | 15,4 mm*     | 0 %              |
| Total centre distance         | n. a.  | ---          | 91,4 mm      | ---              |
| Pivot pin distance            | n. a.  | ---          | 78,9 mm      | ---              |
| Bogie axle stand              | 2.500 mm   | 11,4 mm      | 11,1 mm      | - 2,6 %          |
| Wheel diameter                | 910 mm   | 4,1 mm       | 3,9 mm       | - 4,9 %          |
| Service weight                | 967 t**  | ---          | 210,6 g      |                  |
| Axle formula                  | Bo'Bo'+ Bo'Bo'+ Bo'Bo'+ Bo'Bo'+ Bo'Bo'+ Bo'Bo'+ Bo'Bo'+ Bo'Bo'+ Bo'Bo'+ Bo'Bo'+ Bo'Bo'+ Bo'Bo' |              |              |                  |
| Power system                  | 25 kV ~ / 16 2/3 Hz  |              |              |                  |
| Power (twelve-part)           | 12.075 PS / 8.800 kW   |              |              |                  |
| Permitted maximum speed       | 220 km/h   |              |              |                  |
| Manufacturers                 | Nippon Sharyo, Kawasaki Sharyo, Kinki Sharyo, Kisha & Hitachi                                  |              |              |                  |
| Years of construction         | 1963 - 1988  |              |              |                  |
| Units produced                | 3,216 Wagen / 201 locomotives  |              |              |                  |
| Retirement                    | 14. December 2008  |              |              |                  |

\* max. width of the Model 15,9 mm over the steps

\*\* Specifications for a 16-piece set

The operating inscriptions are sparse in comparison to German railway vehicles, but they are complete on the model, as far as we can see, and flawlessly printed. They include the wagon numbers printed in silver colour and the 1st class numbers on two wagons.

As they are historically documented and binding for this model, we reproduce them in their correct order: 21-26, 26-52, 25-251, 26-25, 35-51, 26-252, 15-26, 16-26, 35-52, 26-51, 25-52 and 22-26.

The only thing not clearly readable under the magnifying glass are the printed replicas of the metal signs for the maiden journey of the train.

The original design of the train has already been mentioned several times. This not only serves to formally differentiate it from the earlier competitor model, but actually brings with it some different features.



**A close-up of all the different operating markings: Except for the printing on the sign next to the right-hand entrance door, they are all flawlessly executed. The delicate but movable pantographs are also a sight worth seeing.**

One of these is the six scissor pantographs distributed across the train. They have been very finely executed, including the contact strips, and are also an important feature of the twelve-car train. In line with the prototype, they can be found on every second car. In operation, therefore, all six units were always connected to the contact wire.

Because this was a cause of high wear on the overhead line and also generated avoidable noise, the trains later received a continuous roof-mounted power line to supply all units, and the number of pantographs could thus be reduced. However, this was not yet the case in 1964.



**Another detail which highlights Rokuhan's skills: On F-Toys (centre and rear models), the end walls were designed and the insulators at the end of the car were finer, but due to the lack of a closed train design and rigid, coarse scissor pantograph, the overall appearance does not come close to the new product. The wind deflectors were a later addition and conceal the poorer impression to some extent.**

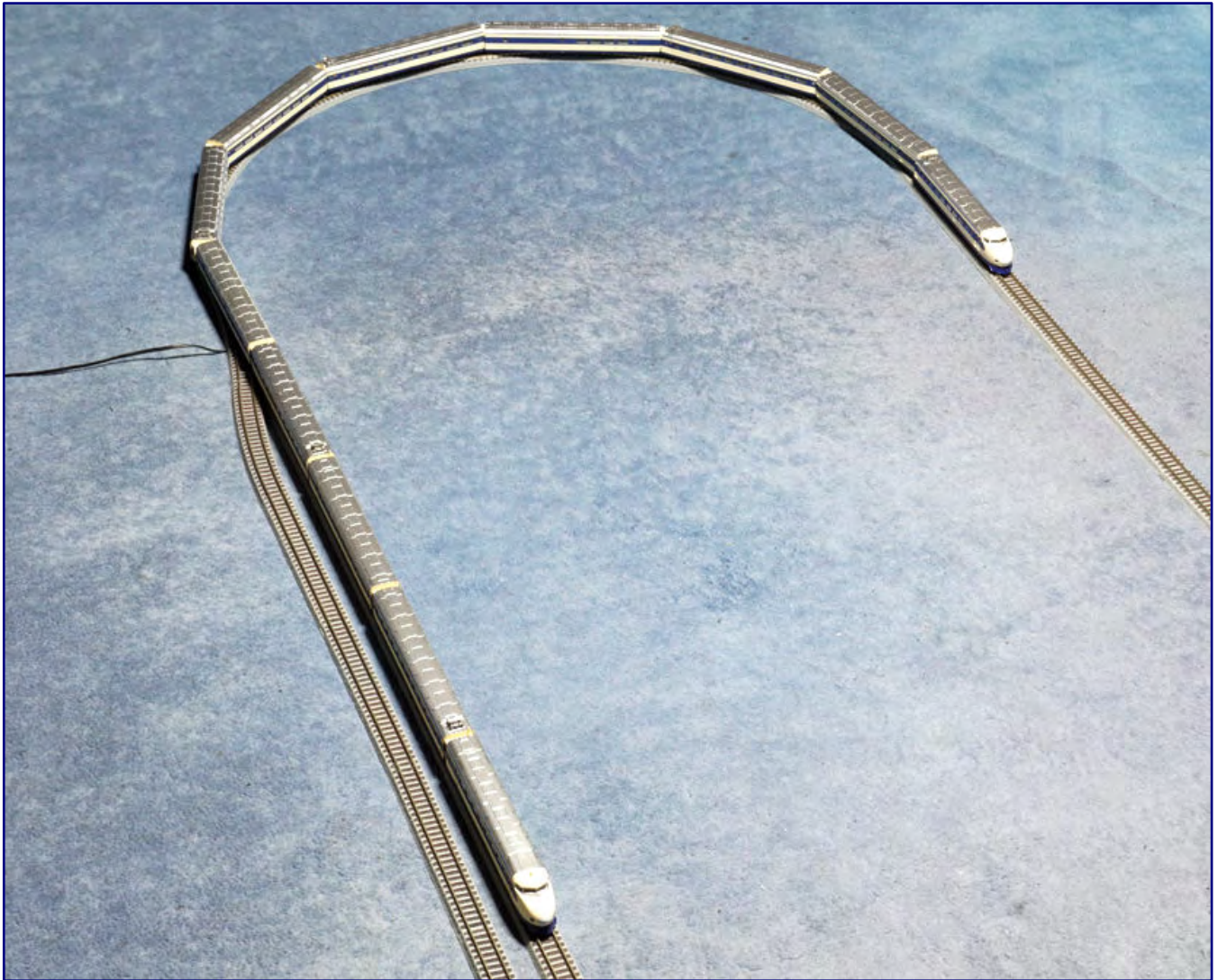
Now we come to the final points of the visual assessment. The appearance is harmonious and also reflects the proportions of the train in every respect. This is also confirmed by the dimensions taken from the model. They are almost all spot on, deviations are limited to the expected measurement tolerances.

In percentage terms, only the bogie centre distance and the traction wheel diameter catch the eye. But if we look to the left in the table, it also becomes clear that we are talking about fractions of a millimetre in this case too.

The interim conclusion is that Rokuhan has presented us with a very attractive and interesting model in terms of railway history. This is also emphasised by the special packaging, which differs from later editions. The two book boxes, in which this train is packaged, are enclosed in a slipcase, which refers to the special event with a historical photo and reproduction of the car numbers in the correct sequence.

The train comes with the following printed accessories (stickers): two reproductions of the "Hikari No. 1" train running signs, a print of the historical ticket and four different stickers for the rotary knob of Rokuhan speed controllers with motifs of the 0 Series Shinkansen.





We could only think of one way to visualise the total length of the twelve-part unit: A carpet railway with an oval track on which the complete unit can be photographed travelling around curves. Never before has such a long Z gauge train been sold under a single item number!

## Technology and operating characteristics

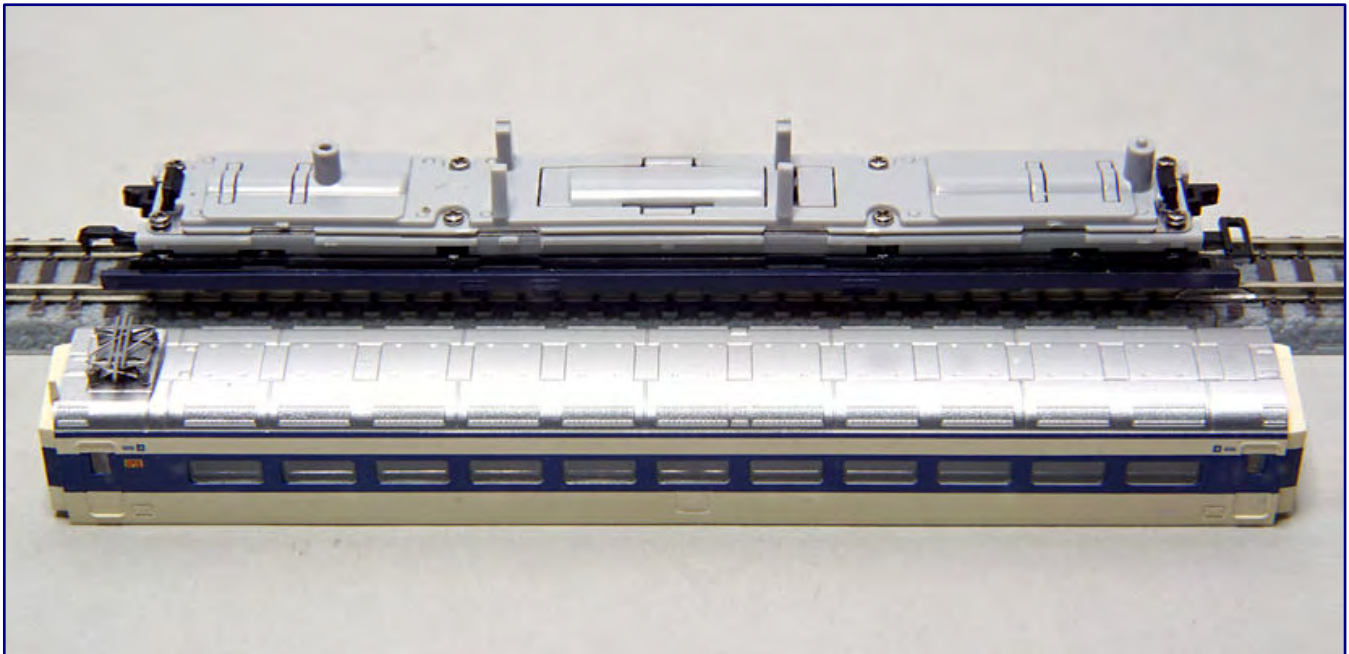
The first consideration in the technical section of our test is the weight of the multiple unit: A total weight of 210.6 grams exceeds all previous test candidates, which is not surprising. After all, we have never before presented a newly designed multiple unit here that is offered in twelve parts in one set – expansion sets will not be necessary.

With weights of mostly just under 15 g each, the individual cars are within the expected range of a long passenger car. The two motorised coaches operating in parallel in the 4th and 6th positions in the prototypical train formation of the inaugural run contribute to the total weight.

They bear the road numbers 26-251 and 26-252 and weigh 31.2 and 31.0 g. This also puts them in the average weight range for Z gauge locomotives. However, their tractive power is significantly higher because, as is usual with Rokuhan, they are equipped with traction tyres, which at the same time also make electrical contact.

Although there is no noticeable rolling resistance, all wheels are included in the power consumption on both sides. There are no electrical connections between the carriages. This means that each of the twelve carriages draws its own power for the head, tail, and/or interior lighting as well as the motors.

Once again, this works amazingly safely, but also has its pitfalls in secured signalling areas: As the two motor coaches are close to the centre of the train, they are regularly only detected by powered-off sections in front of signals when the head of the train has long since entered the secured area.



**Two motorised coaches move the long train: they can be recognised by their significantly higher weight, two diagonally offset traction tyres and the lack of interior fittings: a modern bell-type armature motor operates under the central cover.**

At the same time, only individual train sections will stop at the signal with the interior lighting switched off. We can see that the requirements of Japanese model railway enthusiasts are completely different from the habits of European customers.

It is necessary to adapt to this, but hardly any Shinkansen will run together with DB models on the same layout. It should therefore be feasible to configure the layout accordingly. Customers will also be grateful because the power supply for the interior lighting is considerably more reliable than with Märklin's previous solutions with power lines across carriage crossings.

Let's just stick to the lighting. The retrofittable interior lighting, which we only installed after the electrical measurements, works with warm white LEDs. We find this very pleasant and prototypically accurate.

However, it remains a mystery why the manufacturer continues to use yellow headlights for the two-light headlights. With the exception of the class 181<sup>2</sup> series, this has always been the case up to now, but we hope that this will be improved in the future. The two rear lights at the other end are red, as usual.

The manufacturer specifies a minimum radius of 195 mm; our test runs were carried out on non-elevated 220 mm curves without super elevation with the Märklin 67011 transformer, in order to ensure comparability with our other tests.



Voltage and current readings were recorded with a suitable electronic measuring device; the speed measurements were carried out using the MTS-100 speed sensor from Halling Modelle (see **Trainini®** 3/2022).



A circuit board is installed in the nose area of end carriages. It is responsible for the yellow-red light change in the signalling lanterns and also for the light in the hemispherical cover, which once gave rise to the term "bullet train."

Let's now take a look at the driving characteristics that are decisive for many customers: At 0.4 volts DC on the track, the train slowly starts to move at the equivalent of 9.4 km/h. At this speed, it safely passes all points.

If we turn the transformer up further, it reaches its prototypical maximum speed of 220 km/h at 5.2 V track voltage. At 10 V this is 462.4 km/h, at 12 V even 557.9 km/h. However, the transformer for our test case even manages to put 13.5 V on the track with the low load.

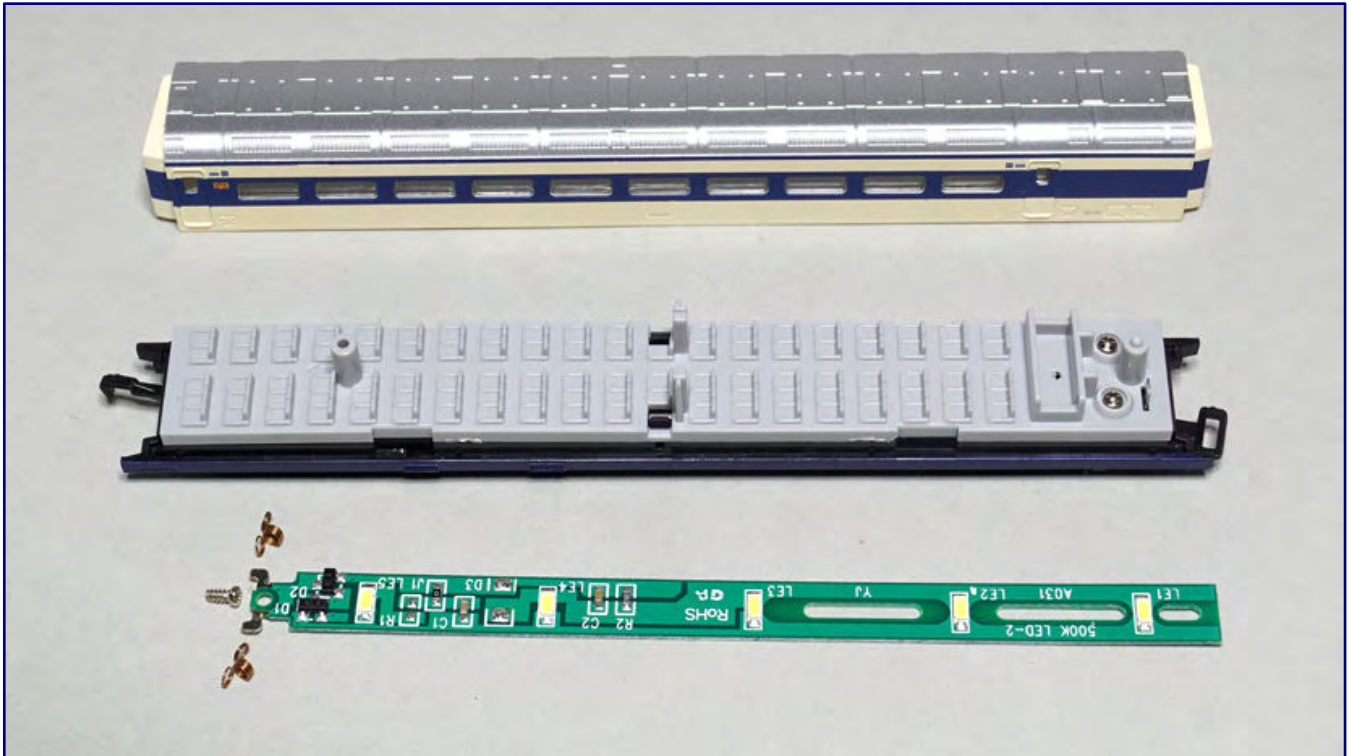
At 569.4 km/h, the 0 Series Shinkansen would have finally beaten the TGV's world rail record. But this would have failed because the train would have been carried off the track in the curves at this "flying speed."

What conclusion do we draw from this: The company's own speed controllers offer significantly more driving comfort for these models and also offer a parking light function. Our test experiment exceeds the maximum operating voltage specified by Rokuhan and even Märklin and must not be the customer's standard case anyway.

However, the transformer we use is still widely used and dates back to the time of the five-pole motors, which placed a considerably higher load on it. Even with Märklin's more recent deliveries, we often notice a tendency to run off the rails, where the old motors caused the track voltage to collapse to the nominal value.

A steady hand and a trained eye should therefore help us to use this beautiful train in such a way that we will enjoy it for a long time. We deliberately ignore the fact that it is far outside the NEM expectations, because the control on the rotary knob is more important to us, and it works. Nevertheless, we suggest changing the gearbox tuning in future designs at the expense of the top speed.

As expected, the current consumption is low, but differs more than we had expected in both directions of travel. For this reason, we do not give average values for all measurements, but those for travelling forwards (towards Shin-Osaka) and backwards (towards Tokyo), separately.



One of the long PCBs (A031) has been placed in front of the open housing in the opposite direction to its mounting direction: It is installed with the LED radiating downwards on the pins of the interior fittings. The screw hole can be seen on the far left, which will be visible again under one of the slotted holes. The two tension springs are attached to the “arms” of the plate next to which they are positioned. The counterparts protrude from the grey interior part on the far right.

At transformer position 100, both motors together draw around 38 mA forwards and 44 mA backwards. At position 150, these values rise to 56 and 64 mA. Nevertheless, they remain fully in line with expectations.

They are on a par with the values of other bell-type armature motors used in Z gauge. The doubling of the current consumption with parallel connection corresponds to the laws of physics compared to only one operating drive.

## Summary

With this new product, Rokuhan has once again taken great care to deliver a visually and technically appealing, high-quality product. The long train is sufficiently and properly motorised to also perform its high-speed service in the model.



The successful transposition of the prototype's appearance into the model, which is perfect in every respect, does the rest to guarantee operating fun on the layout. And in view of the popularity and high profile of the train, which also provided important impetus for the development of high-speed services in France and Germany, this will certainly not be limited to Japan alone.



Hikari number 1 is on its inaugural journey from Tokyo towards Shin-Osaka. The photographer lies in wait for it as it exits the tunnel onto the river bridge and successfully captures it on celluloid. Writing railway history is now possible on a small scale with the help of Rokuhan. Photo: Christoph Maier

We are also expecting a noticeable level of interest in the two other main Z gauge markets. Anyone who is not already enthusiastic about the prototype should now catch fire after seeing this model.

And this is why we acknowledge the importance of this successful new product by nominating it for our best New Product of the Year 2023 award in the locomotives category.

**Model manufacturer:**

<https://www.rokuhan.com>

**Website of the German importer:**

<https://www.rokuhan.de>

**Trainini TV – Folge 21 (Middle December):**

<https://www.youtube.com/TraininiTV>

The Shinkansen of 1964

## Bullet Train

*Japanese high-speed transport has its roots in the bullet train project. Once the decision in favour of bullet trains had been made and the first Shinkansen line was under construction, the vehicles began to take shape. With their round noses, in which a sphere seems to be integrated, and their high speed, they earned the nickname bullet trains.*

The 0 Series Shinkansen were not given their series designation until later, when it became necessary to distinguish them from the other types that followed them. They were the first multiple units to be built in Japan for travelling on high-speed lines.

Shinkansen actually means “new main line” in the literal translation and therefore does not refer to the trains travelling on it. However, the term developed into a synonym. One reason for this may have been the large-scale advertising in the early years.



This 0 Series Shinkansen appeared on 5 May 1967 near Yurakucho Station in Tokyo in the same state as the new Rokuhan model. At this time, the high-speed service was not yet three years old. Photo: Roger Wollstadt (CC-BY-SA-2.0)

It made the trains and their design world-famous. However, hardly anyone understood the term Shinkansen or knew how to categorise or define it correctly. The fact that Japan would opt for multiple-unit trains was not even planned at the beginning.

At a time when great attention was being paid to growing car traffic and increasing air traffic, it was already courageous to rely on the railway for long-distance transport and create a completely new infrastructure.



Due to the mountainous topography, the Cape gauge (1067 mm track gauge) , which is not suitable for high speeds, had traditionally prevailed in Japan. The planned “bullet trains” were therefore accompanied by a change to the standard gauge system (1435 mm).

At the same time, it was assumed that locomotive-hauled trains could be used again for rolling stock. Whether they should run with steam or electric traction was still an open question at the time. In the end, multiple units were chosen because they promised a more even distribution of the masses and would reduce the stress on the superstructure.



When this twelve-car unit was put into service on the Tōkaidō line in 1971, the commercial success of the Shinkansen was already clear. The trains have already undergone their first modifications. For example, the parabolic nose has been painted in the same colour as the car, changes have been made to the bottom of the apron, and the number of pantographs has been reduced. Photo: Wilford Peloquin (CC-BY-SA-2.0)

In order to secure the political decision in favour of the new system, the project costs were deliberately set too low; in fact, they ended up being twice as high as requested. However, the Shinkansen system developed into a successful model and later became a revenue and profit generator: passenger numbers tripled between 1966 and 1973!

Further figures underline the success of the JNR after privatisation: the four lines existing at the end of the 1980s were used by over 200 million passengers a year. The total length of this network was 1831 kilometres. When this twelve-car unit was put into operation on the Tōkaidō line in 1971, the commercial success of the Shinkansen was already clear. The trains had already undergone their first modifications. For example, the parabolic nose was painted in the same colour as the car, the underside of the apron was modified, and the number of pantographs was reduced.

The market share of total traffic between Tokyo and Osaka (approx. 500 km) was 85 %, between Tokyo and Hiroshima (approx. 800 km) 65 %, and between Tokyo and Hakata (approx. 1100 km) around 30 %. The train delay during rush hour was up to four minutes.

The first construction work for the first line began on 20 April 1959, using structures already built for the bullet train project, such as the Nihonzaka Tunnel in Shizuoka Prefecture.

Nevertheless, enormous earthworks were necessary, as curve radii of 2500 metres (later increased to 4000 metres) for the high speed led the route over many an obstacle. However, the high performance of the new multiple units also allowed gradients of 1.5%.

On 1 May 1961, Japanese National Railways (JNR) received loans totalling 80 million dollars from the World Bank for its project. This was linked to the strict condition that the line had to be opened by the 1964 Olympic Games in Tokyo, which was achieved on time: the inaugural journey from Tokyo to Shin Osaka took place on 1 October 1964, with the Games starting nine days later.



After the privatisation of the JNR, JR West was responsible for the San'yō line. It later repainted the trains back to their original colour scheme. A four-car unit reaches Hakata station as a Kodama connection in July 1998. Photo: © DAJF / Wikimedia Commons / CC BY-SA 3.0 & GFDL

Due to a lack of commercial experience, the cruising speed of the new trains was initially no higher than 200 km/h and was only subsequently increased to 220 km/h, making them the fastest commercially used railway vehicles in the world when they were introduced.

Around 60 years after the three-phase test runs on a German line near Zossen, a state railway reached and exceeded the “magic” mark for the first time in scheduled traffic. This also symbolised Japan's economic recovery after the Second World War.

continues on page 27





Pictures of the front end and carriage transition with wind deflector in the last years of operation. Photos: Cassiopeia sweet (PD-self; top) / Takeshi Kuboki from Amagasaki, Japan (CC-BY-2.0; bottom)

## From tests to regular operation

Before the first line was put into operation in 1964, test runs were carried out on a 37 km section between Odawara and Ayase in 1962. The JNR had already acquired land here for the original bullet train project and was able to enter the construction phase quickly.

This test track was used to test the Shinkansen 1000 series prototypes and also to develop the 0 Series trains. When the entire line was completed, the test section finally became part of the Tōkaidō-Shinkansen.

A special feature of the train technology and track equipment was the automatic driving control of the multiple units, which made signals superfluous. Both acceleration and braking were initiated automatically and controlled externally. Only the last few metres at the platform are influenced by the driver, so that a Shinkansen stops precisely at the platform.



Scenes like this one with a 16-car unit on 26 April 1992 also prompted our translator Christoph Maier to create a suitable operating diorama for this extraordinary train. Photo: Gohachiyasu1214 (CC-BY-SA-4.0)

All information (track status, etc.) and driving commands reach the driver's cabs via coded signals in the AC frequency of the traction voltage. The trains themselves also send signals in advance, which determine the route at track junctions. If seismographs register an earthquake, all trains receive impulses to stop immediately for safety reasons.

After the two Shinkansen prototype trains of the 1000 series, a third was produced as a prototype for the 0 series. This third set consisted of six carriages: a dining car, a 1st class carriage, and four 2nd class carriages. With the start of regular high-speed services, this train was used commercially with 12 carriages.

continues on page 29





Shinkansen trains traditionally have a 3 + 2 configuration, as can be seen on the way to Kyoto in 1971 (photo above) and in the Kyoto Railway Museum (photo below). Photos: Wilford Peloquin (CC-BY-SA-2.0; above) / Cheng-en Cheng (CC-BY-SA-2.0; below)



The standard trains were painted cream white with a blue window strip and blue frame and rail clearances. Their round shapes at the front still bore witness to the previous decade, before angular designs became more modern. A special feature was the translucent, but not transparent, spherical noses of the first trains, through which light shone.

480 carriages were originally available, which were combined into 40 trains – i.e., twelve parts per unit. Each of these trains was divided electrically into six units of two carriages each, and each half of the train had a dining car.

Each train assembled in this way offered space for around 1000 passengers. Each axle of a train had its own drive with a 185 kW shaft motor. Extrapolated to the entire train, this is 8880 kW (12075 hp). They are supplied from an overhead line with 25 kV voltage at an industrial frequency of 60 Hertz.

The construction of the 0 Series Shinkansen trains began in 1963 and lasted until 1986. In view of the expected service life of only 15 years due to the high load, instead of the usual 30 years for other rail vehicles, it is clear that at no time all the units were ever built-in service at the same time.

A total of 3216 carriages were built in 38 series. The maximum number in the stock in 1976/77 was a proud 2338 wagons in 146 trains, with two wagons serving as reserves.

Most of the vehicles were actually taken out of active service after the estimated period. The last trains of the series covered here ran as the Kodama service on the San'yō-Shinkansen line of JR West and on the Shin-Osaka - Hakata line.



As Kodama 635, this six-car unit is travelling in new, very attractive colours as it passes in front of the photographer's lens in Higashi-Hiroshima on the San'yō line. The train runs from Shin-Osaka to Hakata. The train shows wind deflectors next to the pantographs, and the smaller side windows described in the text, which were temporarily installed to save costs. Photo: © DAJF / Wikimedia Commons / CC BY-SA 3.0 & GFDL



This leads us to the order of the multiple units during the entire operating period: until 1971, the trains were twelve-car units (two dining cars, two 1st class cars, and eight 2nd class cars). From 1969, however, 16-car units were also put together due to the strong increase in passenger numbers, and these became the norm from 1971 onwards.

The composition of these trains varied depending on the line travelled. The ratio of 1st to 2nd class carriages was different. From 1985 until the last year of operation in 2008, only four- or six-car units were used on less popular routes.

Shortly before being withdrawn from service, the units still in the JR West fleet, which had been responsible for the San'yō-Shinkansen since 1987, were repainted in their original livery (cream blue / white). Their last scheduled service took place on 30 November 2008, and on 14 December 2008 they were finally retired with their last journey.



A 0 Series Shinkansen is also preserved in the Kyoto Railway Museum and can be viewed there via a promenade. Photo: Brakeet (CC-Zero)

The fact that these conventional trains, developed long before three-phase AC technology came to dominate railway technology, were built and used for so long is closely linked to the economic situation of the former JNR.

Due to a massive increase in land acquisition costs, blockades, and resistance to the expansion of the network, as well as labour disputes, the Japanese National Railways increasingly got into financial difficulties and was ultimately overindebted. The network and technology stagnated, and passenger numbers even fell, at times.

The government countered this in 1987 by initiating privatisation and founding four regional companies, which were to be responsible for individual routes of the Shinkansen network. Subsequently, further development picked up speed noticeably.

Due to their special historical significance, some 0 series trains and carriages have been preserved: JR West donated a head end to the Railway Museum in York (England) back in 2001. In August 2008, a complete train was transferred to the Saitama Railway Museum.

Webpages with model information and data:

<https://www.hochgeschwindigkeitszuege.com/japan/jr-baureihe-0.php>

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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Die Vielfalt offener Wagen

## Massenphänomene und Exoten

**Offene Wagen stellten einst die zahlenmäßig größte Gruppe an Güterwagen im Staatsbahnbestand. Und das sollte auch über Jahrzehnte Bestand haben. Entsprechend groß ist auch die Modellvielfalt. Fast 30 Jahre nach Erscheinen seines Bands 3 der Güterwagen-Reihe aktualisiert die erste von zwei Broschüren das erforderliche Wissen.**

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Zum Herausgeber, aber nicht alleinigen Autoren der vorliegenden Broschüre brauchen wir wohl nicht viele Worte zu verlieren: Stefan Carstens ist im Bereich der Eisenbahnliteratur bestens bekannt. Er hat viele wichtige Archive übernommen, fotografiert selbst und hat sich ein profundes Wissen besonders im Bereich der Güterwagen erworben.



Werke, die seinen Namen tragen, stehen ohne Ausnahme für herausragende Qualität. Auf seine Rechnung gehen viele heutige Standardwerke, darunter die inzwischen auf drei Verlage verteilte Güterwagen-Buchreihe, die mit dem hier besprochenen Sonderheft angereichert und inhaltlich ergänzt wird.

Aufbauend auf Band 3 der genannten Reihe, der erstmals 1996 erschien, liefern die von Stefan Carstens beauftragten Modellbauer und Autoren, darunter auch wieder er selbst, viele neue Informationen, die in den bisherigen Auflagen noch nicht enthalten sind.

Ein wesentlicher Grund dafür ist, dass sich in den fast dreißig Jahren seit Ersterscheinen viele Erkenntnisse ergeben haben, die frühere Annahmen widerlegen, stützen oder auch vervollständigen. Erinnert sei hier nur an den Zusammenbruch des Ostblocks, der überhaupt einen Zugriff auf Archive und Quellen erlaubte, deren Existenz 1996 nicht mal bekannt war.

Aber auch auf der Modellseite, die seit Beginn der Reihe nicht ausgeklammert war, hat sich viel getan. Solche, die einst betrachtet und für Umbauten oder Superungen herangezogen wurden, mögen heute

als überholt gelten. Vielfach sind sie auch gar nicht mehr erhältlich oder wurden von deutlich besseren Nachfolgern ersetzt.

So macht auch der Erfinder und Herausgeber der Reihe keinen Hehl daraus, dass nach dem Ursprungsband über gedeckte Wagen der Regelbauart auch die folgenden Titel über die Sonderbauarten (bisheriger Band 2) und danach die offenen Wagen (Bände 3 und 4) eine Aktualisierung verdient haben.

Doch jedermann weiß: Zeit ist begrenzt und Band 2.1 nicht fertig, ein völlig neuer Band 10 zugleich aber auch im Entstehen. So war es sicher eine gute Idee, all das angesammelte Wissen und neue Erkenntnisse, die sich nicht in einem Buch unterbringen lassen, zu einer Broschüre zusammenzufassen.

Und so ist der Vorbildteil dieses Mal ausführlicher als bei den bisherigen Broschüren der Reihe „Modell & Vorbild“. Schwerpunktthemen sind die von den Staatsbahnen gebauten Wagen, die geschweißten Wagen der Deutschen Reichsbahn sowie die während des Zweiten Weltkriegs im Ausland gebauten Wagen.

Achtung: Damit sind nicht nur Wagen nach deutschen Zeichnungen gemeint, sondern auch solche Fremdwagen, die unter deutscher Besatzung in hiesige Bestände gelangten und auch nach Kriegsende weiter Dienst taten. Viele bislang unbekannte Fotografien wie auch Originalzeichnungen untermauern die spannenden Texte.

Das Konzept der Broschüre ist dennoch unverändert geblieben, aber der Anspruch an den Modellbau hat sich deutlich verändert. Bewusst sollte der Fokus nicht auf Perfektion und eine Stufe des Feinmodellbaus gehoben werden, wie ihn nur wenige beherrschen und er nur eine kleine Zielgruppe anzusprechen vermag.

Ein Grund dafür war auch, dass es in allen Spuren sehr viele zeitgemäße Modelle offener Wagen gibt, die häufig nur wenige Handgriffe und kleine Verfeinerungen brauchen, um noch näher an ihr großes Vorbild zu rücken. Das sind beispielsweise Zurüstungen von Pufferbohlen, Neubeschriftungen oder Alterungen, wie wir sie auch selbst gern zum Thema machen.

Zusätzlich eingeflochten ist zum Ende des Hefts ein Vorbildartikel die epochenübergreifende Kohleförderung, den verschiedenen Kohlesorten und -arten und zu deren Transport. Diese Hilfen sind wertvoll für den Leser, weil Kohle einst das meisttransportierte Ladegut in diesen Wagenbauarten war.

Ganze 132 reichten aber nicht aus, um alles neu Zusammengetragene hier unterzubringen. Deshalb wird Anfang 2024 noch ein Band 2 folgen, der weitere Vorbilder näher betrachtet und diejenigen Modelle dazu vorstellen wird, die seit Erscheinen des Referenzbuchs neu auf den Markt gekommen sind. Auch dazu sollen dann kleinere Basteleien vorgestellt werden.

Mit Blick auf die große Zahl unterschiedlicher Modelle auch im Maßstab 1:220 und der Bedeutung offener Wagen beim Vorbild können wir die besprochene Broschüre auch Zetties nur wärmstens ans Herz legen. Auch ohne explizites Betrachten unserer Baugröße lassen sich viele Ideen, Vorschläge und Anregungen mitnehmen – auch bei uns haben sie Projekte für künftige Beiträge in Gang gesetzt

**Publishing pages:**  
: <https://www.stefancarstens.de>  
: <https://www.modellbahnunion.com>



## Europalok und Geschwister **Über Grenzen hinaus**

***Der Begriff „Europalok“ brachte deutlich und unmissverständlich zum Ausdruck, in welche Richtung sich die Bundesbahn orientierte, als sie Mehrsystemlokomotiven entwickeln ließ und (in kleinen Stückzahlen) auch beschaffte. So kommt ihnen doch eine besondere Bedeutung zu, die in der Fachliteratur bislang vernachlässigt wurde. Doch das ist nun endlich vorbei!***

Harald Jordan | Manfred Lohmann | Mathias Oestrich |  
Tobias Pukallus | Manfred Traube  
Die Mehrsystem-Eloks der Deutschen Bundesbahn  
Die Baureihen 181, 182, 183 und 184

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Die Geschichte der von der Bundesbahn entwickelten Elektrolokomotiven scheint beinahe abgearbeitet, was die Baureihenbibliothek im EK-Verlag betrifft. Doch immer wieder mal werden wir eines Besseren belehrt. Das liegt an Lücken bei Fahrzeugen, die nicht so sehr im Fokus stehen, wie auch neuen Erkenntnissen.

Auch ist die Einsatzgeschichte der meisten Konstruktionen, die ab etwa 1951 begannen, bis heute nicht zu Ende. Einige Lokomotiven stehen unverändert mit einzelnen Exemplaren im Dienst privater EVU. So ist es auch bei der Baureihe 181<sup>2</sup>, die eines der Themen im vorliegenden Band ist.

Sie hat erst vor wenigen Jahren ihre zweite Karriere begonnen und unverändert gelangen neue Maschinen zurück in den aktiven Dienst. Gleichzeitig steht sie am Ende einer Entwicklungsreihe, deren vorausgehende Glieder weitgehend in Vergessenheit geraten sind. Auch die Baureihe 181<sup>2</sup> war mit nur 25 Exemplaren bei der DB eher eine Randerscheinung, wenngleich auch kein lokales oder nur regionales Phänomen.

Doch wie es auch sei, auf der Modellbahn steht sie aktuell sehr im Fokus. Nach Roco und zuletzt auch Piko hat sich auch Märklin diesem Loktyp im Maßstab 1:87 angenommen, bei der Spur N war es Arnold. Auch im Maßstab 1:220 hat sie dank Rokuhan eine schon fast zehn Jahre währende Geschichte. Das Interesse der Modellbahner am Vorbild sollte also ausgeprägter denn je sein.

Und damit haben wir schon die Zielgruppe dieser Lektüre erfasst. Wegen eines hohen fachlichen Anspruchs sind das vor allem die technisch versierten Eisenbahnfreunde, aber ebenso die Statistiker und eben auch diejenigen, die lediglich Inspirationen und Eindrücke für den Einsatz ihrer Modelle suchen.

Alle drei genannten Gruppen kommen mit diesem Buch auf ihre Kosten soviel sei schon vorausgeschickt. Gewiss gilt es, das noch etwas zu differenzieren und aufzusplitten, aber als Fazit, das sonst am Ende dieser Rezension zu lesen, wäre, taugt es auf jeden Fall: Dieses Werk darf in keinem Bücherregal derjenigen fehlen, die sich zu einer der drei Gruppen zählen.



Doch was erwartet sie bei der Lektüre? Gleich fünf Autoren, darunter bekannte und weniger bekannte Namen, haben daran gearbeitet, die Geschichte von gleich vier, zahlenmäßig nur als Splitterbauarten geltenden Baureihen aufzuarbeiten.

Erstmalig wurden, von einer früheren EJ-Broschüre zur einzigen Serienbauart aus diesem Themenfeld mal abgesehen, die Mehrsystemlokomotiven der früheren Bundesbahn in einem Buch gewürdigt. Um ihre Bedeutung herzuleiten, beginnen die Ausführungen mit geschichtlichen Abrissen.

In einem Überblick werden die behandelten Baureihen kurz dargestellt, um dann einen Rückblick auf die Elektrifizierung bei den europäischen Bahnen zu geben, der die Vielzahl an Systemen erklärt und die Probleme des grenzüberschreitenden Verkehrs verdeutlicht. Eine besondere Rolle kommt dem Saarland und seinen Eisenbahnen zu, die hier zum Verständnis ausführlicher zu beschreiben war.

Dann folgen die bekannten und klassischen Teile eines EK-Baureihenportraits: Technik- und Einsatzgeschichte – hier für alle vier Baureihen einzeln und chronologisch aufgearbeitet. Auch die Unterbaureihen 181<sup>0</sup>, 181<sup>1</sup>, 181<sup>2</sup>, 184<sup>0</sup> und 184<sup>1</sup> werden dabei differenziert betrachtet.

Als „Europalok“ gefeiert und ins Licht der Öffentlichkeit gezerrt, herrschten hier trotz aller einstigen Probleme hohe Erwartungen. Auf einem Kontinent, der Krieg und Nationalismus zu überwinden haben schien und politisch näher zusammenwuchs, war das Zusammengehören mehr als eine kurzfristige Modeerscheinung.

Doch was heute selbstverständlich erscheint und kaum noch so deutlich wahrgenommen wird, bereitete auf technischer Seite große Herausforderungen und brachte auch Fehlschläge mit sich. Die „Europalok“ E 410 (Baureihe 184) wurde keine, stattdessen erwies sich erst die Baureihe 181<sup>2</sup> als voller Erfolg, der aus den Erkenntnissen mit der E 310 (Baureihen 181<sup>0</sup> und 181<sup>1</sup>) resultierte.

Dass die zuletzt genannten Vertreter der Baureihen 181 und 184 auch als formschön gelten, dürfte trotz ihres Status als „Exoten“ im Betriebsbestand sicher der Hauptgrund dafür sein, dass sie auch in verschiedenen Maßstäben auf die Schienen fanden.

Der EK-Baureihenband wird so allen Erwartungen gerecht, zeigt aber auch einige Unterschiede zu anderen Büchern der Reihe. Das ist nicht als Kritik zu verstehen, unser Hinweis soll nur vor Fehlannahmen beim schnellen Durchblättern schützen: So ist kein als eigenständiges Kapitel abgetrennter Statistikteil zu finden.

Grund dafür ist einfach die Standorttreue aller Maschinen. Maximal zwei verschiedene Betriebswerke der DB verzeichnen ihre Betriebsbücher, wenn wir innerhalb Saarbrückens mal von einem „Wechsel der Straßenseite“ absehen.

Und auch bei den gedruckten Farbaufnahmen stellen wir einen Unterschied fest. Anders als bei vielen anderen Bänden sind sie nicht in einem separaten Farbteil gesammelt, sondern übers Buch verteilt in kleineren Einheiten zu finden. So ist der Gesamtanteil zumindest gefühlt noch deutlich höher als bei Auflagen zu deutlich älteren Konstruktionen.

Die Wiedergabequalität und Auswahl ist wie die Textverständlichkeit auch hier wieder auf höchstem Niveau. Und so ziehen wir doch noch ein weiteres Fazit: Vor uns liegt ein neues Standardwerk, das wir verdient für die Neuerscheinungen des Jahres 2023 in der Kategorie Literatur nominieren.

**Publishing pages:**  
<https://www.eisenbahn-kurier.de>  
<https://www.ekshop.de>



Model train days in Hochdahl

## Participating instead of just watching

*The Erkrath-Hochdahl railway and local history association organises model railway days every year. Thanks to the help of supporting clubs from the surrounding area, they always offer a colourful programme. Trainini® was also there for the first time. The programme for youngsters seems unique and is worthy of imitation, as we report.*

Size isn't everything, as exhibitors and visitors to the 2023 Model Railway Days in the Hochdahl engine shed recently discovered. The local press was also impressed and generated even more demand with an enthusiastic report. In fact, everything was just right here, which is why we want to take a look back: Imitation explicitly desired!

What had happened? The Erkrath-Hochdahl Railway and Local History Association (EHEH) invited visitors to a model railway exhibition within its historic walls, as it does every year. The model railway clubs from the surrounding area, from Düsseldorf to Wuppertal, support it by actively participating with exhibits and personnel.



With this four-strong delegation, **Trainini®** supported the model railway days in the Hochdahl engine shed. In addition to promoting Z gauge, the main focus was on a craft and hands-on programme for children, which was extremely well received.

After a nice contact, good exchange and helpful support in working in and with archives in spring 2023, a delegation from **Trainini®** was also present for the first time. Due to the exhibition profile, which is deliberately aimed at the broad public, and a consistently high proportion of children among the visitors, we decided in favour of a hands-on and handicraft offer.

The EHEH's good public relations work in the run-up to the event also proved to be helpful. The visiting local editor was also personally impressed by the concept and what was on offer, which is why his report on the exhibition weekend provided further impetus.

On the first of the three days, he had explicitly planned to visit the **Trainini®** stand, as the EHEH had already drawn his attention to the new offer here. However, he also realised that he was unable to see and reach this presence without having to work his way through a crowd of people.

Due to the large crowds, all the active participants were in constant use and it was therefore not possible to hold the desired discussion before the end of the exhibition on 1 November 2023. The question-and-answer session therefore only started at 6.00 pm and focussed primarily on the question of why there was such a high demand here.



Shots like these were only possible in a few quiet minutes on the three days of the exhibition: on the left, three (of nominally four) handicraft areas are nevertheless occupied, on the right you can see the EHEH's children's layout, where the youngest were allowed to be train drivers themselves.

Five pages of notes later, our thirst for knowledge was quenched and we are happy to explain how we achieved this success. The local press representative only had one question: "Can I come back (privately) on Saturday and try it out for myself?"

We could not have received greater praise and clearer recognition, because such words speak of personal enthusiasm. However, it is important for us to analyse and understand such experiences and to be able to repeat them in Hochdahl and elsewhere.

If we were to rely solely on statistical data, our report would be interesting, but much less spectacular. The Hochdahl engine shed is now a museum and offers a well-known and popular hall for many events. The building is located close to the current S-Bahn station, is easy to reach by car and public transport, and has historical significance.

The railway line that runs along it is the oldest in western Germany and is particularly challenging topographically. In the first years of operation from 1838, the steep incline between Erkrath and Hochdahl





A large part of the company's own presentation was consistently geared towards young model railway enthusiasts: The children took control of the track oval made of club table layouts while the glue on their beer mat dioramas dried. Inside, they and their parents marvelled at a Shorty Shinkansen (left) and two youngsters' dioramas (centre and right) by former primary school pupils.

could only be negotiated with the help of a steam engine, which pulled the trains up the hill using huge pulleys and chains.

After a short time, it became clear that this method was too complex and expensive, as well as not very promising in view of increasing train weights. From then on, trains travelling downhill pulled their counter-train uphill using the chains and guide rollers. But even this was eventually outdated and the push-pull service familiar from other ramps was introduced.

85 007, which came here for a few weeks after the end of its service on the Höllental railway to earn its place of honour, before it was finally taken out of service, achieved brief, albeit great, fame here.

## History with a future

The locomotive shed, which hosted the exhibition on 1, 4, and 5 November 2023, served as a shelter for the push-pull locomotives and a place to bunker coal, collect water and remove slag. Today, only photos bear witness to this, as there are no longer any tracks directly in front of the shed or inside of it.

To this end, the historical site houses an archive, primarily on local railway history. It proves to be valuable time and again when the EHEH publishes its own books and thus helps to ensure that regional history is not forgotten.

On the fringes of the event, we, therefore, also met author Armin Gärtner for a personal initial discussion, whose latest work we reviewed just last month. He gave us an insight into the follow-up volume on the history of DSG, which he is currently working on.



Author Armin Gärtner (left) proudly presents the book cover design for his new work on the history of DSG, which is currently nearing completion. Editor-in-chief Holger Späing (right) is looking forward to an exciting read with him.

The chosen structure, selection of images and the few passages that we were able to read in advance have inspired us and aroused our curiosity about the publication. This is planned for spring 2024 and is to be combined with a club anniversary. We will keep our readers up to date on this.

The Hochdahl engine shed has only recently developed into a meeting place for model railway enthusiasts. In 2023, there were well over 1,000 visitors, including exactly 650 paying adults and a further 400 children with free admission, more than ever before. This meant that the previous record from 2019, the last pre-pandemic edition, was once again clearly surpassed.

But what makes the Model Railway Days so special is something else, as there are much larger exhibitions. The exceptionally high proportion of children certainly speaks for itself. If we listen to the many complaints about declining interest in the hobby, then a different photo emerges here. So, it can't be down to the children and their parents alone. So, what distinguishes the model railway days in Hochdahl from other club exhibitions?



Here we see the organiser's communication in advance as a success factor. The press must accurately publicise the fact that this is a family event that is particularly geared towards the needs of children.



This old layout still does a good job: it invites you to produce the electricity required to power the locomotive yourself on your bike.

In order to be able to do this, those responsible must also understand what these needs actually look like. Anyone who has children or grandchildren can observe this and experience it first-hand. Everyone else should and may ask their parents. This is also an opportunity to find out how parents imagine a family-friendly destination.

The following key points seem particularly important to us:

- Children want to learn to understand their environment. The German word already suggests that it is not enough to offer something to look at, but that something tangible is required. People don't learn just by looking, their hands convey lasting memories and learning effects.
- This means that it is not the level of design of the exhibits that is the decisive success factor in attracting young people, but the options to participate and touch. Otherwise, children are always taught not to touch anything (while all too often, adults can't keep their fingers to themselves).
- For parents, especially single parents, the expected financial outlay is important. The more expenses for admission and ancillary costs (such as contributions for offers and catering) have to be calculated, the more a visit is examined and compared with alternatives.

Adults paid 2 euros admission, children aged 14 and over 1 euro, and all younger visitors paid nothing at all. 4 euros certainly won't hurt a family, but the number of visitors (and voluntary donations) will still make a tangible contribution to the association's coffers.



At the **Trainini®** craft table, children were allowed to landscape a beer mat and add a tree, flower or figure to fit their individual taste. This offer, which was free of charge for participants, was only possible thanks to sponsors who were mentioned at the table.

The arts and crafts activities were free of charge for the participants and were financed by sponsors and in some cases also privately. This also removed an important inhibition threshold for parents, who often reacted more cautiously than the children because they were first expected to do a necessary cash check.

Part of the company's own concept for success is that it is not adults who actively approach and look after the children, but rather the company's own youngsters. This also requires desire and motivation on this side, but has long since become a matter of course.

### Children in the middle of it all

And last but not least, we must not forget that the children are visiting a model railway exhibition with their parents. If the "grown-ups" are looking at details and technical refinements, the children's eyes will linger on moving trains, raising and lowering barriers, and cars.

And the question arises as to whether they are allowed to intervene themselves. While the answer elsewhere is usually no, the children in Hochdahl usually had a ride controller in their hands before they could even express their wishes.

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The EHEH had set up an old prefabricated layout in the centre of the hall just for this purpose, with no staff to be seen for miles around. When it is clearly communicated, that children are allowed to become train drivers themselves, then their hands are also used, and there is a delighted laugh in return.

The presence of Ralf Junius (**Trainini TV**) and Holger Späing (editor-in-chief) followed a similar concept. Most of the space was taken up by the craft table with a total of four places. Supervised by **Trainini**-juniors Kristin and Hendrik Späing, the children were allowed to design a beer mat with a landscape on their own or with their parents, even unaccompanied parents were not turned away, as long as they did not take a child's place.

During the breaks for drying the glue, they then worked on the layout set up by Ralf Junius from club layouts, on which they ran the mouse train. At the touch of a button, they could also start the circuit for a lorry model. Shorties from Rokuhan such as the Hello Kitty Shinkansen or the "Glass Train" also found the right recipients here.



For experienced model railway enthusiasts, however, there was also a peep-box layout called "PeZuBox." The diorama "On the Swabian railway" created years ago by Kristin and Hendrik Späing and a primary school project entitled "Defective signal at block 53" demonstrated the quality that a first-time project can achieve, if patience and observation skills are present.

Using the example of the therapy diorama (presented in three parts in this magazine in 2022), parents learned that model making does not automatically have to be expensive, if it is to be good. After all, the invisible core is made up of many materials that are already available in the household and craft room. What has to be purchased regularly justifies the purchase through its yield.

Other participations also presented comparable entry options for digital operation and digital control, for example, and met with great interest among the youngest generation ("digital natives").

It seems almost superfluous to point out that paper modelling has its own challenges, but in almost no case does it involve high costs. And, so, it came as no surprise that some families returned during the weekend.

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There was also plenty to see for Z gauge in Hochdahl, including these two box layouts at MEC Wuppertal e.V. In a summery atmosphere, the circus (photo above) invited visitors to a performance, while winter spread its very own idyll in a snowy landscape (photo below).





This railway bus was also on display in the "PeZuBox," much to the delight of the children (photo above). Paper modelling was also represented during the weekend, where visitors were also encouraged to take part (photo below).



Quite a few proudly reported that they had visited a specialist model railway retailer during the two previous shopping days and had stocked up on craft and design materials.

And that's the only way everyone can benefit in the end. Three days of exhibition meant hard work and high capacity utilisation for everyone involved, but also the certainty of having inspired many younger people and infected them with the model railway bug.



Only on the first day of the exhibition was this Z gauge box layout on display at the MEC Wuppertal e.V. stand. At the weekend it was exchanged for the suitcase that can be seen on page 44 above.

It will also be good for the parents to know that their children have discovered a creative and challenging hobby that teaches them many skills, technical understanding, and manual dexterity. And should questions arise in the future, they will also know where and to whom they can ask them. Let's tackle it and our future will be saved.

**Organiser's webpages:**

<https://www.lokschuppen-hochdahl.de>

**Video on Trainini TV (Episode 18):**

<https://www.youtube.com/TraininiTV>

# Adventausstellung Modelleisenbahner Soest

**Modellbahnbörse**

**Fahrbetrieb auf  
allen Anlagen**

**Kaffee  
und  
Kuchen**

**Am 10. Dezember 2023  
Von 11:00 bis 17:00 Uhr**

**Würstchen vom  
Grill**

**H0**

**Neuengeseke**

**TT**

**Neuengeseker Heide 1**

**Unkostenbeitrag: Erwachsene**

**3,50 € / Jugendliche bis 16**

**Jahre 2,- € / Kinder unter 6**

**Jahre frei**

**Z**

**N**



Readers' letters and messages

## Zetties and Trainini in Dialogue

*Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.*

To the photo on page 54 of Trainini® 10/2023:

I've just leafed through the new Trainini. When you leaf through it, you often have a superficial look. And then there's the eye-catcher.

I got stuck on Hendrik's shot. This is not only king track 1, but also a royal photo.

Hendrik has got it right. He focusses on the locomotive; the background is only slightly blurred. And on the right, the passer-by (passenger or is it a railway employee?) with whom you leave the photo.

Even if it is gauge 1, I think the photo belongs in the Trainini 2024 calendar, unique and first-class. Hendrik gets first place from me.

Hans Helbach, Bonn

Editorial response: This is a recording that would not be published under the author's name, but solely under the Trainini® brand as the sole rights holder. In this case, however, the editorial team is of the same opinion as expressed in the letter to the editor. Our up-and-coming photographer has delivered an exceptionally good piece of work, the motif of which has been accurately recognised, the overall image composed to match it and then implemented with impeccable craftsmanship. It was therefore a matter close to our hearts to explicitly point this out and give him the chance to establish himself as a model railway photographer in the long term.

**Missing part 2 to the construction report:**

A long-cherished wish has come true. I ordered the ET 403 from NoBa-Modelle and have since had it delivered. I printed out your report in the 9/21 issue and studied it in detail.

My question: has the 2nd report on the "glass work," lettering and motorisation been published in the meantime? If so, in which issue?

Rainer Leip, per E-Mail

Editor's reply: Unfortunately, part 2 of the model of the 403/404 series from NoBa-Modelle is still outstanding. It is one of a long list of unfinished projects that have so far been prevented from being completed for various reasons, some of which we have also discussed in this issue.

Nevertheless, we decided on part 1 at the time, expecting a prompt continuation, because the train fitted in perfectly with the "50 Years of Intercity" theme, as this was exactly what it had been developed for. It was also one of our biggest wishes for Z gauge models until it was finally granted.

We are happy to reveal our approach to the glass work: due to the large number of panes of different sizes, we prefer liquid glazing with Micro Kristal Klear from Microscale. Typical for this train, however, were the brown tinted and vapour-coated windows (without driver's cab), to which the original paintwork was also elaborately matched.



The Class 403/404 high-speed railcar for intercity services is unfortunately one of our projects that has been put on hold for far too long for various reasons.

This colour effect can also be achieved with this product. For this purpose, some dark brown colour, ideally with the help of the RAL tones also used in the colour tape, is mixed into the adhesive varnish beforehand. An (equally) water-based varnish should be used here; the appropriate dose should be determined by means of preliminary tests. For this purpose, a suitable quantity is first decanted and mixed separately, to be prepared in case of failures.

#### Trainini TV Episode 19 available:

In October, we reported on the Märklintage in Göppingen and the associated Z gauge convention in southern Germany. We also announced an accompanying episode on **Trainini TV**.



Episode 19 was released a few days before this issue was published and can now be seen on our channel on YouTube: <https://www.youtube.com/TraininiTV>.

#### The Micro-Trains new products:

Micro-Trains (<https://www.micro-trains.com>) would like to start and end November with four product deliveries. The series begins with wagon number 9 from the "War of the Worlds" series of novels (item no. 525 00 183). This olive-green flat car is loaded with a load showing debris from Mars spaceships that have been shot down.



This is what this year's Micro Mouse Christmas trolley looks like (item no. 507 00 740). Photo: Micro-Trains

This year's Christmas car (507 00 740) with a new Micro Mouse motif also appears to be indispensable.

Two three-car sets of "Operation Toy Train" (994 01 272 / 983 01 272) also support the work of the Toys for Tots Foundation with their sales proceeds.

The cars are identical, but packaged differently, which also explains the difference in price.

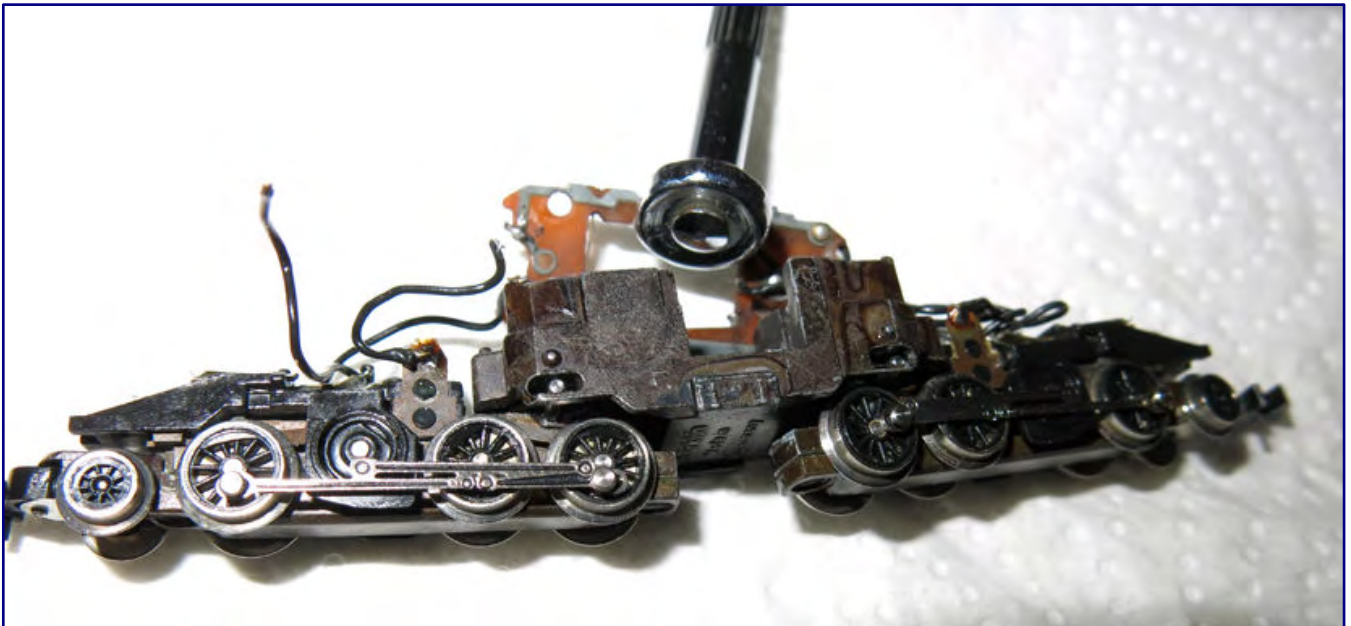


## Neighbourhood assistance on Z gauge:

An attentive **Trainini**® reader would like to offer the community private help as a Z gauge doctor. Daniel Probst can provide the following help for all locomotives with carbon brush motors (three-/five-pole):

- repair,
- maintenance,
- hardened oil removal and cleaning.

This does not apply to models with a bell-type armature motor. His main focus is on models that have been standing around for years, no longer run, or run poorly, and now need to be put back into operation.



Repairs, such as here the replacement of rotten and tearing strands on a Märklin crocodile, are gladly carried out as part of private neighbourhood help. Photo: Daniel Probst

He has access to an extensive stock of parts or procures the necessary spare parts. Zetties who wish to make use of his private assistance must reimburse him for any costs incurred, including postage, at cost price or enclose (German) stamps (or international reply coupons).

It should be clear to anyone interested that such an offer has limits and is not intended to replace commercial offers. If you have any of the problems described, please contact the following e-mail address in confidence: [spur-z-nachbarschaftshilfe@web.de](mailto:spur-z-nachbarschaftshilfe@web.de).

## Märklin-Insider Model 2024:

Märklin presented the 2024 Insider Club models at the trade fair in Friedrichshafen. The DB VT 889 rail bus from Wismar (item no. 88820) is to be released in Z gauge. The "little pig snout" therefore has a purple-red paint scheme with decorative lines and DB lettering in light ivory.

Although the prototype was already taken out of service in 1951, it was probably still used by the young Bundesbahn in view of the great shortage of vehicles in the post-war years – unfortunately, no photo evidence of this is known.

The model is a completely new construction with a finely detailed design. The chassis and superstructure are made of metal. Many add-on parts such as the side access ladder and the roof rack ensure a good overall appearance.



The Wismar rail bus VT 88 902 of the German Federal Railway (item no. 88820) is to feature fine details and an interior. The club members who did not have the chance to acquire a comparable vehicle from Z-Modellbau at the time will be particularly pleased. Illustration: Märklin

The interior fittings and the driver's cab are also to be modelled. A plank replica is reproduced at both ends of the vehicle as impact protection; the model does not have couplers.

This new product is powered by a bell-type armature motor on both axles. The headlights and marker lights (3 x warm white / 2 x red) and interior lighting are dependent on the direction of travel and use LEDs.

#### Great new products from Trafofuchs:

Some of the following new items were already on display in the showcase at the Märklintage, but only now have they found their way onto the webpages and thus into sales. In memory of last summer, nudists are looking for an all over tan in the "Sunbathing on deckchairs" set (Art.-Nr. AS02).

The figures from the "Out and about with children" pack (W10), which could be out and about at a Christmas market, for example, provide a glimpse of the coming winter.





The “Sunbathing on deckchairs” (item no. AS02; photo above) is reminiscent of last summer, while “On the road with children” (W10; photo below) is more about the current weather. Photos: Trafofuchs

The railway enthusiasts have been joined by a cameraman with a tripod (BA02). The “three camels” (WN09) provide mounts for the Three Wise Men and thus enrich the in-house nativity scene programme.

The list of unlit vehicles is rounded off by a jeep with forester and dog (KL19). These new models can be viewed and ordered on the manufacturer's website at <http://www.trafofuchs.de>.

#### **Euromodelbouw in Genk:**

Unfortunately, we did not receive any information from the organiser or any other indication, which is why we have not highlighted the following event in the reports of the previous monthly issues.

However, the Euromodelbouw 2023 in Genk (Belgium) was definitely worth a visit. On 25 and 26 November 2023, from 10:00 to 18:00, Z gauge was also represented in the Limburghal. Two exhibitors were listed in the directory, including Z-Stammtisch Rhein-Ruhr. Among other things, they presented a modular layout covering an area of 11 x 3 metres.

However, experience has shown that new and used items are also easy to buy at Belgium's largest modelling exhibition. Other modelling sectors and offers for children were also represented on site. The exhibition website provided all the information required for a visit, also in English: <http://www.euromodelbouw.be>.



#### Rather quiet month at AZL:

November was rather quiet for American Z Line, as some of the new items were delayed. The only locomotives are two EMD E8s from Burlington (CB&Q), one A and one B unit, in the special colours for the California Zephyr (item no. 62619-1 / -2). An eleven-part car set for this (72102) is already being released for the third time by AZL.



The heavy semi-baggage car in Pullman green paint scheme (74018-1 / -2) was used by the same railway company. Then there is the goods train escort car with a wide-view cabin and lettering for the Chessie system on the C&O side (item no. 921030-1 / -2).

EMD E8A of the Burlington (CB&Q) for the California Zephyr (item no. 62619-1; photo above) and C&O goods train escort car for the Chessie system (921030-1; photo below). Photos: AZL / Ztrack

The green livery of the R-70-20 refrigerated wagons may seem unusual when the keyword "Western Fruit Express" is mentioned. This is the company colour of the BNFE, which can be found in a set of two (914844-1) and a set of four (904814-1).

The 53-foot Articulated Spine Cars (905200-1) follow in five parts in yellow livery with the old TTAX logo now with trailers from UPS. These can again be purchased individually as a double pack (954000-1). Manufacturer photos of all new items can be found at <https://www.americanzline.com>.

#### Product variant at Yellow Dwarf:

Beehives were among the first items that the Czech small series supplier Yellow Dwarf added to its Z gauge programme (<https://www.yellowdwarf.eu>). The beehives II (item no. 60302) represent a new variant of such an insect home.

This is a roofed shelter with floors in which the beehives known from earlier times are set up, which were typical of eras I to III and were still occasionally found in era IV.

Another good idea are food and drink vending machines (60051), which can be found in many places today, but have so far only been realised on a small scale. There are also three new products for decorating building sites: Pipelines / (60227) and II (60228), as well as manholes (60229).

#### GMmodell Torino with its own pages:

GMmodell Torino has been found, at least in Germany. For direct deliveries and supplying other EU countries, however, a separate website is required, which can also be used to distribute information.



Beehives II (item no. 60302; photo above) and food and drink vending machines (60051; photo below). Photos: Yellow Dwarf



The Turin-based duo is now proud to announce that this milestone has been reached. Their own website is now available at <https://www.gmmodelli.com>.

**Unexpected new product at Azar Models:**

Azar Models from France is increasingly developing into a full-range supplier, because a new product that we did not expect is opening a new chapter here. Available now is a pulse current speed controller called "Zmaster" (Art.-Nr. S007).

This is a high-performance speed controller system for operating analogue Z gauge locomotives. The device uses "pulsed direct current technology," known to most model railway enthusiasts as pulse width modulation. This enables smoother starting and fine control over the entire speed range.



The Zmaster is a new PWM speed controller from France for Z gauge. Photo: Azar Models

The special feature of this new product is that there is a choice of two modulation frequencies. The high frequency can be used to control modern locomotives from its own production, while the lower frequency is suitable for older generation locomotives with three or five-pole motors.

Further information was already available on the manufacturer's website at the time of going to press, and online translation aids can be consulted, if required: <https://azar-models.com>. In addition, a short film showing the creation of the models can be viewed on the manufacturer's film channel: [https://youtu.be/FQW\\_exfD\\_OA](https://youtu.be/FQW_exfD_OA).

## Darmstadt's wedding tower:

Klaus Schultheis is the name of the modeller who created the model of the Darmstadt Wedding Tower. Although not realised in Z gauge, the striking and historic building on a scale of 1:350 is certainly suitable as a background model that will also catch one's eye from a distance.

The history of the template for the polyurethane model, which is available in various versions, is described on a separate page, which also lists the illustrations and article numbers as well as a source of supply: <http://www.meine-kleine-stadt.com/hochzeitsturm-darmstadt.html>.

## Märklin deliveries in November:

The Christmas business and, therefore, the peak season for model railways is underway. This is also reflected in Märklin's deliveries since the publication of the last issue. Traditionally, the manufacturer puts its new annual catalogue into circulation just in time for the start of the season. This is also the case this time, with the surprise locomotive 18 201 with a second tender gracing the front cover.

The rolling roof car set (item no. 86682) with three Tams 886 type cars has been delivered in its entirety. The class 50 steam locomotive (88847) with a Christmas design on Witte wind deflectors and tender as well as the Christmas car (80633) have also been delivered in their entirety.



The class 78 (item no. 88086) was the first Mini-Club steam locomotive to also have red marker lights so that it could pull and push this five-car push-pull train with "Hasenkasten" (87074). The steam locomotive also received new lighting, which, in addition to a warm white three-light headlight, now also shows red tail lights on both sides for the first time. It has two pairs of converted three-axle coaches and a "rabbit box" attached, which also has an LED light change. Compared to the first delivery tested in our magazine, the frame in the cab area of this model has now also been painted black in line with the prototype

Something special, offered as MHI one-off series, is the DB 78 245 tank steam locomotive (88068) with push-pull capability and the associated "push-pull train" car set (87074). They are labelled for Era III and reproduce a pushed train as it was used in the BD Essen until the beginning of the 1960s.

The steam locomotive also received new lighting, which, in addition to a warm white three-light headlight, can now also show red tail lights on both sides for the first time. It has two pairs of converted three-axle coaches and a "rabbit box" attached, which also has an LED light change. Compared to the first delivery tested in our magazine, the frame in the cab area of this model has now also been painted black in line with the prototype.

There is also a new design to report, which was delivered in duplicate: Friends of the modern, strike- and weather-stricken railway can look forward to the Sgns 691 carrying wagon, which reached the dealers in





The Märklin magazine car 2023 (item no. 80833) is a new design. The type Sgns 691 depicted with it was also delivered a few days later in the form of a DB-AG car set. Photo: Ralf Junius

blue livery with two containers as Märklin magazine wagon 2023 (80833), and as a set of three wagons (82640). We plan to present the traffic red examples of this set from the Deutsche Bahn AG stock in detail in December.

#### Exceptional models at Full Throttle:

WDW Full Throttle (<http://www.wdwfullthrottle.com>) announces a special new product in November. In response to popular demand, it is offering four double packs of coke wagons. The models were created many years ago and were completed with matching bogies and now supplemented with coke load inserts.



The coke wagons (item no. FT2500) were created many years ago and are now supplied by Full Throttle with a matching load. Photo: WDW Full Throttle

The new product (item no. FT-2500) is a replica of a factory-owned, 33-foot Extended-Top Rib-Side Coke Hoppers. The grey-painted models only have a three-digit operating number and are perfect for equipping a coking plant based on the US model.

**Advent season at Klingenhöfer Miniaturen:**

Klingenhöfer Miniaturen (<https://www.klingenhoefer.com>) is starting Advent 2023 with a new, larger range of unpainted figures: Three chickens or storks and two snowmen are being offered at a particularly affordable price.

For Christmas themes, the manufacturer also offers a Christmas sleigh, a nativity scene set, and, separately, nativity figures.



Nativity figures and the Christmas sleigh are offered for self-painting – presented here as a design suggestion. Photos: Klingenhöfer Miniaturen

The five owls, a Cape buffalo, and two eagles with outstretched wings are already finished.

**Märklin Christmas special 2023:**

Once again, this year, Märklin has produced a new promotional film for the Christmas season and prepared a campaign page (<https://www.maerklin.de/de/weihnachten/esistsoweit>) for acquiring new customers.



This time the theme is the much-quoted Märklin family, i.e., the community of model railway enthusiasts beyond their own circle of relatives. Interested parties can find discounted starter packs, contact details and the nearest specialist retailer on the campaign pages.

If you would like to watch the film, you can also access it from this page and then share it as a link from the YouTube platform if you like it.

#### **The world's smallest Christmas tree:**

The smallest Christmas tree in the world will be on display again this year from 27 November 2023 in the shop window of Galerie Lamers, Kleppingstraße 8 in Dortmund. It can then be viewed and photographed around the clock until 28 December 2023.

With a height of just 14 mm (without top ornament), it is still a valid and externally recognised world record for artificial Christmas trees after 16 years.

Its special feature, which makes it so unique and is a prerequisite for achieving the world's best performance, is its special concept: tree, lighting, and decoration must not form a single unit.

It can therefore be decorated and redecorated at any time. In 2012, this was also demonstrated as part of a public campaign in the Westfalenhallen Dortmund to give it more lustre with new gold jewellery.



Last year, editor-in-chief Holger Späing (left) had the honour of presenting the world record in the Christmas edition of the Dortmund Show. Photo: Markus Bauer

There are plans to expand his shop window presence with a small model railway layout, also at the explicit request of the gallery owner. However, it is not intended to “move into” it, but to have such a showpiece at its side.

#### **NoBa-Modelle is back:**

After the end of the summer break, the duo from NoBa-Modelle have started new developments and are already presenting their first results. They are focussing exclusively on traffic on rails. First of all, the superstructure for an E 040 high-sided wagon (item no. 5328R) should be mentioned here.

This is the open car that was once put into service as the Omm 55. The part is intended for replacement on the running gear of the Märklin item 8622 and is therefore a little too long. However, this offers the option of upgrading a standard chassis from the 1972 launch programme that can be obtained at a reasonable price.

Another series of new products is dedicated to BLS vehicle transport, known as “auto transport” among the Swiss. Residual impressions of drive-on (5325R), ramp (5326R) and centre wagons (with 5323R)



The centre wagon without ramp plate (item no. 5324R), here the fully painted and labelled manufacturer's sample, is part of the BLS car transport system. Photo: NoBa-Modelle

and without ramp plate (5324R)) are offered here. The unit is also available as a complete kit (5327R) or finished model (5327RF).

At the time of going to press, the Bundesbahn class 624/924 diesel multiple unit has only just been presented, which is why we can only show it here as a CAD volume model. The three-part unit is to be included in the range as a blank with chassis and drive (5230R) or as a complete finished model in its original paintwork (5230 RF).

The chassis and housing are made from resin using 3D printing and powered by modified parts from Rokuhan-Shortys. The technical basis of the bogies also comes from this platform and thus offers a possible power consumption from all axles.

#### **Trainini Photo Calendar 2024 with a delay:**

You will soon be able to print out the new **Trainini Photo Calendar 2024** in DIN A3 landscape format, which can then be accessed via our website. You can find it via the selection sequence homepage → Magazine → Photo calendar via the portal webpage.

We were a little behind schedule because we were still working on two episodes of **Trainini TV** at the same time, and the construction of the photo diorama for the cover picture also took some time. But for us, as I'm sure for you too, quality comes before deadlines.

We have carefully selected 13 different motifs in order to present a wide variety and the most representative cross-section possible of Z gauge in all its different facets. We have collected a wide variety of eras, railway administrations, seasons, types of traction and motifs in order to fulfil this requirement.



We very much hope that you will also like the **Trainini Photo Calendar 2024** and that it will adorn many walls in the coming year, thereby promoting our nominal size. Shortly after the German version, the English Trainini Photo Calendar 2024 will also follow on our EU domain, in which the US holidays are taken into account.



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Board of Editors:  
Holger Späing (Editor-in-Chief)  
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Dirk Kuhlmann  
Joachim Ritter

Correspondent North America:  
Robert J. Kluz

English translation:  
Alexander Hock, Christoph Maier, Oleksiy Mark, Martin Stercken

Further voluntary work: Stephan Fuchs, Ralf Junius (**Trainini TV**), Torsten Schubert

Licensed **Trainini Discussion Group** (<https://www.facebook.com/groups/1597746057122056/>): Michael Etz (**Trainini Locomotive Doctor**)

Publisher and Responsible in terms of German press law (V.i.S.d.P.) is Holger Späing, Am Rondell 119, 44319 Dortmund; Contact: 49 (0)231 9598 7867 or by e-mail to [redaktion\[at\]trainini.de](mailto:redaktion[at]trainini.de).

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# Trainini

German Magazine for Z Gauge



Long Trains in Friedrichshafen

Modern Container Transport Cars  
New Semi-Relief Houses



## Introduction

Dear Readers,

Another year is coming to an end, and I'm not the only one wondering where it has gone. These twelve months without the restrictions of a pandemic have brought back what we have all come to love and given us new experiences.

There is an unmistakable urge among model railway modellers to visit exhibitions and trade fairs again. Having already reported on a very successful event in November, this last edition of 2023 contains reports on two more.

We will continue in January with a fourth about the Advent meeting in Zell (Mosel), which only recently took place, and whose impressions are still fresh. Together, they should provide an overview for all readers who were unable to be there in person, especially as they were very far apart.

However, it is also our task to network our scene there and to facilitate an exchange between the groups, associations, and exhibitors. After all, we see ourselves as a small but strong community that draws attention to itself in a harmonised and coordinated way to jointly promote our cause.

As I write these lines, Christmas Eve is fast approaching. I'm looking forward to some contemplative days and hopefully some time to tinker and drive trains. When that time comes, the Christmas business will be over, and the modelling season will be in full swing.

This edition also has something to offer for this: Ralf Junius has test-assembled the new half-relief houses from the Modellbahn Union. Using one of them as an example, he demonstrates how an attractive and useful building for the transition to the backdrop can be created from hard cardboard sheets.

Incidentally, Ralf Junius has taken on a new role in our team. I won't give too much away now, as you can read about this in the news at the end of this edition, as well as in the news on our portal page.

Our long-standing reader Jochen Brüggemann also does the honours. After his reports in the spring, readers also asked how he concealed the turnout drives on his layout. In response to this enquiry, he has prepared another construction report for us, which we are now publishing here.

We also want to mention another important topic in this edition. Just a few weeks ago, Märklin delivered a KLV container transport cars pack containing three new Sgns 691 type container transport cars. They are an important type of wagon for freight traffic today, which is why we have taken a very close look at them.

You can look forward to it! On behalf of my editorial and translator colleagues, it only remains for me to wish you a Merry Christmas and a good start to the New Year!

Sin-Z-erely,

Holger Späing



**Holger Späing**  
Editor-in-chief

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We thank Jochen Brüggemann for his contribution und Eisenbahnstiftung for photo support.

Date of publication of the German language version of this issue: 23 December 2023

### Cover photo:

There were a lot of model railways to see recently at Bodensee (Lake Constance) and in Duisburg in the Rhineland. At Faszination Modellbau in Friedrichshafen, Z gauge was particularly impressive with endlessly long freight trains of sometimes 100 wagons on the US layout on display there. Photo: Stephan Fuchs



## Container transport cars from Märklin **Just OK is not good enough**

*After the recently distributed Märklin 2023 magazine car, Märklin is also launching a prototype-orientated version of the Sgns 691 KLV container transport car for Era VI. In the first impression and many test steps, the cars prove to be very well done. Unfortunately, however, again it appears that they are not without flaws, which could have been avoided.*

The model for the Märklin container transport cars pack (item no. 82640) delivered just a few weeks ago are the type Sgns 691 wagons. As models, they are a new design, announced in February 2023, and therefore receive a detailed review today.

But first, let's take a brief look at the models that Märklin has produced here. In the mid-1980s, there was some movement in container transport when the length of the containers could be increased from 7.15 m to 7.82 m.

In order to be able to transport these containers as well, the Bundesbahn needed new freight wagons. In 1987, it put ten Sgns 694 prototypes into service. They were DB's first 60-foot container transport cars. In 1994, the Ahaus-Alstätter Eisenbahn (AAE) procured a series of these railcars.



The Greece returnee 221 135-7 of the Bocholter Eisenbahn Gesellschaft is in route with a container train in Düsseldorf-Rath on 29 May 2010. It has state-of-the-art container transport cars on the hook that are at least like the model chosen by Märklin. Photo: Wolfgang Bügel, Railway Foundation

Deutsche Bahn AG had ordered similar wagons from Fervet in Bergamo (Italy), but their delivery was delayed. They were delivered between 1996 and 1998 in a total quantity of 500 units by Costamasnaga and Fervet as UIC standard container wagons.

Like the older Sgns 694, they have fish-bellied outer longitudinal beams, which are connected by the end pieces and welded-in main cross beams and cross struts. The DB has classified these wagons as Sgns

691 and has them managed by Kombiwaggon. A fairly significant number of the Sgns 691, between 50 and 90 units on average over several years, are leased on a long-term basis, some of them to BASF.

These wagons are intended for the transport of various containers, swap bodies and trailers of different lengths. As the loading length of this standard design is 60 feet, two or even three 20-foot, two 30-foot or even a single 40-foot container can be transported on the wagon if each individual container does not exceed a weight of 36 tonnes.

The load is secured using 28 hinged container support pins. Some of the wagons are equipped with a locking brake that is operated from the ground. The yellow-painted hand wheel is located on the fish belly beams, so there is no need to climb onto a brakeman's platform.



189 092-0 (ES 64 F4-992 from MRCE) passes through Dinslaken station on 6 April 2011 with a container train from Dortmund to Rotterdam. In second place behind the locomotive is an Sgns type wagon, presumably from the AAE fleet. Photo: Joachim Bügel, Eisenbahnstiftung

In 2007, the stock grew to 504 units. The reason for this was not a reproduction by the manufacturers, but the conversion of the remaining four Sgns 694 wagons. In 2007, Deutsche Bahn AG replaced their Olea hydraulic buffers with sleeve buffers and then assigned them to the Sgns 691. The decommissioning of type Sgns 691 began in 2009 with 45 wagons after only 13 years of service. However, wagons of this type are still in service today.

A special feature of the Sgns 691 is its paintwork: at the time of its procurement, Deutsche Bahn had introduced RAL 3020 traffic red as the new corporate colour. It had already been announced in 1995 that freight wagons would appear in the corporate colour red in future.

This was intended to promote a self-confident appearance and express activity, strength, and expertise. Deutsche Bahn AG also wanted to clearly distinguish itself on the tracks from other railway companies and large state railways.



From the outside, it appears that all Sgns 691 were delivered in traffic red, but this was not done consistently according to the new colour concept. Indeed, even on the last coaches delivered in 1998, only the outer side panels were painted traffic red. The rest of the underframes were painted red brown to RAL 8012, which had long since been discarded at the start of production. We should make good note of this fact for the following model discussion.

## The model implementation

The set of wagons to be discussed today (82640) consists of three KLV type Sgns 691 container transport cars. They are part of the Deutsche Bahn AG (DB AG) stock and have Era VI markings. The structure of their twelve-digit fleet number and the dispatcher's sign therefore follow the NVR (National Vehicle Register at the Federal Railway Authority).

The first of the three container transport cars in the set is unloaded, one of the other two is loaded with a 40-foot container from Maersk and the other with two 20-foot containers from Maersk and ONE respectively.



This is how the wagon set (item no. 82640) with the new shape Sgns 691 container transport cars and their load as designed by Märklin appears.

These are new designs, which is why we are assessing them in such detail here. In contrast to the previously produced carrier wagons, the wagon bodies are not made of die-cast zinc, but of metal-filled plastic.

There are probably economic reasons for this, as the higher working pressures in the zinc casting process also require higher investment than those in injection moulds for plastic. In our opinion, this is Märklin's response to the fact that although modern types of wagons are urgently required in the programme, they are nowhere near as much in demand today as models for eras III / IV.

Another advantage of the selected production process is that the moulds and details of the wagon body are very fine. This is particularly beneficial for the cross girders, which are easily recognisable from above when the wagons are unloaded. The pulling hooks on the buffer beam have also been reproduced to some extent.

However, the folding shunting handles on the wagon heads have not been reproduced, which is forgivable, as these would be in great risk of breaking off, if reproduced to scale. The UIC hooks are also missing on these models, although their supports are included on the side walls.



On the upper edge of the long side, fourteen positions for the placement trunnions can be recognised, which together with the opposite side make up the prototypical number of 28. Instead of trunnions, however, we find holes on the loading area to accommodate the ISO containers.

Earlier models showed complete replicas here, which are actually recognisable due to the yellow colouring.

The 28 prototype's very distinctive and typical feature, the 28 trunnions (pegs), have been included in such a way that their positions are visible from the side, i.e., this part has been replicated. In the top view, however, instead of the trunnions we find drilled holes for inserting the model containers.

#### Dimensions and data for the Deutsche Bahn AG Sgns 691:

|                                      | Prototype                       | 1:220   | Model   | Variance |
|--------------------------------------|---------------------------------|---------|---------|----------|
| Length over buffers (LüP)            | 19.740 mm                       | 89,7 mm | 89,7 mm | 0 %      |
| Load length                          | 18.500 mm                       | 84,1 mm | 84,6 mm | + 0,6 %  |
| Width                                | 2.930 mm                        | 13,3 mm | 13,6 mm | + 2,3 %  |
| Load level height above rail height* | 1.155 mm                        | 5,3 mm  | 6,6 mm* | + 24,5 % |
| Overall centre distance              | 16.000 mm                       | 72,7 mm | 70,3 mm | - 3,3 %  |
| Pivot pin distance                   | 14.200 mm.                      | 64,5 mm | 62,3 mm | - 3,4 %  |
| Bogie centre distance                | 1.800 mm                        | 8,2 mm  | 8,2 mm  | 0 %      |
| Wheel diameter                       | 920 mm                          | 4,2 mm  | 4,5 mm  | + 7,1 %  |
| Dead weight                          | 19.900 kg                       | ---     | 8,8 g   |          |
| Permissible maximum speed            | 120 km/h                        |         |         |          |
| Loading mass                         | 70 t                            |         |         |          |
| Bogies                               | Y25 Lss (Bauart 626)            |         |         |          |
| Manufacturer                         | Costamasnaga, Fervet            |         |         |          |
| Years of construction                | 1996 - 1998                     |         |         |          |
| Quantity                             | 500 units (+ 4 from conversion) |         |         |          |

\* Dimension without manoeuvring handles

This takes account of the fact that almost all manufacturers provide their cargo containers with pins instead of holes to make them look more prototypical when stacked on the quay. The matching freight cars must then adapt in order to be able to reproduce all the loading variations of the prototype, as here.

When we pick up the new models, due to the material, they seem lightweight compared to other wagons of a similar design. However, they run surprisingly well, loaded, and unloaded, and the micro scales also reveals no unpleasant surprises. Consequently, no derailments were detected in the test.

The most important model dimensions are well adhered to, which is why the new moulded models also appear coherent to the eye. The documented outliers, which cannot be explained by measurement tolerances, are due to the excessively large wheel diameters and the model construction of the bogies. These also lead to a lower bogie pivot distance and overall wheelbase. The percentage deviation distorts the view here and makes the difference appear far too large.

A descriptive note should be added to the bogies: Märklin uses its inventory parts for the French Y 25 bogie for this model. The individual variants of this design (see also the designation extension in the





Märklin's moulding tool inventory only includes a single Y25 bogie. The differences between the individual types based on this can be neglected for the Z gauge. More noticeable is the bogie pivot spacing, which differs by about 2 mm from the converted prototype dimensions.

dimension box) with, for example, different directions of rotation of the springs are generally neglected in the 1:220 scale.

What we don't like so much is that all three wagons are equipped with a floor-operated locking brake. In the prototype, this is only a part of the total stock, and we have not yet been able to determine the number of these.

A little variety would certainly do us good in this regard, as this is a phenomenon that reminds us of the decades-long practice of equipping almost every Era II model with a brakeman's cab. When compared with prototype photos, this may no longer create a coherent impression.

## Colour and labelling

Under this subtitle, we recognise great strengths, but unfortunately also weaknesses. The present models are neatly and completely painted in RAL 3020 traffic red, but as we have pointed out in the prototype section, this does not correspond to the original.

Correctly, only the outer longitudinal beams should have this colour, all other parts in between should be red brown (RAL 8012). This is a well-known and typical characteristic of the originals, which is adequately described in the literature.

As this deviation could easily be taken care of with a painting stencil, this is an easily avoidable and therefore, in our opinion, more serious prototype error: Besides a few minor differences in detail, what sets this wagon apart from similar design types and makes it so individual has been reprehensibly neglected.

The only way to remedy this is to rework it yourself, especially as no labelled parts are affected, or to fully load all wagons to conceal this. But, then, unfortunately nothing of the fine support structure will be visible anymore.

Incidentally, the buffers are also fully included in the paintwork, which leaves us with the impression that savings were made here in almost every corner without keeping an eye on the effect. However, red buffer discs and shafts are also immediately noticeable to model railway fans without much knowledge of the prototype.



The selected manufacturing technology allows a filigree reproduction of the cross-member structure of this wagon type (photo above), including its underside (photo below). However, the traffic red paintwork of the cross members and the wagon floor is completely unrealistic. Red buffers (see also photo on page 8) spoil the prototypical effect, even for less experienced buyers.

In our opinion, Märklin should scrutinise and reconsider its model policy in this regard, especially as this set of railway cars is not set at a conspicuously high price. There is nothing to criticise about the quality of the paintwork: It is thin, has good film strength and is applied opaquely, which suits the engraved details.

The gloss level of the new models is somewhat higher this time, than with many other recent deliveries. However, we would have liked a slightly more matt finish on a freight wagon in particular, as their originals receive virtually no care or external cleaning.

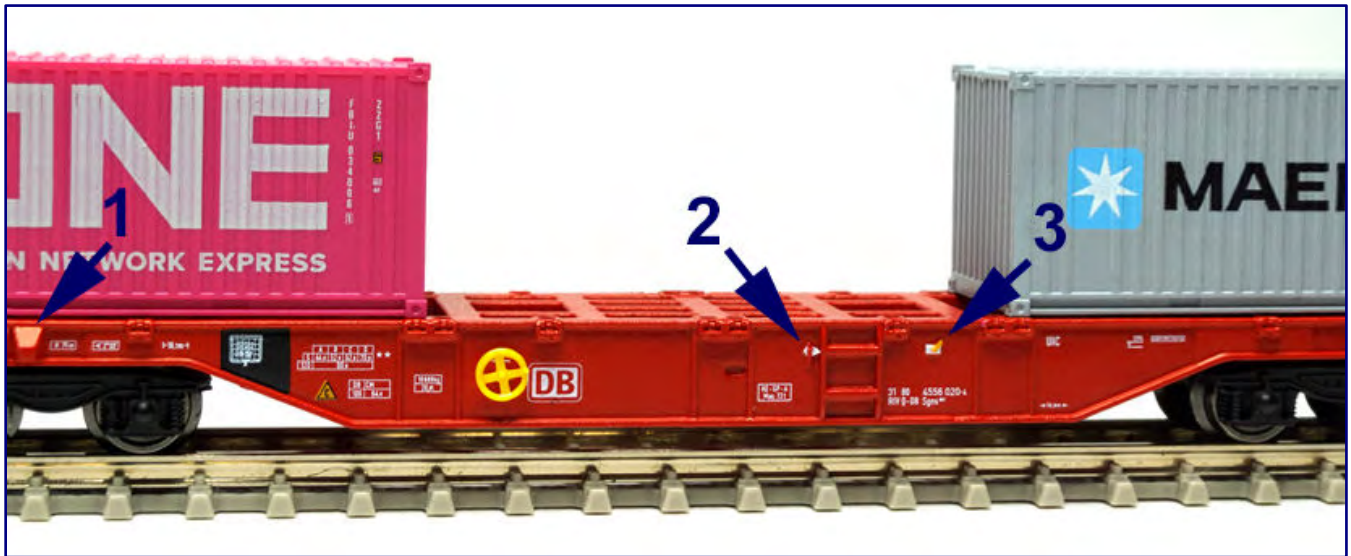
It is very commendable that the changeover levers of the brake system are individually coloured, both the lever and the cover have been considered here! Without research, we can't remember when this has ever been the case with Märklin in this way.

The service markings on the three cars are printed in several colours, cleanly and largely unblemished. Filled labelling boxes are only reproduced by printing, as is generally the case with Märklin. Only pad printing was used on these models.

The containers are also neatly labelled with individual numbers. Märklin has selected the prototypes with the road numbers 31 80 455 6 023-8, 31 80 455 6 030-3, and 31 80 455 6 020-4 for the container transport cars.

As such, they all originate from the Costamasnaga (Sicily) construction lot. They are among the first examples of their type to be delivered. We suspect that this batch in particular is well documented in the





The UIC cable anchors are missing at the point marked with arrow 1, only the base has been reproduced. The applied markings, on the other hand, prove Märklin's high standard, here additionally emphasised by the two-coloured brake system changeover levers (arrows 2 and 3).

Märklin archives and that it is probably true that all three prototypes have a locking brake. This could have contributed to the phenomenon described.

The inspection inscriptions that we were able to decipher date from the years 2000 to 2002, so there is no doubt that the prototypes were still running in their original colour, as they had not yet undergone any regular maintenance.

The black nickel-plated and externally profiled disc wheels that were used correspond to the standard that the company has maintained for years. The conspicuously large distance between the couplers on the models, which is particularly noticeable when they stand next to a passenger train from a more recent delivery, is not very appealing. The long coupling was used, which also guides them safely through the industrial radius R1 (145 mm).

We openly question this everlasting downward compatibility, if the customer cannot be offered a simple exchange, as is the case with Rokuhan. After all, the same applies to these state-of-the-art and therefore long freight wagons as to UIC type X passenger coaches: their appearance is simply terrible when the radii are too small.

## Overall impression and layout operation

We think it is a good idea, particularly in the case of a wagon with visible and uncovered girders, to deliver only two out of three loaded wagons and not to occupy all the stands straight away. This corresponds to the frequently encountered image of container trains from the seaports to inland locations. However, the models should also be painted in the correct colour.

The typical application for the new product are contemporary block trains. Two-axle container transport cars are no longer regularly seen in these trains. Two-car versions with Jacob's bogies are still not offered by the manufacturers.

Consequently, they can be combined with examples from other packs, if such an additional purchase is desired, or with the other four-axle container transport cars of older designs that can be found from time

to time in the Märklin programme. Märklin will certainly follow up with further editions of this type, which will be put together somewhat differently.

Märklin mentions the “Ludmilla” with item number 88136 as a suitable locomotive in its product description; this is a Deutsche Bahn AG diesel locomotive in the original colours of the eastern German Deutsche Reichsbahn.

We consider this not to be appropriate in terms of timing and not a success. Later traffic red versions with NVR numbers, on the other hand, pose no problem. Likewise, not only in traffic red does the still young “Vectron” prove to be a perfectly suitable locomotive, because the majority of its prototypes are used in freight transport today.



All in all, the delicate realisation (photo above), safe operation and the good side view (photo below) speak in favour of the new model. The negligence in the colour scheme, the excessive distance between the couplers of these cars, and the unimaginative equipping of all the three examples with a locking brake, recognisable by the yellow handbrake wheel, deserve criticism.

In the period of the revision addresses, the 139 / 140 series, including the “Zebras” from Locomotion, or the 151 series could also be considered as an alternative, because looking back twenty years ago, these machines were still commonplace.

Let us now briefly summarise our impressions: Märklin has shown the courage to finally expand its programme with modern KLV container transport cars that constructively bypasses the requirements for a double wagon with Jacob's bogie. Fans of Era VI have been sorely lacking such cars until now.



In terms of dimensions and details, the realisation is almost perfect, and we consider the few compromises to be acceptable. This good impression is only spoiled by the serious flaws in the prototypical colour scheme, which we are not prepared to accept as an economic compromise.

Prototype freaks will not be able to avoid doing this themselves, especially as the red coloured buffers look terrible. If you want to go this route, you should also consider matting and patinating, as prototype photos regularly show the usual poor appearance of Deutsche Bahn property.



On the layout, the new model presented here enhances the container traffic of the present day, as until now no supplier has had modern KLV container transport cars in its programme, which characterise the trains in the heartland of Central Europe.

Considering that the painting errors can be corrected in future releases and the very successful design, the KLV container transport cars pack Sgns 691 (82640) is nominated for the best new release of the year 2023 in the wagon category.

Base model manufacturer:  
<https://www.maerklin.de>

## Camouflage instead of burying **Hidden from View**

*In response to the signal box construction report in Trainini® 1/2023, a reader asked about the landscap-ing around the signal box. This reader was interested in the camouflaging of the point machines, which could be seen in two photos. Because of the expected interest, author Jochen Brüggemann explains his method in detail today.*

By Jochen Brüggemann. On the cover of **Trainini®** 1/2023 and in the signal box construction report of the same issue (from page 28), two photos also show sections of the track systems with Märklin turnouts, whose drive boxes are only noticeable at second glance due to their camouflage.



The point machines on Jochen Brüggemann's layout are almost invisible to the observer once they have been treated as described here. The dummy switch lanterns also contribute to the good overall appearance.

When I built my first Z gauge layout in the 1980s (see **Trainini®** 10/2013), I found the large point machines annoying, and subsequently I camouflaged them with simple means on turnouts that had already been installed. Over time, however, I was no longer really convinced by this state of affairs.

Nevertheless, when I started building my current layout (more than 20 years ago), I again decided in favour of Märklin track and turnouts because, as far as I know, only one manufacturer had a complete track system including flex track in its range at that time – Märklin.





The drive of this curved turnout also largely disappears from the viewer's perception, as the deeper perspective shows.

In the meantime, I had also gathered enough ideas and experience to embed them in mineral ballast in a prototypical manner and to camouflage the drive boxes of the turnouts so that they hardly disturbed the overall impression.

When designing my old layout, camouflaging the point machines was made more difficult by the fact that the points had already been installed and were only partially accessible, in places. Due to the difficulties at that time, I decided to prepare the turnouts on the table before installing them when preparing the current layout to make the work easier.

With all these measures, I had to keep a constant eye on the fact that, due to the clearance gauge of the locomotives and carriages, I was only allowed to change the outer dimensions of the switch boxes slightly. Otherwise, I would have risked them coming into contact with the applied camouflage material and getting stuck or derailed as a result.

As with my other handicrafts, I relied on my tried and tested household remedies:

- fine pliers, files and tweezers,
- a pencil block with a fine drill bit (0,5 mm),
- a scalpel with different interchangeable blades,
- sandpaper (220 grain or finer),
- a disposable injection syringe (2 ml) with thin cannula,
- fine, soft brushes (size 0 and 1), and
- very sharp steel pins (diameter 0.7 mm, length approx. 40 mm, of which the tip is approx. 5 mm long; from laboratory supplies).

I used products from Rainershagener Naturals to ballast the tracks and camouflage the drive boxes, which, unfortunately, are no longer commercially available as far as I know.

**Info box**  
The ballast used by our reader was used a few years ago and is no longer available.  
A possible alternative are the various mixtures, including those with a rust patina, from the supplier Koemo (<https://www.koemo.de>), which have also been used frequently in our projects.

These were the products “N track ballast, heavily aged” (item no. 7256D) and “N + Z code 40-55 small ballast, heavily aged” (7253D). The track ballast and small ballast match each other perfectly in terms of colour, although the small ballast is much finer.

The glue used was white glue, sometimes undiluted, but mostly diluted with water (approx. 1:3 to 1:4, with a little washing-up liquid added). I used matt-drying acrylic paints (water-based) for the colour work. For the paper work, I chose very thin paper, so-called airmail paper (usually light blue ex-works, but this doesn't bother me).

### Attempts to build up experience

I spread a patch of ballast and a patch of small ballast on a piece of cardboard and fixed both with a few drops of the white glue/water mixture. After drying, a second fixing process was carried out, analogue to the later ballasting of the track.

After drying (and darkening due to the white glue), the ballast and the small section now showed the expected colour tone, after which I created a suitable brown acrylic paint mixture for the point machine boxes. The ballast colour also helped with the selection and mixing of the similar rail profile paint (also acrylic paint, water-based).

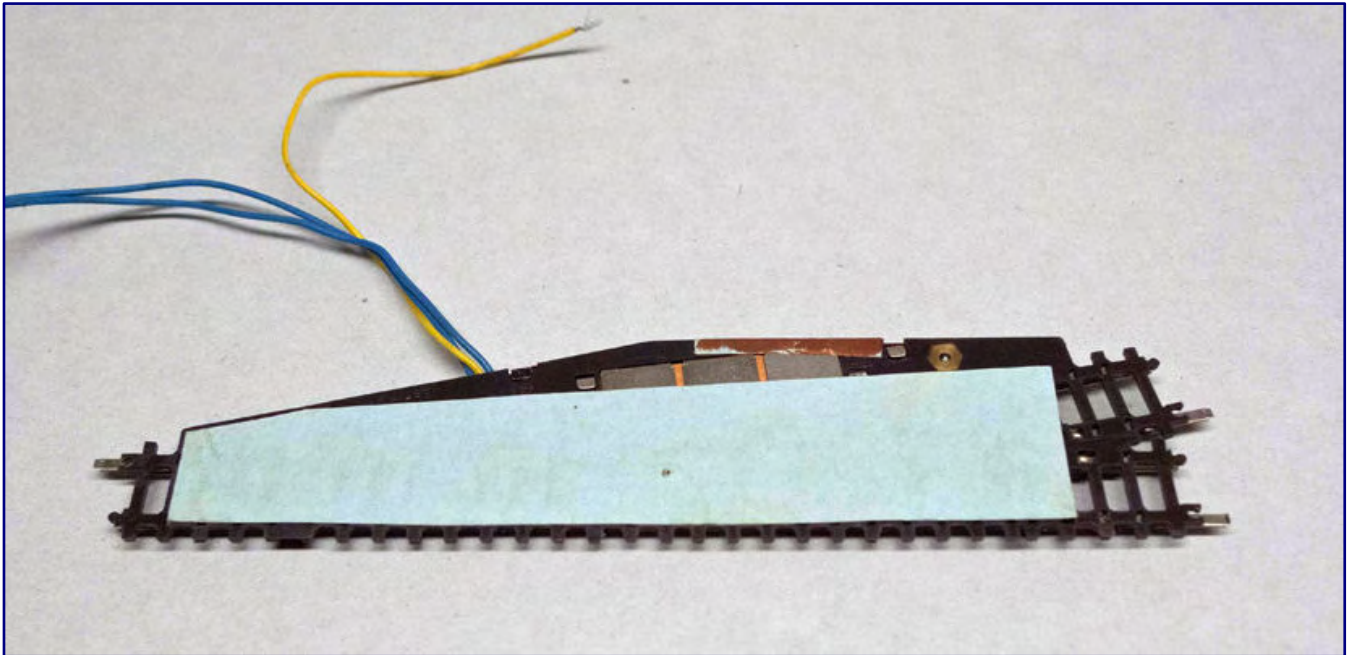


Inconspicuous, but valuable: self-made dumping tool made of aluminium sheet for spreading ballast and small chippings.

I made a flat, elongated chute from the edge of an aluminium tealight pot to apply ballast and small chippings. Holding it between my thumb and middle finger and tapping it lightly with my index finger, this chute allowed me to dose the material quite delicately and precisely.

Before I started working on my brand-new turnouts, I tested most of the work described below on three used turnouts I had bought cheaply. This procedure provided me with useful experience.





Thin and suitably cut airmail paper served as a carrier material for the ballast in the critical area of the points. Before the small ballast was applied, a coat of dark brown paint was applied to the top side not visible here.

Then I got started: First I drilled a 0.5 mm wide hole in the centre of the second sleeper in front of the switch blade joint using a pin block. I used this hole to fix the turnout (with a steel pin) to a plywood board and later to the layout's track board.

To fix the two (slightly curved and movable) contact plates in the centrepiece of the switch in a low position, I put a small drop of white glue into the opening on the underside (below the point where the contact plates are closest together).

I then carefully clamped a match between the two angled rail profiles from above. After several hours of drying, the match could be removed and the work continued.

In the next step, I cleaned the rails and switch blade with a cotton bud moistened with isopropyl alcohol. I then coloured the rails, the switch blade (including the tops of the two connecting bars), the frog sides and the wheel links with the acrylic rail colour mixture intended for this purpose.

The switch blade joint, the outer sides of the switch blade in the rear area and the opposite sections of the rail heads had to remain free of paint, as did the bronze current contact plates under the switch blade and the rail profiles at the ends of the switch (in the contact area of the connecting fishplates).

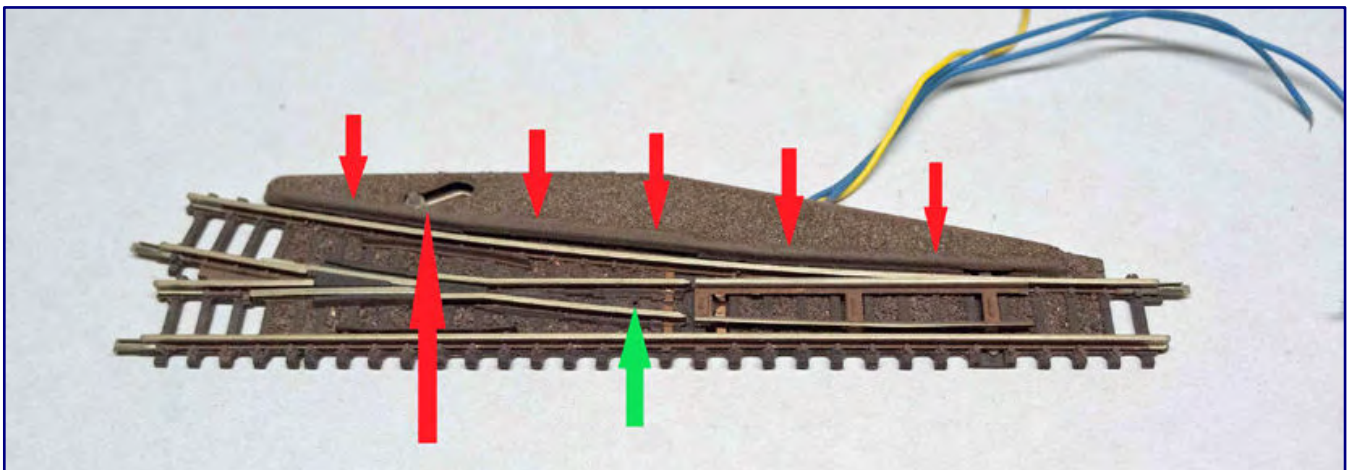
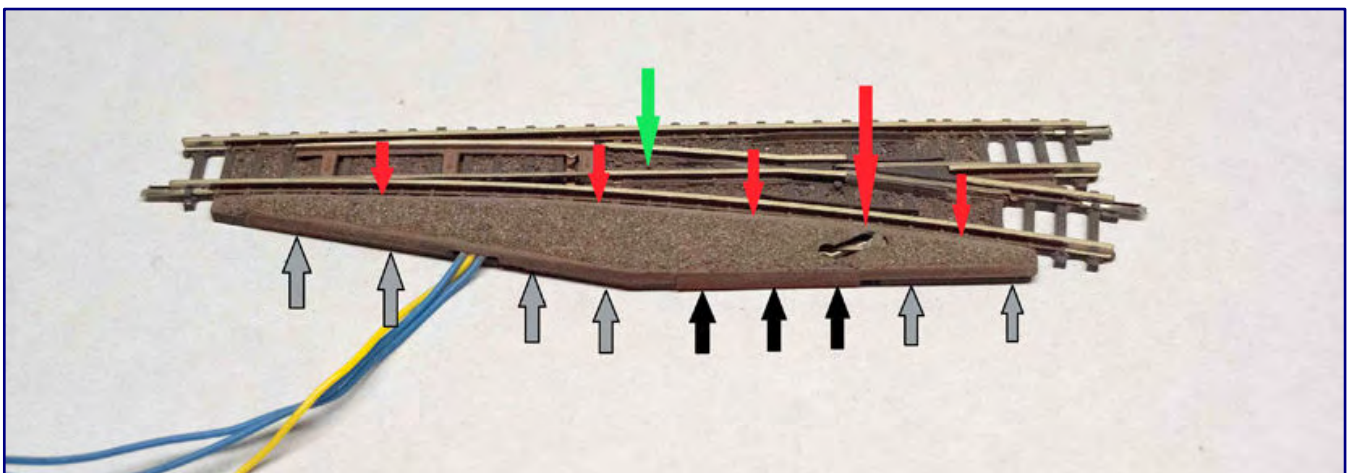
I cleaned the upper side of the rail heads and the frog as well as the upper edges of the switch blade and the wheel links with a firm, lint-free cotton swab slightly moistened with water while the paint was still a little damp.

I painted one side of a sheet of very thin airmail paper with dark brown acrylic paint (gravel colour). From this I cut out a strip about 9 - 9.5 cm long and tapering from 2 to 0.9 cm, applied to the coloured side (!) sparingly with undiluted white glue.

Then, I glued the strip from below onto the sleeper grating of the turnout so that the entire area between the straight outer rail and the drive box was covered, i.e. the later underside of the sleeper grating was light, the upper side brown. After this and all subsequent work steps, it was always checked whether the switch blade remained easily movable; if not, the cause had to be found and eliminated immediately.

I cut strips a few millimetres wide from the same brown paper, folded them 90° lengthways and applied a thin layer of white glue on the inside (on the light-coloured side).

I used these strips to seal the cracks in the turnout drive box on the straight outer sides. This largely prevented the unwanted ingress of ballast or small debris during the subsequent work steps. After drying, the turnout was pinned to an assembly board with a steel pin and the entire turnout drive box was painted with ballast paint. I cut strips of paper a few millimetres wide, folded 90° lengthways and thinly coated on the inside (on the light side) with white glue.



The treatment of the standard turnout is largely complete, only the panelling of the outer (straight) upper edges of the turnout drive box is still missing. Legend to the arrow markings (also to the photo on the next page):

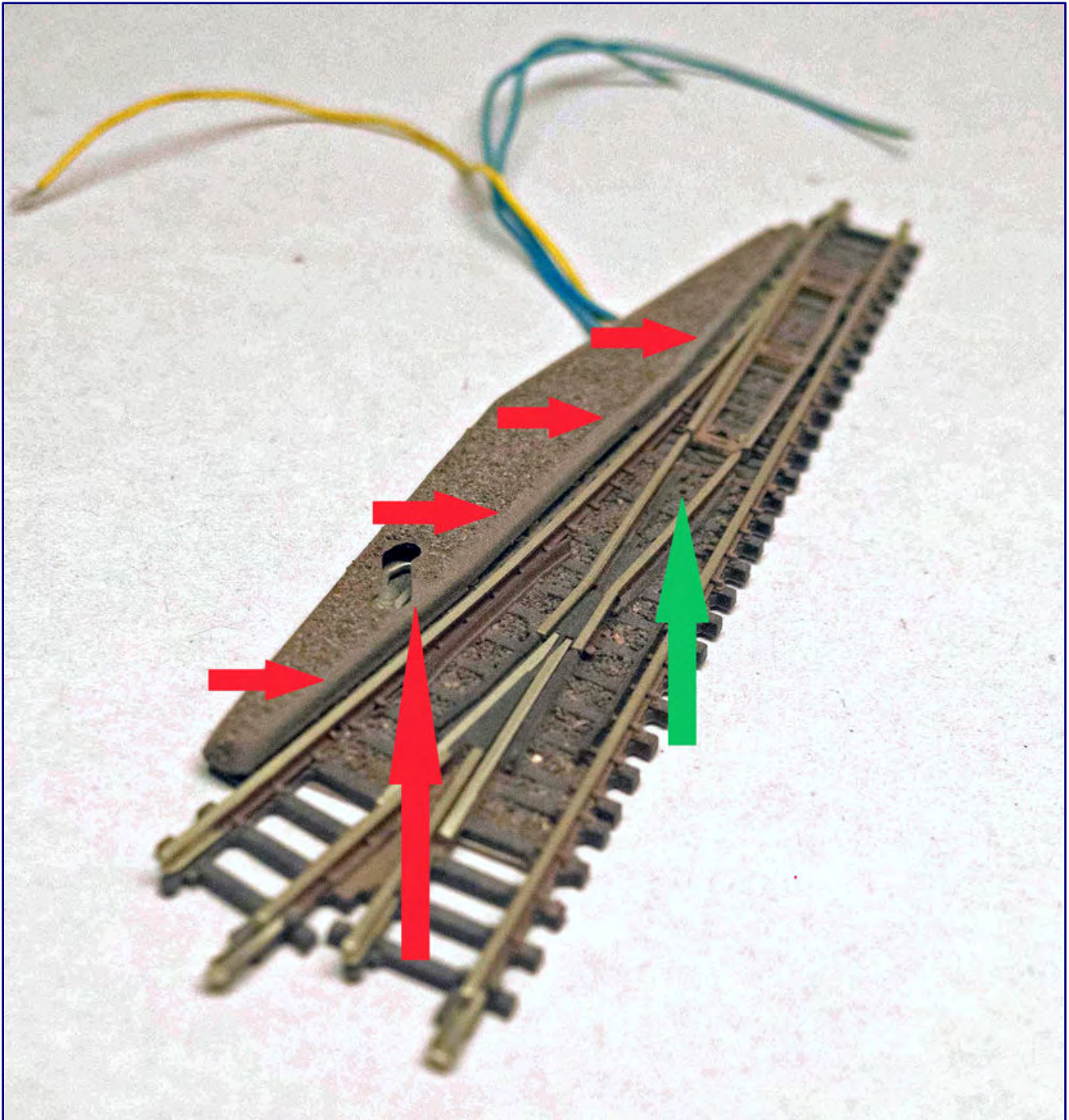
- The green arrow points to the 0.5 mm hole for fastening the turnout.
- The short, red arrows point to the upper edge of the soft drive box, which is free of small parts.
- The long, red arrow marks the low-impact area on the hand controller.
- The black arrows indicate the sealing strip on the drive box of the test specimen.
- Grey arrows point to the additional sealing area as implemented on the drive boxes of all installed standard points.

The paint had to be opaque, but as thin as possible; under no circumstances was it allowed to spread. I also painted the top of the sleeper grating strip between the curved track and the drive box with diluted ballast paint, leaving out the short sleeper heads.



In the next step, I used a fine brush to apply the water-white glue mixture to the upper side of the turnout drive box, leaving an approx. 1.5 - 2 mm wide strip along the edge to the inner curve of the turnout and an additional 0.5 - 1 mm wide area around the manual setting knob opening free.

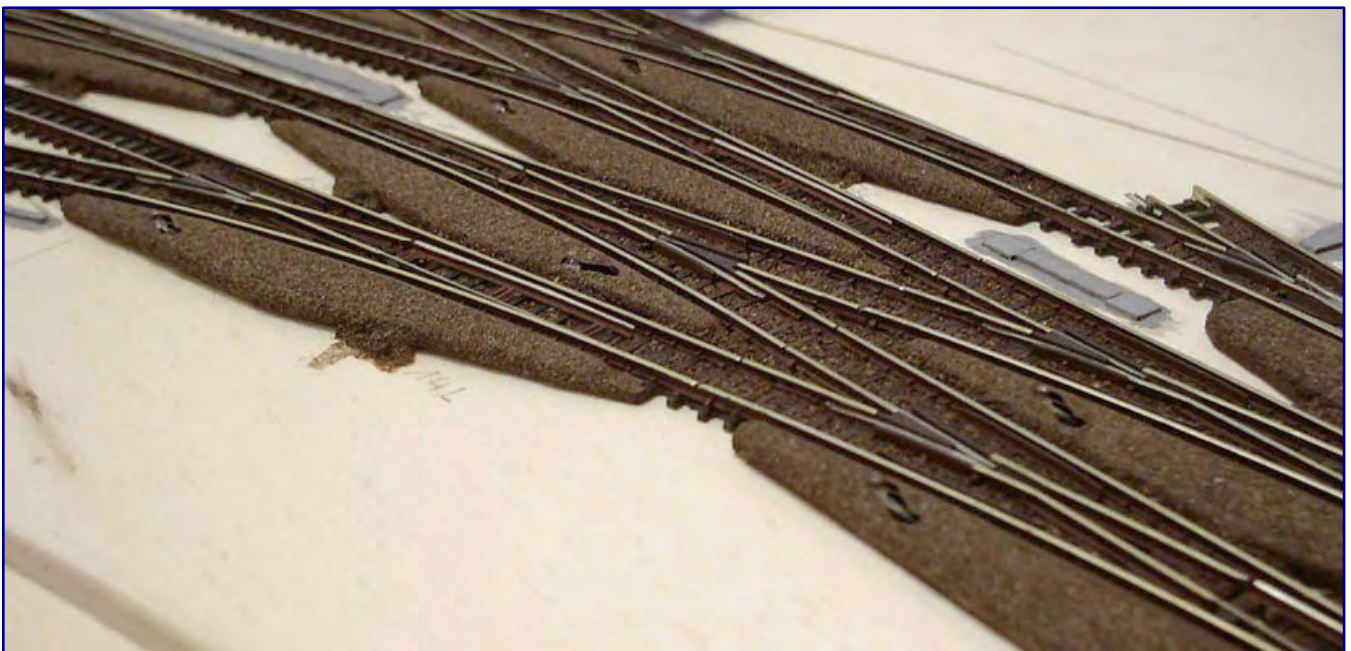
I carefully sprinkled small grit into the damp white glue, which I spread evenly with a fine brush to just cover the surface. Any stone grit that accidentally got there was immediately removed from the areas to be kept clear.



After the white glue had dried, the switch was carefully vacuumed with a Hoover, tapping lightly; I then made any necessary touch-ups. I then coated the straight outer edges of the drive box with thinned white glue and carefully sprinkled in a small amount of chippings, which I spread in a thin layer over the edge of the housing and slightly below it.

I took great care to ensure that the lower part of the drive box remained free of any small impacts. This was absolutely necessary wherever neighbouring tracks were planned with a track spacing of exactly 25 mm.

If the lower area of the outer sides of the drive box was not kept sufficiently clear, there was a risk during laying that dried material would either collide with the outer edges of the sleepers of the neighbouring track or rest on top of the neighbouring sleepers.



Several standard points have now been installed in the track area. The connecting cables have been routed under the layout and all work on the holes, track sides and centres has been completed as described in the article.

In this case, proper track alignment was not guaranteed; the function of the sleepers of neighbouring turnouts could also be impaired. Small debris that had stuck to surfaces of the drive box that had to be kept free despite all caution was removed again immediately using a file, sandpaper or scalpel blade.

After drying, the turnout was cleaned again with a Hoover; unwanted defects in the stone-free areas of the drive box were then repaired.

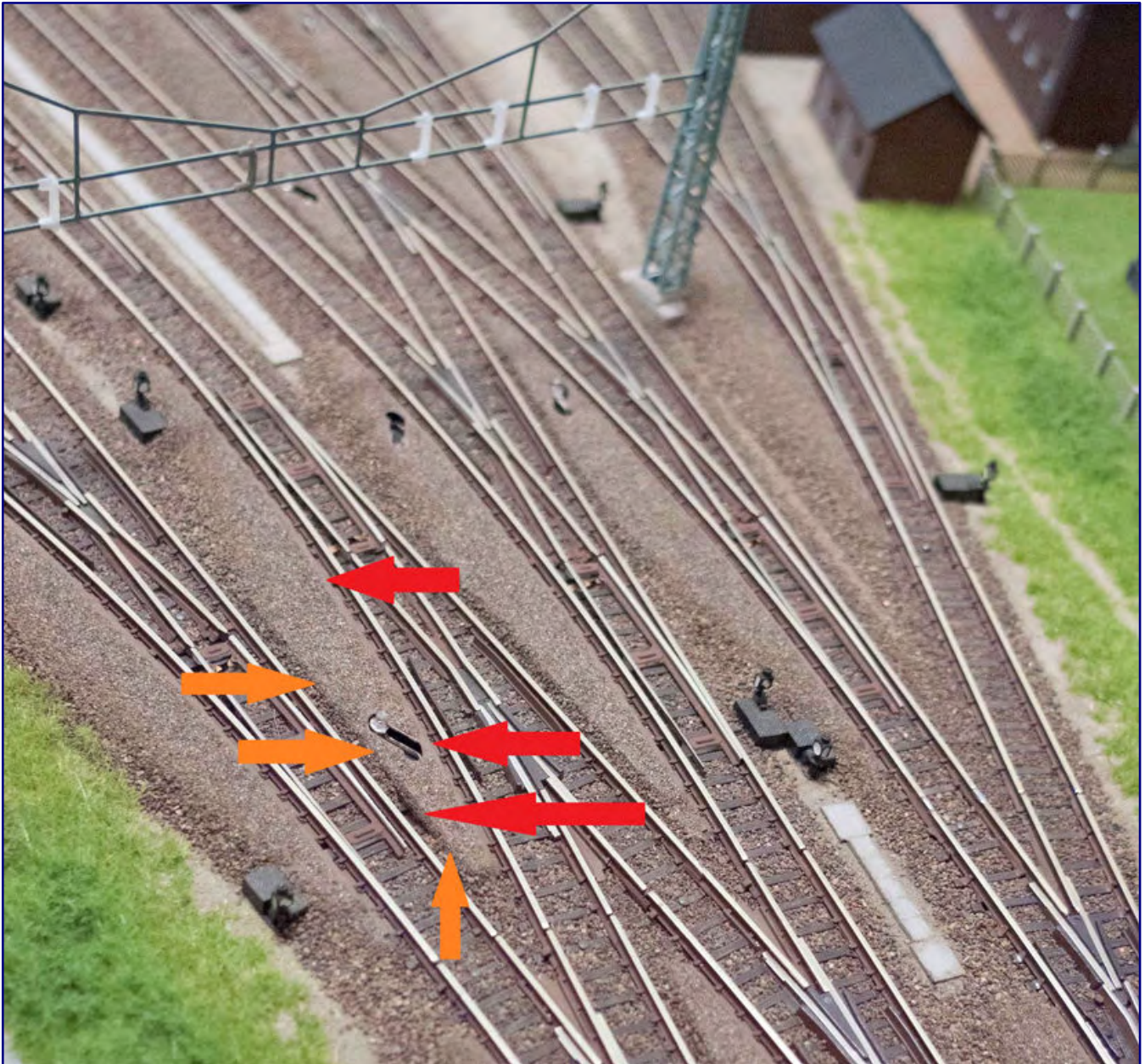
If I noticed any defects on critical edges, I sometimes took the precaution of not repairing them with a small amount of paint, preferring to use ballast paint instead. The functional safety of the points and tracks was my top priority.

Finally, I used a fine brush and heavily diluted ballast paint to paint over all the areas of the drive box that had a slight sheen due to the white glue mixture that had run off.

The next step was to ballast the turnout in the problematic area between the rail profiles. I used the aluminium chute to pour track ballast into the spaces between the sleepers sealed by the paper. The only exceptions to this were those immediately in front of and behind the switch blade joint and under the two connecting webs of the blade, so as not to jeopardise mobility.



I spread the ballast evenly within the spaces between the sleepers using a fine brush. I carefully added any missing ballast; I removed excess ballast with the brush towards the ends of the track. The ballast was also very carefully removed from the sleepers and the frog as well as the switch blade with the brush.



This is how the track area on the last side looks after the camouflage and ballast work has been completed. The long arrow (red) points to the area opposite the sleeper of a neighbouring turnout that is free of small parts and ballast, while the orange arrow points to the straight upper edge of the drive box opposite an immediately neighbouring stationary wheel turnout (legend for red, short arrows follows the previous photos).

I made sure that the ballast surface in the areas in front of and behind the switch blade was approximately level with the top edge of the sleeper, but lower in the area below the movable switch blade to ensure its mobility.



I fixed the ballast with a mixture of water and white glue, which I applied drop by drop into the spaces between the sleepers using a small disposable syringe. The droplets were kept small because the ballast took a few seconds to absorb. If the stones became disordered within a sleeper space, I levelled the surface again with the tip of the syringe needle.

To prevent it from sticking to the work board unintentionally, I turned the switch slightly on the board several times (around the pin as a pivot point). Once the glue had dried, it was possible to touch up any imperfections; then, a second fixing pass was carried out with the white glue syringe.

Finally, I removed any loose material from the turnout, detached the turnout from the work board and cleaned the tops of the rail profiles and the upper edges of the switch blade with a cotton bud moistened with isopropyl alcohol.

I then tested the function of the points. If it could be switched properly and the test locomotives passed the turnout without any problems, this specimen was ready to be installed on the layout.



Here you can see the track area, which serves as the basis for the explanations in this article, from an elevated perspective in the overview.



## Route corrections on points

Most of the standard points purchased over 20 years ago bent slightly upwards, so that the trains always travelled more or less over a hill without countermeasures.

In addition, the three track ends of several turnouts were also slightly warped and twisted in the longitudinal direction. I had already noticed both phenomena when building my first layout in the eighties and again and again afterwards.

I solved this problem as follows:

- 1.) I fixed the bending by fixing the turnout to the track board with a Märklin track nail when installing it (see the 0.5 mm hole in front of the switch joint). The tool I used for this was the Nagelfix from Peter Post.
- 2.) In some cases, the distortion / twisting of the turnout ends took care of itself when laying in turnout lines, so that no further action was necessary.

In other cases, I drilled a 0.5 mm hole in the centre of the last or penultimate sleeper of the respective turnout ends and fastened the aligned turnout at the respective ends with additional Märklin track nails.

Both measures not only worked when laying the tracks on a plywood track board (in the station area and in the staging yard), but also on Jeweha Modelbouw cork beds.

## Completion of the camouflage work

After installing a turnout and the neighbouring tracks, I continued with the ballast and camouflage work. The connecting cables for the points were fed down through a hole drilled just next to the drive box through the track board and connected to the contacts provided on a distributor.

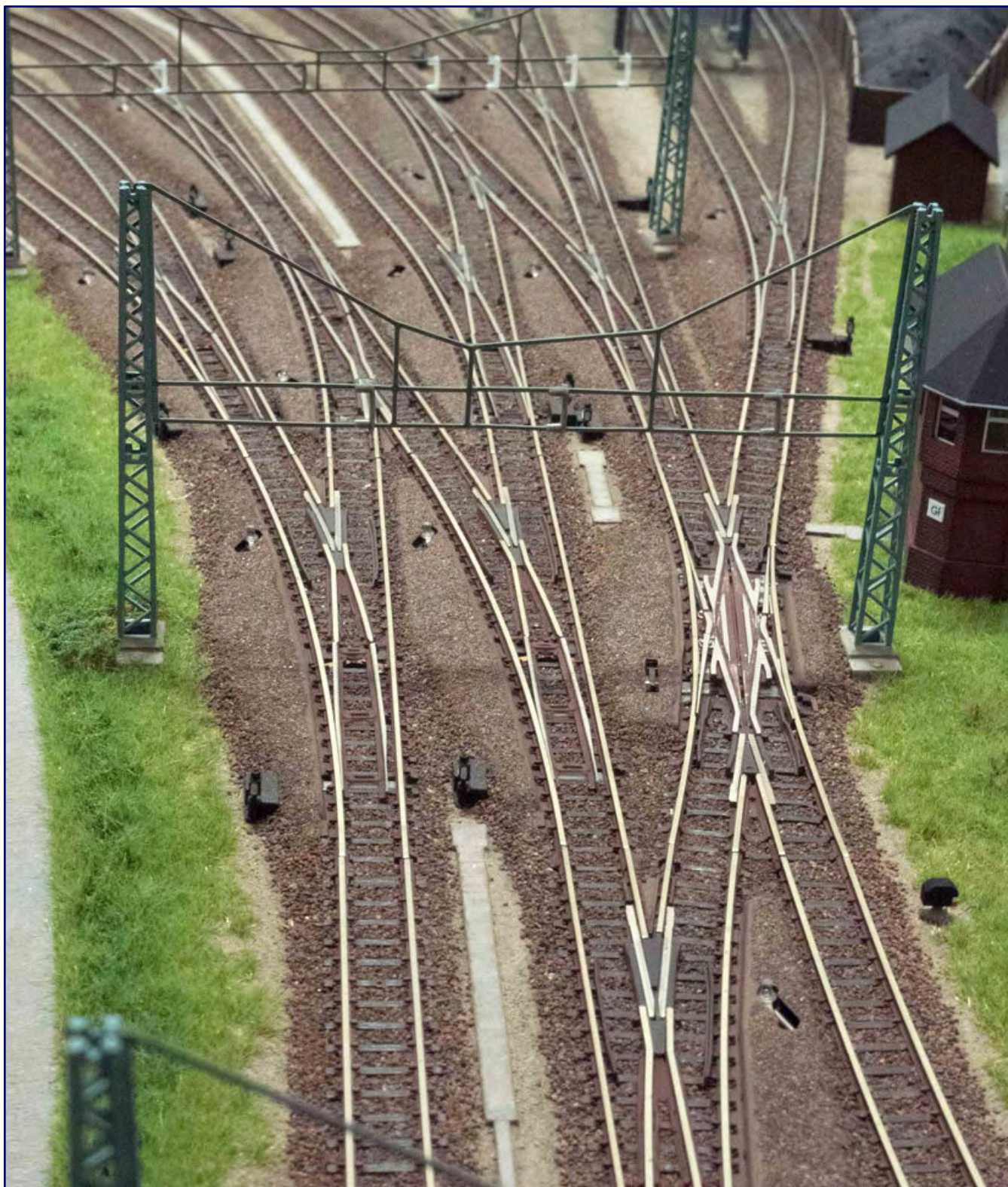
I sealed the hole in the track board and the remaining opening where the connecting cables emerge from the drive box with a viscous mixture of ballast and a little diluted white glue.

If there was a parallel track or another turnout directly next to the turnout drive box, I first carefully filled the remaining outer sleeper spaces of the parallel track or the neighbouring turnout with ballast up to the top edge of the sleeper at this edge.

In doing so, it was essential to keep the neighbouring guides of switch points clear. After fixing the ballast with diluted white glue, the clearance gauge was checked (with a locomotive and, possibly, a short train). After carrying out any necessary repair work, a second fixing pass was carried out with the white glue sprayer.

On the straight edges of the drive box, where no tracks or points were directly adjacent, I formed approx. 45° steep outer slopes from ballast with the aid of a fine brush.

I achieved flowing transitions from the small fodder on the top of the drive box to the gravel slopes by sparingly scattering small fodder on the upper edge of the slopes, whereby it was important to avoid pile formation.



**Change of perspective:** We look at the course of the switch route in the locomotive driver's direction of travel and realise how well the point machines can be camouflaged using a relatively simple method.

Fixing was carried out drop by drop at the bottom of the slope so that the wet adhesive could spread sideways and creep upwards without the slope becoming unstable. A second pass was carried out after drying and any necessary touch-up work.



Once this work has been completed on all the points in a track system area, work could also begin on the areas of the points that were still open, such as outer sleeper gaps and previously untreated track sections at the ends of the points, and on the other tracks.

## Curved and double crossing points

I proceeded in a very similar way with the curved points. I also fixed the two contact plates in the frog of a curved turnout and sealed the sleeper grid in the area next to the drive box with brown coloured paper from below.

I drilled a 0.5 mm wide hole through one of the two wide cross-connections in the outer arch using a pin block to attach it to the baseboard and later install it on the layout (see illustration below this paragraph and on the next page, marked with a green arrow).



I also used a steel pin to fix the curved turnout to a plywood work board for further processing. After the cleaning work, I coloured the rails, switch blade, frog sides, and wheel links with the acrylic rail colour mixture. All further steps corresponded to the process already described for the standard points.

In addition, the connection cable cover and the striking metal manual setting lever on the curved turnout were also given a thin layer of ballast colour. I also coloured parts of the two wide cross-connections on the sleeper grating, so that only narrow imitation sleepers in the sleeper colour (dark brown) remained at the edges.

continues on page 26





The double crossing turnout is only missing the prototypical turnout signal after editing, as described in the text (photo below).



After installing a curved turnout and the immediately adjacent tracks or turnouts, I continued the ballast and camouflage work in a similar way following the familiar steps. Only the outer slopes on the drive box and in the area of the connecting cables were smaller than on a normal turnout due to the smaller dimensions.

I also worked on the double crossing points (DKW) on my layout in a similar way. After examination, I drilled the 0.5 mm wide holes for the fastenings, so that the sleeper grid of the DKW would lie flat on the track board when the track nails were pressed in. The metal, conspicuous manual setting lever of the DKW was given a thin coating of ballast colour, as with the curved points.



Finally, we take a look at the station entrance with two opposing curved points, one of which is partially obscured by the tower mast in front.

My personal conclusion: As you can see from this construction report, the work proved to be very time-consuming and I also experienced some unpleasant surprises. But with a lot of patience, care, endeavouring to avoid dust and dirt and keeping the electrical contact surfaces clean, I managed to achieve my goal. Even after many years, I am still satisfied with the result.

All Photos: Jochen Brüggemann

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## Half relief buildings from Modellbahn Union **Smart Transition to the Background**

*We have often described how important a scenic background is for adding depth and realism to a layout. In urban settings, however, placing a backscene directly behind a row of houses is not ideal. It often makes for a better optical transition to add an additional row of half relief buildings - if it were not for a lack of supply of adequate kits. But that may be a thing of the past, as our collaborator Ralf Junius seems to have found what he was looking for at Modellbahn Union.*

By Ralf Junius. The release of half relief townhouses from the company Modellbahn Union gave me the idea for building a small new “club table” — type module, which I also plan to use for photo and film shoots.

But these buildings are certainly not only suitable for that specific purpose. As can often be seen in larger scales, a row of half relief houses can make for a nice transition to the background of the layout. The description on the manufacturer's website also sounded promising: “Z scale, laser-cut kit made of graphic cardboard. Half relief town house in the style of the Wilhelminian era.”



Two of the new half relief houses from Modellbahn Union, as shown here to good effect here on a diorama by Torsten Schubert.

It goes on to say: “In this period, the houses were mostly built uniformly, with only the façades being designed differently. The house is divided into four floors. The façade is elaborately sculpted and has many beautiful details. All of the houses in this series can be connected to each other, making it possible to build entire streets.” Now that's an announcement!

At first, buildings with two different lengths were released. The narrow house has a length of 70 mm and the length of the larger house is 108 mm. The longer model is also available in two different façade versions. All models are available in three different colours: White, grey and sandstone. What the buildings have in common is their depth of 33 mm and height of 94 mm.



As with every build, all parts are first inspected and checked for completeness. Before we start cutting and gluing, we familiarise ourselves with the instructions.

Further models have since been added. These include a semi-relief corner townhouse and courtyard townhouses - each with different façade colours. In this article, I would like to explain the construction of the "Half relief townhouse 01 sandstone" (item number: MU-Z-H00201).

After unpacking the assembly instructions and all the components, the first step is to check the completeness against the item list in the instructions. It turns out that all the components are present, so nothing stands in the way of assembly.

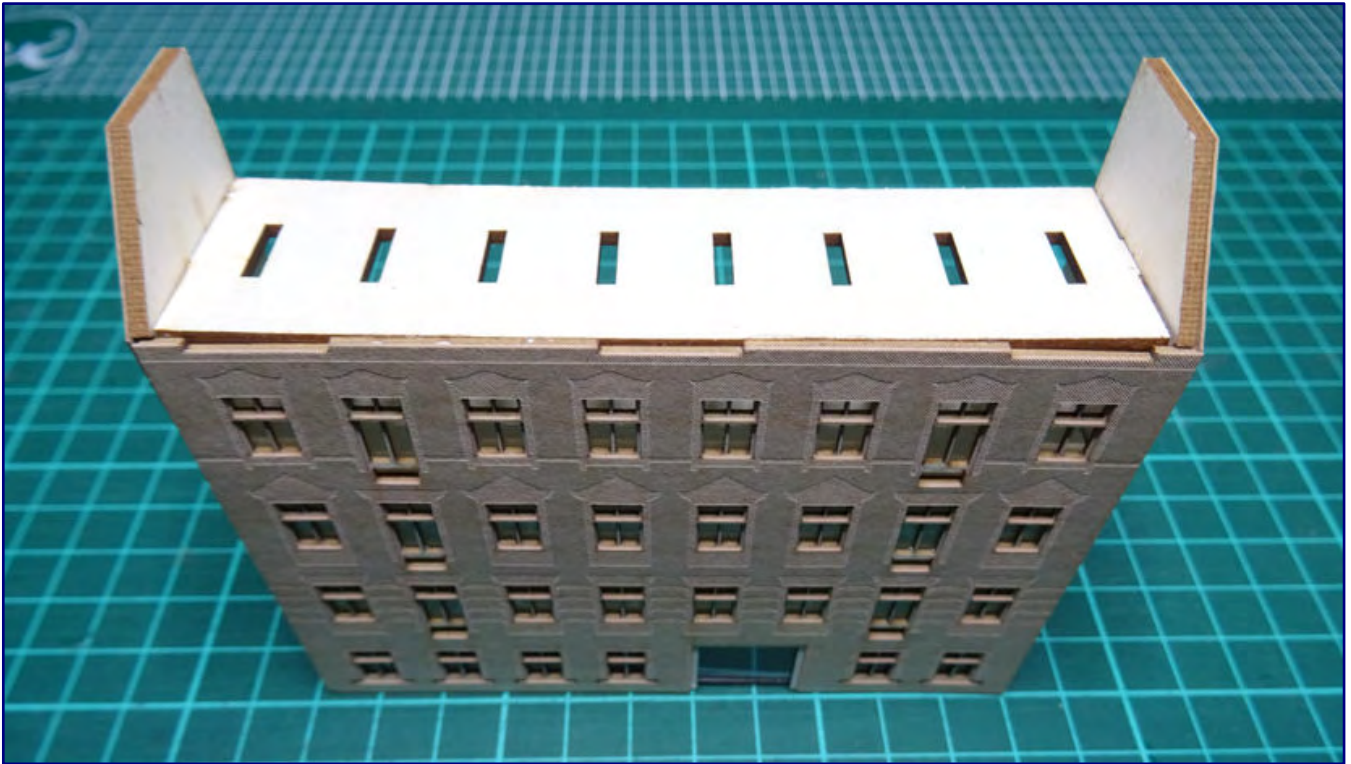
The first step is to assemble the basic frame. It consists of two side parts, a base and the four floors. What I really like here is that the individual floors already come with cable holes which makes wiring for lights much easier.

The first part of the front is then placed on the base frame. This consists of a base that contains the window struts, and another part that represents the coloured façade. This means that the basic core structure



A basic structure made of thicker cardboard makes for the stable core of the building. Note the cable holes for adding lights to floors or rooms.





The three basic façade elements are glued onto the building core one after the other, thus giving the building already a recognisable shape.

can be used for all kits of the same basic dimensions.

In my kit, not all the windows on the substructure were fully cut out at the factory. This can be remedied by briefly cutting them with a scalpel and pressing them out. The windows are fitted with transparent foil on the inside. The façade is now attached to this base. Two façade parts in a matching colour are also attached to the sides to match the front façade.

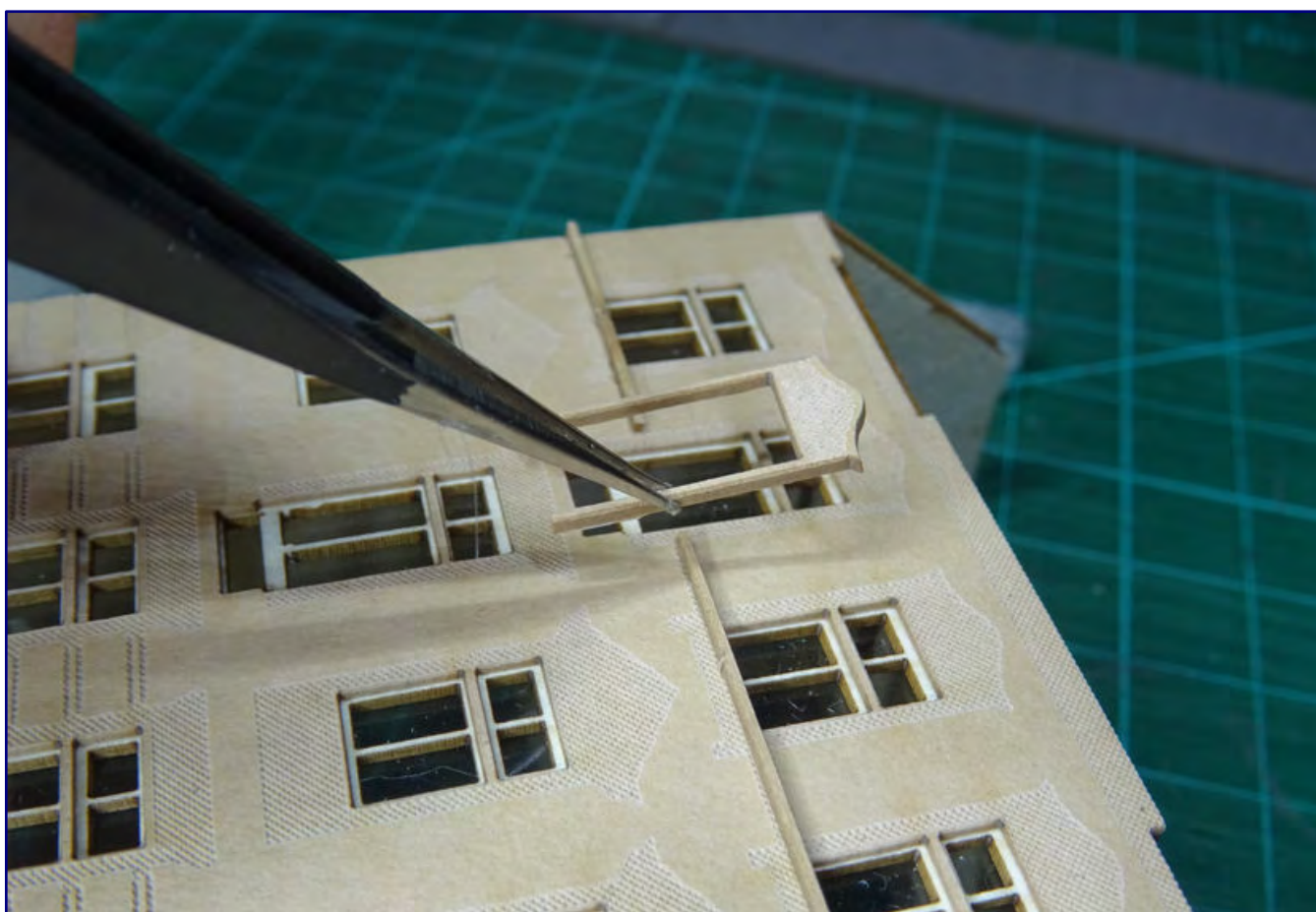
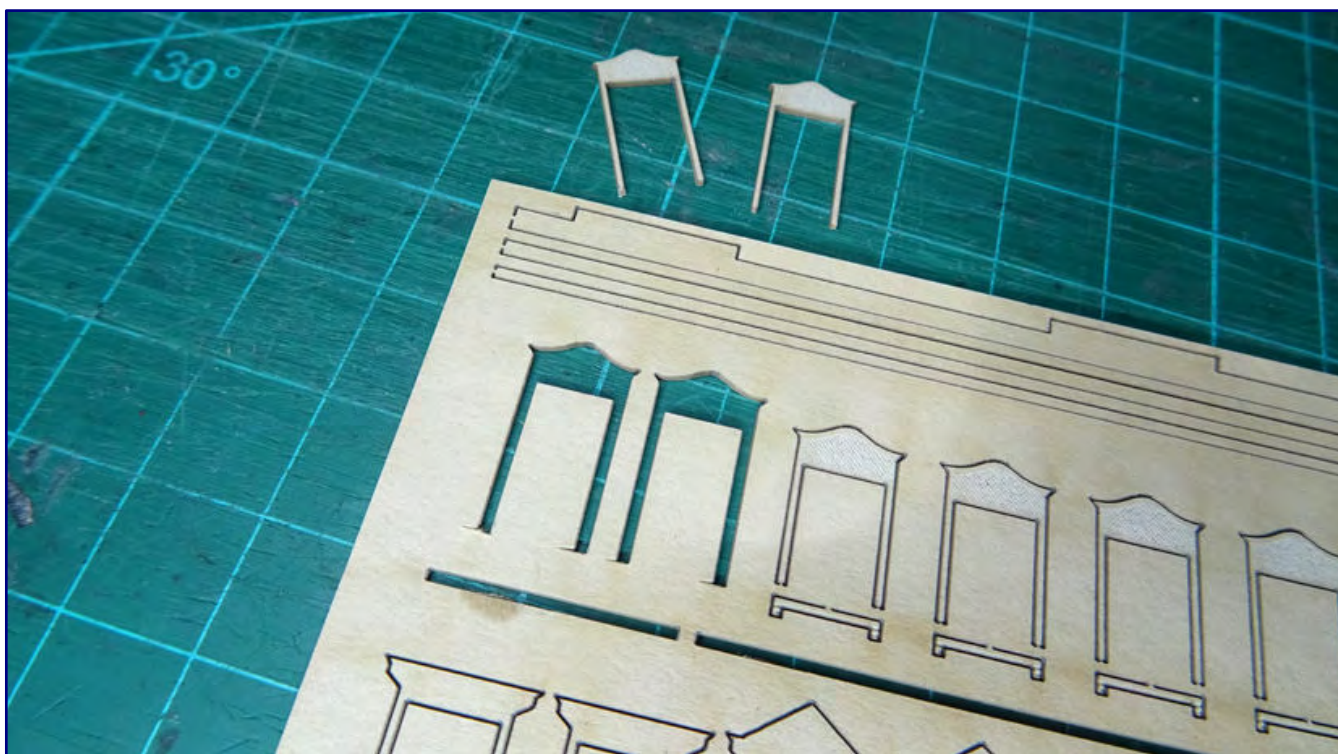
### Lots of windows

Cornices are then added to the façade and the windows are also each matched with a cornice. The parts come on a separate cardboard sheet and are attached to it two or three supports which can be quickly cut with a scalpel. Then nothing stands in the way of positioning the frames on the walls.

I use a special laser-cut adhesive to glue the individual components together. Many companies have a suitable product in their range. I have no particular preferences and use whatever I have at home. An interim realisation at this point: the building has quite a lot of windows!

The next step is the support structure for the roof. Its elements must also be cut out of the cardboard sheet beforehand. With a few drops of glue, they are placed in their assigned position. Together they will later support the roof tile panel.

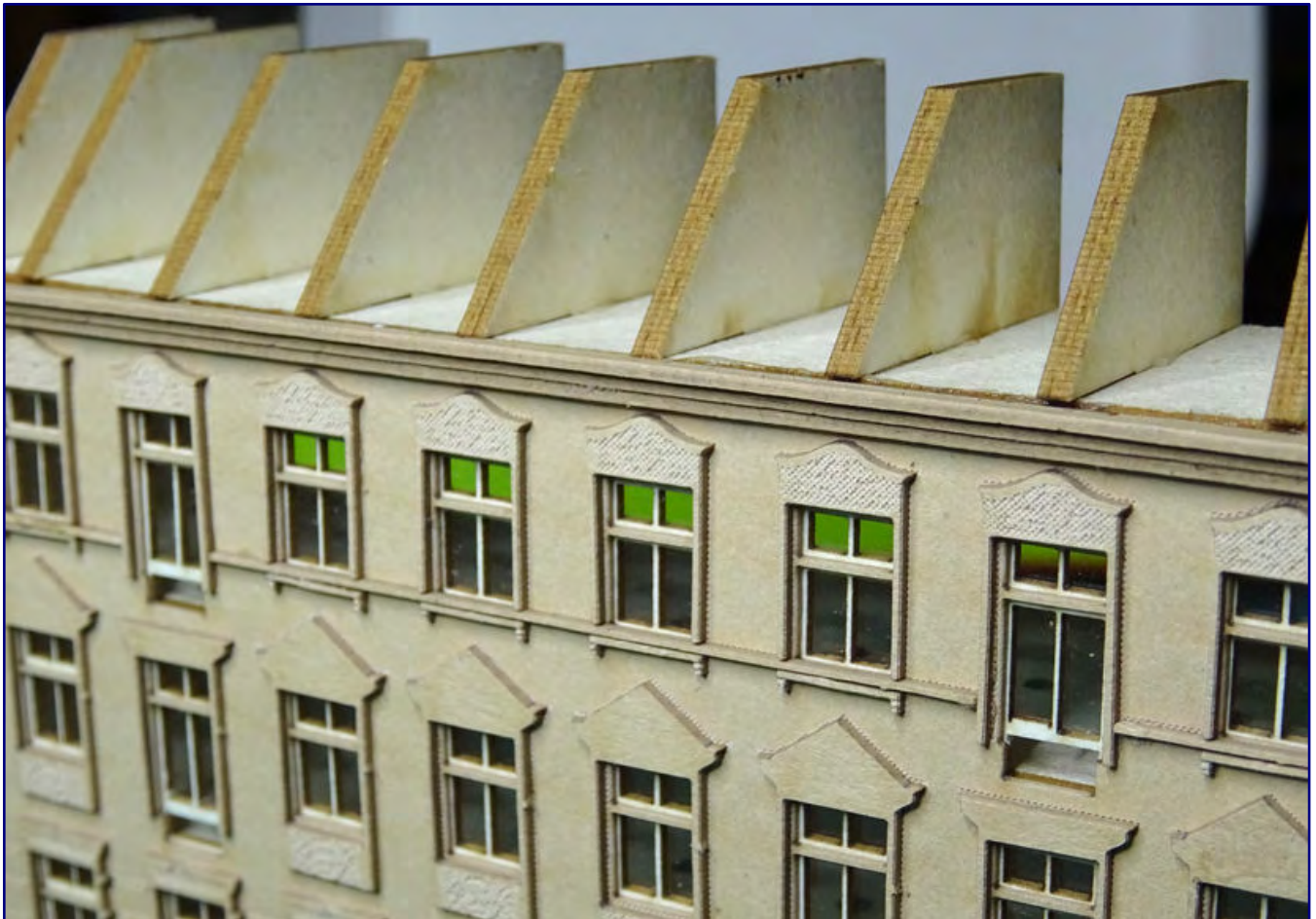
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The various cornices are carefully separated from the sheet by using a sharp craft knife, for example from Mozart. One can also see the long parts for the roof cornice (photo above). The parts are then individually positioned on the façade and glued in place (photo below).



Before the roof is covered, however, a roof cornice (roof box) has to be assembled and fitted. Here I found it a bit difficult to position the individual layers correctly. I couldn't see it properly in the assembly instructions. However, with a bit of trial and error I managed to put it together correctly. This shows how important it is to carry out dry-fit tests before applying the glue!



The supporting structure of the roof has been installed and one can also see the roof cornice above the top row of windows, which is glued together in three parts. The manufacturer has thus taken up the typical design elements that we still find today on Wilhelminian-style buildings.

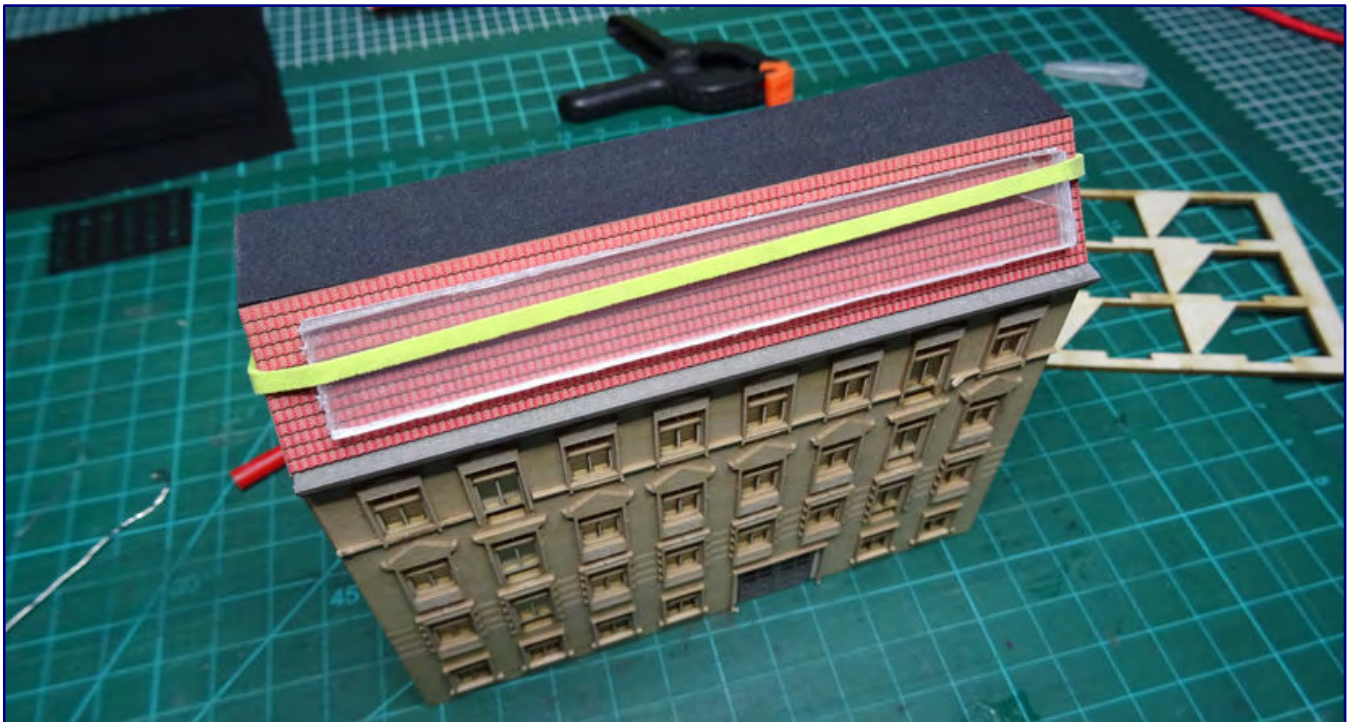
A black panel must then be placed on the top of the roof before its slope can also be covered. The red cardboard engraved with roof tiles follows. I use a rubber band to hold this part in place until the glue has set and hardened.

The final step is to add the balconies to the house. In this kit, I first bent the balcony structure and then assembled it. In my opinion, however, this didn't work out so well.

On a building I did later, I first attached the balcony structure and then bent the sides and glued them on. This proved to be more effective and easier. I'm sure I'm not the only one who learns something new with each kit.

The rear side should face the background and therefore requires no decoration. Accordingly, the designer at Modellbahn Union has provided a light-proof, black hard cardboard panel for this rear end. I have not yet fitted this.





The roof is added and fixed in place until the glue has dried (photo above). The final step is the balconies and parapet (photo below),

My buildings still need interior lighting and furnishings. Raffaele Picollo from Genoa (MrZtraX) has presented a solution for this using LEDs and acrylic glass, an approach which I am trying to copy. If there is any interest in this line of work, please let the editors know and we may follow up on this in one of the next issues.



As stated at the beginning of this report, my aim is to build a small “regular’s table” type diorama with background buildings for photo purposes. The first buildings have now been completed apart from the lighting. And so, at the end of the article, I would like to give you a small impression of how the houses will look when installed.

## Summary

The idea of a semi-relief building is not new in itself. However, until now there have been no 1:220 scale kits on a polystyrene or hard cardboard base in this size and comparable style. I can only praise the creative approach and the realisation of the manufacturer. The material used is of high quality.



The new half relief buildings at the transition to the background are undoubtedly an enrichment for the layout. They also offer many options for individual designs due to the interchangeability of this series of building kits.

It is also very nice and valuable that the series has already been extended by differently designed and dimensioned buildings. There is a consensus among the editorial team that there is nothing comparable on the market in 1:220 scale.

We are therefore nominating the kit presented here, as a representative of the product series, for the best new releases of 2023 in the accessories category of our best annual products awards.

**Supplier for the kits:**  
<https://www.modellbahnunion.com>

**Commercial offers from MrZtraX:**  
<http://www.mrztrax.com/Zcatalogue.html>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

## Neubaudampflokomotive Baureihe 23 der DB **Die Neue, die nicht alt wurde**

***Unter Dampflokomotifreunden nimmt die Baureihe 23 der DB eine besondere Rolle ein. Auch die Bundesbahn wollte wohl unmissverständlich darauf hinweisen, dass sie nicht mit der Einheitslokomotive verwandt ist und begann deshalb, bei der Betriebsnummer 001 neu zu zählen. Doch viele Jahre fehlte explizite Fachliteratur zu diesen Maschinen. 2023 hat sich das endlich geändert.***



Frank Lüdecke  
Die Baureihe 23  
Die letzte Neubau-Dampflokomotive der DB

EK-Verlag  
Freiburg 20232

Gebundenes Buch  
Format 21,0 x 29,7 cm  
344 Seiten mit 149 Farb- und 459 S/W-Abbildungen

ISBN 978-3-8446-6057-9  
Art.-Nr. 6057  
Preis 54,00 EUR (Deutschland)

Erhältlich direkt ab Verlag  
oder im Fach- und Buchhandel

Frank Lüdecke publiziert seit 1979 im EK-Verlag und gehört so gewiss längst zum Kreis besonders fachkundiger Autoren. Für den vorliegenden Band war er auch gewiss deshalb erste Wahl, weil er durch eine frühere Aushilfstätigkeit und später dann im Museumsbetrieb persönlich in engen Kontakt mit der Dampflokomotive kam.

Der heute behandelten Baureihe 23 kommt bei dieser Traktionsart eine besondere Bedeutung zu. Zu diesem Loktyp gehört die letzte an die Bundesbahn abgelieferte Dampflokomotive und nach Stückzahlen war sie mit 105 Exemplaren die mit Abstand wichtigste Neubaulokomotive. Ihr Beschaffungszeitraum erstreckte sich über immerhin neun Jahre.

Das brachte auch Änderungen an der Konstruktion und Ausstattung mit sich, was die Vielfalt der Bauarten zusätzlich erhöht. Immer war sie aber auch Gegenstand von Kritik: Ihre Leistung und Verbrauchswerte stellten demnach keinen Fortschritt dar und Nostalgiker bezeichnen die Baureihe 23.10 der DR gern als die bessere Konstruktion.

Außerdem sei die Lokomotive zu spät gekommen und deshalb nicht mehr gebraucht worden. Mit diesen vorschnellen und überwiegend falschen Urteilen räumt der Autor in seinem Buch auf. Er erläutert die Vorgeschichte und Umstände, die zu einer völlig neuen Dampflokomotive führten, die mit der Einheitsdampflokomotive nichts mehr gemein hatte.

Gewohnt ausführlich wird die Konstruktion beschrieben und auch ausgeführt, was bei den einzelnen Bauteilen jeweils geändert wurde. Auch die Bauartänderungen im Laufe der Dienstzeit schließen sich



an. Mit dem Schwenk zu Versuchen und zum Betriebsmaschinendienst erfährt sie dann eine korrekte, betriebliche Einordnung.

Obwohl sie als Personenzuglokomotive konstruiert worden war, erwies sich die Baureihe 23 im ersten Betriebsjahrzehnt häufig als Behelfsschnellzuglok, um einem Mangel entgegenzutreten. Diesen Dienst, der ihr den Baureihen 01 und 03 vergleichbare Kilometerlaufleistungen einbrachte, verrichtete sie klaglos wie später auch Vorspanndienste vor Güterzügen. Ihre Verbrauchswerte waren seinerzeit günstig, ihre Zugkraft beim Personal geschätzt.

Und nicht grundlos war sie wohl die am längsten eingesetzte Neubaudampflokomotive der DB mit immerhin 25 Jahren Dienstzeit, die aber nur die älteren Exemplare erreichten. Als auch die jüngsten und modernsten Maschinen zur Untersuchung anstanden, wurde auch diese Baureihe aus dem Unterhaltungsbestand gestrichen – eine Ironie des Schicksals.

Aber immerhin stehen 73 Jahren nach dem Erscheinen von 23 001 fünf Maschinen dieses Typs bei Museumsbahnen unter Dampf. Das möchten wir ebenso als Beleg für die Bewährung der Konstruktion werten. In der Spurweite Z hat sie übrigens auch ein Denkmal gefunden, gesetzt durch den Kleinserienhersteller Bahls Modelleisenbahnen.

Für den EK-Verlag ist dieser lange erwartete und überfällige Band ein Lückenschluss in der Baureihenbibliothek und ein neues Standardwerk. Lange Zeit wurde die Baureihe 23 literarisch vernachlässigt, was mit Titeln zu tun haben dürfte, die sich den DB-Neubaudampflokomotiven im Allgemeinen widmeten.

Frank Lüdecke macht all das vergessen, indem er kurzweilig wie fachkundig ein facettenreiches Panorama der beliebten DB-Dampflokomotive entwirft. Beim Lesen mag man es nicht mehr aus der Hand legen. Angenehm macht sich auch das neue Papier, das uns aufgefallen ist.

Matter als das früher verwendete, wirkt es dennoch nicht weniger hochwertig. Der geringere Glanzgrad erleichtert bisweilen das Lesen im Lampenlicht und reduziert auch die Farbbrillanz nicht erkennbar. Werfen wir daher auch noch einen Blick auf die Bildauswahl.

Sie passt perfekt, weil ohne Ausnahmen die gesamte Einsatzzeit der Baureihe damit dokumentiert wird. Das schließt auch alle Betriebswerke und Strecken mit ein, die von der Einsatzgeschichte umfasst sind. Ein weiteres Mal stellen wir fest, dass der frühere Farbteil der Bände aufgegeben wurde und in mehreren Abschnitten in den Buchverlauf integriert worden ist.

Nur ein einziger Fehler ist uns bei den gut gewählten und teilweise unbekannteren Aufnahmen aufgefallen, der verzeihlich ist: Dabei handelt es sich um eine Bildkopie auf den Seiten 69 und 219 anhand der 023 034-2.

Fassen wir unsere Eindrücke zusammen: Von der Entstehungsgeschichte spannt sich über die Loktechnik, Versuchsfahrten, Betriebsmaschinendienst, Bauartänderungen und Lebensläufe ein spannender Bogen zum Einsatz bei den Bahnbetriebswerken. Er schließt sich erst bei den heutigen Museumslokomotiven, die ausnahmslos auch mit ihrer „Nach-DB-Geschichte“ genauestens gewürdigt werden.

**Publishing pages:**  
• <https://www.eisenbahn-kurier.de>  
• <https://www.ekshop.de>

## Vier Archiv-Fortsetzungen Güterwagen und moderne Traktion

*In den Reihen „Deutsches Lok-Archiv“ und „Deutsches Wagen-Archiv“ erschienen vor Jahrzehnten wichtige Nachschlagewerke für beinahe alle Schienenfahrzeugkategorien, die in Deutschland relevant waren und sind. Nach den Standardwerken für Dampflokomotiven legt Transpress nach 30 Jahren nun auch die Reisezugwagen-Trilogie neu auf. Wir haben einen Blick in diese genommen und fassen unsere Eindrücke zusammen.*



Helmut Behrends / Wolfgang Hensel / Gerhard Wiedau  
Güterwagen-Archiv 1 -  
Länderbahnen und Deutsche Reichsbahn-Gesellschaft  
aus der Reihe „Deutsches Wagen-Archiv“

Transpress Verlag  
Stuttgart 2023

Taschenbuch mit Klebebindung  
Format 19,5 x 21,5 cm  
284 Seiten mit 223 S/W-Abbildungen

ISBN 978-3-613-71693-3  
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Helmut Behrends / Gerhard Wiedau / Wolfgang Hensel  
Güterwagen-Archiv 2 -  
Deutsche Bundesbahn und Deutsche Reichsbahn  
aus der Reihe „Deutsches Wagen-Archiv“

Transpress Verlag  
Stuttgart 2023

Taschenbuch mit Klebebindung  
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Dieter Bäßold / Günther Fiebig  
Elektrische Lokomotiven – Deutsche Reichsbahn-Gesellschaft –  
Deutsche Reichsbahn - Deutsche Bundesbahn  
aus der Reihe „Deutsches Lok-Archiv“

Transpress Verlag  
Stuttgart 2023

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ISBN 978-3-613-71695-7  
Titel-Nr. 71695  
Preis 59,00 EUR (Deutschland)

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Wolfgang Glatte  
Diesellokomotiven - Deutsche Reichsbahn-Gesellschaft -  
Deutsche Reichsbahn - Deutsche Bundesbahn  
aus der Reihe „Deutsches Lok-Archiv“

Transpress Verlag  
Stuttgart 2023

Taschenbuch mit Klebebindung  
Format 19,5 x 21,5 cm  
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ISBN 978-3-613-71696-4  
Titel-Nr. 71696  
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Auch bei den vier heute vorliegenden Bänden macht es wenig Sinn, alle einzeln vorzustellen und individuelle Merkmale herauszuarbeiten. Auch hier handelt es sich ausnahmslos um Wiederauflagen früherer Archiv-Bände, die bei Transpress Stuttgart aufgelegt wurden, im Ursprung aber noch auf DDR-Publikationen zurückgehen.

Zusammen mit den bereits vorgestellten Bänden zu Dampflokomotiven und Reisezugwagen bilden sie ein zusammenhängendes Verzeichnis, das noch durch zwei Bände zu Triebzügen unterschiedlicher Antriebsarten vervollständigt wird. Als Nachschlagewerke und Eisenbahnfahrzeug-Lexika dürften sie alle zusammen Eingang in viele Bücherregale finden.

Gründe für die Wiederauflagen in der Classics-Reihe dürften gute Preise von antiquarischen Büchern sein, denn sehr lange Zeit waren diese Titel vergriffen. Die Einordnung als Klassiker stellt dann auch das einzige Manko dar, dass nach den Reisezugwagen auch für die heute vorgestellten Titel gilt: Mangels Überarbeitung und Aktualisierung ist hier ein rund 30 Jahre alter Wissensstand abgedruckt. Geblieben sind damit auch der strukturelle Aufbau und die verwendeten Aufnahmen wie auch Zeichnungen.

Unverändert sind damit sowohl Stärken als auch Schwächen der Bücher. Beim Band zu den Elektro- wie auch Diesellokomotiven freuen wir uns über Einleitungen zur Entwicklung dieser Traktionsarten. Das hilft dem Leser, die im Folgenden chronologisch abgearbeiteten Fahrzeuge einzuordnen. Dies umfasst sowohl Einzelgänger als auch Serienlokomotiven.

Dabei sind uns keine Lücken aufgefallen. Offenbar wurde alles, was auch nur kurzzeitig ein Staatsbahnlogo getragen hat, hier berücksichtigt. Bei den Diesellokomotiven umfasst das immerhin knapp 60 Groß- und 16 Kleinlokbauereihen. Sie alle sind mit Beschreibung der Entwicklungsgeschichte, Betriebsbewahrung, konstruktiven Merkmalen, Fotos und Maßskizzen wiederzufinden. Tabellarische Anhänge mit Daten runden diese Ausführungen ab.

Die beiden Güterwagenbände hinterlassen in Teilen ein abweichendes Bild. Hier ist zum einen der geschichtliche Rahmen deutlich weiter gespannt und setzt bereits in der Länderbahnzeit an. Ein direkter Vergleich mit der umfassenden Güterwagen-Buchreihe von Stefan Carstens führt dem Leser aber auch vor Augen, wie komprimiert diese Inhalte sind.

Das bezieht sich meist weniger auf die erläuterten Inhalte als vielmehr auf die spürbar geringere Auflistung an Bauarten. Es wäre müßig, hier eine „Fehlbestandsliste“ anführen zu wollen. Vertreten sind immerhin alle wichtigen Bauarten, nur nicht mit jeder speziellen Gattung.

So weist der zweite Band z. B. einen direkten Sprung vom dreiständigen Behältertragwagen BTs 50 zum fünfständigen BTmms 598 auf, die Bauart für vier Mittelcontainer (MC) fehlt völlig. Die Abhandlung endet etwa 1989, also noch vor dem Überführen von DR und DB in eine privatrechtliche Gesellschaft, was im Rückblick etwas unglücklich ist.

Trotzdem reicht das Wiederabgedruckte für einen grundsätzlichen Überblick und auch Einstieg in einige Eigenbauprojekte. Es ist eine kostengünstigere Alternative zur großen Güterwagen-Reihe für jeden, der die beschriebenen Lücken in Kauf nehmen kann und mag. Auch mit der Wiederauflage des Güterwagen-Archivs ist folglich ein wichtiges Stück Wagenliteratur wieder verfügbar.

Verlagsseiten:  
<https://www.motorbuch.de>

Christmas greetings from the editorial team



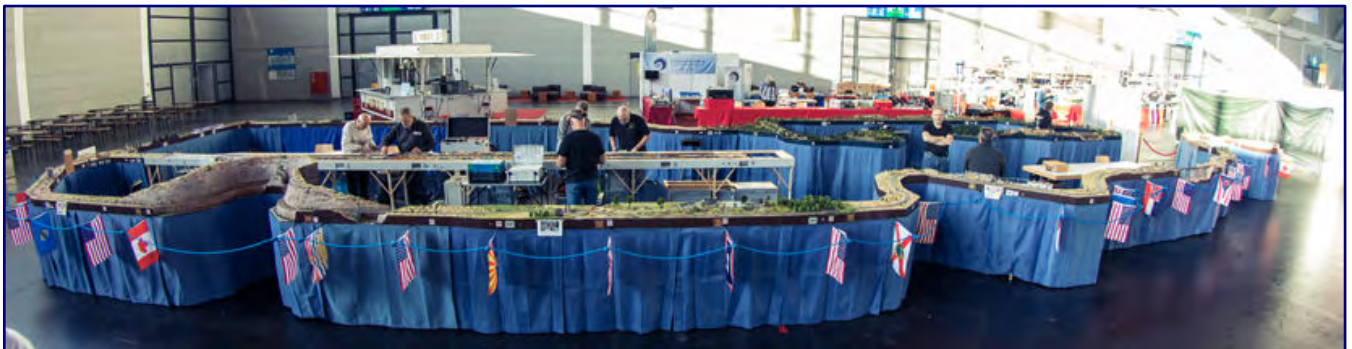


## Faszination Modellbau on Lake Constance **Growing Popularity of the Trade Fair**

*After a few years of distance and deprivation, interest in exhibitions is growing again. It is always exciting to see how this translates into visitor numbers at different venues. The fascination of modelling has regained a foothold, but obviously offers very different things to different age groups.*

By Stephan Fuchs and Joachim Ritter. Around 46,000 visitors came to Faszination Modellbau in Friedrichshafen from 3 to 5 November 2023. This exceeded the previous year's result, which indicates a high level of interest following the end of the coronavirus pandemic and the associated fears.

Overall, the public's behaviour at this trade fair has normalised to a slightly lower level than before the forced break. After all, there no longer seems to be any fear of contact, especially as face masks were only seen very infrequently.



The dominant spatial element of the model railway hall was the large US modular layout in Z gauge, which must have impressed all visitors without exception. Photo: Stephan Fuchs

Saturday was clearly the day with the most visitors, although at least the stand of Z-Freunde International e.V. was also well frequented on Friday. However, there could have been a few more visitors. The number of visitors in the halls on Sunday remained very subdued for a long time, but they only emptied considerably later in the afternoon as the trade fair drew to a close.

After the change in the hall layout concept and significantly more space for the model railway, the "escape route concept with very wide aisles seemed well thought out again. What always works well at Lake Constance are the connecting trips to and from the railway station in the city and at the harbour. This secures the catchment area for the entire tri-border region.

The other fair offers could still be seen briefly: The all-terrain vehicles, the drone flight demonstrations and the pool with ship demonstrations were densely packed. In the nautical area, this popularity even seemed to exceed that of the model railway, which is usually the main attraction.

There was also some activity in the clamp blocks hall, although it was below personal expectations at the time of the visit. The real steam meeting, on the other hand, was really well attended, with the public trains in particular being very busy.

continues on page 42



The drone flights (photo above) were always well attended. Alongside the ship demonstrations and the RC all-terrain vehicles, they were a clear visitor magnet. The real steam meeting (photo below) was also very popular, with many unusual locomotives of different, but consistently large, sizes making a loud noise. Visitor rides were also offered and gratefully accepted.





Hands-on activities such as the BDEF's Junior College have established themselves as an integral part of the trade fair.

In the following, however, we will focus our tour of the hall primarily on the model railway area (Hall 1). It's always great to see that the hands-on activities for the little ones at Märklin and Piko are very popular. And even mum, dad or grandparents can crawl on the carpet and join in the fun.

As far as the number of layouts on display is concerned, we have no comparative data for the previous edition. Personally, however, we got the impression that there were probably more than last year, when the fair was labelled "International Model Railway Exhibition". This year's quality level was definitely higher.

Z. It was impossible to miss due to its huge dimensions, and probably impressed every observer. On Saturday, the longest train took up a whopping 11.80 metres of track!

A highlight across all gauges was of course the America module layout in nominal size



The US modular layout offered many exciting views and even more, sometimes seemingly endless goods trains, which were constantly followed by the eyes of the visitors. Photo: Stephan Fuchs

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The variety of landscapes in the vastness of the United States can also be found on the large modular layout. This offered the trade fair visitors a great variety that the larger scales cannot provide.



Visitors were able to count six locomotives and a good 100 carriages, and those who took the trouble to do the maths came up with a prototypical length of 2.5 km. And the prototypical slow movement of the trains was also well received once the operation had been explained.

While with digital control systems the layout operators very often sit at some monitors and run a programme, it has to be said that this no longer has much to do with actual model railway operation.



Big Boy and Challenger were also to be found with heavy goods trains on the winding routes across gorges and mountains. Photo: Stephan Fuchs

At the USA layout, on the other hand, the operators combined the digital advantages with an analogue driving experience. Each participant had to control their own permanently assigned train. The mobile control devices can be connected at any point in the layout via network sockets and thus move along with the moving train.

Communication is then necessary in bottlenecks, which also benefits the shared experience. However, the observations summarised here were not exclusively reserved for Z scale. The Odenwald layout in H0 scale was also demonstrated in the same way.

### Light and shadow

What we generally found annoying – unfortunately not for the first time: Many layouts were very high and there were only a few steps for children. This can quickly put young model railway enthusiasts off attending such a fair, because even the youngest children want to see more than the legs of adults!

And lifting up the hopefuls of our hobby every time gets on the nerves of even the most patient parents after a short time. There is still a lot of room for improvement and it remains inexplicable to us why the

**Comment from editor-in-chief Holger Späing:**

"The Stammtisch Radolfzell presented itself with a large, blue tablecloth", it flashed through my mind. That was the moment when I saw the photos that my colleagues from Faszination Modellbau had brought with them.

Admittedly, that sounds harsh, and yet it is not meant in any way maliciously. The zeal with which these Zetties go about their work deserves recognition and praise. However, it is not without reason that we often say that first impressions count.

And unfortunately, it consisted of a huge table covered with a blue tablecloth. I'm recording this here because that was definitely not what was wanted. However, many other visitors will probably have felt the same in passing, which is why I have to discuss it here.

And my aim is not to simply swap the colour for a more inconspicuous, neutral one in future. What I'm hoping for is a presentation of our scale that really stands out in a positive sense.

Just as no one was able to avoid the giant US-style layout, the same should apply to all other exhibits. To achieve this, they must be easily recognisable and captivate visitors.

The tiny layouts are a great idea for the club meeting. Like their counterparts at the breakfast table, they should take up little space and make themselves useful. But at trade fairs they go unnoticed, are overlooked by many and are thankless objects for us photographers because their motifs cannot be depicted to fill the photo due to a lack of depth.

Less is more! In this case, however, this explicitly does not refer to the surface area of such a board, but to the proportion of exhibits shown and the surface area used. In Friedrichshafen, it was clearly too much...

basic needs of this target group are consistently ignored despite the many demands made on youngsters. Outside of Germany, there are many good examples of how this can be tackled sensibly.

We are somewhat ambivalent about the presentation of the Radolfzell Stammtisch's board layout. "Well meant" is unfortunately not the same as well done, the composition was simply lost on many trade fair visitors and was often not even noticed in passing.

It is understandable that this young group wanted to present itself to the general public for the first time and was well received by the Zetties. Within the scene, the demand from visitors to the Ravensburger Brett-

chen was also quite high. This report is accompanied by a commentary on the facts, which hopefully explains better where we see a problem.



In its home region, the Stammtisch (Club) Radolfzell met with the expected, noticeable interest. However, inattentive trade fair visitors hardly noticed it in view of its tiny showpieces, which were lost on the table surface. We therefore deliberately chose a very flattering perspective in order to expressly honour their commitment.

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The exhibition favourites of our trade fair reporter Stephan Fuchs also included the “Berk’sche Bimmelbahn” TT-gauge layout from Weimar (photo above) and the “Odenwald” layout in H0 scale (photo below). Both photos: Stephan Fuchs





The miniature wonderland as a model (photo above) is called Mikroland and could also be experienced on site. Meanwhile, Gerhard enchanted visitors with a new snow diorama on a scale of 1:220 (photo below). Photo (below): Stephan Fuchs



Oliver Kessler had to answer a lot of questions, and his self-propelled models provided many aha-effects. The hands-on activities organised by Michael Bernhard and Sylvester Schmidt (in a display case) also made eyes light up, but in this case for children of all ages.



Good advice doesn't have to be expensive: There was always a demand for valuable information at the ZFI stand. In the foreground, looking down, you can see Oliver Kessler, who amazed many trade fair visitors with his Z-Car system, demonstrating what is possible in miniaturisation today.

Erwin Kohout's extremely popular Mikroland also proved to be a stroke of luck. This man is „insane“ in the proverbial sense of the word and left one or two faces in awe and amazement. His way of answering questions was also very well received. It was a real enrichment at the ZFI stand.

Roland Kimmich was also present with the “hawker's tray” of his remaining Railex stock. A few metres away, Alois Forstner was bombarded with questions from visitors. His camera car finally provided exciting views of his Munich layout that would have been impossible to capture with your own eyes.

The Rhosel also attracted attention and some people spent a long time wondering where this river could be found in Germany. At least Jürgen Wagner has clarified the facts, if this may have been necessary in some cases.

We noticed that the track construction site at the tunnel entrance on the left-hand side of the layout has not made any progress this year either, at least in view of the present, this seems very close to the model for a large state-owned company!



Fortunately, no construction progress could be seen at the track construction site on the left edge of the layout "Entlang der Rhosel" (Jürgen Wagner).

On the whole, we did not notice any new products, which is not surprising given the time of year. One exception is the Wismar rail bus at the Märklin stand, as the Insider Club model for the following year is usually announced at this location. In 2023, the display case was even adorned with a very detailed sample from pre-spraying.

**Exhibitor selection with Z gauge relevance:**

<https://amw.huebsch.at>  
<https://artitec.nl>  
<https://www.busch-model.com>

<https://www.eisenbahn-kurier.de>  
<https://www.esu.eu>  
<https://www.faller.de>

<https://www.luetke-modellbahn.de>  
<http://www.mazero.de>  
<https://eshop.microrama.eu>

<https://www.noch.de>  
<http://www.peter-post-werkzeuge.de>  
<https://scenery.shop>

<https://www.uhlenbrock.de/>  
<https://www.vgbahn.shop>  
<https://viessmann-modell.com>  
<https://www.z-freunde-international.de>



Hobby Show in Duisburg

## Model Railroading Day

***2 December 2023 was once again Model Railroading Day and very fortunately the date fell on the weekend of the first Advent. However, many events were also held in the days before and after to draw attention to our hobby. We chose the hobby show at the Duisburg Inland Navigation Museum as a successful example.***

Up and down the country, and probably no longer just in Germany, 2 December has established itself every year as an official model railway holiday. It was proposed by television presenter Hagen von Ortlhoff when he retired.

Manufacturers, associations, clubs and independent model railway enthusiasts were happy to take up this idea and have been bringing it to life ever since. It is impossible to summarise all officially registered events here in one report. They are supplemented by many others that are organised for this occasion, but are not registered.



Hard to beat in terms of atmosphere: "Black Swan" 10 001 in an appealing museum setting on the track 1 arrangement by the duo Viernich & Obschruff, which delighted visitors in the basement of the Herrenbad.

On behalf of everyone, we visited the Hobby Show in Duisburg, which has been held at the same venue every year since 2017 and has thus already become something of a tradition. It was only interrupted in between by the contact restrictions imposed by the coronavirus pandemic.

Organiser Markus Schiavo, a “nine” (for N-gauge enthusiasts), as he calls himself, approached clubs and people in the local area and beyond to get them on board with his idea. The exhibition soon had to move out of the now leased premises of a pub and into the museum itself.

His exhibition has long occupied the entire museum of German inland navigation, with model-making exhibits alternating with ships and demonstration models from the exhibition. The number of exhibitors and visitors is also constantly increasing. The model railway was joined by friends of RC ship and car modelling.



When the Model Railways Day is celebrated at the Inland Navigation Museum, then of course the nautical faction should not be missing. This model of the DGzRS rescue cruiser “Berlin” was therefore also on display at the Stapelberg & Foschepoth stand.

The number of visitors this year just scraped the thousand mark. So, it's a success story that we don't want to ignore either, which is why we planned a visit to the event early on.

We would now like to honour and summarise what was shown with a brief overview and photo series. What cannot be shown here will be supplemented with moving images in episode 22 of **Trainini TV**, which was published almost simultaneously. In this way, the broad spectrum that could be offered from gauge 1 through H0 and N to Z gauge becomes visible.

Fire brigade modelling was present for the first time, represented by the FW Duisburg modelling group. But other sectors also managed without a railway, including paper modelling, clamp building blocks and the exhibits of IG Modellbau Duisburg.

continues on page 54





There is a lot going on at the “small northern harbour” (photo above): Diesel locomotives shunt on the track of the harbour railway, but the ships also sail, dock and then leave again after loading – moved, as if, by magic. Display models, but no less beautiful to look at, are the Köf, mine and field railway models from IG-Modellbau Duisburg (photo below)





Bathed in sunlight, the water diorama by MW-Bastelstube (photo above) unfolds its full effect. Germania Figures, on the other hand, had set its sights on the Stone Age for its diorama (photo below).



This colourful photo was rounded off by paintings, drawings and watercolours from Tina's studio and the wonderful photographs from the Joachim Stöver photo gallery. It is therefore no exaggeration to describe the exhibition as uniquely diverse and highly individual in character.



The modular layout of the Essen interest group offered visitors idyllic branch line motifs (photo above), while Ralf Junius focused more on curiosities on his shuttle train box (photo below), as they are inevitably to be found at and in the amusement park.

Of course, we were particularly excited to see what would be on display in 1:220 scale. We were familiar with the exhibits announced by our editor Ralf Junius. These included, above all, an Advent wreath with a snow-covered landscape, as we were already celebrating the first Advent on the anniversary of the model railways.

He delighted the children with the “Strawberry Box,” on whose tiny oval a Hello Kitty 500 series Shinkansen made its rounds as a shorty. The rail bus was presented at eye level in the “PeZuBox”. This strange name stands for “shuttle train box” and also explains the feeder traffic of an amusement park shown on it.

Opposite was the long layout of the IG-DU e.V. in scale 1, which we mention because we know many of the Märklin vehicles shown there from our scale. However, our ears were captivated by the driving noise of an ETA 150, which we also eagerly await in our nominal size, even though it was not realised by the same manufacturer in 1:32 scale either.



In addition to the ETA 150 battery-powered railcar, which can be seen in our film report on [Trainini TV](#), the V 36 was also frequently used on the IG-DU e.V. modules for shunting and passenger services.

The Modellbahn-Union demonstration stand also attracted a lot of interest. A colliery ensemble from the company's own kits for N scale was shown there and it was also demonstrated how these are successfully assembled. The theme would also appeal to us in Z scale.

There were also demonstrations from Torsten Schubert, who explained and showed interested parties how small SMD components can be soldered by hand and integrated into models. The effect that can be achieved with this could be seen in abundance at his stand. Whether it was the many emergency vehicles or his funfair, it was flashing everywhere.

Star Wars and the preparation of two Transall aircraft from the German Air Force for a UN mission were also on display, although the lighting was quieter. A completely new showpiece, which is shown in its entirety in the Reliefhaus article, was dramatic. The theme here was a major fire brigade operation involving the rescue of people from a house fire.

continues on page 58





The mine diorama of Modellbahn-Union (photo above) in nominal scale N also whets the appetite for something comparable for our scale. Torsten Schubert focussed primarily on Z scale and had brought along his diorama on the subject of relief flights with the Transall C-160 (photo below).

Photos auf on page 57:

The tractors decorated with fairy lights (photo above) in H0 scale were well suited to the pre-Christmas period. André Kammels had chosen an idyllic landscape crossed by the main railway with an inn for resting (centre photo) for his new demonstration layout. Volker Bastek's "Kleinleben" (small life) (photo below) was analogue, but with digital switching. You can see what this means for completely computer-controlled operation on page 58 by looking underneath this layout.

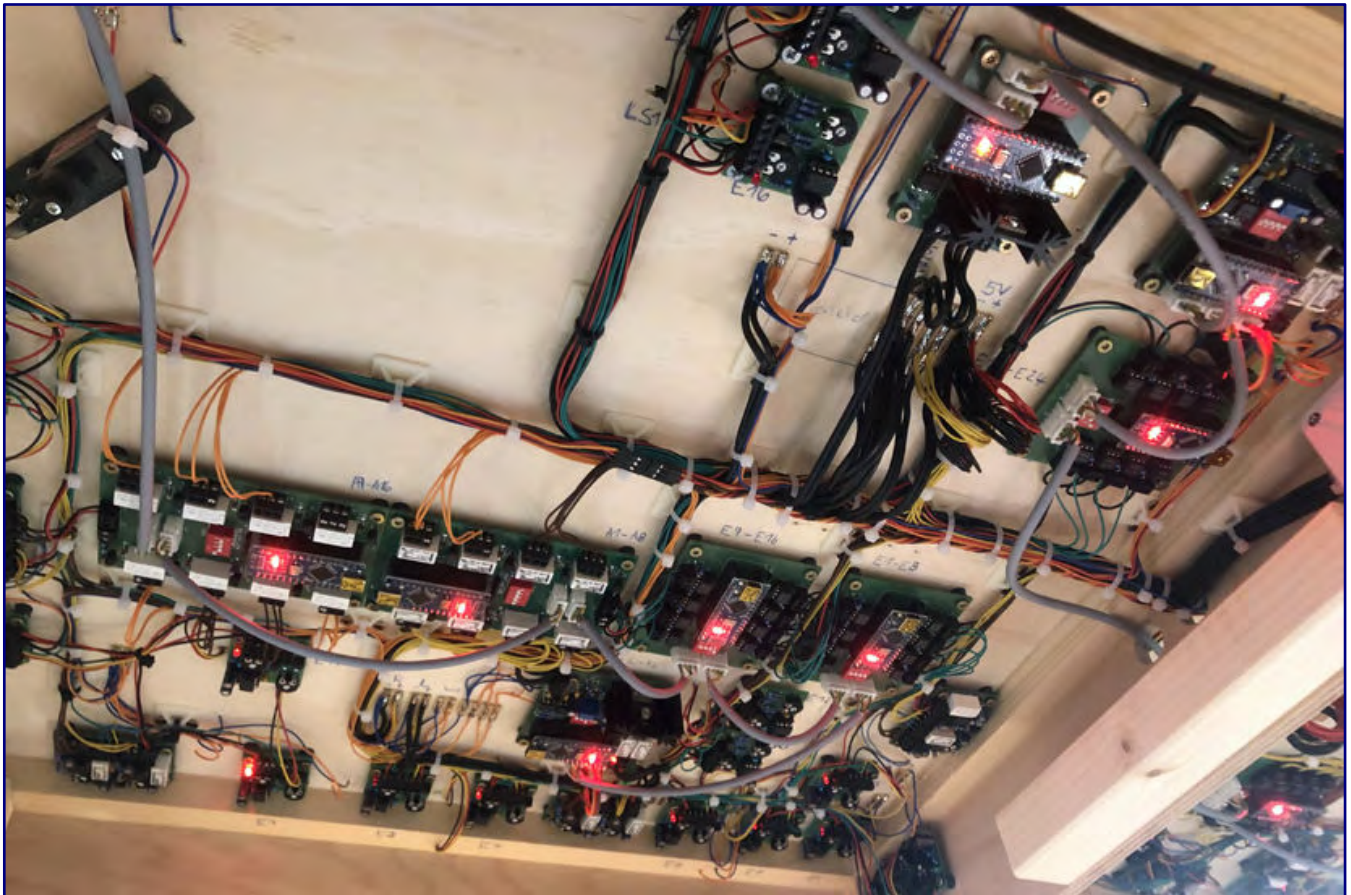






André Kammels presented his “green diorama”. This term refers to a railway layout with a double-track and electrified main line, which is embedded in a lot of greenery. Hikers can explore the great outdoors at various points and, of course, take a look at the passing trains or enjoy a bite to eat in the pub.

Volker Bastek was unable to complete the construction of his new “Kleinleben” layout, but this did not dampen his enthusiasm. Analogue driving but digital switching was his theme. The view of the landscape outside the station, which had not yet been designed, was compensated for by a view under the layout. There was a veritable sea of switching modules that could monitor and control many sections.



This is what it looks like under Volker Bastek’s “Kleinleben” layout. Very few visitors who observe the automatic train changing operation will realise this.

In fully automatic mode, the trains changed trains, met in the station and then left in the same direction or in the opposite direction. Shunting operations were also shown. How this could run so safely without human intervention could only be explained by the view from below, which would remain hidden to most visitors without a hint.

The trio from the Southern Pacific interest group from Duisburg-Neudorf offered an appealing contrast. They presented “Baroch Steel” as their third H0 gauge exhibition layout, which did not focus on steel production, but above all on the repair of the factory railway vehicles.

And, so, we draw a good conclusion: this day passed far too quickly, as it was able to cater for many interests, which is why nobody was bored. Parents with children got their money’s worth just as much as





The “Baroch Steel” layout, built on a scale of 1:87, celebrated its exhibition premiere with its successful industrial patina and will therefore be presented with two motifs at the end.

“old hands”. Newcomers and those interested found great suggestions and ideas for their start in their new hobby.



And so almost all exhibitors will probably be there again next year, and we won't be left out either. And it is just as likely that the popularity will continue to grow and perhaps break the thousand mark, as early as 2024!

Official pages with recorded data:  
<https://www.tag-der-modelleisenbahn.de>

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## Advertising



*Come and visit us again on*

# Trainini TV

**Trainini TV**  
Folge 22

New episode 22  
„Model Railroading Day“  
(German) available soon at

<https://www.youtube.com/TraininiTV>

Readers' letters and messages

## Zetties and Trainini in Dialogue

***Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.***

Readers' report from Railhobby Bremen:

After a long time, I was (...) again (...) at the model fair in Bremen. The hall was about half filled with dealers and half with layouts. The layouts were all of good quality or were deliberately "nostalgic", e.g. with Märklin metal track. (...)

A large modular layout was even shown from Italy. Others limited themselves to finely detailed "peep boxes". The dealers offered new items (presumably at RRP) as well as used items with an astonishing range of prices. (...)



The "Hankenberge" peep box layout in Z gauge was one of the highlights of Railhobby Bremen for our reader. Photo: Jörg Endreß

"Hankenberge" is based on a model railway station – the builder showed me photos on his mobile phone. The road vehicles, buildings, platform edge and even the trees were 3D printed. The highlight is the points with movable lantern that can be operated with a hand lever. Two trains run in two-way traffic.

Otherwise, the Z-Stammtisch had on display well-known layouts, where the detailing at "Sägethal" seems to go on and on. A "fire cock" with a tiny blue LED is really great. A layout still under construction bears the same signature. The 1zu220-Shop has made another layout available on loan.

Jörg Endreß, Bremen





**New editor from 4 December 2023:**

Since 4 December 2023, our editorial team has grown to five members. Ralf Junius, who has been in charge of our **Trainini TV** channel for some time and has already worked as an editor in this context, has joined us in this role.

With the unanimous decision of the editorial team, we want to optimise the exchange between the magazine and the film channel and more closely interlink the editorial work between the two media. In addition, it is easier to make decisions in a group with an uneven number of people in the event of a tie vote.

We are delighted about the new addition to the Board and would like to congratulate Ralf. We are looking forward to working together even more intensively. We are confident that we have also strengthened our external presence and positioned ourselves for the future.

**News from FR Freudenreich Feinwerktechnik:**

As Harald Thom-Freudenreich had already clearly indicated several times that he would be retiring at the end of 2023, we have since received several questions from readers as to whether and how his company and its programme will continue.

In the meantime, the future retiree has announced that a successor has been found, which has already taken off, as can be seen, for example, in the exclusive programme of the 1zu220-Shop.

Although the previous production site in Sanitz will be closed at the turn of 2023/24, this does not mean the end of FR Freudenreich Feinwerktechnik. Long-time employee Heike Schuster, who is very familiar with the production of model railway carriages, will produce special series for dealers and Z-gauge regulars' tables over the next few years.

However, it does not usually supply private customers, especially as it is fully booked with orders well into the coming year. Recently, special models for the 1zu220-Shop and anniversary editions of the Stammtisch Untereschbach have already been produced independently under the new name "Montageservice Uwe Schuster", which continues to use the FR logo.

But Harald Thom-Freudenreich doesn't want to call it a day. He is currently moving some of the technology from the previous workshop in Sanitz to the workshop at home and getting it up and running again. On a very modest scale, he will then continue with the design and production of small model series.

The prepared model of the Norwegian EL-16 is already waiting for production in 2024. Its body is a 3D design by Detlev Stahl, which is to be reproduced in a vacuum casting resin process after appropriate preparation. The chassis is largely based on the Rc locomotive, following the prototype.



The model of the Norwegian EL-16 electric locomotive will be the first project to be completed in the private workshop. Photo: Harald Thom-Freudenreich

A number of new and reissued wagons are also planned, and the website will be updated accordingly in December 2023 or January 2024. However, due to personal events, the ramp-up is unlikely to proceed as smoothly as originally planned.

#### **Innovative supply chain through Azar Models:**

Azar Models is taking a completely new approach in the world of Z gauge and launched a crowdfunding campaign on 23 November 2023 (with a duration of two months) for the production of a TGV for the Z nominal scale using injection moulding technology.

Pre-orderers participate in the costs for the start of production by granting an immediately due credit and in return receive a self-selected configuration of models at a special price. This is managed on the Ulule platform at the following address:

<https://ulule.com/tgv-echelle-z/>

If the financial target is not achieved in this way, the investment project would fail and the participant would be refunded his investment. The risks for the prospective buyer therefore remain very manageable compared to a direct advance payment and for the young provider it is a good way to minimise the entrepreneurial risk.

What can miniature TGV fans expect at this point? The TGV Paris - Sud-Est is available in two versions. In addition to the harmonious and final Atlantic livery, the train with the number 16, which set the world speed record back in 1981, is also being produced on the platform in a unique and exclusive edition.

Azar Models sees its decision as an opportunity to showcase technical expertise and innovation in Z scale, in addition to offering one of the most formative trains for French high-speed transport.

These trains include interior fittings and lighting to make them look as close as possible to the prototype. The plastic injection moulding and painting as industrial production standards should benefit the entire product line. The TGVs are also available in analogue and digital versions.





A 3D printed model of the TGV was on display in Altenbeken back in May 2022. As a seven-part world record unit, it is being offered exclusively to supporters of the financing project. Switching on the Ulule platform reduces the credit risk for interested parties.

#### A delivery from Micro-Trains:

We would like to briefly enhance the only new wagon at MTL (<https://www.micro-trains.com>) in December with some prototype information: The replica 36-foot refrigerated wagon (item no. 518 00 860) was built in 1927 and had wooden outer walls. Its advertising is for W&W eggs.

Behind it were Frank Wescott and Ben Winks, who had already joined forces in 1903 to form “Wescott & Winks Packers and Shippers” in Sumner (Iowa). The company specialised in products such as butter, eggs, poultry and various types of grain, which were transported in wagons like the one reproduced here.



Refrigerated wagon from Wescott & Winks (item no. 518 00 860). Photo: Micro-Trains

#### Exciting facts about St Nicholas Day:

Just in time for St Nicholas' Day, the Modellbahn-Union was able to present two new products for us Zetties. Firstly, modelling and conversion enthusiasts will be pleased to know that the brochure “Offene Wagen (2)” (English: “Open wagons, part 2”) by Stefan Carstens is now available.

At the same time, a new semi-relief building from our own production was released especially for the Z nominal size. With a grey façade, the corner town house at a 45° angle (item no. MU-Z-H00194) now enhances the in-house range. Like the buildings in the kit series released to date, this one is also made of solid-coloured, hard cardboard.

Volume 10 of Stefan Carstens' freight wagon series is expected to be published in the 1st quarter of 2024 with the topic “Low-loader and carrying beak wagons”. 256 pages (including fold-out pages) with around 700 photos as well as 50 drawings and facsimiles are to be expected from the best expert in this specialised field.

**Transfer crane new at Modellbau Laffont:**

The new dock leveller (item no. Z8001) from Modellbau Laffont (<https://modellbau-laffont.com>) is somewhat “idiosyncratic”, but unmistakable and impossible to overlook. With its integrated crane runway, it seems well suited to giving a layout a deliberately individual touch.



The new dock leveller (item no. Z8001) stands out clearly from the familiar models. The N-gauge model of the kit is shown here as an example. Photo: Modellbau Laffont

Up to three tracks can be laid underneath, as the dimensions of the passageway are 67 x 38 mm (width x height). The external dimensions of this light grey building made of solid-coloured architectural hard cardboard are 80 x 17 x 52 mm.

**New wagon series at WDW Full Throttle:**

The Alaska Railroad (ARR) is owned by the US state and stretches from the coastal and harbour towns of Seward and Whittier in the south via Anchorage to Fairbanks and further inland.

At the beginning of the 20th century, the 70-tonne open wagons gradually replaced the smaller versions. The prototype of the model was even fitted with stockpile shields at the ends to prevent loaded coal from falling out. These workhorses still transport the valuable raw material through the region today.



The new item presented today (item no. FT-5603) marks the start of a new 5600 wagon series from the manufacturer. The prototypes are 40 feet long and have three outlet funnels on the wagon floor. The car bodies have external box supports. There is a choice of two double packs with different car numbers.



On the box ends of the new wagon (item no. FT-5603), the elevations are visible, which are intended to prevent the load from falling out. Photo: WDW Full Throttle

You can find the supplier webpages at <http://www.wdwfullthrottle.com>.

**Railway and modelling enthusiasts at the International Spielwarenmesse (International Toy Fair Nürnberg (Nuremberg)):**

On the Saturday of the International Spielwarenmesse (International Toy Fair Nürnberg (Nuremberg)): (3 February 2024), the doors of Hall 7A of the Nuremberg Exhibition Centre will once again open to interested end consumers. Advance ticket sales began on 2 December 2023.

Adults and children aged six and over will only be granted special admission to Hall 7A, which will be open from 10:00 to 17:00 on the Saturday of the fair. For the first time, exhibitors will also be permitted to sell to private customers on this day.

Otherwise, visits to the Spielwarenmesse are reserved exclusively for trade visitors, so an exception applies here, which was resumed in 2023 after an earlier test. Due to the positive response, it will now be continued in 2024.

According to the trade fair organisers, almost 100 companies have registered so far, which is around a third more than last year. The manufacturers Busch, Faller, Herpa, Märklin, Noch, Preiser and Viessmann represented in Hall 7A are relevant for Z gauge. They will also have an action area where various new products will be exhibited. All information and a hall plan can be found at <https://www.spielwarenmesse.de/openday>.

**The Trainini Fotokalender 2024 has been published:**

The **Trainini Fotokalender 2024** has finally been available since Model Railroading Day. It can be printed out individually up to DIN A3 landscape format and hung up in a nice place. Then it will hopefully accompany you through another exciting model railway year.

For the cover, we have chosen the Shinkansen Series 0 as "Hikari Number 1", which we presented in detail in the last issue and which will also be the subject of the new episode 21 of **Trainini TV**.

The following twelve monthly motifs cater to every taste, as we have selected as many eras, railway administrations and tractions as possible to depict a colourful photo of Z gauge. You can find our range under the following link: Homepage → Magazine → Photo calendar.

#### Christmas scents at Yellow Dwarf:

You can almost smell the scent of Christmas here: Yellow Dwarf (<https://www.yellowdwarf.eu>) has presented new products for December, which can be used to create a typical Christmas market as a model, which will certainly enchant the viewer.



The Christmas market nativity scene (item no. 60605; photo left) and the Christmas tree kiosk (60607; photo right) are two of the Czech accessory supplier's seasonal innovations. Photos: Yellow Dwarf

New are two different Christmas / market stalls (item no. 60601 & 60602), a typical Christmas market cot (60605) and a Christmas tree kiosk (60607). The Christmas novelties are complemented by a fibre optic cable (without LED) for Christmas lighting (60603).

#### Freshly arrived from Artitec:

Shortly before the editorial deadline, two new products from Artitec arrived at the dealers, which we are not yet able to document with our own photos at this point. We will do this as soon as we can. Customers can look forward to two finely detailed and elaborately coloured models made from resin.

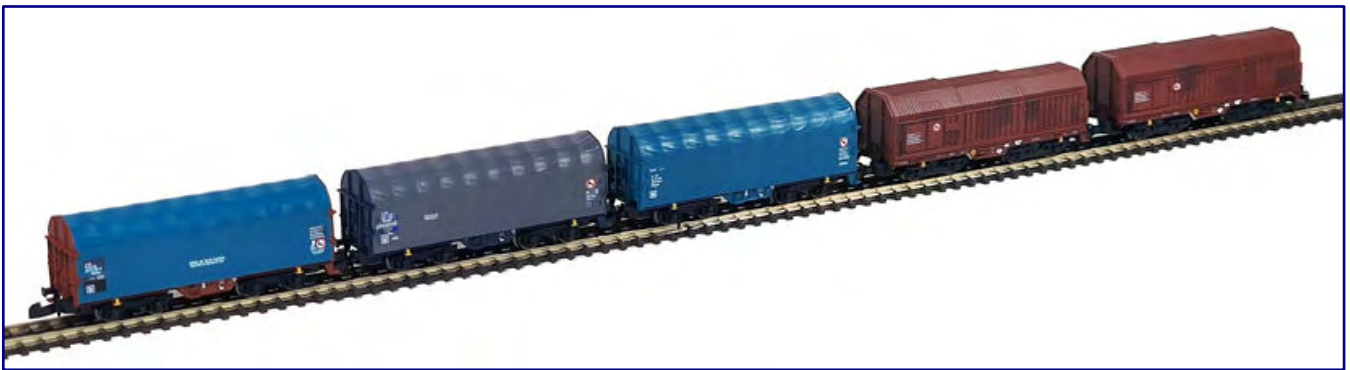
After a long wait, a 7-tonne forklift truck (item no. 322.039) and the open Hanomag K5 crawler loader (322.040) have become available. Even the manufacturer's photos of the first samples aroused great curiosity in order to be able to effectively equip various scenes.

#### Märklin deliveries modest in December:

Märklin's pre-Christmas deliveries are modest. Dealers are once again concerned that a large number of new products will only be offered after the holidays and thus the end of the Christmas business, which cannot be sold in a timely manner.

The "Modern Freight Transport" starter set with a class 285 diesel locomotive (item no. 81875) is important for newcomers. This is the first time that a Rheincargo model has been realised on a scale of





Märklin is introducing a Rheincargo diesel locomotive for the first time with the new starter set (item no. 81875; photo above). The freight in the self-unloading wagon is part of the manufacturer's set, but its open version is correct. The SNCB freight wagon set (86358; photo below) offers the greatest possible variety, with the telescopic hood wagons even showing touch-up patches. Photos: Oleksiy Mark

1:220. In addition to track material and a speed controller, it is accompanied by three VTG freight wagons, including one each of the Eaos, Falns, and Shimmns classes for the Epoch VI.

The Falns self-unloading wagon (F = open wagon of special design) deserves an additional explanation: These wagons do not have hinged lids! On the product photos for the presentation and also on the packaging illustration it is unfortunately to be seen differently. The wagon shown belongs to the type Tad-u (T = covered wagon in special design), whose prototypes have been out of service for 40 years.

The Epoch VI package "SNCB Freight Cars" (86358) arrived as we went to press. With three short sliding tarpaulin wagons of the Shimmns type and two telescopic hood wagons of the Shimms type in current operating condition, it even brings some colour variety to the layout. The two bonnet wagons also have repair patches ex-works.

The real bronze models are enhanced by the SBB RE 4/4II electric locomotive (88597) with round lanterns at the head of the locomotive, which has also been delivered. At the time of going to press, the US diesel locomotive type E8A (88625) in Amtrak Phase 1 livery, manufactured in co-operation with AZL, had also been announced for delivery in the near future.

#### Current deliveries from Noch:

Fairy (Christmas) lights for various track gauges are one of the new products currently being supplied by Noch. One LED is attached to each centimetre of chain and they can be operated with both direct and alternating current of 16 V voltage. They are available with 10 (art. no. 51244) and 30 LEDs (51248). They are intended as a garland of lanterns for festivals, as stall lighting for the Christmas market, and much more.

The manufacturer sets a new quality standard with the Master Tree series. Each wire tree blank is constructed by hand from up to 40 individual wires. The thick trunk is twisted from all the wires, thick branches are created from a bundle of wires, thin branches and the finest ramifications from just a few wires.



The 15 cm high oak tree (item no. 20110) can also be used without hesitation as a solitary tree for Z gauge, as trees in all larger gauges are always clearly too small. Photo: Noch

A great advantage of these NOCH tree wire blanks is that they can easily be bent into shape afterwards. The tree skeleton is naturally coloured and flocked with different materials depending on the type of tree. As in nature, only the fine branches have foliage, while the stronger branches and the trunk are not flocked.

The trees that have now been delivered are intended for much larger gauges, but it seems worth taking a personal look at the dealer, as the scale conversion suggests that some trees such as the oak (20110, 15 cm), poplar (20130, 18 cm), pine with a height of 15 cm (20140) and 18 cm (20141), copper beech (20150, 15 cm) and spruce in sizes 19 cm (20190) and 22 cm (20191) can also be used in Z gauge.

#### **New moulds from AZL:**

A new model in the American Z Line programme is making a name for itself: Initially available in Union Pacific colours and lettering, the EMD SD40-2 diesel locomotive (item nos. 64200-1 to -3) is a bestseller in the prototype with almost 4,000 units. It is technically realised in the manner known from this manufacturer and with individual details typical of the railway.

These are the following features: Late production series locomotive, hazard lights on the cab roof, replicas of the (dynamic) brake, light shields over the side windows, high snow plough, the so-called "beacon light," 4,000-gallon fuel tank, Leslie S3LR compressed air horn, and the HTC bogies, radiator, fan, and muffler typical of the type, and front shape of the '88 design.



Shortly before the locomotive was released, a few freight cars also went on sale. Worth mentioning here is the wide-view goods train escort car in the red colour of the BNSF (921000-4 / -5) with two new road numbers as well as R-70-20 refrigerator cars for the Western Fruit Express of Burlington Northern, offered as a two-pack (914840-2) and four-pack (904810-2) with new road numbers.



New EMD SD40-2 diesel locomotive (item no. 64200-1) from Union Pacific. Photo: AZL / Ztrack

You can find manufacturer information at <https://www.americanzline.com>.

#### First announcements for the new year from Herpa:

Due to the long lead time for new product announcements from Herpa, which will soon be celebrating its 75th anniversary, we are already looking ahead to spring 2024. The aircraft programme also includes new products that may be of interest to model railway enthusiasts of our gauge.

With the Panavia Tornado ECR of the Italian Air Force (item no. 573078) and the Airbus A330-200 from Air France in new colours (572910), these are also two aircraft models from the Wings product range. The cardboard kit of an airport tower (573061), which fits on the apron plates (558969-001), which have already been offered in a similar form, also appears in this range as a new product mould.



The new airport tower (item no. 573061) can be seen here on the slightly modified reissued apron plates (558969-001). Photo: Herpa

Three Snapfit models will then also appear, simplified and realised in flight representation: Airbus A330-200 “Pride is in the Air” from Qantas (614061), a Boeing 787-9 Dreamliner from LOT Polish Airlines (614108) and the Airbus A320 “Salzburger Land” from Eurowings (614122).

#### Museum car 2024 presented:

Märklin traditionally presents the museum cars for the following year at a vernissage on the second Thursday in December. A delegation from our magazine was also present on 14 December 2023 and followed the presentations for you.

Managing Director Wolfrad Bächle gave an impressive introductory speech, paid tribute to last year's partners, announced a surprise and then introduced the sponsors of the new cars, emphasising the connection to his own company that was the reason for the partnership.



Wolfrad Bächle (2nd from right) presents the Märklineum's 2024 museum carriages at the opening together with the company and museum railway representatives.

While LGB honours “the Öchsle”, a well-known museum narrow-gauge railway, the new wagon partner for the Trix and Märklin brands, including Z gauge, is Carl Zeiss and is represented on site in Oberkochen.

Mr Bächle made a connection between the two cooperating companies by emphasising that Zeiss is synonymous with optics worldwide, and Märklin is synonymous with model railways. Carl Zeiss technology is also used in-house.

Dr Dietrich Imkamp, representative of the Zeiss company, echoed the words of the previous speaker and positioned his company as a quality brand. He also pointed out that a measuring device manufactured by his company had only recently been replaced at Märklin after 50 years of use!





This is how the museum car 2024 (item no. 80035) for Z scale looks, just like the other Märklin and Trix models dedicated to the Zeiss company.

He also told the anecdote that Zeiss had once developed a model railway itself and acquired patents for it. It had also been offered to Märklin. The following correspondence contained the result “favourably considered, but thankfully declined”. Now the two companies are coming together in the model railway sector after all.



In H0 scale, the CCS700 crocodile, which previously only existed as a hand sample, will appear as a replica model in January 2024. Production was once abandoned in favour of the CCS 800 model offered from 1947. Photo: Stephan Fuchs

For our gauge, the result is a covered goods wagon G1 11 (item no. 80035), used by the Deutsche Bundesbahn and labelled in accordance with Era III. It has an ivory-coloured paint scheme and advertising design by the Carl Zeiss company from Oberkochen.

It is packaged in a round tin box, as has traditionally been the case. However, this once again does not contain any extras such as a delivery vehicle designed to match the wagon. The 2024 museum wagons in all gauges are now available at the Märklineum and can be purchased during a visit there.

The surprise mentioned at the beginning of this report should also be briefly explained: It was announced that the “crocodile” CCS700 with a shortened running gear will be produced as a replica by Märklin. This locomotive had previously only existed as a prototype that never went into series production. This one-off and the first replica could now be seen side by side in the display case. Garnished with personal words, Mr Bächle also effectively advertised this new product.



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Publisher and Responsible in terms of German press law (V.i.S.d.P.) is Holger Späing, Am Rondell 119, 44319 Dortmund; Contact: 49 (0)231 9598 7867 or by e-mail to [redaktion\[at\]trainini.de](mailto:redaktion[at]trainini.de).

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