

Trainini

Full Issue

2024



International Edition

Free,
electronic magazine
for railroad enthusiasts
in the scale 1:220
and Prototype

www.trainini.eu

Published monthly
no guarantee

ISSN 2512-8035

Trainini

German Magazine for Z Gauge



A Festival in Zell (Moselle)

**Larry's Masterpiece
The Waysides**

Introduction

Dear readers,

Welcome to the year 2024! It's hard to believe that twelve editions are already behind us since I addressed similar or at least comparable words to you.

Z gauge is now a whole 52 years old; we are looking forward to another special weekend in Altenbeken in mid-May and before that we have our eyes on the many new products that are waiting for us.



Holger Späing
Editor-in-chief

But stop! Before we look too far into the future, let's pause for a moment. I would like to take another look at what lies behind us. Our time is fast-moving and all too soon we forget what extraordinary experiences we have had or were able to follow.

That's why we always start a new year with a look back. The only thing we deliberately postponed to February was the selection of best new products for 2023. Until close to the end of the year, it remained exciting to see whether any models would still be on sale that would need to be considered in the upcoming decisions. So, this time we're making it exciting...

So first, you can look forward to what this January edition promises. I am particularly pleased that we can report on the V 60 that Ronald "Larry" Schulz has realised for interested Zetties.

His work quickly took on a scale that came close to a new building. All that remained was the "coat" that Märklin had been supplying since 1972. And even that had to be adapted. We expressly wanted to present this effort before Märklin's new design goes on sale and possibly makes a marvellous achievement fade away.

A personal "topic of concern" was our annual focus for 2023. We in the editorial team thoroughly planned and compiled which aspects of landscape design should be highlighted here; in other words, which are important, but tend to be neglected and therefore receive too little attention in technical reports.

New products that were not delivered as planned have jeopardised our plans, and so, today, we are presenting what was originally planned as the final episode. This rounds everything off, but the two missing articles will follow as soon as we receive the products they are based on. We have gone to great lengths and broken new ground, and not in vain.

What remains to be mentioned at this point is the Advent meeting in Zell an der Mosel. After four years, this time it even had something very special about it, although it is always a personal highlight every two years, anyway. Our film is already online, and you shouldn't miss it either!

I hope you enjoy looking at and reading this edition, which also includes our two book suggestions and the latest news, including the first new releases. The big updates will come next month. So long!

Sin-Z-erely,

Holger Späing

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Date of publication of the German language version of this issue: 31 January 2024

Cover photo:

The rail bus crosses the level crossing on the outskirts of Wakenfeld. The small layout has almost everything that model railway enthusiasts could wish for in an atmospheric and lively layout. It not only fits in perfectly with our theme for the year, but was also on display in Zell (Moselle).

Old V 60 pimped out

Give it everything you've got!

How can the Bundesbahn's V 60 be described briefly and accurately? Small, robust, and an all-rounder. As such, it remained inconspicuous, because the great fame of an express locomotive was always denied to it. However, it was and is indispensable in modelling, which is why it is so popular. For this reason, the Märklin model from 1972, which was full of compromises, required many optimisations in order to meet all requirements.

“The smallest production railway in the world – Märklin Mini-Club” is how Z scale was introduced to the public in 1972 with an advertising campaign. The very first locomotive models included item number 8864 with the road number 260 417-1 – a triple-coupled diesel shunting locomotive that had been procured from the German Federal Railways as V 60.

However, customers were only able to purchase it as V 60 (with the road number V 60 107) in Z gauge in 2011. The fortieth anniversary of our nominal scale had almost been reached by then, but even for the 50th anniversary, the model had not changed significantly in technical terms, apart from a new generation of engines.



Ronald “Larry” Schulz: Neither the wheel diameters nor the wheelbases were correct. Despite the 5-pole motors installed later, the running characteristics could never be described as good. This was due to the rigid chassis without a floating axle.

Neither the wheel diameters nor the wheelbases were correct. Despite the 5-pole motors installed later, the running characteristics could never be described as good. This was due to the rigid chassis without a floating axle.

For many model railway enthusiasts, this meant having a highly sought-after locomotive type in the range, but one that was, at best, suitable as an entry-level model in a starter set, due to its poor running characteristics and very compromised implementation. However, this was to change in the end, but without any help from Märklin.

How it all began: 14 April 2020, Forum Z-Freunde International. Ronald "Larry" Schulz, a forum member, receives a request from another writer asking if he could make him coupling rods for the V 60. Larry, who loves filigree modelling as a hobby and always meticulously realises it, got himself a V 60, took measurements and constructed new coupling rods.



Larry in his home workshop: There are a few customer models of the V 60 on the workbench, which he is currently working on and rebuilding at the time of our visit.

The Z gauge community was thrilled. But he couldn't let go off this topic. He finally lowered the roof by 0.5 mm and thus already achieved a significantly improved effect of the locomotive. He then continued with the running gear. On 19 April 2020, just five days later, after eight hours of continuous tinkering, including construction and milling, he presented the prototype of a new running gear with a bell-shaped armature motor and dummy shaft: an incredible achievement!

In order for the housing to fit on the chassis again, it had to be milled out accordingly. But as it had not yet been decided which bell-type armature motor would be used, this had to wait for the time being.

In any case, the modified locomotive was to have a clear view of the driver's cab. Forum member Holger Hertlein helped a lot with the creation of the 3D models and created the file formats required for the cast and milled parts.

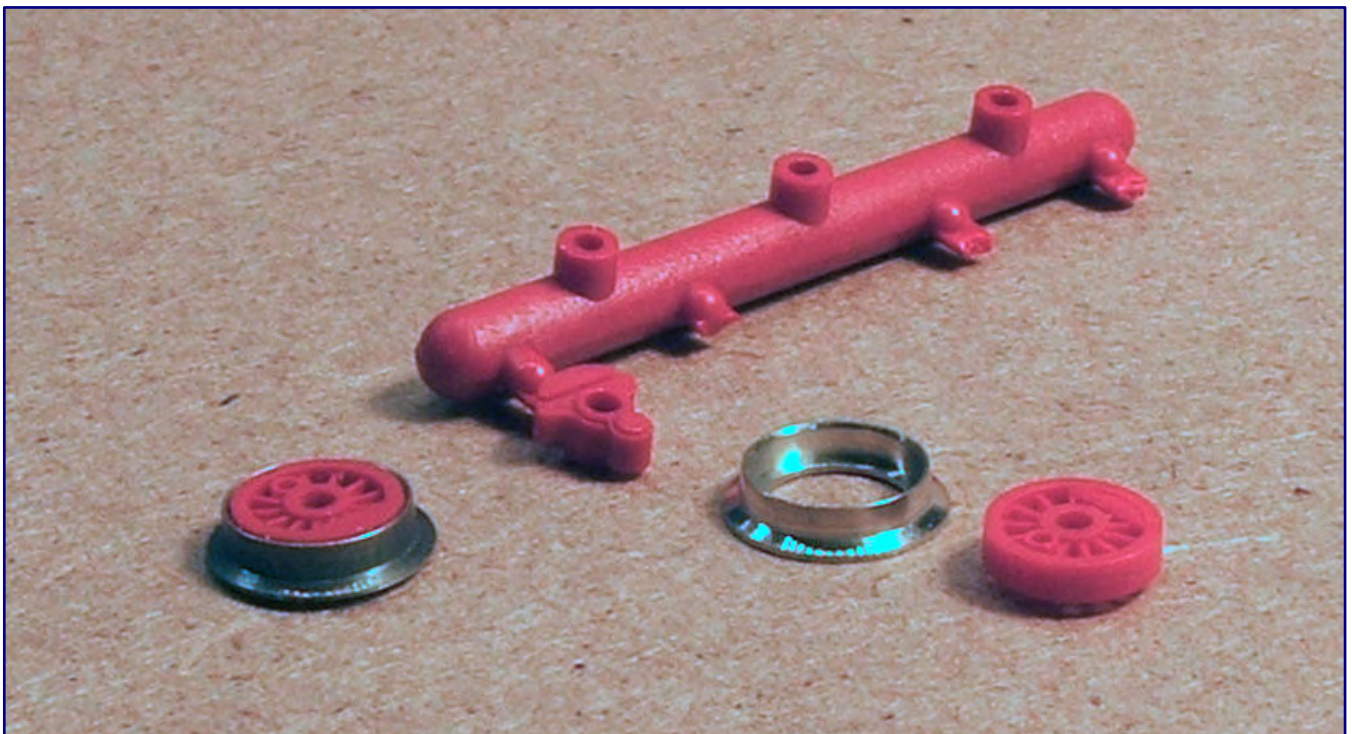
The wheels were also to be adapted, which was not an easy task, as was to become apparent, various wheel types from the Märklin programme were tested and those of the V 36 were found to be good. Of course, replicas of the brakes should not be missing from the new gearbox cover.

First test run

On 4 May 2020, the time had finally come and the first test run could take place. The project made great progress on the prototype for a planned short run. Larry posted the first test run (without rails) with a new gearbox and bell-shaped armature motor as a video on YouTube. Everything ran smoothly and evenly.

Two days later, the first test gearbox cover was ready. However, at this point it was still unclear how these parts could be produced in a series of 75 units.

A further milestone was reached on 11 May when the prototype was completed. Now it was time to find a suitable source for the series parts. The fact that Märklin did not want to supply any wheelsets was a major shock, as the project was about to be cancelled.



The wheel tread and the wheel flange are lathed parts, which are supplemented by plastic injection moulded wheel spiders. With the help of this manufacturing process, the V 60 finally also has a dummy shaft.

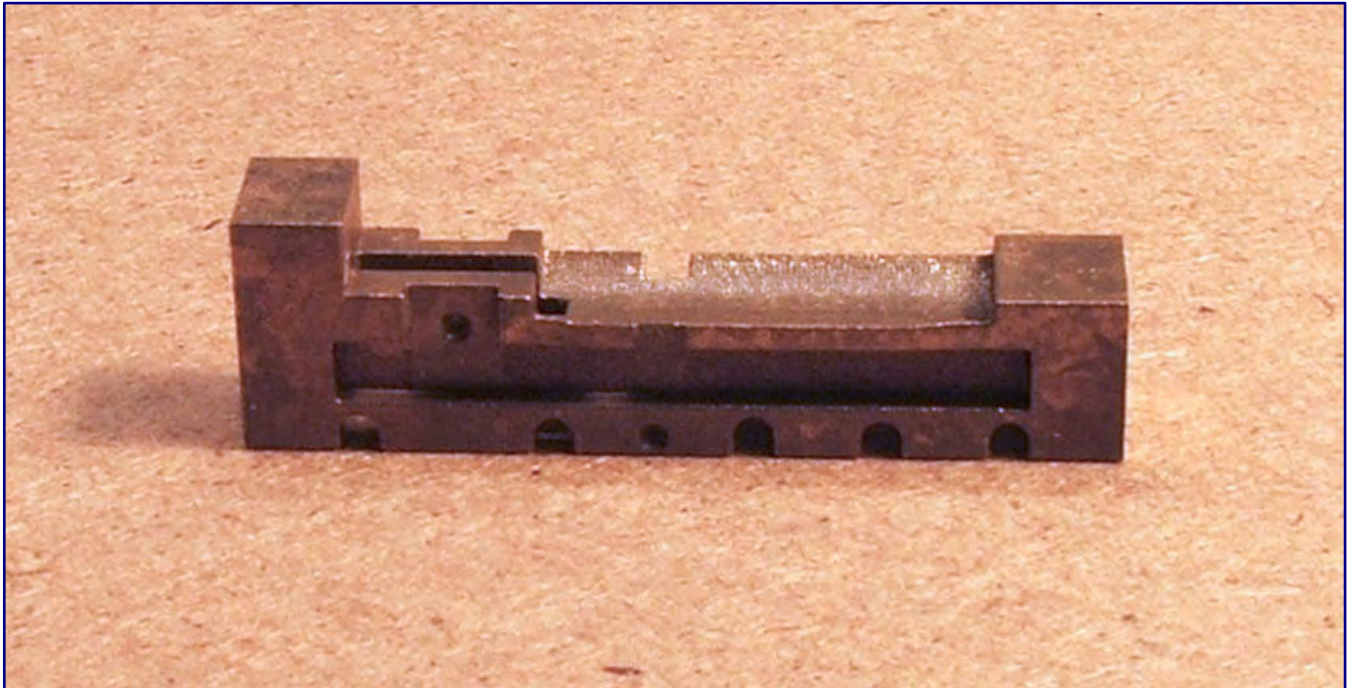
However, thinking positively also means seeing something good in this, because the wheelsets previously envisaged had module 0.3 and would not have been ideal for such a small locomotive. It therefore seemed better to produce the 300 wheelsets ourselves and to do what seemed technically possible here, too.

This was certainly no easy task, as suitable suppliers also had to be found. A model construction company in the Hanseatic city of Rostock finally produced the injection-moulded parts required for the wheel spiders.

Another cornerstone of this project was the subsequent familiarisation with a special manufacturing process for cast parts. The foundry contacted had printed a new wax core for each part to be cast, and then cast it in bronze, using the lost wax process.

This made it possible to produce very even surfaces. With conventional casting, on the other hand, the wax blanks are cast in a silicone mould. These wax tubes can then always become a little crooked when moulded.

This process is at best, of limited use, for gear parts. However, the foundry found after a long search was able to supply parts that could be used almost without reworking. This was almost sensational, the proverbial needle in the haystack seemed to have been found.



After a failure with the foundry, the gear block was made from CNC-machined parts, which were later completed with the end pieces with the buffer planks.

But then there were problems again. The owner of the foundry didn't get back to us and didn't deliver any of the ordered parts. At some point, almost unexpectedly, a large quantity of faulty parts arrived, but they were never invoiced.

Unfortunately, finding a new foundry was unsuccessful. In this situation, Michael Bahls (Bahls Modelleisenbahnen) helped with the crucial tip: If no cast parts are available, then the only thing that resolves the issue, is to have them milled.

The design was therefore changed and the actual gear block was CNC-milled from two parts. The two end pieces with the buffer planks were attached to this block as cast parts.

Of course, many helpers were needed to successfully complete such a project. These included Claudius Veit (Velmo), who drew and ordered the tiny circuit boards for the current collectors. And this even happened twice, because the first batch unfortunately turned out to be unusable.

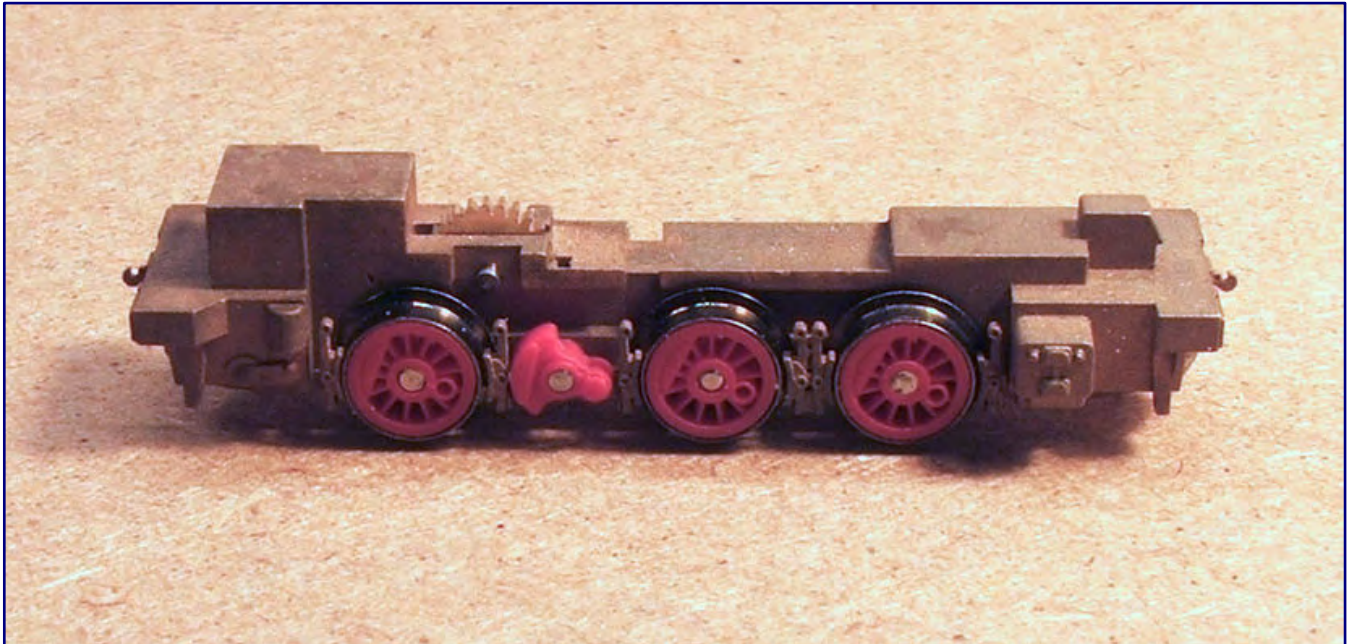
And Harald Freudenreich (ex Freudenreich Feinwerktechnik, now Atelier Barkvieren), an expert in the production of small series models, was always available to answer any new questions that arose. In the end, all hurdles were overcome and the exciting project, which had grown immensely, could be completed.

Successful conclusion

In November 2023, the time had finally come: the first “dream model” from the small series was ready and could be delivered to its customer. The enthusiasm was huge, as was to be expected after the previous descriptions.

The locomotive, which is almost completely new thanks to the many modifications, does not shy away from comparison with the Märklin model because it is “light years” ahead of it and, of course, finally has the required outstanding running characteristics. You can't really put a price on a model like this and Larry deserves a huge thank you for his insistence on making model railway dreams come true!

Larry wrote down for us just how complex the assembly of the V 60 is. We would like to reproduce his explanations in their original wording: “The assembly of the bogies is a little more complicated than expected.”



The photo of the test assembly shows the completed transmission block with the exception of the current collectors. A test for smooth running was carried out in this condition.

I always do a test assembly first. Not every gear works with every other gear. The tolerances are somewhat rough. You have to try a bit to make sure that everything runs smoothly afterwards.

In principle, I first assemble everything without current collectors, check the smooth running, then take everything apart again, insert the current collectors in place, and then reassemble everything. This requires a small auxiliary device, because it is quite tight in the running gear.

Once the motor has been glued in place, it is almost impossible to reassemble the running gear. “I therefore strongly advise against completely dismantling the locomotive.”

The fact that it ultimately took a year and a half for the first locomotive to reach its customer had various reasons, of which only the technical ones have been explained so far. Equally, it should not be forgotten that Larry's bread and butter is not model railways.

He earns his living in Bad Doberan as the owner of the trade fair construction company “eXact Messebau”. After the end of the coronavirus restrictions, he received a lot of orders and often had to interrupt the V 60 project for long periods of time.



Larry has added further refinements to his own locomotive: We see filigree railings and narrower wheel flanges. A little colour makes all the difference in the window area.

It should already be clear that many parts had to be produced differently than originally planned. This was the reason why the rebuilding of the models (assembly) was able to slip into this order-intensive phase in the first place.

Finally, we would like to make a note to all readers who are learning about this project for the first time today. This locomotive conversion can no longer be ordered! Märklin has long since announced a new design of the shunting diesel locomotive (with matching dimensions and features), which is expected during 2024. The need for such a comprehensive rebuild is therefore no longer necessary!

Let's hear from the inventor himself once again: “If I had realised this, this project would never have happened. I am therefore very grateful to the community that none of the interested parties backed out after Märklin's announcement.” This is hopefully a clear sign of how the Zetties stand by each other and feel a friendly bond.

Manufacturer of the base model:

<https://www.maerklin.de>

Model building webpages of the designer:

<https://www.die-minilok.de>

Film documentation of the project:

https://youtube.com/playlist?list=PLYSRXyUJHhc7CRp69coX-MPcaBeANgObq&si=3mMjQcqt_jE

Layout details (part 6)

To the Right and the Left of the Way

Traffic routes cut through natural and cultivated landscapes. They form a foreign body that is exposed to the forces of nature from the day of its completion. Little by little, plants are reclaiming their space, wherever they are unchecked and can find a way. And so, routes sometimes become an integrated part of nature. These are the roadside details that are the focus of today's article.

Our 2023 annual focus theme series on layout details was interrupted two months ago due to scheduling issues. The original plan was to include an article about the placement of figures on a model railway layout.

At the Nürnberger Spielwarenmesse (Nuremberg International Toy Fair) at the beginning of last year, accessories manufacturer Noch surprisingly announced its intention to launch a whole range of multicoloured 3D-printed figures, with June 2023 as the originally planned delivery date.



Today, we look at intersecting traffic routes, such as the railway crossing here near Hüinghausen, and at their immediate surroundings.

Since this is the type of new product that deserves to be explicitly recognised in our articles, we had also planned our articles accordingly. More than half a year has now passed, however, and we have had to reschedule.

Under no circumstances do we want to cancel the two planned articles, and we will submit them therefore as follow-up parts outside the 2023 annual focus theme series, as soon as possible. In the sequential



Tree-lined roads are beautiful to look at and draw the eye to their course when viewed from above. If implemented correctly, this can also be used for good effect on a model railway layout. At the same time, we also see that water towers do not always have to be located at a railway maintenance depot.

numbering, episode 8, which was intended to be the final instalment, therefore appears today as part 6, with which we close our series about layout details.

Our theme today is road- and trackside side details, and they are modelling topic where mistakes are made time and again, and not only by beginners.

Initial enthusiasm during a build often gives way to disillusionment, because all too often a scale depiction of transport routes does not really look realistic but rather contrived and artificial. In order to avoid this, we have prepared this article to analyse and understand the reasons for doing it wrong. This should then allow to derive solutions and increase the awareness of the transitions between nature and man-made structures.

Many types of routes

Before we look at routes and to their left and right, we should first think about the types of routes we encounter in our environment and therefore also on a railway layout. A meaningful definition of the term will help us in this respect and could go something like this:

“A route is a connection between two geographical locations that is suitable for people, animals or vehicles to move along. In a narrower sense it includes connections on the ground, on bridges, footbridges and in tunnels, underpasses, etc.”

For this reason, we will not consider here air and shipping routes, although we would like to take a look at the banks of lakes and streams that lie at the edge of our footpaths.



In this photo we see different types of routes and intersections that allow the passage of trains, ships and boats, or pedestrians.

Let's start our observations with the banks of bodies of water. At lakes, we often find areas for bathing where the shore slopes gently into the water and is characterised by fine gravel or even sand that is not overgrown with plants. This is certainly also due to the frequency of visitors in the summer months, which is also the growth phase for vegetation.

These are contrasted with steep banks, which are characterised by rocks and require a suitable topography, which does not necessarily have to be alpine in character. In all other places, the appearance largely resembles the banks of flowing waters, which we would like to place more in the centre of attention.



Ditches are man-made and serve to drain water from the landscape. This is why they usually run dead straight, as can be seen here. You can also clearly see that there is a narrow strip without vegetation between the water level and the turf.

We often see streams on model railway layouts where the edge of the turf extends seamlessly into the water. However, this does not correspond to the natural appearance and is more typical of drainage ditches, which are usually dry.

Ditches are also man-made and run as straight as possible. This means that they offer little resistance to the water to be drained off and do not provide any surface for its erosive forces to attack. The situation is different with natural streams and rivers, which run through a landscape in curved lines.



This stream runs in a slight bend to the right. Looking at both banks, the left bank is steeper than the right one. The viewer's attention is mostly drawn to the area below the branches reaching over the water.

However, since the water cannot choose the ideal course of the inner radius like a cyclist and lie down in the curve, it constantly flows straight ahead to the bank in the outer radius, only to be deflected in the direction of the course – the angle of incidence equals the angle of departure.

As a result, the highest flow velocity regularly prevails in the outer bend, erosive forces act and erode the bank, while sediments tend to be deposited again in the slower-flowing water of the inner bend.

Typical for streams and rivers are therefore break-off edges in the transition area from the green landscape to the water surface; we tend to find a flatter bank area interspersed with sand and aquatic plants in the inner bend.

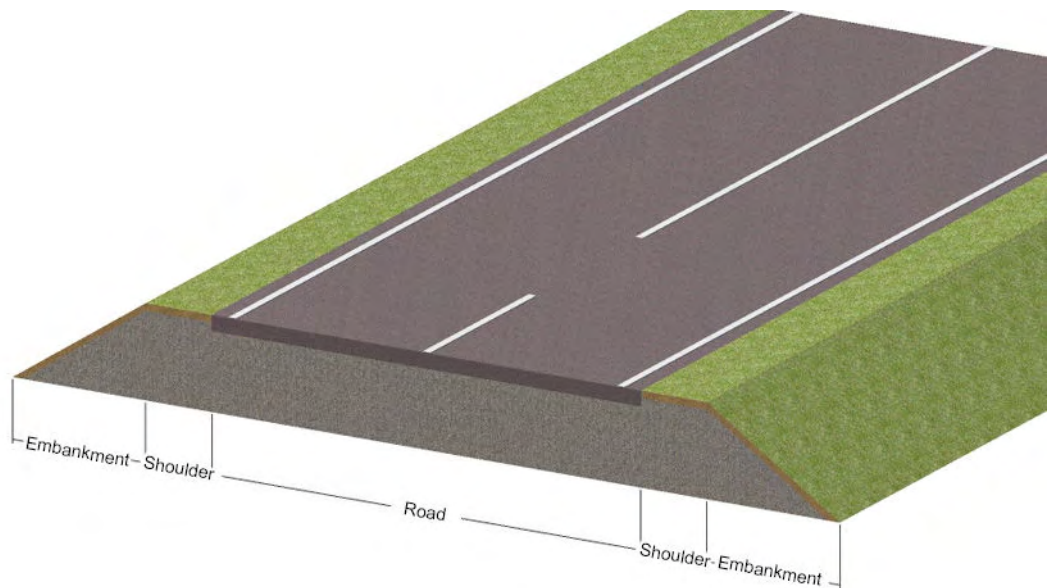
Every model railway enthusiast will hopefully have access to a smaller stream in their immediate vicinity so that they can observe and document what is summarised here. With just a few self-made photographs of a real-life scene, it should be no problem to realistically incorporate a stream into the layout and to utilise the calming effect of water in a targeted manner.

Road and trail shoulders

One of the most classic ways to build a layout was once a board covered with a grass mat.

Track was then nailed and road tapes glued to the board. The effort required for such a project was minimal, but, unfortunately, so was its exemplary effect.

We've already talked about the variety of shapes and colours in nature in previous episodes, but here we'll just focus on the road. Have you ever followed the construction of a new road?



This is not achieved by tipping and rolling tarmac over a flower meadow. And that is why the model railway should not look as if the village street was once created in this way. It is important that the road subgrade remains recognisable to the observer.



This country road in the Lüneburger Heide in northern Germany provides us with many typical features that are to be considered for modelling as well: The distance between the signposts and guard rails to the tarmac, the higher position of the road in relation to the fields and the tarmac on top of its subgrade.

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The photos on this page show two examples of level crossings of different paths. Here, too, it is important to recognise how they meet and how transitions such as edges are actually created and look. A railway line is often higher than the roads it crosses (photo above), which is why a small incline needs to be created. Two paths or roads, on the other hand, usually approach each other at the same height. Turning is possible, which is why the intersection is rounded off by tangents (photo below).



Agricultural roads and trails make for a very special chapter of modelling. They seem to be milled into a wide strip of meadow between the fields. In addition to tall solitary trees, a typical accessory to be placed on their edge is a wayside cross or shrine (photo above), here concealed by two thuja trees. If we look closely at the unvegetated grooves, tyre tracks from tractors can often be seen (photo below). It is always important to choose a soil colour that matches the region.

The road we are looking at is not absolutely level with the surrounding field or meadow. And the adjoining area does not extend seamlessly to the road surface. No matter how tiny the separating layers may be in terms of scale, they remain perceptible and are also immensely important for the model.

A road is only created by artificially building up an earth body on the subsoil that is flush with the aforementioned subgrade. This substructure is necessary if the height of the road has been changed by embankment filling or if an insufficiently load-bearing subgrade has had to be replaced. The influence of the terrain topography as well as the purpose of the road (bicycle traffic, car traffic, or heavy goods vehicles) become clear.



The layout of a paved path with rain (left) and drainage ditch (right) can be seen here just as well as the many places where greenery penetrates even the compacted subsoil with a gravel layer.

On top of this is the superstructure, which comprises all layers that are structurally necessary for the load-bearing capacity of the traffic surface. For the carriageway of a road, it is usually between 40 and 90 cm thick. Various construction materials are used in the individual layers.

Chippings are usually used under the visible surface course of asphalt, concrete or paving. On unpaved roadsides on rural roads, several centimetres of this can often be seen on the verge. Nature fights to reclaim this strip over the years, starting from the embankment.

Hardy plants without major requirements are then followed by grasses as soon as sparse vegetation has ensured that sand and soil carried around by the wind gets caught and begins to form a humus layer with dead biomass. The question of how old our roads should be, is, therefore, also crucial for modelling.

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Two examples of cracks and patches in the asphalt are intended to inspire reproduction on the model railway. At the same time, they show how nature is returning. Equally important are the use of manholes (photo above) as well as drainage covers (photo below).



In addition to the transition from the carriageway over the verge to the embankment (photo above), the curvature of the carriageway to drain rainwater to the edges can also be seen in this photo. The road by the rape field (photo below) is a special case: The two separate directional lanes can be easily distinguished from each other by cracks, but the delineators set into the carriageway surface are unusual. Nature has already found its place there.

We can recognise a road's age by its colour, which appears increasingly lighter over the years, cracks and frost damage as well as subsequent patches. The overall picture should be consistent and is best determined by taking reality, as an example.

Equally important are roadside features such as trees, crash barriers, delineators (formerly kerbstones or bollards), traffic signs, and also the masts of overhead power lines, which we still find today, especially in rural areas. The correct distance to the edge of the road is important for credible positioning, as is their sequence in the viewing direction from the centre of the road towards the edge.



We discovered this transformer house near Bad Laer in southern Lower Saxony. Relics of bygone times like this are still frequently found in rural areas to this day.

Just as a side note, we would like to point out that a road is not absolutely flat, but is built with a slight incline to one side or with a central saddle and sloping edges. This serves to speed up rain run-off, but is hardly noticeable at a scale of 1:220 and can therefore usually be ignored.

Once ubiquitous, transformer houses rarely appear on layouts from eras III and IV. This is all the more surprising because many of them can still be seen today, even though they have long since lost their function.

Bridges and viaducts

Where there are traffic routes, they will also cross each other. We encounter various crossroads and railway crossings at the same level. If routes meet at different heights, we need underpasses, bridges or viaducts.

They all have typical features that should also be taken into account in the model to ensure a harmonious overall appearance. For example, it was once considered desirable for engineering structures to blend into the landscape as harmoniously, as possible.

This was often expressed in the use of regional stone types for masonry structures, which can also be visible in the landscape in the form of rocks. It was also important to choose the shapes of viaducts, their possible ornamentation, and the curves of arches in such a way that they had an appealing effect on people and did not give the impression of obscuring the view of the landscape.



The Altenbeken viaduct is a good example of how the architects have integrated an imposing structure into the landscape. It dominates the view, but still looks harmonious in its surroundings.

No stone viaduct, whether built of bricks or large quarry stones, will be able to convey a sense of lightness. But our world is not black and white and it was important for the architects of the past to achieve what was possible here and thus have a positive effect on people.

A good example of this is the Altenbeken Viaduct, which spans the Beketal valley with 24 vaulted arches over a length of 482 metres and should be well known to all Z scale enthusiasts because of the biennial international Z scale convention in Altenbeken. It was completed in the summer of 1853 and is therefore also characterised by the spirit of the time in the middle of the 19th century.

The unmissable and dominant yet harmonious structure has become Europe's longest limestone bridge and the symbol of the railway town, which even bears it in its coat of arms.

In the 20th century, concrete became increasingly popular as a building material, processed as reinforced or prestressed concrete, allowing tensile and compressive forces to be absorbed. This also meant that engineering structures became increasingly simple and less consideration was given to their effect on the landscape. This can be seen in the area of many concrete bridges on the new and high-speed railway lines of the German Railway.



The “coat hanger”, the Fehmarnsund Bridge, is an example of a 20th century concrete structure. Here too, lightness and harmonious integration into the landscape were the architectural goals.

Only brief mention should be made of underpasses at this point. They are designed in a similar way to viaducts and masonry or cast bridges. However, the decisive feature that sets them apart is their short length, so this is primarily a question of definition.

If larger spans were required, for example to span a shipping lane, steel bridges were usually used. They have an upper and lower chord through which the forces are transferred.

The characteristic arches of round or elliptical shape on longer models can often be seen as a fish belly (protruding downwards) or rising upwards. Both are among the favourite bridge shapes of model railway enthusiasts. This is probably also due to the fact that their builders once attached great importance to making these unavoidable interventions in the landscape as unobtrusive as possible.

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The abutments of the two bridges (photo above) are clearly visible. However, as they are literally embedded in them, the abutments are largely concealed. On the upper bridge, however, one can be seen on the right if you look closely. The situation is different with the steel truss elements that span the Lenne and Ruhr rivers at their confluence (photo below): At their upper end, the piers clearly form the support bench on which the bridge bearings stand out clearly from the background.

They achieved this, which also applies to the box bridges, through the building material and a truss construction method that leaves plenty of space to look through and at the same time does not obstruct the view of crossing trains. When painting was required, inconspicuous colours such as medium or dark grey and green were always chosen.

Striking blue or even red colours, which are deliberately intended to attract attention because the bridge is perhaps seen as a work of art or an architectural aid to urban planning and landscaping, are more characteristic of more modern times. Note: The shapes and colours of any bridge structure are an expression of the time in which it was built!

The abutments are often forgotten or incorrectly modelled in the model. In the prototype, they are of central importance because the brick or cast bases at the ends of each bridge transfer vertical and horizontal forces into the ground. In the transition area to the bridge superstructure, they also stabilise the earth embankment in its position because they also absorb earth pressure forces there. Wing walls, as with tunnel portals, can therefore also be present.



Overpasses are bridges with a smaller span. They are often indispensable on a model railway. If we take a look at this motif, we immediately realise why this is the case.

They form the transition from the earth embankment to the bridge superstructure and must be dimensioned accordingly in order to withstand the calculated forces. When scaled down, however, they sometimes appear to be stepchildren and barely make an appearance – sometimes they are even completely absent and the model rests directly on the earth embankment.

It should be obvious that a bridge constructed in this way, even without additional load, would sink into the ground at the latest at the first major rainfall. Nevertheless, its trough does not rest directly on the abutment.

Temperature fluctuations result in the bridge body expanding differently, i.e., changing its length. It must be dimensioned in such a way that it does not crack the abutments in the event of high temperatures and remains in secure contact with them even in its shortest state.

This cannot be achieved with rigid connections. This is why each abutment has a support bench on which the bridge bearings that support the bridge superstructure rest. The bearings themselves are usually made of metal and have an upper and lower bearing surface.

In between, we find different parts at the two ends: On one side a rigid connection, on the other a roller or spike as a loose connection, which enables the expansion movements. Only those who take this into account in the model and implement it consistently will ensure that their bridges will look prototypical when the layout is completed.

Along the railway line

We have often come to reflected upon the principle “First came the landscape, then came the railway”. This results in special structures, as only a few railway lines were built through flat terrain.

Special engineering solutions were also required away from valley crossings or tunnels, as trains can only climb gradients to a limited extent. Railway embankments and cuttings alternate along the route. While in some places a more or less high embankment had to be built to level out the track in order to achieve the required track position, in other places hills were in the way.



A railway line does not always run through such flat land and manage with such a comparatively low embankment.

They were removed or, in the case of rocks, blasted away to create a passage. The track cuts through the surface of the surrounding terrain.

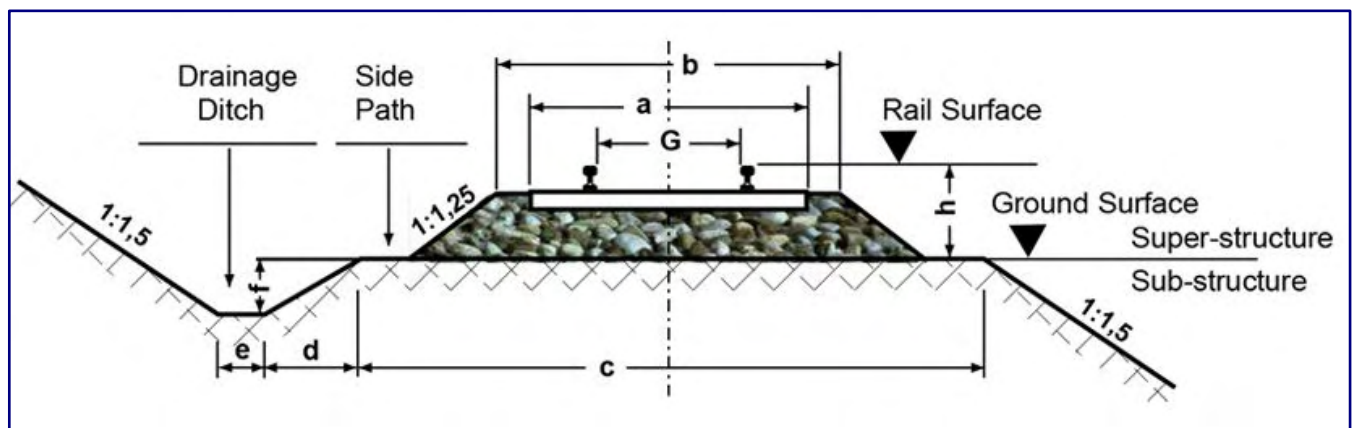
When designing a layout, we should first consider how our railway line could have been built. We then choose whether rugged rock faces should surround it, whether it needs a retaining wall, or whether it is flanked by an overgrown slope.

However, as with the railway embankment, the decisive factor for this edge and its appearance is that the vegetation must be controlled. Due to the risk of fire in steam locomotive operation, the railway lines during that era had unvegetated and cleared protective shoulders, with trees removed as well to prevent the risk of them falling onto the tracks or overhead line in a storm.

Since 1994, however, things have been different, as the railway prefers to save money for this form of maintenance and then close entire lines for days on end in the event of a storm “for safety reasons.” In eras V and VI, vegetation along railway lines can deviate from the approach described above and, sarcasm on, smaller stations can be modelled as botanical gardens.

Railway embankments also fulfil another function: their inclined slope ensures that rainwater can run off the subgrade and avoid erosion. Its inclination is therefore not a completely arbitrary design feature.

Drainage is somewhat more difficult to achieve in a cut in the terrain, as the substructure and superstructure of the track would form the lowest point here. And this is exactly what needs to be prevented on the prototype. The solution, as on roads, is a side ditch that takes over this task and drains off water.



NEM 122 provides important information on the layout of the railway body in the model and how it is integrated into the landscape. In addition to the code letters, a table provides all the important dimensions for each scale. Illustration: MOROP

Railway modelling norm NEM 122, dealing with the cross-section of the railway track for the standard gauge, illustrates the construction method very well and also shows the typical side path along the track. This can be found to the right and left of the super-structure on the substructure, but is often covered by vegetation on less frequented branch lines, and is usually no longer visible.

We do not wish to go into detail today about the correct positioning of catenary, telegraph and signal masts and panels, as this can be derived from prototype photographs with regard to their sequence and respective distance from the centre of the track. In addition, norms NEM 102 and 103 (clearance perimeters) can also be consulted for that purpose.

In this article we wanted to emphasise the edges and transitions between different elements of a landscape. They are of varying natural or human origin, created in a fixed chronological sequence and therefore delimited from each other in a recognisable and typical way.



The track of the Küstenbahn Ostfriesland e.V. heritage railway is surrounded by nature right up to the edge. And the flora is increasingly trying to reclaim the track superstructure. For the model railway enthusiast, this provides important inspiration for the prototypical and believable design of branch lines.

We are usually not consciously aware of them, but they all form a very important feature whose absence we would sense immediately, even if at first we might not be quite able to point out finger at what is missing.

NEM-Norms website:
<https://www.morop.eu>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Anlagenvorschläge mit Leben **Ideen für höhere Ansprüche**

Anlagenplaner und Illustrator Ivo Cordes krönt sein zeichnerisches Schaffens- und Lebenswerk mit einem zweiten Buch, das seine Aktivitäten nun abschließen soll. Aus seinen vielen Entwürfen hat er 24 weitere Vorschläge ausgewählt, die sich in Buchform an Leser richten, die höhere Ansprüche stellen oder erreichen möchten. Lesenswert ist es allemal, zumal es auch ohne Übernahme des Plans viele Anregungen für eigene Entwürfe liefern kann.

Ivo Cordes
Modellbahn-Anlagen mit Flair
Konzepte, Gleispläne, 3D-Ansichten
24 geniale Vorschläge für anspruchsvolle Vorhaben von Ivo Cordes

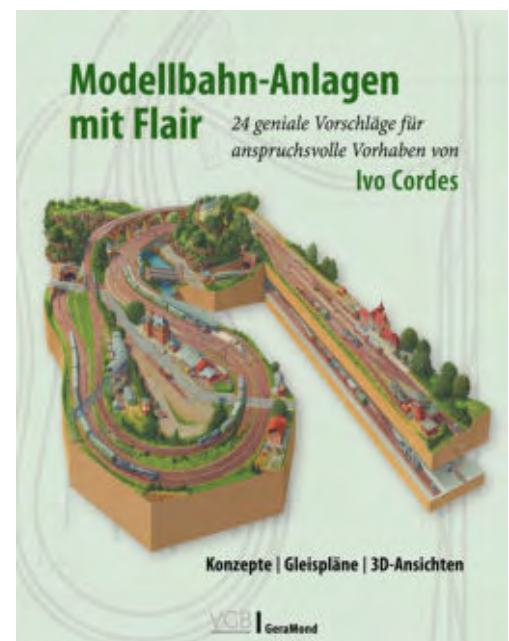
VGB | Geramond Verlag GmbH
München 2023

Gebundenes Buch
Format 22,8 x 29,6 cm
128 Seiten mit ca. 220 farbigen Abbildungen

ISBN 978-3-98702-000-1
Preis 34,99 EUR (Deutschland)

Erhältlich direkt ab Verlag
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Ivo Cordes ist ein Name, der langjährigen Lesern der papierhaften Fachmagazine bekannt sein könnte. Aufgefallen sind ihnen dort aber gewiss seine in 3D-Perspektive gezeichneten Illustrationen zu Anlagenentwürfen. Mit dem vorliegenden Buch möchte das Kapitel Anlagenvorschläge nun abschließen, wie er im Vorwort bekundet.



Seine Präsentationsform erleichterte das Vorstellungsvermögen und dürfte bei vielen Modellbahnern erst Lust auf eine eigene Anlage geweckt haben, zugleich auch die passenden Ideen und Ansätze geliefert haben. Und das reicht schon weit zurück, denn zu Beginn seiner Aktivität konnten Computerprogramme für den Hausgebrauch vergleichbare Ansichten noch nicht generieren.

Und heute reichen sie längst noch nicht an das lebendige Bild heran, dass in diesem Buch wiederzufinden ist: Modellbahn ist schließlich ein Thema mit Emotion, was eine digital programmierte Maschine nun mal nicht bieten kann.

Blicken wir auf die Autorentätigkeit für verschiedene Verlage und Redaktionen zurück, dann sollte es erlaubt sein, die Zusammenfassung in Form eines Buches mit Musikalben zu vergleichen, in denen Künstler die besten Werke ihres Schaffens zusammenfassen.

Gewiss ist und bleibt das immer subjektiv, denn Geschmäcker sind nun mal verschieden. Deshalb wollen und können wir – mangels vollständigen Überblicks über alle Entwürfe – uns nicht anmaßen, ein Urteil zu fällen, ob diese Auswahl diesbezüglich „perfekt“ sein kann.

Was sie ist, ist auf jeden Fall vielseitig und ansprechend. Nach dem bereits an dieser Stelle besprochenen ersten Band „Modellbahn-Anlagen mit Pfiff“ folgen nun 24 weitere Vorschläge, die sich deutlich vom ersten Teil abheben sollen.

Ivo Cordes schreibt selbst in seinem Vorwort, dieses Mal die in der konstruktiven Durchbildung anspruchsvolleren Ideen ausgewählt zu haben, die häufig auch thematisch enger umrissen wurden. Übertragen auf die Spurweite Z bedeutet das, dass eine gewisse Vorsicht und Skepsis walten sollte: Gleispläne sollten immer geprüft werden, ob sie in Bezug auf die maximalen Steigungen und Zugriffsmöglichkeiten in verdeckten Bereichen die Anforderungen unseres Maßstabs erfüllen.

Das ist explizit nicht als Kritik zu verstehen, denn kaum jemand wird eine der hier präsentierten Ideen 1:1 in die Realität umsetzen können, weil meistens auch die Raummaße etwas abweichen dürften. Zudem ist auch keiner der Gleispläne, die entworfen wurden, für die Spur Z ersonnen worden.

Die Anforderungen an jeden Modellbahner gleich welcher Spurweite sind aber immer dieselben: Wer Gefallen findet, prüft das für sich Machbare, die eigenen Räumlichkeiten und pickt sich dann die Ideen und Themen heraus, die ihm wichtig erscheinen und realisierbar sind.

Da reicht es schon, wenn innerhalb desselben Maßstabs ein anderes Gleismaterial favorisiert ist. Etwas Anpassen und Umdenken sind immer gefragt, macht eben den Reiz unseres Hobbys aus. Wichtig bleibt, dass niemand überfordert werden soll.

Selbstverständlich erscheint uns das nicht, denn die hier zusammengetragenen Anlagenentwürfe sind für einen meist überschaubaren Platzbedarf aufgestellt worden, aber eben im Vergleich zum vorherigen Band für gehobene Ansprüche.

Viele orientieren sich an Vorbildern und bieten deshalb besonders realitätsnahe Betriebsmöglichkeiten. So ist ein lebendiger Modellbetrieb gesichert, denn der vielfältige Bezug auf echte Vorlagen gewährleistet auch die durchdachten Abläufe des Großbetriebs, die besonders einem Neueinsteiger kaum bekannt sein dürften und sonst schnell zu eigenen Fehlplanungen führen.

Aus den vielen Vorschlägen, von denen eben kein einziger neu ist, picken wir uns zum Veranschaulichen mal einen heraus. Dieser ist wegen der nahen Messe Anfang November am Bodensee geeignet, in vielen Lesern schon gleich ein Bild des Vorbilds in den Kopf zu projizieren: der Hafenbahnhof von Friedrichshafen mit dem Umstieg zu den Fähren in Richtung Romanshorn (Schweiz). Weil der Betrieb einst noch anspruchsvoller und bunter war, wurde das Zeichenprojekt einfach in die Epoche III gelegt.

Aber auch alle anderen 23 Anlagen stehen in den zentralen Punkten – mit anderen Ansätzen, Konzepten und Vorlagen – kein Stück zurück. Mit detaillierten Plänen, Schaubildern, Skizzen sowie Hinweisen zur Betriebsabwicklung liefern sie durch die Reihe alle wichtigen Informationen, die der Leser sucht und braucht.

Und so ist dieses Buch für alle unsere Leserinnen und Leser, die eine Anlage planen oder ihr Können weiter steigern wollen, ein guter Einstieg in zukünftige Projekte. Am Anfang steht der Plan und bei diesem Schritt finden wir hier wertvolle Hilfen.

Erst danach geht es weiter mit dem Umsetzen und Gestalten der Ideen, aber das ist schon wieder ein anderes Kapitel, zu dem es im selben Verlag ebenso gute Ratgeber gibt.

Übersicht deutscher Schienenfahrzeuge **Das Lexikon aller Traktionsarten**

Vor allem Transpress macht immer wieder durch Neuauflagen erfolgreicher Eisenbahnbücher auf sich aufmerksam. Im vorliegenden Fall hat das Buch seinen Titel gewechselt, aber auch eine umfangreiche Aktualisierung erfahren. Da es von breitem Nutzen sein sollte, haben wir es uns für Sie angesehen.

Thomas Estler
Enzyklopädie Deutsche Loks und Triebwagen

Transpress
Stuttgart 2023

Gebundenes Buch
Format 24,0 x 29,0 cm
304 Seiten mit 314 S/W- und 256 Farbabbildungen

ISBN 978-3-613-71690-2
Titel-Nr. 71690
Preis 49,90 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Das vorliegende Buch ist die Neu- bzw. Wiederauflage eines früheren Titels (Ersterscheinen 2011), den der Verlag selbst als Standardwerk bezeichnet. Ein weiteres Mal überarbeitet, liegt er nun mit neuem Titel vor uns.



Unsere Leser sollten selbst in ihre Bücherregale schauen, ob der frühere Titel schon vorhanden ist und sich ein aktualisieren auch für sie lohnt. Wir hatten jenes Werk bei früheren Auflagen nicht rezensiert, weshalb wir nun gern zur Lektüre gegriffen haben. Vom selben Autor erschien auch ein vergleichbares Buch, das sich auf deutsche Dampflokomotiven beschränkte und uns gut gefallen hatte.

So war unsere Neugier geweckt und dieser Titel schnell vorgemerkt. Autor Thomas Estler (1958 - 2022) wird diese Besprechung nicht mehr zur Kenntnis nehmen können, denn er ist im September 2022 überraschend gestorben und hat das Erscheinen dieser Auflage nicht mehr erlebt.

Als Fachautor ist er uns aber gut bekannt. Er war auch beruflich im Umfeld des Verkehrswesens tätig und hat deshalb eine ganze Reihe an Büchern zu diesem Thema und auch speziell der Eisenbahn geschrieben. Dabei war ihm durchaus anzumerken, dass er schon von Kindesbeinen an großes Interesse an der Eisenbahn mitbrachte.

Wegen der geschilderten Begleitumstände führte mit Michael Dostal ein ebenso bekannter Schreiber die Aktualisierungen zu einem erfolgreichen Ende. In den seit 2007 verstrichenen Jahren hat sich auf Deutschlands Schienen viel getan, was die Aufnahme einiger neuer Baureihen erforderlich machte.

Beispielhaft genannt seien verschiedene Vectron-Modelle, die Euro-Dual von Stadler oder auch zwei Triebzugfamilien aus dem Hause Siemens. Und auch die TRAXX-Familie von Bombardier erlebte inzwischen einen Generationswechsel.

Mit der Brennstoffzellentechnik hielt zudem noch eine völlig neue Antriebstechnologie Einzug in den Bahnsektor. Das hatte zugleich aber auch zur Folge, dass bei der vorgegebenen Seitenzahl eines Buchs Inhalte an anderer Stelle weichen mussten.

So verweist der Verlag selbst schon im Vorwort darauf, dass sich die Schmalspurdampflokomotiven jetzt nur noch auf die wichtigsten Typen beschränken, die noch im Dienst stehen. Die Lücke zu schließen vermag das Werk, das wir im zweiten Abschnitt gestreift haben.

Die Spanne der Inhalte umfasst laut Eigenaussage die zwanziger Jahre bis heute. Das markiert ein Abgrenzen bei den Fahrzeugen, die in den Bestand der Reichsbahn gelangten und von ihrem Nummernsystem erfasst wurden.

Die unübersehbare Fülle an Maschinen der Pionierzeit bleibt also außen vor, gleichwohl sind trotzdem sehr viele Länderbahnkonstruktionen in diesem Verzeichnis wiederzufinden. Vermisst haben wir da nichts. Deutlich wird diese Zeitepoche übrigens auch am recht hohen Anteil der S/W-Aufnahmen.

In unsere Gegenwart hinein möchte das Buch auf neuestem Stand bis zur Drucklegung sein und das sehen wir auch als erfüllt. Doch einige Lücken sind uns auch aufgefallen: Obwohl der Titel keine Einschränkung auf Staatsbahnbestände beinhaltet, zeigen sich genau hier die Grenzen des Umfangs.

So finden wir beispielsweise Einzelgänger der Bundesbahn, die nach Betriebsübernahmen nur kurzzeitig im Bestand waren, und auch angemietete Fahrzeuge wie die V 320, die DE 2500 (Baureihe 240) oder den Europrinter Es 01 (Baureihe 127), aber beispielsweise keinen Schienenzeppelin oder die AEG 12X (Baureihe 128). Ableiten lässt sich das nur aus der Beschreibung auf der Rückseite des Buchdeckels.

Präsentiert werden dem Leser jeweils kurze Portraits mit den wichtigsten technischen Daten in einer Tabelle sowie Beschreibungen zum Fahrzeugtyp, die einen Überblick über Geschichte, Anlass der Entwicklung und Betriebseinsatz geben.

Illustriert wird dies mit mindestens einem Foto pro Dateneintrag, an dem wir hinsichtlich Auswahl, Dokumentationsfähigkeit und Reproduktionsqualität nichts auszusetzen haben. Thomas Dostal, der die Neuauflage abschließend begleitete, unterzog auch die vorherige Bildauswahl einer kritischen Durchsicht, wie seitens des Verlags zu dieser Auflage angemerkt wird.

Eine gute Idee war es sicher, zu Beginn der Lektüre Abkürzungen und Nummernsysteme der deutschen Eisenbahnen zu erläutern. Diese Ausführungen, die sich von Beginn des Buches bis zur Seite 18 erstrecken, schließen auch die deutsche Länderbahnen und das 2007 eingeführte NVR-Register mit ein.

Da die alten Baureihenbezeichnungen mit dem nationalen Fahrzeugeinstellungsregister weitgehend überholt scheinen und bis heute nicht jedem geläufig sind, ist das Beibehalten dieses Kapitels sehr sinnvoll. Auch Länderbahntypen sind heute nicht mehr für jeden Modellbahnfreund leicht und auf Anhieb zu entschlüsseln.

Zusammengefasst möchten wir für diesen Titel nicht zwingend die Bezeichnung Enzyklopädie vergeben, weil es doch noch viele weitere Facetten des Eisenbahnwesens gibt, die sich dann behandeln ließen und den Seitenumfang immens aufweiten würden.

Ohne Zweifel ist es aber ein brauchbares, hilfreiches und gut nutzbares Lexikon deutscher Lokomotiven und Triebwagen über einen Zeitraum von mehr als hundert Jahren. Es steht dem Dampflokband, der eingangs erwähnt war, in Struktur und Inhalt nicht nach, weshalb wir hier auch abschließend zum gleich guten Gesamteindruck gelangen.

Publishing pages:
<https://www.motorbuch.de>



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The 2nd Advent Sunday in Zell (Moselle)

An Advent Gathering

How we all missed that! Every two years, passionate Zetties meet up in Zell an der Mosel to spend some lovely days in a contemplative Advent atmosphere. Due to the corona break, a whole four years have now passed since the last get-together. The joy of seeing each other again was correspondingly great.

When the traditional Advent event, officially known as the “model railway exhibition”, had to be cancelled in 2021 because of the restrictions imposed, our world was not right anymore. Many long-standing participants now realised what they were now missing at the end of the year.

The meetings in Zell an der Mosel are and will always be something special. Just before the middle of Advent, when we slowly turn our focus towards Christmas and count down the remaining working days of the year, it's time for one last highlight of our hobby.



After a forced break due to the pandemic, many Zetties eagerly headed back to the Moselle to spend an Advent weekend in a familiar atmosphere very close to the double-decker bridge, to which our gauge has a very special connection, as well as to the DB 181² series

Zetties then get in their cars and drive from far and wide to the Moselle, as if they were following a star. As early as Friday evening, the former waiting room of the Zell railway station building fills up with pilgrims from the model railway scene.

Many dialects can then be heard as Zetties from all directions chat about their common interests over good home cooking and a glass of wine. And even if the set-up doesn't start until Saturday, this informal get-together has been a long-standing event.

When they arrive back at the Schwarze-Katz-Halle in Zell the next morning, after having had a good night's sleep, they don't need a full day to set up and commission their dioramas, modules, and layouts. Here, likewise, the personal exchange and marvelling at the construction results is so much more important.

Our magazine was represented on site by two editors (Ralf Junius and Holger Späing) and another helper (Hendrik Späing). And we had come up with something different. This time we wanted to "run" the double-decker bridge in Bullay, which has been in the spotlight for years thanks to Rainer Tielke's model, so we put on our running shoes and walked there at a brisk pace, crossed it, and ran back to Zell on the Alf side of the river.

This should hopefully open up completely new viewing opportunities, as many details can be seen much better. At the same time, the idea of hopefully using a break in the rain during daylight hours to find a drone launch site and record the unique bridge construction from an elevated viewpoint was pretty appealing.



Ingo Sindermann (right) was keen to put ZFI Exhibition Commissioner Sylvester Schmidt (left), who usually prefers to work in the background, in the spotlight and to express his thanks by honouring 40 years of commitment to the Z gauge.

Regardless of how the weekend actually turns out, our readers will hopefully experience the emotions that make every Advent event in Zell an der Mosel so special. It is and remains a meeting of a manageable size with a familiar character, which also can attract model railway friends who only come to look and not to present.

The official highlight, before the public presentation day on Sunday, which is advertised in the surrounding area, is always the dinner on Saturday evening. This time, the last participants stayed up until 1:00 a.m. and were reminded of the hour by the innkeeper.

So there was a lot to catch up on and chat about, which also made Sunday an entertaining day. As a fun run was taking place at the same time as the exhibition this time and the centre of Zell remained closed to traffic, it was quieter than usual in the exhibition hall for large parts of the day. However, this does not mean that there were few visitors. In any case, everyone who had already planned to attend the event, did so.



This **Trainini®** diorama focussed on the realistic design of concrete surfaces and the use of patina markers from accessories manufacturer Noch.

And so, we couldn't complain about a lack of interesting conversations either. We were happy to answer questions about the magazine, take up suggestions and also received a valuable tip on the area of special literature, which will be included here soon.

Tour of the hall

Before the public part of the event really got underway, Ingo Sindermann took to the stage and asked Sylvester Schmidt, the exhibition commissioner of the organising Z-Freunde International e.V., to join him. Sylvester, who usually prefers a quiet appearance in the background, seemed irritated, but accepted the request without hesitation.

To the applause of the exhibitors present, Ingo then explained that Sylvester Schmidt was celebrating a milestone anniversary and that he wanted to honour him for this, which was met with equal approval by those present: 40 years of commitment to Z gauge were to be honoured here. Dear Sylvester, we would also like to take this opportunity to express our deepest thanks.



These Rokuhan beauties were waiting at our own booth to be operated by the visiting children on the large oval track.

And finally, it was time to get started: As in previous years, we set up a large oval of track with a passing siding at our booth, placed the speed controller and control panel at the front, and invited children to control the trains themselves. There was a Shinkansen E6 and the 0 Series Shinkansen to choose from, like the track material, both from Rokuhan.



This DeLorean DMC-12 from Z-Doktor Modellbau accelerated strongly towards the future on the country road of our diorama.

As expected, the latest racer from the Rokuhan programme also attracted the interest of the Zetties who were present. They admired its bright and uniform interior lighting, the accurate driving behaviour and, above all, the always cohesive train appearance thanks to the movable car gangways. We received praising feedback on several occasions that multiple units are simply one of this manufacturer's core competences.

But we had also planned to bring exhibits to the Moselle that matched the theme of the year. That's why, for example, we presented the "Osterdeicher Weg" diorama, one of our colleague Dirk Kuhlmann's masterpieces. We deliberately chose not to include a train to focus on a film-famous DeLorean DMC-12, which is accelerating rapidly along the country road towards the future.



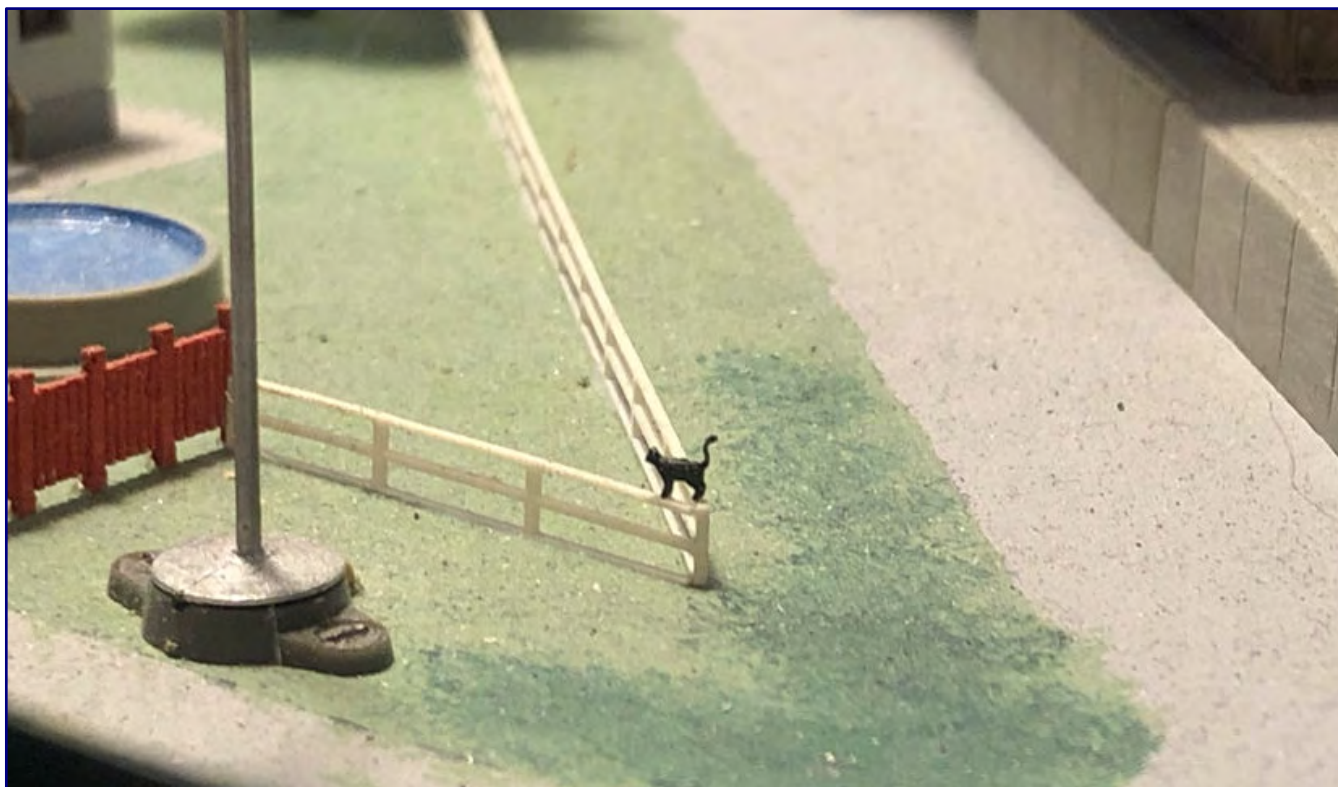
To be able to turn a Big Boy, you need an oversized turntable. Rainer Tielke presented such a model on his diorama.

Our therapy diorama, which completely spared the railways in 2021 and only focused on landscaping with comparatively simple but effective means, also fitted in well with the planned theme.

In contrast, our airport ramp diorama, which shows a section of an airport and will be the subject of an article in our first special issue of *Trainini Spezial*, celebrated its exhibition debut. The Goldhofer aircraft tractor delivered by Herpa just last year was on display.

Our neighbour at the table was Reiner Tielke, who wanted to show his new US turntable, which he built himself. He chose a model whose diameter was selected for the length of the Big Boy, because these giant machines also had to be turned somehow at the turnaround stations. By German standards, it really was a huge turntable!

continues on page 39



Viewers of the retro layout “Schwabenland” (photo above), which deliberately picks up on the design and colours of the 1960s, should not be superstitious. Ulrich Günther's shunting game (photo below) also encouraged visitors to contemplate and participate.

At best, Torsten Schubert was able to steal the show with his colourful light show, which we had already reported on for Model Railway Day. The latest Z gauge exhibit on show is the burning house with fire brigade intervention, for which he drew on the relief buildings of the Modellbahn-Union.

Peter Rogel and Kai Waibel presented retro layouts at their joint booth, deliberately setting themselves apart from the modern layout design. Track layouts and choice of materials according to historical conditions, in the case of Z gauge, of course, this only goes back around 50 years, but are fully in line with the trends of our time.

Ulrich Günther made a relatively modest appearance. He had indeed taken his Neuschwanstein Castle with him, but without the surrounding layout. Instead, he had built a test oval around the fairytale castle and attracted visitors with a shunting game next to it.



This suitcase by Thomas Heß was geared towards Japanese tastes and appealed primarily to the younger audience at the model railway exhibition.

Dressed up appropriately, Ingo Sindermann also turned his attention to younger visitors with his Dwarf Land, in this case the very youngest visitors to the hall. The Booschot Statie installation, based on historical models from Belgium, with which Pieter Willems and Jan van Casteren had made one of the longest journeys, was destined for their parents.

The completed chemical plant by Claudius Veit (Velmo) is also intended for adults. Although the many trains and lorries on the layout fascinate visitors of all ages, there are also scenes that can only be seen by adults. In addition to a chemical spill, these include the boardroom of the tower block, which really questions the morals of the caste of company leaders.



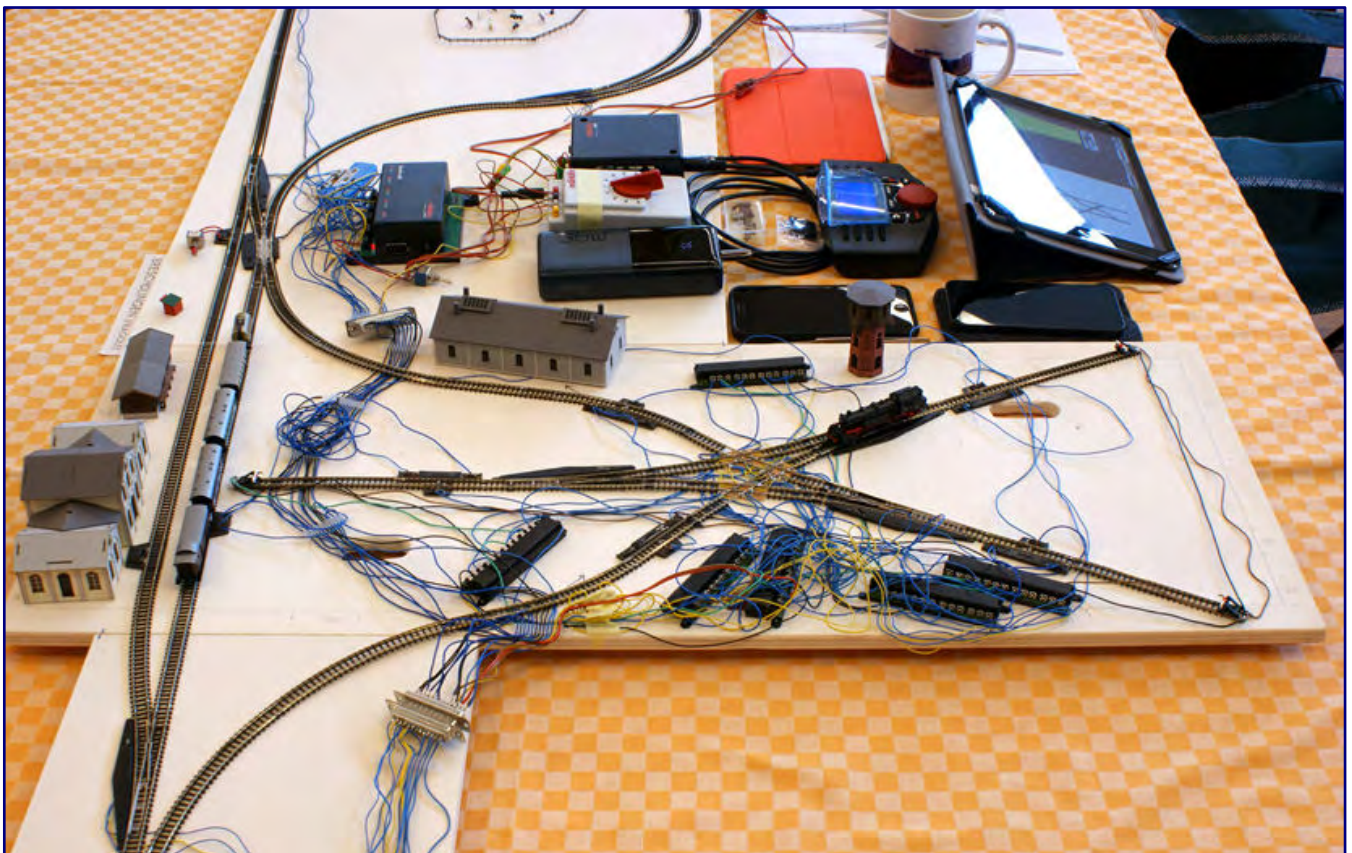
“Klapplingen” (photos above and below) offers visitors two completely different themes separated by a backdrop.

Thomas Heß had three showpieces in his luggage. The “Hello Kitty” suitcase, which presented the corresponding Shinkansen-Shorty model in a colourfully illuminated oval, appealed primarily to girls and their mothers. The “Winzlingen” suitcase, which featured a digital train, tended to attract more interest from male visitors.

Most of the action, however, was right in the middle of these two exhibits on “Klapplingen”, which also ran a much larger number of digitally controlled trains past the visitors, but was also able to appeal to everyone equally with great design scenes.

One star that...

Based on the model in Mals (South Tyrol), Matthias Vogel has started to develop and build a turnaround star that can also be seen at the edge of a terminus station. In Zell, he had set up a makeshift model, which was used for control and driving tests, but gave visitors and participants an impressive demonstration of what is to take place on the future layout.



It is unmistakably a temporary solution, but Matthias Vogel's turnaround star has passed the test run impressively and is looking forward to the layout that will soon be built.

Editor Ralf Junius took to the stage with his pendulum train box (PeZuBox) and was delighted with the journeys made by the mouse rail bus, which had repeatedly refused to work at previous exhibitions. As usual with him, he directed further objects at the younger visitors, who were able to follow the train at eye level on the small board oval track layout.

His strawberry box mostly presented the “Transparent Train” built on a Shorty from Rokuhan and also offered fruit to the children. However, they were not allowed to take and taste fresh fruit, but fruit in the form of gummies. Most of the young model railway fans were probably happy with this!

Manfred Forst is an artist of paper modelling who presented various buildings and petrol stations made from paper on his dioramas. He also had his coal box with him, which shows a section of an engine terminal in peep-box form.



Manfred Forst also presented many interesting car models in our scale from various manufacturers on his dioramas with paper buildings.

Gerhard Maurer's dream loop layout with its autumnal landscape in front of rock faces is also slowly becoming a classic, with many guests attentively following the long train rides through the station, bends with tunnel sections and parade route.

As a suggestion, we would like to see equally beautiful modules from his hand again, of which we no longer have any current images. We remember the barite pit, in particular, (barite mining), as offering many beautiful motifs for photos.



Right before the start of winter, Gerhard Maurer's "Dream Loop" created an autumnal atmosphere with its shades of yellow and red.



Rainer Helfenstein referred to the trial runs for the Bundesbahn's high-speed services in the 1980s with the class 410.

Gerhard's neighbour Rainer Helfenstein also offered intensive train operation with his modular layout.

The term segment layout would probably be more appropriate here, as the station at the centre of operations, which explains the many train movements, is unlikely to follow any known modular standard and its layout can also be operated on its own.

The visitors probably didn't care, because what mattered to them was what exciting scenes they could discover and trains they could see.

And Rainer clearly made a great deal of effort to fulfil their wishes, including ours when taking the photos for this report.

Trainini® *International Edition*

German Magazine for Z Gauge

The US system, which is presented by Trafofuchs, made another appearance, but had not yet been seen in this region, at least not in its current stage of development. Oil production and livestock farming alternate in its motifs with railway photographers. Ernie and Bert, who all the children find sooner or later, are also a regular feature.



Michael Boost advertised his offer with illuminated passenger wagons (photo above), but also had an Advent wreath with a snowy landscape and a small layout (photo below) at his disposal.

Michael Boost promoted his electronics programme with a showcase presentation of illuminated models, including interior and end lighting as well as specific effects. He showed all interested visitors the first

samples of a new lighting board that can be operated in either analogue and digital modes, and can be shortened in multiple locations to be used in a wide variety of train car models.

We will certainly soon be able to present this as a new product for this year. All guests who were not particularly interested in technical solutions for nominal size Z were entertained by a small layout and a circle of tracks, on which Michael's trains ran and, sometimes, deliberately created a Christmas atmosphere.



At the 1zu220-Shop booth, the 6284 series crossed the causeway across the sea on the “Det Lille Sommerhus” showpiece.

Our tour is now coming to an end and we are standing in front of the last booth, which is still missing from our explanations. Our own booth neighbour was Jörg Erkel (1zu220-Shop), who had deliberately dispensed with his “vendor's tray” and had chosen his presentation in such a way that he could also see something of the exhibition and at the same time had time for discussions.

Nevertheless, he also had two new products to present: First, the chrome oxide green 631 equipment car (item no. 49.343.71 from FR for Era IV, which went on sale after the exhibition and is no longer available.

The first sample he was able to show was the retractable tarp cover car of the Shimmns type with Wascosa paintwork and inscriptions, which was ordered exclusively from Azar Models. In the meantime, these models have been delivered in a pack of three and are still available. They are covered in more detail in the news items in this edition.

Although seaside holiday motifs did not fit the season, the “Det Lille Sommerhus” diorama is one of the most beautiful in its collection and knows how to immerse all train compositions in a particularly appealing ambience.

There was a wide variety of train operations on the “Wakenfeld” layout, particularly characterised by a range of rail bus sets. It was no coincidence that Jörg had placed two chairs in front of the peep box.

Many visitors accepted this invitation, took a seat and enjoyed the calming effect of passing trains, only to realise by chance that digital operation is taking place here. How else could you explain the background noise of a “red hummer” travelling past them so perceptibly that only the diesel smell is missing?



“Wakenfeld,” on the other hand, was home to the “red hummer.” Thanks to the staging yard behind the scenery, there was an alternating train service which also accommodated the Bundesbahn class 798 / 998 several times, in different formations.

Unfortunately, we have now come to the end of this report far too quickly. And that's exactly how all the participants felt: it was great, but it went by far too quickly time and time again. Now we have to wait another two years and we won't be the only ones who will miss travelling to Zell in less than twelve months.

But we would like to look ahead happily, because the big meeting in Altenbeken is getting all the closer. The time will come again in mid-May and the Zetties will be travelling to the railway town in the Egge mountains from much further distances. The only thing missing will be the Christmas atmosphere of the 2nd Advent weekend!



Webpages of represented exhibitors:

<http://www.michas-moba.de>
<https://www.rainer-tielke-modellbau.com>
<http://www.trafofuchs.de>

<https://velmo.de>
<https://www.z-freunde-international.de>
<http://www.z-lights.de>

<https://www.1zu220-shop.de>

Advent Meeting 2023 - Episode 23 on Trainini TV:
https://youtu.be/iE1gLc5j5ZU?si=XsvROF7Jh4raRWk_

A year with Z gauge

Our annual Review

Before we turn our attention to the new spring products and new ideas inspire our senses, we take a traditional look back. 2023 was the first year after the big gauge anniversary and also the first that was no longer characterised by the pandemic. Many were therefore drawn back into the company of other model railway enthusiasts.

There are certain traditions that we start with every year. These include, for example, the new releases of the year, which we sometimes announce in January, otherwise in February. This also depends on how close we are to the end of the year for deliveries that deserve to be considered and evaluated in this respect.

2023 began with the awards. Märklin's V 188 had proven to be an all-round success and attractive, so it should have come as no surprise that it also emerged as the winner of the editorial vote.



The year 2023 began with our editorial awards. Our construction report on the Ardelt 57 tonne crane was particularly well received, while the prize in the Technology category had already gone to NoBa-Modelle.

This was tighter in the area of cars, where a Märklin model was also able to prevail because the Pwgs 41 goods train baggage car was also able to close a large gap in the programme. Because all good things come in threes, we have also honoured Märklin for its complete work, as the reference year marked an anniversary that only this manufacturer, as the inventor, has completely covered with its range.

But the winners in the accessories and technology categories are also worth a brief mention. Artitec is one of the most outstanding accessory manufacturers and has emphasised this with its car ferry for river crossings. In the case of the 57 tonne Ardelt crane from NoBa-Modelle, it was initially not so easy to find the most suitable category.

After all, this model fits just as well in the trolley category, but offers far more application options with its swivelling body and fold-out supports as well as various body options. That's why it competed in the technology category and was the clear winner. Many reader opinions that we received also proved us right here.

And so we finally entered year 1 after the 50th anniversary of the gauge. We started this year with a handicraft contribution from Jochen Brüggemann, who had built some of his layout buildings entirely by himself. The JNR's EF58 locomotive, which Rokuhan had sent into the race as a model and was portrayed by Oleksiy Mark, spread international flair.

The news that Panzer-Shop.nl was discontinuing its Z gauge range due to a lack of demand was met with regret. Fortunately, the decision only lasted a few months before this supplier returned. However, we did not receive any explicit explanations as to why a rethink took place.



The ETA / ESA 150 from Märklin is representative of the toy fair that is taking place again after the pandemic and many great new products that are eagerly awaited. Illustration: Märklin

February is always the month in which all the new spring products are or will be announced, which is why we carefully collate, prepare, and present them in the magazine in as much detail as possible. After all, this issue is also intended to be a reference work for at least the first half of the year.

In 2023, however, we were also concerned about the loss of importance of the Nürnberger Spielwarenmesse (Nuremberg International Toy Fair) for the model railway sector, as hardly any manufacturers came there. Even if there are signs of a recovery in 2024, it remains to be seen how subsequent years will present themselves.

If we look back at the announcements made in the spring, we can see that there are quite a few backlogs. The V 60 and the ETA / ESA 150 from Märklin and the eagerly awaited 3D printed figures from Noch, which were already scheduled for June 2023, are particularly memorable.

Looking back, books rarely seem worthy of a headline. They are the salt in the soup for many model railway enthusiasts, but certainly not a highlight of the year. The situation is probably different with the titles on special travel agency traffic and the Bundesbahn couchette coaches: they are definitely on the wish lists of our scale because they stand out on the layout.

They also awaken holiday memories in older readers that would have deserved to be recreated in a model. Thanks to the meticulous work of Armin Gärtner, this time will not be forgotten and our imagination and longing will be stimulated. In November, we had a personal meeting with the author, which we also reported on briefly, and were able to see for ourselves that the project is almost complete.



Our reader Reinder Rutgers left our readers with astonished faces with his 750 mm 1:220 scale narrow-gauge model railway. Photo: Reinder Rutgers

The northernmost Z gauge meeting in Bad Schwartau will also be remembered for a long time. We reported on it in detail and also produced a separate episode on **Trainini TV**. We personally regretted that none of the members of the editorial team, at the time, could be there – it was simply not possible at the end of January during the preparation of the February issue.

The issues for March and April were received with a bang by many readers. While there were once suggestions from this circle to also report on T gauge, which does not fit into this magazine due to its scale, it was then included in a roundabout way, spread over two issues.

This was triggered by our reader Reinder Rutgers, who had long been enthusiastic about the narrow gauge and had found a way to transfer the 750 mm gauge to our area using the even smaller scale.

He passionately described his deliberations, plans and tests, and then set about building moving locomotives and carriages, as well as creating visually appealing stand models.

He impressively illustrated the diminutive size of his models, which nevertheless boasted many details, and also created suitable backdrops to make them look close to the original and by no means so tiny. Rarely have our readers discussed our articles so extensively outside the magazine as they did here. Even months later, members of our editorial team were still being asked about it.

Märklin offered its first “Message Wagon” for the Z nominal scale on 10 March. It was dedicated to a work of art and motto by the Dutch artist Vincent van Gogh, as was a follow-up model, which followed in the second half of the year and was portrayed in detail by us (**Trainini®** 11/2023). Both together open up a new collector's series that can also appeal to customers outside the model railway scene.



Märklin's “Message Wagons” opened a new chapter for collectors and enthusiasts. Both cars from 2023 were dedicated to Vincent van Gogh.

The On traXS! exhibition in Utrecht always has something to do with art in the broadest sense. We also reported extensively from there, as this was only the second time that a Z gauge layout had been selected for this special event: the “Boisshot Statie” by Pieter Willems and Jan van Casteren, which we honoured.

We also don't want to forget the Faszination Modellbahn, which was held again in Mannheim with a large Z gauge presentation, as we also reported on this exhibition in detail in this magazine.

In April, however, the “Breakdowns” section was occupied by the Märklin 01.5 series express steam locomotive, which initially went on sale without the typical mixing, preheater box – a test report followed



Märklin did not prove to have a good hand with new moulds last year: The V 320, which was actually very successful, was affected by a product recall.

together with the V 320. Four months later, we were able to explain to our readers how Märklin improved Germany's largest diesel locomotive.

Viessmann, who entered the hard cardboard modelling market with a small workshop under their Vollmer brand, had done better from the start. They had further developed the building material into Polyplate, which proved to be particularly stable in our test, as well as, close to prototype appearance of the surfaces.

We then enthusiastically gave the small kit an attractive interior and presented the overall result in the magazine. This was so well received by the manufacturer, that we were asked to produce and supply new product photos of the model when it was finally delivered. The result can now be found on the company's own website and will also adorn the product packaging in a new edition.

Another sensation followed in May: we were finally able to present the Märklin bunny box. The Zetties have had to wait far too long for this important driving trailer. The previous model with the Karlsruhe head developed at the beginning of the seventies also appeared until the end with hopelessly outdated light bulbs, which usually only gave a hint of light emission.

In the same month, we reported in detail on the Intermodellbau in Dortmund, which also had a lot to offer for the Z scale – sometimes we just had to search for it. This did not apply to the highlights of the new Miwula section “Patagonia and Antarctica”, realised on a scale of 1:87, the reports from Hamburg on new sights are a worthwhile excursion destination.

In an obituary, we honoured the work of Klaus Hammesfahr for the Z gauge. He was an active member and co-founder of the ZMRR. In this capacity, he was one of the pioneers of networking within our scale for the exchange of ideas and experiences in the 1980.

The Italian supplier GMmodelli Torino made its first appearance in our nominal scale. Initially offering a 3D printed locomotive body for the 1:220 scale, this was soon followed by its first car model.

He soon found a suitable sales partner in Germany in the form of the 1zu220-Shop.

The start of the relief house series at the Modellbahn-Union, which culminated in a construction report in December, is also particularly worth mentioning in a review.

Meanwhile, the Czech accessories supplier Yellow Dwarf, which has long been a regular in our magazine's news, has been a talking point month after month.

June marks the official start of summer with the arrival of the summer solstice.

This also had an impact on our theme for the year. Having already focused on nature and the chaos that defines it, we used the early



GMmodelli Torino is a newcomer to Z gauge and immediately found a German sales partner for its outstandingly successful products in the 1zu220-Shop.



In the "Fringe phenomena" series, we look at the many little things that are hardly noticed and often forgotten, but which make up the charm of an installation.

summer to take a look at the variety of colours and shapes that define the individual seasons and our environment, in general.

At the beginning of the seventies, colours were also a topic at the Bundesbahn. We therefore used the latest pop car exhibition in July to provide an overview of this experimental concept.

At the same time, we also put together when and how it found its way into the 1:220 scale, in order to then also present a self-built loner.



The Intellibox 2neo from Uhlenbrock is a tried-and-tested digital control centre that is popular in Z gauge. We have therefore presented the device, which was released at the end of 2022, in detail in this magazine.

extremely popular topics: The aforementioned steam crane from NoBa-Modelle now also received a detailed building report and thus really came to the attention of many customers and interested parties.

And the “Werdersheim” private layout also made many a jaw drop when the appealing motifs shown on it, were travelled through with the eyes. Incidentally, both reports also received their own episodes of **Trainini TV** to make the many impressions even more vivid with moving images.



Our reports on “Werdersheim” were very well received by our readers. Incidentally, owner Dirk Rohwerder has also immortalised his family name in one of the shops in the town centre, as this photo shows.

Returning to the theme of the year, forgotten and abandoned things (July) and forgotten and dilapidated places (August and October) are worth mentioning as grateful elements for layout design. Digital railway enthusiasts, on the other hand, will probably remember the report on the Intellibox 2neo from Uhlenbrock.

There was also an exhibition that had more the character of a large private meeting to describe in high summer: “Meeting of the regions” was the title we used when we reported on the gathering of two regular clubs in Sindelfingen – incidentally, there was already a repeat at the same location at the end of December 2023.

The August issue then contained two

Jochen Brüggemann also had his say again, presenting another of his own building projects in two parts. We visited the Modelleisen-bahner Soest e.V. (MES 03), who also have an active Z gauge group, to celebrate the club's 20th anniversary. The summer was also an opportunity for visits within the **Trainini®**-team.



In the summer, we were guests at the MES 03 in Bad Sassenberg to celebrate the 20th anniversary of the association and to experience the Z gauge digital layout on display.

We concluded the third quarter with a report on the Black Forest Railway Museum. Contrary to what the name might suggest, everything here revolves around model railways. In addition to the many custom-built 2-gauge models, there is now also a Z gauge layout, which provides contrasts and is the centrepiece of a special exhibition.

We drew attention to gaps in the programme with a conversion project for a Märklin tender. Our idea could close a gap in the programme, make new models possible and finally retire a form that has been “worn out” for over 50 years.

From an editorial point of view, it is definitely worth mentioning at this point that we grew to four people in September 2023 and then to five in December: Harald Fried and Ralf Junius have since joined the editorial team, bringing a breath of fresh air as well as new ideas.

Although not specifically designed for our track gauge, the Mobile Station WLAN, which we presented in October and also nominated for the best new releases of 2023, is nevertheless helpful and offers added value.

Märklin's customers have had to wait a long time for a wireless control unit that can be used as a small control centre as well as a hand-held control unit on a central station. The haptics of the device are likely to appeal to many readers, as a mobile phone with a touch screen will hardly be able to offer the same utility value in practical operation.



As Märklin does not want to consign its hopelessly “worn out” tender mould to the scrapheap, even after 52 years, we already had a construction proposal ready last year. NoBa-Modelle is currently examining whether a model with its own running gear should be offered.

The trade fairs and exhibitions in the second half of the year, which we reported on individually, offered colourful impressions: We started with the Märklintage in the October issue, which we reproduced in colourful abundance. We made stops at all the stations of this exhibition in order to capture the impressions outside of the Z nominal size.

We also processed them in a separate episode of **Trainini TV**, and we have now also published a review of the year, which also includes some new impressions of individual events.



The trade fair season began in September with the Märklintage in Göppingen, which once again included the Southern Germany Z gauge convention. There were also some special wagons for the occasion.

We took a look in a Far Eastern direction in November. We enthusiastically presented the twelve-car Shinkansen Series 0 in the “Hikari No. 1” version from Rokuhan. This was not just for our readers outside our core market of Germany and Europe, as the world’s first high-speed train is also attracting a great deal of attention everywhere else.

That is why we have also focussed separately on its role model and produced a separate feature on our film channel, which also clearly shows the installation of the interior lighting. At the same time, the operating shots on a new diorama by our translator Christoph Maier will leave a lasting impression.



Shinkansen and our translator's diorama are a perfect match! Rokuhan's novelty could thus be successfully staged. Photo: Christoph Maier

The subject of another episode on **Trainini TV** was the model railway exhibition in the Hochdahl locomotive shed (near Erkrath near Düsseldorf). In the November issue, we impressively showed how many 1:220 scale exhibits there were to see and also described in detail how a club was able to consistently organise its most important event with children in mind, and how it met with a correspondingly good and high response from young model railway enthusiasts and their families.

The club model selected by Märklin for the following year is usually also presented in November. With the Wismar rail bus in DB design announced and presented for the first time at the trade fair in Friedrichshafen, the manufacturer seems to have struck a chord with its customers. We also see this vehicle high up on the wish lists of the Zetties.

The report on the trade fair did not follow until December and took a critical look back. While Z gauge was already well represented in terms of space with a large US modular layout, it also presented a multi-faceted picture at the Z-Freunde International stand, but in some places missed the opportunity to attract further attention from walk-in customers with targeted eye-catchers.

We also reported extensively on Model Railway Day. We visited the Hobby Show in Duisburg, which was one of the first events when this worldwide celebration of small railways was launched. It has been held regularly ever since, and always brings together many scales and themes in a very special atmosphere.

Jochen Brüggemann's construction report on camouflaging Märklin turnout drives was the result of a reader's question and request. It deliberately presents a simpler, but also effective, alternative for all interested parties who cannot, or do not want to, swap to underfloor drives.



This photo was taken at the Hobby Show in Duisburg on Model Railway Day and shows the "PeZuBox" layout of our editor Ralf Junius.

All you need is a steady hand and a bit of tinkering. This is also required for Märklin's fourth new model that we were able to present. The Sgns 691 container wagon certainly fills a gap in the range for the modern eras.

It was not without reason that we ran the headline "It doesn't work without faults": The Göppingen-based manufacturer knew how to deliver this new model with avoidable deviations from the prototype. Although it only concerned red buffers and a neglected colour differentiation between the outer solebars, from the customer's point of view, a bland aftertaste remains.

If 100 % of all new moulds show deviations from the prototype, which even a layman can quickly recognise, the company's own claim to have developed Z gauge into a "professional model railway" is again called into question, at least in part. We are therefore deliberately trying to clearly dictate some "house rules" here.

The end of the year is always marked by two events. One of these is the vernissage for the new museum trolley, which traditionally takes place on the 2nd Thursday of December in Göppingen. Our representatives who attended were enthusiastic about the atmosphere and discussions, but we also heard some critical voices from our readers.

The selection of a covered goods wagon was perceived as boring, especially as no delivery vehicle is included. These impressions were reinforced by the Insider Club wagon 2024, which was announced on 8 January 2024 with the spring innovations.

This is another covered wagon and not for the first time one of the Dresden design, which also provided the mould for the museum wagon. According to the opinions we have received, this is perceived as unimaginative and lacking in creativity, a criticism that we have already levelled at the model for 2022. It seems to have gone unheard.

So let's take a look at something that everyone agrees will be remembered as a success. We are referring to the biennial Advent meeting in Zell an der Mosel. We report on it in detail in this issue and the family Christmas atmosphere, which is always worth a trip, is always praised.



This review will end with one final point, which will also look ahead. Our annual focus topic for 2023 did not live up to the editorial team's own expectations. "Marginal phenomena" offered many topics and facets that we specifically wanted to put in the spotlight.

Unfortunately, the postponement of the delivery date for the 3D printed figures from Noch has spoilt our plans. They seemed predestined for the two articles on human and animal figures, which we have prepared and will also accompany with a film sequence.

They were originally due to be delivered in June, but even at the time of going to press for this issue, we had not yet received them. We have therefore made a decision in response to this.

The article originally planned as the last instalment, which was actually supposed to deal with the edges of roads and waterways, will be brought forward. We will submit the two outstanding reports later, but will include them in the counting sequence as soon as the figures are available.

This also means that it is time to name the new focus topic for the year. We are once again focussing on model railway beginners and their needs, but are deliberately taking a two-pronged approach here. Under the title "The path to your first layout," we would like to present two alternatives.

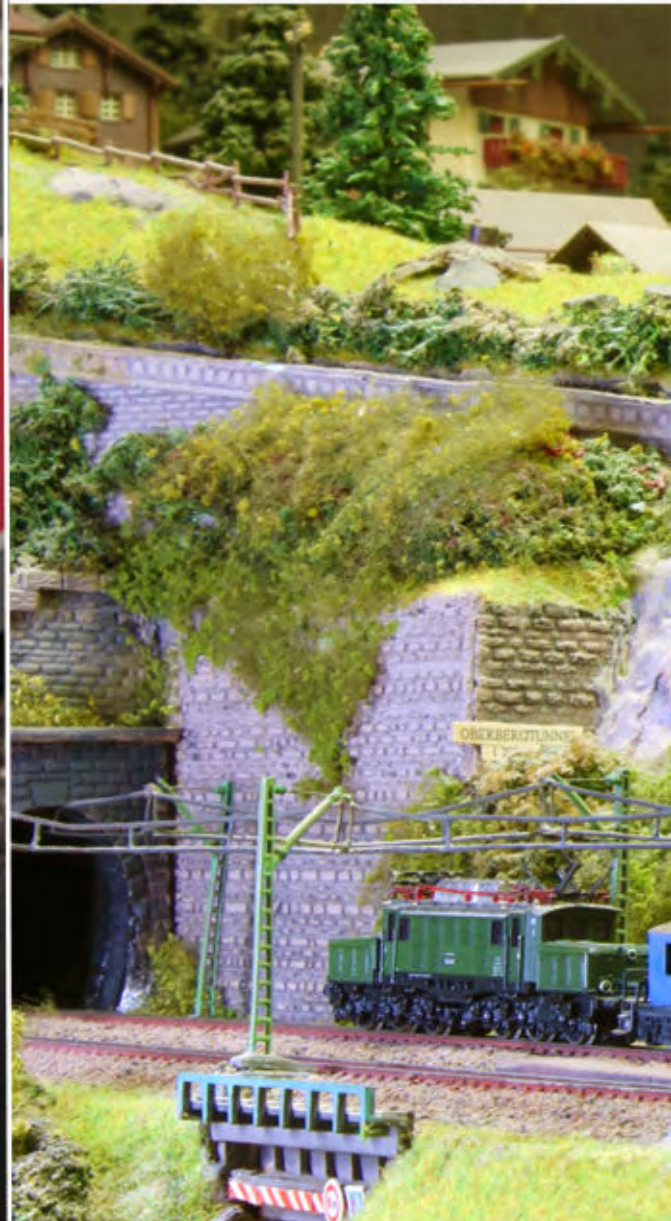
They differ in the craftsmanship and design skills required, but also in the track material used and the season and landscape depicted. So there should be something for every taste and something for every reader that can be used for their own projects.

However, it should also be clear that this cannot possibly be accommodated in a single year without overloading the monthly issues. We are therefore deliberately stretching the reports over two full years for the first time, making it the focus topic for 24 months in 2024/25. You can look forward to it!

Review of 2023 - episode 24 on Trainini TV:
<https://youtu.be/GeC7yxfsv4?si=x-OmrcekCtI8O1yt>

SONDERAUSSTELLUNG KLEIN TRIFFT GROSS

50 Jahre Baugröße Z, 1:220
19. März 2023 bis 17. März 2024



märklin



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Besuchsinformationen:

www.eisenbahnmuseum-schwarzwald.de
Infotelefon +49 (0) 74 22 / 29 - 3 00

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78713 Schramberg



Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Every year, a special greeting delights us:

Thanks again for all the good reading material in the magazine, it's fantastic to be able to read so many articles about our gauge. Best wishes to you all!

Martin Zimmermann (Schweiz (Switzerland)), by E-Mail



This sample shows historical train traffic and a railway building at Semmering. Photo: Martin Zimmermann

Answer of the editors' board: The New Year greetings we receive from our reader Martin Zimmermann are a cherished tradition. As the old year draws to a close, they already come to mind and we are eagerly waiting to see which photo will delight us this time. We would therefore like to take this opportunity to publicly thank him and return his best wishes! It's just great to have such loyal readers.

Request for assistance from a reader:

I had Oliver Passmann add lighting to a number of Z passenger coaches years ago. Over the years, I have had a lot of fun with them. In the meantime, however, some of the carriages should be overhauled. Because I assume that the Trainini team has the best overview of the Z scene, I would like to ask who does such work today (like Mr. Passmann used to do).

(...) I would like to take this opportunity to thank and compliment Trainini. (I) have been a loyal reader for years and look forward to great reports and information every month. I have already been able to use many a useful tip.

Above all, I admire your voluntary commitment and the time you put into each issue. We readers appreciate what you and your team offer us throughout the year. All the best to you and a successful 2024.

Joachim Karsten, Weinstadt

Answer of the editors' board: We are very pleased about the praise and greetings. As Oliver Passmann, unfortunately, died unexpectedly and suddenly, there was no handover of the company or technology. We are therefore unable to name a permanent successor or suitable maintenance company. At best, a free enquiry to companies that have the necessary technical knowledge will help. We can think of Bahls Modelleisenbahnen (link via our homepage), Velmo (link via our homepage), or Michael Boost (<http://michas-moba.de>)

Faller kit delivered:

Faller's hard cardboard kit of the Langwies railway station (item no. 282702) has arrived in stores. The Swiss-style building is made from 91 individual parts in seven colours. With its wooden appearance, it fits perfectly on alpine layouts and shows a special design style. The manufacturer characterises this kit, which can be used from Era II, as sophisticated.

Withdrawal from the railway museum:

After more than 23 years, the Stammtisch Untereschbach e.V. announced at the beginning of January that the association had decided with a heavy heart to give up its beloved location in the Dieringhausen Railway Museum. Instead, it intends to focus on organising events elsewhere in the future.

The model club has been active in the Bergisches Eisenbahnmuseum since 2000 and was present with Z gauge exhibition wagons. The active members showed a colourful cross-section of the history and possibilities of our scale in refurbished freight wagons, in which they also welcomed visitors and were always ready for a chat.



The exhibition car of the Stammtisch Untereschbach e.V. was once clearly visible in the entrance area of the museum.

The sudden departure from this unique form of a Z gauge exhibition is probably no coincidence: The museum hosting the exhibition has now found a new owner in Aggerbahn GmbH.

This is likely to be accompanied by changes in the concept and exhibits, as well as its orientation.

For the Stammtisch Untereschbach e. V., this would have meant new costs that the association did not want to bear, and would not have been able to bear in the long haul, as decided at its annual general meeting.

Therefore, as announced in a press release, the decision was made to discontinue the permanent exhibition at the end of the 2023 calendar year. As a result, the three club-owned goods wagons were also sold.

Its expansion had taken around 3,300 hours of work by the end of 2022. During the exhibition days (Saturdays) from mid-2000 to the end of 2022, around 10,000 people visited this unique presentation. Children's bags were always handed out on the driving days so that even the youngest visitors would remember their visit for a long time to come.

Winter new products at NoBa-Modelle:

We are particularly pleased that we can now also show photos of the VT 246 diesel multiple unit (item no. 5231R) presented as an announcement, as this outstandingly successful model appeared somewhat compressed on the CAD representations.



The DB class VT 264 / 624 diesel multiple unit (item no. 5231RF) with the available inscriptions in its original purple-red livery is finished. Photo: NoBa-Modelle

This is not the case with the kits that have now been delivered, which are driven by modified Shorty bogies. NoBa-Modelle also supplies lettering for the purple-red, three-part train, but painting it makes some demands on the buyer, due to the decorative strips and front mask.

If you don't have the confidence to do this yourself, you can now also switch to a ready-made model (5231RF), for which you will of course have to wait a while.

The ÖBB diesel locomotive Rh 2043 follows on the heels of this new product. Three versions are available here: a finished model with its own running gear (5023RF), and a kit without (5022R) and with running gear (5023R).



The ÖBB diesel locomotive Rh 2043 (5023RF) is also an eye-catcher in the Valousek design. Photo: NoBa-Modelle

The new Fiat 500 (6731R) not only fulfils the passion of one half of the tinkerer duo. Any customer who is no less enthusiastic about this traditional and revitalised vehicle should be able to paint it.

The 10 privacy screens (110022) made of resin are suitable for hiding

all the things that small figures on the layout should not see, but the observer should, because they are part of the realistic scene design. Once painted, they are ready for use and waiting to be glued in place.

All new products can be found and ordered on the following webpages: <https://www.noba-modelle.de>.

Märklin deliveries at the beginning of the year:

As in previous years, Märklin is delivering some new products to the specialised trade after the Christmas business. This year, the “Pantone Color of the Year 2023” car (item no. 82163) and a container wagon pack have opened the series (82664).

This is declared for Era IV and consists of four two-axle Lgjs 598 wagons and a Bundesbahn Sgs 693. The composition is from around the mid-1980s and is loaded with 20' and 40' containers from various transport companies.



New PANTONE Color of the Year 2023 Car delivered (Art.-Nr. 82163).

The changeover levers of the brake system have not been colour-coded on the models, and the ends of the containers are not printed either. Unfortunately, the four-axle carrier wagon, like all its predecessors in the Märklin programme with type Y25, has been given the wrong bogies (correct: type 664 “Minden-Siegen”).



The new container wagon set is suitable for Era IV and includes three 20-foot containers from the former DDR.

The three 20' containers from the DDR are certainly a plus point, as they represent a première on this scale. Hardly noticed at the time, they also appeared more frequently in the West and brought export goods to the Federal Republic, which were important foreign currency earners for the DDR (East Germany) planned economy.

The two-part passenger car set “compartment coaches” (87042) for the DB around 1958 is still fresh in the race for customers' favour. They already have an interior from the factory and each has a different grey roof colour. The two cars match their predecessors from the set visually, but not in terms of lettering 87040.



The two compartment wagons (87042; photo above) and the 44 1315 in its locomotive shelter (88975; photo below) are also among the new products delivered in January 2024.

The reason for this is that the newly delivered copies have already undergone the class reform. They bear 1st and 2nd or only 2nd class markings, whereas the previous edition still had 2nd/3rd class markings.

The freight steam locomotive 44 1315 (88975) is also currently arriving at the dealers, which is on the Märklin factory premises as a monument locomotive. Its scope of delivery also includes a hard cardboard kit for the underground, which is reminiscent of Märklin H0 packaging and protects it from the weather. The locomotive model is prototypically equipped with disc wheels (front bogie) and spoke wheels (rear bogie) on the tender.

News from Wunderland:

Miniatur Wunderland is continuing its “I can't afford it” campaign in January 2024. Every day except Saturdays, socially disadvantaged people can take advantage of free admission until 31 January after making an honest self-assessment.

Waiting times must be accepted, but the operator's website provides information on the most favourable times. Anyone who cannot afford the entrance fee should indicate this at the cash desk with the above statement or present a slip of paper with the same message. They will then be issued with a free admission ticket without being asked.

We would therefore like to mention and honour this commendable action, which is only possible in an economically successful and healthy company, as a special commitment in our magazine.

It is also an extraordinary success for the founding twins that their eventful childhood and youth, which ultimately led to the realisation of the idea for the Miniature Wonderland as they grew up and matured, has been made into a film.

After a successful and sold-out cinema premiere in Germany's third largest screening theatre with almost 1,000 seats, the distribution company has now decided to bring this documentary to cinemas in the rest of the country, as well.

The screenings will start on 7 March 2024 and all visits up to 10 March will also count towards next week's success charts. It is now in the hands of model railway enthusiasts to make this film a success, too.

New Azar special series:

At the suggestion of the 1zu220 shop, Azar Models (<https://azar-models.com>) has produced a three-part "Wascosa" sliding trolley set (item no. W03-WA3). On the layout, the modern wagons in dark blue-orange design certainly stand out and enrich Epoch VI layouts without any doubt.

This will also ensure that a spelling mistake on the long sides (tarpaulins) was not noticed in time to change this during printing: The German-language page refers to a "goods wagon system".

We think this is worth mentioning because it cannot be ruled out that this would be corrected in subsequent editions, resulting in relevant variants for collectors.



Fans of prototypes should not only pay attention to the paintwork and lettering, but also realise that the new telescopic hood cars from Azar Models (item no. W03-WA3) also differ from Märklin in their basic shape because the origin of the prototypes is not identical.

Equally available are three-packs of SNCF without logo (W03-SL3) and with SNCF-Fret symbol (W03-FR3). A good sample of the latter version was already on display in Altenbeken 2022.

It is also important to note that the terms of the financing plan for the TGV have been amended to accommodate interested parties: The end date on Ulule has been extended to 29 February 2024, the financing target has also been lowered and instalment options have been added.

According to the manufacturer, almost 50% of the new target has now been reached thanks to contributions from Switzerland and Germany. If the campaign target for Ulule (<https://ulule.com/tgv-echelle-z/>) is not reached, the project will be cancelled and the contributions will be refunded. If it is reached or exceeded, production will take place.

Moïse Rogez, owner of Azar Models, emphasised his ambitious plans to our editorial team: "My only goal is to start production of the smallest travelling TGV in the world." He explained that it was not just about this one model. All other Azar models should then gradually benefit from all the technical and technological innovations that have been planned for it.

AZL continues on a bright note:

Shortly after the last issue went to press, we received further information from American Z Line about deliveries of new products. So we now have a longer list to work through here.

For example, there are the five-part Trinity RAF column wagons with semi-trailers, which are being added in two versions: with advertising for Amazon Prime (item no. 905201-1) and Triple Crown (905232-1). Two semi-trailers are also available separately (954001-1 / 954008-1).



The Ortner bulk goods wagons are new in shape and will initially appear for the CSX (item no. 905353-1). Photo: AZL / Ztrack

The Ortner bulk goods wagons are being launched as a new design. The first edition is painted metallic and labelled for the CSX. This version is already provided ex works with a load insert whose matt grey painted surface is intended to represent ballast, but cannot be perceived as such due to the lack of angular stone structures. Packs of two (905353-1) and four (905303-1) are offered in this case



The EMD SD40-2 appears in other versions, including for the CSX (64209-3; photo left) and as a tunnel locomotive for the SP (64100-1; photo right). Photos: AZL / Ztrack

The wide-view freight car is now being issued in the red livery of the SCL/LN (921027-1). The heavy passenger coaches are now being followed by a Santa Fe half-baggage coach rolling on three-axle bogies in olive green livery (74028-1 to -3).

The EMD SD40-2 is followed by a few more variants and versions: ATSF (64201-1 to -3), CSX in two roof colour versions (64209-1 to -3) and tunnel locomotives SD40T-2 of the SP (64100-1 to -3) with "bloody nose".

Something special from Modellbau Laffont:

Modellbau Laffont delivers a very special model with the semi-relief model of Aachen Central Station (item no. Z8401). The model was found at the company's own production site and appears to be perfectly reproduced.

The Art Nouveau building has been reproduced from the railway side and thus allows a larger station near the edge of the layout to be credibly reproduced and prototypically closed off from the background scenery.



Aachen Main Station (item no. Z8401) as a semi-relief model is an interesting kit for many urban layouts. Photo: Modellbau Laffont

Like all kits from this supplier, the parts are made of solid-coloured architectural hard cardboard and feature the finest and most irregular quarry stone engravings, following the original. This building can be used from Era II to the present day. The scope of delivery also includes a platform, stairway and maintenance walkway.

The original is served daily by long-distance trains of the highest category and is a dividing station in the border triangle of the Netherlands / Belgium / Germany and acts as a gateway to Europe with important international connections.

The manufacturer can be found with its own sales organisation at the following address and can also be found in the list of links on our portal pages: <https://modellbau-laffont.com/c/bausaetze-spur-z-masstab-1-220>.

Personal visit possible:

Buying tools is a matter of trust and requires not only advice but also a personal impression of the planned work aid. Jürgen Schirmer from Werkzeuge Peter Post knows this, too. That's why he informed us of his planned trade fair appearances right at the start of the year.

It all starts with Erlebnis Modellbahn in Dresden from 9 to 11 February 2024, followed by the next opportunity to visit a stand in Mannheim, where Faszination Modellbahn will take place in the Maimarkthalle from 8 to 10 March 2024. Intermodellbau at the Dortmund Messe (Exhibition Centre), which will take place from 18 to 21 April 2024, will then offer a traditional opportunity for advice and purchases.

You can find the pages of this provider via our link list on the portal pages or reach them directly at <http://www.peter-post-werkzeuge.de>.



Yellow Dwarf – completely original:

Not a month seems to go by without new products from the Czech Republic. Yellow Dwarf (<https://www.yellowdwarf.eu>) has supplied valuable accessories for the weekly market with two different fruit and vegetable stalls (art. nos. 60481 and 60482).

However, the “Car Wrecks I” (60261) seem even more impressive, turning every scrapyard into an eye-catcher for layout viewers. Suitably painted, no-one will pass them by, and certainly will take a close look at vehicle after vehicle.



The “Car wrecks I” (item no. 60261; photo above left) represent a previously unavailable item of equipment, the two weekly market stalls (60481 & 60482; photo below) are similar, but stand out clearly in their detailing. Photo: Yellow Dwarf

Used models and spare parts:

Freight specialist Küpper from Aachen, which is looking back on its 30th anniversary these weeks, recently pointed out that, in addition to various spare parts, a number of different freight wagons are once again available as used goods from its own sales department.

Of course, these can also be stocked and delivered by the supplier himself with the goods required by the customer. Interested parties can check the stock directly on the website and put it together in the shopping basket. The manufacturer can be contacted at <https://spur-z-ladegut.de>.

Current Micro-Trains deliveries:

Micro-Trains has reissued light passenger coaches with end platforms, called “Business Cars”, as well as observation cars and plans to deliver them this month. MTL is offering the first type of car for the PRR (item no. 556 00 021), the UP (556 00 061) and Norfolk & Western (556 00 240).

The observation car is currently only available in the UP version (551 00 011) and for the Norfolk & Western (551 00 240). The prototype length of both types of passenger coaches is 83 feet. There is also a yellow heat protection car with lettering from the company W. S. Reister & Sons (518 00 861).



Observation trolley (item no. 551 00 011; photo left) and saloon trolley (556 00 061; photo right) in UP design. Photos: Micro-Trains

The “War of the Worlds” series includes a new goods train escort car with a gun on the roof (535 00 510) and an EMD F7A diesel locomotive (980 01 550), which is also painted olive green.

Brief review of Artitec:

As already reported in the last issue, Artitec has delivered two new products: the Hanomag K5 crawler loader (item no. 322.039) including mounted driver figure and the 7 tonne forklift truck (322.040). These are now available in the editorial office and can be discussed.

With the deliveries of the last few years, the Amsterdam-based accessories supplier has created a very high level of quality, which the two latest models also fulfil in every respect.

This can be seen in the many details of the models, such as the gear lever or steering wheel (on the forklift truck) and extremely fine attachments, of which, the lifting device and the perforated sheet metal roof of the forklift truck, are particularly striking.

A great deal of effort was also put into the paintwork and lettering, as the colour scheme is multicoloured, which means that not only are many parts set apart from each other, but also operating wear, ensure an authentic impression.



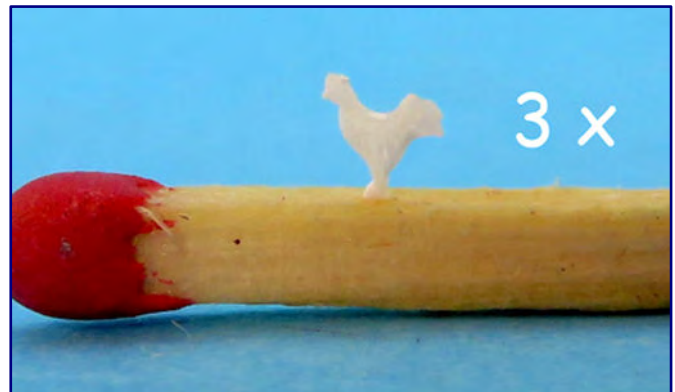
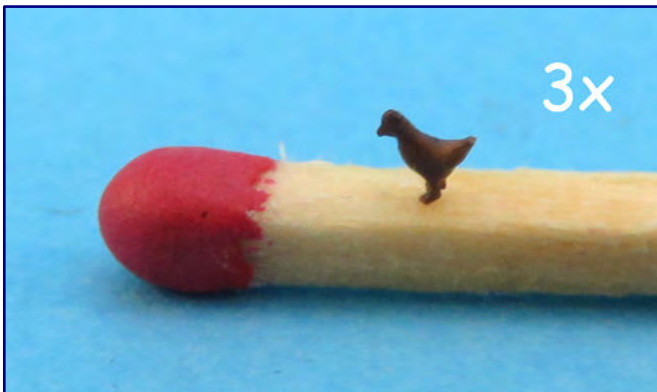
The Hanomag K5 crawler loader (item no. 322.039; photo above) and the 7-tonne forklift truck (322.040; photo below) also match the high quality of previous years.

It is worth having a magnifying glass ready for the caterpillar: otherwise, the manufacturer and type markings are difficult to make out, and the yellow and black warning sign on the lifting mechanism of the clearing shovel could also be overlooked.

On our portal pages, we have placed another photo for you with the corresponding message.

News from Klingenhöfer:

New figures and a reissue are available from Klingenhöfer Miniatures. New, for example, are the five brown ducks (item no. Z-TE10-1), which are suitable for reproducing female mallards. The five-part wedding party (EH07) is being reissued to celebrate the best day of your life in style.



The wedding party (item no. EH07; photo above) is a new edition. It is joined by three hens (Z-TH18-b; photo below left) and three roosters (Z-TH19-w; photo below right). Photos: Klingenhöfer Miniaturen

Three self-painting hens are available in the basic colours white (Z-TH18-w), brown (Z-TH18-b) and black (Z-TH18-s). The matching rooster is also available as a three-piece configuration for self-painting on a white base (Z-TH19-w).

To order, the supplier webpages can be found at <https://www.klingenhoefer.com>.

FR models already sold out:

The 1zu220 shop (<https://www.1zu220-shop.de>) had two FR models made by the Uwe Schuster assembly service. However, both were sold out as soon as they became known and available. We are therefore only mentioning them here for documentation purposes.



In our opinion, the ocean blue design of the 629 equipment wagon (item no. 49.343.81) should show an Ege biscuit instead of Lettern, but otherwise it is a complete success.

These are two construction train wagons based on the former Pwg 54, which may have been in service from 1974 as equipment wagon 629 in ocean blue (item no. 49.343.81) and the equipment wagon 631 in chrome oxide green (49.343.71), which in our opinion is probably incorrectly labelled with the letters DB instead of an Ege biscuit, which also bears UIC lettering and thus most probably already bore a designed logo in the prototype.

Heart and soul at Full Throttle:

William Dean Wright, owner of Full Throttle (<http://www.wdwfullthrottle.com>), professes to be a thriving supporter of the short-lived Penn Central railway company. He attributes this to his childhood home and family background.



The short-lived Penn Central honours WDW Full Throttle with this collector's pack (item no. FT-CLO7B). Photo: WDW Full Throttle

He has now reissued ACF bulk freight cars with cylindrical, covered car bodies (item no. FT-COL7B) as a collector's double pack. One example is designed in the green of the PC, the second in inconspicuous grey still bears the New York Central logo. Together they represent the heyday of the Penn Central after.

The following should be noted about the wagons and the railway company: Due to numerous early mergers, ACF Industries is represented in most states of the northern USA. This vehicle manufacturer is responsible for many innovative freight and passenger cars used in modern railway transport.

The Penn Central (PC) is known to railway enthusiasts as the ultimately failed attempt to unite three traditional competitors (Pennsylvania, New York Central and New York, New Haven and Hartford Railroads). It was active from 1968 to 1976 until its assets, along with other bankrupt north-eastern railways, were incorporated into the U. S. government owned Consolidated Rail (Conrail)(subsequently privatized).

In Germany, FT products are available from Case-Hobbies (<https://case-hobbies.de>), among others.

Spring innovations are published:

At the time of publication of this issue, further new products for spring 2024 are also being announced, including the design and accessory products from Artitec and Noch. We have already been informed about an important and central innovation from Wangen and recommend taking a close look at the brochure.



The most outstanding of all the new products known to date is certainly the Märklin class V 90 multi-purpose diesel locomotive (item no. 88510). With this prototype, even manufacturers of larger tracks have had to capitulate to features that the market leader from Göppingen also has the confidence to produce on a scale of 1:220. Illustration: Märklin

Märklin has already presented on 8 January 2024. We can expect a colourful programme that covers different eras with interesting products. In addition to the Wismar rail bus announced at the end of 2023, there is only one locomotive as a new shape.

However, the V 90 heavy shunting diesel locomotive (item no. 88510) is a hit and is already showing signs of becoming a sales success. Märklin can finally realise this locomotive in scale thanks to the slim bell-type armature motors.

The market leader also has the confidence to realise the red and white lanterns in the narrow box on the shunting platform. We will be presenting this new product and all the others in detail in the February issue following discussions with representatives of the manufacturers.

Imprint

ISSN 2512-8035

Bibliographic information of the German National Library: The German National Library lists this publication in the German National Bibliography. Detailed bibliographical data and editions can be found in the DNB catalogue at <https://portal.dnb.de>.

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International Edition

Free,
electronic magazine
for railroad enthusiasts
in the scale 1:220
and Prototype

www.trainini.eu

Published monthly
no guarantee

ISSN 2512-8035

Trainini

German Magazine for Z Gauge



Surprises in the Spring

Editorial Awards 2023
Better Pantographs

Introduction

Dear Readers,

Today I want to be completely honest. And I'm sure you won't blame me: I feel worn out and exhausted. It's not just because of the half marathon I completed as a training session last night.

Another reason is the amount of work this edition has given me. Of course, I'm not alone – I have three editorial colleagues at my side, whose help reduces my workload in many respects and complements my work.



Holger Späing
Editor-in-chief

And the workload peaks every year in January and February, when almost all manufacturers present their new products and projects. Again and again, we must go after some of them, because somehow it does not stick in their minds that nobody can buy something that they do not know, and that has not been made public.

Overall, things were better this year, and we are all happy about that. That's why I'm explicitly mentioning it here because I'd also like to say thank you for that. However, it seemed to me that the number of new products for Z gauge also increased significantly.

Please make up your own mind and reward the entrepreneurial courage if this impression can also be confirmed outside the editorial team. We were able to gather and analyse a lot of information and include it in the report, especially in my central role as a kind of hub.

It took me many hours and days, but it was also a lot of fun, because Z gauge is a hobby, I am very passionate about. Z gauge is after all a passionate hobby for me and not a profession.

I was rather expecting a hesitant disclosure of new items, because in the years of Altenbeken the meeting was and is often used to present something new "with a bang." If this continues, then it should definitely be an exceptional year for our little gauge.

The work of all of us in the editorial team should always provide an incentive to make good things even better, to close the "most painful" product gaps in the 1:220 scale, to promote creativity and to remain loyal to this nominal scale as a supplier.

This is why we also award our editorial prizes year after year. We did the same this time too and gave ourselves just four weeks more time. Today, we are sharing with you who was nominated, who made the cut, and what motivated us to do so.

Initial feedback has already been received and confirms our motivation. We would also like to provide such in a short but important article: there are well-intentioned items, but on closer inspection are somehow out of date. And there is always someone who can do it better. Stay tuned and have fun reading!

Sin-Z-erely,

Holger Späing

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We would like to thank Gustav Pavlou for his help with the trade fair research at short notice.

Date of publication of the German language version of this issue: 27 February 2024

Cover photo:

The "Zum Alten Markt" inn is perfectly integrated into village life, continuing the series of buildings that fit in with Westphalian Westheim. There must be something special in the adjoining corner shop today, because the neighbourhood is already queuing up.

Prototypical locomotive operation

And who invented it?

For many years, Z gauge suffered from two shortcomings: steam locomotives without replica controls, and electric traction with pantographs, which made every model look bad. Shortcoming number 1 seems to have been finally remedied since 2012; the second one, unfortunately, persists even after product maintenance. We would like to show you how big the problem really is and what possible solutions there are.

At the latest with the appearance of the Rokuhan class 181², every Zettie realised how filigree pantographs on the roof of a 1:220 scale electric locomotive should be. If we compare this with Märklin, then we are dealing with the proverbial apples and pears.

While Rokuhan strives for exact scale, Märklin's focus is on electrical function. And this requires significantly greater stability and must also be able to guarantee contact reliability at the same time.



Today, HOS Modellbahntechnik offers its own system of filigree masts with a thinner overhead line. However, this can also clearly demonstrate that Märklin's pantographs are anything but true to scale. In this case, not even the design is correct.

But now let's look deep into our eyes: Which Z gauge railway enthusiast uses an electrically functional overhead line? Their number is likely to be very small, because this only brings the advantage of independent two-train operation with analogue control.

This comes at the cost of the locomotive model not being able to change direction, as it draws its energy from the contact wire in line with the prototype.

Since only one pantograph is usually raised in prototypical operation, there is only one contact point available for current pick-up, and there are at least four on the wheels on all Märklin models ever offered.

The resulting lack of contact reliability can often only be compensated for by also applying the second current collector.

In the prototype, this only existed on old locomotives with a single contact strip. More modern locomotives with double contact strip only run with both pantographs on the contact wire in special situations, such as ice formation on the overhead line.

In Z gauge, it is almost always the norm. What may not bother the toy railway enthusiast will be unthinkable for the model-oriented hobbyist. Digital operation with far more extensive options for the separate control of significantly more railway vehicles would be more tempting here.

In short: We do not consider the electric overhead line operation to be of much help, and because of its punched contact wires from Märklin, it is even dispensable. The parts, which are now 50 years old, are no longer what we would call "contemporary".

And the market has also seen alternatives in the meantime: Heckl Kleinserien already offered an overhead contact line system for contactless operation many years ago, which was based on the system of the Deutsche Reichsbahn-Gesellschaft.

In terms of its effect on the observer, it meant a quantum leap: what the eye could not see in full scale simply has to be there for our minds. A locomotive with a fully extended pantograph and no contact wire, usually looks just as disconcerting as one that is travelling completely lowered.



If we look at 111 111-1 during its turnaround at Stuttgart main station on 7 August 2023, we hardly notice the thin contact wire above it. But, the single-arm pantograph also gives a significantly different impression than the Märklin conversions. Photo: Alexander Hock

Today, the supplier of a finer overhead line is HOS Modellbahntechnik. And Heinz O. Schramm's range has also been greatly expanded. At the same time, his range also includes small clamps that limit the height clearance of a pantograph.



The class 181² from Rokuhan demonstrated how a single-arm pantograph SBS 67 should look to scale, and, thus, also justified requests for bodies for Märklin models. The additional shortcoming that Märklin's SBS 65 counterpart cannot be laid flat is particularly disturbing on the dining cars.

They prevent contact with the filigree contact wire, an untensioned mast boom or tunnel gantry, as well as the strange appearance of a locomotive whose roof equipment stretches too far into the sky.



The first filigree and true-to-scale models on the Z gauge market were the SBS 10 from Heckl Kleinserien. They were designed for the announced and never delivered E 44⁵. Today, they are suitable, if still available, for the E 91⁹ from NoBa-Modelle, among others.



In the seventies and eighties, the huge pantographs were still accepted by the market, because that was as fine as it got in 1974 when the first 1:220 scale locomotive appeared. And, for a long time, Märklin had no competition. Two comparisons should show what is possible today, also to illustrate the need for action: Rokuhan's class 181² meets the class 101 as the second Starlight Express locomotive from Märklin (photo above).

Below, Märklin's class 150 has to compete with the shorty of the glass train from Rokuhan. It can be seen not only that the cheaper model has a much smaller pantograph, but that the contact strips are the same size. The diagonal struts, which are completely missing on the Märklin model, are also striking.

Weak point: pantograph

Up to this point, we have already been able to show solutions for some Zetties that shy away from using electric locomotives for various of the reasons mentioned. However, anyone who has made a decision as to whether they would rather dispense with the overhead line altogether, only install masts or cover them with a thinner overhead line from a small series manufacturer is still a long way from reaching their goal.

The pantographs themselves also proved to be a disruptive factor. While they were accepted as a compromise for a long time from 1974 onwards, and represented a familiar sight for us, we were finally made aware of what they actually were: out of scale and clunky.



The scissor pantographs on the roofs of the JNR 485 series electric multiple unit from Rokuhan look successful: filigree, correctly sized, correctly mounted and fitted with diagonal braces. The front, lowered pantograph also lies flat.

The SBS 10 old-style pantograph, also developed by Heckl Kleinserien, which was used on the E 17 (realised by Westmodel) or E 91 (announced by NoBa-Modelle), for example, looks very delicate compared to its Märklin counterpart, which is intended to replicate a DBS 54 (item no. 8955), isn't it?

Explanation of the pantograph designations
The names of pantographs were once chosen by the manufacturers themselves, but allow conclusions to be drawn about this and usually also about the approximate year of construction:

- SBS 10 Siemens railway pantograph (from approx. 1910)
- DBS 54 Dozler railway pantograph (from approx. 1954)
- SBS 65 Siemens railway pantographs (from approx. 1965)

The SBS 65 was the first single-arm pantograph manufactured by Siemens and Dozler. The following types were numerically incremented.

Dozler originally used the abbreviation DSA for Dozler current collectors, but then also changed to the Siemens scheme.

The differences here are not limited to the thickness of the material, but also the dimensions of the parts and, to a large extent, the height of a deposited specimen. Märklin has never been able to convince on either point.

There was therefore great joy when the single-arm pantograph, the first version of which (8956) was intended to replicate the SBS 65, was to find a successor. But even this was not smaller or significantly more filigree. The only immediately noticeable difference is a different lower shear, as the tube can now also be recognised as such, here.

However, its task was clearly to replace its predecessor one-to-one, i.e., to retain the electrical function, use the same mount in the vehicle roof unchanged, and offer no visual advantage.

Unlike the engine technology or the steam locomotive rods, the aim here was probably not to create a leap in time to what is possible today. So, the small step towards something closer to the prototype takes a back seat to the larger dimensions.

And, unfortunately, the gain is only marginal when the locomotives are not in use. This becomes particularly clear with multi-system locomotives, such as the Vectron, which have four of them on the roof, which literally kill a very successful model.



The newly delivered Vectron series 475 of the BLS (see also the news in this issue) has four of the newly designed Märklin single-arm pantographs. Please decide for yourself whether these really represent an improvement in appearance. Photo: 1zu220-Shop

As already mentioned at the beginning, Rokuhan has demonstrated just how big the difference is between the chosen solution and a full-scale realisation. The SBS 67 designed by this large-scale manufacturer is certainly not for rough hands, but its appearance is unbeatable. There was already plenty of experience with his Japanese models.

The search for alternatives

Realised in a similar size, but made of more stable metal, was our expectation of the new design from Märklin, which unfortunately was not fulfilled. But there is also the DBS 54 scissor pantograph, which can be used for (almost) all locomotives that do not fit a single-arm pantograph, even if they do not belong to the German Federal Railways.

The only exception in the Märklin programme is the US locomotive GG-1, which received a pantograph (609582) specially designed for it. This is larger than the German versions, but shows a few more details, and, also, appears more filigree due to its larger prototype dimensions.

Unfortunately, we look in vain for its diagonal struts on the German model, although they characterise the appearance of the prototype. In the model, the two scissors only appear to be connected via the sliding moulding.

For a long time, the alternatives were few and far between, because an SBS 10 with a completely different appearance cannot replace this type of pantograph. However, there is still an outstanding pantograph specialist who has long since shown a heart for the Zetties: Sommerfeldt.

There are two versions of an SBB scissor pantograph in its programme. Both are made of metal, so they are sufficiently stable and present themselves with the filigree design that we had hoped for from Märklin, as a recognisable step forward.



We have fitted the latest E 41 from Märklin with the SBB pantographs without insulator supports from Sommerfeldt (item no. 768) as a trial. Even if the distance between the scissors and the width of the contact strip are slightly too small for a DB locomotive, this retrofit part brings a clear gain in appearance! The diagonal braces, alone, make a big difference.

Because the contact wire in Switzerland is laid in a smaller zigzag pattern than on the DB, the SBB also uses narrower contact strips. This means that the entire pantograph can also be made narrower.

Nevertheless, the Sommerfeldt product without insulator supports (768) is the better choice, because this pair, which is nickel-plated white ex-works, is considerably closer to the scale size and does not overwhelm a particularly well-made locomotive model such as the still young E 41. On their roof, the original parts look like laundry (drying) racks that someone has not folded together after removing the dried clothes.

Here, too, Rokuhan had (unintentionally) made it clear to us how far apart scale and realisation are. On the shorty roof of the glass train, we find a single-arm pantograph and a scissor pantograph in a more stable version.

The retractable pantograph does not reach the filigree of its brother from the 1812 series by far, but it also forgives minor clumsiness and still has the edge over Märklin. The difference between the two is even clearer with the scissor pantograph: The contact strip looks almost like a foreign body on the fine, but thanks to the metal, very stable scissors.



The version with insulator support replaces two other Märklin parts and also changes the shape and alignment of the insulators in a fine design.

They have diagonal struts, which is already a plus point. However, if we place a Märklin model with its DBS 54 next to it for comparison, we are astonished to realise something important. The very large contact strip is almost identical in size to the one on the Märklin roof!

Only now does the prototypical nature of the Rokuhan scissors become apparent, and, with it, the hopeless oversizing of the entire Märklin pantograph. Time to act! What use is the most beautiful and finely detailed locomotive, if the roof details rob any illusion of a prototype, in advance?

Fortunately, there is an exchange option for older SBB locomotives that produces the expected photo and creates satisfaction. In addition to the Sommerfeldt pair already mentioned, this is also one with an insulator carrier (680), which also produces the correct carrier and insulator arrangement on the old vehicles.



And this is what a Swiss Crocodile Ce 6/8II looks like at today's painting and printing standards when it is fitted with the Sommerfeldt pantographs (680), in exchange. Here, too, the finer parts emphasise the delicacy and elegance of the historic locomotive.

After all, fans of Swiss models know that a railway without electric locomotives has been unthinkable for the Swiss for more than a hundred years. And Sommerfeldt has understood this. We therefore conclude this report in the hope that a single-arm design will follow from the specialist, which will make the more modern SBB vehicles, as well as the German and contemporary ones for the whole of Europe, look considerably better.

Manufacturer of the SBB pantographs:

<https://www.sommerfeldt.de>

Suppliers of overhead lines and masts:

<http://www.hos-modellbahntechnik.de>

<https://www.maerklin.de>

<https://www.rokuhan.com>

Grenzenloser Modellbahnspaß in 1:220

**9. Internationales
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New releases of the year 2023

Groundbreaking and Award-winning

The process of selecting the winners of our 2023 awards was an exciting one for the editorial team. There are several reasons for this, which we will explain in a moment. One month later than usual, we are now announcing our best products of the year awards and the winners of our honours category.

The past year was undoubtedly an eventful one in many respects. Generally speaking, it marked the end of the coronavirus pandemic, albeit with a somewhat slow return to normality in the model railway world. Everything that had longer lead times was visibly still affected by the events of the previous years.

This was already evident at the Nuremberg toy fair, which took place again for the first time after the break due to the pandemic, but recorded such a massive drop in exhibitors, compared to the previous edition, that it is unique in this form for the model railway sector.

The past few years have also been characterised by disrupted supply chains in many sectors. This caused many delivery schedules to be cancelled. And if you want to work profitably, you have to over-plan rather than under-plan your capacities so that the production lines do not suddenly grind to a halt in the event of unforeseeable problems. Even in normal times, this means that some production is carried over to the following year.



For the first time, five editors (from left to right: Harald Fried, Joachim Ritter, Holger Späing, Dirk Kuhlmann, and Ralf Junius) discussed the most outstanding and successful new Z gauge products of the past year. In the end, they certainly did not have easy decisions to make.

For our editorial awards, the delays in the release of new products already posed the dilemma of having to decide in some categories between only two nominated and almost equally excellent products. In one case, it even happened in the end that there was only one product to choose from, and the only question was whether it deserved the award or not.

Unfortunately, we also must note that the number of candidates has shrunk extremely, especially for the rolling stock categories, namely locomotives and wagons; we only have assured access to test models from Märklin and Rokuhan, with AZL unfortunately not responding to our enquiries, and Micro-Trains not coming out with any newly designed models for years, now. So, that is the situation with respect to the large-volume manufacturers.

As for the smaller manufacturers of rolling stock, the product pipeline seems to have dried up at the moment. Railex and Schmidt were discontinued many years ago, and no new models are expected under the Westmodel brand continued by Modellbahn Scholz. Heckl Kleinserien also seems to have disappeared a few years ago, and enquiries have not been answered since then, either.

And, we have not been able to find anything new amongst other producers either for a while. Most recently, FR Freudenreich Feinwerktechnik announced that he would be retiring from business at the end of 2023, which, as we all know, he has done.

Provided that we only nominate items that have been tested according to our criteria by our editorial team or a neutral authorised representative, the range of small volume producers of rolling stock is currently reduced to NoBa models.

Finally, the addition of two more people to the editorial team last year had a further influence on our selection and voting process: For the first time, five, instead of three editors participated in the vote for the 2023 best new products.

While the previously smaller group of editors were already quite good at gauging each other's preferences, market assessments and preferences for key product features, the voting process within the now larger group was more exciting than ever before.

All in all, this means that our decisions were not easy and certainly not predictable. What was lacking on the market side in terms of the excitement was made up for in the decision-making process. It is therefore important again that not only the vote of each editor counts in the end, but also to explain the reasoning behind the final decisions.

And with these explanations, we now begin our selection of the winners.

Locomotive category

It is not every year that models from small and large volume producers compete against each other. In 2023, the number of nominated products was again limited to those from the industrial sector. Märklin had launched two new locomotives in the period under review, one of which made it into the nomination: the V 320, which first appeared as a club model in DB design (item no. 88320).

Despite its weaknesses of a buffer beam that is too high and the engine room lighting that only covers the side windows, but not the skylight windows, it was rated as a very good model. The product recall had not deterred us, because Märklin had reacted quickly and purposefully, here.



Despite its existence as a loner in real life, it was clearly seen as an enhancement for Z gauge, and also proved to be a powerful draught horse for the heaviest trains. And it certainly found support amongst the diesel traction friends in our editorial team.

Nevertheless, the Shinkansen 0 Series “Hikari No. 1” (T020-1) from Rokuhan came out on top by a narrow margin of three votes to two. As an argument in the voting, it was also emphasised that, as an internationally read magazine, we should focus equally on foreign products.



In the locomotives category, the 0 Series Shinkansen “Hikari No. 1” (item no. T020-1) came out on top by a narrow, but deserved margin. Photo: Christoph Maier

And the fact that this model did not show a single weak point in the test, also spoke in favour of the Shinkansen. Precisely scaled details such as the finely detailed scissor pantographs and constructive consideration of special features such as the backlit parabolic tip of the first two trains made the small, but ultimately decisive, difference here.

Further arguments in favour of this twelve-car train were its significance in terms of railway history (model selection) and the cohesive train design, which only Rokuhan seems to be able to implement regularly, in such a successful way. This has also been pointed out to us several times by readers, as particularly praiseworthy, and confirmed Rokuhan's expertise, especially for railcars and multiple units.

And so, it seems well deserved that this year's award in the locomotive category goes to Japan, where this praise from the editorial team (and readers) was gratefully received, also as a motivation and incentive for future developments.

Wagon category

Here, too, there were only two nominated products, both of which came from Märklin this time. It was therefore clear from the outset who would win the award, but it remained exciting to see which of the two new products would emerge victorious and with which voting ratio.

We found both of the products on offer to be equally successful designs, but the test also revealed minor weaknesses that could be rectified. These always concerned the colouring, which is relatively easy to rectify.

The container wagon Sgns 691 (82640) not only impressed with its good detailing, which can only be achieved with plastic injection moulding, and whose shortcoming of a low dead weight Märklin was able to compensate for by moulding it with metal-filled plastic.



The “Bunny Box” BDNf 738 model (from the 87189 set) had only very few weak spots, including the black frame that did not extend all the way to the driver's cab. Märklin corrected exactly this point with the subsequent edition, so that nothing stood in the way of our award.

This model is also immensely important for depicting freight traffic today. Two-axle carrying wagons no longer have any operational significance in the KLV sector today, and there was also no four-axle alternative of modern design in the Märklin programme. And a desired model addressed to FR Freudenreich Feinwerktechnik in a two-part design with a central Jakobs bogie has not yet seen the light of day in the model railway world.

Container transport, which, in addition to block trains for bulk goods such as coal, ore, and oil derivatives, are now major features of everyday life on rails, could, therefore, hardly be credibly depicted on a scale of 1:220 before this new product appeared.

A similarly painful gap existed in the category of push-pull trains, which, with the exception of a “Thunderbox” passenger coach that could once be modelled with the help of a Westmodel kit, could only be reproduced in Z gauge from the early seventies onwards.



This was also noticeable because there was no shortage of suitable locomotives for the period up to 1970: V 10020, V 160, V 2000 and E 41 are the most prominent examples, here. Only the model of the “Bunny Box” BDnf 738 (87189), which was also released in 2023, brought full operational benefits for these locomotives, which is particularly important under the guiding principle of “thinking in trains.”

It fits perfectly into the picture of the existing “Silberlinge” coaches, as it is based on the chassis of the existing control car with a “Karlsruhe head”. However, this model has finally been converted to LED lighting, which also eliminates a weak point.

As a result of its technical platform, the model showed only one downside: The frame replica does not extend to the front buffer beam and Märklin refrained from painting in black its continuation on the upper part of the housing.

However, this has already been made up for with the Era III model also delivered in the reporting period as BD4nf-59 (87074), while the error on Sgns 691 has so far remained uncorrected. After extensive discussions, the decision was even

unanimous in favour of the Hasenkasten: In the end, everyone agreed that one of the customers' most desired models had been realised here.

Accessories category

Here, too, exactly two new products competed against each other. The fact that there were not more was because we were unable to get in time our hands on two very interesting Artitec releases, which we were only able to present in more detail in the January 2024 issue.

This reduced the field of contenders to two building kits, one of which is representative of an entire series. This refers to the semi-relief townhouses from Modellbahn-Union. Editor Ralf Junius built the “01 Sandstone” model (MU-Z-H00201) as a test, and showed what can be done with this model.

He praised the kit's favourable price, its ease to build which makes it accessible even to beginners, and its versatility of use on dioramas and layouts. Although they are not the first relief buildings on the market, they are currently the most suitable for urban themes with larger buildings. His assessment was confirmed by feedback from other Z scale enthusiasts who had something like this on their wish lists.

However, the race in this category was won, albeit not unanimously, by the small workshop kit (49590) from Viessmann's Vollmer brand. It was not an easy decision to make, but, perhaps, the deciding factor was the use of "Polyplate", a composite material, that gave the kit a touch of innovation.



The small workshop kit from Vollmer (49590), not only made an impression with its Polyplate composite material. The possibilities to easily equip it with interior lighting and an interior details, also appealed to us. What's more, this small building can really be used anywhere!

Viessmann not only uses wood and hard cardboard as familiar materials for laser-cutting technology, but also combines its new products with synthetic resins, i.e., liquid materials from the plastics industry. This provides even more stability, and, together with the factory colour treatment, makes for an exceptionally realistic look.

Our award to this product comes also with a clear wish: This new product line should be continued and has the potential to bring the Vollmer and Kibri brands back into the top ranks of building kit suppliers.

A note from editor Dirk Kuhlmann, which summarises some of the comments from the preliminary editorial discussion, is also decisive for this assessment: "We have been missing the smaller buildings, in particular, over the years, which makes this a meaningful move by Vollmer."

Technology category

This is a versatile category that gives equal weight to tools, functional models, technical, products and innovative manufacturing methods. We had therefore kept an eye on the multi-coloured 3D-printed figures announced by Noch in order to assess and nominate them under this category if they deliver what we believe they promise.

Unfortunately, there were unforeseen start-up problems in the production process, which made any scheduling moot. Dr. Rainer Noch talked about this in our recent 2024 Nuremberg trade fair report on **Trainini TV** (episode 25), which is why we would like to recommend the interview to our readers.



Märklin's Mobile Station WLAN gives the operator freedom of movement and allows him to experience the layout better. At the same time, it is just one building block in an expandable system that is ideal for beginners, and can also be compatible with Z gauge.

The Intellibox 2neo from Uhlenbrock, which was initially discussed for inclusion, would also be a candidate with a good chance. However, this product was launched at the end of 2022 and was also considered by many trade magazines in that year. We were only able to test it in the first quarter of 2023, but unfortunately, it had already missed our selection criteria.

To our complete surprise, the Mobile Station WLAN from Märklin (60667) remained the only candidate in the race. If it did not fail due to votes against it, the crown could no longer be taken from it. To cut a long story short, the decision was very clear with one abstention (due to a lack of expertise).

What is much more important now is what features of the hand-held device, also known as "MSW" in model railway parlance, were convincing? Of course, the possibilities mentioned in the test report, which this device offers, without having to get used to the wired device, were mentioned here.

At the same time, solid technology was confirmed, to which Märklin traditionally attaches great importance. Finally, the feel and basic structure were mentioned after other editorial colleagues had this device in their own hands. And the Mobile Station WLAN is equally suitable for digital entry as well as for expanding existing digital configurations, making it an worthwhile investment.

Category Literature

The market for railway literature was also one that was plagued by the pandemic. Many publications were put on hold by publishers or did not reach the shops on time after being announced because of interrupted or unconfirmed print paper supplies.

Even though the situation has recently improved again, we felt this very clearly in view of the 24 annual reviews of selected books, brochures, films, and occasionally also programmes or games with a railway connection.

We only nominated two reviewed titles for this category that clearly stood out from the rest. The first is the brochure “Bahndienst- und Dienstgüterwagen: Band 1” (“Railway company service and cargo cars: Volume 1”) from the publisher Eisenbahn-Dokumentation by Stefan Carstens (ISBN 978-3-9823-2222-3) and distributed by Modellbahn-Union.



Its author does not need a special introduction, as he is regarded as the best freight car expert in Germany. We always fall back on his knowledge and his publications when we need to analyse prototype specifications. His books on open wagons are now many years old and are outdated in the model section, but also with respect to some of the prototype data.

The brochure, together with volume 2, which has also been published, in the meantime, makes for a much welcome update and also corrects some of the earlier information that has been refuted. This also underlines the expertise and sincerity with which the railway history is researched here. Many Z scale enthusiasts also benefit from this.

However, because the model section does not take our scale into account at all and the focus is primarily on designs that were not previously available in Z gauge, the vote was unanimously in favour of the other title.

That was “Die Mehrsystem-Elloks der Deutschen Bundesbahn” (978-3-8446-6039-5; “Multi-system electric locomotives of the German Federal Railways”) from EK-Verlag. There is hardly anything to add about this book in the “blue series” either, because time and again, as here, too, they prove to be the gold standard on the subject in question.



Although Dirk Kuhlmann was particularly fond of the engines featured in the film, because he had seen them himself in the Opladen railway repair shop and the 181² series was also waiting to be scrapped there, in the end it was the usual qualities of this book series and this particular volume that made for a clear winner.

Joachim Ritter sums it up with the following sentence: "Great model research, good photo selection, and topics that are fun to read and provide knowledge and background information that you don't want to miss."

Layout category

This is one of the two honorary awards that our editorial team would like to award each year. It is also our youngest category, which we are including for the fourth time only. It is characterised by the fact that this is not an open competition, i.e., we do not allow several showpieces to compete against each other in public.

Instead, we go to trade fairs and exhibitions with our eyes wide open, observe the maturing of suitable objects and take note of those pieces that we think are worth considering. Once their builder considers them finished, it's our turn to discuss whether we want to award the prize or not.



The Hirschsprung (stag leap) layout has taken our fancy this year: And on Jürgen Walther's layout of the same name, the famous rock with its stag monument has been recreated for the first time alongside the railway station.

That is why we also do not mention any of the alternatives we looked at. As our readers, who are deliberately drawn into the spotlight here, are not commercially active, they do not experience a corresponding demand for their “product”.



The seemingly far too large station in no man's land is the centre of operations of the layout, which today we are honouring as Layout of the Year 2023.



Under no circumstances do we want a second winner to suddenly feel like the first loser. Every year, many great pieces are presented, into which a lot of time, money and passion have been poured. Since not every exhibition has the same reach, one or the other may escape us. And everyone should know and feel that we appreciate and want to recognise their showpieces as Z scale ambassadors.

Our award, which this year goes to the “Hirschsprung” layout by Jürgen Walther, should be understood as recognising high standards of layout engineering, creative flair and effective presentation.

In fact, we consider this to be the first Hirschsprung layout with an almost uncompromising reference to the prototype setting of this station in Germany's Black Forest. Long-time Z gauge fans know that it is not the first layout tackling this theme and that we have already presented one in this magazine.

For editor Joachim Ritter, it does an excellent job in recreating the actual local atmosphere. That's why he puts it like this: “I find this to be a great realisation of a hometown theme. I like how the model is so consistent with the real

thing.” And, Holger Späing also comments on the unanimous vote that even the choice of the rolling stock corresponds closely to the prototype.

Complete Works Category

This is not an open competition either. This category, which we set up back in 2010, was and is intended to recognise long-standing and special achievements that are outstanding, but could not fall under one of the other categories because they cannot be linked to a single product.

These are often merits that elsewhere would be labelled as a lifetime achievement. We deliberately call it a body of work because we do not see our award as an end point and, if possible, do not want to award it posthumously.

This is why our nominee was decided early on in the year and then also received a unanimous vote. Harald Thom-Freudenreich had already announced more than a year in advance that he intended to retire at the end of 2023.



FR Freudenreich Feinwerktechnik enhanced the Z scale product range for decades with a wide variety of (not only) wagon models, such as the then brand-new Klagenfurt type open wagon.

Our readers were very concerned with how his brand FR Freudenreich Feinwerktechnik and small volume production would continue. This was only announced shortly before the end of last year, and even for today's award winner, it does not mean a complete farewell. Although at a slower pace than before, he is continuing with new developments, which are certainly characterised less by a business interest than by personal preferences.

Nevertheless, this is a unique opportunity to look back on around thirty years of small volume development and production. The product range changed considerably over time, as editor-in-chief Holger Späing noted. Initially, the focus was on North America, before switching to Europe.



FR Freudenreich Feinwerktechnik has built up its own market in Scandinavia, but this supplier has also become an important and determining factor for Germany, in particular.

Many open wagons and the covered Oppeln design are likely to be particularly memorable for enthusiasts. There are also countless accessories.

“You only realise what you have when it's missing”, said Harald Fried, summarising the impressions exchanged. The products always had their price, but there was a consensus that this was always backed up by good quality.

His products covered many themes, eras, and international railway companies, but they always convinced us without exception.

That's why 2023 was exactly the right time to honour the milestone of the brand transition and the move to Atelier Barkvieren.

In conclusion, we would like to congratulate all the winners and motivate them, as well as everyone else, to draw attention to themselves through outstanding achievements, and to continue to promote our scale.

Pages of our winners (in order of category):

- <https://www.rokuhan.com>
- <https://www.maerklin.de>
- <https://viessmann-modell.com>
- <https://www.eisenbahn-kurier.de>
- <https://fr-model.eshop.t-online.de>

Trainini TV – Episode 25:

- <https://youtu.be/WweUilwW7To?si=qh2L3uKy7QY5RwVN>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Geschichte der preußischen G 10 (Teil 2) **Preußische Einsatzgeschichte**

Auch nach dem Ende des Zweiten Weltkriegs gab es noch durchaus ansehnliche Bestände der Baureihe 57¹⁰⁻³⁵. Und wegen ihres geringen Achsdrucks schienen auch noch längst nicht abkömmlich, obwohl sie schon bald ein achtenswertes Dienstalder erreicht hatten. einige Exemplare erlebten sogar noch den neuen Nummernplan. Band 2 des EK-Baureihenportraits spürt ihnen nach.

Hans-Jürgen Wenzel
Die Baureihe 57¹⁰⁻³⁵
Band 2: Einsätze nach 1945 – DB, DR und Ausland Teil 2

EK-Verlag GmbH
Freiburg 2023

Gebundenes Buch
Format 21,0 x 29,7 cm
224 Seiten mit 398 überwiegend S/W-Abbildungen

ISBN 978-3-8446-6048-7
Best.-Nr. 6048
Preis 45,00 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

Da der in der September-Ausgabe 2023 vorgestellte Band 1 und diese Fortsetzung in zeitlich recht kurzem Abstand in die Regale des Buchhandels kamen, möchten wir kurz zusammenfassen, was wir bei der ersten Besprechung festgestellt haben und auch für diesen Band gilt - zumal wir diese Fortsetzung ja schon gleich im Fokus hatten.

Auch dieses Lokportrait der preußischen G 10 macht keine Ausnahme mit Blick auf die Baureihenbibliothek des EK. Nahtlos schließt es an den ersten Band an und die Teilung zwischen beiden Werken scheint bestens gewählt.

Mit Hans-Jürgen Wenzel ist der Autor identisch geblieben, was wir einen wichtigen Faktor halten, um ein auf zwei Bücher aufgeteiltes Portrait wie aus einem Guss wirken zu lassen. Ein eigenes Vorwort hat die vorliegende Lektüre übrigens nicht erhalten.

Der vorliegende 2. Teil setzt 1945 mit dem Kriegsende an und behandelt vor allem die Einsatzgeschichte bei der DB und DR im Osten Deutschlands. Auch die Auslandseinsätze nach Kriegsende gehören wegen der chronologischen Teilung hierher.

Werfen wir noch mal kurz einen Blick auf die behandelte Baureihe: Mit einer mittleren Achslast von nur 13,5 Tonnen wurde sie Anfang des 20. Jahrhunderts für den Güterverkehr auf Hauptbahnen mit leichtem Oberbau sowie auf Nebenstrecken entwickelt.



Mehr als 2.500 Exemplare wurden dann von neun Herstellern zwischen 1910 und 1924 gebaut. Wegen ihrer geringen Achslast, gleichzeitig Leistungsstärke und robusten Konstruktionen erlangte sie in beiden Weltkriegen Bedeutung für Militärtransporte.

Gerade das brachte die Maschinen in viele Länder Europas und ist ein Grund dafür, dass die Bestände – von Verkäufen abgesehen – weit verstreut waren. Dieses Buch zeichnet ihre Geschichte außerhalb der deutschen Grenzen für den Zeitraum nach 1945 nach.

Den Hauptteil bildet aber die Betriebsgeschichte bei DB und DR. Dazu wird zunächst die Nachkriegssituation beschrieben und hergeleitet, wie die Eisenbahn von den Besatzern geführt wurde und schließlich zu zwei Bahnverwaltungen in Ost und West werden sollte.

Abgehandelt wird im Anschluss zunächst directionsweise die Betriebsgeschichte bei der DB, danach die der Deutschen Reichsbahn in der SBZ und der späteren DR. Einen Technikteil gibt es nicht, denn alle relevanten Informationen dazu enthielt bereits der erste Band.

Der bereits erwähnte Auslandsteil ist ans Ende des Buches gestellt und ihm folgt nur noch ein durchaus spannender Teil zu erhaltenen Lokomotiven. Hier ist besonders wieder das Ausland interessant, weil nur eine einzige DB-Maschine erhalten blieb, die immer noch in Siegen hinterstellt ist.

Wie schon beim Vorgängerband sind viele Informationen ausgewertet worden, gut dokumentiert und anschaulich vermittelt worden. Dies korrespondiert wieder mit einer treffenden Fotoauswahl, die sowohl die unterschiedlichen Lokausrüstungen als auch Einsätze im Güter- und Personenverkehr belegt. Auch die Reproduktion der historischen Aufnahmen ist tadellos.

Es ist und bleibt faszinierend, welchen Umfang dieses Baureihenportrait angenommen hat: 1979 war es als einbändiger Titel erschienen und sollte mit Blick auf die verstrichene Zeit überarbeitet werden. Unerwartet viele neue Erkenntnisse ließen seinen Umfang auf mehr als das Doppelte anwachsen!

So umfasst auch dieser zweite Band ganze 224 Seiten mit fast 400 Bildern, darunter auch Laufpläne und Fahrplanbuchauszüge. Über 600 Maschinen waren nach 1945 auf dem Gebiet der späteren Bundesbahn verblieben, 134 weitere bei der Deutschen Reichsbahn.

Fast 100 weitere Maschinen zählte die ÖBB, die sie bis 1968 im Dienst behielt. Während die DB ihre letzte G 10 im Jahr 1970 ausmusterte, blieben ihnen bei der DR immerhin zwei Jahre mehr. Die Einsätze im Ausland, darunter die Türkei, Rumänien und Polen, wehrten oft deutlich länger. Zusammen zeigen sie auf, welche Spannung und Themenfülle diese formschöne Dampflokomotive bietet.

Leider fehlt sie bis heute in der Spurweite Z und wäre aus unserer Sicht ein perfektes Großserienmodell. So verstehen wir die zwei Bücher zu ihrer Geschichte auch nicht nur als wertvolle Literatur für eingefleischte Vorbildfreunde mit Technikinteresse, sondern auch für alle Modellbahner, die für den Tag gewappnet sein wollen, wenn das Warten hoffentlich ein Ende hat.

Publishing pages:
<https://www.eisenbahn-kurier.de>
<https://www.ekshop.de>

Erste Drehstromlok Baureihe 120 **Die Zeitenwende bei der DB**

Die Baureihe 120 war nicht irgendeine Ellok: Sie war eine Pionierin und öffnete die Türen für die moderne Eisenbahntechnik. Mit ihr entstanden 1979 die weltweit ersten Drehstromlokomotiven hoher Leistung. Doch als sie in Serie gebaut wurden, waren sie beinahe schon überholt. Der Lok-Vogel dokumentiert ihre Geschichte mit ganz eigenen Schwerpunkten.

Hans Sölch | Michael Fuhry
Der Lok-Vogel Sonderausgabe Nr. 18
Die Baureihe 120 – Die erste Drehstromlokomotive der Bundesbahn

Xyania Internet-Verlag Hans Sölch
Rosenheim 2023

Broschüre mit Klebebindung
Format 21,0 x 29,7 cm
100 Seiten mit über 180 farbigen Abbildungen

ISBN 978-3-961-65120-7
ISSN 1862-9415
Preis 14,90 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder bei ausgewählten Buchhändlern

„Der Lok-Vogel“ ist keinesfalls der Titel einer neuen Zeitschrift im Bereich der Eisenbahnfachpresse. Verlagsleiter und Hauptautor Hans Sölch ist vielmehr ein alter Bekannter der Szene, nur stellen wir heute erstmals ein Werk aus seinem Verlag vor.

Neu ist allerdings, dass der Vertrieb seiner gedruckten Publikationen nicht mehr über den Bahnhofsbuchhandel erfolgt. Interessenten bestellen daher bitte direkt, bei einem ausgewählten Buchhändler.

An dieser Stelle stellt sich vielleicht die Frage, wie wir nun auf den heute zu sprechenden Titel gestoßen sind und was uns zu einer Rezension bewegt hat? Das ist schnell und einfach erklärt: Dem erfahrenen Publizisten Hans Sölch stand als Mitautor Michael Fuhry zur Seite.

Und dieser ist nicht nur ein fundierter Kenner und Liebhaber der Baureihe 120, sondern zugleich auch ein leidenschaftlicher Zettie. Bei einem fachlichen Austausch über die erste Drehstromlokomotive der Bundesbahn und ihre Modellumsetzungen sowie eigene Modifikationen stießen wir auf diese Sonderausgabe – und schon war das Interesse geweckt. Was erwartet uns nun also?

Werfen wir dazu erst mal einen Blick aufs Vorbild: Die Baureihe 120 wurde 1979 mit viel Vorschusslorbeeren als künftige Universallokomotive der Öffentlichkeit präsentiert. Es schloss sich ein langjähriges Testprogramm an, das neben reinen Versuchsfahrten auch den Betriebsalltag umfasste.

Der Zeitraum stellte sich als deutlich länger dar, wie wir es von anderen Prototypen gewohnt sind. Dies liefert schon ein deutliches Indiz dafür, wie neu und unerprobt die Technik war, die hier zum Einsatz kam. Die fünf Vorserienmuster stellten tatsächlich eine Revolution im Bau von Eisenbahnfahrzeugen dar.

Mit Testfahrten und Vorführungen sowie verschiedensten Einsätzen im Güter- und Reisezugverkehr sollte die Alltagstauglichkeit bewiesen werden. Das Sammeln von Erfahrungen mit der neuen Technik und das Finden von Schwachstellen und Kinderkrankheiten war sehr wichtig.



Erst ab 1987 gelangten 60 Serienlokomotiven in den Dienst, was den Gesamtbestand der Baureihe auf 65 Exemplare an hob. Dabei sollte es auch bleiben, denn ein Weiterbau unterblieb ebenso wie das Beauftragen eines weiterentwickelten Nachfolgers in Form einer Baureihe 121.

Dafür gab es mehrere Gründe: Zum einen war die Technik der Baureihe 120 wegen großer Fortschritte in der Leistungselektronik schnell wieder überholt und die sich ankündigende Bahnreform ließ den Bedarf an einer Universallokomotive schwinden.

Trotzdem erfuhren die Lokomotiven eine wechselvolle Geschichte: Die fünf Vorserienmaschinen unternahmten die Testfahrten für die Serie, danach für die nächste Lokgeneration und erlebten schließlich ihr Ende im Messzugdienst.

Die Serie lief als erste Baureihe über die Neubaustrecken der DB und blieb für mehr als zwei Jahrzehnte ein wichtiges Standbein im lokbespannten Fernverkehr. Doch dann kam 2020 ein schnelles Ende, das nur für wenige Exemplare einen Wechsel zu privaten Einstellern mit sich brachte.

Genau das liefert ausreichend Stoff für eine spannende Lektüre, denn bislang haben die Veröffentlichungen fast komplett einen Schwerpunkt auf die verbaute Technik gesetzt. Hier stehen die Einsatzgeschichte, optische Unterschiede und Veränderungen im Fokus. Dabei wird der gesamte Zeitraum des Betriebs dieser Baureihe bei der Bundesbahn wie auch der Deutschen Bahn AG betrachtet.

Auch die Gegenwart bei Käufern der Lokomotiven ist noch einbezogen worden. Gäbe es diese Form der Nachnutzung nicht, läge ein abgeschlossenes Kapitel Geschichte vor uns, das hier vollumfänglich behandelt wurde.

In den einzelnen Kapiteln erfahren die Leser alles Wissenswerte zum Aussehen der Vorserienmaschinen sowie der orientrotten und verkehrsroten Serienexemplare. Im Zuge der Recherche konnten auch - dies dürfte maßgeblich auf den Mitautor Michael Fuhry zurückgehen - ein paar zuvor unbekannte Fakten entdeckt und integriert werden.

Beim vorliegenden Heft handelt es sich also gewiss nicht um bekannte und nur wiederaufgewärmte Tatsachen in neuer Struktur. Wir waren beim Lesen verblüfft, wie viel Stoff und Kurioses die Geschichte dieser besonderen Baureihe noch beinhaltet.

Die Sonderausgabe dieses Lok-Vogels stellt also eher einen Lockvogel dar, der schnell auch die Lust auf weitere Titel weckt. Mit der Broschüre ist also auch derjenige gut bedient, der bereits ein Baureihenbuch hierzu besitzt, weil es sich als perfekte Ergänzung offenbart.

Ihren Anteil haben aber längst nicht nur die detaillierten Texte, sondern auch zahlreiche, gut reproduzierte Bilder und Statistiken. Zur Bildauswahl bleibt dann auch festzuhalten, dass sie gut und mit Bedacht getroffen wurde, um Besonderheiten auch belegen zu können, jede Aussage geeignet zu dokumentieren und auch den Lesespaß durch ein ansprechendes Aussehen zu unterstützen.

Lediglich das große Titelfoto wäre aus unserer Sicht besser gegen ein farbenfroheres Landschaftsfoto mit kontrastierendem Grün ausgetauscht worden. Die dadurch erzeugte Spannung hätte auch für alle, die diese Rezension nicht lesen, eine stärkere und sehr werbende Wirkung.

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Spring innovations for Z gauge

A bountiful Table

The International Toy Fair reported the 50 millionth visitor to the Exhibition Centre Nuremberg. And, as you would expect, it also announced further success stories. However, the model railway sector is further thinned out, and a relic of times gone by, although it has at least recovered slightly compared to the previous year. However, most of the new products were presented apart from the trade fair.

The share of model railway manufacturers in Hall 7A at the Nuremberg International Toy Fair is manageable, and a tour of the exhibition is completed quickly. We have already published our impressions and important information for their customers in a film on **Trainini TV** (episode 25) a fortnight ago.



Goodbye? The question remains as to what significance the Nürnberger Spielwarenmesse (Nuremberg International Toy Fair) will have for the model railway sector at the beginning of 2025.

+++ Acrylicos Vallejo +++

This manufacturer was present with a booth in Nürnberg (Nuremberg) and is expanding its colour palette introduced last year as well as the new brush range. Beyond that, however, we were unable to find any new products that might be particularly worth mentioning for Z gauge modellers.

www.acrylicosvallejo.com

+++ Airgoo +++

Founded in 2015, this is still a relatively young supplier that we were able to visit for the first time. Based in the Netherlands, the company defines itself as a manufacturer, wholesaler, and specialist in airbrush tools.

The range includes well-known products that are bought in and sold by the company itself. However, it is certainly worth looking at the website. Direct sales are also offered there. We do not know to what extent a trade licence is required to do this. However, even in such a case, it should hopefully be possible to find authorised dealers through whom an order can be placed.

www.airgoo-europe.com

+++ Álvaro Cortes +++

The Portuguese car specialist also has some exciting new products in store for spring 2024. The focus on European vehicle models is clearly recognisable, and not just luxury sports cars.

The current Maserati MC20 from Modena, which has been built since 2020, is the only exception here. We could continue here with Lamborghini or Porsche, as these were once also brands for agricultural tractors. But the Case Puma 210, as a modern vehicle, has a different manufacturer.



The Case Puma 210 is a modern tractor for agricultural applications. Photo: Álvaro Cortes

In our focus on new models, which we are the first to show here, the Volkswagen Transporter T3, which rolled off the production line in various factories between 1979 and 2002, is still missing. It is available with round and square headlights and replicates the Caravelle and Transporter variants with a closed load compartment.



The three planned variants of the Volkswagen Transporter T3 have not yet been presented on their website. You can now see this great new product here, first. Photo: Álvaro Cortes



These former everyday vehicles have long been featured on the company's own website, but should be easy for anyone to recognise, even without further descriptions: they are precisely the cars that are far too rarely in the focus for models. Photo: Álvaro Cortes

The other novelties that Álvaro Cortes has already shared on his pages are the Alfa Romeo GTV6 from 1986, a Lada Niva, the Citroën 2 CV as a box-type lorry and saloon, the TVR T440R, and, with the Renault R4 and Fiat Panda, two European everyday cars from years gone by.

alvaro_mcortes@hotmail.com

<https://www.facebook.com/My-Z-scale-models-1648628058694887>

+++ Archistories +++

Archistories did not present any new products of its own at the beginning of the year, but is still active as a manufacturer of exclusive kits. Accordingly, new products can be found among the clients

www.archistories.com

www.archistories-shop.de

+++ Artitec +++

The accessories specialist from Amsterdam is always good for surprises. At the end of January, it unleashed a veritable surprise package of new products, with which it now also appears as a figure manufacturer in 1:220 scale.





Field workers (item no. 522.003; photo above), who work kneeling before the mechanisation of the agriculture for planting seedlings or harvesting, have never been offered across manufacturers. Let's pause for a moment with the steel workers during their break (522.004; photo below). And the tractor drivers (522.006; photo on page 33 below) will also enhance some of the farm tractors already on display at the customer's premises. All photos: Artitec

The following sets with three to six figures and matching accessories have been announced: bricklayers 1930 - 1990 (item no. 522.0001), carpenters 1920 - 1990 (522.0002), field workers (522.0003), steel workers (522.0004), industrial shunters (522.0005), and tractor drivers (522.0006), which will certainly not only fit behind the wheel of their own models. As with the following new items, they are all ready-painted models.



Together with the river tugboat, the Rhine motor vessel "Helena" (322.042) is now returning to the programme as a finished model. Photo: Artitec

he Rhine motor vessel "Helena" (322.042) and the river tugboat (322.043) are being reissued, which should certainly increase the desire for water motifs, a sufficiently dimensioned section of the layout will

certainly become an attention-getter. The Atlas 1302 excavator (322.044) has been completely redesigned and is suitable for the construction sites of modern layouts, having previously served Era III with two models.



The two new kits “Pommesbude” (722.022; photo left) and “Trinkhalle” (722.023; photo right) will also help to vividly depict earlier everyday scenes on the model railway. Photos: Artitec

Also, completely new are two kits for the catering sector, which were already known from a larger scale and have attracted interest: a chip shop (722.022) and a drinking hall (722.023), both typical sales buildings for the Ruhr area, which was once still characterised by coal and steel.

www.artitec.nl

+++ Artmaster +++

In view of last year's trade fair discussion (see **Trainini**® 2/2023), we have followed up on the latest status. Although no new Z gauge products were presented in Göppingen in September 2023, the commitment to our scale remains unchanged. However, the higher-selling gauges continue to take priority.

www.das-kantoor.de

+++ Atelier Barkvieren +++

Harald Thom-Freudenreich now operates under the name Atelier Barkvieren, after Freudenreich Feinwerktechnik moved to a former employee who took over the FR trademark as Montage-service Uwe Schuster.

While vehicles from the previous assortment and new developments by Harald Thom-Freudenreich are only made into models for resellers, regulars' tables and clubs, the former boss is still developing new products on his own account in retirement.



Partly retouched, this depiction of an AB new product was created: the NSB sliding wall wagon His 210. Photo: Atelier Barkvieren

In addition to some new and re-issued cars, the production of the Norwegian EL-16 is planned, the body of which is to be reproduced using the vacuum casting resin process. The undercarriage is largely based on the Rc locomotive. Delivery is planned for the second half of the year.

The NSB's His 210 sliding wall wagon is expected to be available in April, although a prototype of this has not yet been completed. The following three new items, on the other hand, are already available for delivery.

One is the TGOJ material wagon (item no. 46.801.11) based on a Märklin model, which the private railway company used in blue livery for transporting material on its ore railway.



These three new products are already available: TGOJ material wagon (item no. 46.801.11; top left photo), NSB wood chip wagon Fb (47.804.21; top right photo) and TAGAB passenger coach AB3 (46.223.01; bottom photo). Photos: Atelier Barkvieren

The AB3 mixed-class passenger coach from TAGAB (46.223.01) has a retro paint scheme. Its eventful history is outlined in the product description on the supplier's website. There you will also find information on which models already offered can be supplemented by this wagon.

The last new item available is the NSB wood chip wagon Fb (47.804.21). This is a wagon type already known from the range (-11), but it has been given a new fleet number and is now aged and has been given repair patches.

www.fr-modell.de



+++ Atelier Dietrich +++

Andreas Dietrich is currently very busy with orders and is, therefore, not presenting any new products at the start of the year for the time being, as he told us on request.

www.atelier-dietrich.at

+++ Atlantic Case-Diorama +++

We had discovered this supplier at last year's trade fair, and, therefore, also paid a brief visit to 2024. We noticed a decorative acrylic glass display case with transparent shelves and side panels and a black back panel.

www.atlantic-case.com

Photo left:

We discovered this open display case at Atlantic Case-Diorama

+++ Avantgarde Modellbau +++

For his "Geislinger Steige" project, Philipp Meyer is now also turning his attention to Märklin rolling stock. The focus is on the electric locomotives of the 110 and 194 series as well as some freight wagons, which are to be sold under the AVA Modellbahnen brand.

The following products will initially appear here as fine scale models with various refinements:

- German crocodile 194 012-1 of the DB (item no. 101), Augsburg depot
- German crocodile 194 086-5 of the DB (102), Freilassing depot

The models show prototypical, individual features such as colour patches, special lettering, white decorative line (Freilassing depot), adapted front ends and lanterns as well as the sunshade roofs missing on the Märklin originals, which were retrofitted to all machines on the German Federal Railways.

The series of freight car models is considerably longer, all of which can also be assigned to Era IV (deviations are explicitly mentioned):

- Covered goods wagon Gbs 253 of the Deutschen Bundesbahn (401)
- Wagon pack with three Deutschen Bundesbahn covered goods wagons (402), consisting of two Gbs 253 and one Gbs 245
- Deutschen Bundesbahn self-unloading wagon Fals 167 (403) as a white dot wagon with iron ore load
- Wagon pack with two Fals 167 of the Deutschen Bundesbahn (404) as white dot wagon with iron ore load
- Deutschen Bundesbahn Fals 175 self-unloading wagon (405) with different details such as extended loading opening with straight end walls, wider wagon platforms and heavy-duty buffers with iron ore load
- Wagon pack with three Deutschen Bundesbahn Fals 175 self-unloading wagons (406) with iron ore load
- Deutschen Bundesbahn self-unloading wagon Fals 177 (407), design with shortened side walls for transporting ore, loaded with bauxite ore
- Wagon pack with three self-unloading wagons Fals 177 of the Deutschen Bundesbahn (408) loaded with bauxite ore

- Self-unloading wagon with hinged lids Tads 961 of the Deutschen Bundesbahn (409)
- Wagon pack with three self-unloading wagons with hinged lids Tads 961 of the German Federal Railway (410)
- Deutsche Bundesbahn refrigerator car Tehs 50 (411) with yellow corners, Era III
- Deutsche Bundesbahn Eaos 106 high-sided wagon (412), loaded with scrap metal
- Wagon pack with two Deutsche Bundesbahn Eaos 106 high-sided wagons (413), loaded with new scrap
- Deutschen Bahn AG Eaos 106 high-sided wagon (414, loaded with car scrap cubes,
- Wagon pack with two Deutschen Bahn AG Eaos 106 high-sided wagons (415), loaded with new scrap, Era V
- Era V SBB Eaos high-sided wagon (416), loaded with new scrap, Era V
- Deutschen Bundesbahn high-sided wagon E 037 (418) with sleeve buffers, loaded with old scrap stampings
- Wagon pack with three Deutschen Bundesbahn E 037 high-sided wagons (419) with sleeve buffers, loaded with gravel



Samples of the Eaos 106 (left) and Gbs 253 (right) are shown here as examples of what customers can expect. Photo: Avantgarde Modellbau

All freight wagons show authentic signs of use, depending on the purpose and type of wagon. Parts are also exchanged or supplemented for this purpose, including label boxes, rope hooks, brake adjusters, heating leads, ladders, steps, and grab irons.



This oak has been covered with the new foliage mats. Photo: Avantgarde Modellbau

Photo bottom right: Summer lilac and Aaron's rod on the railway embankment. Photo: Avantgarde Modellbau

The new material differs from other products not only in its natural, muted colours but also in its significantly greater thickness of 15 - 20 mm.

Together with an irregular structure, this allows the material to be used very easily, and in a time-saving manner, avoids the need for subsequent lamination of areas with thin foliage.

These mats will also be available from specialised retailers in the future. The range is complemented by special foliage scissors for irregular cut edges.

The in-house vegetation range will be further improved with new production methods to make trees even finer and more detailed.



Deviating buffer designs are taken into account, as is the grease on the buffer discs or frame, and door replicas inside the open wagons.

Bogies are also correctly moved to the outside, which requires the retrofitting of a close coupling.

The catenary masts for a functionless catenary are still without an item number and are intended for eras III to VI. They are based on reworked Märklin masts, with a thin nylon thread to scale being used as the catenary wire.

According to the manufacturer, however, the most important new product for 2024 is in the field of scenery construction, with which Avantgarde Modellbau also entered the market.

Various foliage mats (types: birch, beech, hornbeam, oak, black poplar, winter lime) are planned for the self-construction of trees, bushes, and ground vegetation, also on a scale of 1:220.

The ragweed plants announced for 2022 are now available: Aaron's rod, fireweed, summer lilac, thistles, and various other small flowering herbs in different colours and shapes.

www.avantgarde-modellbau.de
[motewe\(at\)gmx.de](mailto:motewe(at)gmx.de)

+++ Azar Models +++

Azar Models is preparing the six-axle electric locomotive CC6500 of the SNCF as a model novelty. The very first painted and labelled sample, which still needs a few corrections, was available in time for the completion of this edition.



Azar Models announces the CC6500 of the SNCF as the next new novelty. Even the first sample suggests a successful model. Photo: Azar Models



The new single-arm pantographs made of plastic take many details into account. Photo: Azar Models

will follow a few days ago: <https://youtu.be/whevGsJiS94>.

azar-models.com

However, the photos already give a good impression of a successful model. It will be the first electric locomotive from Azar Models to receive new single-arm pantographs made of injection-moulded plastic.

The crowdfunding project for the TGV, whose minimum funding target was reached shortly before the editorial deadline, is entering the finish straight.

The following video, which refers to the campaign for this major project, shows what a unique model interested parties can expect here: <https://youtu.be/dSbPDPXv-38>. Another video, which also emphasises special product features,

+++ AZL (American Z Line) +++

AZL does not follow the cycle of the toy fair with its new product announcements and deliveries. Various versions of the EMD SD70ACE for the following railway companies have been announced for short-term release: UP, UP (historical heritage and advertising), BNSF, CN, CN historical heritage, and EMD demonstration locomotive.



However, there have been two new models in the past few weeks, and these are currently appearing in further variants. Three D&RGW road numbers (art. nos. 64101-1 to -3) now follow as the SD40T-2 tunnel locomotive. The standard EMD SD40-2 version is the Florida East Coast livery (64217-1 / -2), which has been customised in many details for this railway company.



Tunnel locomotive EMD SD40T-2 of the D&RGW (item no. 64101-1; photo above) and EMD SD40-2 of the Florida East Coast (64217-1; photo below). Photos: AZL / Ztrack

Also labelled for the FEC are two double packs of the Ortner bulk goods wagons (905355-1 / -2), which are still younger models and are once again rolling to the customer with load inserts. The series of heavy passenger coaches is supplemented by a semi-baggage wagon for the C&O (74045-1 / -2).



Ortner bulk goods wagon (905355-1; photo left) of the FEC and semi-baggage wagon for the C&O (74045-1; photo right). Photos: AZL / Ztrack

This month, the Trinity column carrier wagons will be joined by three 53-foot semi-trailers from England Intermodal (905234-1), which are also available separately (954010-1).

The Northern Pacific wide-view goods train caboose (921023-1 / -2), of which two different road numbers are offered, completes the series.

www.americanzline.com

+++ Bahls Modelleisenbahnen +++

The Blomberg-based Z gauge specialist is launching new metal base plates for several large series steam locomotives (e.g., all 2'C1' and all 1'E engines). They will be available with and without brake replicas. Also, new are static wing signals made of metal. A new steam locomotive will also be presented at the Z gauge weekend in Altenbeken.

www.lokomotiven-bahls.de

+++ Beli-Beco +++

This family-run company was once again present in Nürnberg (Nuremberg), but did not present any new products. In conversation, it was emphasised that all lanterns are handmade and therefore allow for individual design.



Z gauge is not explicitly listed in the catalogue, but most of the lanterns for the nominal size N could also be shortened for the 1:220 scale by using thinner masts and smaller canopies. This has been valid up to now, and will remain so in the future.

Anyone looking for a customised and affordable product is therefore welcome to select their template from the programme and place a feasibility enquiry or order with this company.

www.beli-beco.de

+++ Böttcher Modellbahntechnik +++

At Stefan Böttcher we found a snow powder in 250 g containers (item no. BM 7750 / BM 7751) and 500 g (BM 7752). Our own range also includes many gritting materials such as fine or coarse gravel, rock face stones in various colours, coal, iron ore, sandy beach, field and road grit. It is worth taking a look at the company's products, which can be found on its website.

www.boettcher-modellbahntechnik.de



+++ BuBi-Model +++

This supplier from the Czech Republic, which we are reporting on today for the first time, has a very large and varied Z gauge programme of buildings and freight. New to the range are some buildings that are available exclusively from MBS Modell + Spiel GmbH Sebnitz (<https://www.mein-mbs.de>) as inexpensive ready-made models.

The existing "Sebnitz" series has been expanded to include the semi-relief houses "Schillerstraße No. 3" (item no. Z60201) and "No. 19" (Z60202). The silo house (Z60210), measuring 25.8 x 3.4 x 7.9 cm, is a truly long building for the edge of the layout. With its use, a main agricultural theme should already be established.

The "Schillerstrasse No. 3" (item no. Z60201; photo above) and "No. 19" (Z60202; photo below) complement an existing row of semi-relief buildings. Photos: BuBi-Model



The long silo house (Z60210) can also form the end of the system against the background and create a backdrop for agricultural themes. Photo: BuBi-Model

Other new additions include a railway caretaker's house (Z60205), a house with a basement garage (Z60206), a house with an envelope (Z60207), another based on the Sebnitz model (Z90208), and a detached house with a garage (Z60209).

The sawmill with interior (Z60211) promises to be exciting, as the product descriptions do not yet reveal how extensive the interior will be.

www.bubi-model.com

+++ Busch +++

The accessory supplier Busch always knows how to react to current events. It proves this again with its spring 2024 innovations. Large stands of spruce in German forests have been severely damaged following a summer of drought. The bark beetle finally did the rest to the weakened trees.

Today, we find large areas of dead trees that can be recognised from afar by their lack of greenery. Whereas in the past it was individual trees that stood out among the many crowns, today it is a mass phenomenon. Busch is therefore expanding its programme to include dead spruce trees.

Photo right:
The dead spruces (item no. 6167) at different heights represent the current state of our forests. Photo: Busch





This diorama scene is intended to give an impression of the realistic use of the layout with the familiar green spruces. Photo: Busch

The trees, which are up to 50 m high in nature, are an excellent fit for our nominal size, although they are marketed for H0 and TT scales, in which they would be much too short. Two dead spruce trees with dried needles are available in the following heights: 130 mm (item no. 6166), 160 and 180 mm (6167) and 180 and 190 mm (6168).

www.busch-model.de

+++ Digimoba Elektronic +++

Digimoba Elektronic has finalised several new developments in 2023, which are now available and will be listed here.



The digital reverse loop module (item no. 2181) is also interesting for use in Z gauge.
Photo: Digimoba Elektronik

The digital reverse loop module (item no. 2181) promises simple installation without additional sensors, track or switching contacts. The polarity is recognised when extending or retracting and switched without interruption.

The servo switch (2191) controls standard servos as point, signal, or barrier drives, and is triggered by push-buttons.

The travel can be set using the separately available "Servo programmer" programming device (1170).

A segment connector set (3010) helps when wiring the model railway system to a control panel.

With its help, segments and modules can also be quickly and electrically disconnected from each other. A 24-pin connection is made using ribbon cables of different lengths (can be ordered separately) with reverse polarity protected plugs.

Last but not least, there is the switch button (2172), which allows convenient operation of up to two motorised point machines with just one button. Each press of the button causes switching in the other direction. At the same time, LEDs can be connected for synchronised position indication. Operation takes place with direct or alternating voltage between 8 and 18 V.

www.digimoba.de

+++ D&H Doehler & Haass Steuerungssysteme +++

In addition to Zimo (see Zimo), Doehler & Haass is also focussing on the E24 interface for new decoders. A vehicle decoder (item no. DH24A) and a sound decoder (SD24A) for the SX1, SX2, DCC, and MM formats have been announced.

Unfortunately, the manufacturer has not yet specified any dimensions, so that we cannot check the actual suitability for our scale. It should be mentioned, however, that the E24 interface was developed specifically for decoders that are installed in the small gauges and that ESU, from whom this advance originates, has already proven the suitability of a Z gauge model.

The DH24A features eight amplified and five unamplified function outputs, two of which are alternatives to the SUSI interface. Storage capacitors on the model are also directly supported, as the 15V storage circuit is already integrated on the decoder.

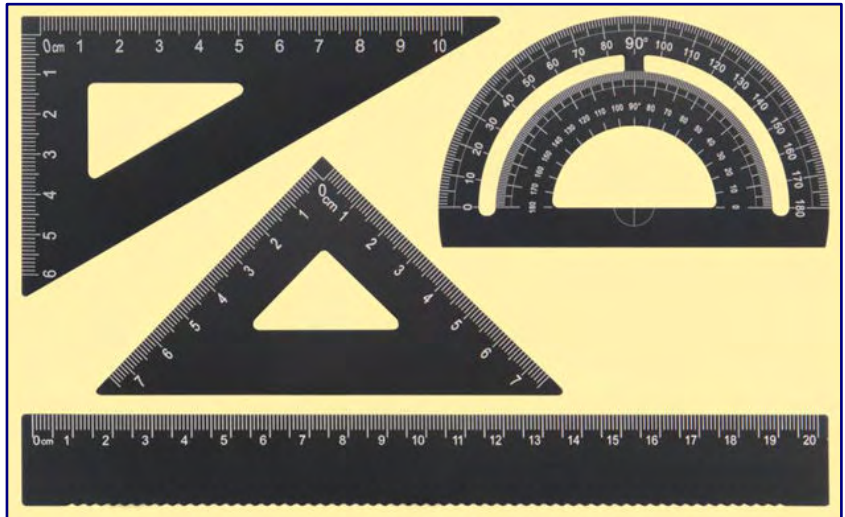
www.doehler-haass.de

+++ Donau Elektronik +++

This electronics and tool specialist also has some new products for successful model making. The four parts of the ruler, and an angle set (item no. ML4S4) are made of high-quality aluminium and come in a practical storage box.

The two round die holders “16.5 x 5” and “20 x 5” (MSEH3 / MSEH4) hold the die securely in the holder with four fixing screws. The holder can be used by hand or with the aid of an SW17 socket spanner. The height of the holder is 16 mm, the diameter 25 or 30 mm.

The titanium-coated, right-cutting blind hole taps are designed for use on blind holes. Depending on the drill, the cutting length is 5 to 7 mm and sizes M2.0 (MMSB20), M2.5 (MMSB25), M3.0 (MMSB30), and M4.0 (MMSB40) are available.



Ruler and square set (item no. ML4S4), shown here without the storage box. Photo: Donau Elektronik

New to the range are also new stranded wires in various designs and colours as 3 x 0.25 mm² twisted strands (325-014 to 325-818) in various reel lengths and rings with 2 x

0.50 mm² twin strands (250-01-25 to 250-38-25).

shopware.donau-elektronik.de

+++ Eichhorn-Modellbau +++

This manufacturer offers frog points for retrofitting Märklin turnouts and crossings. They have been on the market for several months, but we have not yet been able to present them because the supplier was unable to provide us with the necessary information due to time constraints. The programme includes:

- Switch frog for 13° crossing (Art.-Nr. 43020-X),
- Point frog for 13° left points (43020-L),
- Point frog for 13° right-hand points (43020-R),
- Point frog for curved points left (43134-L), and
- Point frog for curved points right (43134-R).

All frogs are made of etched nickel silver parts and have a soldered lead to their polarisation.

www.eichhorn-modellbau.de

+++ EtchIT-Modellbau +++

Edgar Seubert is not currently presenting any new 3D printing products for Z scale, but he did have another piece of news to share. For technical reasons, he had to slightly change the start address of etchIT-Modellbau's electronic sales organisation.

If customers would like to change their bookmark saved in the browser, please enter one of the following two addresses: <https://www.etchit.bplaced.net/data/etchithome/index.html> or simply [etchit.de](https://www.etchit.de).

www.etchit.de

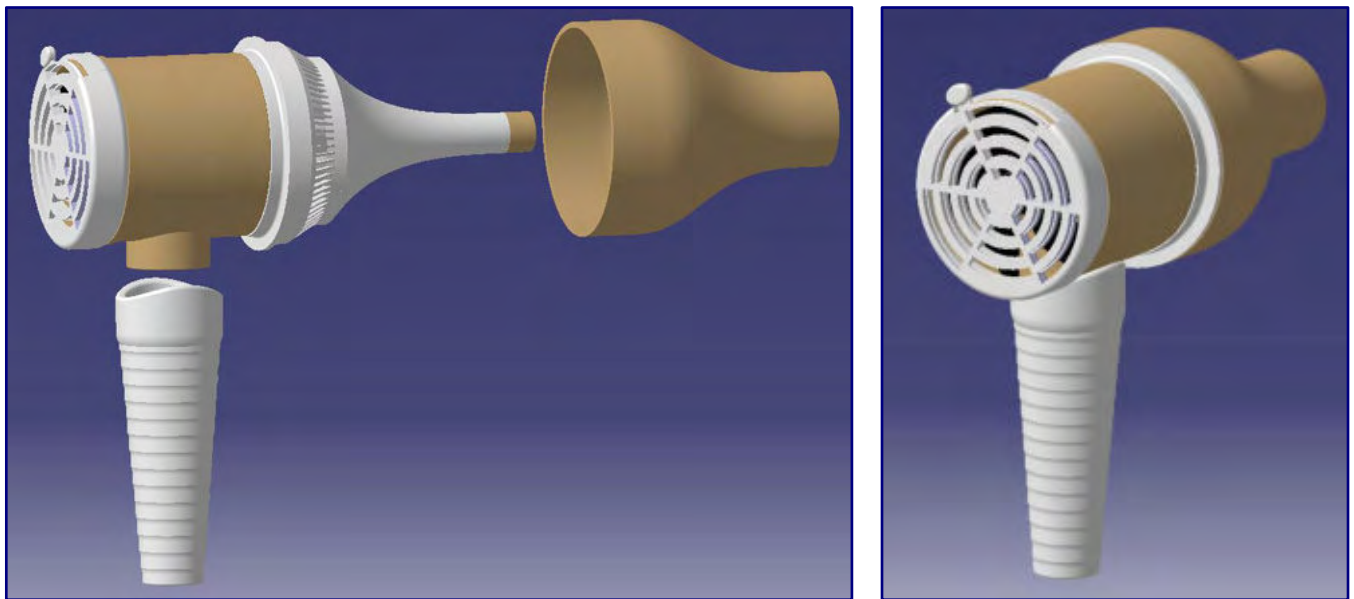
+++ Faller +++

Faller did not present any new products for Z gauge in Nuremberg. However, Managing Director Horst Neidhard pointed out to us that new products are announced on an ongoing basis throughout the year and hinted that we can expect something for Z gauge, as well.

www.faller.de

+++ Franke WF +++

We would like to mention the Dust Fairy, which is made from 3D printed parts and for which further information and conditions of purchase are available on request by e-mail. The Dust Fairy is a combined suction and blowing attachment for a domestic Hoover, which not only helps to clean models and landscapes.



The CAD illustrations visualise the structure and function of the adjustable dust fairy. Illustration: Franke WF

A slider can be used to regulate the increase and also in the opposite direction: In the closed position, only vacuuming takes place, otherwise vacuuming and blowing at the same time. Simultaneous whirling and vacuuming of the dust ensure that small and sensitive objects are neither sucked in nor damaged.

[franke.wf\(at\)web.de](mailto:franke.wf(at)web.de)

+++ GMmodelli +++

The website, which was still missing when the Z nominal size was launched, has now been set up, and further information was added shortly before the editorial deadline, including the catalogue with the complete range.

www.gmmodelli.com

+++ Herpa +++

The Dietenhofen-based manufacturer is celebrating its 75th anniversary with three new 1:200 scale moulds. The world's largest transport helicopter Mil Mi-26, which, unfortunately, serves almost exclusively military purposes for the warmonger Russia, was first announced and shown as a hand model in Nürnberg (Nuremberg).



The Douglas DC-7, a four-engined classic of the skies, is entering the Herpa Wings programme as a new 1:200 scale model. This model was already on display in Nürnberg (Nuremberg).

A classic of the skies is the four-engined Douglas DC-7, which, together with its range-enhanced successor versions, marked the end of the era of aircraft with piston engines. It was a direct competitor to the equally legendary "Super-Connie", which was powered by the same engines. However, the DC-7 had the edge for a long time.

The third and final new mould for the Wings series is the Beechcraft / Beech 1900D, a modern feeder aircraft for up to 19 passengers. It had two engines and was built from 1984 to 2002. The version selected by Herpa has a higher cabin roof and small winglets on the wingtips.



With the Beechcraft Beech 1900D, the jubilarian from Dietenhofen comes up with another new design. The small commercial aircraft can be categorised in the recent past. Photo: Herpa

For the new products now announced for March and April 2024, we are again focussing on those models that do not exceed a length of 30 cm and are suitable for use on or above European-style layouts:

Aeroflot Tupolev TU-144D "Technik-Museums Sinsheim" (Art.-Nr. 556323-001),
TWA – Trans World Airlines Lockheed L-1649A (558372-001),
Swiss International Air Lines Airbus A220-100 (558471-002),
Embraer E195-E2 "Profit Hunter – Tech Eagle" (572989),
Lufthansa Lockheed L-1649A Super Star (573030), and
ITA Airways Airbus A220-300 (573054).

These are joined by historical passenger stairs with a towing vehicle for the TWA (573122) and Interflug (573153) of the DDR as accessories. The range of models based on military prototypes is completed by the following two products based on German Air Force prototypes:

Airbus A400M Atlas – LTG 62 “Air Defender 2023” (572965), and
Panavia Tornado IDS – TaktLwG 51 “Air Defender 2023” (573085).



Unfortunately, the world's largest transport aircraft, the Antonov AN-225 “Mriya” from the Snapfit series, only had time for this photo behind glass.

No model matching our scale will be delivered in the Snapfit range in the near future. Here too, however, Herpa has already shown a new model in Nürnberg (Nuremberg) in a fully labelled version. This is the world's largest six-engine Antonov AN-225 “Mriya” transport aircraft in 1:250 scale, the prototype of which was destroyed in Russian attacks on Ukraine.

www.herpa.de

+++ High Tech Modellbahnen +++

The plan is still to clear the backlog and then launch the new products announced so far.

www.z-hightech.de

+++ Imprintium +++

Imprintium informed us about its own new products and those that are being developed in co-operation with Minichamps. We would like to list them all here. Let's start with Minichamps, whose models are printed by Imprintium.

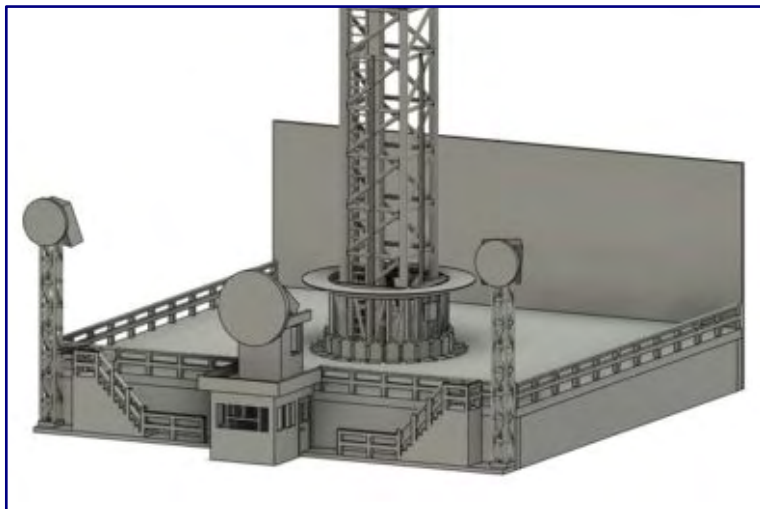
The following new products will be available at Intermodellbau 2024 in Dortmund, where they will also be presented to the public for the first time. There are four new, multi-part models.

It starts with the Opel Rekord from 1966, followed by the Triumph TR6, which is three years younger than the prototype, and a more recent vehicle, built in 2016, which goes by the name of Porsche 911 R. The line-up is rounded off by a horse trailer with an opening tailgate.



The 1966 Opel Rekord (left) and the Porsche 911 R (right) are contributed by Minichamps. Photo: Imprintium

The list of Imprintium's own productions is somewhat longer. At the first presentation in Dortmund, the main focus here will be on funfairs and fairgrounds. To match the free-fall tower kit (item no. D220-3030), there will also be a matching fleet of fairground vehicles.



The manufacturer at least shows us a CAD illustration of the freefall tower. Illustration: Imprintium

The kit of the free-fall tower is modelled on a prototype with a height of 55 m from the nineties. The resin parts also include a sticker sheet for designing the ride.

Only during Intermodellbau will there be a reduced trade fair price for this and for the road vehicles on the theme.

There are five different tractors and trailers to match. They will also be available painted at Intermodellbau. This results in the following overall line-up:

- Two-axle tractor unit with platform (D220-2002)
- Three-axle tractor unit with flatbed (D220-2003)
- Three-axle tractor unit with small crane and platform (D220-2004)
- Three-axle tractor unit with superstructure for transporting fairground rides (D220-2005)
- Three-axle tractor unit with crane at the rear (D220-2006)
- Two-axle trailer, short with drawbar (D220-2020)
- Three-axle trailer with drawbar (D220-2021)
- Four-axle trailer with drawbar (D220-2022)
- Four-axle trailer with drawbar and load protection wall (D220-2023)
- Four-axle trailer roller with drawbar and superstructure for transporting fairground rides (D220-2024)
- Combination pack consisting of D220-2002 + D220-2020 (D220-2120)
- Combination pack consisting of D220-2003 + D220-2021 (D220-2121)

- Combination pack consisting of D220-2004 + D220-2022 (D220-2122)
- Combination pack consisting of D220-2005 + D220-2024 (D220-2123)
- Combination pack consisting of D220-2006 + D220-2023 (D220-2124)



Here we see all five combi packs in one photo, as they will be offered as a special item at Intermodellbau in Dortmund. Photo: Imprintium

All five combi packs will be available as a limited-edition special item at Intermodellbau. All regular spring new products can be pre-ordered from Imprintium with immediate effect and will be available from the 17th calendar week.

A ghost train kit with functions, caravans and other vehicles related to the funfair is planned for the second half of 2024. This will definitely include a tractor unit with living container and veranda. A mobile crane will then follow at the end of the year.

imprintium.de

+++ JMC Scale Models +++

On 15 October 2023, this Portuguese car model supplier presented the Henschel F261, a German truck in concrete mixer and tipper versions. However, we had not reported on this because we had not received any information about it.



At this point, we would like to add the two versions of the Henschel F261, which are presented here in their finished form. Photos: JMC Scale Models

Among the new products currently on show is a vehicle that had a pioneering character. The Wittler bread van will no doubt be unfamiliar to many today, but it once became famous as a delivery vehicle for the Wittler bakery in Berlin.

In addition to the striking red-brown paintwork with advertising message, this was also due to its status as an early electric vehicle, as we are talking about a car from the 1920s here.



The Wittler bread van (photo left) was an electric pioneer in the 1920s. The planned Ford Condor Motorhome (right) was built in 1963. Photo / illustration: JMC Scale Models

The small IFA G-5 ADKIII-3 mobile crane from the DDR is younger, but has also been around for a long time. The W-series Dodge with a gate as a loading platform reflects the development of commercial vehicles on the American continent. It marks the period from 1941 to 1947.

We travel back to 1963 with the Ford Condor Motorhome to discover the vastness of the American continent. The Euclid TS-24, an earthmoving machine, comes in handy in many areas of work.

jmc-scalemodels.blogspot.com

www.facebook.com/pages/category/Games-Toys/JMC-Scale-Models-341193572911400/

+++ Juno-Modellbau +++

This provider, which still does not have its own website and uses the sales platform listed below, became active as early as 1999. At that time, for example, it offered road signs for various eras, before taking a longer break in 2008.

He has been active again for some time now and what else can an engineer offer for the development of amusement rides but models of these very rides? That's right, he uses 3D printing and electrical components to realise booths, carousels, or Ferris wheels on a small scale. In addition to N gauge, these are also in Z gauge.

However, the rather diverse range also extends to useful tools that are manufactured in the same way. Even an excavator, slurry tanker, or a small locomotive (display model) can be found on his to be found on his offer page: It's worth taking a look!

<https://www.ebay.de/usr/smattack-de-tl>



The Steyr 91 with flatbed appears as a 1:220 scale kit. The photo shows a sample for N scale. Photo: Kastenbahner

+++ Kastenbahner +++

A lorry modelled on the Steyr 91 without superstructure will soon be available as a kit. It is suitable for layouts based on Austrian, German or Swiss models. Especially the ÖBB, the road construction office, municipalities, and the post office had this type in mass use.

The model is to be supplied as a highly detailed, unpainted kit and includes the lorry without superstructure. Matching tipper bodies can be ordered separately. The windows can be modelled with commercially available adhesive paints.

www.kastenbahner.com

We were not prompted to mention this supplier here by any new products that were labelled as such, but by the fact that they now have their own website on which the products are presented. Whereas these used to be etched parts kits sold under the name Babmessing (Behnke), they are now 3D printed models.

The range consists of various small locomotives, lorries, an excavator and fire engines. A complete overview can be quickly obtained by filtering on Z gauge.

k-b.design

+++ Klingenhöfer Miniaturen +++

Two new children's figures form the new spring range from Klingenhöfer Miniaturen. The first is still seasonally appropriate and shows the offspring with a pointed cap tobogganing on a sledge. When it soon gets warmer again, parents can safely let their child out again on their tricycle. The figures are sent to the customer painted in one colour (skin colour), who can then paint them as they wish.

www.klingenhoefer.com

+++ Krois-Modell +++

We have found the Krois Model Car System 9053 road filler (item no. KM9053) as a new in-house product on this supplier's website. It is supplied in a 1 kg container. According to Krois-Modell, it is easy to use and produces a very smooth surface thanks to its microfine fillers.



A child on a sledge (photo above) or tricycle (photo below) are the spring novelties at Klingenhöfer Miniaturen. Photos: Klingenhöfer Miniaturen

It is a highly elastic, two-component polyester fine filler based on a styrene-free formula. It is intended for substrates made of wood or cardboard for filling the magnetic tape for a car system, as it also exists for Z gauge.

krois-modell.at

+++ Krüger Modellbau +++

After etched running boards for four-axle compartment wagons were announced last year, the following three sets of parts are being added this year without further product explanations: running boards for “4 axle compartment wagon 2nd class” (item no. Z98814.02), “4 axle pack wagon” (Z98814.03), and “4 axle mail wagon” (Z98814.04).

www.krueger-modellbau.de

+++ Märklin +++

Across all gauges, there seems to be only one anniversary this year that is being highlighted with the new products, albeit, almost always rather cautiously, and hardly emphasised. This is the 50th anniversary of the once so unpopular and today almost nostalgic colour concept ocean blue / ivory.



This year's special exhibition wagon for the dealers (item no. 80134; see photo on page 55) is a reminder of this, for which a two-axle old tank wagon was selected, with a bright contrasting stripe applied to its ocean blue boiler. The colours are familiar, but in this arrangement, and on a goods wagon to boot, the message would hardly be recognisable without a note in the box.

If we leaf through the new items brochure, we find only one model according to the 1974 paint scheme specifications, and that is 218 446-3 (88808) from the Deutsche Bahn AG stock. The shapes of the mega-spring bogies and body with Behr radiator are a perfect match for this locomotive, which is classed as Era VI.

It is still used in intercity service today and has a nostalgic livery with an Ege biscuit – US railroaders would call this “heritage colours.” We are secretly hoping that with the other new product announcements this year, passenger coaches will also be added in the design as they saw the light of day in 1974: black frame without a separating stripe to the ocean blue belly band.

Let us now take a look at the MHI exclusive new products: We had already presented the Wismar rail bus VT 889 in DB livery (88820) for club members. As is the case every year, it has also been included in the new items brochure. A largely finished hand sample was now on display in Nürnberg (Nuremberg).



A fully painted and labelled sample of the DB Wismar rail bus VT 889 (item no. 88820) was on display in Nürnberg (Nuremberg) for the first time.

Another MHI special series is the class P 8 steam locomotive in the original Prussian livery (88995) and three matching four-axle compartment cars (87568) with the three car classes common at that time. These are also the only new products for Era I.



Era I for the MHI: P 8 steam locomotive in original Prussian livery (88995) and four-axle compartment cars (87568). Photo: Märklin

This is now followed by two new models. The diesel-hydraulic multi-purpose locomotive V 90 (88510) in purple red became a bestseller as soon as it was announced and attracted a lot of interest. After all, it is one of the most important desired models in our hobby. A finished-looking prototype was already on display in Nuremberg.



Surprisingly, the purple V 90 (88510) was also on display in Nürnberg (Nuremberg) as an almost finished model and could be admired.

This series poses great challenges for the design and has probably not been so long in coming for no good reason. However, CAD volume representations and the hand sample suggest that the scale and proportions should be very good here.

This was only made possible by a slim bell-shaped armature motor in the front end and tiny SMD light-emitting diodes are required for a complete light change from warm white to red. What Märklin has undertaken here for the smallest scale has already caused the failure of a project in a larger scale.

But Märklin goes one better: An unobstructed view through the driver's cab, in which an interior decoration is also one of the product features to be emphasised. Even the metal buffers deserve a closer look, as they also feature engraved structures on their plates for the first time, as we are familiar with from the prototype.

From the Göppingen point of view, the two pairs of Off 52 (86235), the first double-decker car transport wagon of the DB, are also new. They are based on the open wagon of the Omm 52 type, which was redesigned in scale length a few years ago. Accordingly, these wagons now also have the correct length compared to their predecessor.



The new Off 52 (86235) car transport wagons are long in scale and have brown platforms to match their operating condition. Photo: Märklin

Each pair is permanently coupled together, without factory loading and also shows a brown platform on the upper deck in 1961 operating condition. Earlier painting regulations had still prescribed a black colour for such add-on parts. A change can also be seen here compared to the earlier, non-standardised requirements.

The cast bronze series continues with a Glmhs boxcar (86004). If the type designation does not mean anything to you, please refer to item number 8605, under which this car type first appeared and was offered for many years, until a mould revision in the running gear area.

Unfortunately, the Altenbeken museum locomotive 044 389-5 (88976) will not be released in time for the Z gauge weekend, which will also continue a series and create a special model for the event. It is Märklin's first realisation of a coal-fired steam locomotive of this class with a computer number.

Externally, it corresponds to known versions, so it will also appear with the very old tender model, but will have different wheel designs (spoked wheels at the front and disc wheels at the rear) in line with the prototype. The rain roof retrofitted to the museum engine presented outdoors will not be reproduced, so that the model will remain "operational".

Completely equipped with spoked wheels on all three tender axles is a Prussian veteran class 055 (88986). A version in the last operating condition now rounds off the range of this freight locomotive. We have left a note that, unlike the product illustration, the prototype did not have lantern brackets.

The three-axle DB conversion wagons are once again attracting attention with Era IV lettering. After the 2nd class pairs only recently appeared in the range, all three possible combinations are now available in a display (87061) with 6 pairs of coaches to choose from.

Friends of the DR will also be treated to two new Era III items. Less spectacular is the "Rügen Sunbathers Mixed Use Train" (87503) wagon set with three Bi 341 thunderbolts as well as a G-04 covered wagon and a tank wagon with brakeman's cab.



The DR class 78 (88069) can certainly not be denied its visual appeal with Witte wind deflectors. However, it is also interesting that these are now attached prototypically for the first time.

The class 78 tender locomotive (88069), on the other hand, was immediately met with approval, as it is the first to be fitted with Witte wind deflectors in accordance with its prototype used on the East Island. It

should also be noted that these do not fall out of the familiar die-cut moulds, but have been newly adapted to this locomotive and also show four holders in the correct places in accordance with the prototype.



The diesel locomotive V 125 of the Württembergische Eisenbahn-Gesellschaft (88211) is this year's exhibition locomotive. For this purpose, it also has individually attached fans on the long bonnet. Photo: Märklin



A wagon pack with Sgns 691 is followed in 2024 by this eight-part display, in which the wagons are assigned to different railway companies and are loaded differently. Photo: Märklin

But the joy is increased even further, because the class 01.5 express steam locomotive, which was delivered for the first-time last year, now also has a DR counterpart for Era IV with a computer number. In terms of the mould used, it is identical to the previous variant.

An old acquaintance is the Hbis-t 299 sliding wall wagon (82156) as a private wagon in the "Staatl. Fachingen" advertising design, which is now being offered individually with the latest livery. The SKW Trostberg AG carbide bottle wagons (82092) had already been leaked once and now actually appear in the brochure. Five examples without a brakeman's cab with different operating and car numbers await the customer here.

"Lady Hundred" also underwent a cure to become this year's exhibition locomotive as the V 125 diesel locomotive of the Württembergische Eisenbahn-Gesellschaft (88211). Not only has the colour been adapted to the WEG concept, but she also has separately attached fans on the long bonnet.



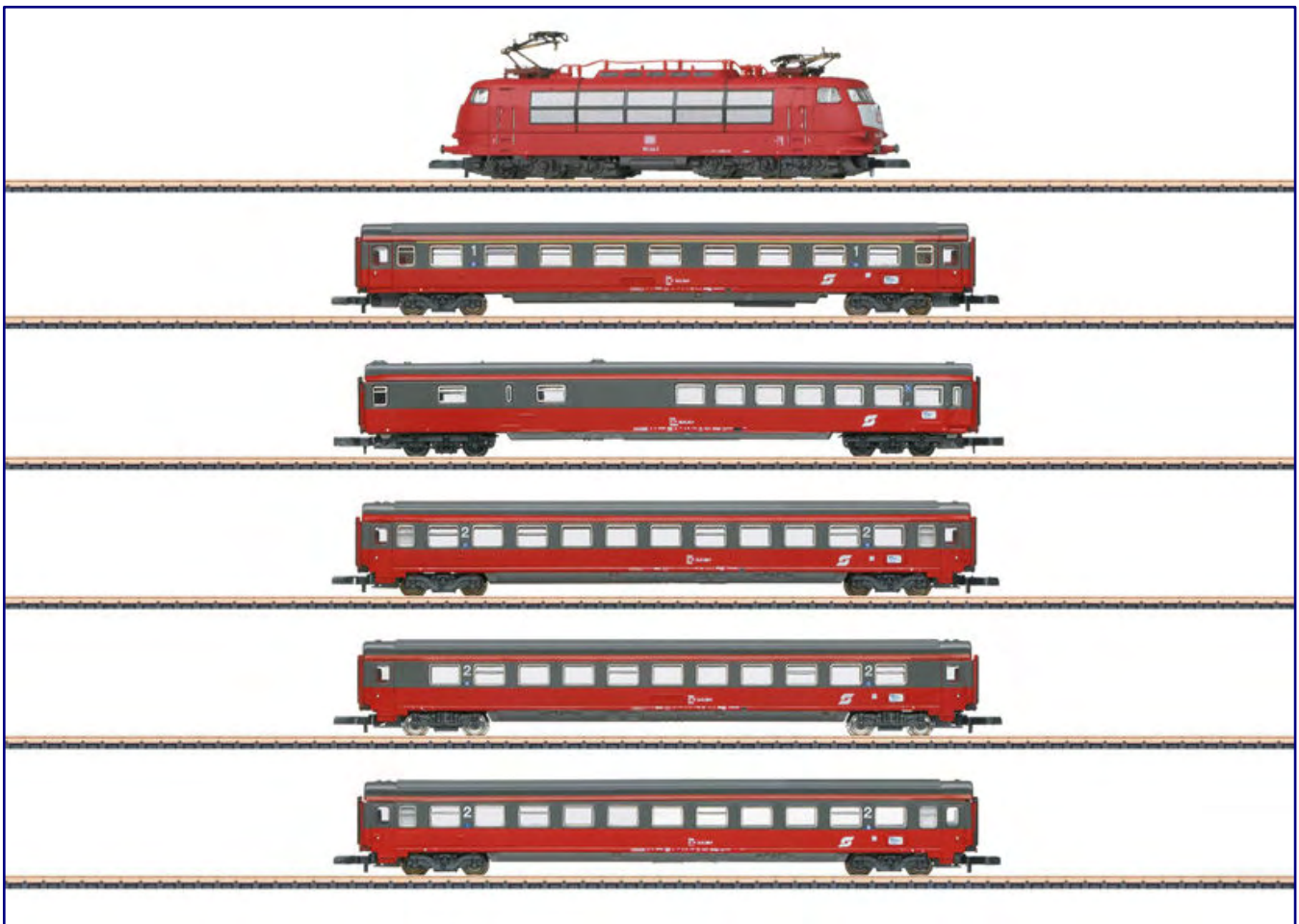
The SBB locomotive Re 4/4II of the SBB (88594) is modified for a model of the first delivery series with only one pantograph in the roof area. Photo: Märklin

Perhaps it will then pull the five Eanos-x 052 (82542) high side cars, also in Era V, of which four are painted traffic red and one is painted red-brown. They also show differences in the reproduced technical equipment with brackets and handbrake wheels. The scrap loads included are also individual as they are produced individually and do not have a standardised shape.

A new display (82641) has been released to counter the lack of modern container wagons that existed until last year. In it we find eight different versions of the class Sgns 691. They not only have different containers of 20 and 40 feet in length, including tank containers, but are also assigned to other railway companies in addition to Deutsche Bahn AG, which is also expressed in partially different frame paint scheme.

We then continue exclusively with our European neighbours. The SBB locomotive Re 4/4II (88594) undergoes a metamorphosis and appears as an Era VI model with only one pantograph from the first series. In order to keep the price affordable, Märklin has dispensed with a new front with a slightly different inclination, especially as the difference would hardly be noticeable when viewed from above. The distinctive, differing roof superstructures, however, are of course taken into account.

A “Type EW IV” passenger coach set (87663) with one 1st class and two 2nd class coaches, as well as a panorama coach will be released to go with this. The mould selection for the EW IV types is not yet final, and will probably be changed from the product illustration after customer input.



The EC 64 “Mozart” is the subject of a six-part train set (81282) with ÖBB coaches and DB express locomotive. Photo: Märklin

The Rh 1020 of the ÖBB returns to the programme in the classic blood orange livery with the “Pflatsch”. Since a product update, it already has the cab front windows and fan openings on the front ends that have become so typical of an Austrian locomotive. It is pulled by three four-axle Res 3936 (86664) stake wagons from the same railway administration, one of which is loaded with logs.

The EC 64 “Mozart” was travelling between Paris (Est) and Vienna and is offered as a six-car train set (81282). The train locomotive is a German Federal Railways class 103 Orient red express locomotive with a black bordered fan belt. The five ÖBB cars Avmz (1 car), WRmz (1 car), and Bmz (3 cars) also have a colour scheme that matches Era V.

The latest locomotive innovation is a diesel locomotive with snow plough (88362), for which the market leader has also come up with something special: The NOHAB Di3 628 round nose of the NSB is equipped with newly designed parts. These are snow ploughs cast from bronze and a roof attachment made from the same material. The locomotive body is moulded in metal-filled plastic. In addition to the directional headlights, the model also has functional warning lights on the roof.



A real surprise is the NSB's Di3 628 diesel locomotive with a cast bronze snow plough (88362) and functional warning lights on the cab roof. Photo: Märklin

Three hard cardboard kits are available for the accessories range. The two special bumpers (89013) certainly require some explanation, as in the prototype they are not used to bump locomotives or wagons, but to reposition a slipped wagon load, which should actually be prevented by suitable measures, such as tying down.

We are already familiar with the Hanau Hbf temporary signal box (89808), which consists of a blunderbuss placed on a wall base, from larger scales, as well as the Jagstzell station (89708), which also replicates a real-life model and therefore fits perfectly with Württemberg railway motifs.

The new Märklin programme "Track Planning 2D/3D" in version 11.0 (60524) on USB stick, also available as an activation key (60525), is only available in German. The 2024 Insider annual car, a model from the former Dresden class district with Telefunken advertising, should be mentioned again briefly.

www.maerklin.de

+++ MBZ Modellbahnzubehör +++

What we experience is also reported by our readers: Communication with this provider is very difficult, even though delivery is reliable. In our experience, enquiries (outside of orders) are regularly not answered.

This also makes it difficult to compare new products year after year, as we have to rely on the undated page entries. However, we have again found some products that have not yet been featured in our magazine, and that look very appealing.

This is, for example, the Bondorf L16 railway caretaker's house (item no. 16647), built in the Black Forest in 1935. A single-track steel girder truss bridge (16319) is modelled on the Trogenbach viaduct near Ludwigstadt.



The Trogenbach viaduct near Ludwigstadt (item no. 16319) is one of the new kits that MBZ lists on its website. Photo: MBZ

The old bowling alley (16130) from Nordhalben is used for leisure activities. All MBZ kits, including those not mentioned here, are delivered to the customer without a colour scheme and are made of grey Finn cardboard. With the travelling voltage regulator for Z gauge (73040), we have found another product tailored to our scale that delivers up to 9 volts to the track and can supply 750 mA current. The manufacturer emphasises good low-speed characteristics.

www.mbz-modellbahnzubehoer.de

+++ MHN Horneber Collection +++

We met this supplier of glass display cases and shop fittings at the toy fair in 2023. Our most recent discussion at the fair revealed that glass showcases for private users are definitely the company's main sales focus.

The focus here is always on customised solutions. The programme on offer provides orientation through various product lines, but is also manufactured according to individual dimensions. This means that the customer does not acquire an off-the-peg product that takes no account of their individual space requirements and needs.

As a result, we cannot name any specific new products here, but refer interested parties to contact us directly for advice and customisation.

www.glasvitrienen.de

+++ Michael Boost +++

The new digital interior lighting is expected to be available from February 2024 and can be individually shortened to a minimum length of 35 mm, allowing it to be used in different passenger carriages.

www.michas-moba.de



“SOSO Pears” refrigerator car (item no. 518 00 852; top photo) and Union Pacific covered car (511 00 164; bottom photo). Photos: Micro-Trains

+++ Micro-Trains (MTL) +++

The new deliveries from Micro-Trains are currently picking up speed again. The EMD F7A diesel locomotive (item no. 980 01 550) matching the “War of the Worlds” series is available for delivery together with the freight car (535 00 510) accompanying this train.

Customers can also look forward to three refrigerated wagons, including two operating numbers for the MT&L RR as “Piggy Pears” (518 00 851) and “SOSO Pears” (518 00 852) as well as P&E “Kansas Egg & Poultry Co.” (518 00 862).

Two covered cars with simple sliding doors, but different car numbers, appear for the Union Pacific in the typical yellow colour (511 00 163 / 164).

www.micro-trains.com

+++ MI Modellbahn Innovationen +++

In this case, it is a very new supplier that offers a staging yard control system on its website. A reflex light barrier module has now been added. However, this can also be used independently of the basic module_01 (Basismodul_01) offered as a track occupancy detector or feedback sensor. This applies regardless of the existing track system and track gauge.

A description of the product is available as a PDF document on the company website. The scope of delivery includes the photoelectric sensor module, the reflex sensor (transmitter and receiver diode in plastic housing) with 4-pole ribbon cable and two 2-pole socket connectors, as well as an adapter cable with connection terminals.

modbahninno.de

+++ MKC Modellbahn Kreativ Chemnitz +++

We have not found any new products from the specialist for fine castings, but following the reorganisation of electronic sales, all Z gauge products can now be found again with photos on the pages. Interested parties who did not or no longer have this programme in view are recommended to visit the pages to find what they are looking for.

www.modellbahn-kreativshop.de

+++ Modellbahn-Decals Andreas Nothaft +++

Among the new items presented by Andreas Nothaft are also some that are suitable for scaling to 1:220 scale for use on Z gauge models. For example, Märklin models of the Swiss Ae 6/6 could be re-labelled to the heraldic locomotive 11520 "Langnau" (item no. 11520).

The Ege biscuits in pop colours with white inner filling (2376), which have been on offer for some time, are now also available without filling colour (2375) and would be part of your own painting projects or for repairing older models. Addresses for road vehicles of the DDR post office for radio and television studio technology (3595) are intended for DDR nostalgics, assuming matching car models.



This labelling set (item no. 6295) promises a visually appealing version of the Eurorunner from Siemens, which has remained quite rare. Illustration: Modellbahn-Decals Andreas Nothaft

The labelling sets 3316 (without background), 3317 (brown background) and 3318 (traffic red background) can be used to re-label FR or Märklin models of the Eanos-x 052 open car to an Eanos-x 055.

One labelling sheet is sufficient for five wagons. The prototypes are externally identical to type 052, but were once built for the AAE and leased by DB AG.

Märklin models always reproduce the 2184 substructure type with regard to the roof equipment and most of the bogies, for which the next sheet fits perfectly: A current version of Deutsche Bahn AG for Era VI is thus possible with an individual number (6293).



The labelling set with brown background (3317) is one of three for the Eanos-x 055. Illustration: Modellbahn-Decals Andreas Nothaft

The class 143 belongs to the same period after being re-lettered for the railway construction group (6294). The Siemens Eurorunner has not yet been blessed with many variants at Märklin. The last re-lettering set mentioned here assigns it to the Railadventure company (6925) after a suitable colour scheme.

www.modellbahndecals.de

+++ Modellbahn Digital Peter Stärz +++

New is a hand controller with 4-line display for Selectrix, Selectrix-2, DCC and Motorola (item no. HRS 2 db / sz / mg / sg), available as a ready-made module in four colours – shown in dark blue on the product photo. Display and input options allow simultaneous access to a locomotive and a Selektrix switching item.



Foto: Modellbahn Digital Peter Stärz

The quick access memory can be used to switch separately between two locomotive addresses and up to eight switching article addresses per SX bus (SX0 and SX1).

Numerous setting options via the menu also allow the display and operating functionality to be customised to personal taste.

Also included are detailed operating instructions; further information on the product can be found on the manufacturer's website.

www.firma-staerz.de

+++ Modellbahn Union +++

Modellbahn-Union had already expanded its relief house series shortly before the turn of the year, which we have already reported on. Most recently, various corner houses were added to

the programme. We would therefore like to briefly mention them here. However, we are also in constant dialogue with this provider regarding ideas.

www.modellbahnunion.com

+++ Modellbauatelier Steinbrecher +++

This supplier works to order, which is why there are usually no new items to be found on the website. The reference Spur-Z-Atelier by Karl Sinn, who also relies on supplies from this supplier, also speaks in favour of the good price-performance ratio.

www.modellbau-steinbrecher.de

+++ Modellbau Laffont +++

Modellbau Laffont from Aachen has also been one of the most reliable suppliers of 1:220 scale building models for years and has a few surprises in store for us in spring 2024, which will be available again gradually over the course of the year.

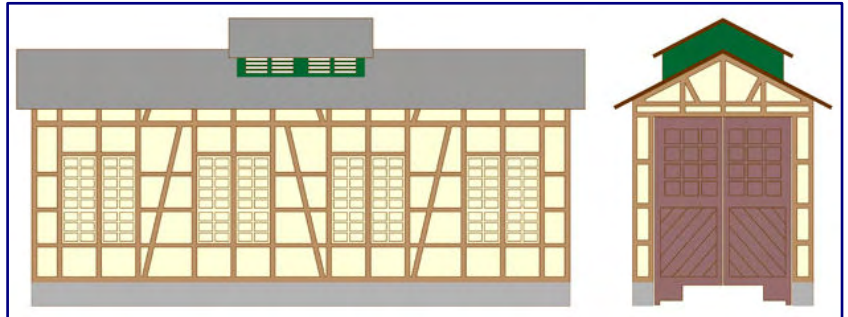
On the one hand, there is the Wiesen railway station (item no. Z6801), which can be found in the prototype in Switzerland between Filisur and Davos on the Rhaetian Railway. The kit is made of stained plywood and hard cardboard, and impresses with the finest roof tile and quarry stone engravings on the loading ramp. Wooden mortises engraved on both sides are another feature that is explicitly pointed out.



The Wiesen railway station (item no. Z6801) certainly enhances the model railway with more than just Swiss narrow-gauge motifs. Photo: Modellbau Laffont

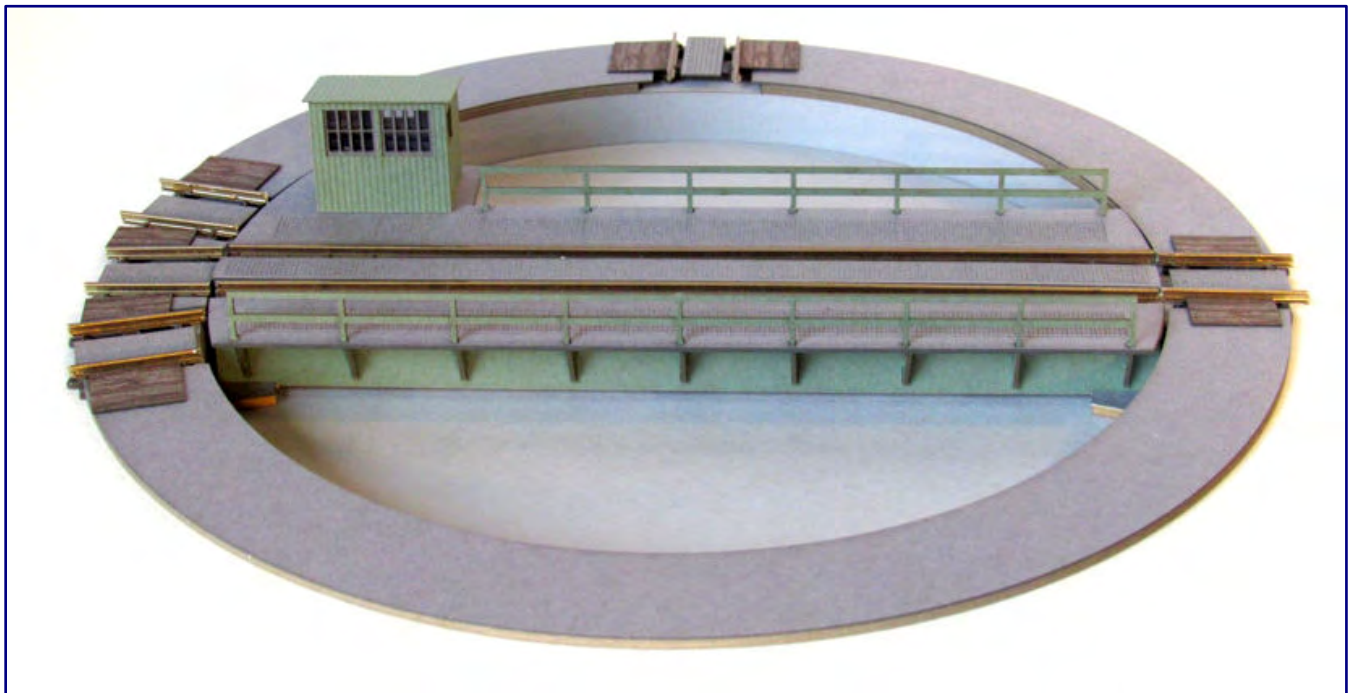
A single locomotive shed (Z9301) is interesting, especially as a turntable (Z9101) is also announced at the same time. This is a dummy without tracks ex-works, but the platform (without drive) can be turned manually.

Five track exits are provided here, but more are also possible with simple cuts. The highlighted product features include finely engraved chequer plates in the platform area, and for the track exits, as well as four irregularly engraved wooden plank transitions, and a pit with a brick wall.



A town gate (Z9501) from the Alpine environment with a half-timbered extension is also planned.

So far only this illustration of the engine shed (Z9301) exists. Photo: Modellbau Laffont



The new turntable (Z9101) provides an alternative for the Länderbahn and early Reichsbahn era as well as terminus stations on hub lines. Photo: Modellbau Laffont

www.modellbau-laffont.de

+++ Modellland +++

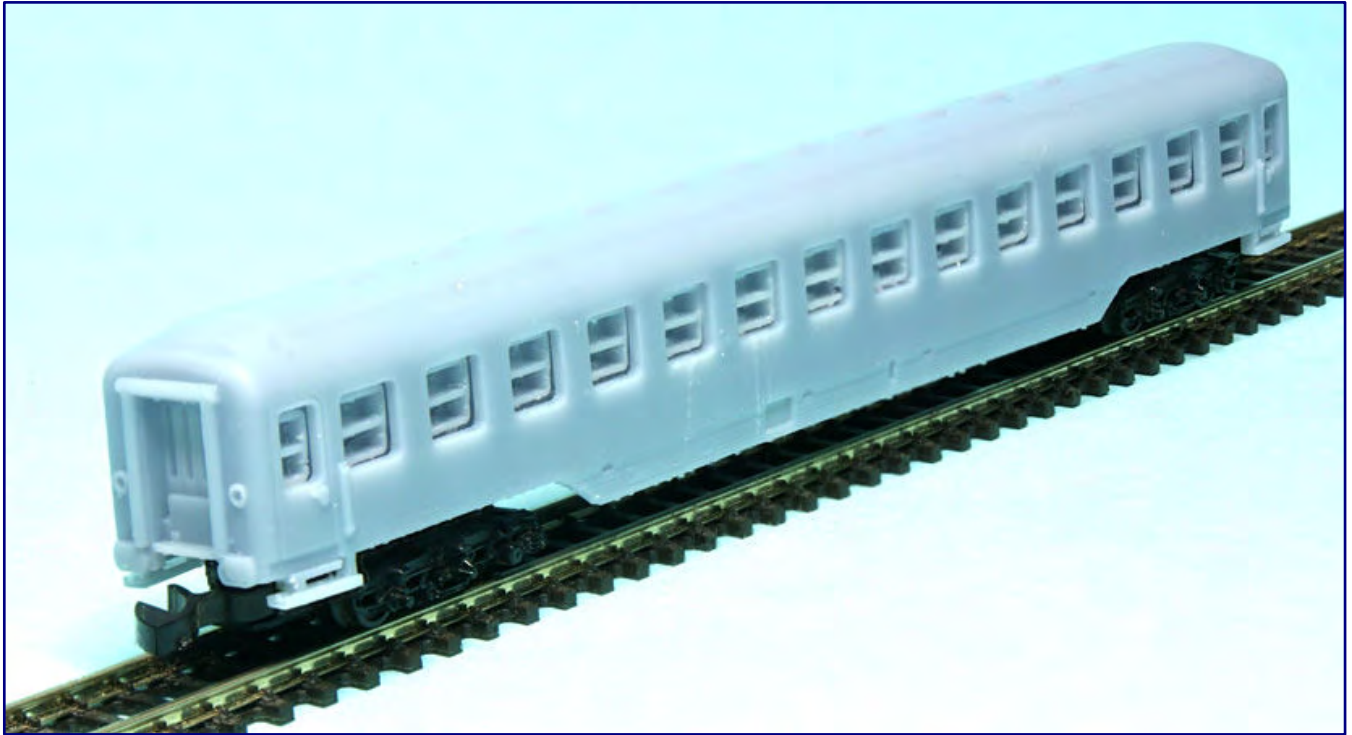
At the moment there are no new products for Z gauge, this manufacturer informed us. However, packing stations for DHL and Amazon are planned. When exactly these can be released is still uncertain at the moment. The pictures show what the models will then look like.

www.modellland.de

+++ NoBa-Modelle +++

As if we had expected it, NoBa-Modelle has some real hits to share with us. At the same time, they are also switching to Märklin components, as a parts supplier. But first, we would like to add that the class 624/924 diesel multiple unit can now also be ordered as a finished model (item no. 5231RF).

The DB couchette coach Bc4ümg-53 (5109), the later Bc(ü)m 251, which is also being 3D printed but runs on Märklin bogies of the Minden-Deutz type, is also almost ready for delivery. It establishes a completely new family of cars in Z gauge, marking the start of the Bundesbahn's new construction programme.



The DB couchette coach Bc4ümg-53 / Bc(ü)m 251 (item no. 5109) will be available after this year's summer break. The prototypes initially ran for Touropa from 1953 and later, also, for Scharnow. In addition to the apron, the revolving doors, fixed steps and the folding doors of the car gangways are striking. Thus, a completely new family of coaches saw the light of day here!

The prototypes were procured for regular services and were mainly used by Touropa, whose livery and inscriptions these carriages with aprons also bore. In the age of the economic miracle, they helped people to take their first holiday trips again, including to neighbouring countries such as Austria or Italy.

A central development goal for the family of wagons created here was technical and visual compatibility with Märklin wagons. This means that this wagon can not only be coupled with Märklin models and has identical running characteristics, but also harmonises in terms of height, width, and other appearance. For example, the supplier has introduced a new concept for the reproduction of the compartment windows.

The class E 910 (5024 / 5024L / 5024RF) articulated locomotive was nearing completion of development at the time of going to press. The prototypes ran as shunting locomotives until the seventies, some of them were even at home outside Bavaria in the Oberhausen-Osterfeld Süd depot for a time and were used in the local freight service of the coal and steel transport system.



The three-part class E 91⁰ locomotive, which uses the running gear of the Swiss Crocodile from Märklin, is now about to go into production. The prototypes were last used in Munich and Oberhausen. Attention: The photo still shows a handmade sample.

Due to their three-part construction with two-part chassis, they are just as attractive to look at on the layout as the Swiss Crocodile, on whose chassis these models are built. Visually, however, they are also a real attraction getter due to the large number of details depicted.

However, this manufacturer is currently also taking to the skies. It is doing so with a model of the modern Airbus H135 (14000) helicopter, which is used by the German Armed Forces, the police and as a rescue helicopter, among others.

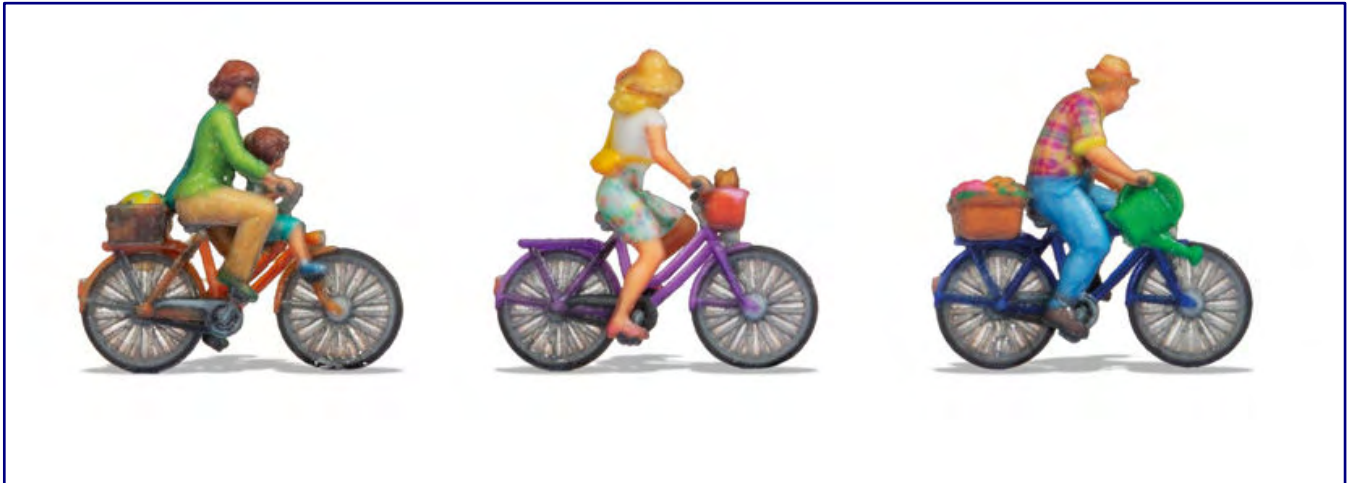
www.noba-modelle.de

+++ Noch +++

As in the previous year, the central theme for the Z gauge is "colour 3D printing". In **Trainini TV** episode 25, Managing Director Dr Rainer Noch explains in an interview the technical problems in the production start-up that led to the huge delay in delivery dates for last year's new products.

Now that these have been rectified, production can continue, and the new delivery date has been slightly adjusted to the end of March / beginning of April 2024. The new 2024 models that have now been presented are due to follow in October of this year and impressed us as samples on display, just like last year.

In the following figure packs we usually find six human figures, some with matching accessories. Announced were warehouse workers (item no. 44502), hunters and woodcutters (44503), farmers



The warehouse workers (item no. 44502; picture above) and cyclists (44532; picture below) are two of the new figures from Wangen. The forest animals (44543) are also particularly appealing, of which there is unfortunately no product illustration. Photos: Noch

(44503), travellers (44522), cyclists (44532; 3 figures on bikes), anglers (44533), forest animals (44543; eight different animals); as well as sheep and shepherds (44544; shepherd, dog and seven sheep).



It was not easy to make the occupants of the Volkswagen Transporter (from Noch 44604) visible in this photo using the trade fair model.

His innovative production method still knows but also even more because even car models can also be convincingly built.

If the official product photos still show layers of print, we can't say the same about what we saw ourselves in Nürnberg (Nuremberg). It is particularly commendable that four vehicle types were genuine everyday cars and that the focus was not once again on priceless dream cars.

The Trabant 601 (44601) as a symbol of German reunification, the Opel Corsa A (44602), the Citroën GSA (44603), and the Volkswagen T2 (44604) will make the start. The French saloon will hopefully also support Azar Models' aspirations,



Eight in one go: all four new car types, each in two colours, are presented here on a village street. Photo: Noch

while the Volkswagen bus with its rather large windows impresses with a view into the interior, which even makes a driver recognisable!

“Off to the south!” is the motto with four new palm trees (21973) for H0, TT, and N gauges. But to be honest, with a tree height of just 4.5 cm, the equivalent of almost 10 metres, they are much better suited to our scale. So, if you have the right motifs for them, you are welcome to include them in your choice.



The four new palm trees (21973) are not suitable for every layout. Photo: Noch

The trees in the Master series, some of whose new products are also suitable for our scale, are handmade to meet the highest demands. They are already available for delivery and we therefore briefly presented them in the December issue (after announcing them last September).

The Micro-Rooms system for building lighting was also announced in the September 2023 issue. The starter pack and other LED elements will now be available from April 2024.

The new texture powder for road construction promises improved material. It is now finer-grained and will be included individually (60822) and in a starter pack for road construction (60821). Noch's rigid foam products, which can also be individually moulded, are very popular. It, therefore, made sense to make this material available in sheets of five (58301) for customised cutting and building ideas.



The texture powder contained in the road construction starter pack (60821; photo left) is now finer-grained. This material is also available individually for replenishment (60822; photo right). Photos: Noch

The new product selection for spring is rounded off with a new item from the laser-cutting department. A 13.5 cm long and 3.2 cm wide stone arch bridge (44380) is made of high-quality, solid-coloured hard cardboard and is 4.3 cm high.



The stone arch bridge (44380) is made of hard cardboard and therefore has a matt, very realistic surface. Photo: Noch

www.noch.de

+++ Panzer-Shop.nl +++

We would like to take this opportunity to point out once again that this supplier reactivated its very extensive Z gauge programme last year. Since then, figures, military and civilian vehicles have been available again as unpainted, 3D printed products in very good detail.

www.panzer-shop.nl

+++ Picetool +++

Here we list a supplier from China whose range belongs to the field of model making, but still keeps a noticeable distance from model railways. The programme includes many 3D metal puzzles, some of which may well be of special interest to individual Zetties.

Some ship models from the cinema series "Pirates of the Caribbean" seemed to us to be suitable for scale, which certainly do not reproduce an everyday theme on the model railway, but can nevertheless become an unusual module theme with an eye-catching effect, as a film location with a film backdrop.

The range includes the large sailing ships "The Queen Anne's Revenge" (item no. HP239-KG) and "The Flying Dutchman" (HP240-KS) as well as, the smaller "Black Pearl" (HP151-KG) and "Abyssal Ghoste"

(HP315-KS). The kits are made of stainless steel and are assembled without glue by folding and mortising.

The range includes the large sailing ships "The Queen Anne's Revenge" (item no. HP239-KG) and "The Flying Dutchman" (HP240-KS), as well as the smaller "Black Pearl" (HP151-KG) and "Abyssal Ghoste" (HP315-KS). The kits are made of stainless steel and are assembled without glue by folding and mortising.

[only via sales partners](#)

+++ Preiser +++

Figure manufacturer Preiser is celebrating its 75th anniversary this year. With this in mind, we are keeping quiet about the unchanged backlog of deliveries of new products from previous years.



The ship "The Queen Anne's Revenge" (item no. HP239-KG) is shown here, as an example.

A flock of sheep with 60 animals (item no. 88580) without shepherd and dog was announced at the toy fair. This will enable Zetties to increase their animal population to realistic dimensions and thus also reproduce the typical lines of movement of the fleeing animals.

www.preiserfiguren.de



Sheep (item no. 88580) are finally being released in a confection with 60 animals, allowing a herd to be reproduced without having to keep back an unnecessary number of dogs and shepherds. Photo: Preiser

+++ RATIMO Rainer-Tielke-Modellbau +++

Rainer Tielke has already shown us his spring innovation in Zell (Mosel). New in his production is a horse trailer, which is offered as a painted finished model and should be available soon.

www.ratimo-z.de

www.rainer-tielke-modellbau.de

+++ Rokuhan +++

In the January 2023 issue, we presented the Japanese EF58 locomotive in detail as a new design. Two further variants of this model are now available from Noch-Vertrieb. These include another brown version with the road number 150 (item no. 7297977 / T039-2) and the later blue version as EF58 127 (7297978 / T030-3).

Noch recommend the new 14K passenger carriages, which are available in a four-part (7297704 / T006-1) and a two-part supplementary pack (7297705 / T006-2) in blue livery.



Rainer Tielke presents this horse trailer in Zell (Mosel) as a spring 2024 novelty.



The EF58 127 (7297978 / T030-3) was presented as a new addition to the Noch range, now in blue livery. Photo: Rokuhan

The 0 Series Shinkansen “Hikari No. 1” is not listed in the new products brochure, but can also be ordered from Noch via dealers (7297942 / T020-1).

We have also already reported on two new Shortys: They reproduce the “glass train” of the 491 series in the colours Olympic blue (7297943 / ST015-1) and gentian blue (7297944 / ST015-2). They have very fine and much more true-to-scale pantographs than Märklin models. Only the collector strips appear too large and actually have almost exactly the same dimensions as those of the Göppingen manufacturer.

Two Shortys of the US EMD F7 diesel locomotive, each as a duo of A and B units, are also new to Noch sales: Rio Grande (7292973 / ST012-3) and Southern (7292974 / ST012-4). The separately available bogies, with which the Shorty models can be made rollable or drivable, have been in the range for several years now.

www.rokuhan.de

+++ RP Toolz +++

We have discovered a very interesting tool manufacturer from Hungary. Péter Horváth is not new on the market, but caught our eye because of its extensive programme. It offers a wide range of tools that are required for processing etched parts kits.

An overview can be quickly obtained from the manufacturer's website, but the products are not described in detail, and therefore require tool knowledge.

www.rptoolz.com

+++ Schreiber-Bogen Kartonmodellbau (Aue-Verlag) +++

There are also some new kits for paper modellers that are so close to Z gauge in terms of their scale that they can be used for suitable purposes. One of these is the Falkenstein Ludwig II Castle on a scale of 1:250 (Art.-Nr. 814).



This castle was abandoned and burnt down towards the end of the Thirty Years' War. It was only when King Ludwig II of Bavaria acquired the ruins around 200 years later that they were to be used again. The kit reproduces the castle as it should have looked after completion.

The barque Theone (Art.-Nr. 816; scale 1:200) was one of the emigrant sailing ships. In contrast to many other ships, which were only used for this purpose in the 19th century due to the economic situation, it was designed specifically for this purpose. The ship enables harbour or museum scenes in combination with a railway in epoch I or that of the present day.

Although the Marienkirche Lübeck (Art.-Nr. S126) is on a scale of 1:300, as a very large building it is more suitable for model railway purposes. We would just like to remind you that many extremely large buildings have been and are reproduced in a different scale on display layouts without the viewer noticing it.

A special model for the 25th anniversary of the H MV is the floating dock with the fast steamer “Kaiser Wilhelm der Große” on a scale of 1:250 (3483), consisting of 4,280 parts. A laser-cut parts set (Art.-Nr. 4483) is available separately, to add to and refine the model.

In addition to the floating dock, the kit also includes the fast steamer “Kaiser Wilhelm der Große”, which has now been given an underwater hull for this type of presentation. The dock consists of four modules so that the ship fits inside.



The floating dock with the fast steamer “Kaiser Wilhelm der Große” (item no. 3483) is a dominant theme when it is to be integrated into a harbour facility. But the Falkenstein Ludwig II Castle (814; photo on page 77) cannot be overlooked either. Photos: Aue-Verlag

The dock is also heavily aged and thus contrasts with the freshly overhauled steamer ready to set sail. The following details can be added with the 857 parts of the above-mentioned set: complete railing for the ship and floating dock, companionways and ladders, crane parts, anchors, anchor chains, benches, deckchairs, davits, mast parts and shrouds.

For those interested, the brochure “Zur Geschichte des Kartonmodellbaus (The History of Cardboard Modelling), Heft 23” (Art.-Nr. agk23) has also been newly published.

www.schreiber-bogen.de

+++ Silhouette Modellbau +++

It's not easy with this manufacturer: great products, but considerable deficits in communication. Even at the time of going to press, the new spring 2024 products were still not available on the supplier pages. Enquiries were fruitless, only dealers could help us.

New are 2 cm high micro-bushes, which are at least the height of a hazelnut. They are available in the colour variants spring, summer, early autumn and late autumn (art. nos. 230-41 to 230-45).

The Profiline maple and chestnut deciduous trees, which are also new, have a height of approx. 8 cm in the size allocation N/Z, which represents relatively small trees. The maple appears in the colour versions early spring, summer, early autumn, late autumn and red (art. nos. 230-61 to 252-65), the chestnut in spring, summer, early autumn and late autumn (art. nos. 291-61 to 291-64).

Grass patches are also available for spring, summer, early autumn and late autumn (art. nos. 726-61 to 726-64), the same for a version with weeds (Art.-Nr. 726-71 to 726-74). In addition, there is a pack of 10 to 20 meerscham tree blanks (Art.-Nr. 200-99) and a starter composition "embankment – undergrowth" (Art.-Nr. 200-98), consisting of meerscham, foliage, and tufts of grass.

Due to the lack of options for model railway layouts, the announced expansion of the Fantasy series is mentioned here without listing these products individually.

www.mininatur.de

+++ Sparmax +++

The renowned Japanese manufacturer Iwata, known for its precision, apparently owns the Sparmax brand from Taiwan, which is distributed in Europe by Harder & Steenbeck (Hamburg). However, with the exception of a few compressors, the company strictly separates its products from its own programme, which also includes Hansa.

The core of the range consists of compressors and very favourably priced spray guns, which are, nevertheless, powerful and precise.

This makes them the discovery of this year's toy fair for us, as we were able to get our hands on an inexpensive entry-level model and paint a sheet of paper with an 0.2 mm nozzle as a test.



As with any new tool, it took some getting used to the lever pull and the spring force, but it felt good in the hand, as usual, and was characterised by a very even and constant spray jet at low working pressure.

The DH 102 model is a spray gun from the Taiwanese manufacturer's programme.
Photo: Sparmax

www.sparmaxair.de

+++ Spur Z Ladegut Josephine Küpper +++

Throughout the year, the Aachen-based load specialist always draws attention to relaunches of its products or newly designed load inserts. For this reason, it is currently not possible to make any explicit announcements at the start of the year. Customers should note that this supplier no longer takes part in trade fairs and exhibitions, but can still be contacted electronically to mark the 30th anniversary of its product range.

www.spurzladegut.de

+++ System Jörger +++

Manfred Jörger announces on his website that his railway barrier is available again. Attentive and interested readers have already noticed this, as this information was already posted last year.

www.system-joerger.de



+++ TCS Train Control System +++

This US supplier is currently advertising its LT-50 DCC starter system, which includes a hand controller, a single LT-50 board with cover plate, screws for mounting, a detachable track power connector and a single RJ-45 cable.

The 15 V power supply unit also included is designed for the US domestic power supply system.

The LT-50 device can be operated as a stand-alone control centre, but can also be integrated into an LCC network. The handset has a rotary knob with a finger recess, which is supplied a second time as a replacement.

www.tcsdcc.com

+++ Trafofuchs +++

Trafofuchs continued to be creative between Christmas and New Year. The result is the following new products for spring 2024: Six seated figures (item no. EP07) and five travellers (EP03) are new to the range for Era I.

For installations based on contemporary motifs, the security forces will be reinforced by four police officers (EK03) in blue and black uniforms and the mounted police (EK05).

DCC-Startsystem mit Handregler LT-50. Foto: TCS

Christmas market. That's why there is now a popcorn trolley with a vendor and two small customers in addition to the sales stands (S11).

www.trafofuchs.de

According to this provider, popcorn tastes good everywhere, be it on the railway platform, at a folk festival, market or



Five travellers (item no. EP03; top left), the police on horseback (EK05; top right), and the popcorn cart with vendor, and two small customers (S11; bottom) are current examples of Birgit Foken-Brock's creativity. Photos: Trafofuchs

+++ Uhlenbrock Elektronik +++

Uhlenbrock also no longer organises its new product announcements after the toy fair. Most recently, new products were announced in the autumn, which we also briefly presented in this magazine. Further updates are to be expected in the course of the year, possibly as early as Intermodellbau in Dortmund.

www.uhlenbrock.de

+++ Unique Scenery Products +++

This supplier not only took over the Minitec range a few years ago, but also stocks many other brands that are relevant to our gauge. It, therefore, always makes sense for landscape designers and hobbyists to take a look at the website. Incidentally, Unique is currently advertising landscaping seminars on various dates, which are apparently being held with extremely high-quality design material.

www.sceneryproducts.eu

+++ Velmo +++

Velmo is still working at full capacity with the production of its decoders. Claudius Veit has only been able to present two new decoders since 2020. If the order situation eases to such an extent that the focus can be shifted back to new developments, this will be communicated via our magazine or presented in Altenbeken at the beginning of May 2024.

www.velmo.de

+++ Viessmann +++

No new products were announced for the Kibri, Viessmann, and Vollmer brands. However, there was great joy at the trade fair stand about the editorial award for the small workshop from the previous year. We learnt that not only the composite material Polyplate, but explicitly also the Z gauge have a future in the company.

viessmann-modell.com

+++ WDW Full Throttle +++

In February, William Dean Wright is offering its customers a collector's pack of two 70-tonne bulk wagons, equipped with three discharge hoppers, from the CSX fleets (item no. FT-COL66). The models are equipped with different shields at the ends of the car bodies, which enable a higher dumping cone and at the same time should prevent the loss of load while travelling on gradients.



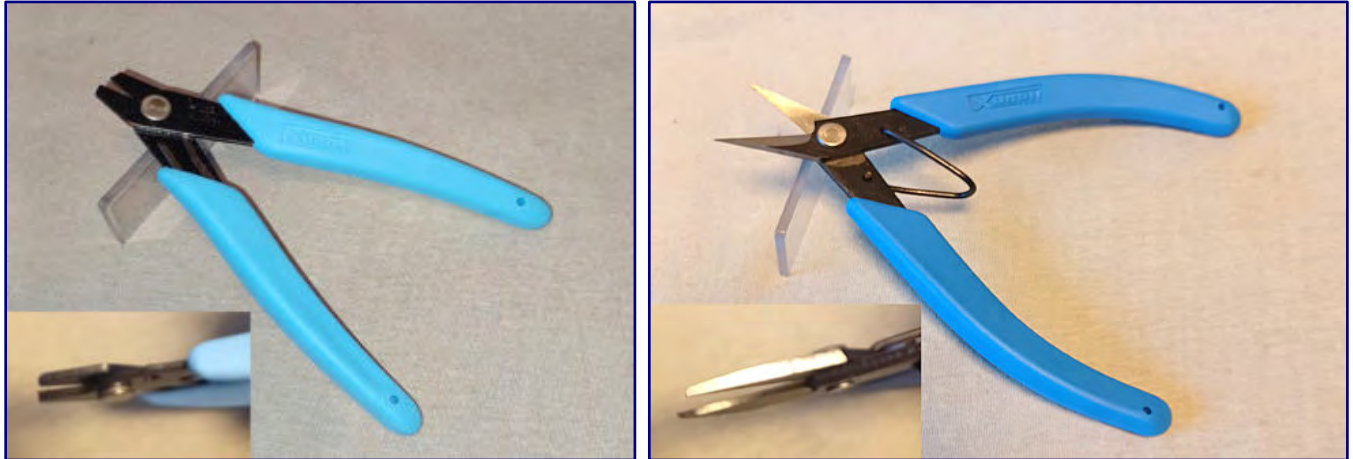
The unusual feature of the current wagon set (item no. FT-COL66) is the different shields on the head ends of the wagon bodies. Photo: WDW Full Throttle

The first example is a car of the then Family Line system of the Seaboard Coast Line Industries, the second example can be assigned to the Western Maryland and the Chessie system. CSX later emerged from the lines and railway administrations.

www.wdwfullthrottle.com

+++ Werkzeuge Peter Post +++

A new addition to the range is a double-sided wade-free side cutter (item no. 71013), which can cut soft wire up to 2 mm and semi-hard wire up to 1.3 mm in diameter. Applications include silver, gold, craft, copper, or bead wire. It is equipped with the spring and handles from Xuron.



New additions to the tool programme are the wade-free side cutter (item no. 71013; photo left) and the tin snips (71014; photo right). Photos: Werkzeuge Peter Post

The new sheet metal shears (71014) are truly universal shears for heavy-duty applications. Developed for cutting applications where serration is not required or not desired, it is suitable for cutting soft sheet metal up to 0.8 mm such as copper, brass and aluminium sheet, cable ties, webbing, narrow plastic strips (flat profiles), or fabric.

www.peter-post-werkzeuge.de

+++ Woodland Scenics +++

In Nürnberg (Nuremberg), we were able to ascertain that this manufacturer has not currently announced any new products in the field of design materials. However, our readers should be familiar with the existing range, which is distributed in Germany by Liliput.

woodlandscenics.woodlandscenics.com

+++ Yellow Dwarf +++

We have also received information on valuable new products from Prague.

In the month of the International Toy Fair, the theme is waste disposal. Various rubbish bins and containers ensure that the model railway is always kept tidy.

Black 240-litre waste bins (item no. 60001) for residual waste are likely to reflect the familiar look of the 1980s and 1990s, before waste separation became popular. They



Colourful 240-litre waste bins (item no. 60001) ensure waste separation on the model railway. Photo: Yellow Dwarf



The 1,100-litre bins are also available in different colours for recyclable materials (60004; top left) and black with visible waste (60005; top right). There are also collection bins for electrical waste and textiles (60006; bottom left) and used glass (60007; top right). Photos: Yellow Dwarf

then followed in colourful form (60002) for separated recyclables.

Waste containers with a volume of 1,100 litres can be found in larger residential units. They are also available as black plastic containers for residual waste (60003) and coloured for recyclable materials (60004), as well as in the black form, also open with visible waste (60005).

There are also collection bins for electrical waste and textiles (60006) and round glass recycling bins at the collection points (60007).

www.yellowdwarf.eu

+++ Zahn Pinsel +++

“Drybrushing” is an Anglicism that is on everyone's lips among modelling enthusiasts: it is a technique for applying very small amounts of paint or pigment to a model with a dry brush. This approach is highly effective when creating surface details on figures, buildings and models, as it emphasises and highlights details or can create highlights.

Depending on the task, flat, angled or round brushes are recommended, which must be soft enough to glide over surfaces without damage and at the same time only release their colour at the edges due to their sufficient firmness.

This is achieved by a shorter hair length and goat hair, which distinguishes them from bristle brushes. A corresponding range of brushes is now new in the manufacturer's programme and can be seen in our photos.

We have also found weathering brushes with a stippling tip made of easy-to-clean foam in the supplier programme. They ensure good colour transfer for textured work, as required for ageing, weathering, and shading. They can, therefore, be a useful addition to the aforementioned range of brushes.



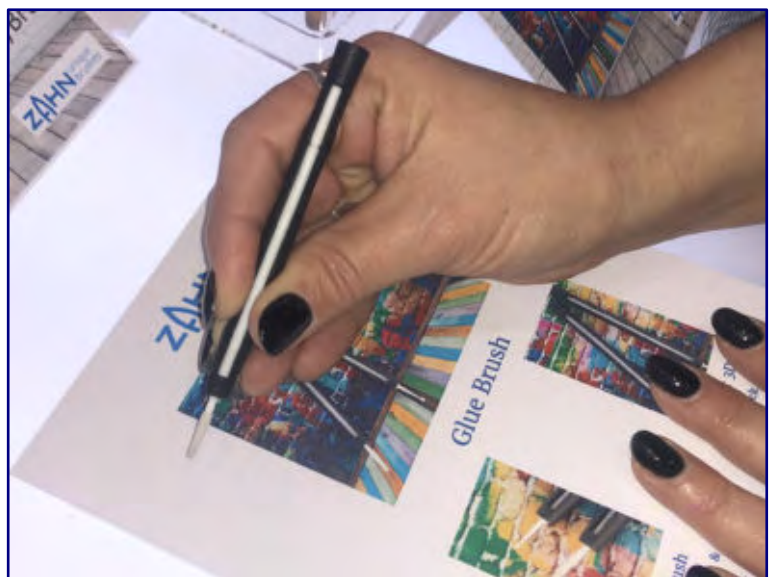
Various examples of the new brush products laid out in groups (from left to right): Holder 3D with nano brushes, weathering brushes, dry brushes for grading work, and ordinary dry brushes.

A small but very helpful tool is the “Holder 3D”, which the manufacturer presented to us at the Spielwarenmesse. It will not be presented via the company's own information channels until March 2024, which is why there is no information about it on the website yet.

The 3D-printed holder accommodates “microbrushes” from the dental sector, which are also widely used in modelling as the finest substitute for brushes and dot-gluing tools. However, due to the very thin handle, they sometimes cannot be guided precisely and securely enough for our applications.

The holder, into which the brush is inserted, gives this tool more grip and can be used for a wide range of delicate work without tiring too quickly. As Zahn Pinsel also manufactures products for dental applications, it made sense to introduce two versions of the microbrushes to this product segment as well.

The handle and nano brush with 3 and 13 mm microfiber tips are sold together under the names “Glue Brush” and “Nano Brush.” This is very interesting for model makers because, as medical products, the brushes are not usually sold directly to private users or require too large application quantities.



The “Holder 3D” handle for nano brushes, in particular, is always a useful tool for guiding the tiny devices for precision work with ease.

Corresponding consumables from the model making sector are therefore also offered by resellers.

www.zahn-pinsel.com

+++ ZDC Z Dream Collection +++

Michael Koppensteiner let us know that there is currently no time available to develop new models. He is hoping for summer 2024 and promised to actively inform our editorial team (and our readers) about this.

z-dream-collection.jimdosite.com

+++ Z-Doktor Modellbau +++

Björn Plutka is still active and supplies addresses for the products of NoBa-Modelle, for example, but also for kits from Archistories. For this reason, there are currently no new products of his own to report, as production is currently focused on design parts for Archistories new products, described under the client 1zu220-Shop.

www.z-doktor.de

+++ ZetNa220 +++

Further new products are also expected from Sicily, making it increasingly attractive to devote oneself to beautiful motifs from the land where the lemons blossom. This is primarily the FS type D.141 diesel-electric locomotive with a speed of 80 km/h in the prototype. The prototype was built from 1962 to 1964 and, like the German V 100 produced at around the same time, has a center cab.



The FS type D.141 diesel-electric locomotive is currently on the Zet-Na220 transfer list alongside a number of freight wagons. Photo: ZetNa220

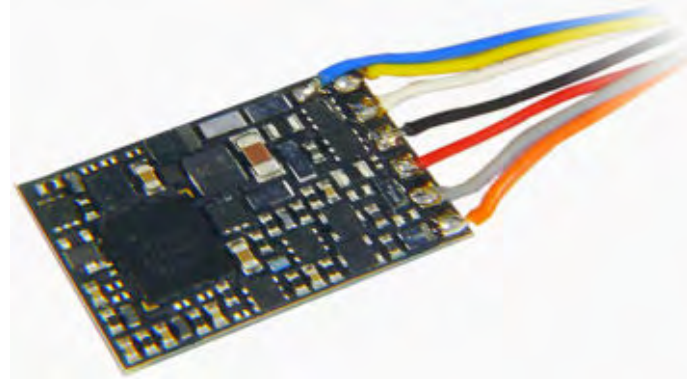
These are joined by various versions of older Tipo F and H pointed roof coaches, i.e. covered coaches and heat protection/refrigerated coaches. Also of interest is an older beer wagon of the brewery "zum Felsenkeller Zürich," which is also easily recognizable as being different from the German association designs.

<https://sites.google.com/view/zetna220>

+++ Zimo +++

Two new decoders have dimensions that are also interesting for Z gauge models: With different connection variants, this is primarily the MN160 flat decoder with four amplified function outputs and two logic level outputs. It is permanently fixed for 0.5 A total and motor current, with a peak current of up to 1 A. With dimensions of 13 x 7.5 x 1.6 mm, only the top side is equipped, while the underside is labelled. The MN250 decoder has been specially developed for low-voltage motors operating at 3 to 6 volts. As such motors are also used in small self-built vehicles, it can also play a role for the 1:220 scale, as it is very compact at 9.9 x 7.5 x 2.1 mm.

It also has four amplified function outputs and also offers an energy storage connection (5 V) for gold caps or tantalum capacitors. The motor current here may be 0.2 A in the long term.



Flat decoder MN160. Photo:Zimo

Two E24 decoders announced for the 2nd quarter of 2024, one with (MS540E24) and the other without sound (MN140E24), are also interesting in many respects due to the 28 connectors used (24 poles plus four guide pins also used). The installation dimensions also sound interesting, at 8.7 mm each and 2.3 and 2.8 mm thick, they reach a dimension that already causes problems on our scale.

If you see possible applications for your models, you will find all the necessary information on the equipment on the provider pages.

www.zimo.at

+++ Zmodell +++

A digital locomotive decoder plate for AZL models of the EMD SD40 diesel locomotive family is new. It can process the DCC, MM, Selectrix 1 and 2 protocols as well as analogue operation. The maximum input voltage may be 14 V, in analogue mode 12 V.

Two latest-generation current accumulators are installed on the circuit board to buffer current interruptions; special lighting functions for different lighting variants depending on the railway company can be configured via CV 33.

www.facebook.com/Zmodelltrains/
[zmodell\(at\)ukr.net](mailto:zmodell(at)ukr.net)

+++ Z-Otti +++

Hans-Jörg Ottinger has constructed a so-called Russian swing and is now offering it for sale. The model of this special form of a smaller, but faster-running, Ferris wheel is movable and motorised. It is available in the "Standard" and "Profi" versions.

While the former is intended more for decoration and only occasional movement, the Profi version is more elaborate: a brass shaft is used here for stability. A construction plan and the sketch for the Arduino are included for motorisation using an Arduino, in which the wheel starts up and brakes slowly and turns the other way round after a pause.



This is how a "Russian wheel" appears, realised on a scale of 1:220 by Hans-Jörg Ottinger. Photo: Z-Otti

Two LED outputs are programmed, one for when the wheel is turning and the other when the wheel is stationary. The carriage with the historic organ, the ticket office with cashier and the filigree fence are part of both sets.

Towards the end of the year, there will once again be a new new-silver set. It will be a multi-part set, but we won't reveal any more at this point.

zotti.lena-johansson.de/shop

+++ Z-Panzer (Andrew Hart) +++

Andrew Hart is still busy, but fully engaged in contract development and production for the Norwegian-American supplier AZL.

After consultation with him, he is allowed to share an exclusive model with us, which is only offered through American Z Line.

He designed and manufactured two new US armoured vehicles to equip its waggon model, which can be seen in the accompanying photos: the M60A3 and the M88A1.

www.z-panzer.com



Z-Panzer developed these two tank models for the model railway manufacturer AZL, so we will find them in their range. Photos: Z-Panzer

+++ 1zu220-Shop +++

As an in-house novelty, we would first like to mention a product that is not offered for sale individually, but is supplied as a free addition to the Märklin Club Model 2024 when it is ordered from this dealer.



This luggage trolley with suitcases is the free supplement to an Insider Club 2024 model ordered. Photo: 1zu220-Shop

The luggage trolley and the “suitcase” load always provide good service on the platform, but the suitcases can of course also be transferred to the roof rack of the Wismar rail bus and therefore also fit perfectly with the Märklin mould novelty.

The corner shop is a personal dream come true (not only) for Managing Director Jörg Erkel. The kit was designed and produced by Archistories, in four different colours. The once typical shop is attached to a small inn.

As the “Zum Alten Markt” inn (item no. 103231), it sets a varied tone with the unique colour scheme of the classic 'Greppiner Klinker' brick and at the same time continues the Dörppede house series. Under the name “Zur Alten Mühle” (104231), the

building appears with a Prussian-style solid brick façade and sandstone elements, which favours its use for northern to central German themes.

Variant 3 is called “Zum Alten Bahnhof” (105231) and features a simple, solid plaster façade that can be used faithfully in any region, while “Zur Alten Linde” (106231) has a completely unique, charming effect with a sophisticated combination of plaster and brick surfaces. All kits are made of solid-coloured hard cardboard and measure 71 × 41 × 53 cm (L × W × H).



The new kit for a corner shop appears as the inn “Zum Alten Markt” (item no. 103231; photo left) and “Zum Alten Bahnhof” (105231; photo right). Photos: Archistories

The following product description applies to all the versions mentioned: The simple design as a three-storey residential building with a pitched roof and trailing dormers as well as a reduced façade design with frieze motifs makes this kit universally applicable regardless of era and theme.

Trainini® *International Edition*

German Magazine for Z Gauge

On the ground floor there is a restaurant and a shop with a large display window, which can be furnished in a prototypical manner and also inconspicuously illuminated with the separately available equipment set from Z-Doktor Modellbau.

An attention getter can also be created in the hallway with the staircase included in the extension set, which is why the entrance door can optionally be displayed open.

www.1zu220-shop.de



This version is called "Zur Alten Linde" (106231) and completes the quartet together with the fourth version "Zur Alten Mühle" (104231; see title page). An interior design from Z-Doktor Modellbau (picture top right) will also be available separately.



WUNDERLAND

VOM KINDHEITSTRAUM ZUM WELTERFOLG

Miniaturland
Wunderland
HAMBURG

DIE GROSSE
KINO-DOKU

AB
7. MÄRZ
NUR IM KINO

EINE MAGISCHE REISE DURCH DAS
GRÖSSTE MINIATURLAND DER WELT

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Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Manufacturer's note from a reader:

I've been a Zetti for a long time and always enjoy reading your reports. (...) But I'm actually writing for something else today. I got to know JuNo-Modellbau. Jürgen, that's his name, sells 3D funfair functional models in N and, especially, Z scale. I would highly recommend him to you, he has a great repertoire. I have already bought several model kits from him and can only recommend him to others. He is definitely worth a report.

Sascha Lindner, Unna

Editor's reply: Dear Mr Lindner, we have already made contact and had a nice phone call. In fact, it is a manufacturer that we knew from its beginnings in 1999, a few years before this magazine was founded. In the meantime, he was no longer active for many years, which explains why we never got in touch. This has now been resolved and we are in dialogue. He can already be found in our extensive new products report in this issue.

Feedback on our editorial prices:

With Noch (for the Rokuhan brand) and Viessmann (for the Vollmer brand), we were able to personally congratulate two of the honoured manufacturers in Nuremberg on their new releases for 2023. In addition to the impressions recorded via video and published on our **Trainini TV** channel, we also received written feedback.

So the joy in Japan was over the awards, because this meant that in the opinion of our editorial team, and with it the readers, they had probably built the best powered Z gauge model of the past year.

In addition to the enthusiasm of all those involved in this project, this is also a great incentive for future designs. As the Shortys of the ICE 3, and the Glass Train have, apparently, also been well received in Europe, Rokuhan is now also considering turning to a German model again.

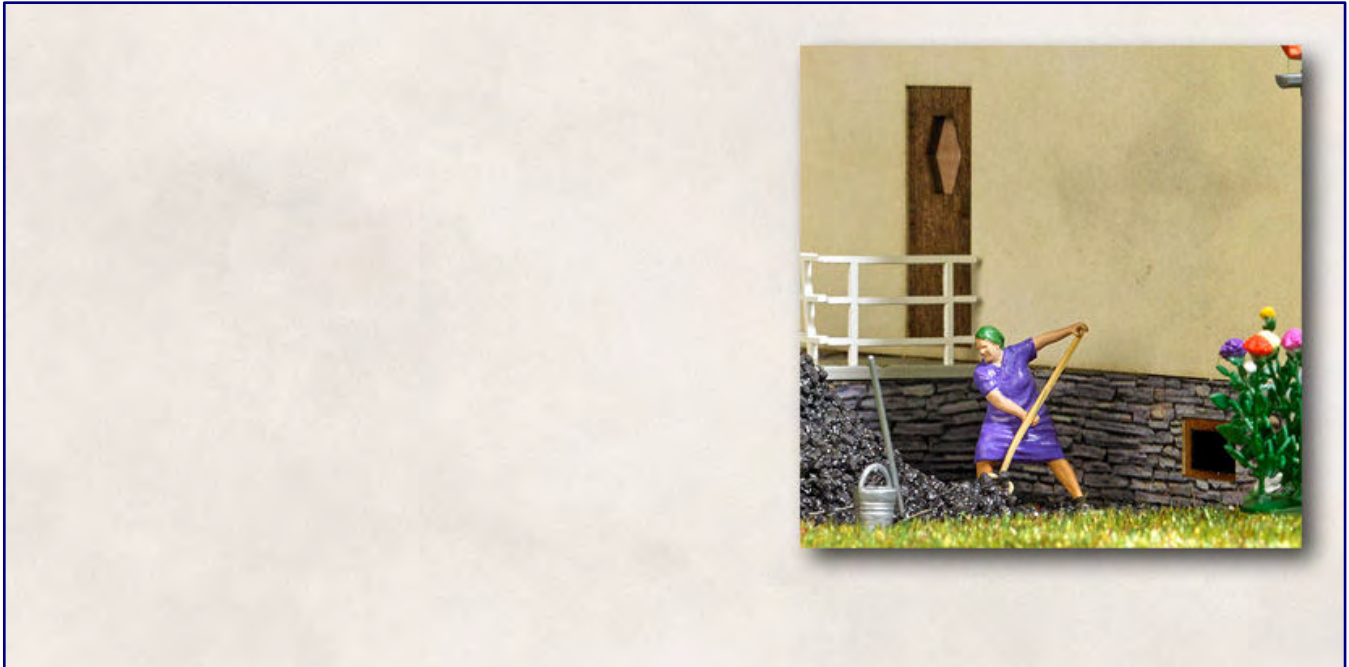


We also received direct feedback from Viessmann at the toy fair from Constanze Viessmann-Kato.

The award for the small workshop with the Polyplate material was also well received at Viessmann and was definitely seen as a seal of quality that they are happy to advertise. The pleasant exchange also produced new suggestions for possible future models, the internal test results of which we are eagerly awaiting.

Wall and decorative panels in matt look:

Busch has announced that the two decorative panels “Facade plaster white” (item no. 7444) and “Light grey, slightly weathered” (7445) from the 2023 novelty year will now be delivered shortly. They have a very realistic and matt surface that looks almost patinated.



The product photo shows the surface and an application example for the new decorative panel “Facade plaster light grey, slightly weathered” (art. no. 7445). Photo: Busch

The manufacturer also promises a special look and feel. With the 210 x 148 mm large and 0.6 mm thick panels, walls, buildings and house walls can be customised across all scales. They are also easy to work with and process.

Summary of business development at Märklin:

At the beginning of the toy fair, Märklin also traditionally comments on the current course of business. Florian Sieber, Managing Partner, summarises: “At 127 million euros, we have almost achieved our sales targets for the last financial year 2022/23. In the context of the difficult economic and political environment, we are very satisfied with this development.” The manufacturer expects turnover of 130.8 million euros for the new financial year.

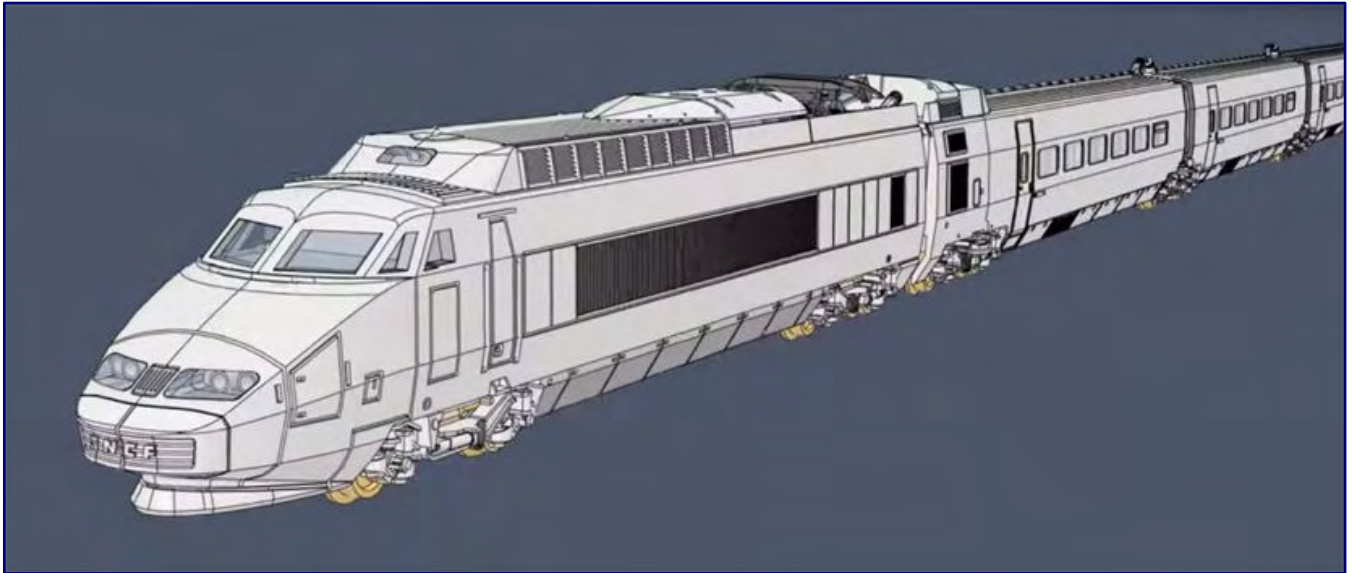
Success story - Azar Models financial target achieved:

Shortly before the editorial deadline and nine days before the end of the crowdfunding initiative on the Ulule platform, Azar Models reached its goal and secured funding for the 1:220 scale TGV. We are now excited to see what will be delivered in the following year, as both the hand sample shown two years ago in Altenbeken and all the CAD illustrations promise a very good and unusual model.

Before that, we received a call for competition that we would like to publish here: The Salon de Saint-Mandé and Azar Models have joined forces to offer a competition for modelling clubs to accompany the crowdfunding campaign.

Anyone who also agrees to take on the challenge planned by the Salon de Saint-Mandé and also prepare a scene module for the 2025 exhibition will be given an additional chance in the prize draw.

The following Z gauge prizes will be awarded to the clubs that win the competition: a complete TGV PSE collector's train in its world record special livery and an exclusive ride on the first pre-production model at the trade fair in Saint Mandé in January 2025 on the winning module (1st prize), a starter pack with the BB67400, three freight wagons, Zmaster speed controller and track oval (2nd prize), and a CC7200 locomotive model (3rd prize).



This CAD illustration is also intended to inspire enthusiasm for the announced SNCF TGV train, which will enhance Z gauge. Illustration: Azar Models

The registration form can be found at the following link and must be submitted by 29 February: https://docs.google.com/forms/d/e/1FAIpQLSdTiN8RWKAYgefsuqMjOWaZJ7S4MieMVUEze4I33Zj7Yq9MLQ/viewform?usp=sf_link.

The winners will be drawn immediately after the end of the funding campaign, i.e., on 1 March.

DVSI on the Nuremberg Toy Fair:

In a press release, the German Toy Industry Association draws a positive conclusion about the Spielwarenmesse, and the opening day for end customers in Hall 7A, where model making and model railways are located.

This was demonstrated by several thousand model railway enthusiasts from the DACH region, who took advantage of the organiser's exclusive offer to find out about the manufacturers' extensive new product ranges.

The upswing that the model railway hobby has experienced during the coronavirus pandemic is emphasised once again. According to the report, model railway enthusiasts have rediscovered or rediscovered the inherent potential of the hobby. The reason for this is simple.

With its offer of deceleration, stress reduction, meaningfulness and fun, the hobby provides an antidote to a world that is spinning ever faster and is characterised by multiple crises. Psychologists at Justus Liebig University Giessen (JLU) and Erasmus University Rotterdam, for example, found in a study conducted in 2020 that active leisure activities can reduce states of emotional exhaustion.

Noch Managing Director Sebastian Topp is also quoted in the report: He sees a trend among adults who are young at heart. This is not really new, because model railways have always been a generational offering. However, they are now openly embracing their hobby again.

Märklin deliveries in February:

Two new locomotive models reached the specialised dealers with the publication of the last issue. One of these is the E 41 374 of the Bundesbahn (Federal Railways) (item no. 88355) in its experimental paint scheme from around 1966. It is a model with five lamps, multiple nozzle fans, and an all-round rain gutter. It, therefore, belongs to Era III and technically corresponds to the two predecessor models, but has received minor changes in the area of the circuit board.

This model is also an excellently detailed locomotive with many attached parts, which is also suitable for reversible trains thanks to the tail lights. However, the two tail lights and lower headlights have been swapped and in this case shine from the wrong lanterns.



A splash of colour on the layout is E 41 374 of the German Federal Railways (item no. 88355), excellently implemented by Märklin. Unfortunately, this time the tail and headlights have been swapped in the lower lanterns.

The Siemens Vectron MS is also now available in its attractive BLS colour scheme as the Swiss series 475 (item no. 88236). The lime green on the silver-coloured locomotive body also suits the model extremely well. This fantastic impression is only spoiled by the roof garden with four single-arm pantographs that are clearly too large.

The next locomotive to be delivered in its entirety was the E 19 12 in blue livery with Bundesbahn inscriptions (item no. 88085). This model continues the series of museum locomotives and features a special paint scheme: E 19 12 was the first locomotive in its series to be painted blue, but had previously had its wine-red paint scheme renewed.

Looking at the b/w photos from that time, a mistake was made with the museum locomotive: although it was given a blue livery, for which it had been the pioneer, the white decorative lines with the cab designations V and H were applied to it, as had been the case in the last wine-red period.



The museum locomotive E 19 12 in blue livery (88085; photo left) and 143 326-7 from DB Gebrauchtzug (used locomotive) (88430; photo right) were delivered in full in February. Also, new on the market is the Pwgs 041 goods train baggage car (86061; photo below) for Era IV.

143 326-7 is one of the DB Gebrauchtzug (used locomotive) (8843) rental locomotives that attract attention with their special livery and clear advertising message. Here, the decorative stripes on the grey paintwork are somewhat reminiscent of the "White Lady", as the demonstration locomotive for the 212 / 243 series was called at the time. The 2023 exhibition model has now been delivered in full for last year's toy fair.



The third model from the current deliveries is the freight car Pwgs 041 (86061) without a caboose with lettering for Era IV. A striking feature of this model compared to its predecessor from the first delivery is a wooden gate printed on each side window.

The three replica boards are printed on the outside, which is also noticeable close up due to the printing over the frame. This is probably intended to represent protective strips that protect the inside of the windscreen against breakage when goods are placed on it.



Start of the trade fair in Mannheim:

From 8 to 10 March 2024, Faszination Modellbahn will once again take place in the Maimarkthalle Mannheim. This exhibition is the first chance for model railway enthusiasts to see the manufacturers' samples with their own eyes in addition to many spring innovations.

Z-Freunde International e.V. will once again be presenting a wide variety of exhibits and will be on hand to discuss technical issues and answer questions. You will find the necessary organiser information for your visit at <https://www.faszination-modellbahn.com>.

Imprint

ISSN 2512-8035

Bibliographic information of the German National Library: The German National Library lists this publication in the German National Bibliography. Detailed bibliographical data and editions can be found in the DNB catalogue at <https://portal.dnb.de>.

The publication of **Trainini German Magazine for Z Gauge** is voluntary and non-commercial. **Trainini German Magazine for Z Gauge** does not aim for any sources of income. This publication is governed exclusively by German law.

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International Edition

Free,
electronic magazine
for railroad enthusiasts
in the scale 1:220
and Prototype

www.trainini.eu

Published monthly
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ISSN 2512-8035

Trainini

German Magazine for Z Gauge



Beginner's Layout finally in Sight

**Magical Vacuum Dust Fairy
Optimised with little Effort**

Introduction

Dear Readers,

This March edition of **Trainini®** is another packed and interesting edition for you - I'm pretty sure of it!

This month, for the first time, I'm taking the opportunity to write an introduction. Writing a text is quite different from creating a video feature for **Trainini TV**. So, it is with great respect that I begin by addressing my words to you now.



Ralf Junius
Editor

Today, we are starting with the first article of our 2024/25 annual focus topic: "The path to the first layout" and it should benefit as many people as possible: Many of us keep seeing beautiful layouts at exhibitions and in articles in various trade magazines. Then the question inevitably comes to mind: Can I build something like that? As the saying goes, the sky is the limit!

And I can only confirm that. Over the past few years, my editorial colleague Dirk Kuhlmann has repeatedly reported on the construction of his segments, compact layouts, and dioramas. And now I would like to recognise myself as a great fan of his work. I particularly like the many details. I was able to pick up so many tips and tricks from the articles and then finally dared to build my first "Kistrath" display case myself.

During my visit to this year's Faszination Modellbahn in Mannheim, there was a layout competition. Unfortunately, no 1:220 scale layout took part, which I regret. Perhaps someone from our readership will be motivated and "empowered" by the new series of articles to build their first layout. And who knows what talent lies within you? Take the plunge and then show the results to the public.

My first work, which I was allowed to present in **Trainini®** 1/2020, will probably be on display in Altenbeken at the beginning of May. Everyone will then be able to see for himself how something presentable can be created in our gauge with simple means. You just must dare...

But prior to the big Z gauge weekend in Altenbeken on May 4 and 5, Intermodellbau is just around the corner in Dortmund from April 18 to 21. At the Spielwarenmesse in Nürnberg (Nuremberg) (International Toy Fair), Dr Rainer Noch and our editor-in-chief Holger Späing agreed to talk about new proposals for future 1:220 scale car models. There is still some time before this date, so the question is: Do you still have any suggestions? An interim report can be found in the news section of this edition.

And how many of you have already been to the cinemas to see the movie about the creation of Miniatur-Wunderland and its success story? We have also managed to include a small insight into the movie for you this month.

Enough said, I hope you have a great time with this edition of **Trainini®**!

Sin-Z-erely,

Ralf Junius

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Currently no items

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We thank Eisenbahnstiftung for their support with historical photographs.

Date of publication of the German language version of this issue: 31 March 2023

Cover photo:

This issue starts with the construction of our beginner's layout. The project should remain manageable, lead quickly to the goal and provide valuable experience. The class 361 shunting locomotive is already on a big test run with the Küpper anniversary wagon.

03 240 at full speed

Small Tender, big Ears

In Trainini® 9/2023, we presented a tender construction project that resulted from a body designed by NoBa-Modelle. With an adapted chassis, it has been providing new variety on our layout ever since. The designer was enthusiastic about our idea and has now added a matching chassis with movable bogies. We have, therefore, breathed even more life into our model today with two further refinements.

The class 03 light express locomotive was born out of necessity. Its big sister, the famous 01, had long been the DRG's first standardised steam locomotive for express train services, but many routes could not cope with its high axle load.

As the chronically cash-strapped Reichsbahn was unable to keep up with the expansion of the lines, a different solution had to be found: slimming down was now the order of the day. The running gear was given a lighter and less powerful boiler, but thanks to a largely identical running gear, it was able to maintain the required top speed of its big sister.



In April 1938, Carl Bellingrodt photographed the 03 240 (Osnabrück Hbf depot) with D 93 to Altona Hbf in Wuppertal-Unterbarmen near his home at Siegesstraße 94. The locomotive can be seen here with a tender 2'2' T 32 of the later 1926 design. Photo: RVM-Filmstelle Berlin (Bellingrodt), Eisenbahnstiftung.

Together, this combination resulted in a successful model, as the lighter 03 series, with 269 units, would eventually surpass the 01: The latter only managed 231 units, which were joined by the ten earlier zero-two units after a rebuild.

The Lightweight was not overly popular in heavy services, as a lack of steam accompanied it in its early years of service. It was not until after the Second World War that DB's optimisation of the induced draught system and use in more appropriate, lighter services resulted in a completely different picture.

The term "saloon steamer" gives the impression of travelling on a sofa, and it was actually coined by the staff who operated it. In fact, the smaller boiler was less coal-hungry and the express steam locomotive rarely ran at its performance limit, especially as it was forced out of high-quality services early on.

However, today we would like to look back at its early heyday on the German Federal Railway. It still had big ears, as the Wagner wind deflectors were sometimes called. Märklin's model has shown a three-light headlamp since 1972, which determines the period of use to around the end of the 1950s.



When 03 240 replenished her water supplies during a seven-minute stopover in Osnabrück on 17 May 1952, she had long been part of the Deutsche Bundesbahn's fleet and had already lost her smokebox central shutter. If you look closely, you can see that it now has a 2'2' T 32 tender of the 1925 design attached to it. The front water inlets used here are unusual for this tender. Incidentally, it was hauled by the F 4 "Mercur" on an unscheduled basis that day. Photo: Carl Bellingrodt, Eisenbahnstiftung

And since very successful passenger coaches for this era have recently been delivered again, the selected model (item no. 88855) is also highly topical again. It was produced soon after the 01 series was released, with which detailed control was introduced in Göttingen in 2012.

Märklin's oldest steam locomotive design also benefited from this progress in order to please not only the Insider Club members with this innovation, but also to maintain exclusivity. It was a stroke of luck for us, because this hastily procured model was a grateful photo subject.

The only thing that dampened our enthusiasm was the tender, which has also been dragged through the programme since 1972. It did not match many standard locomotives in the chosen lettering and after repairs to the injection moulds, it has long since ceased to inspire us either.

In our case, the seller also pointed out that the box contained a tender that was correct in shape but incorrectly labelled. That shouldn't stop us, because with a new locomotive labelling this is quickly rectified and it also offered the opportunity to attach a different tender.

NoBa-Modelle had sent us the upper part of the body, and the matching running gear was already under the supplied tender. We quickly found the right locomotive number for our project with 03 240, one of the youngest locomotives that was still running with a 2'2' T 32 (albeit, a newer design), at least in 1952.

While we described the customisation work for the tender in **Trainini®**, today we will briefly discuss the subsequent work on the locomotive, which will also take it a step forward. At the same time, our idea was also well provided to NoBa-Modelle, which will soon result in a new model with two variants. From then on, every Zettie will be able to convert their steam locomotive in this way.

More variety

When we published the report on our tender conversion, our basic model (Märklin 88855) still bore its factory markings as 03 140. Externally, the condition matched the chosen prototype well. The 03 240 had a very similar number, but came from a later construction lot.

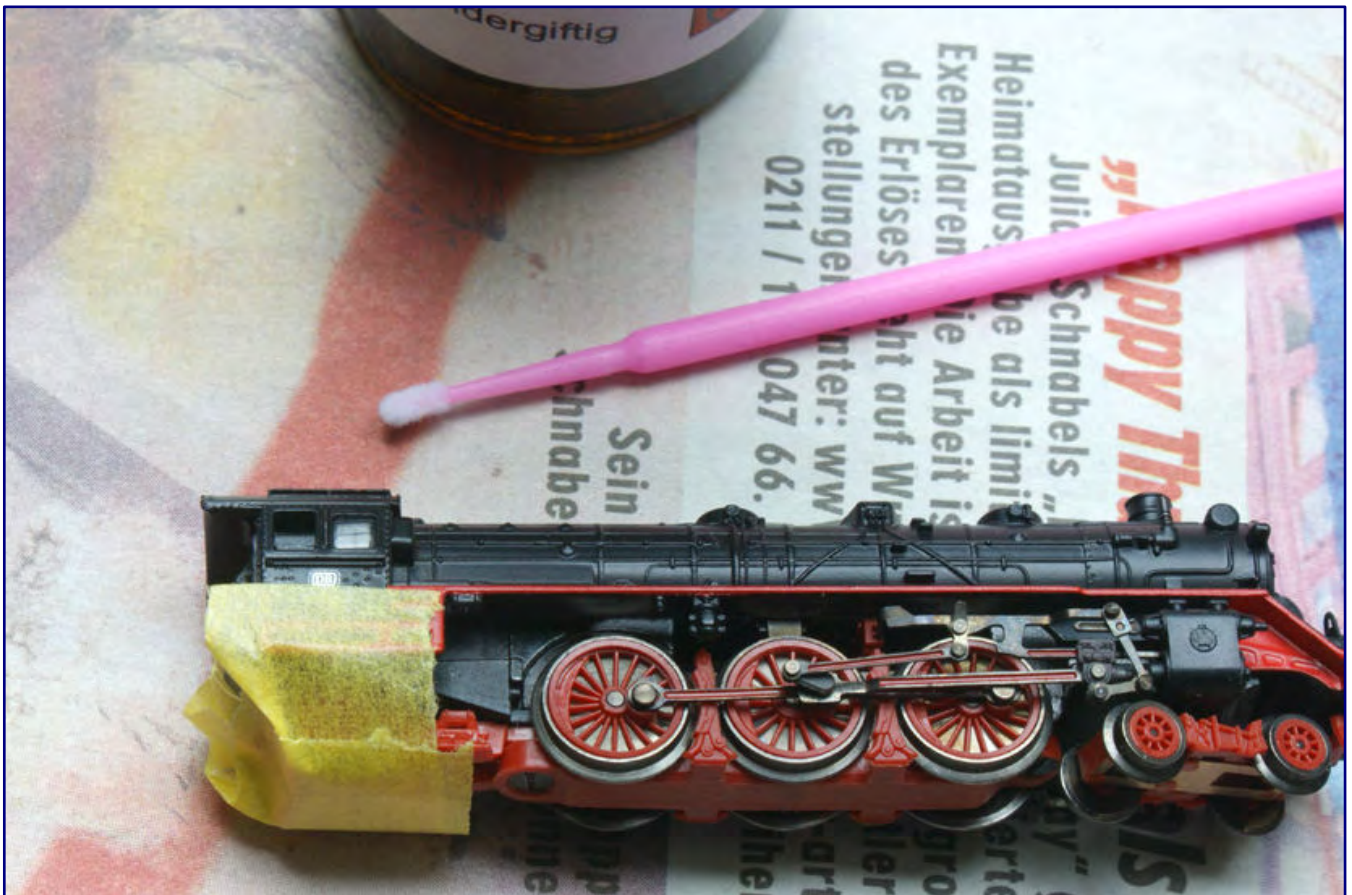
Its prototype was based at Osnabrück Hbf depot during the period in question and was also running with Wagner wind deflectors in the 1952 operating year. It also still had the smokebox central shutter, which Märklin had reproduced on the model from the beginning, in the first post-war years. The third headlight once had a different meaning, but can also be explained on the model.



This is what our model looked like at the end of our first report. The locomotive can also be improved with a few more details, which are the subject of today's report. And at the end, the tender can even be swapped again.

Although the tender design is almost, but not quite, exactly the same, the superstructure of NoBa models has one decisive feature in common with that of 03 240, which was not part of the standard equipment: it also had front water inlets, as proven by several Bellingrodt photos.

That's why it seemed to us to be the perfect prototype for the model, especially as the original must have visited the Emsland route more often, and this is a declared "hobbyhorse" of at least one member of the editorial team.



Before the new etched signs can be attached, the Ege biscuit must first be removed from the model, as the prototype had a narrower but wider sign with the lettering "Deutsche Bundesbahn".

So far, so good. In the meantime, the steam locomotive specialist Bahls Modelleisenbahnen produced the appropriate decals for the necessary re-labelling. This gave us the certainty that the font, size and material would be right to correctly reproduce the impression of the prototype signs.

Incidentally, in 1952 the Bundesbahn did not yet have its own logo and the Ege biscuit had not yet yet been designed. This is why the 03 240, which was classed as a high-quality locomotive and was still a long way from being ready for retirement, had ownership plates written on the side walls of the driver's cab. These are also special features that we certainly don't need to explain to Michael Bahls.

The old lettering could be removed with pad printing remover (unfortunately, no longer available), alternatively we recommend an electric eraser, and the etched labels were held in place with the help of a contact glue stick from Uhu (unfortunately, no longer available).

Björn Plutka drew our attention to one of his offers in good time: He has matching 3D-printed dome covers on offer to cover the housing screw shown above and also to improve the top view of the model a little more.

And so, our 03 240 also received this accessory (included in item no. ZD-220-20001-X) from Z-Doktor Modellbau. And this supplier also produced the white wet-slide decals for the locomotive after the lettering content had been reconstructed using prototype data and photographs.



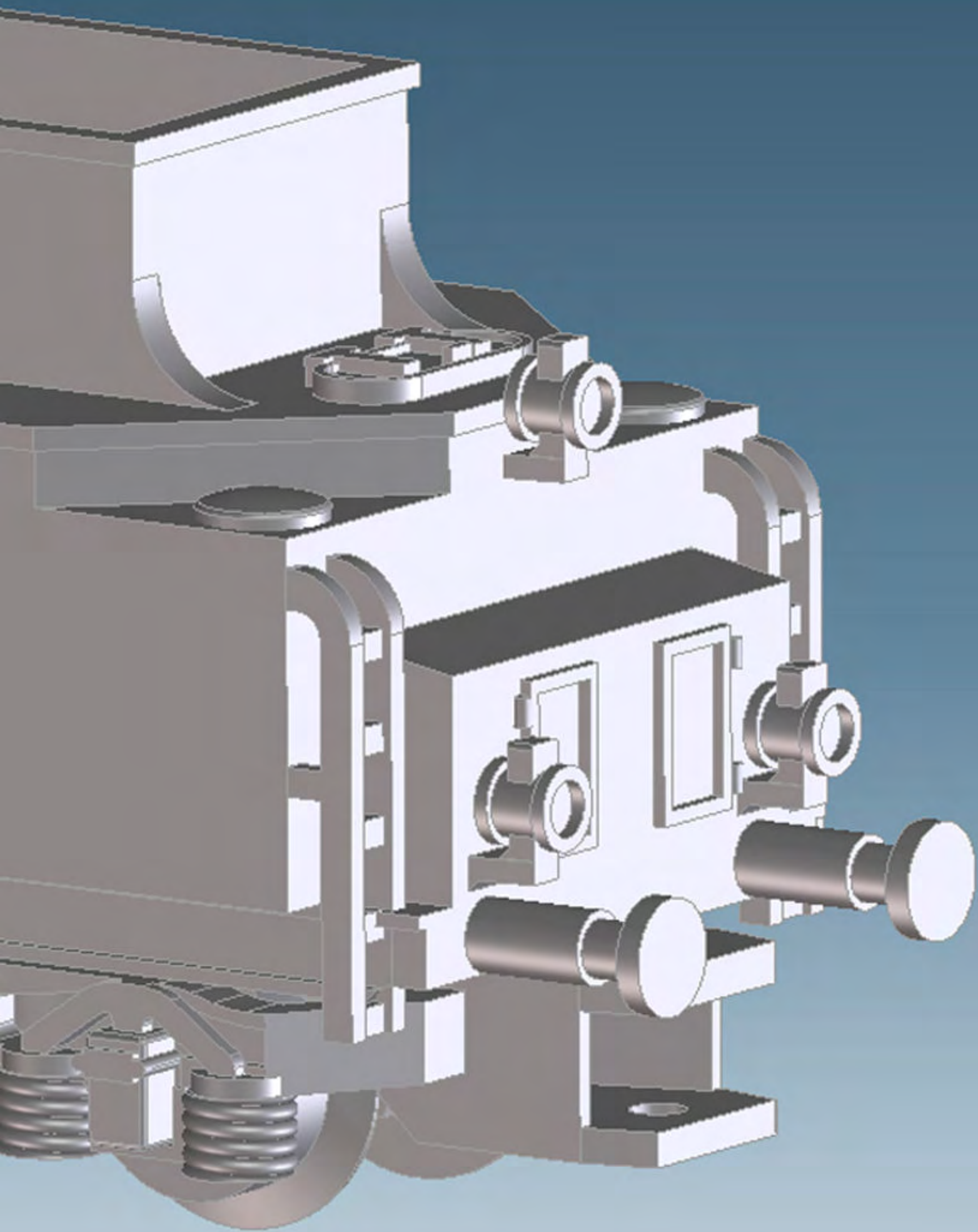
The dome covers from Z-Doktor Modellbau are a visual gain when looking at the model, thanks to which the observer is spared a look at the housing screw. And the Bahls' plates for the new locomotive number are already waiting to be attached.

Our customised express locomotive now appeared almost perfect. The manufacturer of the tender body had obviously not missed out on this. The part used for our conversion had its origins in the Ardelt crane train from NoBa-Modelle, but did not photo the model that was ultimately chosen.

This is precisely why this design has not been used in the in-house programme to date, which Norbert Heller wanted to change. So why not redesign the matching chassis and offer a swap tender directly to all interested parties?

At his request, our model went on a journey a few months ago in order to personally inspect the result and compare it with prototype drawings. At the same time, we weighed up whether to use a rigid bogie like Märklin's or functioning bogies.

continues on page 11





03 240 presents its new 2'2' T 32 tender from NoBa-Modelle before being trimmed, painted and labelled (photo above). This is how the soon to be purchased blank is presented to interested customers. The view of the smokebox with the new locomotive number set by steam locomotive specialist Michael Bahls shows how good the effect of etched signs is.

CAD illustration on page 9:

For the 03 series, NoBa-Modelle has created the tender version with electric lighting, recognisable by the third lantern that can be removed if required, but a version with a gas tank is also available. Illustration: NoBa-Modelle

The choice fell on bogies and thus a solution such as Märklin had used for the class 10 itself. Attachment to the locomotive is carried out in the same way, the rear of the tender offers the customer a standard coupling.

The plug-in boards of the upper section, which were still missing from our sample assembly, were also added. We still recommend using real coal for the contents of the coal box, as its effect is unsurpassable. A further modification will also soon be realised on our model.



After all the work has been completed, our 03 240 goes on a long journey with the new Märklin compartment cars and enhances the layout with a new tender variant.

The DRG drawings for the 2'2' T 32 tender still show a gas tank for the locomotive lighting on the rear side, resting on the tool box. However, this was not (or no longer) installed on the class 03, as electric lighting had been subsequently introduced.

NoBa-Modelle wants to offer this older version for other models, which can be restored to an earlier state in accordance with Era II. For the DB version of the 03 series with a turbo generator next to the chimney, however, the gas tank is also omitted in the model.

Just above its previous position, customers will then find a third headlight in the form of an attached lantern, which matches both later versions after 1958 and the condition of the Märklin base model shown.

This means that the conversion shown by us will soon no longer be necessary. The necessary parts can then be ordered as a blank with an inscription set or (probably) as a finished model to match the target model.

At the time of going to press, the item number for the tender was not yet available. Although it will soon be included in the programme, we cannot, yet, reliably say whether this new product will be available before the summer break.

However, if you would like to add a little more individuality to your steam locomotive stock, you should definitely make a note of this offer.

Basic model for our refinement:

<https://www.maerklin.de>

Supplier for the exchange and super parts:

<http://www.bahls-modelleisenbahnen.de>

<https://www.noba-modelle.de>

<https://www.z-doktor.de>

Easter greetings



The editorial team wishes all readers a happy Easter and relaxing holidays!

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The path to the first layout (Part 1)

It all starts with a Dream

Anyone who gets into the model railway hobby usually dreams of having their own layout on which the newly acquired vehicles can complete their laps and bring joy to their operator. But the way there needs to be considered, even if it doesn't have to be difficult. Our new series is intended to help you orientate yourself and realise your own wishes.

Our new annual focus topic 2024/25 kicks off today and with this article. We would like to address newcomers to Z gauge, in particular, and help them to successfully realise their desires for their first layout.

There are always several ways to reach a goal and this is exactly what this series of articles aims to emphasise. This fact also provides the reason why this thematic focus is planned for the first time over two years: As part of the series of contributions, two layouts are to be created that are as different, as possible, and thus address different aspects and requirements.



Our new beginners' series is designed to help you have fun quickly. The first of the two projects dealt with is dedicated to the much too often neglected topic of winter and is realised with the ballast track from Rokuhan.

One will take up a summer theme and be realised with Märklin material. Dirk Kuhlmann will be responsible for this and will tackle the construction together with new Zetties. The aim is to practise the art of layout construction as comprehensively as possible, also from a technical point of view.

The second layout we are starting with today takes a completely different approach: The basis is a prefabricated layout from Noch, which is to be fitted with ballasted tracks from Rokuhan. We have chosen

“St. Anton” (item no. 87015), a winter landscape in contrast to the second proposal, which will take shape at the same time.

The selected prefabricated terrain is made of textured hard foam and is painted. It contrasts pleasantly with the appearance of the deep-drawn layouts that were once exclusively used, as the rocks here also have prototypically angular structures and considerably surpass their historical predecessors in appearance.

If you want, you can quickly cover such a prefabricated site with track, connect them, insert buildings in the designated places to begin operations, and consider your first layout complete. But. that would be too easy for us.



If you want to own a ready-to-run layout quickly or even consider yourself to be less talented as a craftsman, the quickest and safest way to achieve your goal is with a prefabricated layout. We demonstrate this with "St. Anton" (item no. 87015) from Noch, prefabricated in PU hard foam and supplied with instructions, matching bridge parts, and a tunnel floor base.

We would like to use the good preparatory work that Noch does here ex-works to preserve the appearance in the areas that are to be individually added and to embed it in the overall scene. After all, the winter landscape is already hand-coloured in white and conveys freshly fallen snow that is still a far from melting away.

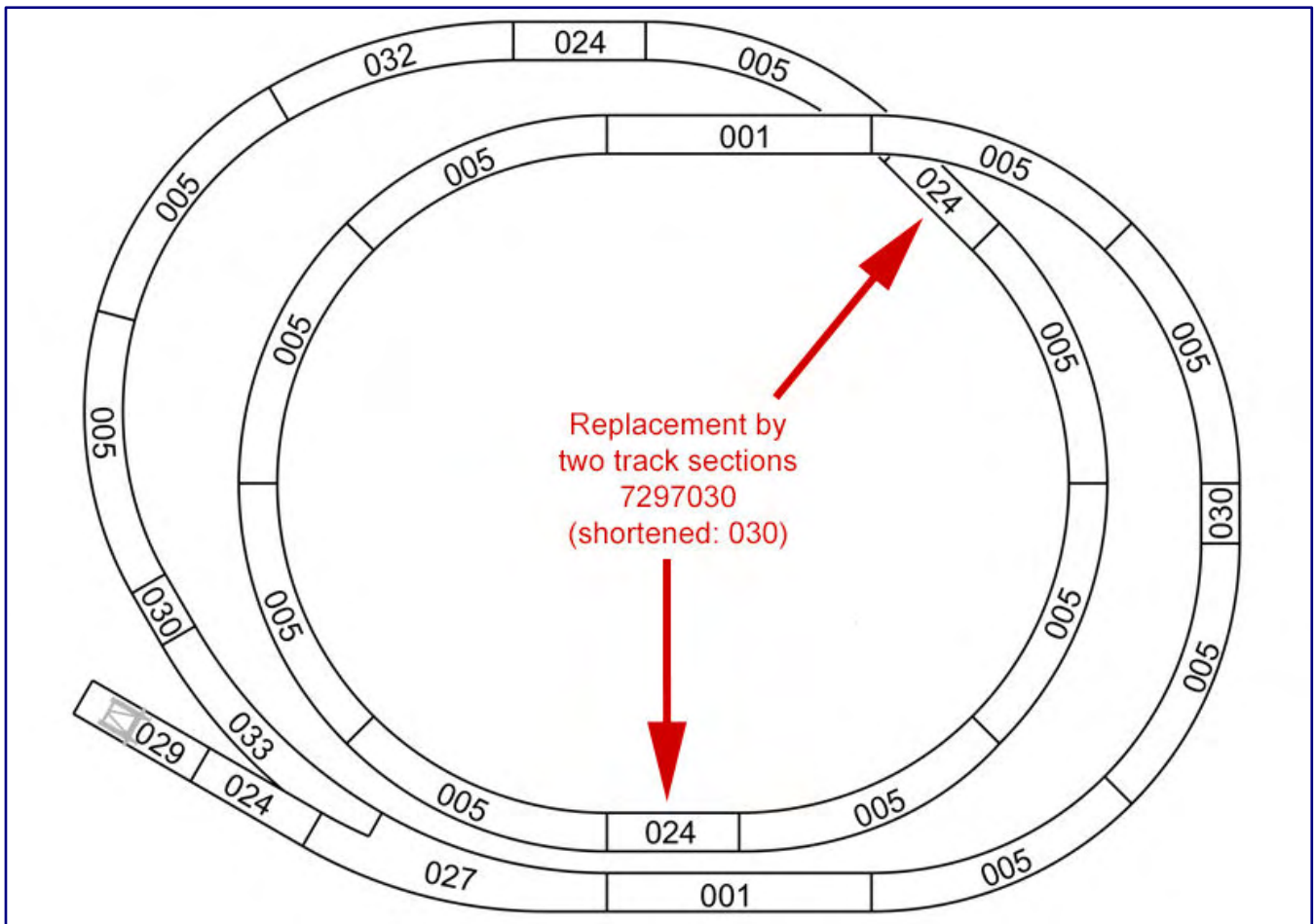
Roofs with no snow cover at all, open roads that show no signs of snow clearance, or even a railway track bed where not a single snowflake has settled cannot have a credible effect on the viewer. And the figures that will later bring life here should also be dressed appropriately for the time of year.

Hopefully, it is already clear at this point that every layout, whether freely planned or designed according to a template, requires a plan. This is the result of an idea that stems from the desire to have a separate outlet for rolling stock.

Small, but mighty

At least one theme must always be planned, an operating concept derived from this, a sensible track plan, and then the appropriate design items. Much of this is already predefined by the manufacturer because it is a prefabricated site. Individuality is therefore limited to designing according to your own ideas.

With edge dimensions of 53.5 x 40.5 cm, this is a very compact arrangement, which naturally does not leave much room for a large number of themes. Accordingly, Noch has specified a rural low mountain range motif, which envisages a small village or even just part of a village on a single-track branch line.



A track plan and the operating concept are already provided by Noch. However, the Rokuhan track plan reproduced here (as of May 2018) contains errors compared to that for the Märklin track, so the two marked ones should definitely be shortened. Track plan: Noch

There is even no need for a track plan in this case, because a track figure is fixed and cannot be changed. It was firmly planned by Noch, when the PU rigid foam part was designed in Wangen (Allgäu). Due to the tight radii, the supplier recommends not using coaches longer than 8.5 cm. This would not look good in the curves, but could also cause operational problems, if swivelling vehicle parts were to hit portals, bridges, or rocks.

The operating concept can be described in a single sentence: Round trips on an intertwined figure of eight with a single, short train set. The short siding is suitable at most for a two-axle goods wagon or a very short locomotive such as the V 60 or a Köf. As no points movements are possible, there are no shunting movements, and the track stub at the buffer stop is more of a decorative feature.

If you want to divide the rather short track section into blocks, you could possibly run two trains one behind the other. This would result in alternating train encounters on the two station tracks, but the traffic would not run smoothly either.

Noch's scope of delivery includes a bridge for the visible area, which spans a valley opening through which both the village road and the route of the inner circle run, as well as a cover plate to be glued into place for the tunnel section. The bridge consists of a straight ramp section (8976) and a sawn-off, curved ramp section (8977) from Märklin.



As the Rokuhan track material is only offered in fixed packaging units (PU), the model railway enthusiast will definitely retain some track pieces. Our compilation shows the purchase according to the Noch list: Only item 7297032, which is supplied with the curved turnout, has been omitted. Its shortened embankment cannot be seen in the tunnel anyway. The correction of the track plan, which also results in changes to the parts list, has not yet been taken into account at this point in the preparations!

The list of suggestions for buildings and accessories is also interesting: as you would expect, the specialist from Allgäu recommends its own design materials, and also its own figures. However, the tip on Faller and Vollmer kits is astonishing, as the company's own hard cardboard kits on identical layouts have long since made it independent of third-party ranges.

We will deliberately deviate from the tips here and use the contemporary Noch buildings, but will also take other suppliers into account when designing further. As already mentioned at the beginning, we have decided in favour of Rokuhan for the track material in this case.

As this manufacturer has followed the Märklin track geometry, there would be identical pieces of track with different numbers to choose from. However, the Rokuhan track plan contains some differences, as two 25 mm elements (Märklin) have been replaced several times by a 55 mm track section (Rokuhan).

Good advice for beginners
 If you want to tackle your first construction project, you are well advised to "bake small rolls" first. Even if your head is full of wishes for many operating options and plenty of functional accessories to be used, there is a cardinal error lurking here.
 Limiting yourself to a smaller system or an operating diorama with a manageable amount of time prevents frustration because the first operating experience is not too long in coming.
 In addition, it is not only completed more quickly, but also allows you to gain important experience in order to avoid basic beginner's mistakes in a large follow-up project.

We had to change this in two places in order to save the 5 mm excess length.

However, while Märklin tracks can be bought individually, the Japanese alternative only offers a choice of pre-packaged packs. This means that there are always pieces of track left over that can only be used in future projects.

Below we list the article numbers for the track (after the above-mentioned correction) and the required quantity.

It must then be compared with the respective packaging unit (PU) in order to acquire the required number of pieces, unless there are leftover pieces in your own stock, or they can be exchanged with model railway colleagues:

<u>Item no.</u>	<u>Description</u>	<u>No. of Pieces</u>	<u>No. of PU</u>
7297001	Straight track, 110 mm, with connection panel (4 pcs.)	2	1
7297005	Track curved R145-45° (4 pcs.)	14	4
7297024	Straight track, 55 mm, with connection panel (2 pcs.)	2	1
7297027	Curved turnout, right R220/195-30° (1 pc.)	1	1
7297029	European buffer stop, 42 mm (2 pcs.)	1	1
7297030	Straight track, 25 mm (4 pcs.)	6	2
7297032	Curved track R195-30° (6 pcs.)	1	0*
7297033	Curved track R220-30° (6 pcs.)	1	1

* The marked track section is required in a single quantity and is already included with the curved turnout as a levelling section (cut at the ballast); it does not need to be procured separately here.

For regulating train operation and switching the light in buildings, Noch recommends its own electronic controller kit (88163), which is manufactured to fit perfectly. It can be operated with a mains connection, or powered by a 9-volt block battery.

Its instructions also contain a description of how to install it. In conjunction with the separately available plug-in power supply unit (88171), this provides an ideal solution for a suitcase layout, as this is exactly what the prefabricated terrain we use is actually intended.

However, we are thinking of finding a different presentation and installation method, which we will not describe in detail today. In view of the planned use, the in-house controller has also been ruled out and we will have to think more about connection sockets at the installation points.

Test laying of the track

Before the first track laying test can occur, the supplied bridge must be inserted, aligned, and fixed in place with the hot glue gun according to the manufacturer's instructions. We attached it with track screws and did a good job, as we will see in a moment.

The individual track sections can then be gradually assembled according to the track plan in the instructions and laid out loosely on the layout. This is the only way to ensure that the specified routes match the track sections and to rule out errors due to incorrect swapping.



The one and a half ramp pieces included as bridge parts are from Märklin and have been designed for their track system. We first have to cut off the clamps for its sleeper strip for covering with Rokuhan bed tracks. The bridge is then painted completely in medium grey.

An error constellation actually comes to light in this step, which is not due to a swap, but is caused by an error in the track plan. We were able to correct this by replacing two pieces of track 7297024 (55 mm) with four pieces of track 7297030 (25 mm).

There is another point in the plan where the Märklin track plan has a 50 mm track length and the Rokuhan plan has 55 mm, but joining the tracks together does not result in any further tensions or even gaps.

The track figure is now correct and lies flat on the base of the layout as soon as the clamps for the Märklin track on the ramp sections are broken off with a pair of tweezers, because the ballast track is wider and rests on them.

The following extensive test runs will now check that the locomotive and wagons do not hit any rocks or the tunnel portals. Everything looks good for the most part, a railbus (Märklin 8831) runs its rounds without any problems and is the typical vehicle for the route shown here.

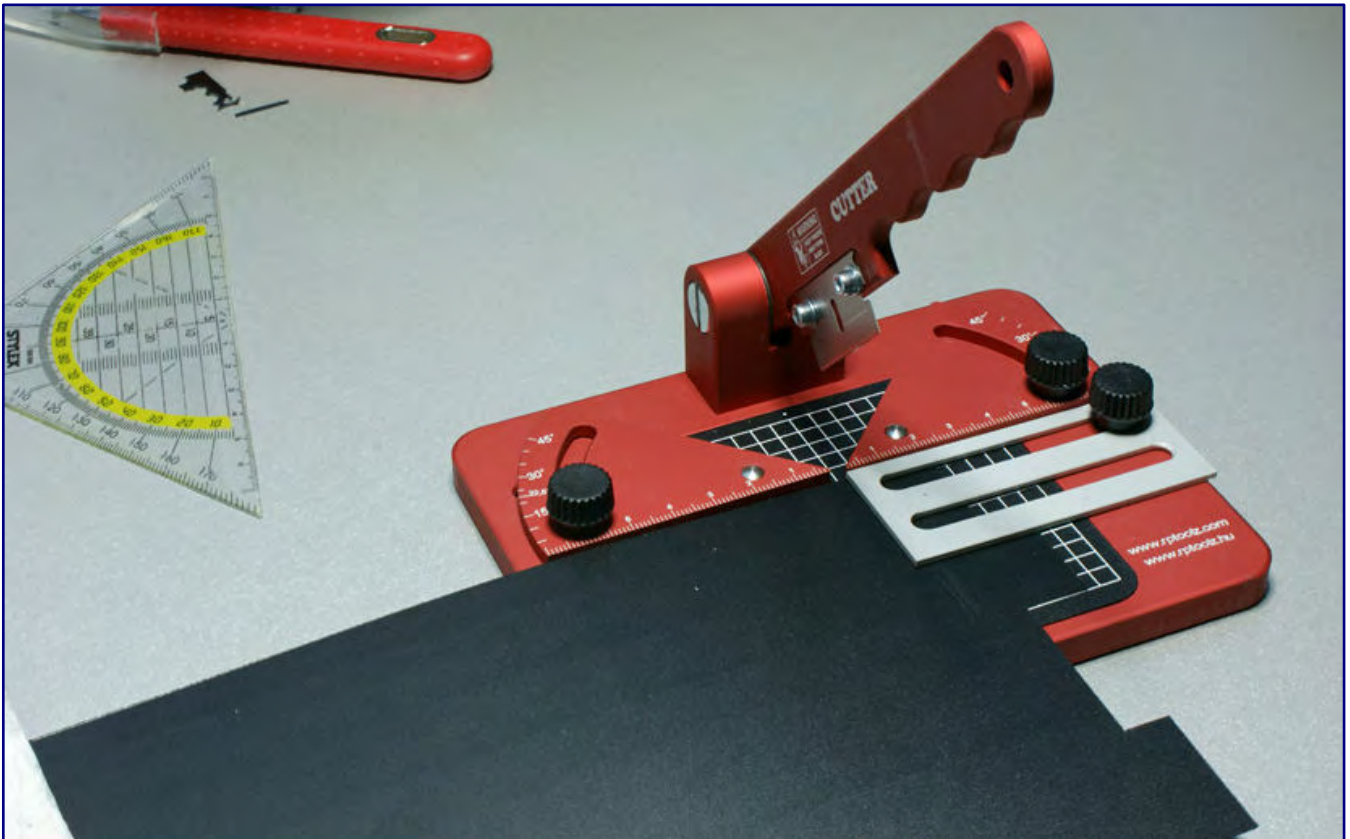
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The test runs show a need for adaptation: While a rail bus (photo below) passes through the track figure without any problems, the bridge clearance is not sufficient for the 361 series (photo above) and many other vehicle models. The bridge, therefore, has to be raised slightly, but this proves to be no problem.

But, of course, we also extend the tests to other, short rolling stock. Unfortunately, there are problems with a Rokuhan "Glass Train" shorty, as well as with a class 361 diesel shunting locomotive (Märklin 88641): Unfortunately, the height of the bridge passage is not sufficient for both.

The reason for this is the bedding of the Rokuhan track, which was not taken into account when this part was originally designed for Märklin parts. Consequently, we have to gain some height here for the descent.



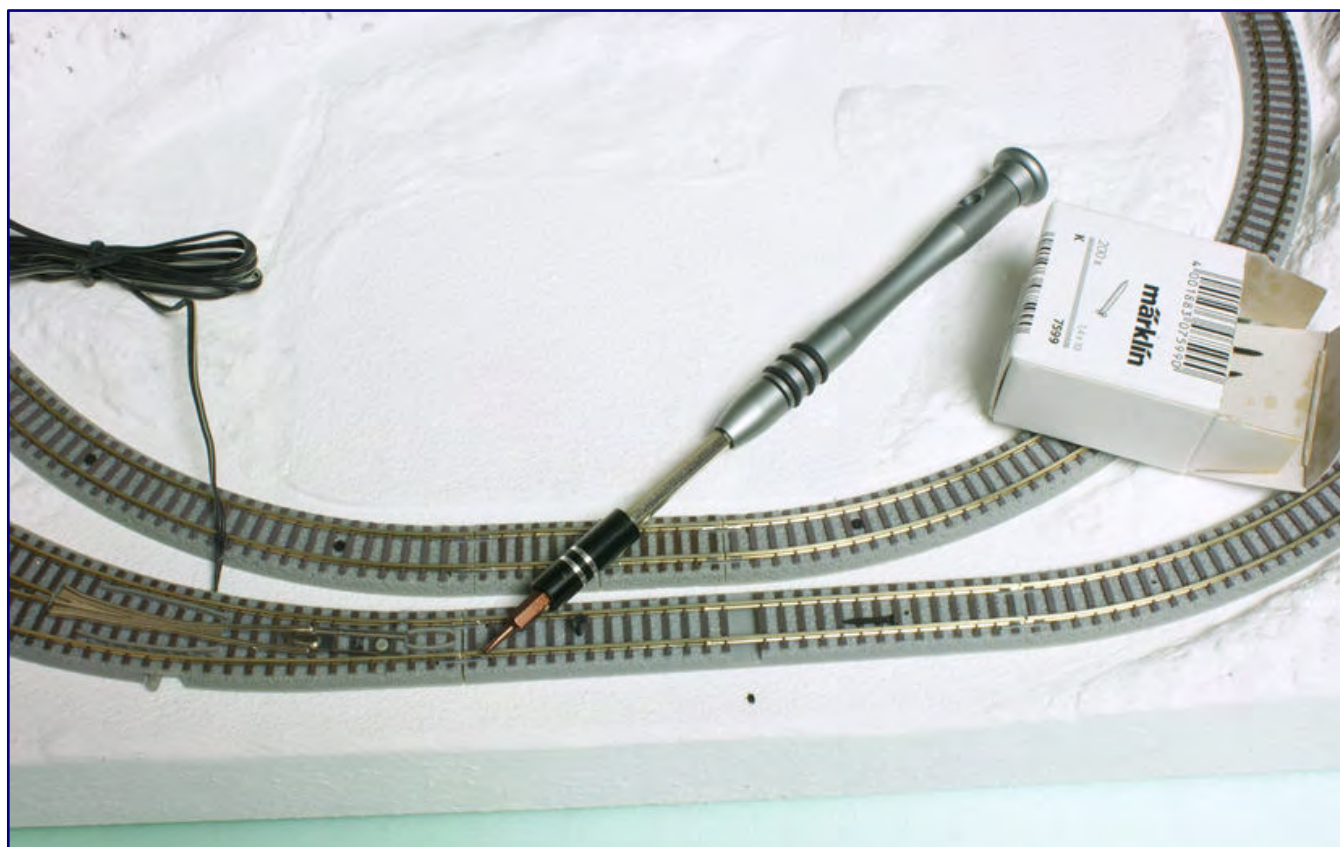
The mitre cutter tool (RP-CUTR) from RP Toolz helps us to produce several identically sized PS plate sections (Evergreen) to rest on the abutments of the bridge until a sufficient clearance height is achieved.

Now, it pays off that we have screwed the bridge on and can easily remove it again. Some material (polystyrene board from Evergreen; sold by Faller) is now added to the bridge piers and abutments until the missing millimetres are gained without making the track ascents too steep for short trains.

Compared to the manufacturer's instructions, we are now adding an important intermediate step at this point: We want to apply rust adhesions to the rail flanks in a prototypical manner. The easiest way to achieve this is with the detail markers from Noch (61155): The brown pigment pen is ideal for quickly and easily applying the desired traces of rust here.

There are certainly alternative methods and not everyone will like the strong brown colour, as the yellow components are often higher in the original. However, the colour impression also depends largely on the ambient light and the contrasting colours. We would therefore describe the solution chosen here as good and also suitable for beginners.

continues on page 24



Contrary to the instructions, we opted for a screw fastening for the bridge (photo above), for which we created a screw hole on the pillar using a pin block and a 1.2 mm drill. Märklin has provided a screw hole on the right. The track screws 7599 (1.4 x 10 mm), which also fit the Rokuhan track, are to be used there (photo below). However, this should also be pre-drilled from below with 1.2 mm at the selected screw positions.



You need to be quick with these two work steps: as soon as access to the track in the tunnel is no longer required, the lower edge of the bedding is coated with Uhu Por adhesive (top image). The cover plate on which the track is to be bonded is then inserted with hot-melt adhesive (bottom image).

It is important that two adjacent track sections are first joined together and then painted over at the connecting lugs. So, we work our way up to the goal piece by piece with several short sections of track.

The rails are then firmly connected to the substrate, initially in the critical areas where there could be bumps and which are no longer accessible later on. The final gaps are closed accordingly, where we can maintain access for a long time and no critical displacements can occur in the track layout.

Noch recommends small track screws for fixing Rokuhan tracks. In many situations, contact bonding should also be completely sufficient, but in view of the stresses we expect due to temperature fluctuations during transport, we would like to follow their advice.



With the detail markers from Noch (61155), the rail flanks can easily be given a rust patina, in the tunnel, of course, before closing the floor. However, as the tip of the pen is too thick for this, a micro brush from the dental sector helps us with the application. It is easy to guide thanks to the "Holder 3D" from Zahn Pinsel presented in the trade fair issue.

We have already used the suggested Märklin track screws (7599), Philips screws 1.4 x 10 mm with size 00, on the bridge, and, so, we are again following the recommendation here. The number purchased is completely sufficient anyway, because Märklin meant well with 200 pieces in the small box.

The installation of the track in the tunnel section deserves a final mention at this point: After the laying work, the track is still floating freely in the tunnel and is not yet load-bearing. The layout is now turned upside down, the track is already fixed and will not fall loose.

The tracks that run through the tunnel are now exposed in front of us. Solvent-free adhesive (Uhu Por) is now dabbed onto the edges of the track bed. Hot glue is then applied from below to the finished terrain at the points, where the track base (PS construction board supplied in the correct shape) is to be attached.

The track base is then glued in place and further fixed to the sides from above with hot glue. After these three steps, the finished terrain can be turned back, and laid down. It is important to think through the assembly steps described carefully beforehand, and then carry them out quickly without pausing to think.



After the steps described in this first part, all test drives with the models intended for presentation, run without any problems. The next part starts with the design of the landscape.

The aim is to ensure that the adhesive does not dry through on the ballast bedding replica, while the floor slab is still being installed. It must be ensured that it also forms a force-fit connection with the base plate (tunnel base), and also securely fixes the rails in the tunnel section.

Finally, we can now define the power feed points on the straight rails with removable sleeper strip element. They should be selected and aligned as inconspicuously, as possible. We then also drill holes for the connection cables to pass downwards at the specified points.

This marks the first step in the construction of our layout. We can now experience the fun of operation, and look forward to designing and equipping it, further. We will then start this process in the following part of our annual focus series.

Basic supplier of our project:

<https://www.noch.de>
<https://www.rokuhan.de>

Other providers mentioned:

<https://www.faller.de>
<https://www.maerklin.de>
<https://www.rptoolz.com>

<https://www.uhu.de>
<https://viessmann-modell.com>
<https://www.zahn-pinsel.com>

The magical dust fairy

Dusting up!

Dust is the enemy of model railways. Even Hamburg's Miniatur-Wunderland has integrated the time-consuming cleaning of its large exhibition layout into its regular work processes, as evidenced by the company's own reports. Dust accumulation can be avoided by covering the landscape, but in operation or even at exhibitions, our elaborately constructed model railways are defencelessly exposed to the falling suspended particles. So, dusting up is essential, but how?

It must have been more than thirty years since a tool called the "Dust Witch" made its name in the hobby. As a device that would be attached to a regular vacuum cleaner, it promised easy and accident-free cleaning of a model railway layout.

Long before the start of this millennium, however, the product seemed to have disappeared from the market. The once so effectively (and long before the multimedia age) advertised device was still well-known, but no longer available, anywhere. But it never seemed to be forgotten.



The Staubfee (Dust Fairy) 3.0 from Franke WF consists of these components, which we explore in this article.

Then, at the 2017 Nuremberg Toy Fair, Lux-Modellbau announced a relaunch of the tool under the name Staubhexe 2.17. In retrospect, we see the extension of the product name as a reference to February of the year of the relaunch, as this is exactly when the presentation took place.

Our editorial team also perceived a noticeable interest, which undoubtedly generated a high demand. Unfortunately, seven years have passed since then and despite earlier statements that the product would soon be delivered, things seemed to go nowhere.

With the outbreak of the coronavirus pandemic, things finally went quiet around Lux-Modellbau and its announcement. From our many conversations at trade fairs, it seems that hardly any readers still believe that it will be released, nor do we, by the way.



With its conical connector, the Staubfee fits on almost every end piece of any floor-running vacuum cleaner of all makes.

Consequently, interest in alternatives is high. These are available on various platforms with different names, production methods and sometimes at prices that are hard to swallow. But, eventually, we came across a comparable device that incorporated new ideas for increased performance and customised control.

This alternative is called Staubfee (Dust Fairy). And, we would like to introduce and explain this tool today, with the aim of demonstrating its practical application in a follow-up article.

Description of the device

The Staubfee from Franke WF is a low-cost blowing and suction attachment for commercially available vacuum cleaners, and, as such, similar to the now almost historical original "Staubhexe" device. It is manufactured through a 3D filament printing process, which allows customised production, in manageable quantities.

It can be almost completely disassembled, and its components are made of polylactides (PLA), also known colloquially as polylactic acids. This material is one of the most commonly used materials for FDM printing.



This image reveals the manufacturing technique chosen for the device, namely PLA 3D filament printing. This can be seen most easily in the centre section of the body, where the print layers are particularly visible on the inside of the connection. The view through the connection piece is in the direction of the front and smaller fan wheel, which is driven by the air flow of the vacuum cleaner.

PLA has low moisture absorption and flammability, but high UV resistance and colour fastness. Its weight-to-strength ratio is relatively high. On the other hand, it has low temperature resistance, as it is a thermoplastic material.

The manufacturer, therefore, warns against exposing the device to temperatures above 40° C, as, otherwise, deformation may occur. It should therefore be kept away from direct sunlight, especially during the summer months.

Saying that PLA is easy to process is almost self-evident, because otherwise it would hardly be an option for the 3D printing process.

However, we feel it is important to point out that the moving parts inside the appliance are mounted on ball bearings.

This makes for smooth running, increases efficiency, and reduces



Inside, two fan wheels are mounted on a common shaft – the photo shows the larger one, which is positioned at the rear. They generate counter-rotating currents. All moving parts are mounted on ball bearings.

wear. The device needs to be attached to the end of a vacuum cleaner hose and is, therefore, not a stand-alone tool, but an extension of an electrical appliance, that is available in every household.

Due to the manufacturing method, print layers are recognisable, and small filament threads can also adhere to parts before they are used for the first time. The fact that the surface is not as smooth as that of injection-moulded parts, is, therefore, not a quality defect, but a result inherent in the manufacturing process. It does not impair the function.



The suction flow passes the inner blower nozzle in the ring in the direction of the vacuum cleaner bag. Should a figure or model part be sucked up, the grid-shaped coarse filter should hold it back and make it easier to find again.

Although we are presenting this valuable tool today primarily as a helper for cleaning the delicate landscape of layouts, there are many other conceivable applications that may justify the purchase: A wide variety of models and model vehicles can of course also be cleaned of dust, and the same applies to clamp-on building blocks, lamps, books and even textiles. And there are surely even more uses.

Operating principle and effectiveness

Two fan wheels, which are mounted on a common shaft, generate two counter-rotating air movements inside the tool. The smaller fan wheel at the front is set in rotation by the vacuum generated by the vacuum cleaner.

Meanwhile, the simultaneously rotating, larger fan wheel at the rear end transports ambient air from the rotary vane to the front where it is ejected as a jet via the small internal nozzle. This blast stream picks up dust accumulations on the layout, which then enters the effective range of the surrounding, outer nozzle.

This is where the suction flow from the vacuum cleaner comes into action, by absorbing the dust swirling in the air and transporting it towards the bag. This enables good cleaning performance without contacting the layout, the most important argument in favour of this tool (as well as for the Dust Witch tool that has been known for decades).



A key feature is this rotary slider on the back of the device, which is used to set the blowing power: In the "Maximum" position shown here, the fan slots are fully open and the Staubfee draws in the highest possible volume of air. However, if they are closed, the device generates a suction flow only.

Simultaneously blowing away and sucking up the dust prevents small and delicate objects from being sucked in and damaged. It is therefore not only used for cleaning model railway scenery, but also for other household tasks, where these properties are desirable.

Thanks to its conically shaped connection, the Staubfee is not limited to a single brand or even type of vacuum cleaner. It proves to be compatible with almost every end piece of a vacuum cleaner hose. We have included this small caveat for the sake of caution, as we are not familiar with all household appliances used worldwide, and, therefore, cannot guarantee that for every single type of vacuum cleaner.



This is what the fully assembled third-generation Staubfee looks like: The outer nozzle (see photo on page 19) is now also fitted. The removable attachment for the inner nozzle, which can be seen here, provides a significant increase in performance compared to the previously available Dust Witch.

It should not go unmentioned that the Staubfee 3.0, as it is presented to us, is not a copy of the Staubhexe offered decades ago, whose relaunch many model railway enthusiasts have been waiting for in vain for seven years now.

Although the alternative in front of us works according to the same basic principle, it features many design solutions that have increased both performance and effectiveness. One example is the air ducts, which make it possible to direct the generated airflow more precisely and effectively onto the inner nozzle.

The Staubfee also has a rotary valve at the rear end that can be used to continuously adjust the blowing power. When fully closed, it only vacuums, while when fully open, the full blowing power is also emitted through the inner nozzle.

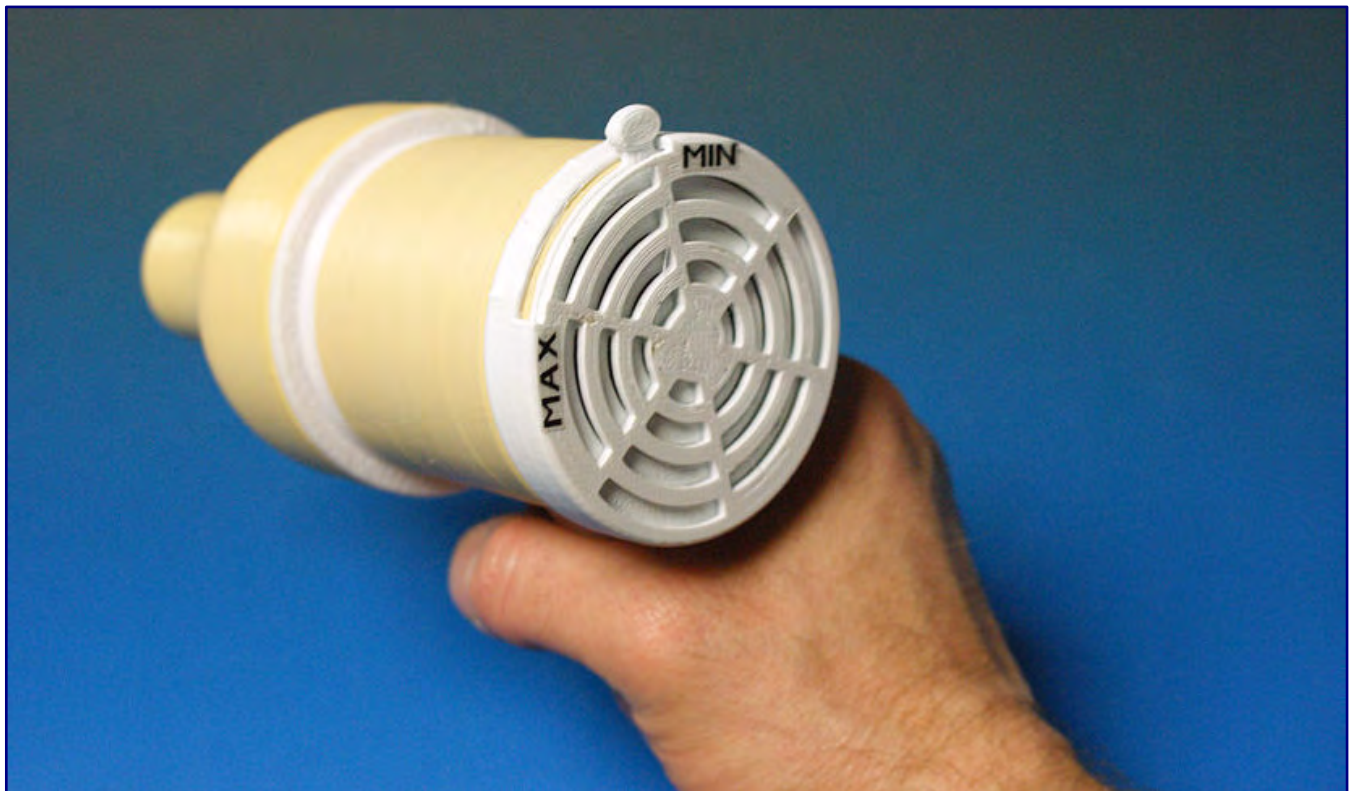
The blowing performance has been stabilised considerably by a rubber O-ring, which effectively seals both air flows against each other between the inner nozzle and the inner part of the suction pipe. The last optimisation, which is a feature of the version 3.0 we tested, concerns the large fan wheel of the pressure stage.

The number and orientation of the fan blades have been changed here, which has increased the blowing power even further. This is another major step forward, because without a powerful and focusable blowing jet, the attachment would just be an ordinary Hoover nozzle. The only difference to the attachments supplied with every vacuum cleaner would be the special shape and individual size.

Application and cleaning

Operating the Staubfee is very simple: after connecting it to the suction hose, it is held and guided with the favoured hand on the vacuum cleaner connection. The vacuum cleaner is switched on as usual and its suction power can be set as required.

With the other hand, the individual ratio of blowing to suction power can be changed using the rotary slide. The individual airflow setting depends on the object to be cleaned: fine objects such as figures should not be blown over or even sucked in, but the deposited, suspended, particles should effectively disappear from grass fibres and other green material, so as not to form a grey haze on them.



The dust fairy is held by its connection to the vacuum cleaner or its end piece during use. It then sits comfortably in the hand and can be easily guided over the sections of the layout that are to be cleaned. In this image it is set to pure suction power, as the rotary valve is closed.

As with any new device, it is advisable to familiarise yourself with this tool first so that you can correctly assess its performance and capabilities and put it to its best possible use. Then, everyone will quickly learn to appreciate it.

After use, it is safely stored in the dispatch box. If you want to save space and keep the container as small as possible, the Dust Fairy can be almost completely dismantled. The connecting piece (white) can be removed, as can the outer nozzle (beige). The supplied sealing cap (also white) fits onto the rotary valve at the rear end (white).

The most sensitive part is certainly the small extension (beige) for the white inner nozzle. Franke WF has deliberately refrained from permanently extending the inner nozzle, instead designing this part as a clip-on piece. This requires a little caution, but allows older versions to be retrofitted, and also allows a more diffuse, less focussed swirl jet, if the user so desires, on a case-by-case basis.

The part consisting of the inner nozzle and the air ducts can also be removed from the main body (beige) in the same way as the end cap with the rotary slide (both white). This is done relatively easy with the end cap, but the nozzle part sits very firmly on the main body with the Hoover nozzle.

We have, therefore, refrained from removing the nozzle for photos, and prefer to show its interior using the manufacturer's CAD illustrations. We did not want to risk damaging it, as disassembly, at this point, is not necessary, in principle.



The efficiency of this tool can hardly be observed at the time of use. However, when the outer nozzle is removed after just five minutes of trial use, the particle and dust ring on the coarse filter reveals the cleaning performance all the more clearly!

It is sufficient to remove the beige-coloured outer nozzle to reach it from the outside. Only the mesh screen at the base is relevant for our access, because the suction flow is guided through it (and further out past the air ducts) to the vacuum cleaner tube.

If a figurine or small part really does get sucked up, it will get stuck here and is easy to salvage. As soon as the job is finished, we take a look here and clean the grid sieve directly with the vacuum generated by the Hoover. This keeps the dust fairly clean and ready for future use.

If desired, the part with the inner nozzle and the end cap with the rotary valve to the main body can also be firmly and permanently connected. We do not consider this necessary, but it is up to the end user to decide. In this case, commercially available superglue would be the method of choice.

To conclude our product presentation, we have some good information for all those who are already familiar with previous versions of the device. Those who appreciate it may now want to upgrade versions 1 and 2 to the higher performance level.

Franke WF also offers owners the necessary retrofit parts to modify an existing appliance to the Staubfee 3.0 presented here. They can all be purchased, retroactively, as soon as the version of the basic appliance has been identified.



After a convincing test, the Staubfee 3.0 is set to be a sure bet for many practical uses that require the gentle removal of dust from layouts and dioramas without having to worry about the permanent loss of loose parts.

They are supplied together with illustrated instructions that explain the necessary disassembly and the insertion or replacement of parts. Defective parts can also be ordered as replacements. After all, model railway enthusiasts should be able to enjoy and successfully use this tool for years to come.

From now on, this group of people also includes us. And, because it has convinced us, we are nominating this new version 3.0 Staubfee for the 2024 best new release in the technology category.

Supplier contact:
[Franke.wf\(at\)web.de](mailto:Franke.wf(at)web.de)

Grenzenloser Modellbahnspaß in 1:220

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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Geschichten hinter dem Miniatur-Wunderland **Wenn Kinderträume wahr werden**

Die Modellbahn bietet selten Anlass für einen Kinobesuch. Doch wenn es um die Lebensgeschichte der Zwillingbrüder Frederik und Gerrit Braun mit ihrem Erfolgswerk Miniatur-Wunderland geht, dann sieht das schon völlig anders aus. Zwar spielt die Eisenbahn bei ihnen eher eine Nebenrolle, aber Emotionen und Leidenschaft kommen auch in Hamburg nicht zu kurz.

Sabine Howe (Regisseurin)
Wunderland
Vom Kindheitstraum zum Welterfolg

B|14 Film GmbH Produktion in Koproduktion mit Tobis
Berlin 2023

Bildformat 16:9
Tonformat 5.1 Digital
Sprache deutsch
Laufzeit 89 Min.

Seit 7. März 2024
in Ihrem Kino



2000 erwuchs aus einer spontanen Idee, geboren in Zürich, ein Projekt, das zur größten Modelleisenbahn der Welt werden sollte. 1,5 Millionen Menschen aus aller Welt besuchen heute jährlich das Miniatur-Wunderland in Hamburg und haben es zu Deutschlands beliebtester Touristenattraktion werden lassen.

Ganz offensichtlich sehnen sich nicht nur die Zwillinge Frederik und Gerrit Braun nach einer weitgehend heilen und funktionierenden Welt. Für viele Menschen ist ihre Schauanlage zu einem Sehnsuchtsort geworden, der verzaubert und es für einige Stunden ermöglicht, abzutauchen und eine andere Welt in ihren vielen Details zu entdecken.

Lebendig wird diese Welt neben den Personen, die sie ersinnen und gestalten, durch die vielen Figuren, die dort zu finden sind. Sie haben Kinder, tragen Mützen und Schuhe, sind verliebt oder singen vor sich hin. Sie lächeln und weinen; gleich, welchen Beruf sie auszuüben zu scheinen: Sie erzählen Geschichten, wie sie auch das echte Leben erzählt.

Diese Emotionen, die dort einfließen und die das Gezeigte ausstrahlt, machen den Unterschied zu anderen Anlagen, die öffentlich präsentiert werden. Das Miniatur-Wunderland ist nicht betriebswirtschaftlich durchgerechnet und auf Gewinnmaximierung ausgerichtet: Sein Erfolg erklärt sich durch den Fokus auf Leidenschaft und Gefühle.

Wer es nicht kennt, wird den Film leider nicht verstehen. Wer davon gehört hat und es besuchen möchte, wird schnell dem Zauber erliegen und die Reise endlich auf sich nehmen. Und wer es kennt wie auch schätzt, der wird nach diesem großartigen Werk schließlich verstehen, woraus die eigene Begeisterung erwächst.

Die Dokumentation erzählt die Geschichte hinter diesem Welterfolg. Es geht um Kindheitsträume und eine Kindheit, die alles andere als die heile Welt war und um zwei Lebenskünstler, die sich immer wieder neu erfinden. Zu Wort kommen ihre Weggefährten, Mitarbeiter und anderen, die für entscheidende Wegpunkte und Richtungsänderungen gesorgt haben.

Unter der Regie von Sabine Howe erzählen die beiden Brüder von ihren Erinnerungen und Erlebnissen. Private Filmaufnahmen bebildern die Kindheit und zeigen ihre frühe Faszination, Alltagssituationen als Modell nachzubilden.

Den Anlass für dieses Werk lieferte die vor sieben Jahren erschienene und von uns ebenfalls rezensierte Autobiographie der Zwillingbrüder, doch hier wurde keinesfalls das Buch verfilmt. Die Schwerpunkte und zum großen Teil auch Inhalte sind jetzt völlig anders und neu gesetzt worden.

Ihre Zeit als Diskothekenbetreiber wird beispielsweise nur kurz gestreift und ebenfalls mit privatem Filmmaterial ausgeschmückt. In deren Räumen fanden erste Vorstellungstermine mit Modellbauern statt, die dort Probearbeiten fertigten. Und auch der Weg ihres Halbbruders Sebastian Drechsler in die eigene Mannschaft erfolgte eher auf Umwegen.

Mehr als 300 Mitarbeitende zählt das Unternehmen heute und sie alle geben dem Gezeigten seinen ganz besonderen Charakter, der aus der Kreativität und Phantasie all dieser Menschen entsteht. Chefmodellbauer Gerhard Dauscher erläutert diesen Prozess sehr anschaulich.

Schicksalhaft war auch ein Treffen mit der Familie Martinez aus Argentinien. Der Hauptteil des Films dreht sich um dieses Treffen, eine Idee, der Skepsis von Gerhard Dauscher und dem Weg, wie dann doch gemeinsam der Bau des jüngsten Abschnitts Südamerika angegangen worden ist. Vieles an dieser Stelle ist für den Rezensenten sehr eindrücklich, konnte er doch auch schon mehrfach persönlich mit den Modellbaubegeisterten aus Buenos Aires sprechen.

Um zwischen Modellen und Menschen einen roten Faden herstellen und eine Chronologie schaffen zu können, mussten die kindlichen Zwillinge in ihre eigene Anlage eintauchen. Sie wurden dazu digital reproduziert, auf Aussehen und Größe der Wunderland-Bewohner gebracht und animiert zum Leben erweckt. So nehmen sie den Kinobesucher mit auf eine Reise durch ihre Welt.

Verblüffend genug scheint, dass eine solche Dokumentation den Sprung auf die Kinoleinwand schafft, werden solche Produktionen doch sonst eher für Plattformen wie Netflix umgesetzt. Aber auch dieses Phänomen zeigt, wie groß die Gemeinde der Miwula-Anhänger ist und welche Emotionen hier geweckt werden.

Spannend bleibt die Frage, wie viele Wochen sich der Film in den großen und kleinen Kinosälen des Lands halten können. Wir wünschen ihm viel Erfolg und freuen uns, wenn die Modelleisenbahn, die in Hamburg wie auch in diesem Film eher eine (wenn auch nicht unwichtige) Nebenrolle spielt, so für einen möglichst langen Zeitraum im Fokus der Öffentlichkeit bleibt und sie weiter salonfähig macht.

Doch auch im Falle, dass mit Erscheinen dieser Rezension kein Kinobesuch mehr möglich sein sollte, wird sie ihren Zweck erfüllen: Ein weiteres Verwerten des Materials auf den genannten Plattformen sollte sich für die Verleihgesellschaft gewiss lohnen, denn ein Erfolg war dieser Film schon jetzt, wenn die Erwartungen realistisch eingeschätzt wurden.

Uns wird der Streifen gewiss lange nicht aus dem Kopf gehen und bei jedem Besuch in Hamburg auf ein Neues lebendig werden. Und so wünschen wir uns auch, dass er vielleicht auch auf digitalen Datenträgern käuflich zu erwerben sein wird!

Information on film and cinemas:
<https://tobis.de>
<https://www.miniatur-wunderland.de/kinofilm/kinos/>

Offene Wagen nach 1945

Vom Zwei- zum Vierachser

180 Seiten Informationen zu offenen Wagen ab 1945, nicht nur eingeschränkt auf DB und DR, sind eine Menge Stoff. Zusammen mit der ersten Broschüre ergeben sie ein Buch von nicht gerade geringem Umfang. Was als Ergänzung eines Buchtitels gedacht war, erfuhr das Ausmaß eines Fortsetzungsbands. Wir haben uns das neueste Meisterwerk von Stefan Carstens angesehen.

Stefan Carstens
Offene Wagen (2)
Hintergrundinformationen, Modell-Bauanleitungen und Basteltipps

Modellbahn Union
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Auch die heute zu besprechende Broschüre baut auf Band 3 der Güterwagen-Reihe auf, die 1996 erstmals erschien. Nach fast 30 Jahren ist dieses Buch, mit dem Stefan Carstens seine Karriere als Deutschlands bester Güterwagenkenner fortzusetzen wusste, in vielen Punkten nicht mehr aktuell.

Das Ergänzen des darin niedergeschriebenen Wissens schien folglich überfällig. Das gilt sowohl für die Modellseite mit in der Zwischenzeit neu erschienenen Güterwagenmodellen als auch für die Vorbildinformationen, die seitdem zugänglich geworden sind und sich auswerten ließen.

So manche frühere Information konnte daher ergänzt werden oder stellte sich gar als falsch heraus und wird im vorliegenden Heft richtiggestellt. Wie wir es von den Autoren rund um Stefan Carstens gewohnt sind, werden auch in diesem Sonderheft wieder sowohl die Vorbilder als auch, stets im Folgekapitel platziert, die zugehörigen Modelle behandelt.

Leider, so möchten wir sagen, beschränken sich die Modellabhandlungen alle auf die Baugröße H0. Sämtliche anderen Spurweiten fanden hier keinen Eingang, was auch im Buch so der Fall war, das hier sinnvoll ergänzt wird.

Und doch gibt es auch in diesem Teil Neuerungen: Folgt den Modellbesprechungen und Verfeinerungsvorschlägen stets dem höchstmöglichen Anspruch, so zeigen sich alle Autoren inzwischen erheblich kompromissbereiter.

Genau das eröffnet dann auch einen neuen Leserkreis, denn die Mehrheit der Modellbahner wird mit ihren Käufen zufrieden sein und ist bereit zu Kompromissen. Nicht gern erfahren Personen aus dieser Käuferschicht viele Dinge, die nicht perfekt sind und sich vielleicht besser lösen ließen, denn vor allem auch der Preis ist als Entscheidungsfaktor nicht zu vergessen.

Und so galt es hier wohl, die oft gegensätzlichen Interessen des Hobbymodellbauers und der Perfektionisten in einer Weise zu vereinen, die niemanden zurücklässt. Diese Aufgabe halten wir für gut gelöst.

Die vorgestellten Modelle, die 1996 noch nicht am Markt verfügbar waren, werden wertschätzend und durchaus lobend vorgestellt. Verfeinerungsmöglichkeiten werden einzeln so aufgezeigt, dass es jedem leicht fallen wird, den individuellen Grad an Nacharbeiten am eigenen Können auszurichten und zu bestimmen.

Es sind sogar Modelle aufgeführt, bei denen sich mögliche Optimierungen wirklich auf ein Minimum beschränken, des Weiteren haben auch das Anbringen von Betriebsspuren und Schmutz eigene Modellartikel im Heft erfahren.

Dieser Teil des Hefts, der – ohne exakt nachgezählt zu haben – rund ein Drittel des Umfangs ausmachen dürfte, wird zweifelsfrei den Vertrieb des wichtigsten Handelspartners unterstützen, denn ein enger Bezug zu dessen Warenangebot ergibt sich hier schon zwangsläufig und in eben auch gelungener Weise. Doch wir müssen natürlich auch die Vorbildseite noch betrachten.

Wie es schon anklang, dreht sich in der zweiten Broschüre alles um die Entwicklung nach 1945: Die Spanne reicht von den ersten wiederaufgebauten Wagen bei DB und DR über die Entwicklung moderner Zweiachser mit stählernen Wagenkästen, die aus Umbau, Modernisierung oder komplettem Neubau hervorgingen, bis zu den vierachsigen Bauarten, die heute allein das Bild der Eisenbahn bestimmen.

Gut nachgezeichnet ist das Beharren der DB auf zweiachsigen Wagen, während ringsherum der Trend längst zu längeren Wagen auf Drehgestellen ging, und die Sonderrolle der DR, die aus Materialknappheit heraus den Sprung zum Vierachser vollziehen musste.

Dieses Phänomen ist durch statistische Daten, die Materialeinsatz und Nutzen in Relation setzen, sehr anschaulich vermittelt worden. Verwunderlich schien, dass die kürzeren Wagen in den achtziger Jahren noch mal eine Renaissance erlebten, die ebenfalls auf die besonderen Probleme von Planwirtschaft und realem Sozialismus zurückzuführen war.

Mit perfekt ausgewählten Fotografien und unterstützt durch ansprechende und anschauliche Zeichnungen werden alle Beschreibungen dokumentiert wie unterlegt, die tadellose Reproduktion der historischen Fotos tut dabei ihr übriges.

Stets richtet sich der Blick dabei auch über die Grenzen Deutschland hinweg, denn der von DB und SNCF gegründete Europ-Pool und die Standardisierungen der UIC stellen wichtige Faktoren für die Beschaffung von Wagen dar, die eben nicht nur bis zur Landesgrenze liefern.

Waren es anfangs noch im Ausland verbliebene Reichsbahn-Wagen, die bei den einstigen Kriegsgegnern eine neue Nutzung erfuhren und landestypisch angepasst wurden, nahm ein spannendes Thema schnell an Fahrt auf.

Bei allen Gemeinsamkeiten der UIC-Standardwagen beleuchten die Autoren auch die verwaltungstypischen Merkmale, die sie unterschieden oder die nach Verkäufen herbeigeführt wurden. Dies erwähnen wir deshalb so sehr, weil sich hier wieder eine Brücke zu den Modellen auftut.

Auch, wenn in der Broschüre nur die Nenngröße H0 behandelt wird, so liefern die Berichte und Aufnahmen viele Ideen und Anregungen für Projekte im eigenen Maßstab der Wahl. Und so wird diese Lektüre noch lange Zeit nachwirken.

• Publishing pages and purchase:
• <https://www.stefancarstens.de>
• <https://www.modellbahnunion.com>

Report from the Faszination Modellbahn

On a Day Trip to Mannheim

Faszination Modellbahn in Mannheim from 8 to 10 March 2024 was the first public exhibition after the presentation of new products in Nürnberg (Nuremberg). And although there was an opening day for end customers there for the second time on Saturday, on which Hall 7A (model railway and construction) was open to everyone, some major manufacturers preferred to use the event in Mannheim. Almost 15,000 visitors responded to their call.

By Ralf Junius. After another token strike at Deutsche Bahn ended on Friday afternoon, nothing was standing in the way of my day trip to Mannheim. So, on Saturday, 9 March, I set off south from Düsseldorf at around 6.30 am.

After just over two hours, I reached my destination and was able to join the waiting line at the entrance to the Maimarkt site shortly before 9:00 am. Spread over three days of the event, almost 15,000 other people did the same. After only a short wait, I finally entered the exhibition hall.



The first place to go at the fair was the Märklin booth. Among other things, the V 125 diesel locomotive of the Württembergische Eisenbahn-Gesellschaft (item no. 88211) with the two mushroom shaped vents on the long engine hood could be seen there.

I immediately noticed the very wide aisles. My last visit to this trade fair was before the coronavirus pandemic and I remembered a different view from that time. Saturday remained pleasant throughout the day, despite the large number of visitors. Only in front of the layout and sales booths did it get a little crowded, otherwise, there was always enough space to walk from booth to booth.



An unpainted hand sample of the V 90, we were already able to show a painted and labelled one in our report from Nuremberg, allowed us to study many of the details reproduced by Märklin.

Manufacturers, dealers, and layouts of all gauges were spread throughout the hall. As a Zettie, I was first drawn to the Märklin booth to see the new 2024 models in person. In addition to the insider model “Schweineschnäuzchen” (piggy’s snout) VT 88⁹, samples of the V 125, V 90 and the class 103 for the EC 63 “Mozart” were also presented there. In addition to the new products, the company’s own exhibition layout was of course also on display.

Several booths offered hands-on activities to get the next generation interested in our wonderful hobby. From the play mat and the autonomous control of a train to the tinkering workshop and the opportunity to build a house or a mini diorama in the BDEF’s “Junior College Europe,” these offers were very popular.

Many different scales were represented among the exhibited layouts. The Miba personal layout competition also took place again as part of the exhibition. This was won by Jonas Sommer with his layout “Kent’s Mainline in Miniature.” By the way, not a single Z-scale layout competed here.

The Z-Freunde International booth was a centre of attraction for all fans of our scale. Various exhibits were presented there: The Z-Stammtisch Rhein-Neckar had set up a track for long trains with the motto “Kringel um die Großen” (“Curls around the big ones”), and had train compositions based on original trains running on the track and circling around an H0-gauge train.

Adjacent to this, the Radolfzell Z gauge regulars’ table exhibited its regulars’ table boards. A picture frame with a commuting rail bus was also on display. Thomas Heß showed on Klapplingen that digital technologies are also possible for Zetties.

continues on page 43



Not on the winners' podium, but still very worth seeing: "Holzwarenfabrikation Bernhard Brettl" (wooden products fabrication) by Iлона Rüger (top photo) and (below) "The Gnat Shack IV – Schotterwerke" (gravel factories) by Heiko Wendler (bottom photo).

And things are also continuing digitally: Claudius Veit (Velmo) presented his chemical plant in very diversified operation, which also includes moving road traffic. New from the Velmo workshop is a digital decoder for the latest version of the E 41, which is now about to go into production.



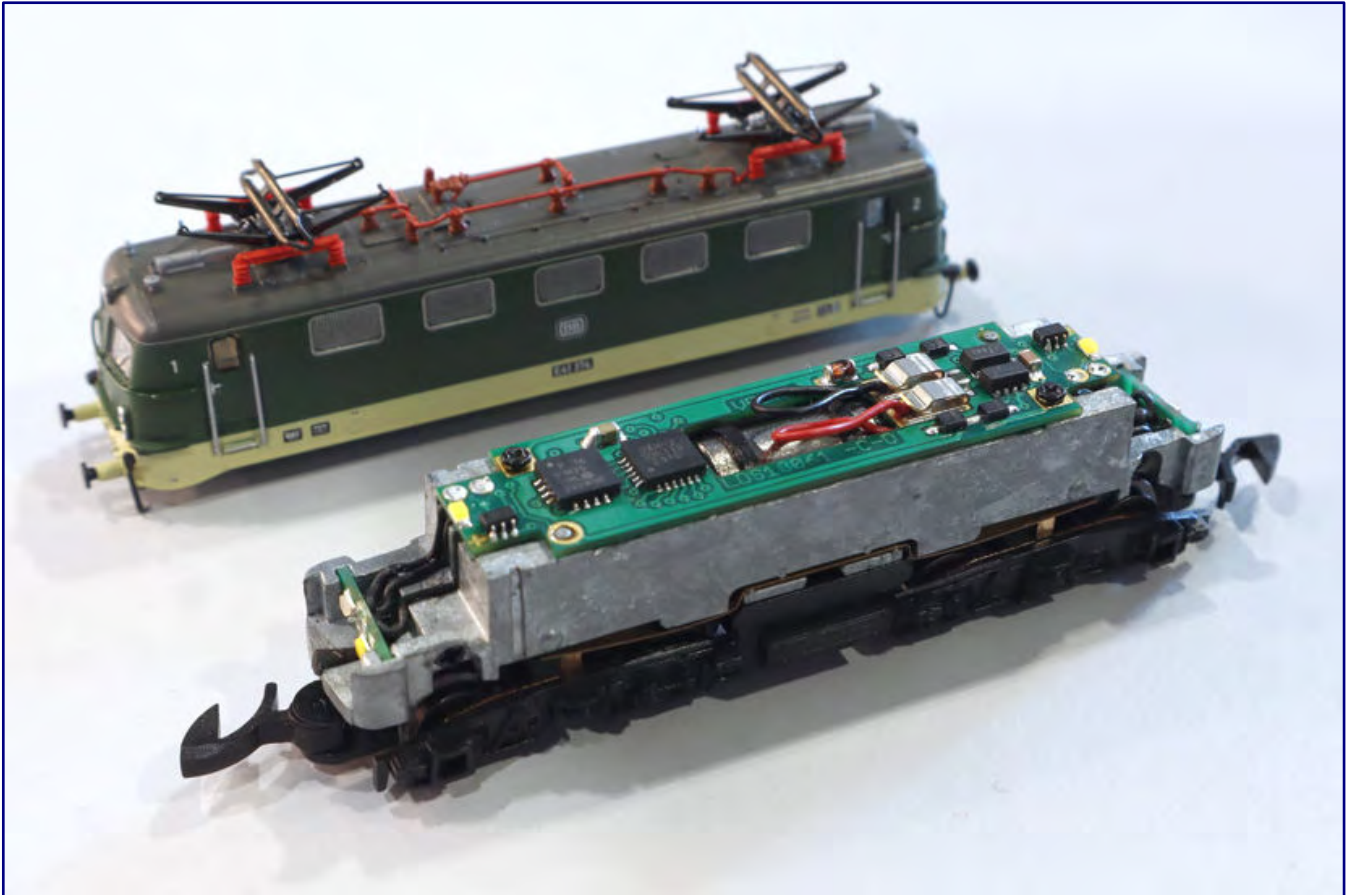
In the picture frame of the Radolfzell Z gauge club, a rail bus commuted back and forth before viewers.

This is already the third version for the model series, as Märklin has so far made modifications to this locomotive with each new model, which then also resulted in an adaptation of the decoder. The new model presented in Mannheim also corrects the swapped tail and lower headlights (see brief review in the **Trainini®** 2/2024 news).

A lot of space was given to Z-Lights, where Torsten Schubert presented his LED models and soldering demonstrations. Thanks to Oliver Kessler's Z-Car system, moving vehicles on a scale of 1:220 have been a reality for seven years now. At Faszination Modellbahn, he presented a new chassis, for which he used a 1:160 scale VW bus as a demonstration model to show visitors how to use it on the next larger scale.

The Z-Freunde International contributed a display case with several models and their annual and special wagons in their booth. Roland Kimmich and Christine Specht were also represented at their booth with dioramas on which they exhibited Railex classics.

Several miniature layouts were set up by the Stammtisch Bayern. On two of them, visitors were able to start the train at the push of a button, in other words, set the signal to Hp 1. Ulrich Günther entertained the audience with his shunting layout, on which everyone was allowed to assemble a train themselves.



Claudius Veit presented a sample of a new Velmo decoder for the E 41 in Mannheim. After Märklin made changes to the chassis and circuit board mounting for this latest edition of the locomotive, a further decoder variant was required, which will now soon go into production.

Gotthard Schmidt and Manfred Forst concluded the Z gauge area with their layouts. A nostalgic layout depicting the eighties was on show by Gotthard Schmidt. Manfred Forst presented his Hunt's large coaling plant in his "coal box" and "Animalistics in Forstberg".



Photo right:
The chemical plant had been heavily upgraded. New Ks signals were also installed, which today combine the functions of the distant and main signals. Two specimens can be seen here.

The day passed far too quickly to see and experience everything on site. After many intensive and as well interesting conversations, I returned home in late afternoon.



There is a lack of space near the factory entrance to the chemical plant, so a small signal bridge was installed here to mount the Ks signal with speed indicator.

Although not all the major manufacturers were represented, and especially not any small series manufacturers for Z gauge, it was still a nice and worthwhile visit for me. The electronics and digital suppliers not explicitly mentioned here and the specialised publishers, who should not be forgotten, certainly contributed to this.

Exhibitor selection with Z gauge relevance:

- <https://amw.huebsch.at/>
- <https://artitec.nl>
- <https://bdef.de>

- <https://www.busch-model.com>
- <https://shopware.donau-elektronik.de>
- <https://www.eisenbahn-kurier.de>

- <https://www.esu.eu>
- <https://www.hack-bruecken.de>
- <https://www.herpa.de>

- <https://www.mazero.de>
- <http://www.peter-post-wekzeuge.de>
- <https://tams-online.de>

- <https://www.uhlenbrock.de>
- <https://velmo.de>
- <https://viessmann-modell.com>

- <https://www.vgbahn.shop>
- <https://www.z-freunde-international.de>
- <https://www.zimo.at>

- <http://www.z-lights.de>
- <https://z-stammtisch-bayern.de>

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Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Questions about signals in the model:

Is there a description of where to place which signal?
The signals in question are 8939, 89390, 89391, 89392, 89393, and 89394.

For example, the light signals 8939 and 89391 are identical. The 8939 is described as a main signal and the 89391 as a block signal. I don't want to describe all the possibilities in detail, but the most important ones should be correct.

Thomas Lindemann, Berlin

Editor's reply: Item numbers 8939 and 89391 are models of different generations for the same purpose and replicate block signals. They belong to the main signals. If we exclude the articles 89390 (leading light signal) and 89394 (light blocking signal), then your list contains further main light signals in the forms of entry (89392) and exit signals (89393). These are used to protect railway stations.

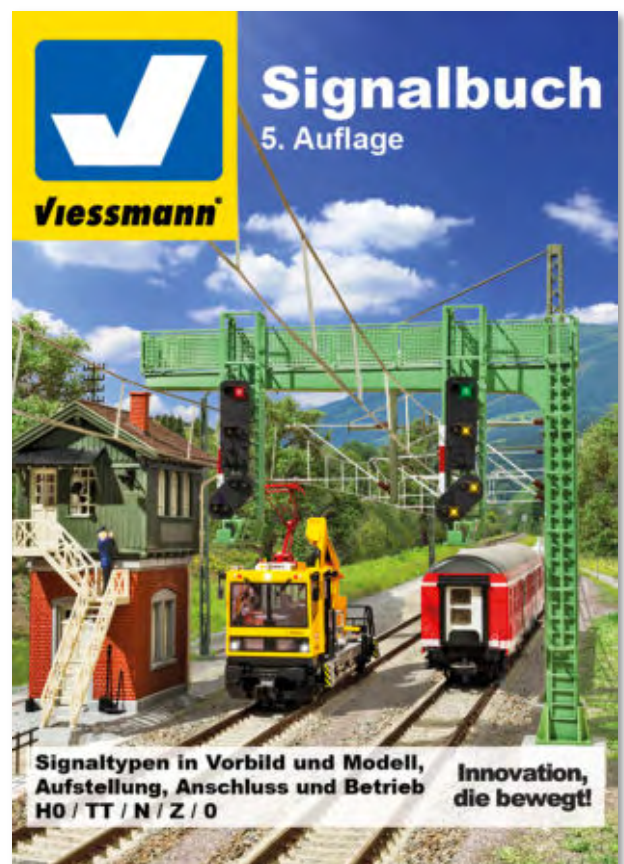
Helpful information on the possible signal patterns, their meaning, as well as the appropriate installation location, can be found in antiquarian booklets as Miba Report 17 by Stefan Carstens with the title "Signals Part 1: Main and distant signals" or, explained there rather on the basis of models, in the signal book of the manufacturer Viessmann (item no. 5299).

Confusion about manufacturers and nominations:

While reading the article about the awards for the new releases of 2023, I once again realised how small the market for our beautiful hobby is.

Michael Meißner, by E-Mail

Editor's reply: Azar Models is still very active, so it does not fit into the description of the article, which is why it cannot be included. At this point, however, we only evaluate new moulds or major revisions that almost correspond to such a model. The first model to be launched on the market is, therefore, always voted on.



Viessmann-Signalbuch (Art.-Nr. 5299). Photo: Viessmann

However, one thing surprised me in the list of the few remaining manufacturers, whether small or large series, Azar was not mentioned at all, and it is precisely these manufacturers who are really endeavouring to gain a foothold with very respectable models.



As part of the new releases of the year, we always evaluate the first variant of a new mould or fundamental revision to be delivered. This resulted in a few small selections for the 2023 review year.

However, no such model was available from Azar Models during the period under review. This will certainly change when the announced TGV is released, which we are eagerly awaiting. As Azar Models is currently switching completely from 3D printing to plastic injection moulding, it is no longer so easy to classify this supplier correctly: Its possibilities are now definitely moving it in the direction of large-scale production.

Views on the overhead line:

(...) I celebrate the functional overhead line (with radially aligned masts in multi-track curves and corresponding cut-to-length contact wires: Provide pusher locomotive? Provide lead locomotive? Pull into the waiting area and lower the catenary, scheduled service pulls in, lower the catenary there, and so on.

A model railway is a model railway. The miniaturisation of reality fails due to the reduced dimensions, which can no longer guarantee stability per se. (...)

Uwe Pfannenschmidt, by E-Mail

Controversial aspects of overhead lines and pantographs:

Rarely has a report in Trainini upset me as much as the article "Who invented it?" about the Z overhead line. (...) Is there any evidence for the assertion that the Z overhead line is used by very few people?

(...) Then the illustration on page 7 above, Rokuhan can again be seen as the supposed standard. Is it a coincidence that the catenary, which is praised as being so much finer, is also further away from the camera lens than the supposedly so coarse counterpart from Märklin? Even from the less favourable angle for the Märklin model, is the difference really so brutal? I don't think so. And in the photo below, a completely unrealistic shorty is used as a counter-argument in favour of Märklin's overhead line philosophy.

(...) Your article is a good example of how, with the support of the trade press, endless demands are made, which then lead to model railways becoming increasingly unaffordable. (...)

I admit that I like the Sommerfeldt catenary better. If it doesn't pick up the current as well, I would stick with the Märklin model. And yes, the original overhead line made of punched sheet metal is out of date



In this photo, the viewing angle was more in favour of the Märklin pantograph. Nevertheless, its appearance is clearly inferior to its Rokuhan counterpart, especially as it does not even begin to lie flat on the roof of the dining car.

and could be updated with a somewhat finer, but still functional model. However, if I had to do without the functionality, I would stick with the previous version. (...)

Dietrich Frank, Düsseldorf

Editor's response: In consultation with our reader, we have had to make a major cut and answer further points in direct dialogue. The assertion that only a very small number of customers use the electric overhead line function has been verified: The ratio of electrified model route lengths can be determined from queried dealer sales and put in relation to the number of electric locomotive models sold. Incidentally, this also reflects our impressions from reader dialogues and trade fair observations.

We would like to respond to the reference to the photos on page 7 with a reference to page 6: Rokuhan and Märklin models can be seen there in opposite perspective. This also emphasises our statements. The Märklin pantographs are not to scale, and, when removed from the contact wire, do not lie anywhere near flat. They look more like a foreign body on the roof of a model.

We were pleased that the positive impression of the Sommerfeldt alternatives was confirmed. We have encouraged this supplier to think about further alternatives and retrofit offers in order to keep our scale attractive. There seems to be no doubt that the coarse Märklin punched parts for the contact wire are out of date.

We welcome every reader's opinion at this point, which enables us to engage in an extensive dialogue in order to contribute to and influence the future of Z-gauge in a targeted manner.

A list of ideas for Noch:

Our appeal in the Nuremberg video on our **Trainini TV** channel is working: We receive messages from some readers with great ideas for future car models, which we can still suggest for multicolour 3D printing at an agreed date at Intermodellbau.

Keep up the good work and let us know your ideas. We will collect them, prioritise them according to the number of mentions, and then add further information, such as the same or similar offers from the past.

We are keeping the result confidential so as not to make the subsequent process, which will take place at the accessories manufacturer, transparent and to be able to surprise every idea provider equally.

Record-breaking speed at Yellow Dwarf:

Every month we report on new products here. Yellow Dwarf is one of the few providers where we run out of things to say. The speed with which we are regularly updated here is almost unbelievable.

And the hard-working 3D printer from the Czech Republic has more surprises in store for March. He is now adding a product to the waste disposal range, that is perfect for large residential units in modern housing estates: Underground containers (item no. 60008) provide above-ground disposal options for waste, which are fed into the large containers at basement level.



From now on, the "Warehouse equipment I" (item no. 60240; photo left) together with the "Shelving IV" (60245; photo right) will enrich many a haulage company, warehouse or larger commercial enterprise. Photos: Yellow Dwarf

New products for road and rail storage are designed to catch the eye: Storage equipment I (60240) is dedicated to palletised goods, while racking IV (60245) accommodates rolling tyre drums, which can also be used for direct tapping in workshops, for example.

The products are available directly from the manufacturer at <https://www.yellowdwarf.eu>.

The latest AZL deliveries:

New deliveries of the EMD SD70ACE were literally announced with fanfare and have now been launched. The unusual "Spirit of Union Pacific" livery variant (item no. 63109-8) on the locomotive with the road number 1943 marks the beginning.

To mark the 25th anniversary of BNSF, the GE ES64AC is now available in its anniversary livery (62415-1). The EMD SD40-2s of Canadian Pacific (64206-1 to -3) and Burlington Northern (64202-1 to -3) are rolling in three examples with the individual features of the prototypes.



The GE ES64AC (item no. 62415-1; photo left) honours the 25th anniversary of the BNSF, while the EMD SD70ACE in the special livery "Spirit of Union Pacific" (63109-8; photo right). Photos: AZL / Ztrack

The Ortner bulk freight wagons now follow in the brown Southern Pacific livery as a duo (905351-1), also available in a pack of four (905301-1). The heavy half-baggage wagon follows, running on three-axle bogies, now in the Southern version (74006-1 / -2), while the Rio Grande wide-view goods train escort

wagon (921029-1 / -2) is to run with the EMD SD40T-2 models already delivered for the same railway company.

The Trinity RAF column wagons with a prototypical length of 53 feet appear as three-part wagons with Marten trailers (905236-1), which are also available in two units each without wagons (954012-1).

The M88 armoured recovery vehicle announced in the last issue under Z tanks

is now also ready for delivery. It has an individually attached dozer blade and a movable boom. It is offered in pairs in sand colour (AZLM88-S), green (AZLM88-G) and NATO olive green (AZLM88-N).



The M88 armoured recovery vehicle is now available in three colours, shown here in NATO olive green (AZLM88-N). Photo: AZL / Ztrack

Further product images are available at <https://www.americanzline.com>.

Good news from Azar Models:

The Ulule financing project for the TGV was successfully completed at the end of February 2024: 140 bookings generated slightly more than the originally hoped-for EUR 100,000.00 financial boost.

This is not only a great success for French Z gauge, because so many positive and enthusiastic messages have been received that Germany's neighbouring country is apparently being completely rediscovered in its diversity and uniqueness.

Azar Models would like to thank all those involved for publicising and supporting the campaign.

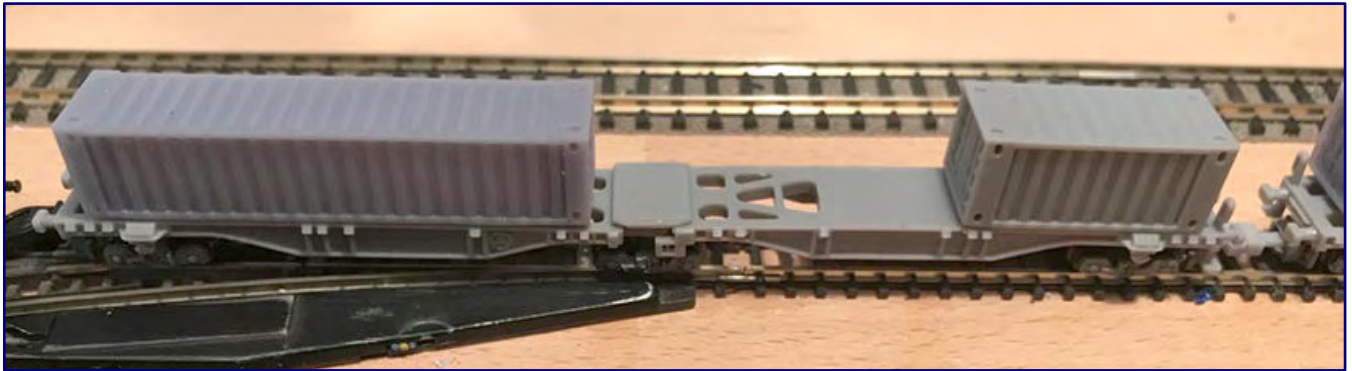
This marks the start of the next adventure in mould construction, test moulds and the first samples, which are due to arrive in a few months' time and will then be presented. But in the meantime, there will also be new products and further releases.



Thanks to a successful capital collection, nothing now stands in the way of this 1:220 scale train. Photo: Falk2 (CC-BY-SA-3.0,2.5,2.0,1.0)

NoBa-Modelle Surprise:

NoBa-Modelle (<https://www.noba-modelle.de>) is also coming up with further new products so soon after the toy fair: The Sggmrss 90 container wagon is fitted with a Jakobs bogie and can be ordered as a finished model with containers (item no. 5330RF) and as an unfinished model (5330R) with or without containers.



Shortly after its launch, the modern Sggmrss 90 container wagon (item no. 5330R) was given another major overhaul in the area of the central Jakobs bogie and now matches its original almost perfectly without having lost any of its running characteristics. Photo: NoBa-Modelle

It is also learning to run with shorty parts from Rokuhan: The Siemens Desiro Classic diesel multiple unit, also known as the 642 series, is also new to the range and will initially be included in the programme as a blank kit (5232R). A traffic-red finished model is to follow soon. Shortly before the editorial deadline, we were also notified of the ET 194 / LAG 895 railcar miniature as a blank (5233R) and finished model (5233RF), and it was added directly to the delivery programme.

The Mercedes-Benz NG 1632 "Auf Achse" (6926R) saw the light of day as a new truck model. The abbreviation NG once stood for "New Generation", with which the manufacturer wanted to emphasise the replacement of its first generation of front-mounted cabs. The Freden signal box (4134R), whose brick construction is offered as a kit blank, was realised at the customer's request.



Although the NG model series has also been presented by other manufacturers, the Mercedes-Benz NG 1632 "On the road" (6926R) has never been realised in this way before. Photo: NoBa-Modelle

We also have good news for fans of the E 919, which we presented as a spring new product in the last issue: Parallel to the model, which is to run on an undestroyed crocodile chassis, Norbert Heller is currently preparing a second, strictly true-to-scale version.

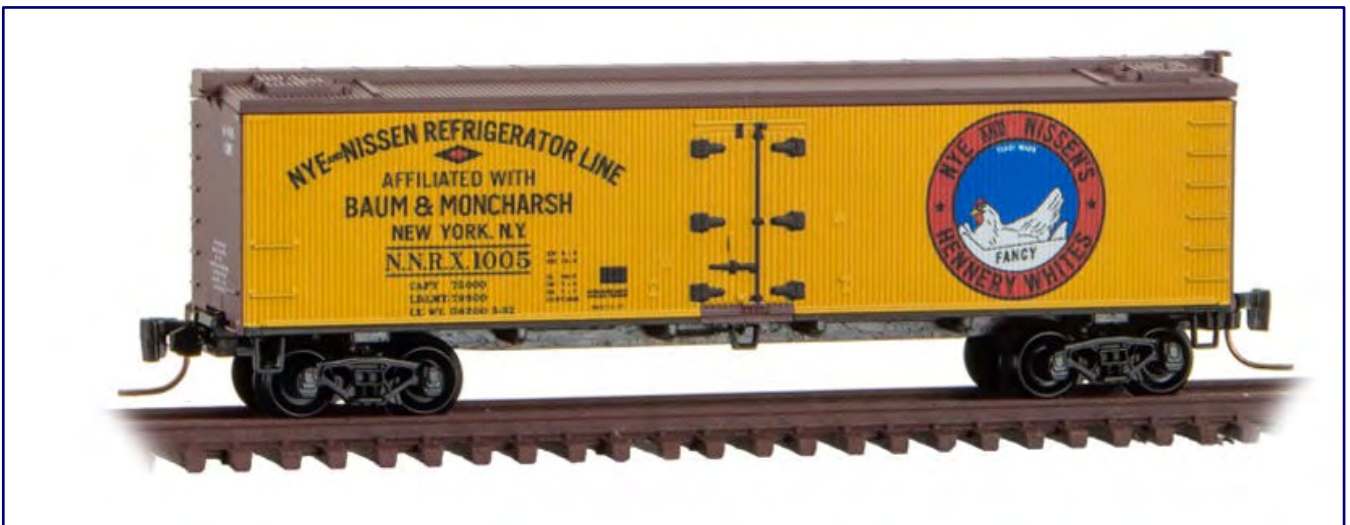


One of the masterpieces so far is certainly the Freden signal box (4134R), which can be seen here already fully painted. Photo: Customer photo (NoBa-Modelle)

It will utilise what is possible on the outside, but will require cuts to the running gear parts, which can then no longer be used for the Märklin Crocodile. At the time of going to press, the couplings had not yet been finalised: If possible, an option should be found here that also allows shunting with uncouplers from the Jörger system.

A delivery at MTL:

In the month under review, we were only informed of a single delivery at Micro-Trains (<https://www.micro-trains.com>). This is a covered 36-foot heat protection wagon with wooden walls, built in 1928 (item no. 518 00 863). It was employed by P&E for Nye & Nissen, who used it to transport eggs, butter, and cheese. This is why the yellow wagon also carries a self-advertisement for eggs.



The original of this insulated trolley (item no. 518 00 863) dates back to 1928 and was used to transport food. Photo: Micro-Trains

Successful film release:

The film "Wunderland" reviewed in this issue got off to a successful start. As the best newcomer, it has reached third place in the German cinema charts and is now on a par with many a box office hit from Hollywood. This certainly underlines the positive impressions that we have summarised in this issue.



In many film scenes, the Braun brothers appear as animated figures in childhood on their large exhibition layout to tell the story of their dream come true. Photo: TOBIS Film GmbH

On 25 April, the Monaco section, including the Formula 1 race and adjoining Provence, will be opened in Hamburg. From the following day, this section will also be open to the public.

Deliveries at Märklin complete the trio:

Märklin recently delivered the old construction tank car with brakeman's cab (item no. 80324), used by the BV-Aral on the German Federal Railroad. It is the latest car in a series with some differences, which now forms a trio worth seeing.



This Aral tank wagon (item no. 82304) together with its two predecessors now forms a handsome trio.

A little later, the "Bosporus Sprinter" also arrived at the dealers. This is a Siemens ES 64 U2 locomotive from MRCE Dispolok GmbH (88588) for Era VI.

The black locomotive from the Taurus family has three single-arm pantographs, of which the two outer ones are electrically functional.

The latest delivery in the form of a train set with a Railbouw Leerdam V 36 diesel locomotive (81771) is already sold out at the factory. The locomotive is

supplemented by three rotary slide side unloading wagons, is located in Era IV and was at home in the Netherlands as a prototype.

With the “Hanau temporary signal box” kit (89808), a spring 2024 new product has also already been delivered. When assembled, the solid-coloured hard cardboard parts result in a coach body of the MCI temporary passenger coach placed on a wall base, as it served as a temporary solution in the Hanau main station area.

Early pioneers of the lightweight:

This month, WDW Full Throttle is commemorating an interesting chapter in railway history. In the early years of the 20th century, a new material, aluminium, was introduced to railway vehicle manufacturers in the USA. From 1931, the lightweight and corrosion-resistant metal also found its way into wagon construction.

The idea behind this was to be able to significantly increase the load weight with less dead weight of a wagon with the same box dimensions. The trials were only interrupted by the Second World War. Two railway companies started the practical trials: the Southern (SOU) and the Chicago, Burlington and Quincy Railroad (CBQ).



With a new double pack, WDW Full Throttle is showcasing two early bulk goods wagons (item no. FT-COL67) made of lightweight aluminium construction. Photo: WDW Full Throttle

The new collector's pack (item no. FT-COL67) with one such 33-foot bulk goods wagon from each of the two companies takes up the memory of this pioneering era. The models are painted aluminium-coloured, have external box struts, two outlet openings on the wagon floor, in-house couplings, bogies, and wheelsets.

Just announced, already available:

Artitec has already completed the first two deliveries just over a month after announcing its new spring 2024 figures. Two packs from the new figure range have already been delivered, which we want to present in more detail in a separate figure article in the near future.



The new bricklayers (item no. 52.0001; photo above) as well as the field farmers (522.0003; photo below) also stand up to critical scrutiny when enlarged several times without revealing any flaws. Worth seeing are details such as the wooden shoes on the farmer's wife's feet, which used to be common in the countryside.

The bricklayers (item no. 52.0001) and the farmers (522.0003) are now available from dealers. The new figures have been produced using a mixed construction method of plastic and etched parts (farmers' baskets), underlined by a perfect application of colour. Details such as facial features and folds in the clothing only really become visible in macro shots.

We are therefore also very excited about the further realisations, as the impression of these completely new products for our scale is at the usual high level of the Dutch specialist. The perfect detailing makes the new figures very interesting for detailed scenes in the foreground, where the observer can still perceive them.

Imprint

ISSN 2512-8035

Bibliographic information of the German National Library: The German National Library lists this publication in the German National Bibliography. Detailed bibliographical data and editions can be found in the DNB catalogue at <https://portal.dnb.de>.

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International Edition

Free,
electronic magazine
for railroad enthusiasts
in the scale 1:220
and Prototype

www.trainini.eu

Published monthly
no guarantee

ISSN 2512-8035

Trainini

German Magazine for Z Gauge



Building a House with Vinyl Foil

**On traXS in Utrecht
Country Railway Station in Pattscheid**

Introduction

Dear Readers,

Much too late, in my opinion, this edition is (finally) going online. It is the result of a lot of hard work and equally hard pushing ahead with projects. As spring was a long time coming, we all used the time to tinker and push ahead with open construction projects.



Holger Späing
Editor-in-chief

Looking back over the past thirty days, I can see that a lot has been achieved that we want to benefit from in the coming months. However, I am also self-critical and realise that my schedule has not worked out. There are still unfinished tasks.

Nobody can do anything about that, because sometimes it's family commitments, other times my job demands more time and finally, unfortunately, there are also unplannable events that I won't go into here.

It is more important to recognise the role that model railways in all their facets play in this mix. It is a balance to the daily hassles, the decisive moment that we want to look forward to in the evening. We create something that we like and relax while doing so.

One result of such processes which impresses me immensely is the customised construction of buildings with vinyl foils. Almost exactly a year ago, our dedicated reader Reinder Rutgers from the Netherlands presented his narrow-gauge small railway projects, realised with T gauge parts. Vinyl was already used as a material here, which sparked the curiosity of our readers.

Today and in a subsequent article, we will be able to answer these questions, because thanks to ongoing photo documentation, we have received a huge number of explanations and instructions. You can look forward to this article, which is very long, and also very instructive!

Dirk Kuhlmann has put together a separate article to give you an idea of how you can implement what you have learnt in a landscaping project. This provides information about the original Pattscheid railway station and its special features to provide ideas for a model realisation.

The historic station, the building of which has been preserved to this day, was practically on Dirk's doorstep on the railway line to Bergisch Born, popularly known as the 'Balkan Railway.' It is also attractive because many of the new vehicles that we received or will soon receive from Märklin can be operated here in an absolutely prototypical manner.

Last but not least, I don't want to forget the visit to this year's On traXS. 'Hopefully we'll see each other again next year,' is still ringing in my ears. And indeed, I agree with this wish. The exhibition and museum are special, and model railway fairs in the Benelux countries also have their very own charm, which we find extremely appealing.

Please enjoy the exciting pages with us which are complemented by book tips, the latest news, and letters to the editor. In the meantime, we'll try to make up a few days for May...

Sin-Z-erely,

Holger Späing

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Currently no items

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Currently no items

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We thank FdE Burscheid, Eisenbahnstiftung and Helmut Dahlhaus for their photo support as well as Reinder Rutgers for his guest contribution.

Date of publication of the German language version of this issue: 29 April 2023

Cover photo:
The half-timbered part of this station building and the walls of the narrow-gauge railcar were made from self-adhesive vinyl. Four layers were required for the house, three for the railway vehicle – today our reader explains how to build such masterpieces. Photo: Reinder Rutgers

Pattscheid on the “Balkan Railway”

An unusual Overland Railway Station

The ETA 150 battery-powered railcar no longer seems far away. One of the routes it travelled was the “Balkan railway,” which is also the subject of a route portrait book. Dirk Kuhlmann was inspired by his regional ties, childhood memories and that book to create an unusual layout proposal.

From Dirk Kuhlmann. In keeping with the book “The Balkan Express” by Kurt Kaiß and the class 515 battery railcar from Märklin that will be released in the near future, we present you with the track plan of the former “Pattscheid Commercial Station”.

This station was in operation from 1902 to 1991 on the line built in 1881 from Opladen to Remscheid - Lennep, commonly known as the “Balkan railway”. At its largest extent, Pattscheid had the lower tracks with platforms 1 and 2, as well as track 3, which was around 5.50 metres higher, with the house platform.



For a long time, the line from Remscheid-Lennep to Opladen was characterised by push-pull trains with silverlings. Here, N 7134 to Opladen arrived at Pattscheid station on 7 May 1979. Photo: Wilfried Sieberg, Eisenbahnstiftung

It was worth noting that a gradient of 1:43 over approx. 500 metres for the upper track was a constant challenge for the locomotive crew.

In the model, however, we are looking more at the condition in the mid-1960s, track 3 had already been converted back into a stub track, and a siding was used by the locally based Illbruck company to load its products.



In October 1975, a unit consisting of VT 959 and VB 142 enters Pattscheid station as N 7131 coming from Opladen. Photo: Franz A. Keck / FdE Burscheid

Rolf Knipper, who sadly passed away far too early, had already presented the track plan of Pattscheid many years ago in Miba Spezial 31. What connects us was or is our membership in the FdE Burscheid e.V., as well as our proximity to the 411 route and the existing stations and stops.

As a youngster, I was often on site here and was even able to witness the Köf supporting a local goods train with a leader, the V 60 probably ran out of power on the incline to Burscheid.

I also know from my grandfather that until around 1965, repaired V 20s and V 36s from the Opladen works completed their test runs on this line. Even a red V 36 of the German Federal Post Office was among them. This at least gives us a realistic run-out for the Märklin Z gauge model (from the package 81341).

The main load in passenger transport at this time was carried by the VT 95 and VT 98, as well as the V 100 with conversion coaches. Freight transport was also carried out by the V 100 and, less frequently, the V 90, which entered service in 1964/65.

In some cases, a V 60 was also used for line services, or even two of the same type. If you were lucky, you could even still see steam locomotives, usually the class 050, until the beginning of 1970.

continued on page 7



The historical photo (above) shows the condition of the station in 1920, the colouring was only done a hundred years later by the FdE Burscheid. On 6 September 1932, 92 879 is on construction train service at Pattscheid station. Photos: Slg. FdE Burscheid (Photo above) / RBD Wuppertal (Eckler), Eisenbahnstiftung (Photo below)

With the exception of the V 90, which was at least announced, and the VT 95, which was not offered, we can also use all vehicle types in 1:220 scale. Let's take a look at the historical context: The double-track branch line was converted back to a single-track main line in 1958, but with its heavy superstructure, it continued to fulfil the requirements for traffic with large locomotives.

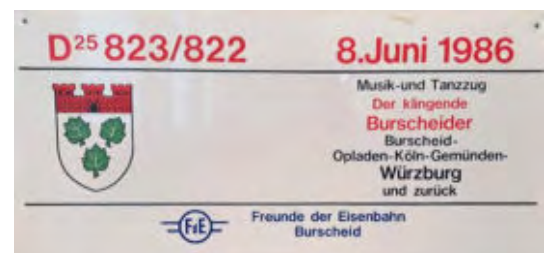


In 1958, 50 264 from the Hagen-Vorhalle depot pays a visit to Pattscheid station with a special train. Photo: Slg. Eisenbahn- und Heimatmuseum Erkrath-Hochdahl / Eisenbahnstiftung

Ideas for model operations

This made another type of model railway operation possible: the special trips that were already popular at the time. Here I would just like to mention the activities of still young model railway club "FdE Burscheid".

The "sounding Burscheider" with the long journeys to Heidelberg or Bremen are still remembered by older people today. Then the class 012 (announced at Modelleisenbahnen Bahls) may also come into play – for photos, please follow the link tip at the end of the article.



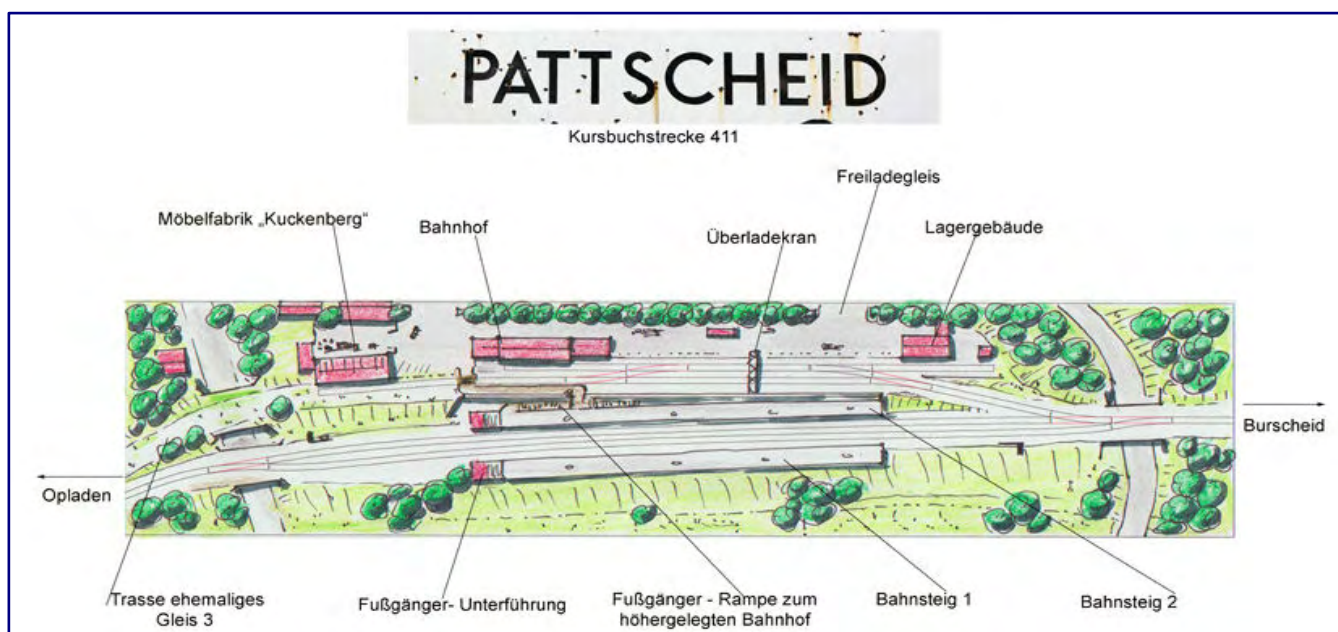
Coll. FdE Burscheid



On 24 March 1973, a special train with 012 063-4 rolls through the afternoon sun over the double-track line of the "Balkanbahn" (KBS 411) towards Bergisch Born station. Photo: Helmut Dahlhaus

The drawn track plan has been slightly modified in the area of the loading road and the buildings so that the route would also be possible in other regions. A furniture factory with its own siding would be an alternative with many decoration options.

I have designed the rise to track 3 to be a moderate 1 cm, as we want to limit the layout to a maximum of 200 cm. Here we are once again playing to our advantage in terms of scale, as a minimum of six metres would probably be required in H0 scale.



I realise that critics will immediately bring up the waste of space, but they are simply following the modern way of doing things. Allow the layout with its landscape the generosity on offer and enjoy the simplicity of the track figures. In our gauge we have the possibility of (almost) realistic expansion of an area. Incidentally, the landscape is characterised by many fields and mixed forests.



The natural landscape opposite Pattscheid station has hardly changed over the decades and contributes to beautiful model railway motifs.

The next point is about the various buildings. If you want to position the typical houses of the Bergisches Land here, you will have to build them yourself or perhaps you will need a service provider for 3D printing technology. Unfortunately, there is no off-the-shelf kit, for whatever reason.

An alternative for the station building could be the Westheim station from Archistories with slight modifications. After all, slate cladding is still omnipresent in Pattscheid today. Here is a small list of possible kits for your own modifications:

- Station 'Westheim' (Archistories 105191),
- Water mill 'Kallental' (Archistories 706141),

- Half-timbered farmhouse plastered (Modellbau Laffont Z501),
- Machine hall (Modellbau Laffont Z311),
- Rural apartment block (Modellbau Laffont Z3101),

- Dwelling houses 20s (Kibri 36782), and
- Half-timbered houses (Kibri 36406).

continued on page 11



Photo above:
212 074-9 arrives at Pattscheid station on 12 July 1983 with a local train from Remscheid-Lenep to Opladen. Photo: Peter Schiffer, Eisenbahnstiftung

Photo below:
The Wuppertal 515 565-0 is on the Opladen - Burscheid - Remscheid-Lenep line as N 7123 to Hilgen near Pattscheid on 24 April 1985. Photo: Joachim Bügel, Eisenbahnstiftung

Construction and operating concepts

We have two good options for creating “traffic” for our exhibit. The pure observer of moving trains naturally prefers the circular traffic, where the classic staging yard can be found beyond the background scenery. Fans of shunting games would prefer “fiddleyards” at both ends of the layout.

Why not give the design part a “dwelling” with integrated lighting, generally referred to as a “peep box.” This allows our exhibit to serve as a pure diorama and fine photo backdrop when not in use, separated from the rest of the arrangement.



Two views of the exemplary restored Pattscheid station building (photo above). The view of the former track and platform at Pattscheid station in the direction of Opladen can now be seen from a cycle path. A plaque commemorates the original condition (Photo below).

The curve elements as well as the staging yard or the “fiddleyards” should stand on independent boxes (frame construction) and could continue to serve their purpose in future layouts.

I would like to reiterate that no track manufacturer is favoured. It is possible to realise the ideas presented here with all types of track. The choice will vary depending on your own preferences.

Whether they are digital or analogue is also a matter of personal preference, just like the other electrical components. Even the scenery does not have to be set in the Bergisches Land, just let your imagination

run wild. One important principle always applies: good lighting of the layout is particularly important to ensure that your trains appear in the right light.

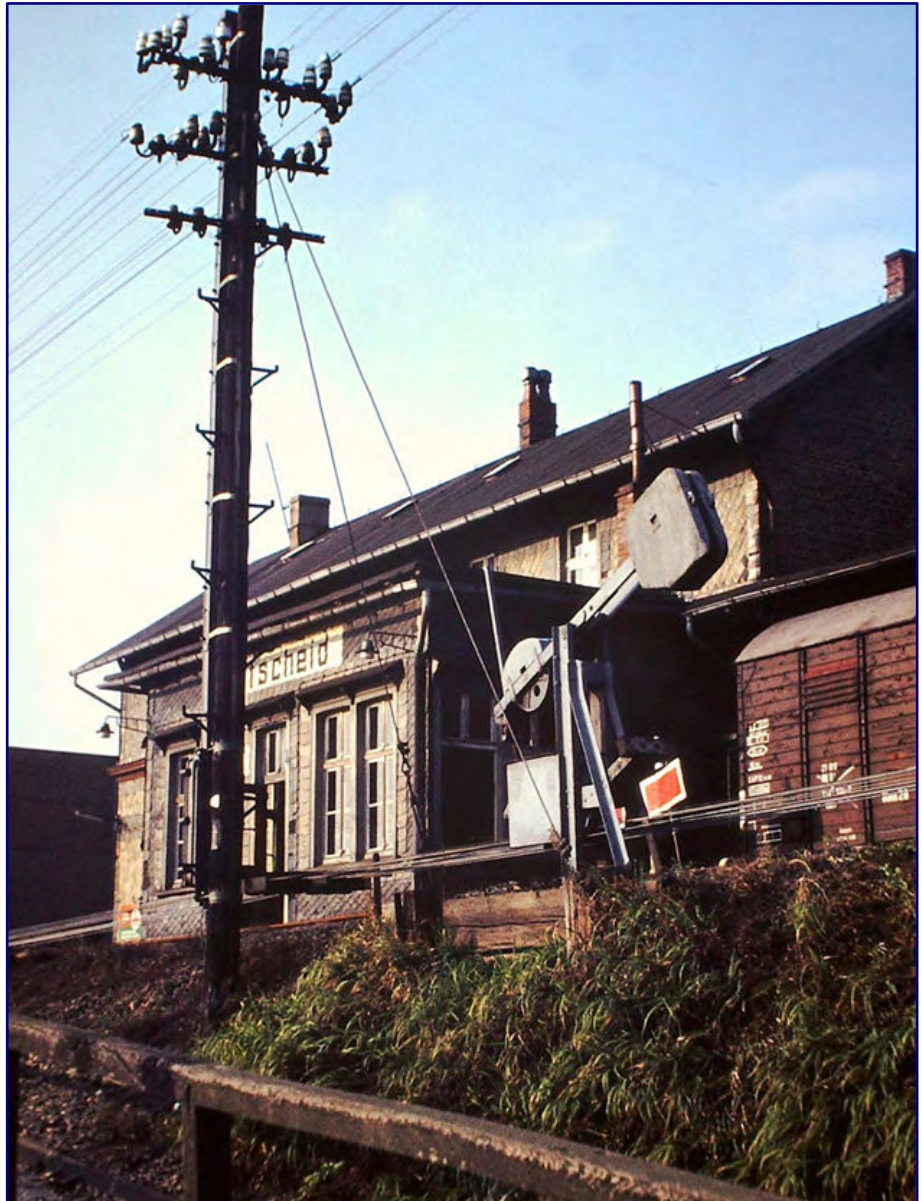
What is actually left of Pattscheid railway station today?

Amazingly, the Illbruck family/-company from Pattscheid acquired the reception building in 1998 and subsequently had it restored in an exemplary manner. Since then, it has been a conference centre.

A cycle path has been running along the old route since 2014.

The first serious discussions about reactivating the line arose in summer 2023. Due to the once dual track, both transport routes could coexist here.

How nice it would be to take an inaugural trip in a historic rail bus, past the fields and orchards and then enter Pattscheid station...



An old Gms 30 stands at the loading ramp belonging to the Pattscheid station building in October 1975, waiting for its freight. Photo: Franz A. Keck / FdE Burscheid

Manufacturer of suitable vehicle models:

<http://www.bahls-modelleisenbahnen.de>
<https://www.maerklin.de>

Kit suppliers for the proposal list:

<https://www.1zu220-shop.de>
<https://www.archistories-shop.com>
<https://modellbau-laffont.com>
<https://viessmann-modell.com>

Interesting facts about the Balkan route:

<http://www.bahnen-wuppertal.de/html/oberberg-balkanstrecke.html>

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Alternative building construction (Part 1)

Modelling with Vinyl Film

In spring 2023, our reader Reinder Rutgers presented his narrow-gauge railway based on T gauge. He also touched on his use of self-adhesive vinyl sheets, which he used to construct many of the buildings. He will be sharing his construction method and technology with the other Trainini® readers in several articles.

From Reinder Rutgers. I have been using self-adhesive vinyl in modelling for over 30 years. During this time, I have developed a specific way of working that I would like to introduce to other readers. The use of self-adhesive vinyl is a modest but, in my opinion, valuable addition to other modelling techniques, especially in 1:220 scale.



The construction of buildings such as this bus shelter using vinyl films is the subject of this article by Reinder Rutgers.

Two methods of modelling walls by hand will be described. I could talk a lot about this, but I think it makes more sense to explain it using detailed templates. Only where necessary will I expand on my explanations with text. Elements from the examples shown here can be combined. Further combinations resulting from this are not described – everyone can make them up for themselves.

My objective is that an average Zettie with some modelling experience can imitate this technique if he or she has a little patience and practice. Not everything will work straight away, but the templates show you where to go.

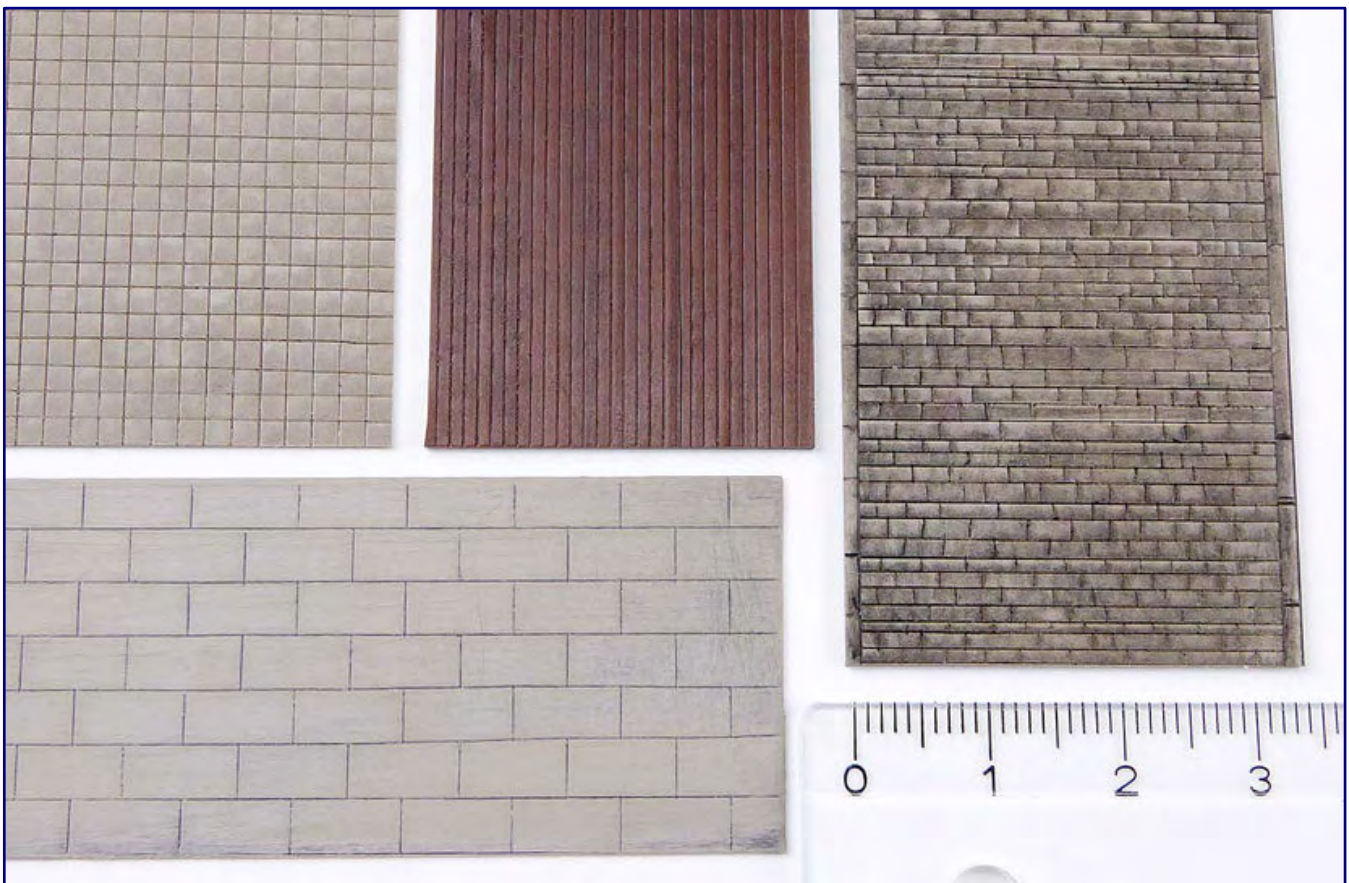
What is described here is not theoretical, it is based on my practical experience. There are undoubtedly alternatives for certain elements, but I can't guarantee that they will work, but that shouldn't stop anyone from experimenting for themselves. All the models shown have been realised on a scale of 1:220.

Finally, to keep the text compact, the words 'vinyl', 'vinyl film', or just 'film' are used as synonyms for 'self-adhesive vinyl film' in the following.

General information

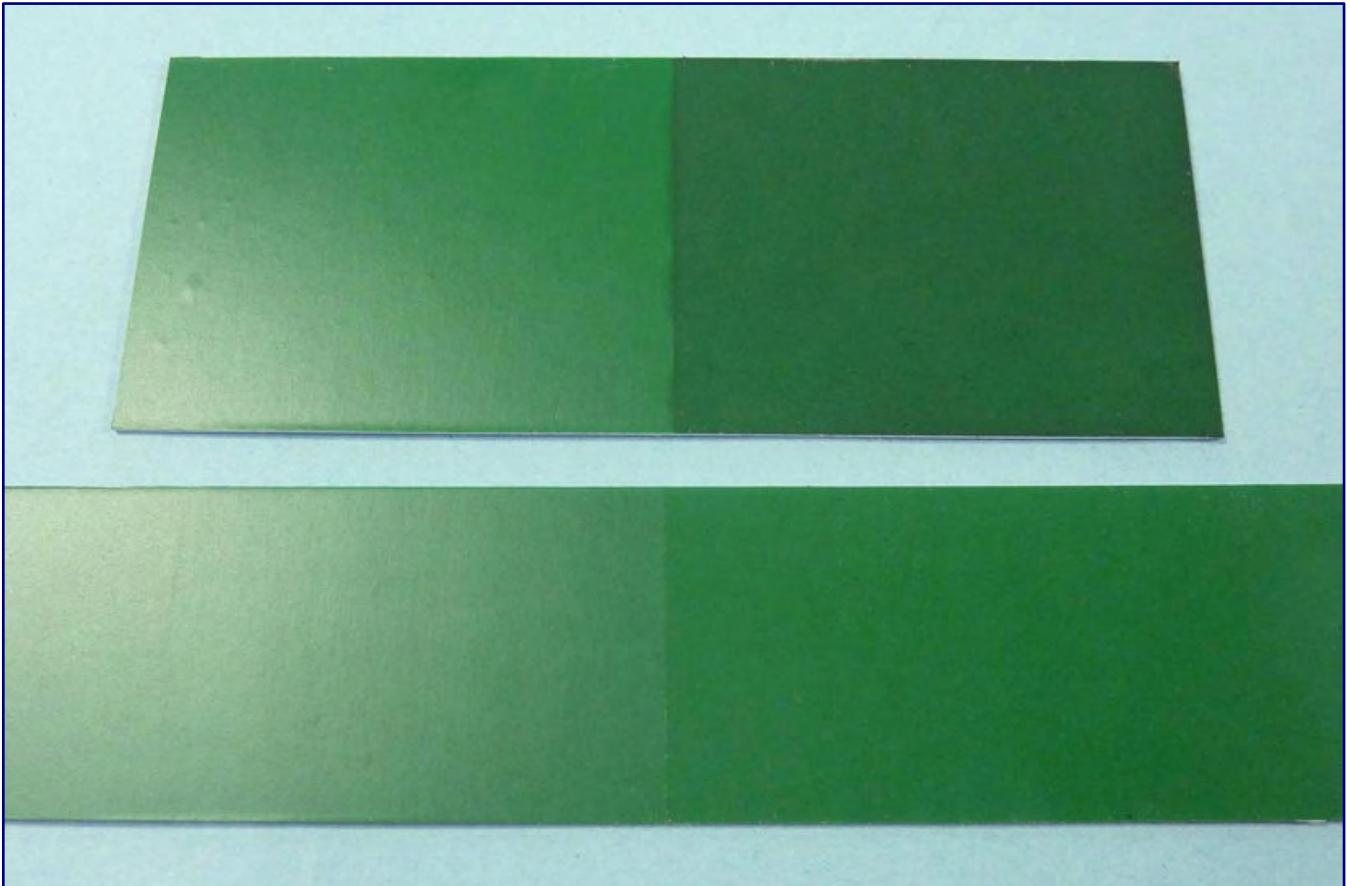
There are generally two ways of working: Cutting by hand or with a CNC cutting plotter. Although I also have extensive experience with CNC, the following text is limited to manual work.

This allows you to achieve appealing results, and most modellers will not own a cutting plotter, and it only replaces the cutting itself, the rest must be done by hand anyway.



Templates for embossing in vinyl: Embossing with a needle on the front is like engraving, and grooves or rivets are created by working through the carrier foil from behind. A cutting mat is suitable as a base for embossing.

The basis for both working methods is an adequate drawing. With manual work, it is not a problem if this drawing is not very accurate; it can also be produced traditionally with pencil and paper and, if necessary, applied directly to the foil. For CNC work, on the other hand, a drawing must be produced by computer because it must be very accurate and meet the requirements of machine cutting.



On the left we see a coloured vinyl; on the right it has been tinted to varying degrees with a black glaze.

Self-adhesive vinyl foil is used in many areas, including for foil imprints on cars, trains, windows, walls and so on. However, it is also used in the hobby sector. Common thicknesses are 70 microns (= 0.07 mm) or 100 microns (= 0.1 mm). On a scale of 1:220, this corresponds to a layer of 15 or 22 mm. Two or more layers can also be layered on top of each other. Vinyl has several advantages:

- Working with vinyl does not require a chemical and photographic process like etching, but can lead to comparable results.
- We work directly on the material and can design freely.
- Mistakes are often easy to correct.
- Vinyl is solid-coloured. Painting is therefore often not necessary, and cuts and edges always have the right colour.
- Vinyl film is easy to matt. This also makes the colour appear less saturated.
- The film can be coloured easily and sustainably with both the solvent-based and water-based modelling paints. The appearance of many films can be realistically tinted with a glaze of black or white. If this is done in a gentle way, it is not noticeable that the cut edges are slightly lighter or darker. Common ageing techniques can be used.

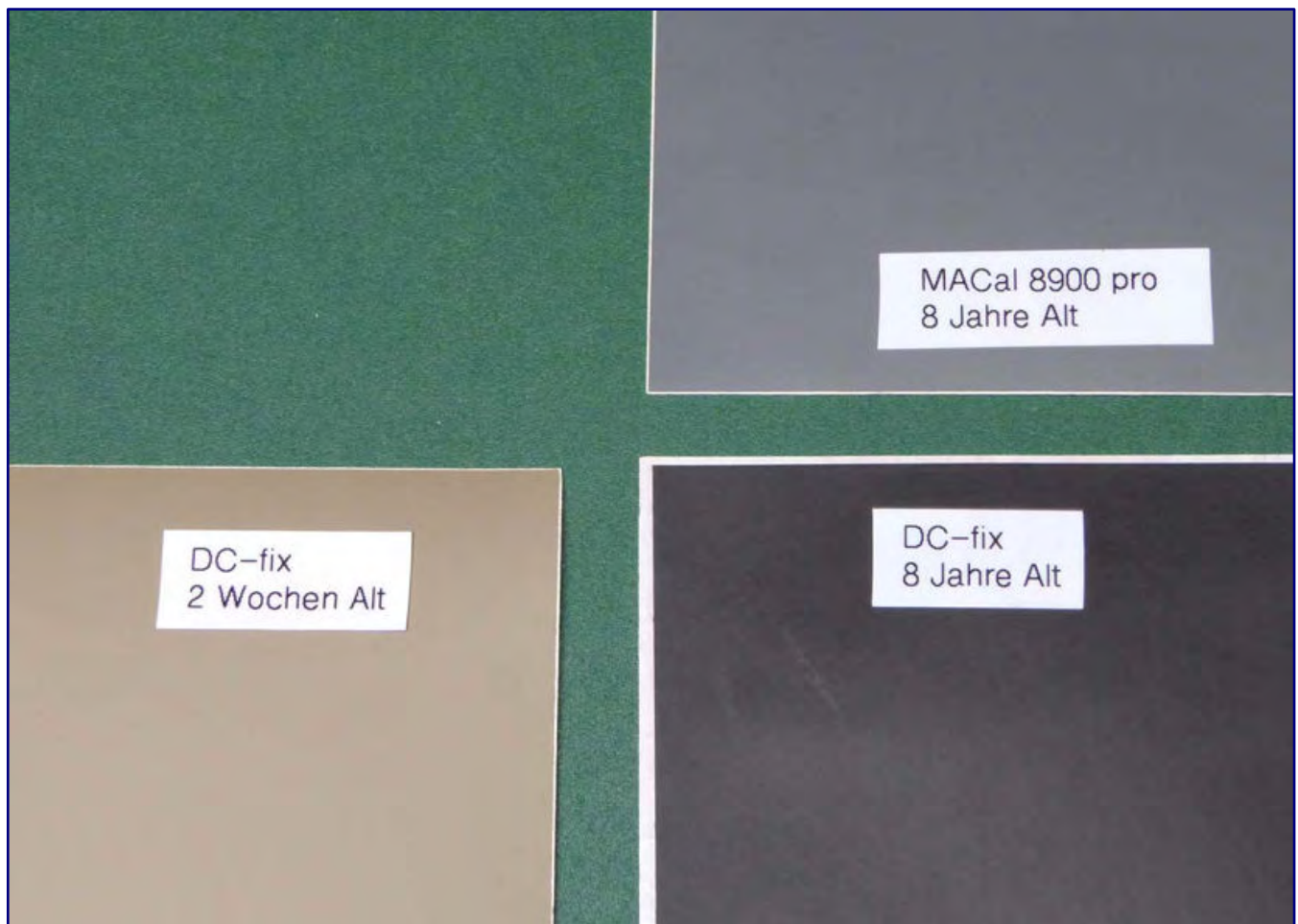
- Vinyl can be embossed well. Embossing also lasts over time.
- Vinyl combines well with other materials.

Solid-coloured vinyl film has several advantages over printable vinyl for inkjet or laser printers. Their disadvantages are insufficient adhesion of the print, white edges when cutting and embossing, but also when making corrections. Also, it is not possible to design directly on the substrate. They also lack a comparable matt appearance, and the grainy print image is distracting.

Vinyl also has advantages over cardboard walls and parts, particularly the thickness, which is true to scale for our Z scale, and the many processing, colouring, and ageing options. All in all, coloured vinyl often produces more realistic results than both other methods.

But every light has its shadow: vinyl film shrinks. How much depends on the manufacturer's brand and the type of vinyl. I have many years of experience with the DC-fix offered in paint shops and DIY stores and the professional MACtac 8900 Pro.

I have found that DC-fix shrinks by around 1.3 % - 1.4 % over the long term. That is quite a lot, especially in modelling. MACtac, on the other hand, shrinks considerably less, I estimate a maximum of around 0.5 %. No doubt there are more professional brands with films that shrink less. However, I have no experience with them. The best thing to do is to look at the technical product data from the manufacturer or ask the dealer.



The shrinkage described in the article is illustrated here using three samples. The white edges of the backing papers show the unavoidable material shrinkage, which must be considered! (2 Wochen alt = 2 weeks old; 8 Jahre alt = 8 years old)

Shrinkage can be accelerated somewhat if the foil is divided into smaller pieces – for example 20 x 20 cm. In my experience, these shrink faster than foil on a roll. The effect can be seen after just a few weeks. However, this means that further shrinkage will be less.

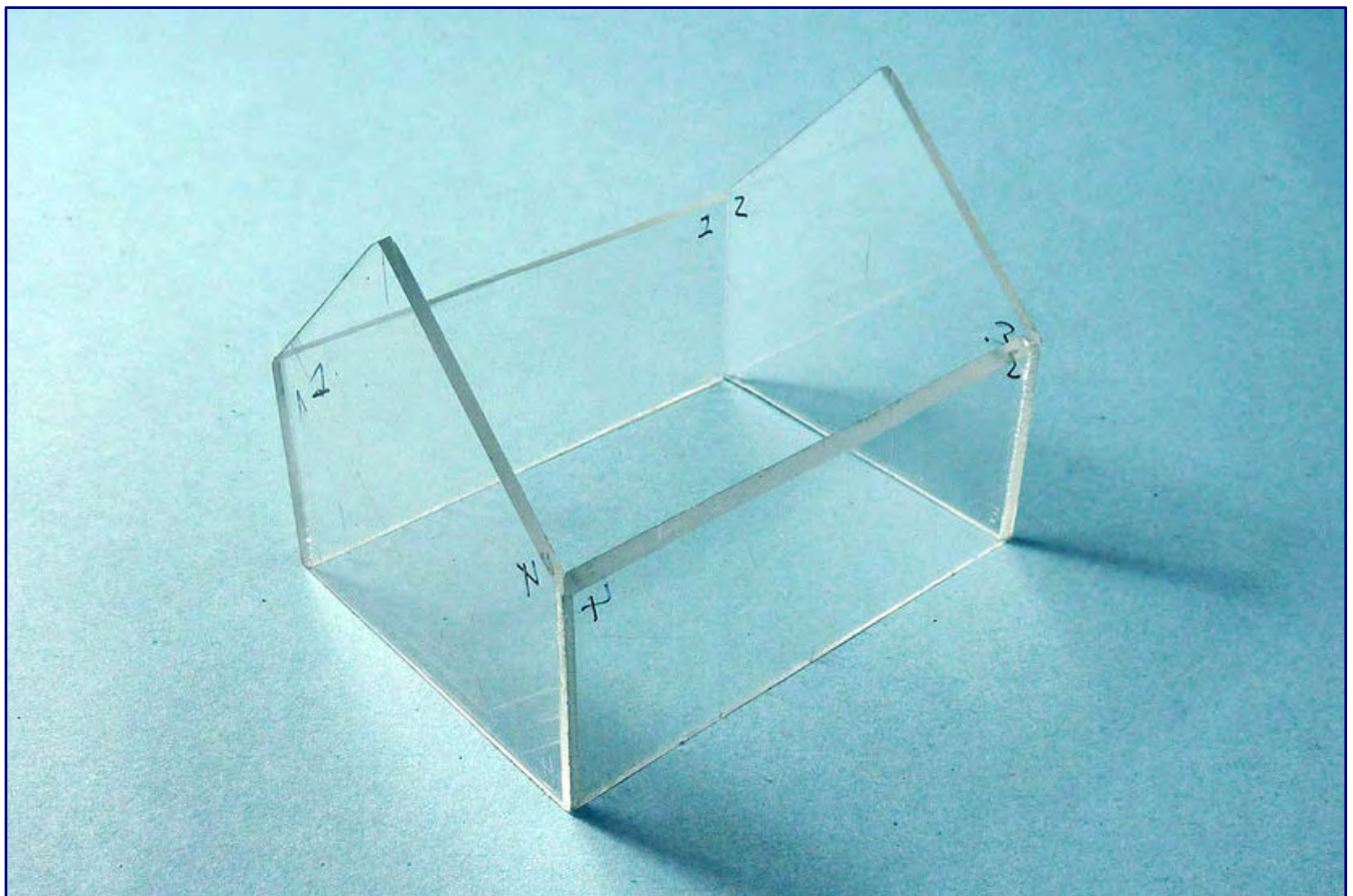
Whether 1.4 % shrinkage is a problem depends on the application. It is not noticeable that parts such as window frames, doors, shutters, etc., shrink. With longer walls, however, this is usually disturbing, except in the case of rustic country buildings.

In addition, walls and details made with vinyl are not rock-solid. It is still possible to move small parts later. This supposed disadvantage becomes an advantage, if something needs to be changed.

The sensitive surface is also not an issue, as everything is more delicate and vulnerable in Z scale. It may be possible to fix things permanently with superglue, but I never felt the need to do so.

Film types

There is strongly adhesive vinyl film and slightly adhesive, so-called repositionable vinyl film. With the strongly adhesive version, we have less room for mistakes on larger surfaces just because it is firm. Residual pieces are difficult to remove, and adhesive residues remain that are difficult to clean off.



Anticipation of method 1 (from page 19): The building base is a 'glass house' (or a 'glass car body'), as shown here. The more glass a building has, the more advantageous it is to work with the foil method.

Repositionable film, on the other hand, may have too little adhesive force for very small parts or if the substrate offers less grip, such as a surface with a texture. Low-shrinkage versions are generally the best choice. Any details designed with it also adhere well in the long term.

Only in exceptional cases is highly adhesive film the better choice. However, it goes without saying that very small details must not be touched when handling a model, otherwise they will move. However, slightly larger structures, such as stripes on the sides of a car, hold well even when gripped carefully.

Manufacturers specify a maximum service life of 10 years for the very best professional films. However, this applies to outdoor use in daylight. Indoors in subdued light, the service life is almost 'infinite.' I have models with vinyl walls that are over 30 years old, and their films have neither discoloured nor become brittle.

The main source of supply for self-adhesive vinyl film is the internet, there are many suppliers. One selection criterion is whether small quantities can also be purchased. Personally, I find it just as important that a brand is common so that identical material can be purchased at any time.

Method 1: simple

First, it should be said that we are showing 'romantic' and nostalgic models here because they are forgiving when it comes to inaccuracies and are therefore ideal for beginners. Modern buildings can be designed perfectly with foil – perhaps vinyl foil is even more suitable for this than for old buildings – but they require a more precise design.

The simplest way to make a building wall or a passenger car wall is to use two layers of self-adhesive vinyl film on a transparent, solid substrate.

Exceptions: a simplified building wall or an ultra-modern carriage wall. Just one layer might be enough.

The idea is to cover a glass building, or a car body, with vinyl film. The corners are mitres. Places where no film is applied replicate the window glazing.

The process described here involves working directly on the substrate material, as this has certain advantages, as will be shown. And now, get to work!

Transparent polystyrene is ideal as a substrate as it can be easily combined with other polystyrene parts such as wall and roof panels. It is also suitable for common modelling plastic adhesives.



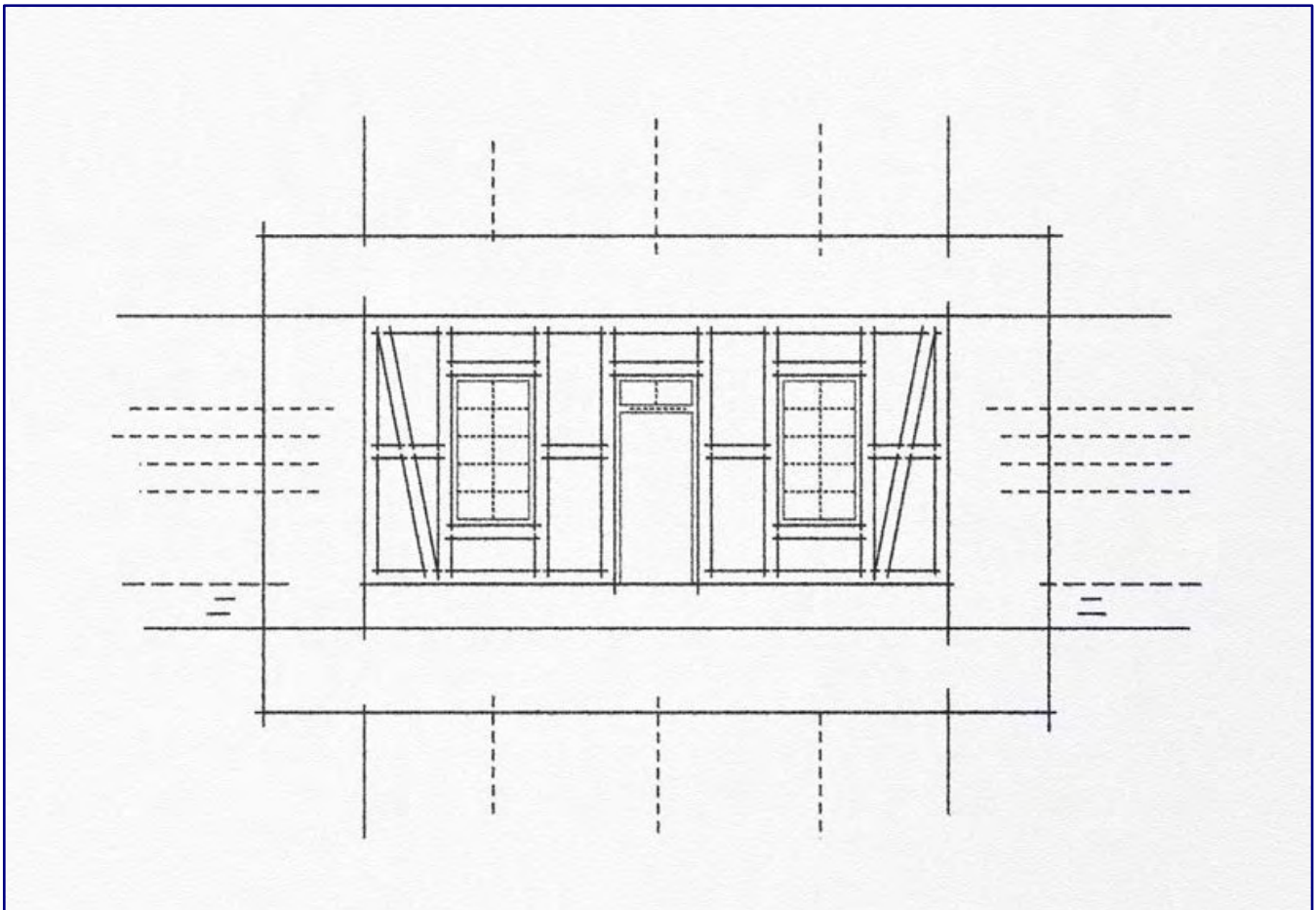
Our material of choice is crystal-clear polystyrene. Alternatively, CD sleeves can also be processed.

It is strange that there are either only very thin, transparent polystyrene sheets (Evergreen 0.13 - 0.4 mm) or very thick ones from 2 mm. The former is usually too weak in terms of construction; the alternative is already quite thick for Z gauge: 1 mm would be ideal in most cases.

One source of crystal-clear material of the desired thickness are transparent CD covers. If you don't happen to have any empty ones, you can buy empty CD covers cheaply. Transparent acrylic sheets are available from 1 mm thickness; their disadvantage, however, is that they can only be bonded with special solvents or acrylic adhesives.

The substrate should be considerably larger than the wall to be created. This creates the necessary space when working. I always apply a protective layer to the back if the material does not have one. Masking film or painter's tape is ideally suited for this.

If the windows do not need to be transparent, a black, high-gloss material could also be used. A black polystyrene sheet with a high-gloss clear coat applied or a white sheet that is subsequently painted in high-gloss black are both possible options.



As a template for our house wall, we first make a drawing in which important markings are to be carried over the edge.

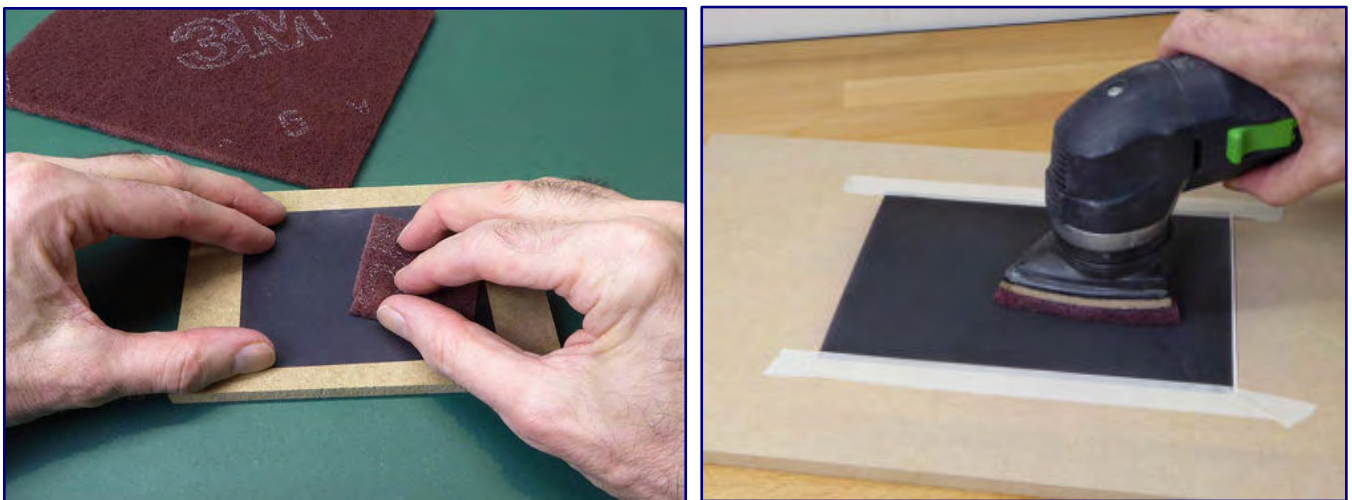
Here we go

It all starts with a drawing. This can be very simple. In this case, we will use a half-timbered house wall as a template, which can be perfectly reproduced using vinyl because the beams do not protrude above the plaster surfaces.

My drawing shows, with a few additions, how the work can be made easier. The lines are slightly extended at the corners so that we have a reference when cutting to show where the cut should begin and end. Reference lines can also be found. The drawing is then copied once, or several times and the original is retained.

A helpful hint: Make the drawing 200 % or 400 % larger than desired and then reduce it to 50 % or 25 % when copying. This makes drawing much easier.

Then place a piece of vinyl film on a board with adhesive tape and tarnish it. The easiest way to do this is with an abrasive fleece (such as '3M ScotchBrite'). It is important to make irregular movements to avoid accidentally creating a pattern on the vinyl. Using 600 or 1000 grit sandpaper may also work. However, it is more difficult to achieve an even result.



Vinyl film is tarnished by hand using a sanding fleece (photo left) or with the help of a delta sander (photo right).

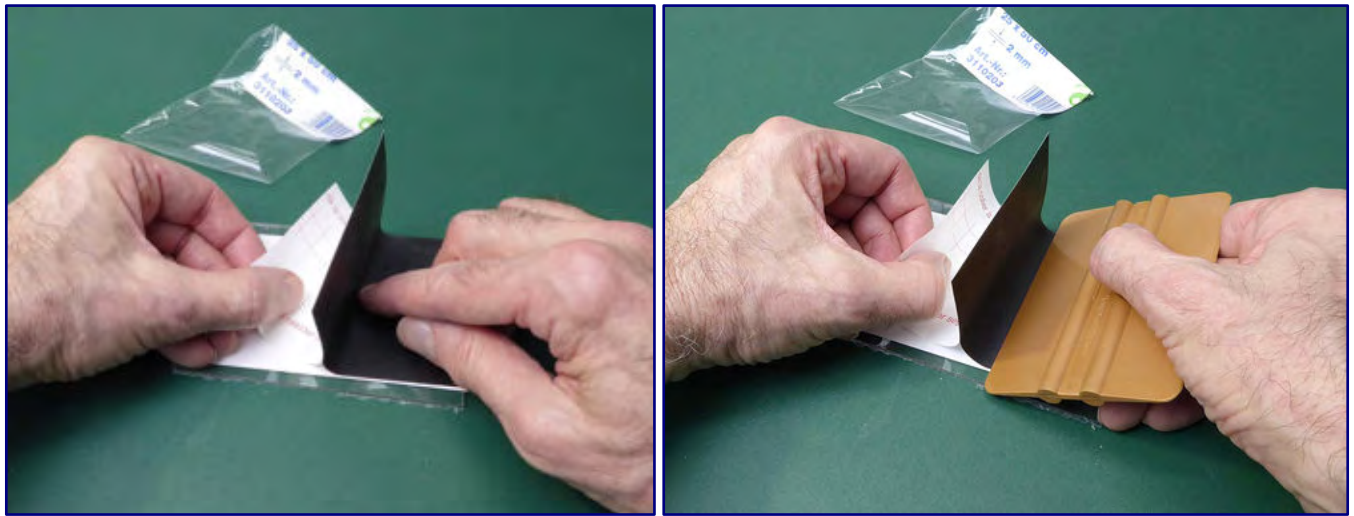
An alternative is sanding with a small sanding machine fitted with the sanding fleece. In this case, the advice regarding irregular movements and the request not to press on the device also applies.

The grain size of the sanding material must not be too coarse, but also not too fine, otherwise either the material removal will be too great, or the vinyl will begin to shine more. For machine sanding, the 'Very Fine' or 'Red' non-woven abrasive variant is recommended. The red variant should be used for hand sanding.

The protective layer is now removed from the transparent substrate on one side. Two layers of tarnished foil, in this case first white and then black, are cut to size and applied.

Just like the substrate, the pieces of vinyl are chosen to be significantly larger than the finished wall. Precise positioning is now less important in the list of requirements, and we can concentrate entirely on the flawless application of the film (without air bubbles and air pockets).

But please do not pull on the film during application! If it is overstretched, it will return to its previous shape even after bonding is complete. After a few months, unwanted seams will appear. I once experienced this painfully with my first trials.



The vinyl film is applied to the substrate using the protective film from the vinyl. This application without bubbles is done either with your fingers (left image) or with a squeegee (right image).

There are several ways to stick the film on. Because the surfaces in Z gauge are very small, you can simply pull a piece of film completely off the backing paper and then stick it on. A slightly finer method is to pull the backing paper loose just a little, then apply the film to one side, rub the paper firmly with your fingers and continue to pull it off piece by piece.

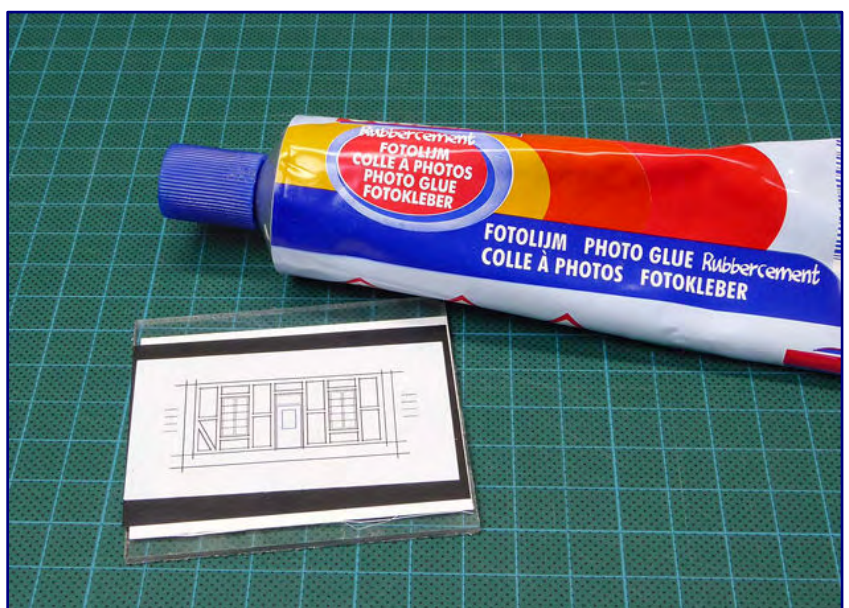
A squeegee, which is commonly used for vinyl processing in the professional sector, can also be used as a tool. I will not describe the equally widespread method of applying the vinyl to a film of soapy water and spreading it out to the edges with the squeegee as I don't consider it necessary for the small surfaces of our scale.

Now affix the drawing

At this point, we glue a copy of the drawing onto the material. One advantage of this procedure is that we no longer have to measure, align or mark anything on the material – this saves a lot of time thanks to the copies made, especially when producing several identical or very similar parts. And this procedure also works for any other material.

The right choice of adhesive is important because it should stick firmly, but at the same time it should not prevent easy and residue-free removal of the drawing afterwards. My choice fell on what graphic designers call 'rubber cement'.

We also come across it under the terms 'elastic mounting adhesive' or 'photo adhesive'. Incidentally, a similar adhesive is also used to repair bicycle inner tubes and is simply referred to as 'tube glue' or similar.

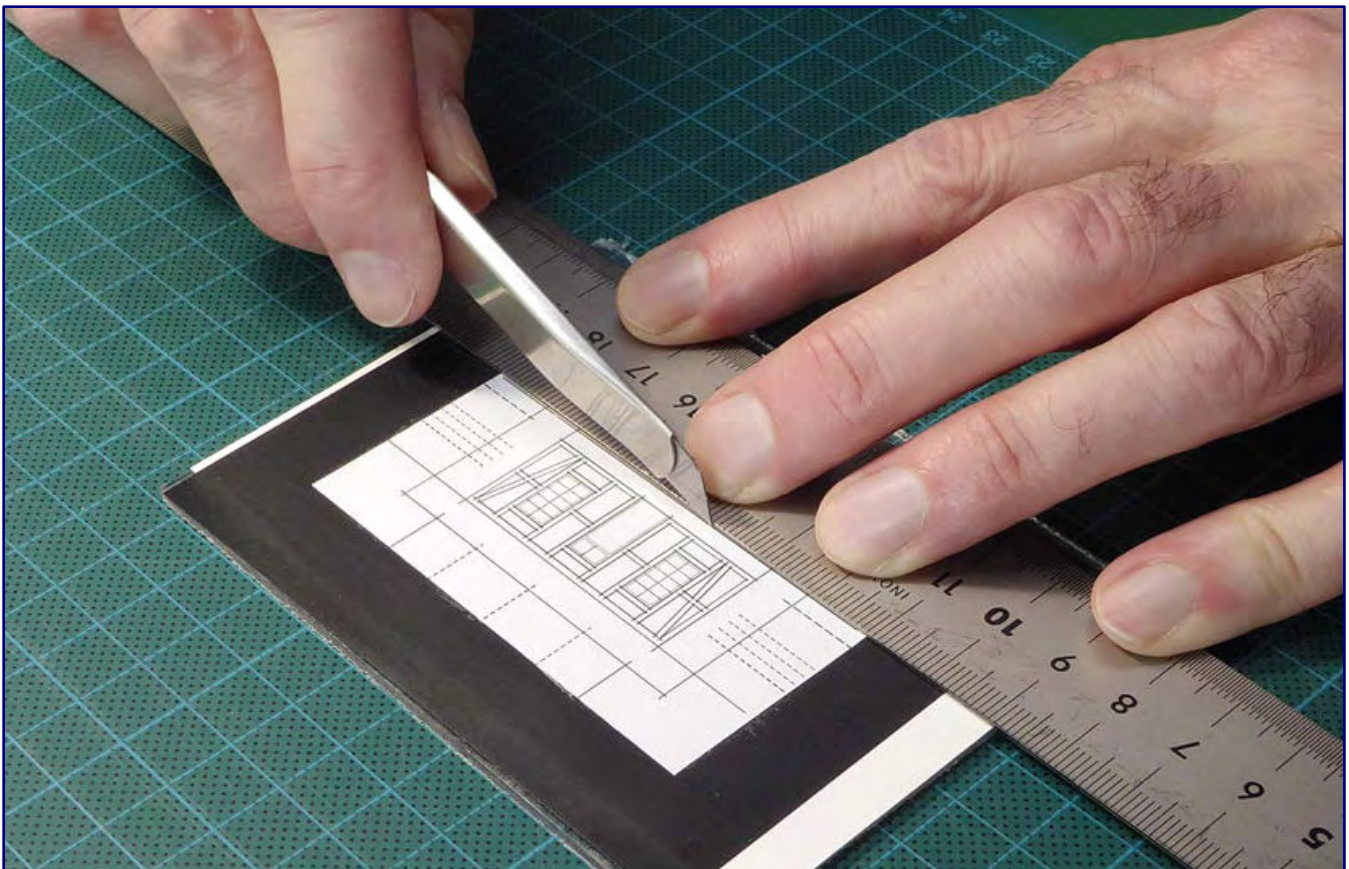


For easy adhesion, it is applied wet, not using the contact method: apply a thin coat to one surface only and apply immediately before the surface dries. The drying time is ten to twenty minutes. This simple application method is usually sufficient for film construction. For a strong bond, apply a thin coat to both surfaces and only press them together after drying (contact bonding). The bond is then immediately complete.

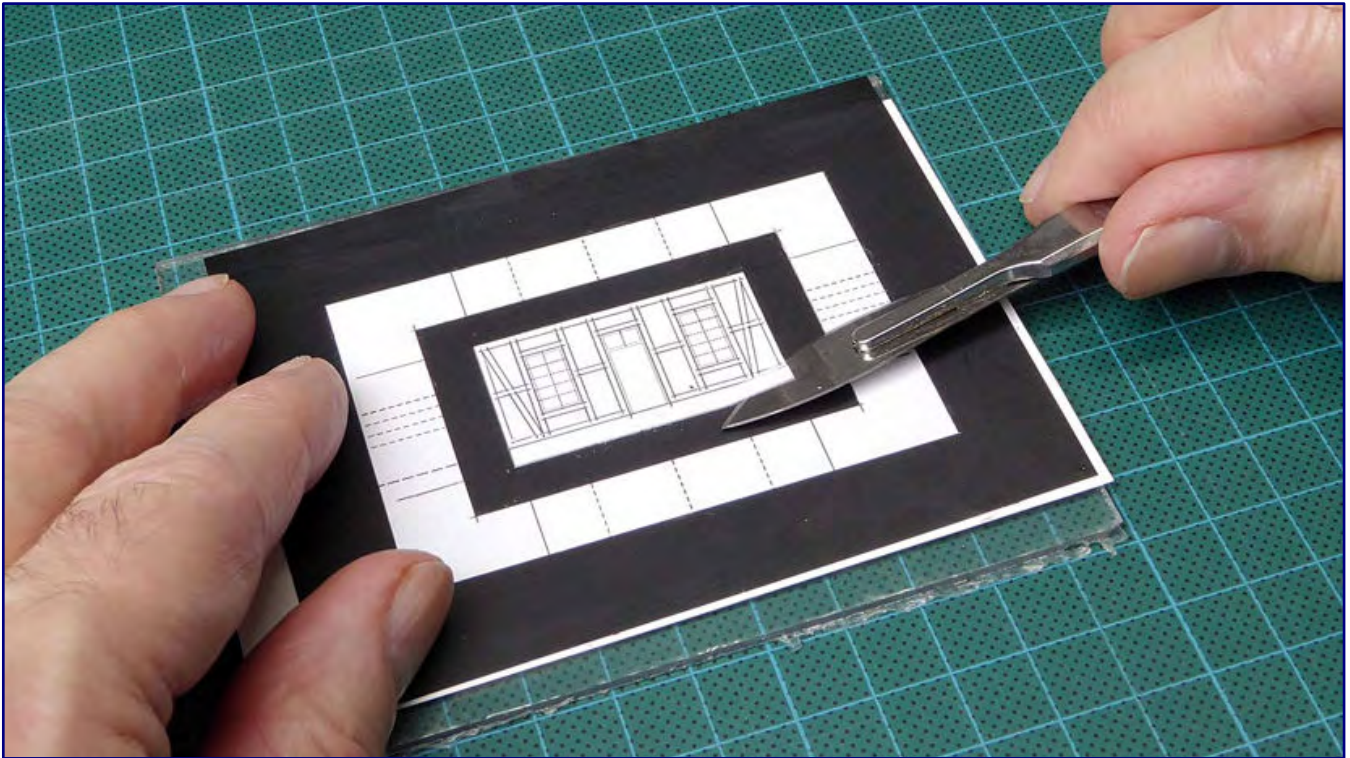
After sticking on the drawing, any adhesive residue is removed. This can be done easily and without leaving any residue by lightly rubbing it off. Vinyl is not affected by 'Rubber Cement,' polystyrene at least not significantly. Sources of supply for this adhesive are shops for graphic designers, artists, or architectural supplies, for the tube alternative the bicycle dealer or for both also the Internet.

As an alternative, I tested a universal adhesive (Pattex-Multi or similar) on one side, which I applied in a thin layer to the paper and allowed to dry for 2-½ to 3 minutes. Then the drawing was glued on and pressed down firmly, otherwise I didn't get a sufficiently strong bond. The drying time and a noticeable dependence on the type of paper or room temperature remain critical.

I have also tested removable spray adhesives from the 3M brand. When I removed them afterwards, they always left unpleasant adhesive residues that could only be removed with solvent. I, therefore, do not recommend them for this purpose.



The drawing and the top layer get now cut with a very sharp and thin knife, in my case a knife with snap-off blades. It is guided along a steel ruler.



After the necessary cuts have been made, the drawing is removed with the sharp blade of the scalpel.

It is important to break the blade regularly in order to always have a very sharp knife in your hand! With other types of knives, it then comes down to replacing the blade or resharpening it if possible.

In principle, it is not a problem if the lines are cut through to the ground, as all parts adhere to the ground. In H0 scale this usually works, but, in Z scale, the parts are so small that the timber-frame fillings are easily torn away when the remaining pieces are removed. It is important to be aware of this and to work very carefully.

For this reason, it is definitely better not to cut any deeper than is absolutely necessary. In the case of timber framing, this only applies to the beam layer, with the exception of the windows. However, this sounds more difficult than it actually is. After a little practice, we can feel whether the correct cutting depth has been achieved.

Theoretically, we could also cut through every beam of the timber frame, as this cut would be invisible afterwards; here too, H0 gauge has the edge.

In Z gauge, however, the beams are very narrow and therefore do not adhere as well. It is, therefore, more helpful to have the entire framework as a coherent frame at the end.

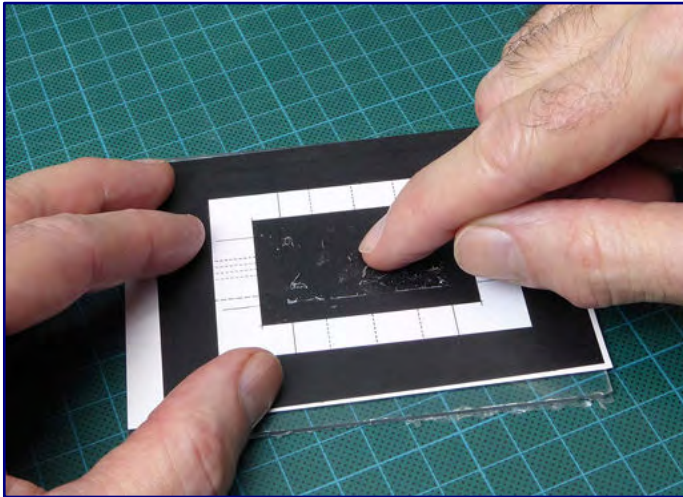
If, despite practising, you are not confident enough to make the depth of a cut so precisely, you could make yourself a simple aid. I will introduce such a tool later.

Tips for cutting

If you have never cut film before, you should always practise before starting your building project!

- Depending on the paper, film, the number of layers or the type of blade, it is sometimes advisable to cut a line lightly several times rather than trying to cut through everything straight away. It is always better to have control over the blade, than to apply a lot of force.
- Always work slowly and with concentration.
- When I use snap-off blades, I cut difficult corners from two sides so that the tip can get into the corner easily. With a very sharp blade such as "scalpel blade no. 11", however, this is usually not necessary.

We have now reached the point where we want to remove the drawing. If it is too stuck, a scalpel with a round blade will help. Keep the knife very flat – the blade must be sharp. It is very important that the outer edge of the drawing is not removed, as we still need it (see accompanying photo on page 24).



We remove adhesive residues with a finger (photo left) or with the help of a piece of rubber (photo right), for example from the sole of a shoe.

Next, we also remove the adhesive residue by rubbing. Professionals use a piece of shoe sole (rubber) to do this, but it works just as well with your fingers.

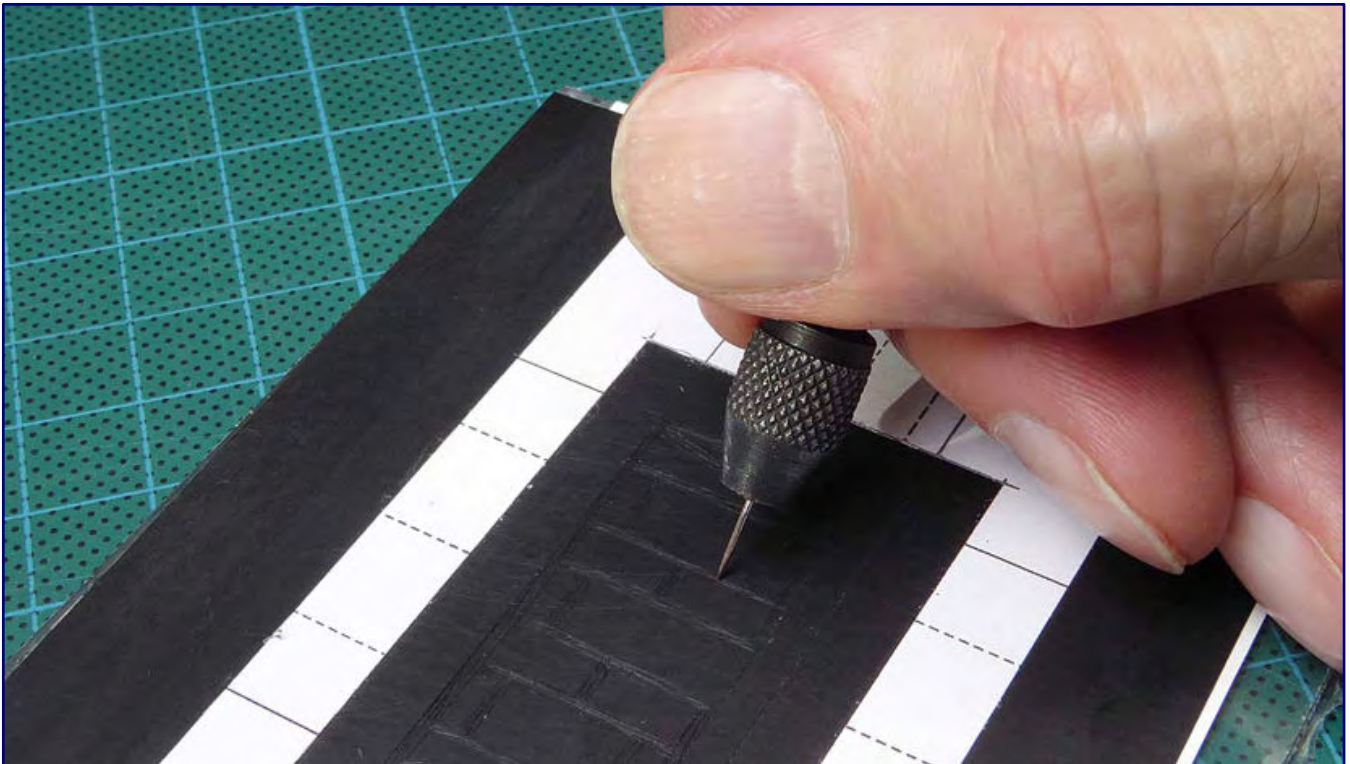
It is now possible to work on the timber frame without damaging the walls. We can either carve a wood grain using a not too sharp pin or create weathered effects with paint.

As the wall remains covered, painting is very easy, and it doesn't matter if some paint ends up outside the timber frame beams. As the vinyl is fully coloured, corrections can also be made easily by scraping off the paint with a sharp knife, like erasing.

Afterwards, all remaining pieces are removed (called 'weeding' among vinyl professionals). The best way to do this is to use a pin vice and insert a needle into it. The skilful removal of these remnants is an important and critical aspect of the foil method for our scale. Attention must be paid to the following:

- The needle should always be moved very flat.
- The needle should not be inserted diagonally into a field to be removed, but parallel to one side primarily. This pushes the film away rather than tearing it out. This not only works better, but also protects the substrate, especially if it is transparent. Once you have made a start, remove the side that is perpendicular to the first stitch and then the remaining sides. But please never try to tear out the remaining piece. If necessary, use a small screwdriver to gently press down neighbouring pieces to protect them.
- Do not pull off the remaining pieces upwards, but, as flat as possible, to the side.
- If the film is very sticky, tweezers can be used to remove it after loosening with a needle. Again, pull the piece away at a sharp angle.

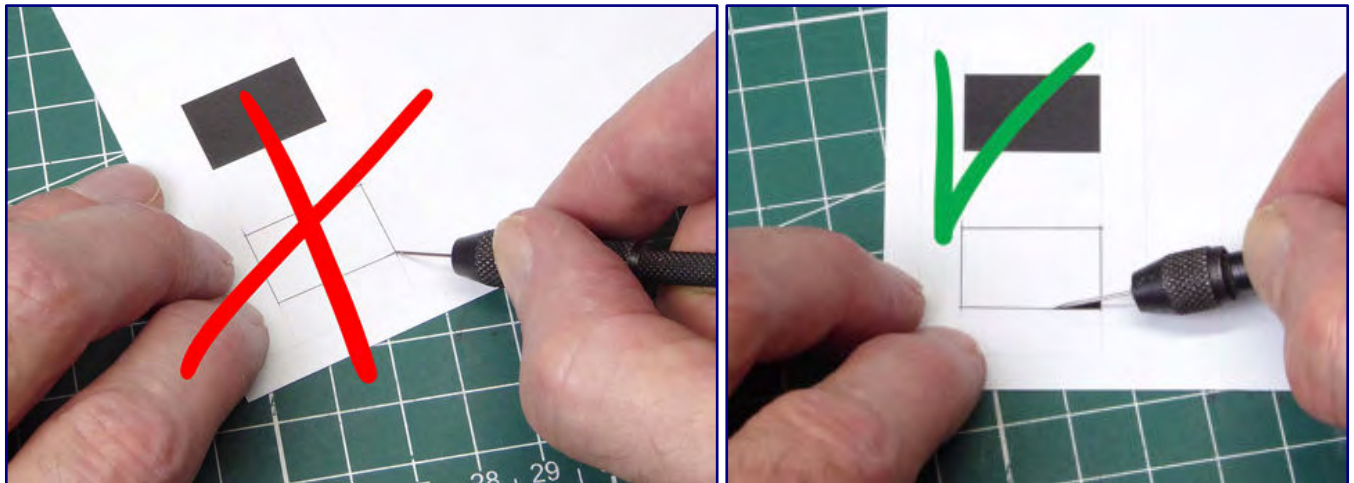
- In the case of very fine elements such as timber framing (half-timber), we ensure that we always work in the direction of the still closed fields. This prevents damage and distortion to the timber frame beams.
- Very small elements can also be carefully held in place with a fine screwdriver or toothpick when removing the film all round.
- If the film to be removed fits snugly around a designed part, it should be cut / divided into several pieces, which can then be lifted off individually.
- If a remnant does not come off easily, it is better to stop immediately and re-cut it if necessary to avoid accidentally removing any of the designed areas.
- It is important at this stage not to remove the outer sides of the drawing with their reference lines yet!



It makes sense to engrave a wood imitation into the vinyl film. A (glazed) patina can then be applied with colours.

Correcting errors, cleaning and details

If the needle was not inserted deep enough when removing the remaining pieces, the vinyl can often be easily pushed back with a blunt piece of wood or mini-screwdriver. Sometimes it is not a problem if a small piece of damage remains visible, as it certainly enhances the wall. In reality, too, there are often uneven areas and plaster damage. If a scratch is too large or deep, we simply present it as missing plaster.



An incorrect (photo left) and the correct blade guidance (photo right) when removing remnants is shown here.

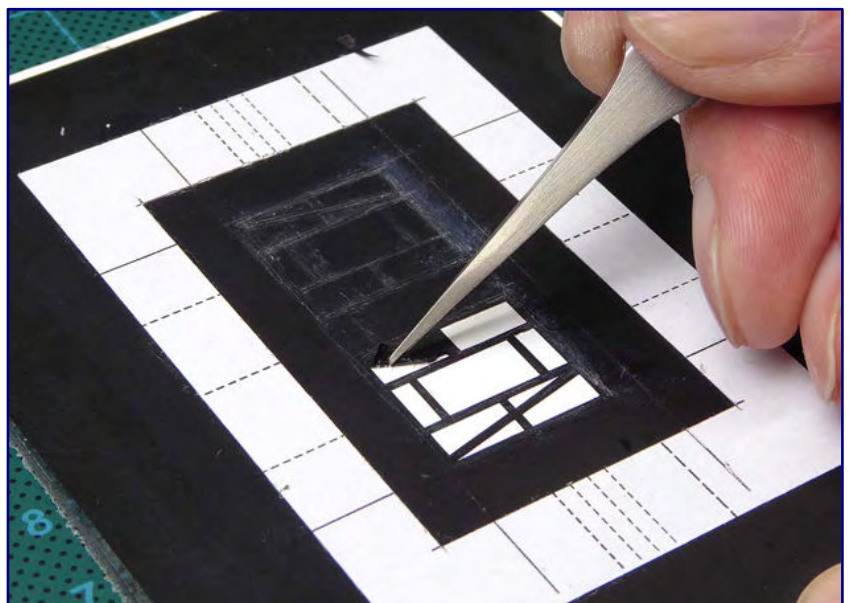
Of course, such damage can also be made invisible using modelling filler. Acrylic modelling filler is ideal, as long as it is sufficiently flexible and water-based. If the cutting lines have not been drawn all the way through to the corners or are not deep enough when removing remnants, the only thing that helps is re-cutting with the tools mentioned.

If it only becomes apparent after removing the drawing that individual cuts are missing, these are simply recut directly on the vinyl. In the case of half-timber framing that has been cut freehand, they are then recut in the same way so that the rustic look is retained.

Beams that are too wide are carefully cut narrower. The same applies to window frames that are too wide. It is more difficult if an element is too narrow: beams that are too thin or damaged are cut out and removed.

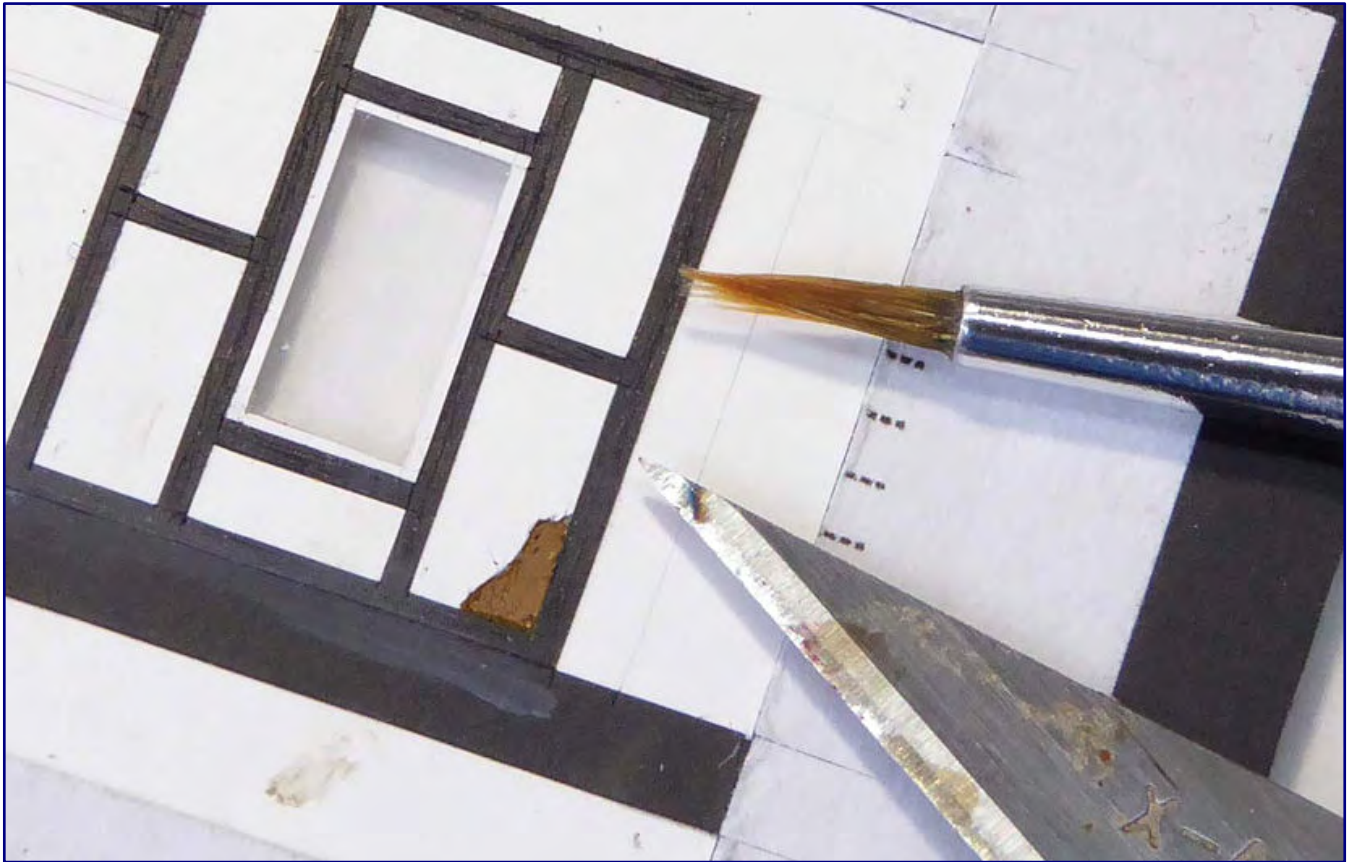
A replacement piece of the correct size can then be affixed individually. This strip should be longer than the missing part. The replacement is applied to the correct position on both sides, overlapping in length, and then shortened to fit.

If a small gap is found between the beam and the timber frame infill, this is only a problem in illuminated buildings. Otherwise, we recommend applying a coat of black paint on the inside to prevent light from escaping.



Carefully lift off the remaining pieces of black, cut film. The white foil is now revealed underneath, reproducing the plastered compartments.

Failed window frames are replaced using the same technique as just described for beams that are too thin or damaged. Tiny pieces of film that no longer adhere sufficiently to the surface are fixed with superglue. We use a strip of paper as a medium to transport the tiny amount of glue with precision, never a finger or the tip of the glue container.



Faulty parts do not always have to be replaced straight away. Here, a piece of vinyl film has been removed down to the base material and repaired with paint. This shows the damaged plaster.

I usually consider small mistakes to be 'happy accidents.' They seem to bear witness to life and time, i.e., the traces that the environment leaves on every real object. Believe me, this has all happened to me several times over the years.

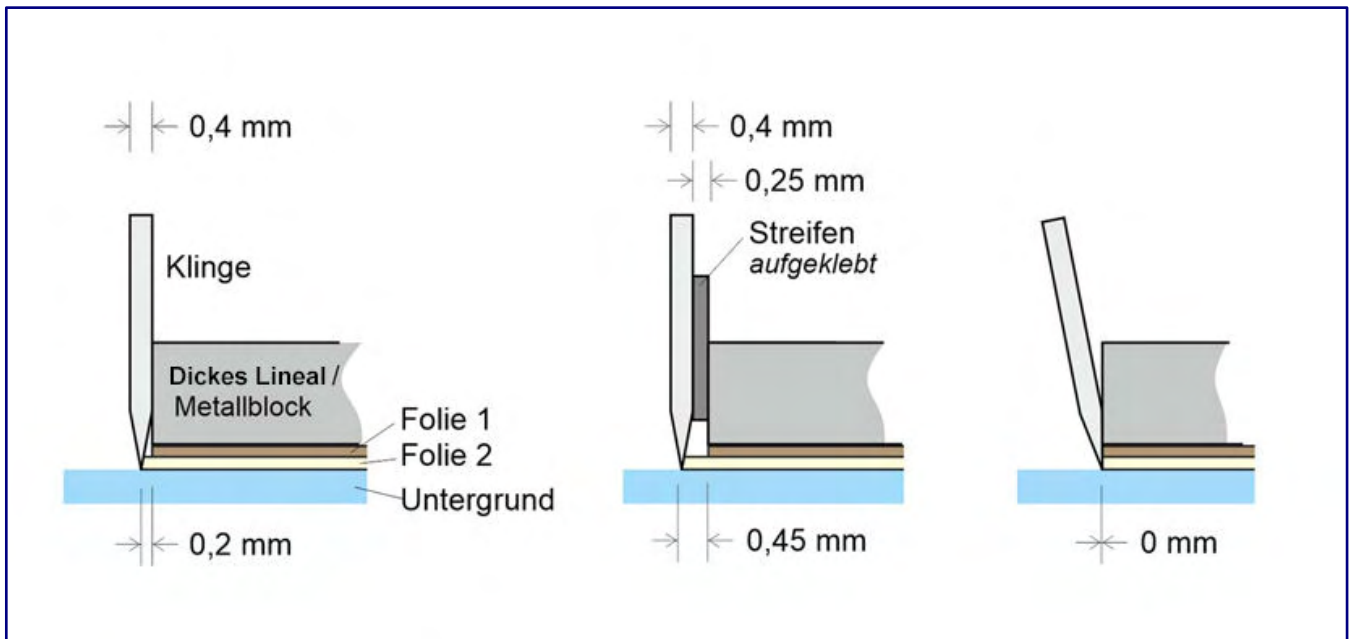
It may now be necessary to remove any adhesive residue. To do this, carefully wipe the surfaces with isopropanol (2-propanol) or cleaner's solvent. A cotton bud is helpful here. With some foils, this step can also be omitted.

The next step is to cut the window frames. To do this, glue a piece of polystyrene to a snap-off blade using superglue. The thickness of the piece determines the width of the frame to be cut.

In my case, the blade is 0.4 mm thick and symmetrically sharpened, so the cutting edge is in the centre at a material height of 0.2 mm. The polystyrene here is 0.25 mm thick. This results in a total cutting width of 0.45 mm.

A sharp-edged and straight piece of metal with a thickness of about 10 mm or more is placed as a ruler exactly along one edge of the window opening on the timber frame. This is relatively easy to do because of the black and white contrast on the timber frame.

The knife is now guided vertically, with the polystyrene piece pressed against the ruler. We now cut all the frames in this way. Before removing the remaining pieces, it is still possible to apply an ageing treatment, as the windowpanes are still covered at this stage. They can then be removed as described above.



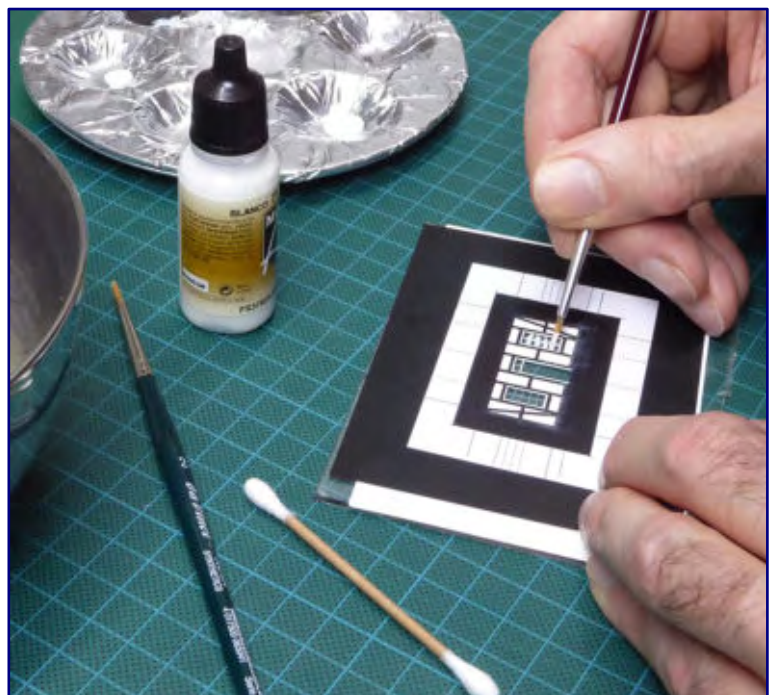
The position of the cutting blade and spacer strip determine the position of the cut on the film.

We then scratch the window braces into the transparent plastic using a needle. The lines of the division are drawn through to the outer edge on the drawing (dotted lines), which helps us now. We work our way from two directions until we reach the side of the frame without damaging it.

We then remove plastic shavings. We then have to decide whether the result is satisfactory or whether the window's mullions and transoms crosses should be re-coloured with white paint. In some cases, water-based white paint is applied and then carefully wiped off with a damp cloth; this only leaves colour in the recesses.

A door is still missing. It is simply modelled directly on the base with paint. If it turns out to be too transparent, the back of the glass plate is also coloured black. Previously, wood grain structures could also be engraved into the substrate.

After engraving the window's mullions and transoms and cleaning the chips, white paint is poured into the newly created recesses.



An alternative is a door made of foil. Cut precisely from foil, it is then glued into the door frame with the flexible mounting adhesive. Wooden structures can also be carved into the foil.

We now remove the black film strip from the underside of the wall to reveal white vinyl. The easiest way is to paint this strip cement grey to imitate a concrete or plaster foundation.

A more ambitious approach is to emboss stone joints using a needle. Reference lines were also included in the drawing for this purpose. Some texture in concrete or stone can be added to the vinyl using 80 to 180 grain sandpaper.



The finished wall rests on its base material. Below the foundation strips, we show the differences between the alternatives: Z scale wall panels from Slaters (above) and Kibri (below), prove to be out of scale.

However, it is also possible to remove the vinyl completely and adhere polystyrene wall strips. Suitable building panels can be found in the Slaters or Kibri programmes, but they are still a little out of scale for Z gauge. A building base made of a grey foil strip is also conceivable.

Finally, it should also be mentioned that you could also reverse the order of the beam and plaster layers. This would replicate buildings where the plaster structure overhangs the beam edges.

continued on page 33

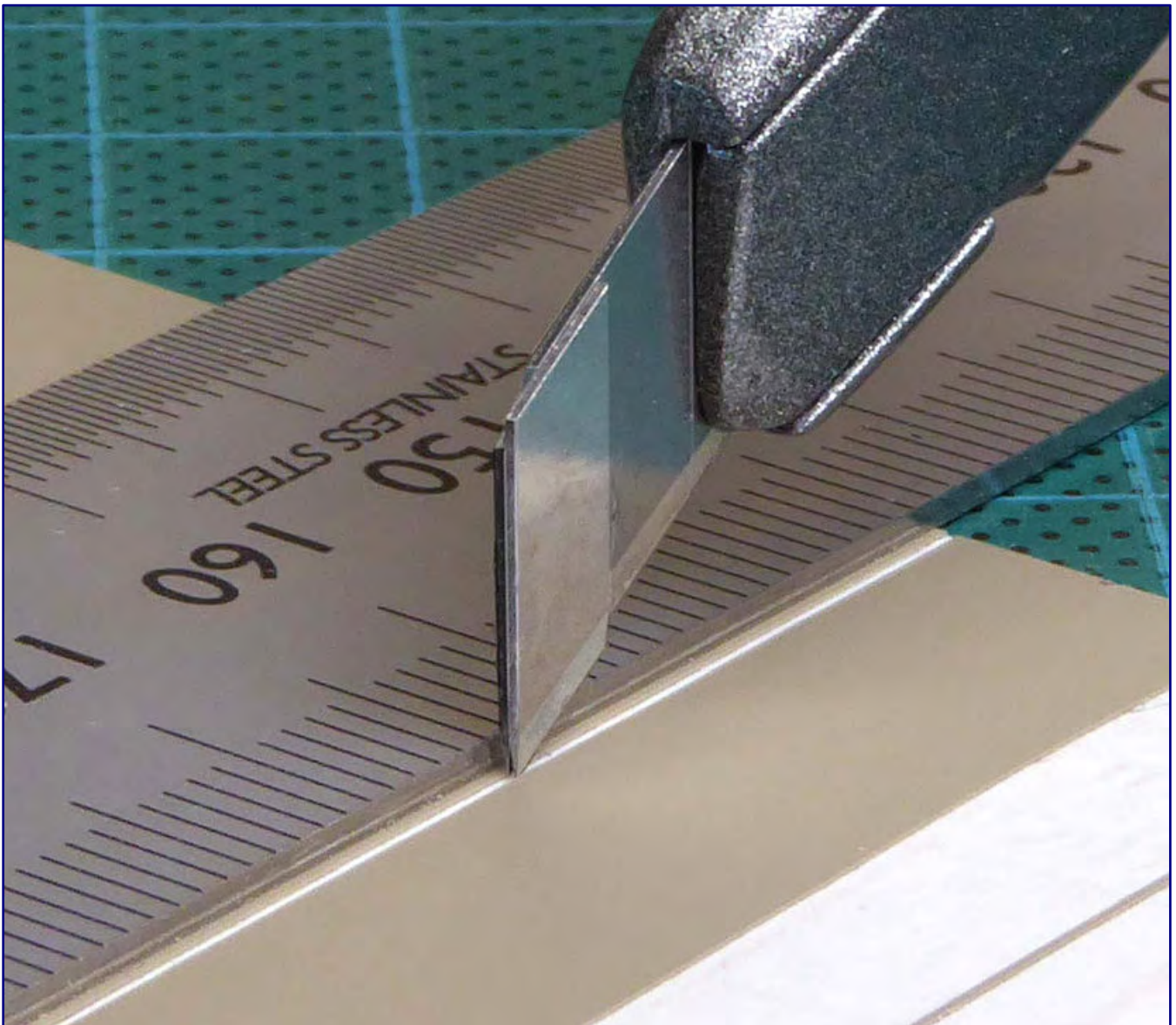
Excursus

Cut, fit, and fix microstrips

If window divisions are not too complicated and you are not afraid of tricky work, vinyl strips can also be used. Firstly, a snap-off blade is prepared. We break off one part of the blade.

This must of course be sharp. The resulting fragment is then glued back onto the blade remaining on the knife using superglue, making sure that the cutting edges are at the same height. In this way, a double cut is made in one movement, which produces a very fine and absolutely precise strip.

Snap-off blades are 0.4 mm thick. This means that two blades glued together produce a 0.4 mm wide strip – fine enough for our window's mullions and transoms.



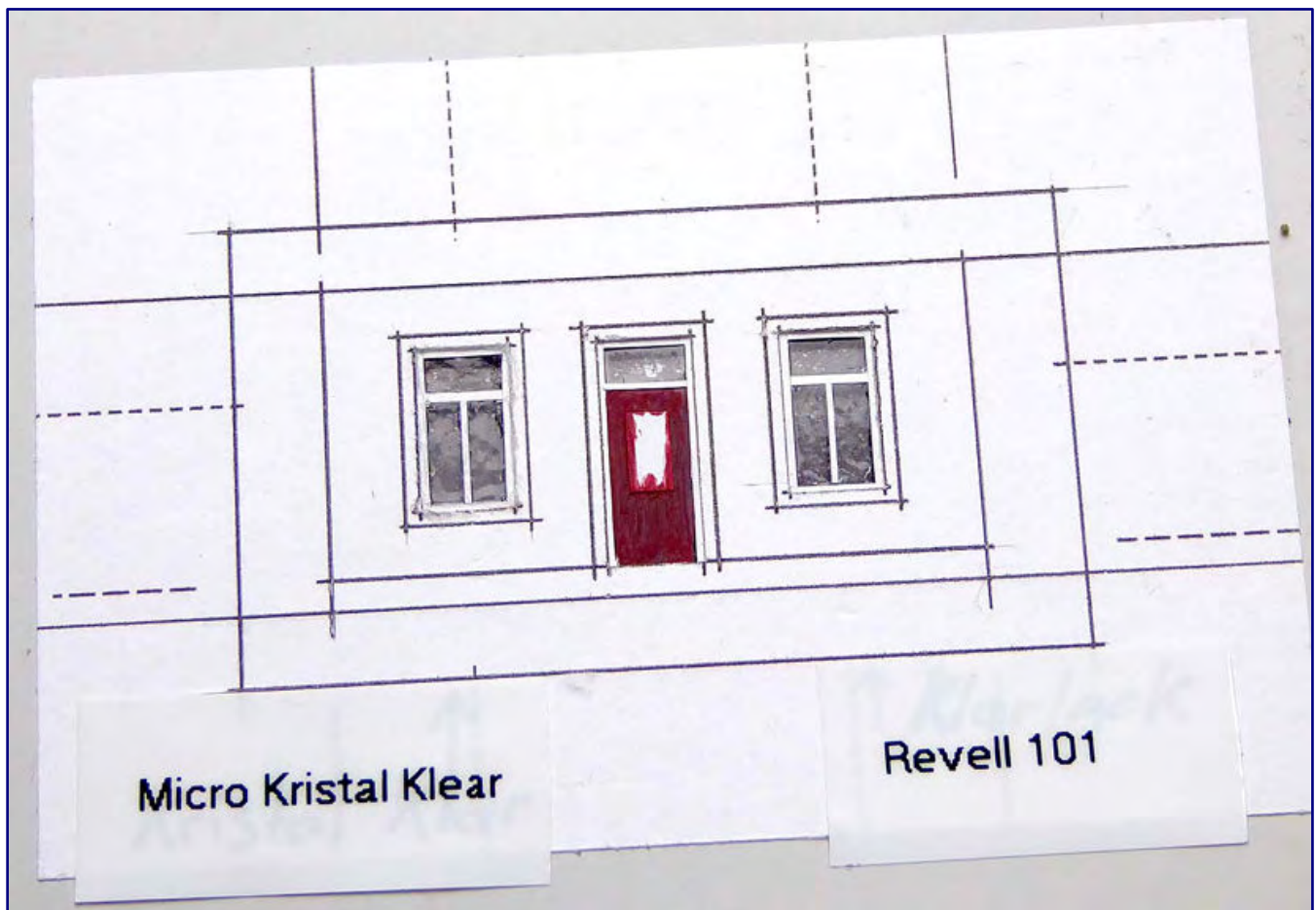
A piece of this strip, longer than the window opening to be covered, is then placed at one end of the window frame. This side is held in place with a finger and then a 'secret weapon' of the Z-gauge model builder is used: a single-edged razor blade. In German, it is also known as a 'Bügelklinge'.

It is extremely thin (0.25 mm) and – nomen es omen (the name speaks for itself) – razor-sharp, which means it can cut very precisely. However, this is even more important because vinyl is soft, and the part is extremely small. This means that it is not pushed aside, as is quickly the case with thicker blades.

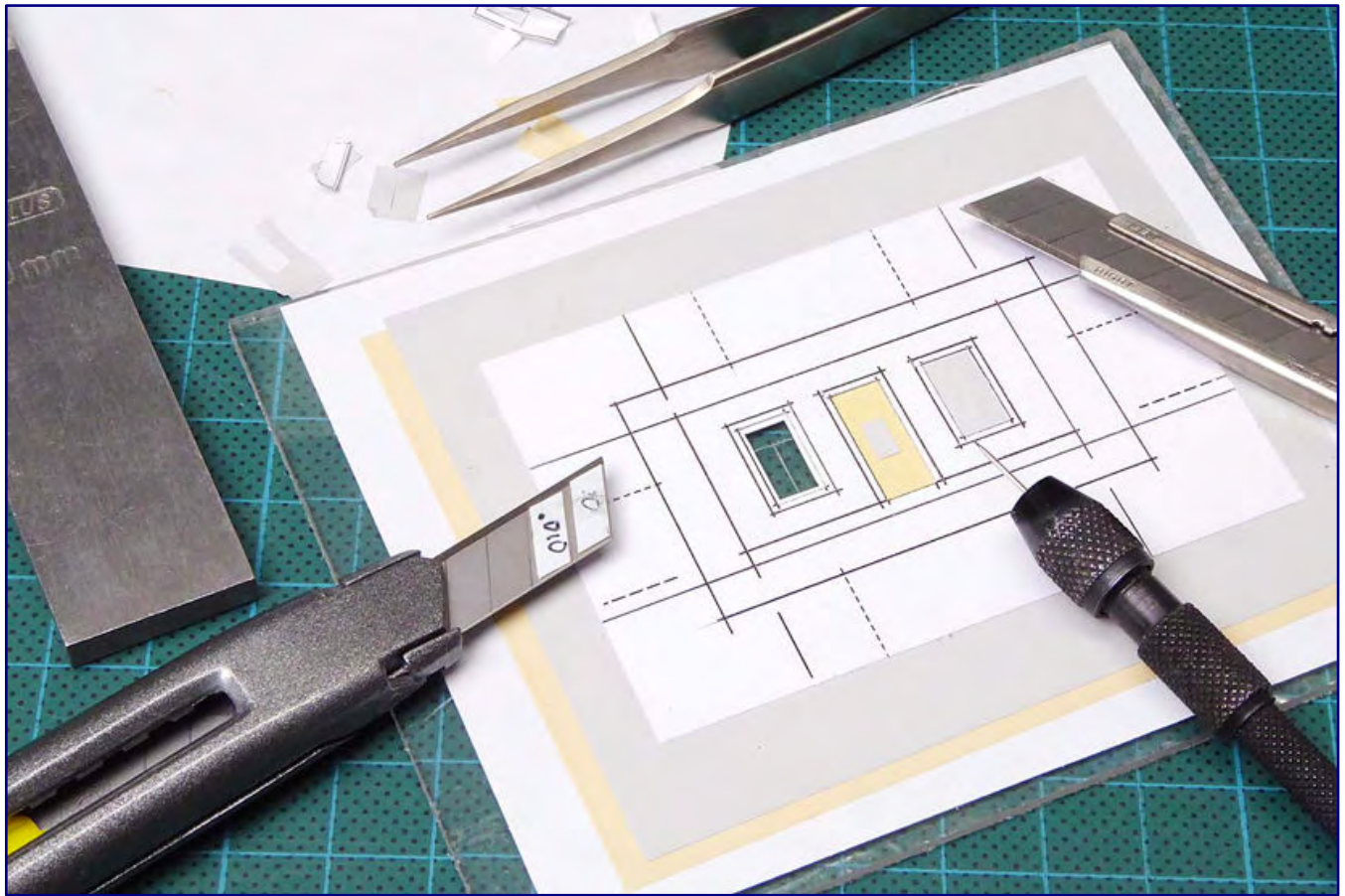
If a small gap remains after cutting, it can be repaired with a tiny drop of white paint. Incidentally, it is not a problem if stripes cross each other on the windowpane. This is not noticeable on the finished building.

However, we are not yet finished after sticking them on. The vinyl strips are so small that they hardly adhere and would shift very quickly. High-gloss clear varnish provides a remedy in this case. We use it to fill the windowpanes or paint the entire window, including the strips and frame.

After drying, the strips are firmly fixed. As the drawing is still glued on, we could also think about spray painting with clear varnish. Hand-applied varnish gives the effect of old glass, which is perhaps better for historical buildings from earlier centuries.



I also tried the well-known Micro Kristal Klear from Microscale (adhesive varnish). However, I didn't like it in this case, because it is too thick to apply.



From right to left: Four work steps, three times cutting and one time engraving, produce the illustrated work result.

Method 1a: alternative

Most of today's buildings do not have timber framing. This is an opportunity to emphasise some aspects that have not yet been described. I will not completely repeat steps that are identical.

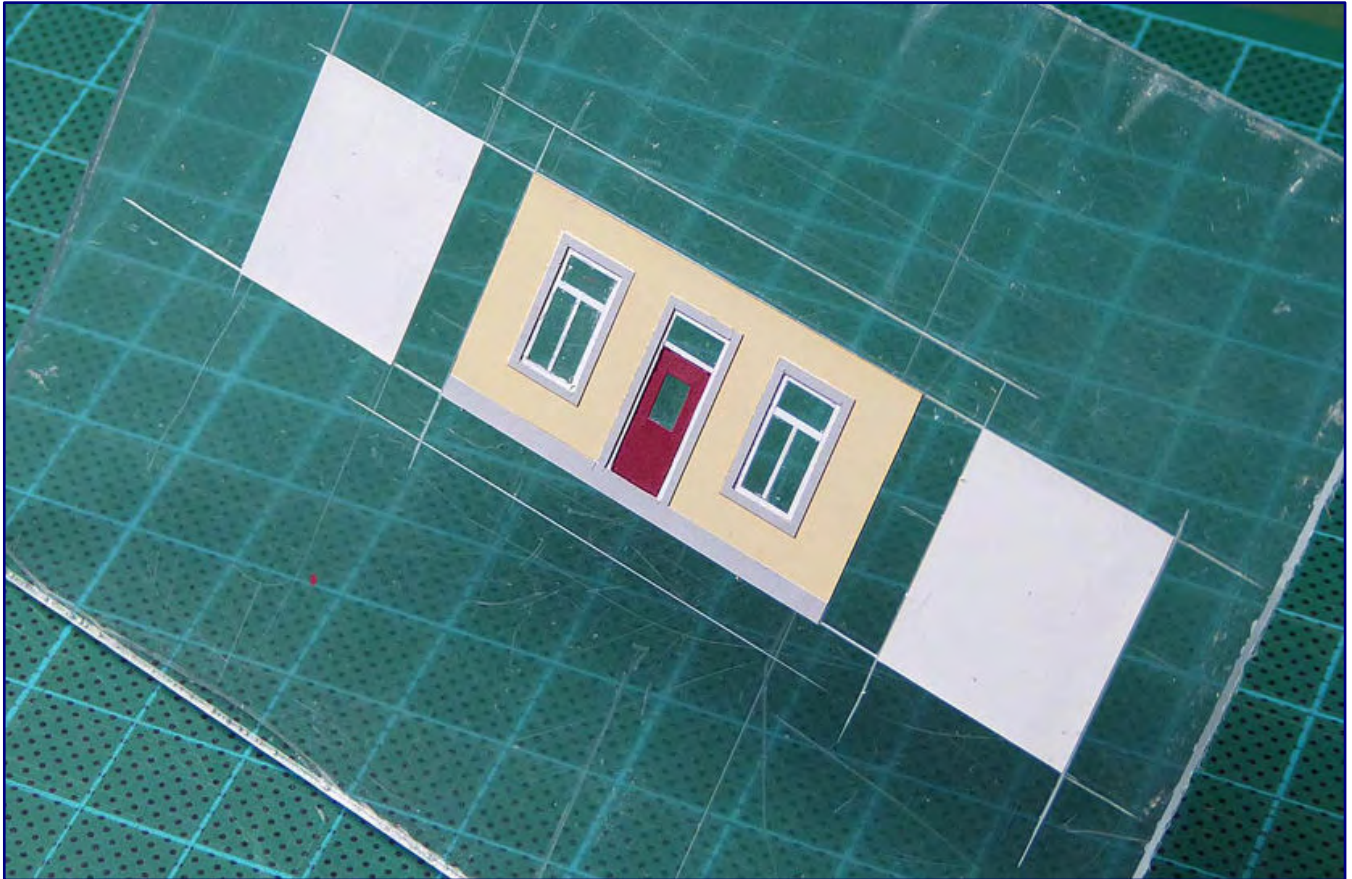
In this case, three layers of self-adhesive vinyl film are applied to the transparent substrate. From bottom to top, these are white, pastel yellow and grey. A copy of our drawing (template) is then applied on top.

Now the window openings are cut out layer by layer so that the windowpanes (the base material) become visible. In contrast to method 1, the drawing is not removed afterwards. Window and door frames as well as the window's mullions and transoms can now be carved. If there are no guidelines on the sides of the drawing, simply place a copy of the drawing under the workpiece. Adhesive tape is then helpful for fixing.

The door is reproduced in colour, as before. This time, however, a piece of the white film remains attached and serves as a mask for a window in the more modern door. Finally, we remove the drawing and the remaining grey pieces.

After finishing the vinyl layers, cut the workpiece to the size of the respective outer wall using a jigsaw. The easiest way to do this is to saw slightly outside the edge and do the finishing work with a file or sanding block.

Thin, transparent CD cover material up to 1 mm thick can usually be notched and broken off in the same way as polystyrene sheets of the same thickness. Thinner polystyrene (0.2 - 0.4 mm) can be cut through completely with a few careful cuts.



The finished plastered wall is ready for further use. The vinyl parts still stuck to the sides can be used as a reference for further work. Cutting lines drawn through the drawing are also helpful for reworking.

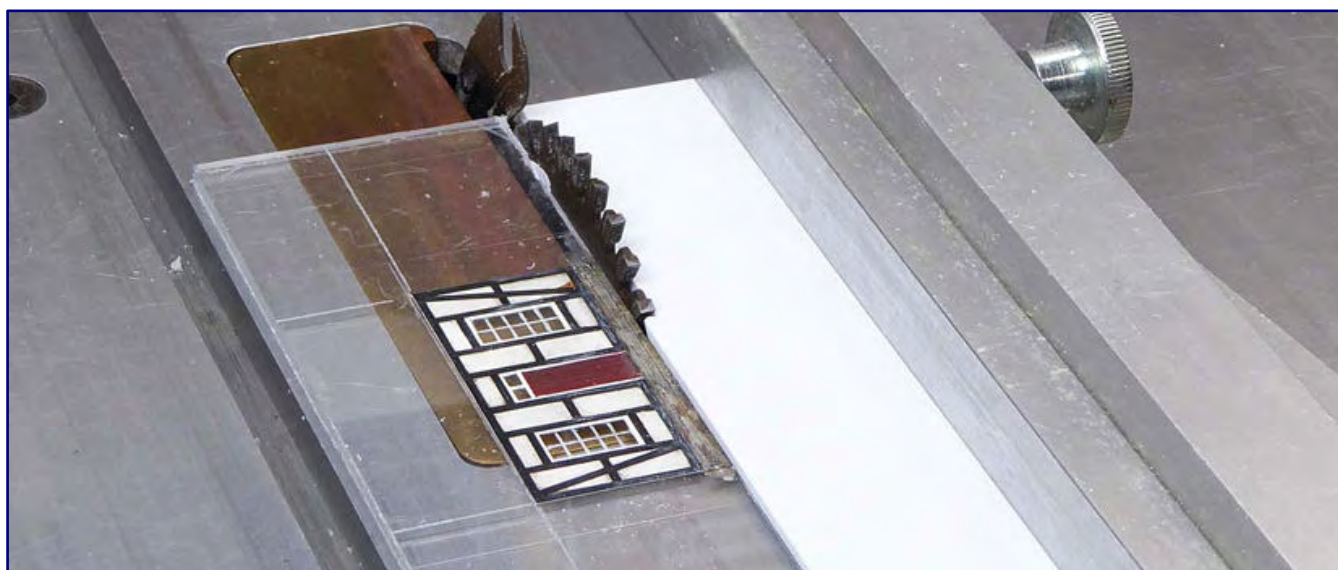
Working with a modelling table mitre saw is a luxury option. As the foil layers and the drawing have been applied freely, i.e., not completely parallel to an outer edge, the question arises: How am I supposed to saw so precisely here?

There is a trick for this. We can simply cut or saw a piece of 1 mm thick polystyrene sheet with parallel sides. It should be pre-cut wider than the distance between the bottom edge of the house wall and the edge side of the substrate.

This piece is then aligned with the underside of the model wall and glued to the substrate. This gives us a reference that we can use for the rip fence of the table mitre saw. The remaining three sides can subsequently be sawn precisely too.

Finally, I create a 45° mitre on the vertical outer edges. I use a file, a sanding block, or the bench grinder for this. It goes without saying that the protective film on the underside of our 'glass body' should only be removed once all this work has been completed.

In the following part 2 of this small series, I will present a second construction method with deep-set windows.



After completing the vinyl layers, the entire part is cut to size using a jigsaw (picture above). An alternative is to use a table mitre saw, but we need an auxiliary strip to align and fix it to the rip fence.

All photos: Reinder Rutgers

Supplier webpages and sources of supply:

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<http://www.peter-post-werkzeuge.de>

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Grenzenloser Modellbahnspaß in 1:220

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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Bunte Fünfziger Rare Motive in Farbe

Carl Bellingrodt gilt als Altmeister der Eisenbahnfotografie. Schon deshalb sollten sich Bücher mit seinen Aufnahmen bis heute auch problemlos verkaufen. Doch längst dürften alle seine Werke bekannt sein – dachten wir. Das vorliegende Buch widmet sich seltenen Aufnahmen, die nur wenig Wiedererkennungswert besitzen.



Udo Kandler
Farbfoto-Raritäten von Carl Bellingrodt
Die DB in den 1950er-Jahren

VGB | Geramond Media GmbH
München 2023

Gebundenes Buch
Format 26,8 x 28,9 cm
192 Seiten mit ca. 200 farbigen Abbildungen

ISBN 978-3-96453-643-3
Preis 49,99 EUR (Deutschland)

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Udo Kandler hat schon viele Bücher veröffentlicht. Sein leidenschaftliches Thema sind jene, die sich mit der frühen Bundesbahnzeit beschäftigen. So hat er auch den Nachlass von Reinhold Palm und dem Hamburger Direktionsfotografen Walter Hollnagel ausgewertet.

Dieses Mal hat der 1958 in Düsseldorf geborene Autor im Farbbildbestand von Carl Bellingrodt, einem Pionier der Eisenbahnfotografie gestöbert und daraus einen weiteren Bildband zusammengestellt.

Wegen seiner Leidenschaft für die Geschichte und Entwicklung der Eisenbahn dürfte der Schwerpunkt seiner Werke kaum verwundern. Sich mit den wenig bekannten Farbaufnahmen von Carl Bellingrodt auseinanderzusetzen, lag irgendwie auf der Hand. Und für den Verlag verspricht dieser Name wohl eh einen risikolosen Erfolg.

Rund 200 kostbare Farbfotos des berühmten Eisenbahnfotografen, die überwiegend, aber eben nicht ausschließlich aus den fünfziger Jahren stammen, hat der Verfasser des Buches ausgewählt. Ergänzt wurden sie allerdings auch um Motive, die bereits in den Sechzigern zu datieren sind.

Den Einstieg in die Lektüre bildet ganz klassisch ein Vorwort. Darin setzt sich Udo Kandler mit seiner Idee und dem Anspruch ans Buch auseinander. Er versucht darzulegen, warum er ein weiteres Bellingrodt-Buch für sinnvoll hielt, obwohl längst alle Aufnahmen dieses Fotografen veröffentlicht sein dürften.

Carl Bellingrodt gilt nun mal als Altmeister der Eisenbahnfotografie in Deutschland. Deshalb sind seine Schwarz-Weiß-Bilder längst durch zahlreiche Bildbände bekannt, zumal er auch zu Lebzeiten selbst veröffentlicht hat.

Erste Versuche mit Farbaufnahmen unternahm er bereits 1939, sie wurden durch den Kriegsausbruch aber jäh beendet. Es dauerte nach Kriegsende dann auch noch einige Jahre, bis er wieder an farbige Bilder denken konnte. Und wieder gehörte er zu den Pionieren.

Kaum ein Fotograf stieg so früh in die Farbfotografie ein und auch Carl Bellingrodt nahm hier keinen radikalen Wandel vor. So blieben auch seine Farbfotos Raritäten, die aber hervorragend die Wirtschaftswunderjahre mit dem Fokus auf die Schiene dokumentieren.

Abwechslungsreich belegen sie den Bahnbetrieb bei der DB in den Fünfzigern und Teilen der Sechziger. Dabei sind selbst manche Blickwinkel für Carl Bellingrodt eher untypisch und überraschen den Betrachter, weil sie neue Einblicke in sein fotografisches Schaffen ermöglichen.

Auch wenn einigen Aufnahmen ihr Alter deutlich anzusehen ist, rechtfertigen die Motive das Berücksichtigen bei der Auswahl auf jeden Fall. Der Autor erläutert eindrucksvoll, in welchem Zustand sich einige der zu digitalisierenden Vorlagen befanden. So hat sich auch Ausschuss ergeben, der nicht im Buch gezeigt wird. Welche Arbeit einfluss, lässt sich erahnen, wenn wir nun auf die großformatig gedruckten Bilder schauen.

Sinnvoll gewählt erscheint uns auch die Gliederung der Kapitel: Zunächst erfolgt ein Abriss des Lebens von Carl Bellingrodt, das sich nicht nur auf sein fotografisches Schaffen beschränkt. Berufliche Tätigkeit, Umzüge und Familienleben hängen eng damit zusammen und werden ebenso wiedergegeben. Eine Chronik am Ende des Werks fasst alle Stationen noch mal zusammen.

Perfekten Anschluss finden dann die Aufnahmen, die er in seiner näheren Umgebung anfertigte. Ausgewählt wurden hier und anderswo im Buch dafür auch Bilder, die nicht nur die Eisenbahn zeigen. Auch Straßen- oder Überlandbahnen sowie Innenstadt- und Bahnhofsbereiche wurden im Bild festgehalten.

Rheinromantik bietet das Folgekapitel. Wo sich Felsen, Wasser, Schiffe und Bahn begegnen, da sind spannende Motive vorprogrammiert. Auf sie folgen Bahnbetriebswerke und die frühen Schienenfahrzeuge der Bundesbahn. So erhalten wir einen Überblick vom Betrieb hinter den Kulissen und dem modernen Erscheinungsbild, das die DB sich geben wollte.

Quer durch die Republik führt das Kapitel „Die Bundesbahn in den Fünfzigerjahren“. Unter dieser Überschrift lässt sich ja alles vereinen, was der Buchtitel verspricht – und hier fanden wir auch die Motive aus den Sechzigern.

Das bunte Bild von Landschaften und unterschiedlichsten Fahrzeugen ist nicht nur eine Augenweide. Es vermittelt dem vordigitalisierten Modellbahner einen unverfälschten Eindruck von Zügen, Zugzusammenstellungen und der Zugbildung jener Jahre, die sie oder er gern im Modell wiedergeben möchte.

Auch sind solche Dokumente vergangener Zeiten eine große Hilfe, wenn das Straßenbild oder die Areale nahe der Schiene authentisch gestaltet werden sollen. Denn kaum einer unserer Leser wird sich exakt erinnern können, wie die Umwelt im Detail wirklich in seiner Jugend ausgesehen hat – und hier gehen wir ja noch viele weitere Jahre auf dem zeitstrahl zurück.

Das Buch lädt also zum Stöbern ein und will entdeckt werden. Wertvoll und sehenswert ist es aber auch schon ohne jeden Hang zur Modellbahn. Eingang ins Werk fanden ja in erster Linie seltener abgedruckte Bellingrodt-Motive, weshalb zu keinem Zeitpunkt der Eindruck aufkommt, all das doch schon mal gesehen zu haben.

Lok-Archiv vervollständigt Ein Reihenschluss

Mit den Bänden des Lok- und Wagen-Archivs hat Transpress einen Klassiker der Eisenbahnliteratur wiederbelebt. Die Bücher waren mit Daten und Zeichnungen stets eine gute Quelle für Modellbahner, die ihre Fahrzeuge umbauen wollten oder auch selbst eines zu konstruieren wussten. So werfen wir heute auch einen kritischen Blick auf die letzten beiden Bände, die das Gesamtwerk nun wieder vervollständigen.

Rainer Zschech
Akku- und Elektrotriebwagen
Deutsche Reichsbahn-Gesellschaft • Deutsche Reichsbahn • Deutsche Bundesbahn
aus der Reihe „Deutsches Lok-Archiv“

Transpress Verlag
Stuttgart 2024

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Treu möchten wir der Vorgehensweise bleiben, die jeweils zeitgleich wiedererschienenen Bände zusammen vorzustellen. Da es sich um unveränderte Wiederauflagen früherer Bücher handelt, erscheint uns eine dadurch verkürzte Rezension völlig ausreichend.

Den Ursprung haben alle Bücher der Reihe in Titeln, die in der früheren DDR erschienen sind und dann auch den Weg in den Westen fanden – vermutlich dürften sie gut als Devisenbringer getaucht haben. So erweiterten die Autoren einst auch ihren Fokus von den DR- auf die DB-Maschinen.

So wurden die Wagen- und Lok-Archiv-Bücher zu solchen mit einem breiten Querschnitt: Sie blickten auf die frühere DRG und behandelten die Nachkriegsentwicklungen sowohl von DR als auch DB. Anderenfalls wäre der Nutzen dieser Lektüre für Bundesbahner auch sehr eingeschränkt gewesen und das Feld durch Alternativen besetzt worden.

Nachdem nach und nach, begonnen mit Dampflokomotiven und ihren Tendern, über Diesel- und Elektrolokomotiven sowie Güter- und Personenwagen alle historischen Bände in den überarbeiteten Fassungen wiederaufgelegt worden waren, blieben nur noch zwei Titel übrig.

Diese liegen nun vor uns und technisch sinnvoll gegeneinander abgegrenzt worden. So sind auch die Triebwagen insgesamt zwei Büchern zugeteilt worden: Akkumulator-Fahrzeuge wurden den ihnen nahestehenden elektrischen Triebwagen mit Stromabnahme aus der Oberleitung zugewiesen, die wenigen Dampftriebwagen ließen sich der Dieseltraktion zuordnen, ohne jenen Band zu überladen.

Beide Titel grenzen die behandelten Baureihen aber nicht ganz richtig ab: Es ist keinesfalls so, dass der Inhalt erst bei der späteren DRG, also etwa um 1920, einsetzt. Einleitend finden wir in beiden Werken eine Zusammenfassung der Entwicklungsgeschichte, die auch Meilensteine der Länderbahnen aufgreift und Zeichnungen dieser Triebwagen präsentiert. So fanden sie auch ohne einen Steckbrief ihren Platz und vervollständigen den Inhalt in sinnvoller Weise.

Wären die nur noch antiquarisch erhältlichen Büchern aus den Neunzigern noch mit einem festen Einband versehen, hat sich Transpress bei der Wiederauflage für die Form des Taschenbuchs bzw. der Broschüre entschieden. Hinsichtlich der Einbandgestaltung wurde eine einheitliche Linie gewählt und beibehalten, womit alle Bände auf Anhieb als zusammenhängende Reihe erkennbar bleiben.

Vorbei ist die Suche in Antiquariaten, die bestenfalls über viele Suchen und mehrere Jahre zu einem Gesamtwerk führte. Allerdings dürfen wir auch nicht vergessen, dass hier 30 Jahre alte Daten und ebenso altes Wissen unverändert nachgedruckt wurde.

Die Zeichnungen (Maßskizzen) von damals haben nicht an Wert verloren und erfüllen auch heute ihren Zweck ohne jede Einschränkung. Die ebenso weiter verwendeten Bildaufnahmen erleichtern das Einordnen der Baureihen, wären heute bei einer Überarbeitung aber sicher in bunter Wiedergabe gewünscht.

Die Schwächen der gesamten Reihe zeigen sich leider bei diesen beiden abschließenden Büchern am deutlichsten: Fehlten zuvor natürlich auch die modernen Entwicklungen der Diesel- und Elektrolokomotiven, so blieb deren Gesamtzahl noch einigermaßen überschaubar.

Im Bereich der Triebzüge und Triebwagen hat sich hingegen weitaus mehr getan, weil nicht nur die Deutsche Bahn AG, die hier komplett fehlt, verstärkt auf diese Fahrzeuge setzt. So ist das modernste Fahrzeug der elektrischen Traktion der ICE 1 in Serienausführung. Selbst der heute schon etwa betagte ICE 3 der Baureihen 403 und 406 erschien bereits mehr als fünf Jahre nach der letzten Aktualisierung.

Bei den Dieseltriebwagen sieht es nicht anders aus: Den Schlusspunkt markiert der „deutsche Pendolino“ Baureihe 610, der bereits vollständig ausgemustert ist. Alle Triebwagen, die in der Folge der Neuordnung des Nahverkehrs ab 1994 von der Bahnindustrie entwickelt wurden, fehlen komplett.

So erhalten wir hier keine Informationen über den Regioshuttle RS 1, den Siemens Desiro Classic oder auch die erfolgreiche LINT-Reihe von Bombardier. Selbst die Neigetechnik-Züge der Baureihen 611 und 612 suchen wir hier vergebens.

Dessen sollten sich die Käufer der Bücher bewusst sein. Mit einem nur historischen Interesse sind sie hingegen gut bedient und werden sich über die gelieferten technischen Daten ebenso freuen. Da tritt auch unsere Beobachtung zurück, dass der mit 364 Seiten pralle Band zu Dampf- und Dieselfahrzeugen, rein historisch bedingt, mit gerade mal 179 Bilddarstellungen auskommen muss.

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: <https://www.motorbuch.de>
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The On traXS in Utrecht Show of Superlatives

'Het Spoorwegmuseum' in Utrecht is no longer an insider tip for railway fans. The annual On traXS event held here has also contributed significantly to this. This year's 15th edition included a Z gauge layout for the third time. So, we also travelled to the Netherlands to take part in this small anniversary.

The annual 'On traXS' exhibition at the Spoorwegmuseum Utrecht (Netherlands) is a very special show. And this is not only due to the fantastic layouts, but also the special backdrop of historic railway vehicles and the format of presentation that is so special to this museum.

There is no doubt that this museum is well worth a visit even without a model railway exhibition. The historic 'Mailebaanstation' railway station has been modified to meet the new requirements, and additional halls have been added for the new purpose.



Thirty minutes before the exhibition opens, long queues form at the entrance to the former Maliebaan station: the On traXS 2024 attracts model railway friends from near and far.

In addition to the outdoor area, which also houses a turntable, a functional mechanical signal box, a coaling station, and small warehouses, these are used to store the various prototype vehicles. The railway history of the Netherlands from its beginnings to the present day is documented there.

There is even a glimpse into the possible future of the railway. A visit becomes a family experience thanks to the various adventure worlds that bring the exhibits to life. For example, a 3D rollercoaster is used to travel through different countries and times on the railway.

The 'Stalen Monsters' ride, in which the spectator travels in a gondola through a maintenance workshop, looks like a ghost train. Here, the effort and hard work involved in keeping steam locomotives operational and repairing them can be followed.



The atmosphere of model railways amid historical railway exhibits is unique, which is impressively demonstrated by the large crowds. The Mitropa stand can be seen at the front right, where modelling demonstrations are given, and questions answered. Photo: Nicole Späing

'The ground breaking invention' also takes us on a journey through time: We get into a lift and travel deep into an English mine of the early 19th century. We experience how lorries were put on rails to roll better. Eventually, people realised that iron wheels on iron rails had less rolling friction.

As we step out into the daylight, we see one of the first steam engines, which uses a lever to drive a pump to drain the shafts. The railway had not yet been invented, but the idea of putting the steam engine on wheels and rails was not far off.

As we continue our way, an arrow labelled 'Longridge' points us to our destination: we come across the workshop of Michael Longridge, George and Robert Stephenson. It was here that pioneering steam locomotives were constructed that heralded the success of the railways on the continent.

In the engineering office and workshop, this period in the first half of the 19th century is brought vividly and seemingly to life. The legendary 'Rocket,' which won the Rainhill railway race, can be seen here as a model. The German 'Adler' and the first steam locomotive in the Netherlands also originate from this site.

In 1839, it was still running on a broad gauge from d'Eenhonderd Roe station to Haarlem, and it marked the beginning of the railway era in our neighbouring country. As we step out of the workshop, it immediately catches our eye: The station is festively decorated and 'De Arend' is waiting in front of the building on the track, ready to depart with the inaugural train. The first steam locomotive in the Netherlands was rebuilt in 1938 and is one of the most beautiful pieces in the railway museum.



We were just in the locomotive workshop at Longridge, but now we are standing at the station and taking part in a historic event as we look over the standing boiler of the "De Arend" ("Eagle"). Photo: Nicole Späing

There are a few more themed worlds to discover, but we have now introduced you to the most spectacular ones in terms of presentation. We would now like to take you on a short tour of On traXS 2024.

Like a championship

Exhibitors cannot apply for On traXS; they are invited. Throughout the year, representatives of the organising 'Team Mitropa' travel around the country and abroad to discover and examine suitable exhibits.

Ideally, anything that fulfils the requirements of this exhibition and has not yet been shown in Utrecht – this also applies to other trade fairs and exhibitions in this city – will then lead to an invitation being sent to the builder of the piece.

Preference is given to installations of a manageable size with their own lighting that can be easily integrated into any part of the museum. The peep-box system is perfect because it provides a fixed viewing side and shields from interfering and stray light. These preferences appeal to the public, but also fit in best with the overall scenery on site.

Installations that work with illusions and can simulate great depth for the viewer are always very popular. This only works from previously defined angles because the design runs towards a fixed vanishing point that must follow the viewer's gaze.



“Rijk von Nijmegen” created the perfect illusion of a kilometre-deep layout, with trains, lorries, ships and even a zeppelin moving in the rear areas. Accordingly, the spectator prize also went to its builder Loek Bronkhorst.

The perfect example of such a concept is 'Rijk von Nijmegen', with which builder Loek Bronkhorst won this year's On traXS. He has repeatedly amazed visitors with this concept in many ways, but the depth effect of this exhibit surpasses all previous ones by far. He has amazed visitors with this concept in many different ways, but the depth of this exhibit surpasses all previous ones by far.

In any case, the highest possible quality of landscape design is mandatory for a selection for this exhibition. It is therefore fair to say that On traXS can be seen as a European championship of model railway construction. Anyone who has something new to offer and is invited to participate will certainly not turn it down.

It is not possible to honour all the presented layouts in this report, as we would lose the focus of this magazine. Instead, we would like to highlight a few representative examples that present as colourful a picture as possible and are particularly appealing to us for various reasons.

Let's start with 'Ontario' by Maurice Kleverwal. It is not only the beautiful Canadian forest scenery that generates excitement in this case, with the 'Northlander' passing through, formed from former NS and SBB TEE multiple units, a train that is very popular in Europe.

A dam formed by tree trunks in the river, which crosses under the railway, is sure to catch the eye. Beavers have clearly "shaped" the landscape here and three of the rodents and the beaver lodge can also be recognised nearby. We spot wolves on the rocks above the watercourse. This gives the impression of unspoilt nature.



Maurice Kleverwal's "Ontario" layout is home to the legendary Northlander. Its wonderful natural scenery behind the bridge is formed by a beaver lodge and dam, complete with rodents and wolves howling on a rock.



Scene with figures on “Merzebach”(photo above) and manual unloading of coal from an open PKP wagon on “Wysoka Gorzowska” (photo below).



French small-town scene on “57 bis rue Eiffefe la Suite” by Henri François Joyau (photo above) and “Ode aan de modelbouwer” by Huib Maaskant (photo below), where Z gauge depicts a garden railway.

The 'Merzebach' layout realised in scale 0 by the Vames club from Apeldoorn also seems extraordinary to us. With a length of around 11 metres, it is probably the largest at the exhibition, and with many of its own constructions, which by no means only concern the buildings, it is also something special. Its branch line motifs fit in very well with the '75 years of the German Federal Railway,' which would be celebrated this year.

A title favourite from our point of view is also the 'Wysoka Gorzowska' by Tomasz Florczak. We have captured a very nice loading line motif from this layout, which benefits from its harmony of landscape and railway with working life. In fact, it took second place in the spectator ranking.

Henri François Joyau's '57 bis rue Eiffefe la Suite' layout features a variety of small French town motifs, which he presumably found in his home country. Here, too, it is always worth letting your eye roam the streets to discover delightful little scenes.



"Witlov" by Marc van Nieuwenhove offers a peep box with two different stages, here the back with the Campenhout brewery.

The small railway depicted on "Witlov" by Marc van Nieuwenhove offers a peep box with two different stages. Realised in 0m gauge, the tram-like trains and a small locomotive with goods wagons run through different streets of the town, which is based on the prototype.

Z gauge can also serve as a model railway on a model railway. This is particularly evident in the showpiece "Ode aan de modelbouwer" by Huib Maaskant, presented in a picture frame. Here, Z gauge vehicles serve as locomotives in a park railway format, which the operators use tools to run up the track or drive past the house while the driver sits on the first carriage behind the locomotive.

And that brings us to the real reason for our trip: Another Z gauge participation has lured us to Utrecht. Dietmar Allekotte's small "Waterkant" peep-box layout with railway station, lighthouse, jetty and sandy beach section, which was presented last year at Intermodellbau, also found its way to On traXS.



A class 64 tender locomotive stands with its silver locomotive at Sieltief station. The layout of the same name was Dietmar Allekotte's contribution to the exhibition.

In terms of design, it is perfectly capable of keeping up with the other exhibits on a larger scale, but we must also honestly note that the themes unfortunately appear to be repetitive. After all, this is the fourth time that the builder has presented a layout that interprets the theme 'branch line on the North Sea coast' and it always includes the same design elements.

This is not to belittle this beautiful work, as it has been successfully realised. We have therefore shared our impression with Dietmar personally. After all, such repetitions can also lead to a model railway artist being reduced to a single theme. And then his creative skills are in danger of only being recognised to a limited extent - and that would certainly be a shame.

A farewell anniversary

We have always admired the shunting game of Jan Reijnders, who has been performing at exhibitions as 'Meester Jan' for many years. He attended On traXS for the 15th time in 2024 and has therefore attended every year of this event.

But the retired teacher, who is happy to give us an interview about his way of working with young people, tells us that it must end at some point. This 15th participation is to be his last and we are certainly not the only ones who will miss him.

We don't yet know whether anyone will continue his work, but this fantastic and well-received programme certainly deserves it, just as it deserves to be copied by other clubs and groups. What will definitely remain is the house construction offer at the exhibition, where children can gain their first modelling experience.

The children's layout is also very popular: the little ones can run various trains from the Märklin-my-World programme with the hand lever remote control on different routes. And the children are busy and focussed.

They move their high-speed trains quickly along the tracks, but we don't see any accidents despite the many protagonists. Showing consideration and slowing down from time to time are obviously the order of the day, even without the intervention of adults.



Jan Reijnders, alias Meester Jan (left), who has been attending the event from the very beginning, has now announced his retirement. Trainini® helper Kristin Späing (right) received one of the last shunting diplomas he was to issue.

What we see here makes us happy about the next generation of model railway fans, but also sad when we think about how an On traXS without Meester Jan will look. We look forward to seeing an equally committed successor next year and very much hope that this idea will continue.

By the way, various model railway and accessory manufacturers also take part in the event with their regional sales companies. These are joined by smaller suppliers such as Artitec or classic small series manufacturers. This means that there is always something on offer for our scale that is not yet widely known.

continued on page 52





Around the Mitropa booth, some outstanding works were on display. Here, for example, you can see how realistic freight wagons with traces of use and repair patches look to the observer.

Photo on page 51:

Locomotive 9902 from RXP is now advertising a children's book with a railway theme, and, therefore, fitted perfectly into the exhibition as a guest during On traXS.

Our contact at the museum suggests that we come back in 2025. This is an easy decision for us, as the museum and exhibition have their own special appeal. And if the Z gauge can establish itself here permanently and annually, then the deal should be clear: In this case, we will also become a permanent guest of On traXS!



Webpages of the railway museum:

<https://www.spoorwegmuseum.nl>

Commercial exhibitors with Z gauge relevance:

<https://artitec.nl>

<https://mbrmodel.eu>

<https://www.rts-greenkeeper.de>

<https://scenery.shop>

Report on Trainini TV (Episode 26):

<https://www.youtube.com/TraininiTV>



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Z 44500 **Feuerwehr** NEU



Z 44501 **Bauarbeiter** NEU



Z 44511 **Rangierpersonal** NEU



Z 44510 **Bahnpersonal** NEU



Z 44520 **Reisende** NEU



Z 44521 **Sitzende** NEU



Z 44530 **Badende** NEU



Z 44531 **Bergwanderer mit Gipfelkreuz** NEU



Z 44542 **Pferde** NEU



Z 44540 **Kühe** NEU



Z 44541 **Hirsche** NEU



Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

The topic of "pantographs" is moving:

I'm referring to the mail from reader Dietrich Frank (it's a coincidence that we're both from Düsseldorf; I don't know Mr. Frank) on the subject of catenary lines and pantographs and can only support him, but, also, Mr. Pfannenschmidt (on the same subject).

I was just as disgruntled about the last Trainini (February), but I think almost more so about the current March issue. I've had my Z scale for just over 40 years and my first locomotive was a BR 103. The overhead line was added as quickly as possible as a matter of course, because anything else would not be prototypical in my eyes.

Over the years, my focus has remained on electric traction. Steam makes up 5 % of my traction, diesel about 15 %. All lines used by the electric locomotives are equipped with catenary lines, even in the depot with transfer table and over sidings.

Both the catenary line and the pantographs (always in contact with the catenary line!) have worked perfectly throughout the entire time (over 30 years with electrical function), and I have never had to replace a pantograph after derailments, etc.



Märklin itself has also taken steps to improve the appearance of the pantographs, but has not retained them: On the right, the class 189 (item no. 88191) with red lower shears, which we had chosen as the best new release of the year 2008, on the left one of several successor models without this feature. Photo: Rainer Hecker

I find the comparison with Rokuhan inappropriate on this point, in particular. I also have a BR 181, but the pantograph has no contact with the catenary line. In my view, this is a weak point, not to mention the lack of function. I also don't want to imagine what would happen if the pantograph were to get stuck somewhere.

In my opinion, if you want to advance Z gauge in terms of prototypes, Märklin could have continued on the path it took years ago. One of the first variants of the BR 189 not only had very precisely and flatly aligned pantographs, but the lower scissors were also red. On a more recent model, the pantographs are of the same design, but not quite as precisely aligned and do not appear as fine (see photo).

On the other hand, Sommerfeldt's direction is also very constructive. My Ae 6/6s are fitted with Sommerfeldt pantographs, which fit the locomotives well and are also stable, but also somewhat more delicate. Finally, the pantographs are also made of metal and not plastic. To be honest, I have never seen a prototype locomotive with plastic pantographs! ;-)

P.S.: I would also be interested to know how representative the dealer survey was that led to the verification?

Rainer Hecker, Düsseldorf

Editor's reply: Apart from plastic pantographs, we have not yet seen any locomotives in the prototype whose superstructure was made entirely from such a material. We would like to add a portion of this, because the prototype and model are hardly comparable in terms of design.

We also like the Sommerfeldt pantographs very much, which we have also communicated to the manufacturer. At the same time, we suggested that the DBS 54 pantographs, which are so typical of German locomotives, should also be constructed to scale with the correct scissors. We see great sales potential here as replacement parts.

However, such components are always a subject that is judged very differently, depending on personal preferences and personal taste. Readers, who are now presumably silent out of approval, have often expressed their views to us on other occasions and in previous years, which we have set out and defended in this report.

With regard to the representativeness of the dealer contacts, we would like to point out that we focussed on a few companies with higher sales, which together probably generate around 50 % of German Z gauge sales. The information was clear and unambiguous and also reflects the findings from discussions we have had with the manufacturer in the past. The main figures used here were metres of track and catenary line sold.



Jan Reijnders (right) in an interview with Holger Späing for Trainini TV. Photo: Nicole Späing

Feedback on episode 26 of Trainini TV:

I watched the film with great interest and pleasure. I'm glad your daughter found it very entertaining. For me, it was the last time I visited On traXs as Master Jan.

I've been there from the start and have issued almost 1,700 shunting diplomas in total. There is a detailed report about On traXs 2024 in the magazine "Railmagazine," including photos of me surrounded by lots of children in action on the children's track.

Perhaps this is also of interest to you. Before the On TraXs awards ceremony took place, I was honoured by the director of the railway museum for my contributions to On TraXs. Many thanks for the film.

Jan Reijnders alias Meister Jan, Eersel (Niederlande)

Editor's response: Dear Jan, you have definitely earned this award and we are delighted that your work has also been explicitly recognised by the museum. Personally, we very much regret not seeing you in your familiar role at this exhibition. But, at some point, it has to end and, of course, we fully understand that. So, it is now up to us to take this opportunity to thank you for your incomparable commitment to young model railway enthusiasts, and to wish that you will find many imitators.

New from Yellow Dwarf:

Yellow Dwarf from the Czech Republic is providing storage accessories in 1:220 scale this month. The "Shelves III" (item no. 60244) are suitable for storing various goods (on shelves to be inserted), and there are also the "Storage Accessories I" and "Storage Accessories II" (60246 / 60247) sets. Behind these are loose Euro pallets and pallets that, with the exception of one, are loaded with sacks.



The inventory programme also includes various bag loads, which are also stacked on pallets for transport.

However, these show different layering states and are, therefore, well suited to making the processes in commercial operations colourful and varied together with the new products.

The manufacturer can be contacted at the following address: <https://www.yellowdwarf.eu>.



Yellow Dwarf is constantly expanding its product range, which is why several products and solutions are already available for individual topics. New this month are "Shelving III" (item no. 60244; photo top left) as well as "Storage Accessories I" and "Storage Accessories II" (60246 / 60247; photos below). Photos: Yellow Dwarf

Märklin deliveries in April:

Märklin delivered a class 41 steam locomotive with a new boiler (item no. 88277) during the reporting period. The 41 069 from the Osnabrück Hbf depot (Münster depot) was reproduced in its summer 1960 operating condition.

In keeping with the prototype, this locomotive bears replica aluminium plates with so-called "wide digits" in the Pr VII 35, 2nd edition font, which also applied to some other Bundesbahn locomotives for a long time.



The class 41 coal (item no. 88277) that has now been delivered is a design variant that differs from the previous oil engines not only in the tender. Among other things, this locomotive does not have a steam bell, in keeping with the prototype.

The two raised sandboxes on each side of the car are incorrect, this arrangement belongs to the 03¹⁰ series, from which Märklin derived its model.

It is also interesting that Märklin has reproduced a chimney without a chimney cap on this model. This was to be observed more frequently on newly-boilered steam locomotives with coal firing on the DB, while their oil-fired sisters were fitted with a chimney top. However, this affected remarkably few class 41 coal-fired locomotives and we were unable to document this for the 41 069, which was taken out of service in Emden as early as 1970.



With the SBB Re 6/6 (88240) in red livery, a new design was also introduced to the specialised trade in April. This locomotive, which is very important for Swiss rail transport, was already presented with the spring 2022 new products.

Fortunately, however, the 2'2" T 34 tender has been reworked or moulded from a different mould, as this time the three lanterns on its rear are much narrower and closer to the prototype. The modelled coal supply has remained unchanged. In this model, too, the front edge of the cab has not been painted red, which somewhat detracts from the "face" of the locomotive. All in all, this is an attractive model with minor flaws and compromises.

The Deutsche Bahn AG three-phase locomotive 120 140-9 (88529), a limited-edition model in orient red livery, was delivered shortly before. It shows the well-known advertising lettering "Nice here. But have you ever been to Baden-Württemberg?" on the long sides.

This locomotive corresponds technically to other, current editions of this series and, according to the manufacturer, was produced in a limited edition of 100 units. It was not or not yet to be found in the product database at the time of going to press.



An attractive new product and splash of colour on the layout is the "DB Netz with BR 120 electric locomotive" train set (81528), which was not yet fully available at the time of going to press.

The SBB class Re 6/6 electric locomotive (88240) in a red paint scheme is now also available in speciality shops. This new model, which was a long time in coming, will be presented in detail in the future because of its chassis design, which differs from all previous Märklin vehicles.



Märklin's track planning programme in version 11.0 is based on the well-known Wintrack.

Only a few words need to be written about the "DB Holiday Train" (81304) train set, which comes from familiar moulds: The class 140 locomotive in chrome oxide green differs from the earlier 8840 model not only in the LED lighting and its bell-shaped armature motor, but also in the roof design.

Although a model with rubber strips instead of a surrounding rain gutter has been reproduced again, the roof area has now been given an umbra grey instead of white aluminium-coloured design. The front of this model is now not decorated with an Ege biscuit. Three pairs of B3yge (2nd class) conversion coaches are attached to it.

The "DB Netz with Class 120 Electric Locomotive" train set (81528) is also arriving at dealers these days. The yellow express locomotive, former 120 160-7, in Era VI condition is pulling a RAILab 1 measuring car and a Dienstmz 307 cab

control car. The cab control car in particular has a very elaborate colour scheme and thus attracts attention in a very attractive way.

Shortly before the editorial deadline, the Märklin programme "Track Planning 2D/3D" arrived in version 11.0 on a USB stick. We also want to take a closer look at this planning aid in an article.

Half-time discount with the load specialist:

Spur-Z-Ladegut Josephine Küpper is celebrating its 30th anniversary this year. To mark the occasion, the sprightly couple are offering their customers a "half-time discount" of 15% on all orders in the second quarter of 2024.

This is not calculated and deducted automatically during the ordering process, but only afterwards when the invoice is issued. For this reason, customers should not pay in advance, but wait for the invoice. We would like to use this kind gesture to thank them for their many years of commitment.

The order pages for loads, some accessories, spare parts, and used goods can be found at <https://spur-z-ladegut.de>. Parts of the programme can also be purchased at the Trafofuchs stand in Altenbeken.

Dead spruce wood from Busch:

Was it the long drought or did the bark beetle strike? The answer to this question is irrelevant for model railways, but thanks to Busch, the subsequent damage that can still be observed in many forests can now also be reproduced on a small scale.

The Viernheim specialist now offers the dead spruces with the remaining, now brown needles in three size combinations, all of which are suitable for Z gauge if the trees are to be given a prototypical height.

Packs of two are available with a height of 130 mm (item no. 6166), 160 / 180 mm (6167) and 180 / 190 mm (6168). Let's hope that we only have to get used to such a sight on the model railway and that our flora will once again have enough water to live on in the future.



April at AZL:

The Union Pacific EMD diesel locomotive SD70ACE has been delivered in a D&RGW nostalgic livery (item no. 63109-2B), which shows it with the details of today's operating condition. It is accompanied, at least as a model, by the General Electric ES44AC demonstration locomotive (62416-1).

In this diorama, which illustrates the atmosphere created by the new products, Busch has installed specimens from all three packs that have now been delivered. Photo: Busch

Trainini® *International Edition*

German Magazine for Z Gauge

There are also other versions of the EMD SD40-2, for example for the Milwaukee Road (64204-1 to -3), but also Conrail (64208-1 to -3). It is complemented by models of the EMD SD45 from the same manufacturer (63206-1 to -3) from earlier production.



Union Pacific diesel locomotive SD70ACE in D&RGW nostalgic livery (item no. 63109-2B; photo left) and General Electric demonstration locomotive ES44AC (62416-1; photo right). Photos: AZL / Ztrack

Among the wagons, the Northern Pacific heavy half-baggage wagon (74033-1 / -2) and the Ortner bulk goods wagons, which are offered with the Conrail inscriptions to match the focus of the locomotive models, stand out. They are available as a double pack (905362-1) and a four-pack (905312-1).



Northern Pacific's heavy half-luggage wagon. Photo: AZL / Ztrack

The only thing missing from our list is the wide-view goods train escort car in the Burlington Northern design (921019-3 / -4). Product pictures of all new products can be found on the supplier pages at <https://www.americanzline.com>.

A major innovation at NoBa-Modelle is ready:

Before the summer break (20 April to 20 October 2024), the duo from NoBa-Modelle (<https://www.noba-modelle.de>) have released some of their new products. First, there is the Mercedes Benz NG 1632 "On the move" as a finished model (item no. 6926RF).

One of this year's two main new products also made it into the delivery programme in time. However, due to the high demand, which still exceeds expectations, new orders cannot be processed until after the summer break.



With the Touropa couchette coach Bc4ümg-53 (item no. 5109RF), one of the two main new products 2024 has now been delivered. Photo: NoBa-Modelle (customer photo)

We are talking about the DB's first type of couchette coach, the Bc4ümg-53 (later Bcüm 251), which initially travelled for tour operator Touropa. Its sapphire blue paintwork in combination with an iron-grey roof and apron was not the only thing that made it stand out from the rest of the Bundesbahn's rolling stock.

We offer blanks (5109R), which can be completed as a kit and which we are also preparing for a contribution, as well as finished models (5109RF), which are then delivered to the customer painted and labelled.

The supplier went to great lengths to obtain authentic operating data for an example based at the Dortmund railway depot for the sliding pictures for these coaches. The tinkerers also carried out various tests to determine the most suitable lettering colour for the distinctive Touropa lettering and the DB letters.

Many deliveries from Noch:

The "micro-rooms" LED interior lighting systems for buildings from Noch, which we first presented in the February issue, have now arrived at specialist retailers. However, we have received a veritable flood of new products from a production technology company.

Noch is the first manufacturer to use innovative 3D colour printing technology for its series production of 3D master figures. This technology makes even the smallest details visible, as we have now been able to see for ourselves. The matt appearance of all the figures that have now been delivered also ensures a realistic look.

This technique can also be used to depict checked shirts, striped blouses, colour gradations and gradients, which was previously impossible with hand-painting. As a result, new details are possible that were previously impossible to achieve with hand painting.



The travellers (item no. 44520) are one of ten new figure packs supplied by the manufacturer Noch from Wangen (Allgäu).

Ten figure packs announced last year are now available. The packs each contain six figures and some accessories: firefighters (item no. 44500), construction workers (44501), railway staff (44510), shunting staff (44511), travellers (44520), bathers (44530) as well as mountain hikers with summit cross (44531) and cows (44540), deer (44541) and horses (44542).



The newly delivered shunting staff (44511) are now also hard at work on Z gauge locomotives and wagons.

This means that, in addition to the car models, only the seated models (44521) are still outstanding from the spring 2023 new items, which should be available at the end of July or beginning of August. At this point, we would like to give you an important tip on how to handle these: Due to the material and production method, the filigree figures have less residual flexibility than PS injection moulded figures. This means that there is a greater risk of breakage if they are handled carelessly!

All the figures have also been depicted in very realistic poses, which makes them very appropriate for layout design. Hopefully, the photos provided here can already convey this.

However, we will also take up and present these new products in more detail in the two reports still to come from last year's annual focus topic "Marginal phenomena" and show practical application examples.



This is what the new special model of the 185² class looks like that Märklin is issuing for the European Championship in Germany. Illustration: Märklin

Announced at the Intermodellbau:

At the beginning of this year's Intermodellbau in Dortmund, Märklin announced a special model for the upcoming European Football Championship. The decision was also made at short notice in Göppingen to add a Z scale model (item no. 88488) to the H0 scale model.

This is a class 1852 electric locomotive in a fictitious design for the sporting event of the year (see photo on previous page). A bell-type armature motor drives all axles of both bogies, warm white headlights and red tail lights from LEDs alternate at the ends of the locomotive depending on the direction of travel.

The functional, gold-plated pantographs contrast with the black nickel-plated wheelsets. The elaborate design shows a football stadium, a waving German flag and a football flying high. The lettering "2024 Germany" reveals the reason for the special edition.

High profile visit to Hamburg:

After more than 150,000 hours of work and over EUR 5 million in construction costs, Prince Albert II, Princess Charlène and their twins Prince Jacques and Princess Gabriella opened the new Monaco section of Miniatur Wunderland Hamburg at 5:30 pm on 25 April 2024.

The two Braun brothers thus achieved an incredible PR coup that they had been working towards for years. The Princely Family was allowed to start a Formula 1 race, which only went off without a hitch at the second attempt and was won by the racing car chosen by the Princess.



The two Braun brothers Gerrit (far left) and Frederik (far right) welcomed the Monegasque princely couple Grimaldi and their children to Hamburg for the opening of the Monaco section of Miniatur Wunderland. Photo: Miniatur-Wunderland

This 70 square metre section, which merges into the Provence landscape, adds another attraction to Wunderland. The Formula 1 track technology alone took eleven years to develop! Another highlight in this area is the black and white design of the Prince's wedding from 1956, by which his son Albert felt very honoured.

New Herpa aeroplanes:

According to our selection criteria, only one aircraft remains from Herpa's Wings new products for the 1:200 scale, which is scheduled for delivery in May / June 2024:

Malév Hungarian Airlines Ilyushin IL-62M (Art.-Nr. 573047).

Among the Snapfit models, we found three 1:200 scale models and two 1:250 scale models (Boeing 747 "Jumbo Jet"):

TAP Air Portugal Airbus A330-900neo (612227-002),
Virgin Atlantic Airbus A330-900neo (614085),
DHL Aviation (European Air Transport) Airbus A330-200F (614139),
Braniff International Boeing 747-100 "Big Pumpkin" (614146), and
Pan Am Boeing 747-100 (614153).

On 29 June 2024, Herpa will also celebrate its 75th anniversary at the Diethofen site with the traditional summer party, including special models and special offers.

Imprint

ISSN 2512-8035

Bibliographic information of the German National Library: The German National Library lists this publication in the German National Bibliography. Detailed bibliographical data and editions can be found in the DNB catalogue at <https://portal.dnb.de>.

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International Edition

Free,
electronic magazine
for railroad enthusiasts
in the scale 1:220
and Prototype

www.trainini.eu

Published monthly
no guarantee

ISSN 2512-8035

Trainini

German Magazine for Z Gauge



Strength for Alpine Inclines

**Intermodellbau in Dortmund
Ideas for Building your own Layout**

Introduction

Dear Readers,

Where has the time gone? I ask myself that once again in May! This issue is being published late, particularly much later than we originally planned. All of us, the entire team behind it, produce **Trainini®** on a voluntary basis in our spare time.

But, this is not to justify another month that has turned plans into a waste of time. No, things are still somewhat different in large parts.



Holger Späing
Editor-in-chief

We are also often behind the scenes representing the interests of Z gauge. We talk to manufacturers, make suggestions or are often actively asked for ideas. This is both an honour and a challenge, because it is not wishes that are bought, but mature products that find enough interested parties.

Estimating their number as correctly as possible, and finding valid indications or evidence that make our assessment appear realistic is a preliminary task that we face. If we are correct and a manufacturer takes up a proposal we have discussed, this also strengthens our expertise.

We are currently very excited to see what Noch will do in this regard: In an episode of **Trainini TV** at the international toy fair, Dr Rainer Noch and I called for suggestions for future car models. The company boss has now received the results at Intermodellbau and expresses his thanks in this issue.

Of course, we also report in detail on the trade fair and our finds in this issue. We were pleased to see that the Z gauge was quite noticeably represented here. Both layouts on display were small, but of good quality.

An important difference to Altenbeken is that many thousands of people from a large catchment area with very different interests flock here. The Z gauge weekend, on the other hand, is more of a small gauge meeting that remains increasingly unknown to people outside our community.

The results of new developments traditionally have to face our critical gaze. This time, the focus is on the Re 6/6 from Märklin. It is a great locomotive that also appeals to us. However, we have also found room for improvement. Stay tuned!

The circle closes with an article by Dirk Kuhlmann: We have announced that we want to build and present two very different beginners' systems, spread over two years. This will not be done one after the other, but, in part, deliberately in parallel.

And so today he begins with the preparations, basic ideas, and planning. After all, the following instructions are intended to lead to success in a targeted and not random manner. Enjoy reading them!

Sin-Z-erely,

Holger Späing

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Date of publication of the German language version of this issue: 31 May 2024

Cover photo:

Like all SBB Re 6/6s, the locomotive with the road number 11666 also bears the name of a Swiss town, in this case Stein am Rhein. With an Intercity on the hook, it shows itself to the photographer in all its splendour during a stop.

The Re 6/6 from Märklin

Bulls for the Gotthard

When they first appeared, the Re 6/6 were the most powerful SBB locomotives produced in series. They also stood up to comparison abroad. These locomotives were able to haul all trains over the mountain whose weights the traction equipment could withstand. We take a brief look at the prototype and would like to know whether the Märklin new product has become comparably strong.

At the beginning of the seventies, the SBB recognised an urgent need for powerful locomotives for the heavy service on the Gotthard. Since the 1950s, when the Ae 6/6 had been developed in response to increased demands, the tonnage transported had tripled, and the number of trains had doubled.

There was no doubt that an answer had to be found in the form of a new, powerful locomotive. Compared to the Ae 6/6, for which they were also intended as a replacement, a high cornering speed and other advantages were at the centre of development.



Re 6/6 11666 is in IC service and presents its long side at a station stop. The view falls on the engine room with a surprising depth effect; the model also leaves a good impression in other respects.

The requirements could only be realised with a six-axle engine. However, due to the required curve running, three two-axle bogies were chosen over the predecessor Ae 6/6. The middle bogie also had to be able to move sideways to minimise wear on the rails and wheels when cornering.

All three bogies were connected with flexible transverse couplings to ensure better cornering. Their principle is based on a technology that was originally developed for the Ge 6/6^{II} of the Rhaetian Railway.



Re 6/6 11601 "Wolhusen" was the first of two prototypes for the new Gotthard locomotive. It can be seen here with wagons from the Apfelpfeil organisation between 1973 and 1980. Photo: ETH Library Zurich, Image Archive (Photographer: Hans-Peter Bärtschi / SIK_03-107655 / CC BY-SA 4.0)

And the Re 6/6 was impressive: It had a real 10,000 hp output, putting it on a par with the German Federal Railway's class 103, which was designed as a high-speed locomotive. It thus produced four times the power of the legendary "Crocodiles" and 80 % more than its predecessor, the Ae 6/6, with the same weight.

In terms of performance, it was only surpassed by the "Landi-Lok" 11852, which made it the SBB's most powerful locomotive when it first appeared. Although the technology responsible for this was based on the Re 4/4^{II} / Re 4/4^{III}, the Re 6/6 had both a power transformer and a regulating transformer. They are located in the locomotive body between the bogies.

A total of 89 locomotives were built, 85 of which went into series production between 1975 and 1980. They were preceded by four prototypes delivered by SLM and BBC in 1972, the first two of which still had a locomotive body split down the middle.

As the articulation can only move vertically in order to adapt to changes in gradient, they did not count as articulated locomotives, unlike the older "Crocodiles". The other two prototypes used a continuous locomotive body in conjunction with a softer secondary suspension of the centre bogie. As this proved to be successful, it became the template for the subsequent series.



The same locomotive as on page 5 is shown here after its modernisation on 17 February 2013 in Buchs/SG. The split locomotive body, which only the two prototypes had, is clearly visible. Photo: Joachim Kohler (CC-BY-SA-4.0)

The visual design of the Re 6/6, as well as the operation, is also based on the Re 4/4^{II} and Re 4/4^{III}. Similar to Germany, the SBB chose a kind of “standardised face”, the DB even developed a standardised driver’s desk – at least a basic parallel to the locomotive type discussed here.

It is interesting to note that the Re 4/4 and Re 6/6 can be distinguished quite easily from a distance: Because the six-axle vehicles have a higher roof, they have a “high forehead,” which is a key distinguishing feature. When viewed from the side, the greater length immediately catches the eye.

In addition to the high power and its maximum permissible speed of 140 km/h, the technical limit for Swiss lines at the time of its appearance, the Re 6/6's multiple controls also make it universally usable.

Instead of a double traction of two Re 4/4's, she was therefore often used alone in front of heavy passenger trains on the Gotthard line, usually together with a locomotive from the Re 4/4 family for freight transport; this combination is then referred to as Re 10/10. Together they can carry 1,400 tonnes over the ramp with its maximum gradient of 26 ‰.



Märklin's prototype locomotive Re 6/6 11666 was photographed at Rotkreuz station on 9 August 2013 and allows a direct comparison of its front end and cab window to the model. Behind her is her direct predecessor Ae 6/6, which has long been part of the Mini-Club programme. Photo: Lord Koxinga (CC-BY-SA-3.0)

The 89 machines were allocated to the Erstfeld, Bellinzona and Lausanne depots; the fixed allocation was only cancelled in 2002. Overhaul work is carried out in Basel and in the Limmattal marshalling yard. The Bellinzona industrial plant is responsible for overhaul work. Locomotives of this type are still in service for SBB today.

However, there were changes when the division of SBB into different business units came into effect on 1 September 1999. Operational numbers 11601 to 11613 initially went to the passenger transport division, the rest to the freight transport division. On 1 January 2003, they were swapped for Re 460s, and since then all the locomotives have been in service for SBB Cargo.

However, their number had long since been reduced to 88 units, as 11638 was withdrawn from the fleet in 1990 following an accident. However, when the vehicle numbers and series were converted to the UIC scheme in 1992, the locomotive was still included in the calculations.

continued on page 9



Re 6/6 11615 "Kloten" leaves the south portal of the Gotthard tunnel in Airolo in 1983 with an express train (photo above). A DB Eurofima coach Avmz 207 can be seen directly behind it. Photo: ETH-Bibliothek Zürich, Bildarchiv (Photographer: Comet Photo AG (Zürich) / Com_LC1943-001-019 / CC BY-SA 4.0)

As Re 10/10, this duo with leading Re 6/6 is travelling with a steel train between Ambri-Piotta and Rodi-Fiesso on 13 February 2009 (photo below). Photo: Kabelleger / David Gubler (CC-BY-SA-4.0)

In a departure from the usual procedure in Switzerland, the new road numbers of the type now known as the Re 620 were not set from 000, but 001. Thus, 11601 became 620 001, with all others numbered consecutively.



On 6 August 2008, the SBB Cargo locomotive now known as Re 620 061-2 (ex Re 6/6 11661) is on its way to Landquart with an oil train. Photo: Kabelleger / David Gubler (PD-self / <https://bahnbilder.ch>)

Since ETCS Level 2 and GSM-R radio were introduced on the Gotthard and Lötschberg base tunnels, Stadler Rail has also retrofitted Re 6/6 machines. Prototypes 11601 to 11604 have now been retired from the fleet: 11603 and 11604 were sold to Rail Adventure in August 2018, but the latter was sold on to the Oensingen-Balsthal Railway at the beginning of 2024.

11601 and 11602 went into private hands in 2019. 11601 is to be refurbished in running order, while 11602 serves as a spare parts donor. Her sisters are still in daily service with SBB Cargo.

The Märklin Model

Almost twenty years ago, Swiss Z Lines (SZL), an offshoot of the Norwegian-American supplier AZL, was the first model railway supplier for Z gauge to venture into the Swiss Re 6/6. The impetus for development and production probably came from co-owner Rob Allbritton, who had chosen the Gotthard north ramp for his layout project in 2001 (see **Trainini**® 7/2011).

The convincingly designed models were produced in small series, so it was not possible for us to obtain a sample for a comparative test. Visually and technically, this and the Märklin new product appear to be very close to each other, which would have been interesting in view of the widely differing development times and prices.



In today's test, the new Re 6/6 11666 from Märklin (item no. 88240) has to face the critical eyes of our editorial team.

But even without a comparison option, our test promises great excitement. After all, the model (item no. 88240) has been a long time coming. Märklin presented an unpainted sample made from series parts in Altenbeken in May 2022 to mark the 50th anniversary of the Z gauge, but the locomotive was not to be found on dealer shelves for a long time, yet.

The time was not due until April 2024 and we also held a sample in our hands. Its first impression is excellent and leaves a harmonious impression. However, the weight seems rather low for such a large locomotive. So, we will also be looking into this.

First, however, the new product will be scrutinised from all sides to see how Märklin has implemented the various details. This will reveal both the bright and the dark sides. The tension is rising!

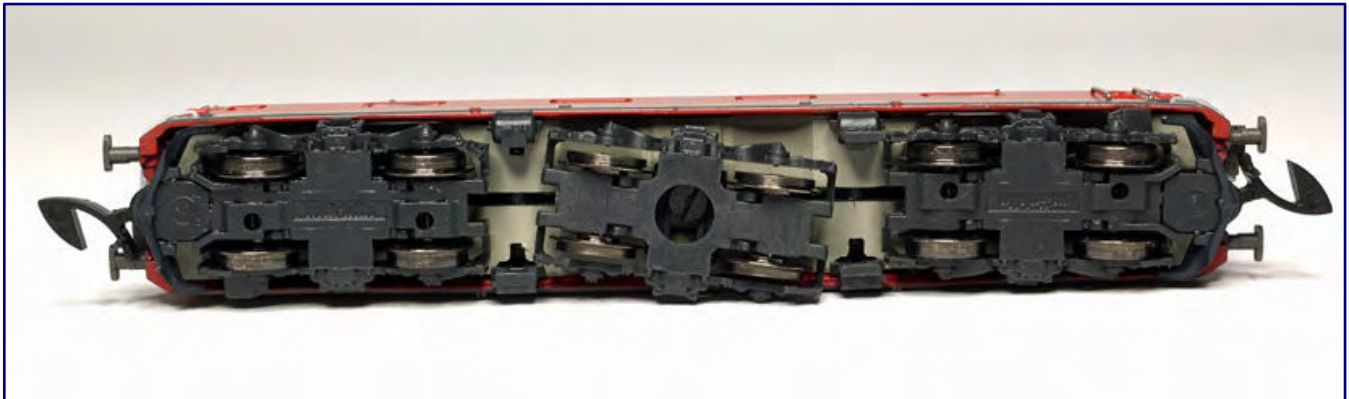
One of the first features we notice is the significantly narrower running surfaces of the wheels in the centre bogie. They are usually too large for any track gauge, just as wheel flanges cannot be realised to scale for the required operational safety. In this case, however, they look very prototypical, while those in the outer bogies follow the usual picture.

It should therefore also be added that the drive only acts on the four axles of the outer bogies via a worm and spur gear with cardan shafts. The centre bogie is not driven and, as on the prototype, can move freely in the frame.

This allows the locomotive to maintain contact with the rails in any track position. In terms of driving and train behaviour, it should be comparable to models of four-axle bogie locomotives. This is also supported by the fact that it is actually remarkably light for its size, weighing only 31.6 grams.

Therefore, we will now continue with the technical measurement results. The operating behaviour fits quite well with the photos of the Re 4/4^{II} with bell anchor motor and, in our opinion, should be quite suitable to form a unit together as a so-called Re 10/10. We had already heard this wish when the Re 4/4^{II} was released.

On the test oval, our Re 6/6 starts up slowly at 0.4 volts track voltage. The MTS-100 model speedometer from Halling Modelle verifies that it is travelling at the equivalent of 0.8 km/h. But the locomotive stops at the first points. A safe crossing is possible at the equivalent of 27.8 km/h with 2.4 volts of pure DC voltage.



The centre bogie moves freely in a similar way to the large prototype. Its narrow running surfaces are noticeable, but do not cause any faults in test operation. Incidentally, we do not test the triggering of switching tracks in our cycle.

It then reaches the prototype's top speed of 140 km/h at 7.0 volts. Unfortunately, we have to compare two voltages for the possible maximum speed of the model: The instructions still state 10 V as the maximum, whereas the cardboard box states 12 V. They allow the miniature to accelerate to 200.8 and 277.8 km/h – i.e., up to almost twice the original speed!

We are torn when evaluating this result. On the one hand, a lower final speed would further improve controllability, although we find this quite pleasant. On the other hand, the differences to the Re 4/4^{II} model constructed around ten years earlier would then be so serious that the typical double tractions of the prototype could not be reproduced.

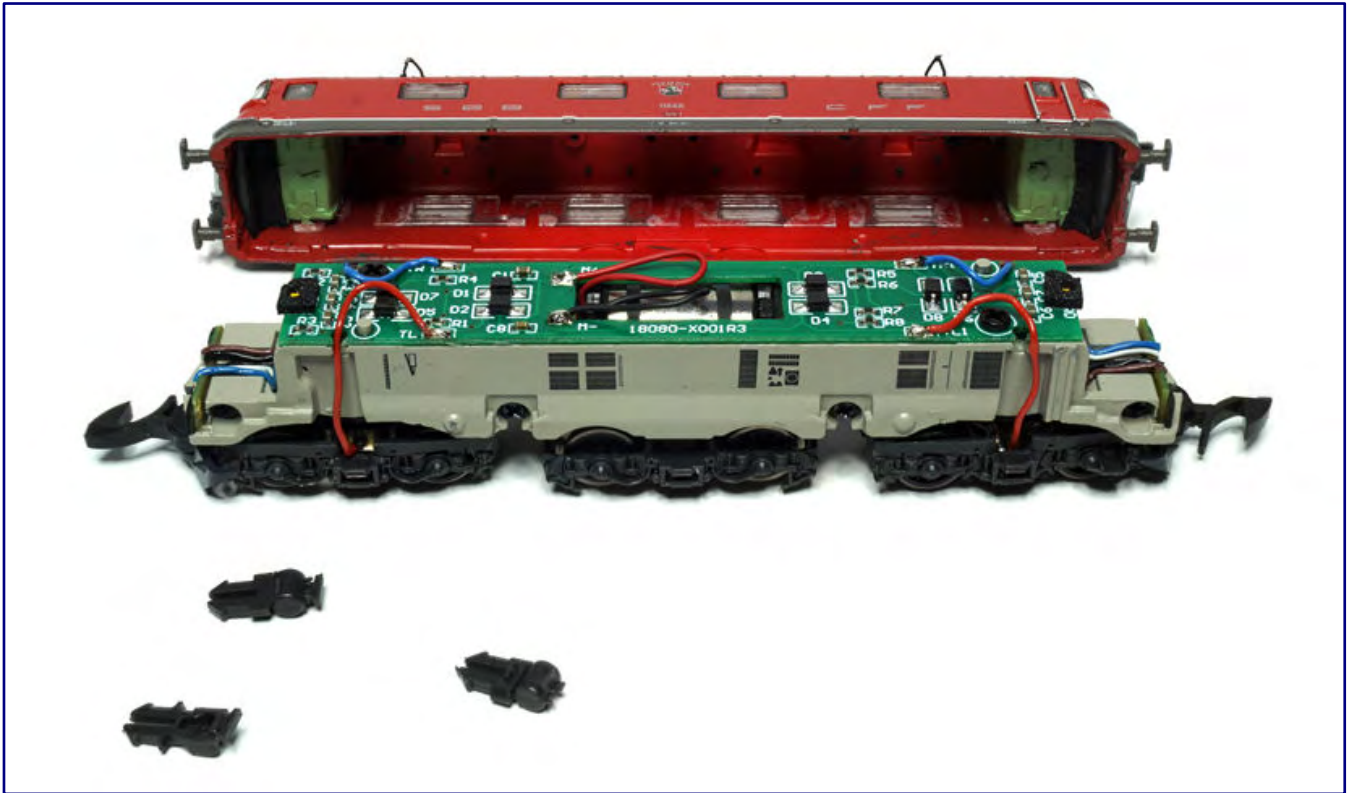
However, the latter aspect seems to us to be more important for operational practice on the layout. The fact that the new product runs rather quietly should also contribute to the hoped-for harmony. Motor noises are perceptible, but no longer in comparison to the earlier drives.

Power consumption and tractive force

The next step is to measure the current consumption and tractive force. We regularly measure at transformer positions 100 and 150 on the white Märklin transformer 67011. 28 and 35.5 mA are no surprises here either and correspond to the values of other Märklin models with the same motor. Under full load, the measured values increase to 64 mA – certainly important for digital railway enthusiasts in order to be able to select the appropriate decoder.

However, we are not happy with the tractive force. On level ground, the locomotive pulls just 3.5 grams over the coupler; on a 3% gradient, this value drops to just 3 grams. Although this measured value should not be confused with the gross weight of the movable wagons, trains that look like prototypes remain a pipe dream here.

As a comparison, we use the Vectron, which weighs almost exactly the same: its measured values were around one and a half times higher! This is also a noticeable difference when operating the layout. In our opinion, Märklin should think about how the Re 6/6 can be given additional weight: perhaps by adding ballast weights or moulding the body in metal-filled plastic?



The interior looks largely familiar, the pantographs are, as with all new locomotive designs, without electrical function. The engine room replica printed directly onto the running gear block and the attachments fitted on both sides over the four centre chassis retaining screws, three of which are in front of the locomotive here, are worthy of note.

Incidentally, the run-out from the converted prototype speed is around 65 mm. This value does not seem large, but is probably sufficient for an emergency stop without derailing the following wagons. We did not find a flywheel mass inside, at most the brass bells, in which the cardan shafts are inserted at the shaft ends, may have a slightly positive effect here.

The single-arm pantographs may form the transition from the technical inspection to the visual features. Märklin's new standard components are used, i.e., the revised version from a new design. The collector shoe is therefore clearly too wide, which all Zetties could probably live with.

However, its general weak points also strike mercilessly here and considerably detract from the appearance of the locomotive, which is well reproduced overall: The dimensions of the new parts are probably closer to N gauge than our scale, and they are not electrically connected.

When retracted, they do not lie flat and disturb the photo. The massive base, which clearly does not belong to the Re 6/6, also contributes enormously to this, because Märklin has correctly not modelled any support insulators next to it. A dimensional deviation of over 25 % in the height of this locomotive is anything but a flattering value.

According to the instructions, the current collector folds up when its collector strip is turned slightly counterclockwise. We would advise caution here, because in our case, both pieces were very hooky and could hardly be induced to make this movement. There was a serious risk of causing damage here. The black nickel plating does not come particularly close to the original impression.



Both pantographs on our test model turned out to be hooky. They are certainly not a feast for the eyes: they do not lie flat and look like a foreign body on an otherwise successful locomotive. Accordingly, they cause a massive dimensional deviation outside of any tolerance.

Then we would like to respond to a request from our readers: After the fine scissor pantographs, Sommerfeldt should also develop finer, electrically usable single-arm pantographs with a smaller collector shoe and offer them as replacement parts!

If we now take a look at the rest of the roof, we can describe one of its strengths: The equipment appears complete, finely detailed and engraved as well as coloured. The roof lines have been realised with wire and attached to the insulators. In contrast to many older models, the cables on this new design also extend to the pantographs.

External impressions

We look at the engine room windows, somewhat astonished. Behind them, we see a replica of the interior, but it still looks a little different than usual. It seems to have been moved further into the depths, giving the impression that there could still be a passageway between the window and the engines (see also photo above).

If we remove the housing, we realise that the reproductions are constructed in a similar way to how Märklin has always done it, but now there is no carrier (sticker) as a basis for printing. The cast block (split lengthways to poles) was painted light grey and printed directly.

One of the distinctive features of Swiss locomotives of this generation are the front windows. The difference to the Re 4/4^{II}, which has to be visible here, has been well made and also allows the leading locomotive of a double to be recognised immediately in the model.



The front view shows the “high forehead” of the Re 6/6 well. However, the narrow red bar between the front and corner windows, which is completely transparent here, could have been better solved by omitting it.

desired, while the side windows in the engine compartment are perfectly aligned. An interruption in the windscreen wiper arm is too clearly visible. There is a slightly visible interruption between the engravings on the window pane and locomotive body – an expression of the criticised inaccuracy.

If you spend as much time looking at the windows as we did, you won't miss the replica driver's cab either.

The reseda green insert is easy to recognise, but not easy to capture on camera.

It may be helpful to note that in Switzerland trains regularly run on the left-hand track, so the train driver does not sit on the same side as with DB.

We are also delighted with the other engravings and features. Everything seems to be correct and no important feature has been forgotten.

Photo right:

From this perspective, the reseda green-coloured driver's stand is also visible. However, the perceptible interruption in the wiper arms is disturbing. The skirt under the coupling, which is included as an accessory, is fitted here.



Once again, it is difficult to implement the front and almost directly adjacent side windows.

This was already a cause for criticism on the Re 4/4^{II}, because the rounded front windows on the outside looked too angular. Märklin improved this in subsequent editions.

There is also a small weak point on the Re 6/6: The shape of both window parts is correct and in direct comparison even more finely executed, but there is a transparent gap between the two frames, which should actually be painted red.

Leaving out this barely visible bridge would have convinced us more.

The accuracy of fit of the front windows also leaves something to be

Dimensions and data for the Re 6/6 the SBB:

	<u>Prototype</u>	<u>1:220</u>	<u>Model</u>	<u>Deviation</u>
Length over buffer (LüP)	19.310 mm	87,8 mm	88,0 mm	+ 0,2 %
Height above rail head	3.932 mm	17,9 mm	22,5 mm	+ 25,7 %
Width	2.950 mm	13,4 mm	14,3 mm	+ 6,7 %
Total wheelbase	14.300 mm	65,0 mm	63,8 mm	- 1,8 %
Bogie wheelbase	2.800 mm	12,7 mm	12,6 mm	- 0,8 %
Wheel diameter	1.260 mm	5,7 mm	6,0 mm	+ 5,3 %
Service weight	120 t	---	31,6 g	
Axle layout	Bo' Bo' Bo'			
Power output	10.608 hp / 7.800 kW			
Permitted maximum speed	140 km/h			
Manufacturer	SLM, BBC, SAAS			
Years built	1972 (prototype), 1975 – 1980 (series)			
Number produced	89 units			

The handrails on the driver's cab steps, one on each side of the locomotive, are attached separately, which seems to have become a new standard. We are pleased about this and commend it.

The impression of the proportions taken is also confirmed when the other measurements are taken. With the exception of the height, the deviations are minimal and all in a positive direction. The appearance of the prototype has certainly not been distorted.

In the running gear area, we also approve of the engravings of the bogies and the attached parts. We quickly realise that the wheels of the model appear larger than those of the Re 4/4^{II}, they are identical on the prototype. We measure 6.0 mm instead of the converted 5.7 mm.

The 0.3 mm difference is not a point of discussion, especially as new wheel discs and inserts were developed here. Emphasising this feature is aimed solely at the Re 4/4^{II}, as this had a wheel diameter of 4.9 mm, almost 1 mm different from the correct dimension. As the deviations between the two are in opposite directions, this is now all the more noticeable.



Märklin's strengths can also be seen on this model with its clean, silky matt paintwork and clearly legible prints. We are also impressed by the bogie covers with suspension and cables as well as the large wheels, which make the undersized Re 4/4^{II} stand out all the more. The roof cables attached to the insulators also deserve praise, thanks to which the diagonally positioned feed-through insulator is particularly eye-catching.

continued on page 17



Although the attachable aprons leave a gap between them and the buffer beam, they could be made finer than on the Re 4/4^{II} (photo above). We commend this. However, the very brightly coloured ventilation grilles and roof superstructures (photo below) appear somewhat garish and barely match the typical operating impression of the prototype, even if this may follow the paint scheme.

We put a final question mark over the enclosed skirts, which can be found as add-on parts under the model's deep-drawn insert. We remember that the reproductions moulded directly onto the bogie of the Re 4/4^{II} were rather disturbing.



From a higher perspective, the single-arm pantographs are not so disruptive to the eye and therefore do not obstruct the view of what is a successful design overall. The only thing we would have liked to see in this model is a much stronger pulling power!

Märklin seems to have found an approach here to make them much finer. What Märklin has forgotten, however, is a printed insert that explains to an uninformed customer which parts are involved and how they are mounted in which position.

We therefore add that they are inserted into the tiny opening in the coupling guide under the bogie. The opening in the apron faces the shaft, the tip of the V formed is at the front. Guided by a pair of tweezers, both parts can be brought into position.

Conclusion

We conclude our description of our visual impressions with our judgement of the paintwork and lettering. With its red livery, the model reflects the operating conditions of the 1990s, before SBB was split into divisions.

This also includes the lower lanterns in an angular design as a typical characteristic of a modernised locomotive. All the colours, especially the contrasting elements in the roof area, perfectly match the prototype. The neatly applied silky matt surface is well suited to a well-kept prototype model.

The lettering is also impeccable, complete and cleanly applied. In addition to the coat of arms, locomotive name and road number, we can also easily read the type designation Re 6/6 on the locomotive body. According to the imprint, the last revision of the R3 stage took place on 9.4.99 in Bellinzona.

We have already provided ideas for train formation on the domestic layout in the prototype section: The prototype was used in Intercity/Eurocity traffic as well as in goods train services. The photos of heavy trains can be correspondingly colourful.



Freight transport has always been an important field of application for the Re 6/6, which is why we are using it here in front of a container block train, which still characterises today's Alpine transit photos.

Passenger coaches could accommodate the limited tractive power of the model, but truck or container block trains probably look more familiar to viewers. Of course, mixed goods trains can also be formed, but a block train has a longer effect on the viewer, and can make the technical weakness of the new design recede, at least in the viewer's impression.

As with the Re 4/4^{II}, we hope that the manufacturer will find ways of ironing out the most significant weaknesses in future editions. In order to emphasise our good overall assessment despite some points of criticism, we nominate the red SBB Re 6/6 (88240) for the new releases of 2024 in the locomotives category. In doing so, we are also recognising the choice of prototype, as this is one of the most important Swiss locomotives.

Manufacturer of the model:
<https://www.maerklin.de>



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Z 44542 **Pferde** NEU



Z 44540 **Kühe** NEU



Z 44541 **Hirsche** NEU



The path to the first layout (part 2)

Beginner's Layout or maybe a bit more?

Our 2024/25 focus topic deals with building a beginner's model railway layout and will cover two completely different ways of doing so. The aim is to let newcomers and those returning to the hobby pick the best approach depending on their personal skills and preferences. After kicking off our series (Trainini® 4/2024) with a preformed winter layout base, Dirk Kuhlmann is now presenting an alternative concept for a first layout.

Welcome to building a different kind of model railway! The **Trainini®** team has been known for many years for its trailblazing approach. Themes and trends that the printed modelling magazines typically pick up with a delay has often already been done by us long ago.

We have the courage to come up with unusual ideas and some innovations in the industry. We are not only inviting our enthusiastic beginners and those returning to the hobby, but believe that experienced Z gauge enthusiasts and friends of other scales should and will also benefit from this series of articles.



The (landscaped) staging yard of the US-themed layout “Old Port Shop” serves as a source of ideas for Dirk Kuhlmann’s series on a beginner’s project, which we are running parallel to the winter layout project throughout our 2024/25 editions.

What could a beautiful and completely self-built Z scale model railway layout look like, especially for beginners or someone returning to the hobby? How high should the play value be? How much effort should it take to build?

Well, when conceiving our focus topic for the next two years, I asked myself a lot of questions and subsequently drew quick sketches of possible realisations. Initially, it was supposed to be a closed loop, combined with the opportunity to observe and enjoy moving trains. But, that only brings us back to the classic oval on a board, including the usual accessories. This is always nice to look at, but a little boring, at least after a while.



Who says that a staging yard should always be hidden away? Building the Old Port Shop layout led to new ideas which are brought to maturity with a new layout project.

The breakthrough came in autumn 2023. I was sitting in front of the staging yard of the US themed layout “Old Port Shop” and had a really long coal train with three American diesel locomotives in front of my lens for photo work.

This layout segment is 180 cm long and therefore predestined for exhibiting such long trains. Even at rest, the scenery looked overwhelming, and it could still fulfil another purpose.

The staging yard served as my source of ideas, and even the complete Santa Fe passenger train from AZL fit onto the showpiece. And, finally all the pieces of this layout design puzzle fell into place:

- a 180 x 25 cm long showpiece for long trains,
- a cameo type layout presentation with lighting and background,
- a front cover optionally made of wood or Plexiglas, as well as
- four modules without landscape to close the loop.

But first things first! Nowadays, the space available in many people's homes is often very limited and simply too small to fit a big dream layout into the free or allocated area.

Adapting to these constraints often takes years, during which train display cases somehow, and almost unwittingly, fill up and multiply. Imagining our future dream layout, we keep buying rolling stock which some day, maybe, we would like to witness in operation on a nicely landscaped stage.



Impressed by how the Santa Fe passenger train, led by four diesel locomotives, travelled through the staging yard and found space there, ideas began to bubble up about the ideal track plan for a beginners' layout. Even far away from the usual, mostly boring ovals, there must be alternative approaches for beginners.

Imagine for a moment a display case with the above-mentioned dimensions of 180 x 25 cm. To operate trains, just remove it from the wall, attach the simple segment modules to the sides and the back to close the loop, and after a few minutes the fun can begin.

We are now looking at a complete model railway, and one which could also appeal to pure collectors. After all, a complete passenger or goods train takes up a lot of space, even at a scale of 1:220.

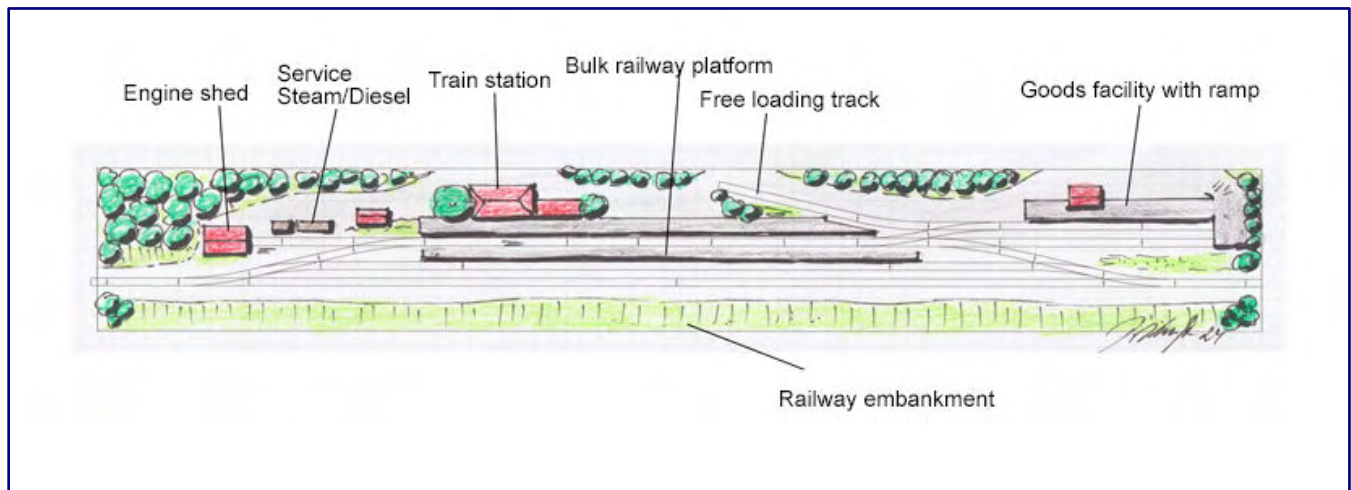
Such a solution cleverly takes into account all the aspects of having only a limited amount of space for the hobby. Perhaps your XXL display case will even be an aesthetically pleasing object which might get the stamp of approval from your spouse to be put up for permanent exhibition in the living room.

Now, what would be a good and adequate trackplan for such a project? That is a difficult question which can only be answered with several track plans. After all, every enthusiast has his own ideas of what

constitutes a beautiful model railway. I will therefore limit myself to three proposals, but do invite our readers to submit other proposals to our editors.

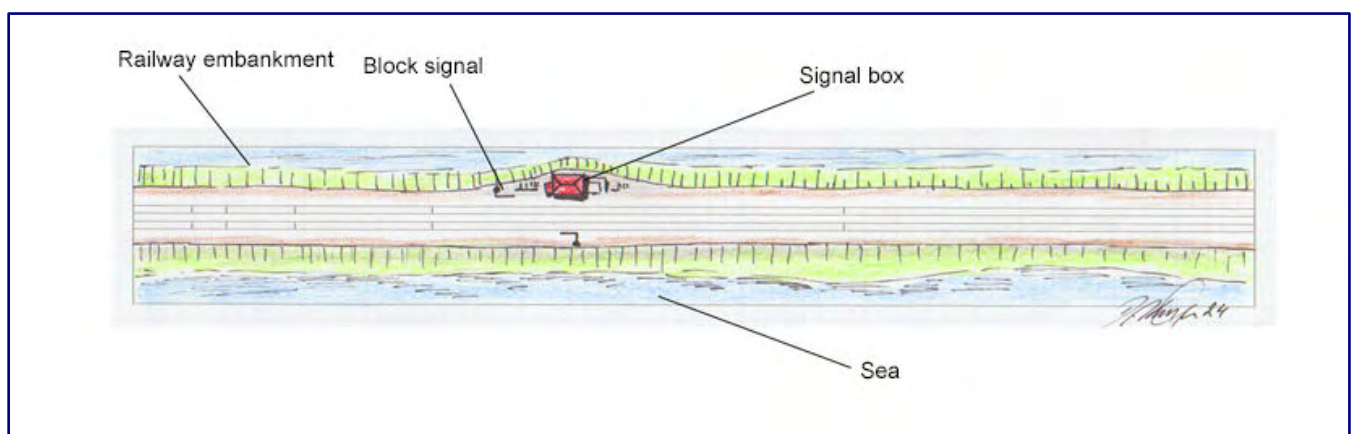
- Proposal 1 – the branch line

A small rural station with one through track and two passing sidings. This track plan can also be used for extensive shunting operations. The loading ramp, an open loading track and a small engine shed for the “yard dog” (e.g., the new V60 locomotive from Märklin) complete the overall picture. This showpiece fits into almost all eras and regions. It is an approach that evokes the idyllic countryside charms of times gone by.



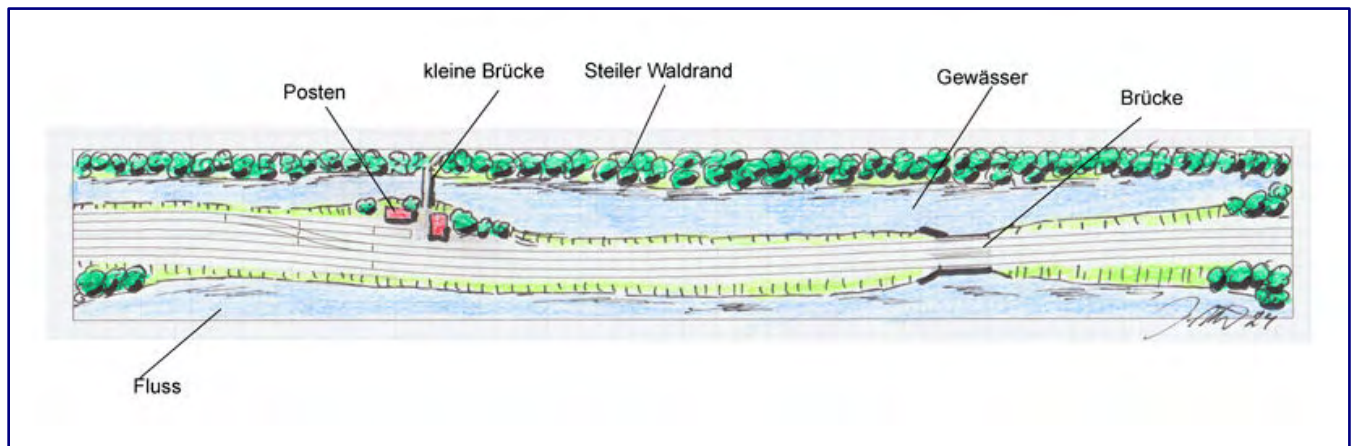
- Proposal 2 – the double-track railway embankment

With this version, train encounters can be wonderfully observed, whether at rest or in service. If you add a small post and water on both sides of the embankment, it could even represent the passage to the island of Sylt the Hindenburgdamm. The motorail trains still pass here to this day, for example.



- Proposal 3 – by the forest, by the river, in no man's land

A curved, double-track line along a river expands to three tracks. A terrain with bushes and trees that rises to the rear completes the scenery. With this version, you can depict typical train-in-landscape scenes. Anything from Germany to Sweden to the east of the USA would look authentic in one such showpiece.



- Proposal 4 – - the variable bypass (as an addition to the aforementioned proposals)
In total, the bypass consists of four easy-to-handle modules that can also be easily stowed away. The “staging yard” can also be extended in the future and can be used again and again for possible new showpieces that may be added over the years. There are no limits to the imagination.

First planning phase

Proposal 3 turned into my personal favourite. It allows me to offer a future alternative to the “Old Port Shop” staging yard; similar scenes can be observed on the Hudson River north of New York with the CSX and Amtrak railway companies.

Small, almost hidden recesses on the railway embankment allow me to quickly fix Scandinavian overhead line masts and pronto, a Swedish train is ready to make its rounds. The same would also work with the German railway. This is also where the maximum effect of flex track material comes into play: curve radii corresponding to the prototype. More detailed explanations will follow in the other parts of our series.

If you wish to do so, you can of course also set an entire scenery in Portugal, with the pictures below providing an example. Or, you could also vary the dimensions of the layout to suit your requirements.



We, the editorial team, are sticking to the given format, so hopefully I can still guide you through all the basic construction stages. For some layout building techniques we will also refer you to older issues of **Trainini®**. After all, over the years we have already shed sufficient light on some aspects of model railway engineering.

Attentive readers will certainly have noticed that we already presented a related track plan with Pattscheid station in the April 2024 issue. The historical relevance alone is enough to inspire us to realise its special features, but this only as a side remark.

Of course, there will be a lot of practical tips waiting for you in the forthcoming episodes and several techniques actually even come from the professional model making. What sometimes looks spectacular is actually quite simple to make, but time-consuming. Patience can be a valuable asset, especially in landscape modelling. But, don't worry, even our beginners and those returning to the hobby can do it!



The Hindenburgdamm leading to the island of Sylt with a Class 628 multiple unit in the model (left) and as a prototype (right) exemplifies layout proposal 2 on page 23.

Here is my first tip: If you are still a little unsure in some areas, a small test board is always the best solution. Simply put yourself and new techniques to the test without any risk!

This is exactly how I learnt and developed my own techniques with some guidance from the “greats” of model railroading at the time. Fortunately, we can shorten some things considerably nowadays with modern means and tools.

Almost at the same time, I will be setting up a smaller layout in my modelling club (FdE Burscheid e.V.) for new members and regular guests with a project similar to our **Trainini®** annual theme, sorry, two-year theme.

This is similar to the already familiar “Wakenfeld” display layout, which has found its home at the 1zu220-Shop. The only unusual feature here is the simultaneous presence of 6.5 mm, 9 mm, and 16.5 mm track in the concealed area.

The visible and interchangeable landscape section will measure 70 x 40 cm, so anyone interested can really let off steam with their own special scale. This means that exotic gauges such as Nm and H0e are also possible.

Participation is expressly encouraged, and I will accompany the construction from the first wooden board to the final design. Any difficulties that (possibly) arise during construction will also be addressed in this place over the next episodes to avoid, from the beginning, the risk of failure.



This shot shows the optimal view of your own rolling stock, plus a background and plenty of light for a perfect overall effect.

In the next article, we will discuss the baseboard construction in more detail. I will present you with a typical cutting plan and list for the carpenter, not for the DIY store, because only a specialised company can produce the boards to the best and desired dimensions. The necessary track and other materials are also ordered at the same time.

Pages of our editorial colleague:

<https://www.helenensiel.com>

Presentation of the FdE Burscheid:

<https://www.fde-burscheid.de>

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Modell-Landschaften entstehen

Techniken zur Anlagengestaltung

Auch im Modellbau gibt es häufig viele Wege zum Ziel. Techniken, Materialien und Ideen konkurrieren miteinander – und jeder Modellbahner muss die für ihn erfolgversprechendsten finden. Deshalb beschränken wir uns nicht darauf, unsere Vorschläge zu präsentieren, sondern stellen Ihnen auch ein aktuelles Buch vor, das zu den Inhalten dieser Spezial-Ausgabe passt.



Thomas Mauer
Modellbahnbau in Perfektion: Landschaften gestalten
Die Anlage wird grün: Vegetation, Felsen, Gewässer, Details

VGB | Geramond Media GmbH
München 2023

Gebundenes Buch
Format 22,5 x 27,1 cm
128 Seiten mit ca. 400 farbigen Abbildungen

ISBN 978-3-98702-022-3
Best.-Nr. 02022
Preis 27,99 EUR (Deutschland)

Erhältlich direkt ab Verlag
oder im Fach- und Buchhandel

„Eine perfekt gestaltete Landschaft macht die Modellbahn-Anlage erst vollständig“: Dieser Verlagsaussage stimmen wir uneingeschränkt zu. Aber mindestens genauso elementar ist der Weg, der zu diesem Ziel führt.

Wie alle anderen Bereiche des Lebens auch, so ist ebenso der Modellbau stets im Wandel und in der Entwicklung. In den letzten Jahrzehnten haben sich sowohl das Gestaltungsmaterial als auch die Techniken stets weiterentwickelt.

Parallel wurden alte Vorgehensweisen wiederentdeckt oder mit heutigen Möglichkeiten kombiniert. Zwangsläufig ergibt sich daraus die Einsicht, dass es immer mehrere Wege zum Ziel geben an – Beispiel Wassergestaltung: Die farbbehandelte Raufasertapete ist ein Klassiker, auf Anbieterseite stand ihr einst vor allem die Seefolie gegenüber.

Es folgte eine Ära der Gießharztechnik, die schließlich auch etwas aus dem Fokus geriet. Fensterfarben und schmelzbare Gießmassen verdrängten sie für längere Zeit, bis weitere Angebote vor allem für Fließgewässer die Aufmerksamkeit auf sich zogen. Vergleichbares ließe sich über das Gestalten von Felsen auch niederschreiben.

Doch deutlich geworden sollte längst sein, dass unser Hobby es nicht verdient hat, mit Scheuklappen durch die Welt zu laufen und stur an einer einmal eingeübten Technik festzuhalten. Auch wir sollten uns weiterentwickeln und die Augen offenhalten.

Genau deshalb gehört es einfach dazu, auch andere Autoren und Modellbahner zu Wort kommen zu lassen und ihre Ansätze gleichermaßen in den Fokus zu rücken. Einer davon ist Thomas Mauer, der

aktuell mit seinem Titel „Landschaften gestalten“ aus der VGB-Reihe „Modellbahnbau in Perfektion“ im Buchmarkt vertreten ist.

Bei diesem Buch handelt es sich um die vollständig überarbeitete und erweiterte Neuauflage einer Veröffentlichung aus dem Jahr 2019. Wie wir nach dem Lesen bestätigen können, steckt sie voller Anregungen, Tipps und Tricks.

Auch Modellbahner unserer Baugröße erhalten hier viel Marktüberblick zu den kommerziell angebotenen Produkten verschiedenster Hersteller und deren Einsatz. Gewiss lässt sich nicht jede Technik oder Idee problemlos von der im Werk behandelten Spur H0 in unsere Nenngröße übertragen.

Doch jedem, der sich mit der Modellbahn beschäftigt, sollte hinreichend Kreativität und Phantasie gegeben sein, um Sinnvolles und Verwertbares herauszupicken und auf seine Belange zu übertragen. So hilft der Autor sowohl Einsteigern als auch Experten gleichermaßen.

Eine besondere Stärke dieser Lektüre sind die vielen Schritt-für-Schritt-Anleitungen, die einfach verständlich und nachvollziehbar geschrieben wie strukturiert sind. Selten kann ein Buch eine so wertvolle und pragmatische Hilfe bei den eigenen Projekten sein, wie es hier gelungen ist.

Stark profitieren die Kapitel auch von einer großen Zahl an verwendeten Fotografien: Im Schnitt mehr als sechs pro Doppelseite sind eine klare Ansage! Dabei handelt es sich keinesfalls um einen reinen Bildband, wie die bisherigen Ausführungen klar gemacht haben sollten. Doch ohne viele und große Aufnahme wäre ein Band zum Anleiten zum Erwerb neuer Kniffe und Fertigkeiten wohl wertlos.

Zu erwähnen ist dringend auch, dass die Fotos, die Eingang in dieses Buch fanden, durchweg von handwerklich einwandfreier Qualität sind und ebenso hervorragend reproduziert und abgedruckt worden sind.

Nur brandneue Produkte und frische Neuheiten wird ein Leser hier vergeblich suchen. Das resultiert zum einen aus der ehrlichen Ansage, dass vor uns die Überarbeitung eines inzwischen gerade mal fünf Jahre alten Titels liegt.

Auf der anderen Seite ergibt es sich auch daraus, dass hier ein Titel etabliert wurde, der nicht dem „letzten Schrei“ hinterherlaufen soll, sondern einen dauerhaften Nutzen stiften soll. Es gilt also, zugleich modern zu sein und irgendwie auch das Zeug zum Klassiker mitzubringen.

Weil dies gelungen ist, spüren Sie sicher beim Lesen dieser Rezension unsere persönliche und anhaltende Begeisterung. Ohne Zweifel wird der Autor auch unser Arbeiten und unsere Ideen auf längere Zeit mitbeeinflussen – warum also nicht auch Ihre?

Inhaltlich reicht die Fülle der behandelten Themen über beinahe alle Herausforderungen des Landschaftsbaus: Unterbau und Boden, Wiesen und Felder, Wälder, Felsen, Bäume auch aus dem Eigenbau, Sträucher und Gestrüpp sowie Blumen und verschiedene Gewässerformen.

Rund machen die klar in Kapitel aufgeteilte und sinnvoll aufeinanderfolgende Themenvielfalt drei Abschnitte zu höchst unterschiedlichen Ansätzen, die sich für Dioramen eignen: eine Villa mit Parkanlage, ein Schwarzwald-Idyll und ein Römerpark als an die Bahn angeschlossenes Ausflugsziel.

Wer hier angekommen ist, hat ausreichend viel aufgesogen und gelernt, ein so umfassendes Projekt anzugehen. Blickt er oder sie nun auf den Anfang des Buches zurück, dann fällt eine mehrseitige Bildergalerie wieder ein, die auf ihren Doppelseiten zum Einstieg schmackhaft machte, was auf den folgenden mehr als hundert Seiten Schritt für Schritt vermittelt wurde.

Publishing pages:
<https://www.vgbahn.shop>

Ganz und gar nicht einheitlich **Baureihe E 50 der DB**

Was die Re 6/6 in der Schweiz ab den siebziger Jahren darstellte, das war die E 50 für die junge Bundesbahn. Ab 1957 in Dienst gestellt, erfuhr sie während ihrer langen Bauzeit viele Änderungen und Verbesserungen. Eine ganz und gar nicht einheitliche Einheitsellok wird nun in einem neuen EK-Band portraitiert, der eine Lücke in der Baureihenliteratur schließt.

Roland Hertwig
Die Baureihe E 50
Einheits-Ellok für den schweren Güterzugdienst

EK-Verlag GmbH
Freiburg 2023

Gebundenes Buch
Format 21,0 x 29,7 cm
208 Seiten mit 397 teilweise farbigen Abbildungen

ISBN 978-3-8446-6062-3
Art.-Nr. 6062
Preis 54,00 EUR (Deutschland)

Erhältlich direkt ab Verlag
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Nach seinen Werken zu den Baureihen E 10, E 40 und E 41 im selben Verlag ist Autor Roland Hertwig auch die letzte Lok aus der ersten Typen-Familie des DB-Neubauprogramms angegangen. Mit diesem neuen Buch schließt er eine Lücke in der Fachliteratur über die Einheitslokomotiven.

Aufbau und Struktur der blauen EK-Baureihen-Bibliothek bleibt er treu, weitgehend auch seinem persönlichen Schreibstil. Einige wenige Unterschiede sind uns jedoch aufgefallen, die nach unserem Eindruck die Zielgruppe dieser Bücher eher noch vergrößern können und der Lektüre folglich guttun.

Gemeint sind damit die technischen Beschreibungen der behandelten Lok: 34 Seiten macht dieses Kapitel aus, in dem die Merkmale nach elektrischem und mechanischem Teil getrennt behandelt werden. Oft ist dies ein größerer Abschnitt, der für Laien der Lokomotivtechnik nur schwer zu verstehen ist.

Roland Hertwig verstand es jedoch, das Wissen so aufzubereiten und zum Leser zu transportieren, dass alle wichtigen Eigenschaften und Errungenschaften der Entwicklung gut nachzuvollziehen und einzuordnen sind.

Häufig sind dazu Rückgriffe auf die fünf Prototypen der Baureihe E 10 erforderlich. Immerhin wurden darin wichtige Komponenten erprobt, um auszufiltern, was praxisbewährt und zukunftsweisend sein wird. Bei der E 50 betrifft das vor allem den Antrieb, dem eine lange und kontroverse Diskussion vorausging und zu zwei unterschiedlichen Kraftübertragungen führte.

Erprobungsträger für die schwere Güterzugellok des ersten Typenprogramms waren auch die Baureihen E 44 und E 94, die noch auf die Vorkriegszeit zurückgingen; die E 94 wurde übergangsweise aber als schwere Güterzuglokomotive nach- und weitergebaut.

Hilfreich zum Verstehen ist daher auch ein geschichtlicher Abriss, der die Vorläufer der Einheits-Elektrolokomotiven kurz vorstellt, den historischen Kontext auch auf der Zeitschiene und in



grenzüberschreitenden Erfolgen wiedergibt und so das Typenprogramm der DB in seiner Bedeutung einordnet.

Mit 194 Exemplaren und einer Bauzeit von 1957 bis 1973 war die E 50 bzw. spätere 150 nie eine einheitliche Erscheinung. In die lange Bauphase flossen immer wieder technische Verbesserungen und Änderungen ein, von denen nicht alle auch am vorhandenen Bestand nachgezogen wurden. Die Unterschiede, die zumindest äußerlich auffielen, werden in einem eigenen Kapitel aufgezeigt.

Als der Neubau anlief, gab es nur in Süddeutschland elektrifizierte Strecken. Die Bundesbahn trieb allerdings ein ehrgeiziges Elektrifizierungsprogramm voran. Mit ihm wuchsen sowohl der Bedarf an Maschinen als auch ihre Einsatzgebiete.

Dieser Zusammenhang wird in der Chronik der Baureihe E 50 aufgezeigt, aber auch der Niedergang in den Jahren um die Jahrtausendwende. 2003 beendeten die letzten von ihnen den Dienst. Zwei Lokomotiven sind derweil museal erhalten geblieben, die auch in eigenen Kapiteln behandelt werden – dasselbe gilt auch für Unfälle, die sich leider auch nicht vermeiden ließen.

Rund werden die aufgenommenen Inhalte durch die Einsatzgeschichte in den Heimatbetriebswerken über die vielen Betriebsjahre sowie eine lückenlose Abhandlung aller Loklebensläufe. Dies ist gewiss eine Fleißarbeit gewesen, wird dieses Werk aber auch über Jahre hinaus zu einem Standardwerk machen, denn Schwächen haben wir auch an anderer Stelle nicht gefunden.

Randthemen zum Betriebseinsatz sind Kapitel über Einsätze in der Schweiz und im Schiebedienst, das wertende Attribut ist hier der Stückzahl an Maschinen zugeordnet, die in Bezug auf die Gesamtheit dazu herangezogen worden sind.

Eine gewisse Wiederholung stellt im Vergleich zu anderen Baureihenbänden das Thema der Ausbesserung dar, das sowohl die bearbeitenden Werke als auch die Instandhaltungsstufen aufgreift. Hier sind keine grundlegenden Unterschiede zu anderen Baureihen zu erwarten, wohl aber spezielle Inhalte. Ansonsten sind Änderungen der Zeit zuzuschreiben.

Abschließend verdient auch das Bebildern noch eine gesonderte Betrachtung und Würdigung. Wie beim EK-Verlag durchweg gegeben – wir können uns an keine Ausnahme erinnern – sind alle Fotos tadellos reproduziert: Sie sind gestochen scharf, aber natürlich überwiegend schwarz-weiß abgedruckt. Anderes ließ sich auch nicht erwarten, denn viele Aufnahmen gehen zeitlich weit zurück.

Einen fest abgetrennten Farbbildteil gibt es nicht mehr. Verteilt durchs Buch sind immer wieder mehrere Seiten mit solchen Fotos zu finden, was den Eindruck aufgelockert hat. Lob verdient auch die gute Auswahl der Fotografien, die es ins Buch geschafft haben.

Wir hatten nicht den Eindruck, dass diese Bilder schon hinreichend bekannt seien. Es scheint also auch geglückt, ein paar echte Raritäten zu entdecken und einzusetzen. Verlag und Autor gebühren deshalb auch an dieser Stelle Lob und Anerkennung.

In Summe schreiben wir hier über einen weiteren Band, der die hohen Erwartungen dieser Bibliotheksreihe voll erfüllt und fortsetzt. Er bereitet von der ersten bis zur letzten Zeile eine große Lesefreude und vermittelt ein umfangreiches Wissen.

Danach steht er als Nachschlagewerk jederzeit bereit. Weil hier wohl gezielt die Zielgruppe erweitert werden sollte, darf der Verlag auch auf eine höhere, verkaufte Auflage hoffen. Der Kauf lohnt sich für jeden Eisenbahnfreund und Modellbahner mit technischem Vorbildinteresse.

Publishing pages:
<https://www.eisenbahn-kurier.de>
<https://www.ekshop.de>

Intermodellbau Dortmund 2024 Light and Shadow Sides

As the world's largest trade fair for model making and modelling, Intermodellbau regularly scores with large numbers. However, we are primarily interested in model railways and, in particular, in Z gauge. The following report is intended to show what was on offer for the Zetties and what will be remembered – and it wasn't even that little!

When Intermodellbau took place in Dortmund from 18 to 21 April 2024, it was looking back on 41 years of history. Traditionally, it brings together all areas of model making, including drones and robots, which have become a real crowd-puller over the years.

Still quite new at this exhibition are the clamp building blocks, which took up even more space at their second appearance, to be honest, only fit in with classic modelling to a very limited extent.



Trade show souvenirs are always popular: Märklin produced this special car (item no. 80732) with a coal load insert for the Z scale, the prototype of which is employed by Eisenbahn und Häfen GmbH.

Due to a dinosaur exhibition taking place at the same time, the trade fair had to limit the space available for its latest edition: Halls 3 to 7 were used, Hall 2 was not included last year either, and Hall 8 was occupied by the other event.

Dortmund counted 370 exhibitors from 17 countries, but does not differentiate these figures according to individual modelling sectors. The figure of 61,000 visitors is therefore clearer for us: The records of



With Kato, at least the model railway section has become even more international: Japan's leading model railway manufacturer exhibited in Dortmund for the first time. Its range also provided recognisable impetus for the Rokuhan programme.

previous years were almost 100,000 guests, and before the pandemic the total number had levelled off at around 80,000 people.

The drop in visitor numbers therefore appears to be immense, but this does not reflect the impression in the halls. Despite the very wide aisles, there was a dense crowd there for many hours on most days. At times, it was almost impossible to get through and take photos.

We, therefore, suspect that the total number is also largely due to the spatial restrictions. We also cannot rule out the possibility that the allocation of the total area to the individual sections does not adequately reflect visitor preferences. However, this would then mean that the crowds of people in the two model railway halls would be faced with quite empty aisles in other halls.

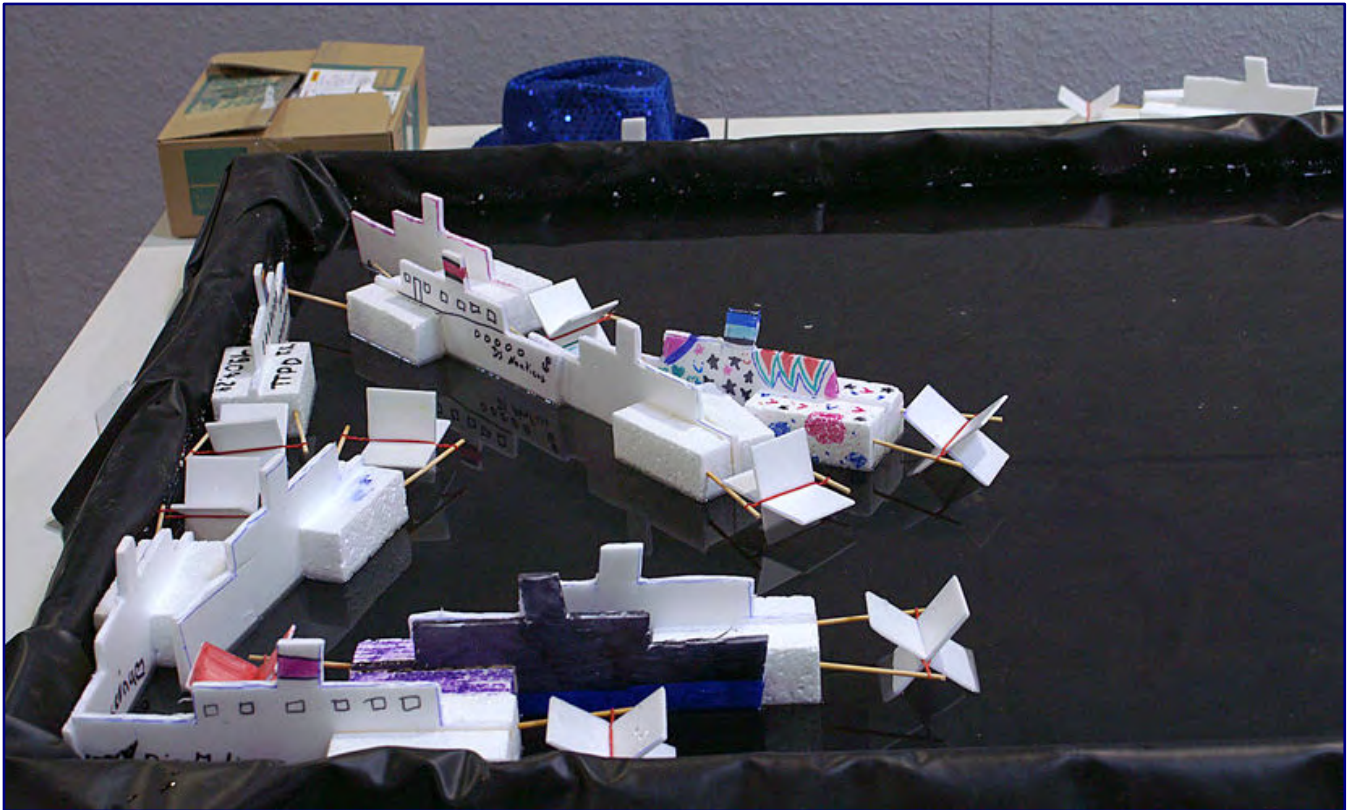
Be that as it may, every trade fair must continue to develop today and, in the future, if it wants to assert itself on the market and survive. We are therefore not surprised that the organisers see new trend toys as potential crowd-pullers, which could be established not only among children but also adults, like the aforementioned clamping stones.

However, the word "model" also requires a template from the wider world, which is why care should be taken not to forfeit its character as the "world's largest trade fair for model making and sport". However, we also see this as a very real danger and are addressing it at this point.

We are concerned, for example, about the noticeable decline in the areas reserved for young people working in classic model making. Children can still have fun with the model railway on the carpet at

Märklin, glue together a kit at the Viessmann stand, or build a simple model ship in the neighbouring halls. This short list is by no means complete.

However, because our young reporters have been searching and researching here for years, it did not go unnoticed that the majority of cross-manufacturer areas have, unfortunately, disappeared and these offers were therefore no longer as conspicuously placed as we knew them from previous exhibitions.



One of the remaining and perceptible handicraft offers for children was this one from the Ship Modelling Association. As you can see, it was well received. Photo: Trainini® / Hendrik Späing

However, the associations remained active in the important areas of shipbuilding, aviation, and model railway construction and offered their own craft activities. In the case of model railways, however, this was unfortunately limited to the BDEF with its “Junior College Europe”, which was relocated to the less popular Hall 7 instead of Hall 4, the centre of this sector.

And, so, the attractiveness of the trade fair for this important target group must at least be questioned. At the same time, we also noticed a great deal of interest in visits from school classes or kindergartens – in the latter case, this could not be overlooked because the participants were then uniformly dressed in neon-coloured waistcoats.

These children certainly don't want to look at layouts that are presented far too high for them, but would certainly like to try their hand at modelling. The reduced offer will certainly have reached and sometimes exceeded its limits here! We are unable to recognise a sustainable concept.

However, two forms of presentation in drone and aeromodelling were explicitly noted as positive: both the exhibition flights and flight demonstrations are similarly secured, namely by acrylic glass panes or



There was also still a large play area at the Märklin stand. Here, the young model railway enthusiasts experienced impressively how Märklin products and Lego type blocks harmonise: May the model railway prevail! Photo: Trainini® / Hendrik Späing

nets stretched over a wide area. As they reach down to the ground, they do not prevent children and wheelchair users from seeing the exhibit.

From the exhibitor's perspective

It is not only in Dortmund that the organisers like to advertise with the enthusiasm of commercial exhibitors. The 2024 edition is no exception. This is usually based on statements from industry giants who are known far and wide.

For example, Eric-Michael Peschel, Head of Event Marketing at Märklin, is quoted as saying: "The halls were always full from morning to night; the number of visitors of all ages was truly impressive. Intermodellbau has once again both convinced and inspired us."

This is also in line with our impressions, but there is regularly no critical feedback from official sources. Small suppliers in particular, whose products are not available from specialist retailers and who like to use a trade fair to generate sales and make personal contact with their customers, often come to a different conclusion.

It has not gone unnoticed by us that the number of manufacturers speaking out in a much more critical manner is increasing: The stand fees charged have reached a level that can no longer be paid because the expected turnover cannot outweigh these costs.

In fact, we have noticed that some suppliers who used to be regulars are now conspicuous by their absence. We would like to add Joswood and MO miniatures to the missing list, on behalf of our gauge.

The fact that these are not isolated cases is demonstrated by large open spaces that have been filled with chairs to create quiet zones, or stands for glasses cleaning products or crêpes that are completely unrelated to the industry. As chairs do not generate any revenue and no additional visitors come into the halls for a French pancake, we conclude that these large areas could no longer be sold in total.



“The Golden Track 2024” brought Märklin a total of five awards. Gunther Schneider (Head of Sales, right) proudly accepts the award for the 50 Kab series in Z gauge from EM editor Peter Wieland.

The organisers at Messe Dortmund have changed several times over the past five years. If we have followed it correctly, almost every edition has brought new contacts to light, and there is no longer any trace of consistency and experience.

However, “new brooms sweep clean” is a saying that is not universally true. From our point of view, those responsible seem to have lost sight of the brand essence. The changes that are tried out year after year seem too arbitrary and ill-considered: It starts with a completely different logo instead of a modernised one; the trade fair remains recognisable only by its name.

The feeling of helplessness can be continued with a colourful interchange of sectors between the halls, a growing and then suddenly shrinking range of children's products and a number of Lego type building blocks and their largest manufacturer, so that the traditional trade fair name should be dropped in one or two years at the latest if these observations continue.

Markus Hönig, Sales Manager at Donau Elektronik GmbH, summarised what makes Intermodellbau special and sets it apart from other exhibitions: “The international composition of the public, in particular, makes this trade fair unique. The modellers are eagerly awaiting this event and the halls are already very busy from the very first day. Our main aim is to understand the needs and wishes of the visitors and determine which products they are missing.”

No other modelling trade fair in Germany is so conveniently located that its catchment area covers not only the Benelux countries but also France and the British Isles. If it continues to lose its identity-creating character, this will undoubtedly make customer dialogue more difficult. Around 25% fewer visitors compared to the good years before the pandemic is a clear statement in view of the renewed interest in such events.

So we hope that our critical view and our arguments will be listened to and received just as critically in order to scrutinise and evaluate them. Because it is also a fact that Intermodellbau has always been and should remain a good trade fair for the Z scale. 2024 was no exception, we just had to take a closer look in some places.

For the first time, the trade fair provided the setting for an important reader award from the publishing house VGB | Geramond. At the end of the first day of the fair, the Golden Track award ceremony took place in Hall 4.



Group photo of the winners of the “Golden Track 2024” award in various categories together with Michael Hofbauer (Editorial Director Geramond Media GmbH, far right) at the evening event at Messe Dortmund.

Märklin came out on top in its own “Z gauge locomotives” category, but surprisingly not with the new V 320, but with the class 50 steam locomotive with a steam tender, which has been known since 1984. The



The surprise of the evening was that the “Hasenkasten” (“Bunny box”) for nominal scale Z was only narrowly beaten by an N scale model in the “N/Z wagon” category in the readers' poll. There had never been anything like it before!

first boiler moulded version (with three instead of four domes) in almost forty years met with the greatest response from readers.

In terms of wagon material, however, the Z gauge models always compete with the larger N gauge models. The small number of new moulds does not allow for a separate category here. This time, however, it was really exciting, as the Hasenkasten, the driving trailer of the Silberling family, was only just beaten by the 1:160 scale.

Never before has a 1:220 scale model achieved one of the top three places. No clearer signal could be sent to Göppingen about how eagerly this model had been awaited. And, so, the next day we eagerly made our way to the Märklin stand to scrutinise our own impressions. You can see and hear the result in episode 27 on [Trainini TV](#).

Colourful programme

Only a few visitors were probably aware of the awards during the trade fair, although the winners always like to present their medals in their display cases to the public. However, the focus of the paying guests was more on the spring novelties, which could now be seen with their own eyes, and the special exhibits of the ideal exhibitors.

Here, Intermodellbau 2024 had what we consider to be a very colourful bouquet in store. In addition to old and new model building sectors, there were also the annual seminars and lectures in Hall 4, while some manufacturers also seem to have recognised that demonstrations of products and techniques at their stands can attract a wide audience.

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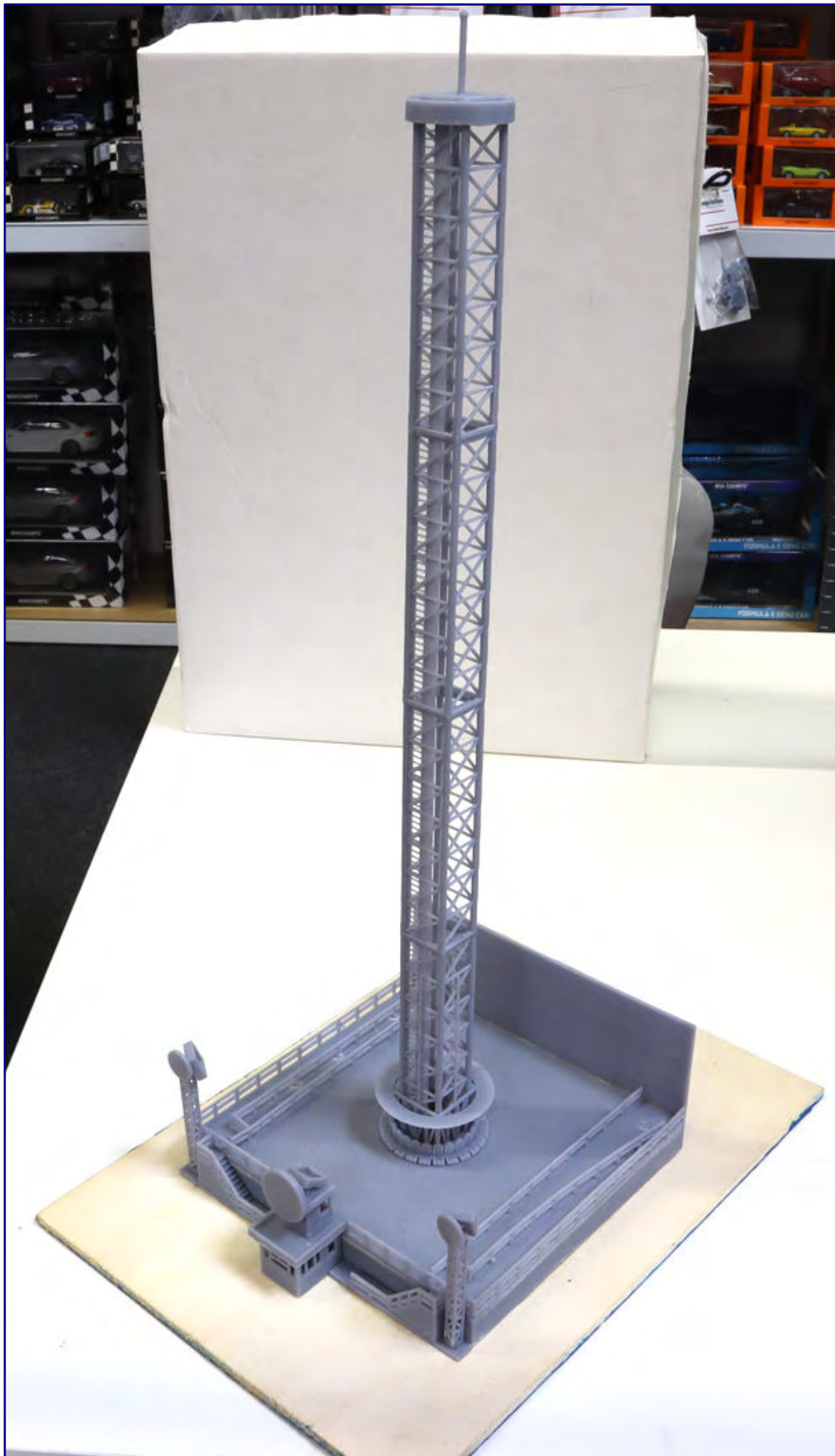


Photo left:
Freefall tower with model height of 55 m from Imprintium (Art.-Nr. D220-3030)

Photos on the right (from top to bottom):
Opel Rekord C Saloon from 1966 (D22 047000), Alfa Romeo Alfasad Sprint from 1976 (D22 120820), Triumph TR6 from 1966 (D22 132570) and horse trailer with opening tailgate from 2006 (D22 905120; all Mini-champs)

Tractor with crane and platform (D220-2004) and brown bears (no Art.-Nr. known yet; both Imprintium)

Special trade fair offers are always a magnet, especially for the first day of the event. Imprintium, for example, had already announced such an offer in our February issue. However, samples of the announced new products, which were not yet available in three-dimensional form two months earlier, were also on display at the stand run jointly with Mini-Champs.

Just a few metres away, we also found a 1:250 scale field railway at Papiermodellbaufreunde (cardboard modelling fans), which was very lovingly designed and carefully built. Especially with such narrow-gauge vehicles, the slight deviation in scale from our scale is hardly noticeable and therefore deserves to be mentioned here as well.

It is already a small tradition at Bahnreisen Sutter to display a Z gauge suitcase on the stand. We noticed that it's not always just a rail bus that's making its rounds here, but now also an SBB locomotive, presumably a pointer to the many destinations that can be booked here.

Since Märklin also upgraded a few years ago, we have also seen an appealing layout at the largest model railway stand at the fair, on which recognisably changing and as up-to-date rolling stock as possible is also shown. However, it was also worth taking a look at the display cases, where samples of new products were on show.



The heat protection car 504 210 [P] of the former Dortmund Union Brewery for the H0 nominal scale is documented by photographs and exudes much more local colour than the special car in the Z gauge. Members of the Insider Club were presented with an adhesive magnet showing a historical Märklin catalogue motif.

One of the announced V 60 locomotives was still missing and the ETA / ESA 150 battery-powered railcar will probably make it to the announced delivery date without one. However, the heavy shunting locomotive V 90 caused quite a stir. What we had already seen in front of the lens in Nuremberg was now seen by many in person for the first time, as surprisingly only an unpainted sample had been presented in Mannheim.

As usual at major exhibitions, Märklin once again welcomed its members of the Insider Club at a separate counter. In addition to kind words, they also received a trade fair gift: a fridge magnet printed with a historical catalogue motif.

There was also help and advice at the large stand, as product management staff in particular were available to provide information at any time. As already reported in the last issue, Märklin also used the trade fair to present a special model of the 185² series on the occasion of the European Championships. It will also be released for the Z scale, in which no sample has yet been seen (Art.-Nr. 88488).



At the Dietmar Allekotte stand, Michael Boost presented his new DCC interior lighting for passenger coaches in the display case. As it can be individually shortened down to a length of just 35 mm, it can be used in a wide variety of models.

In addition to various museum wagons from the Märklineum, there were also special models to buy as souvenirs of this exhibition. For the Mini-Club, this was a red-brown self-unloading wagon from Eisenbahn und Häfen GmbH (80732, see photo on page 31).

However, the model selected for the H0 nominal size appealed to us more because it has a closer and more easily recognisable connection to the city: This was the company-owned thermal protection car 504 210 [P] of the former Dortmund Union brewery in a historically documented design. The car, based on association design drawings, resembles the well-known G 10.

Anyone who was still convinced that model railways could not appeal to young people was proven wrong at the HAW Hamburg stand. Behind the stand was the Hamburg University of Applied Sciences, represented by several students and their professor.

He repeatedly stated that model railways have a firm and important role in university education in engineering professions. And, so, it is very popular because it presents challenges and enables practical experience.

This could be experienced using a train storage facility in the form of a lift. A train arrives there (in the staging yard) and is then moved vertically to another level for parking and storage using the lift function. The entry track is then occupied by a free storage track, unless a train waiting on the opposite side has to clear it first.

The construction and function of lifting bridges, which have to be opened and held in this position with as little force as possible, were also clearly demonstrated. The models could also be printed and built on a scale of 1:220, as we learned. For the students, however, they certainly had a far more “supporting” function.

Other manufacturers such as Busch, Donau Elektronik, Faller, Kuswa, Tams Elektronik, Viessmann, and Werkzeuge Peter Post, which have long been traditional exhibitors at this fair, were represented with well-known products and services. This also applies to EK-Verlag und Geramond | VGB.

Among the dealers present, we must not forget the Modellbahn-Union with its own brands. To the best of our recollection, Schrax was present for the first time, which offered a chance to get to know each other personally and to rub our eyes in amazement.



Schrax presented a very special new product at the trade fair: a scaled wheelbarrow for gardens and construction sites in Z gauge. To illustrate its tiny size, we photographed it on a 2-cent coin.

Not only were the latest new products that we had the honour of presenting in the magazine on display at the stand, but also a modern wheelbarrow as a trade fair surprise. This may come as no surprise to long-time Zetties, but being able to produce it from a single piece with its delicate handles using 3D printing turned out to be a great feat. This only becomes generally comprehensible with the help of a two-cent coin for size comparison.

Michael Boost also used Dietmar Allekotte's stand this year to show models in the display case in which he has installed lighting and digital decoders. Also on display were a shuttle track operated with Shorty models, and a test and demonstration diorama for the Z-Car system.

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Photo above:
To make the difference to the previous North Sea layouts by Dietmar Allekotte visible, we have captured his work shown in Dortmund in a general view.

Photo below:
High-ranking visitors on the Schaarhörn (right), the king is on holiday. Meanwhile, in the background, people are busy working and loading cattle; there is also a cattle market in Müsum today. Meanwhile, the steam railcar brings more guests to the pier for the ship.

As a contrast to the Z gauge selection, he also showed a T gauge ICE running on an oval in a roundabout, as well as a garden railway diesel locomotive of the 218 series with a matching 1:220 scale counterpart. Those who gave the display case more than a cursory glance also got to see a new lighting board for passenger carriages, which can be shortened in many ways and can therefore be used for a wide variety of models.

However, the main attraction of the stand was the small "Hp Sieltief" peep-box layout, lovingly designed by Dietmar Allekotte as usual and once again based on North Sea motifs. Once again, the familiar motif elements of a small stopping point, the bridge over a dyke, a short stretch of sandy beach and a cow pasture were used.



We also look beyond the horizon and take a look at three selected H0 gauge layouts. This first one based on Norwegian motifs is called "Nordslandsbanen" and was shown by the Masslijn MST.

On the other hand, a commercial enterprise where boat parts such as ship propellers are repaired offered variety. It was given a siding to the branch line, and the loading hall can also be driven out from behind the scenery, invisible to the viewer.

The small layout "Müsum" by Kai Wüstermann, which is set on the coast of Schleswig-Holstein in 1896, has not only delighted us. Even though the organising body, the Modellbahnverband in Deutschland e.V. (Moba), has given it the attribute "German premiere," long-time Zetties know very well that this fine piece has already been to many exhibitions but had never been displayed in Dortmund.

Once built as a competition entry for the railway journal, we were not allowed to portray it during the period of performances and voting. Nevertheless, it subsequently appeared in various exhibition reports, as it could be admired almost regularly, especially in Kai's Nordic homeland.

The main motif is the bathing steamer jetty with its railway siding in the small town of Müsum, whose buildings can be seen in the background. There is a lot going on in the small town and on the promenade, as it is market day and there is also a lot of livestock and fuel waiting to be loaded. A small shipyard on the left-hand side of the layout is hard at work.

The exhibits are lovingly and vividly designed and give a very authentic photo of what life must have been like near the coast in imperial times. Only at second glance does it become clear how many of Kai Wüstermann's own constructions he has created and incorporated here. For our episode 27 on **Trainini TV**, we asked him to describe and report on the depiction himself.



Rik Martens' "Carolo Metal" made do with a single track, as there was plenty to keep the eyes enthralled for a long time off the tracks.

The Noch stand is always a popular place to go. There were also demonstrations here, for example on patinating rolling stock, but most interested visitors are always attracted by special offers at the fair.

Among them was a number of Rokuhan articles for Zetties, which certainly pleased Tony Ichikawa, who had travelled from Japan with his wife to get a personal impression of model making in Europe and to compare it with that in his home country.

It also finally gave us the opportunity to meet Hiroshi Kato (managing partner of Kato), whose product range is likely to have provided a lot of inspiration for Rokuhan. Kato, Japan's largest model railway manufacturer specialising in the N scale, took part in Intermodellbau for the first time.

At the International Toy Fair, it was also agreed to hand over to Noch any reader suggestions for new car models for colour 3D printing that had been received and prepared at Intermodellbau.



A circular layout where we don't even know which of the lovingly designed details to show you: "La Petite Ceinture de Paris" by Paul de Groot. Keep your eyes peeled for where it will be shown next and go there yourself!

After the end of the trade fair, we then honoured this with a visit to the editorial office followed by a barbecue. The following personal exchange was equally enjoyed by all participants, because now the model railway was allowed to take a back seat. Time passed far too quickly, because the next day at 9:00 a.m., we were back at the Dortmund Exhibition Centre.

And there we visited one last highlight. There were many beautiful layouts of all gauges to see in Dortmund, and we were able to capture some of them in photos and on film. What stood out for us was the circular "La Petite Ceinture de Paris" (The Little Belt of Paris) by Paul de Groot, a master of his trade. Set in Era I, he focussed on life in Paris in earlier years.

Exhibitors with Z gauge relevance:

<https://www.busch-model.com>
<https://shopware.donau-elektronik.de>
<https://www.eisenbahn-kurier.de>

<https://www.faller.de>
<https://www.geramond.de>
<https://imprintium.de>

<https://www.luetke-modellbahn.de>
<https://www.maerklin.de>
<http://www.mazero.de>

<http://michas-moba.de>
<https://www.minichamps.de>
<https://www.modellbahnunion.com>

<https://www.noch.de>
<http://www.peter-post-wekzeuge.de>
<https://www.rokuhan.de>

<https://www.schrax.com>
<https://viessmann-modell.com>
<https://www.vgbahn.shop>



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Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Feedback on our call from **Trainini TV** Episode 25:

Thank you very much for the great suggestions for the expansion of our Z gauge vehicle series. There are some very interesting models among them that we hadn't thought of, and I'm already looking forward to discussing the ideas with my colleagues in our new products team.

Dr. Rainer Noch (Firma Noch), Wangen im Allgäu

On the initiated discussion about "good" pantographs:

I have just read the new Trainini. I can add something to the discussion about the pantographs. You know that the look of the vehicles is my priority.



I have changed something for the showcase models that is very close to reality, as the transformer on the dining cars is the same height as the pantograph. Here, I have removed the lower part and only glued on the upper part.

With the locomotives, I pressed everything down so hard that it cracked a lot. Fixed with superglue. The second goes up as normal. This means that the lowered one is at the correct optical height. With this I am 100% satisfied with this solution as it is extremely close to the original.

Michael Fuhry, by E-Mail

Regarding exhibition report in Trainini® 4/2024:

Many thanks for the very interesting April issue. As I was also a visitor to On traXs in Utrecht this year, I would like to contribute another photo to the "Ode aan de modelbouwer" layout.



Huib Masskant also had steaming hot coffee on the workbench to offer at On traXs He also demonstrated this effect to our reader. Photo: Dirk Rohwerder

The builder Huib Maaskant allowed me to take this snapshot of a steaming hot cup of coffee in the workshop. He sporadically showed the gag to the public. I hope it's easy to recognise?

Dirk Rohwerder, Sprockhövel

Pleasant turnaround after staff absence:

You published my article on crZ in the July 2023 issue. In the meantime, at the end of February 2024, I received my digitised model of the BR 143 (Märklin 88431) back from Mr Ribatzky. As a small surprise, switchable driver's cab lighting was installed.

As agreed, Mr Ribatzky covered the costs of developing the circuit board and installing it, so I only had to pay for the decoder. Many thanks in this way to Ulm.

Peter Taubert, Schwerin

Editor's reply: Thank you very much for reporting this positive experience. We also met Christian Ribatzky in person in Altenbeken and can only confirm the positive impression expressed here.

Meeting with our editor-in-chief in Altenbeken:

I was very pleased to meet you in person at the Z-gauge weekend. I was also very impressed by the modelling quality of the layouts and dioramas on display. The builders of many H0 model railways could take a leaf out of your book. I also took a lot of photos, contrary to my usual habits.

Wolfgang Franke, by E-Mail

Three years of Märklineum:

Within three years, the Märklineum in Göppingen has established itself among fans of the museum all over the world. Exhibits, productions, and interactive media technology cast a spell over visitors on four floors. Very special offers await visitors for the anniversary in June.



For three years now, visitors have been walking through this entrance, which leads them into the Märklineum, where a multifaceted exhibition on the history of the Märklin company awaits them. Photo: Christoph Maier

Right in the inner courtyard, you can see an original class 44 steam locomotive and a garden railway layout. After entering the exhibition, the tin toys from the early decades of the company open the presentations. These include doll's kitchens, ships, early railway models, and the company's own race track, as well as Märklin metal construction sets.

The floor above is dedicated to the many Märklin locomotives in all gauges. This also includes models of the Trix and LGB brands. The highlight, however, is a virtual application on a scale of 87:1. In the enlarged replica driver's cab of the H0 gauge model of an E 10 locomotive, visitors use VR glasses to float into this model in 3D animation, which passes by like a moving exploded drawing.



Märklin boss Florian Sieber personally tests the virtual journey in an H0 model of the E 10 series, sitting on the 87:1 replica of its model driver's cab.

Märklin also promises a glimpse into everyday production with the help of an assembly line that impressively demonstrates the various steps involved in assembling a model as part of “transparent production.”

The 100 square metre display with day-night projection, which occupies the lowest two floors, also justifies a long journey. Accompanied by a multimedia wall and an impressive sound installation, it changes within fifteen minutes from sunrise, accompanied by birdsong and ringing church bells, to sunset and the chirping of crickets at night.

The Märklineum will soon be offering a special supporting programme to mark its third anniversary. During the anniversary week from 18 to 23 June 2024, everything will be themed “Märklineum & Friends.” There will also be an attractive 2-for-1 offer for a visit to the museum.

Hagen von Ortloff will also go on a treasure hunt with visitors on Tuesday, 18 June at 11:00 and 14:00 and present exciting rarities from the exhibition. An autograph will also be available on request, afterwards. The ten per cent anniversary discount in the museum shop is intended as a thank you to visitors.

Into the summer break with new products:

The duo from NoBa-Modelle went on their summer break at the beginning of May, but not without presenting some new products. Production and delivery will resume in October. Customers can then also look forward to the old E 91⁰ locomotive (item no. 5025RF), which is now exactly to scale.

This is fitted with a shortened Märklin crocodile running gear, which can then no longer be swapped back into the basic locomotive. Purists for whom the correct length is important will be happy to accept this, especially as this model naturally looks harmonious all round.



Development goal achieved: Shortly before the start of the summer break, NoBa-Modelle presented the class E 91⁰ locomotive in a scale version (item no. 5025RF) with full system couplers. Photo: NoBa-Modelle

An important development goal, which was only achieved after many months of pondering, designing and testing, was to equip the locomotive with a complete and functional coupling. With Jörger uncouplers, this also allows the locomotive to be used as a shunting locomotive that uncouples above the magnet.



The new "Swap Body" tank container (2045RF) is destined for the company's own carrier wagon, on which it is also loaded here. Photo: NoBa-Modelle

Although the prototype from the Oberhausen-Osterfeld Süd depot was also used in freight transport in the Ruhr area, the prototypes spent almost all of their last years of service in Munich and in the Ruhr area at least mainly in shunting services. For the NoBa models (<https://www.noba-modelle.de>), the full operational value was extremely important here.

The new “Swap Body” tank container (2045R) is intended as a load for the company's own container wagon, which will also provide a varied appearance in block trains. It is also available as a finished model (2045RF).

The same also applies to the Volvo F 12 lorry as a tarpaulin trailer “On the move” (6927R / 6927RF). The Freden railway station (4135R), on the other hand, always wants to be completed by the customer and is the last new model to be presented before the 3D printers are put on hold for a few months.



The Volvo F 12 truck has a colourful design and pulls the tarpaulin trailer “On the move” (6927R / 6927RF). Photo: NoBa-Modelle

Appearance of GMmodelli Torino:

The GMmodelli Torino website (<https://www.gmmodelli.com>) has been redesigned and another new product has also been published there. The new, single-track tunnel portal based on the FS model (item no. CF-0003/1) is typical of Italian architecture, but will certainly also look very decorative and appealing on layouts based on German models. It is already available.

It also states that another new product will be announced soon.



Delivery of Modellbau Laffont:

We received the news from Aachen that a new product has become available in the form of the Wiesen railway station (item no. Z6801). The wooden building in alpine style does not necessarily have to be built on a narrow-gauge line.

Photo left:

The Swiss railway station Wiesen (item no. Z6801) also cuts a fine figure on a standard gauge line. The small station building can be used well away from large metropolises.

It looks just as appealing as a reception building on a standard-gauge SBB line next to crocodiles and other Swiss locomotive legends. The laser-cut kit can be purchased directly from Modellbau Laffont (<https://modellbau-laffont.com>). A direct link to the Z gauge range can be found on our website.

A recent Märklin delivery:

Märklin delivered the class 221 (item no. 88208) ocean blue and ivory-coloured diesel locomotive to dealers during the reporting period. The prototype was located in the Oberhausen-Osterfeld Süd depot, the outgoing depot for this diesel locomotive was.

The prototypes earned their bread of mercy here in mining traffic, primarily on non-electrified lines such as the Angertal, but were also used in front of winter sports trains to the Sauerland or on special trips.



There is certainly a better way to celebrate the 50th anniversary of the DB's ocean blue and ivory colour scheme this year than with a locomotive from the V-200 family. Nevertheless, the class 221 in its last colour scheme (item no. 88208) made it on time.

Even if the colour scheme, especially on the round front, was not very appealing due to the lack of design features, the model, retrospectively, recreates an important piece of railway history and is, therefore, becoming increasingly popular.

Märklin put it on the market with a clean paint scheme and fine and legible printing using the latest technology. This also includes chrome rings around the lower locomotive lanterns, while the upper ones are prototypically without them, and colour-contrasting grilles behind which the typhoons (horns) sat and emitted their signal to the front.

The model with bell-type armature motor has benefited from the sister locomotive of the 220 series, which is why this locomotive also has red tail lights, both illuminated by maintenance-free LEDs, in addition to the direction-dependent headlights.

New from Yellow Dwarf:

Yellow Dwarf did not remain idle in June, either. We were notified of two new products that once again focus on loading and storage: With “Storage Accessories III – Crates” (item no. 60248), these are stored on pallets, as are the “Storage Accessories IV – Barrels” (60249) and are therefore also suitable for scenes that appear to come to life with the help of a forklift truck.



Yellow Dwarf is further expanding its range of loads with “Storage Accessories III – Crates” (item no. 60248; photo left) and “Storage Accessories IV – Drums” (60249; photo right). Photos: Yellow Dwarf

The manufacturer's pages with the possibility of purchase can be reached via a link from our pages or directly via <https://www.yellowdwarf.eu>.

Further Busch new products:

The Viernheim-based accessories specialist has also started to distribute its new products in instalments throughout the year, and the current brochure 2/2024 contains some repeats, but also some genuine new products, which we would like to mention no.

These are, for example, two tree packs “Mixed forest” (art. nos. 6488 / 6588), which are declared for larger scales and contain 10 and 15 trees respectively in pipe-cleaner design.

This inexpensive, but no longer contemporary design is probably primarily suitable for nostalgic layouts, which is why the specimens are so small that they could be used without hesitation for the 1:220 scale.

We know the “Aqua Liner for water design” (7169) in a similar form from the Noch range. It is used for modelling moving water, creating a waterfall or icicles. Transparent modelling water (7580), which can also be coloured with a colour set (7581), is available to go with it.

The modelling snow paste (7173) glitters and can be kneaded directly into shape to form snowmen, igloos,



Three-part colour set (item no. 7581) for the new modelling water (7580). Photo: Busch.



The manufacturer created this demonstration scene with the new modelling water and the corresponding colours. Photo: Busch

snow globes and snow caves. It is also a good choice for snow-covered roofs or effective tyre tracks in the snow.

Our readers should take a look for themselves to find out whether the new “silver corrugated cardboard” (7209) and the photos from the “Sgraffito” sheet (1112), a scratch plaster technique from the years between 1930 and 1960, can actually be used for Z gauge.



Soap operas at WDW FullThrottle:

Two special ACF bulk goods wagons with cylindrical loading space and cover against moisture have been released as new products by WDW Full Throttle. For this collector's pack (item no. FT-COL68), the wagons were given new road numbers.

Included are an orange and a yellow copy for the detergent brands Duz and Tide, which also became popular in the post-war period with advertising inserts in the packs

Photo left:

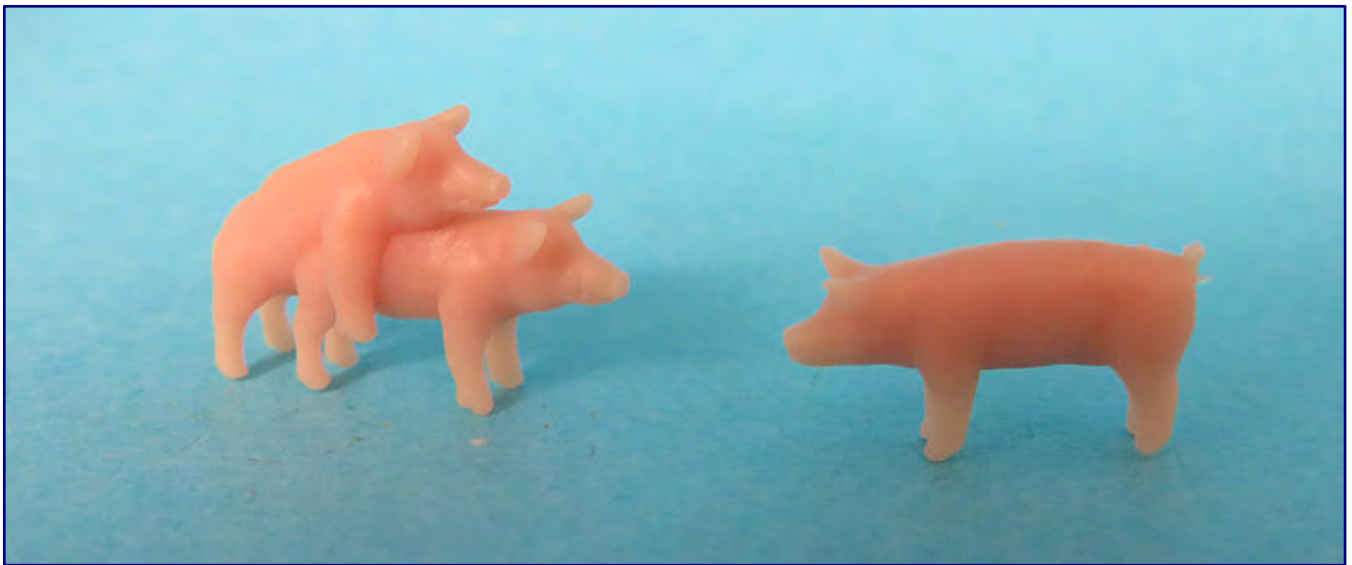
New collector's pack with reference to soap operas (item no. FT-COL68). Photo: WDW Full Throttle

and courted the favour of American housewives on television. It is partly due to these processes that we still talk about “soap operas” today.

The trolleys are available directly from <http://www.wdwfullthrottle.com>, or, in Germany, from Case-Hobbies (<https://case-hobbies.de>).

Pigs from Cologne:

Klingenhöfer Miniaturen (<https://www.klingenhoefer.com>) exhibited its “piga” in Altenbeken and also presented it to us. But don't worry, the new delivery is not that disreputable! The Z gauge figure pack of the same name (item no. Z-TS23) contains a single domestic pig and a sow being mated by a boar.



The new figure pack “piga” (item no. Z-TS23) had already been presented by the manufacturer in Altenbeken. Photo: Klingenhöfer Miniaturen

What used to be commonplace on farms and provided consumers with a supply of meat and suckling pigs will also find its place on the model railway. It doesn't have to be presented in full view of small children...

A summer party to mark the anniversary:

Modellbahn Union from Kamen in Westphalia is celebrating its 25th anniversary with a big summer party on 28 and 29 June 2024. Such a summer party is held every two years, but this time it is under the special impression of the company's birthday.

Everyone is welcome, according to the company. That's why the programme will be correspondingly broad. In addition to presentations and demonstrations by manufacturers as well as opportunities for customers to ask questions, there will also be plenty to see and do.

The visit should also be explicitly worthwhile for families with children. As the Modellbahn Union is also one of the most important supporters of our activities for youngsters, **Trainini®** is also taking part in the festival with a four-person delegation.

For example, we present regulars' table boards as an idea for model railway meetings where the children can control the trains themselves, as well as organising manufacturer's own products (article by editor Ralf Junius).

The beer mat crafting events have been a tradition for several years now and have inspired many children and their parents to take up modelling. In a small area, they can learn the techniques and basics of modelling free of charge and give free rein to their own creativity. Model building demonstrations and explanations for experienced model railway enthusiasts are also planned (contributions by editor-in-chief Holger Späing and Hendrik and Kristin Späing).

And so, we call on you to visit us and the Modellbahn Union in Kamen, to chat and learn or simply to exchange experiences. You will have the opportunity to do so on Friday between 13:00 and 18:00 and on Saturday from 10:00 to 18:00 (change compared to the advert on page 46), the date can be found above.

The destination address for your visit is Gutenbergstraße 3 in 59174 Kamen. We recommend travelling by car for those who live further away. In the immediate catchment area, a bicycle will certainly prevent a lengthy search for a parking space.

The nearest railway station, whose station building the Modellbahn Union has also implemented in its own programme, is Kamen. From there you can reach your destination by public transport or taxi or, if you are willing, in 10 minutes on foot (approx. 1.0 km).

Herpa should not be forgotten:

The cardboard kit "Airport Tower" (item no. 573061) was launched from Dietenhofen. The parts to be cut out together form a 34 cm high control tower for airports, which can be set up on the floor plates (558969-001) not yet supplied.

On 29 June 2024 from 9:00 to 17:00, the manufacturer is once again inviting visitors to a summer festival at the company headquarters in Dietenhofen, where its own products will be on display and can be purchased. The company museum will also be open to the public, and there will also be insights into production.

Locomotives and carriages from AZL:

American Z Line will be supplying its customers with locomotives in May. New on the market here is the SD70ACE from Union Pacific, which has a historic SP livery (item no. 63109-1B). In terms of the technical equipment shown, it represents the current operating status.



The EMD SD40-2 runs for the Rock Island (64215-1 / -2) in a colourful appearance, but is also presented as the SD40-T2 tunnel locomotive of Kansas City Southern (64105-1 / -2). They also show individual equipment features of the prototypes according to the current AZL standard.

ES44AC from General Electric (62413-1 to -3) of the BNSF in historic "Heritage II" livery. Photo: AZL / Ztrack

Last in the group is the ES44AC from General Electric (62413-1 to -3) of the BNSF, also painted in a historic livery. The wagon stock is completed by a heavy Canadian National semi-baggage wagon (74013-1 / -2).



UP's wide-view goods train accompanying wagon (921021-1), which came to the railway company by special means. Photo: AZL / Ztrack

The Ortner bulk freight cars are now appearing for the railway company Georgetown / GRR (905363-1 / -2) in packs of two. A wide-view goods train escort car (921021-1) was transferred from the Missouri Pacific to the UP stock, marking the last delivery of this month.

Wonderland film on DVD:

Miniatur Wunderland Hamburg has announced that its documentary film, which

had a successful run in the cinema, will be available to buy on DVD from 7 June and on Blue-Ray five days later. Signed copies can already be pre-ordered from the operator.

New provider Nano-Models:

Under the name Nano-Models, a new model vehicle supplier called Nano-Models has so far only appeared on the Ebay trading platform. The Italian market newcomer from the Milan area does not yet have its own website.

From design and printing to packaging and sales, everything is done in-house. The current range mainly includes various lorry models with a strong focus on Era III and early Era IV.

In order to avoid conflicts, the brand representations of the lorries cannot be identified on the models. Similarly, no specific vehicle types are described and slight deviations are obviously accepted. Nevertheless, we were impressed by the miniatures, which are printed in several parts from resin, assembled and painted.

As an example, we have selected a timber trailer pulled by a heavy short bonnet (Mercedes-Benz). Our prototype comparisons indicate that the original was probably an export model that was not offered in identical form in Germany.



The timber semi-trailer with detachable load is pulled by a heavy Mercedes short bonnet, which we believe to be an earlier export version.



The “Millipede”, also once built by Mercedes-Benz, has never been offered in Z gauge. Nano-Models has now brought it to life in 1:220 scale, equipped with Trilux rims.

There was also great joy about the “millipede” from the same commercial vehicle manufacturer, which reached us as a platform-tarpaulin version with trailer. Two steerable front axles and only one driven rear axle gave this front-wheel drive vehicle its unusual and striking appearance.

This was due to the strict load restrictions in the domestic market. When they were relaxed, there was no longer any economic reason to continue producing this unusual vehicle, which is so popular today.

Imprint

ISSN 2512-8035

Bibliographic information of the German National Library: The German National Library lists this publication in the German National Bibliography. Detailed bibliographical data and editions can be found in the DNB catalogue at <https://portal.dnb.de>.

The publication of **Trainini German Magazine for Z Gauge** is voluntary and non-commercial. **Trainini German Magazine for Z Gauge** does not aim for any sources of income. This publication is governed exclusively by German law.

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International Edition

Free,
electronic magazine
for railroad enthusiasts
in the scale 1:220
and Prototype

www.trainini.eu

Published monthly
no guarantee

ISSN 2512-8035

Trainini

German Magazine for Z Gauge



Z Gauge Weekend in Altenbeken

Bringing Figures to Life
Model Building Renovation

Introduction

Dear Readers,

This issue has taken an extraordinary amount of time. There was a lot to analyse, photograph, and edit before we were able to present you with another packed magazine.

One important reason for this, but not the only one, was the Z Gauge Weekend in Altenbeken. The pandemic is over and we were excited – our magazine was well represented at the event – about the number of visitors and participation.



Holger Späing
Editor-in-chief

And unfortunately, we have to and want to criticise. We had a lot of fun and saw a lot of great work. But that can't hide the fact that many table metres remained unused and the number of visitors left something to be desired from our point of view.

We will have to talk about this in order to uncover and analyse the causes and draw conclusions for the future. Unfortunately, we have so far had the impression that precisely this process is not taking place, and that the course has already been set to continue the downward slide that has already begun.

We don't want to have to ask ourselves in a few years' time who was right, because this focus is always only on the guilty parties. Our aim is to be honest and self-critical, and to develop suitable solutions for a successful future.

Altenbeken is still a long way from sinking into insignificance within our Z gauge, but let's be honest: Who is aware of this world's largest Z gauge meeting outside our community? Not many people do, and it's bad that this is especially true for the immediate neighbourhood.

Such events are always the most expressive form of public relations work and it is precisely this that almost never takes place. Potential must be recognised and also raised, otherwise, the circle of insiders will only become smaller over time, and with it the number of visitors will continue to decline.

But let's look ahead: today we have the honour of presenting a new small series supplier for our nominal scale with its first product. We also have the honour of delving deeper into a top-class self-build topic.

And things are also happening among the major manufacturers. Noch has been particularly innovative and has known how to use colour 3D printing almost perfectly for our scale. We have therefore included a separate article on human figures and how to set them up effectively.

As always, we supplement the topics with two literature presentations, news items and, above all, letters to the editor. My colleagues and I would like to express our sincere thanks for the current very high level of interest from countries outside Germany. This shows us how important and appreciated our work is. I hope you enjoy reading this issue!

Sin-Z-erely,

Holger Späing

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We thank Reinder Rutgers for his contribution and Eisenbahnstiftung for their photo support.

Date of publication of the German language version of this issue: 30 June 2024

Cover photo:

The Siemens Europrinter 127 001-6 was tested in passenger and freight traffic before it was later used almost exclusively to transport freight as a Dispolok. Even before it was repainted in yellow / silver, it is seen here travelling on a diorama by Phillip Meyer and is reflected in the water of the lake below.

Market entry of Nordmodell

A different Kind of half-timbered House

On the layout, half-timbered houses are among the most popular building models and can be found in the programme of almost all kit suppliers. Laser-cut hard cardboard has also become a common standard and is no longer unusual. And yet, the supplier Nordmodell still managed to surprise us. We would now like to explain why this is the case.

Nordmodell is a manufacturer that has so far only produced for the N nominal scale. In this scale, it is known for fine and accurately fitting models at a high level. This small business works with light cutting technology and 3D printing. Together, they enable a wide variety of shapes and models that can also be produced in small quantities.

These are actually ideal prerequisites for getting involved in the Z gauge market. And that is exactly what has now happened: Eberhard Herbst has informed us that he has decided to gradually offer his programme in this scale as well. He cites the customer response and increasing demand that he has heard at trade fairs and exhibitions as the reason for this, as Nordmodell is regularly present at such public events.



The two manufacturer's samples of the "half-timbered building in refurbishment" (item no. LC-220-332) show different construction variants that can be freely selected by the customer.

The premiere in 1:220 scale takes place with a half-timbered house, labelled "Half-timbered building under renovation" (item no. LC-220-332). The item number indicates the hard cardboard material used and the Lasercut (LC) production process, while the end number indicates the model selected, as it is already known from N scale.

We also find a very apt description on the manufacturer's website that perfectly summarises the appearance, possible history, and function: "A pretty, small half-timbered building that is in need of renovation or is currently being renovated. It is a small residential building, formerly probably part of a part-time farm, which is now being converted into a residential building."



The plastered half-timbering makes for a harmonious-looking building. The panelling of the gable is worth a note: with wood as a natural material, there is always the possibility of a surface with a disturbing appearance. In such a case, painting all similar parts before installation would be a sensible measure.

The building offers a degree of flexibility that is unusual for construction kits, as the degree of visible, i.e., already completed, renovation work can be individually selected and displayed. This is made possible by selected components, which are included in two versions. There is the option of displaying the old compartments, which have been filled with straw and clay plaster, as well as building them with brickwork.

Windows, doors, and gates, on the other hand, always represent the historical and very attractive condition. This not only fits in perfectly with the former purpose, but is also a stylistic element for modernisation work in order to permanently preserve the special charm of such buildings.



Both house models present a view of walls and wall surfaces that have already been renovated. The typical structure of the gable walls of many historic buildings can also be clearly seen from this perspective.

The conservation regulations can also have an influence on the model. The impression that the entrances lead to the stable and a hallway, as in earlier times, definitely has a positive effect on the viewer. The majority of our readers will probably remember templates that have been and are being maintained in an exemplary manner using this approach.

Looking at the miniatures

The manufacturer provided us with two sample buildings showing different superstructure variants of the same kit.

We have used this to convey to our readers the versatility that leads to a very individual photo on the layout and makes the kit universally applicable – after all, half-timbered houses have not been just a regional phenomenon.



If we continue upwards with the camera from the same perspective as above, we can also see a very attractive building from an elevated position.

continued on page 8



These two photos show one of the two show houses from all four sides: The street side is completely renovated with brick-filled compartments, while the courtyard side still shows plaster infill. Refurbishment work is already continuing on one of the two long sides.

Hard cardboard, photo cardboard, and real wood were used, which was cut and engraved using a laser. This can easily be seen on the finished building. The core of the building, on the other hand, is made of grey cardboard, which can only be seen on the underside after assembly.

As this material has a lower density than hard cardboard and can absorb moisture more quickly, we suggest that you post-harden these inner components, at your own discretion, before joining them together.

Pre-treatment with a deep penetrating primer, as recommended by another building manufacturer, or impregnation with liquid superglue, which is absorbed and not only seals but also stiffens the parts, would be conceivable.

With both materials, however, it should be noted that soaked grey cardboard parts must be protected against warping and waves until the moisture has been completely released into the ambient air! The same applies to colour treatment, which is not necessary but can also be an individual requirement. Optional ageing can be applied with powder paints.



The floor of the building and the interior walls are made of grey cardboard. As this material is more sensitive to moisture than hardboard, it may be worth considering sealing these parts with a deep penetrating primer before installation.

A sharp craft knife with a cutting mat, nail scissors, tweezers, toothpicks and wood or laser-cut glue are required for assembly, alternatively a superglue (liquid or gel). We recommend an all-purpose adhesive such as Uhu Kraft for gluing film behind the windows to imitate glass panes.

We would also like to briefly summarise the scope of delivery: In addition to the grey cardboard basic structure already mentioned, finely engraved wall elements in clinker brick and plaster imitation, very fine



We also show a refurbished gable wall in a macro shot. Here, too, the house shows no weaknesses and satisfies the viewer. The natural stone plinth, which forms the foundation of the historic building, is now clearly visible.

window imitations made of through-dyed photo cardboard and gable boarding, as well as half-timbered parts made of real wood await the buyer.

On the crafting table, the half-timbered house grows from the inside outwards and slowly upwards at the same time. This is also no different to all the hard cardboard buildings we have already presented here.



The views of the interior from various construction stages illustrate the room layout, which both makes the interior design appear credible and allows only selected rooms to be illuminated. Incidentally, the photos are taken from the manufacturer's well-illustrated instructions. Photos: Nordmodell

However, it is not always a matter of course that a model is also equipped with interior walls including doors and storey ceilings. This ensures greater stability on the inside, but also opens up the opportunity to illuminate rooms individually or, if there is a view, also to fit them out.

This is certainly interesting here in the area of the fronts due to the larger and more pane surfaces, especially as the large doors can also be shown in the open state. To make things easier for yourself, both the base plate and the floor slabs should have cable openings in places that will not be visible later. It is best to place these in the edge area so that strands do not catch the eye when looking through the windows.

The unique selling point of the supplier Nordmodell remains the half-timbered infill panels. Two complete component arches are included in the scope of delivery. They allow the building to be built completely in red brick structure or plaster, as well as in a state during renovation, as is the case with our samples.



In addition to the parts for the tiled roof, the kits supplied also include a ridge and the elements required for the chimneys: Brick structure and ring anchor on its upper end.

Compared to the “Dörpede” and “Kallental” building series from the 1zu220-Shop, to which the chosen architectural style also fits well, a decision in favour of one or the other alternative does not have to be made before purchase.

Certainly not every building on a layout should be shown in a state of ongoing renovation, but for individual houses, this is certainly a very good idea. The façade, which then appears somewhat more restless, attracts attention and emphasises an individual half-timbered house from its surroundings. This opens the way for ideas in the detailed design that would otherwise probably go unnoticed.



The last view is from the perspective of our figures: the building easily withstands this, too. The large roof overhangs catch the eye, which were once deliberately chosen for the building, and make it stand out from many other half-timbered houses.

Looking back on what we have written, we recognise the launch of the new supplier for Z gauge as a very good incentive to build more individually and thus give your own layout a special feature. And also because of the combination possibilities with the offers of other manufacturers, we honour this first work for our scale with a nomination for the new releases of the year 2024 in the accessories category.

Manufacturer of the models:
<https://www.modellbausatz-nord.de>

Layout details (part 7) **Smaller than Life**

Today, we can finally continue our unintentionally interrupted 2023 focus topic series on layout details. This penultimate part is dedicated to the people that breathe life into our layouts. Nobody can or wants to do without them, but Z scale figures can be a challenging topic, especially when they simply don't want to seem alive.

As long as there have been model railways, there have also been manufacturers producing miniature figures to complement the scenery. The classic that has been around for decades is Preiser, which has outlived many other brands. Its injection-moulded tiny figures were also present right from the start of Z scale.

In recent years, however, things around Preiser figures have, unfortunately, become quieter, and hardly any new figures in previously unknown poses or areas of life have seen the light of day. At the same time, products from small volume producers have become all the more important, while traditional accessory suppliers such as Faller and Noch have recently also introduced their own figures series.



Figures bring layouts to life, like here on a module by our editor Harald Fried. But, it's not just a sufficient number of human replicas that is crucial: it is also important to use them in a targeted way that allows us to recognise scenes from real life.

The most innovative new products have come from the Allgäu region-based company Noch. They are employing a completely new manufacturing process whose results we would like to discuss here. But, first, we would like to take a moment to reflect on what actually brings model railway scenes to life. Only in this way will we be able to select our figures correctly and set them up appropriately.

As so often at this point, it is now time to get out into the “wild” and explore some prototype situations! Unfortunately, it is no longer so easy to photograph people and capture interesting scenes and document them so that we can recreate them at home.

The magic word in this context is “right to one's own image” and the consequent necessity to adhere to the strict requirements of the European General Data Protection Regulation. It is no longer possible to take targeted photos of other people without prior authorisation. We believe this is understandable and correct, even if it imposes restrictions: If we ask people beforehand and receive their permission, the resulting images will certainly seem staged and not very random or natural.



The best stories are still written by life itself, but, when it comes real-life images, we usually have to resort to historical scenes: Travellers looking for a train connection on the departure board at Passau main station on 4 May 1962 draw our attention to the timetable with the direction of their gaze. Photo: Walter Hollnagel, Eisenbahnstiftung

So, for the most part we can only observe and later tap into our mentally recorded images. But, sometimes, we can also fall back on photographs in which people were unintentionally captured as part of a scenery and therefore can now provide us with some information after all.

However, since most of us are modelling era III and IV situations, historical photographs are also a valuable source of inspiration. Although only a few railway photographers have specifically targeted people in photos, they do exist: one prominent example is Reinhold Palm, who frequently produced photographs in which people take centre stage or even form the main motif. He did this for advertising purposes in the service of the German Federal Railways.

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Fortunately, his negatives and diapositives were preserved by the Eisenbahnstiftung (Railway Foundation) and today make for a valuable contribution to its photographic archives. This is also a lucky coincidence for you and us, who are thus able to draw on selected images for this article.

Of course, these are mostly scenes that were taken at railway stations or in their immediate vicinity, but some scenes could equally be transposed to a pedestrian zone, for example. Now then, let's give free rein to our imagination!

Staged or natural?

Everyone may have had the experience: Soon after purchasing a pack of scale figures and carefully distributing and placing them on the platform, we start to having our doubts. Although the eye can see a figure in every corner, the scene just doesn't work. It doesn't give the impression of authentic, pulsating life.

People in real life wait for the train, go to the station kiosk, linger at the magazine or ice-cream trolley, or rummage nervously in their pockets for their tickets. But, that's exactly what the figures we've just put up don't convey.

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View of the platform at Frankfurt Central Station in 1964: We see many people waiting for their train, walking calmly or in a hurry to the platform and strolling along together. However, the viewer of these scenes immediately realises that a random moment has been captured here and that it is not a staged scene. Photo: Reinhold Palm, Eisenbahnstiftung

Photo on page 14:

Two railway men in the Berlin-Pankow marshalling yard posing for the photographer in 1942, even though he was clearly trying for a natural shot. Photo: RVM (Ittenbach), Eisenbahnstiftung



Figure scenes on the model railway must appear plausible, otherwise they lose their naturalness: The driver of the Beetle (photo above) seems annoyed by the long wait at the level crossing and has gotten out to stretch his legs – the closed vehicle door doesn't even matter. Father and son on the bench next to the building (photo below) seem to be silent. Nevertheless, the scene looks extremely realistic thanks to the relaxed postures given to them by the designers at Noch.

If we look at each one individually, we come to the conclusion that everything seems fine and that it cannot be due to the selected object. So, what is the key to success? We must first realise that, unlike the trains, our figures do not move. What is possible for car models with the Z-Car system is not possible for our human miniatures.

No matter which figure we choose and look at, it will always remain a tiny, three-dimensional statue. If we see them as groups, we perceive them as an equally three-dimensional photograph, a frozen snapshot.

Let's take a look at old family photos as a template. Even decades later, we can usually tell straight away whether we are looking at a staged scene, or whether the photo was taken by chance at a random moment.



What makes for a natural effect? The answers certainly include the different roles that people perceive in a situation. The gentleman and lady on the left are engaged in a brief conversation with their porter on the platform of Stettin station in Berlin in 1929, while the gentleman in the hat has averted his gaze – today, he would probably be looking at a mobile phone. Photo: RVM, Eisenbahnstiftung

When the photographer drew attention to himself, all eyes turned to him and the camera. At the coffee table, almost everyone suddenly assumed the same position to present themselves upright, smiling and with their eyes open. (Non-verbal) communication only took place with the photographer at the moment the shutter was released.

The situation was different, however, when the camera was triggered in an unobserved moment. In this case, conversations between two or more people were recorded, with all eyes focussed on the person speaking. The rest of the relatives were perhaps focussing on the cup or the piece of cake. They are clearly not looking at the person with the camera in their hand.

As viewers of the model railway layout, we find ourselves in precisely this role: we take part in the lives of others, our figures. For them, we are not even present. They are talking to each other or have been “frozen” in a very specific situation. Their posture and gestures give them expression.

Transferring this vividly and credibly to a model is one of the great strengths of three Z scale accessory manufacturers: Trafofuchs has been one of them for many years, because co-owner Birgit Foken-Brock has never limited herself to simply adding paint to the mouldings.



Postures, clothing, and tools make scenes authentic: the couple in the foreground maintain physical contact, the gentleman's left arm allows us to perceive the dog lead, which has not been reproduced at all. The farmer in his overalls behind the barrier also attracts our attention, as his posture draws our attention to the tractor, which might have some problem.

According to the customer's individual wishes and requirements, she used to give them typical and unique postures, matching clothing and sometimes even tools.

This is still the case today, but her unique figures are no longer created by dismantling and reassembling various body parts of standard figures and adding accessories. Instead, they are now designed on the computer and subsequently 3D printed.

And while, as far as we know, every figure at Preiser is still modelled by hand from wax, CAD design has long since won out at accessories specialist Noch. They have had their own ranges of figures or many years, which were also extended to Z scale seven years ago.

And whilst the Noch figures tended to be of a shorter and more voluminous body shape, Preiser (including their acquired brand Merten) favoured, and still favours, tall people with a slender frame. Only a mix of figures from both producers did make for a welcome change on a layout and a representative reflection of reality.

However, the current highlight of product development is the recently released 3D printed figures from Noch. What has always made the difference at Trafofuchs is now also available “off the peg” from a large-volume producer.

The Wangen-based company knew better than any other figure manufacturer how to design highly lifelike miniatures. In fact, they appear to be frozen in the middle of a typical movement that is easily recognisable, even when we set them up in studio conditions without the necessary backdrop.



New figures from Artitec convey a special sense of authenticity. For the designs, the company's own employees slip into historical costumes and are then 3D-scanned, as soon as they have assumed the appropriate posture.

So, we recognise how a beach goer inflates a rubber animal, the shunter lays down a brake shoe, the angler casts out his line or a mountaineer scrambles up a rock face. Let's think of the many police officers who have been offered their jobs over five decades: If even one of them were to clip a parking ticket to the windscreen wiper, we certainly wouldn't need to think about what he was (still) holding in his hand, given the position of his arms, or?

We are delighted with the momentum that the figure market has recently experienced. Artitec has also recently joined the market with two releases, which is why the Amsterdam-based company deserves to be mentioned as the third supplier. Here, too, the postures of the farmers and bricklayers are so clearly recognisable that there is no risk of misinterpretation.

Dynamics and interaction

Typical movements and postures that are characteristic of a recognisable situation or profession apparently allow our brain to put ourselves in the situations depicted. Even if it is actually only a static situation that we see, we subconsciously perceive action or sequence of movements.

The dynamics of the moment are therefore created in our heads. We believe we are experiencing a sequence of events that does not actually exist. And that is what makes the difference between successful model railway scenes and those that do not inspire us to dream.



Who hasn't experienced this? Women can spend hours looking at shop windows while he looks annoyed at his watch and fears missing an appointment. In this scene, two figures interact without making eye contact or communicating with each other. This scene was created using misappropriated content from the 3D printed "Figures" pack from the still new products 2023.

It is crucial to think about the scene to be depicted in advance, to let it take place in your mind and to select exactly those figures from the range that fit the desired image. And, of course, they must not all be orientated towards the viewer. They live their life on the layout, and we just happen to be watching from the outside.

We are thinking, for example, of the postman on his bike, which can be found in the Trafofuchs range. Navigating a long curve, we see him leaning at a decent angle, which should in principle make him tip over. But we can almost feel the speed he has picked up to quickly distribute the many letters in his bag and look forward to the end of the working day.

The man from the Klingenhöfer Miniaturen figure range is much more relaxed as he leads a Golden Retriever along a forest path. He is walking at a leisurely pace and is completely focused on leading the leash while enjoying the sounds of the forest.

This is also an expression of interaction between individual figures: humans and dogs are travelling together, communicating non-verbally via the leash. Their behaviour adapts to each other through action and reaction. This only has a coherent effect if the connection between humans and animals is also credibly portrayed.



The postman on the bicycle lends this scene the necessary dynamism with his tilted position and the hand held protectively on his cap. Meanwhile, the yellow colour of the vehicle ensures that this scene cannot be overlooked, even in the middle of the layout.

The same applies to the father on the platform (Trafofuchs), who is waiting for his family. After the train has stopped and the wife heads towards her husband, the small child rushes off excitedly. It runs towards its father, who opens his arms to welcome his little boy and bends down to pick him up.

If we were to turn the figures by just a few degrees so that their eyes no longer meet, the entire scene would no longer be effective. Comparable discrepancies in detail can also render family photos, which we have already used as a reference, worthless.

Today, photos in which a person is photographed with a blink of an eye or eating seem downright strange. The eyelids are only closed for a fraction of a second, making the person photographed appear to be asleep, usually in a very questionable posture.

A chewing movement frozen in the photo disfigures every face because it loses its familiar symmetry. Art and skill are required when we look at purchased figures and try to imagine what situation they could be in.

continued on page 23



Man and dog (photo above) from Klingenhöfer Miniaturen radiate familiar cosiness, photographed together on a walk in the forest in the evening sun. Farewell and reunion scenes are part of everyday life at the railway station: Trafofuchs' little child runs overjoyed into his father's arms (photo below).

In most cases, the product name tells us what the manufacturer had in mind, but we can perhaps see much more in it. If we use them just as credibly in a completely different context, then the viewer will probably not even recognise them, which is an advantage especially with figures from large volume producers Preiser, Merten, Faller, or Noch.

The ability to transfer between different contexts can then help us to reproduce typical everyday situations or professional activities without having to wait for a character who is also intended to do exactly that. And, remember how important communication is.



On 26 June 1961, the mobile sales stand at Hamburg Central Station is bustling with people. The many people in the center and background interact and communicate with each other. Even without movement and sound, it is easy to understand what is happening. Photo: Walter Hollnagel, Eisenbahnstiftung

People chat and talk to each other, several times a day and in almost all situations. This doesn't just apply to a table in a beer garden or a large group waiting for the bus or train. Even a little chat between two neighbours while hanging out the laundry is easy to spot from a distance. It is always important that the people communicating make eye contact.

Aiming at consistency

However, there are also gaps in the now large, and in some cases difficult to navigate range of products on the market. For Z scale, there is often a lack of everyday animal companions such as cats or dogs, and especially children's figures.

Although something has changed in this area over the last ten years, the focus is still on replicas of adults in summer clothing, roughly divided equally between the male and female sexes.

Filling a playground with a whole host of smaller children in a wide variety of poses can quickly become a challenge. This becomes particularly exciting when we also travel back in time: Whether it's the imperial era, the "golden twenties" or the economic miracle period, there are still many gaps to be filled here.

Winter is a good example of how strange it can be when the figure and the environment don't match. Imagine the typical female figure in a skirt and showing a lot of leg, waiting for the train next to large piles of snow on the platform. The icicles on the edges of the roof reveal the icy cold that prevails on a morning like this, when commuters have to get to work for better or worse.



This scene is unmistakably set on a frosty winter morning. If the rushing figures weren't wearing jackets and headgear, the arrangement wouldn't work. Winter-clad figures are almost exclusively to be found in the Trafofuchs programme.

Our figures should therefore also match the season depicted on the layout, which in our example means that long trousers, thick coats, scarves and gloves are probably more appropriate. So far, the almost only producer catering to the segment of winter figures is Trafofuchs, which incidentally are also in the form of children playing with sledges and snowmen.

This leads us to a brief overview of the market: The most traditional manufacturer here is Preiser. In most cases, six figures are put together in one box. Some of the basic shapes were and are also offered in different colours. For example, shapes of the railway staff and the policemen are identical.

However, the Preiser range generally does not include highly specialised figures. Priests, bridal couples and wedding guests, for example, were always in great demand. These figures were a classic in Merten's range, which was later absorbed by Preiser. Many Z scale enthusiasts have already been waiting for new editions for over 20 years, but so far there have only been announcements.

continued on page 26



Children's figures are simply indispensable on a layout but are still hard to source from most manufacturers: Mother and child have parked their bicycles (photo above) and are looking at the water. A single, strategically placed figure can also have a great effect (photo below), as shown by this no longer so sprightly senior citizen, who still has a long walk ahead of him.

If you looked closely, you could rediscover some of these figures at Faller (and also Märklin, by the way). Apparently, the Gütenbach-based company bought them in order to promote sales of their own building kits with thematically matching figures. Gradually, however, we are also recognising replacements by their own creations, which will certainly also arrive in Z scale.

The next large-volume supplier was Noch. The brand from the Allgäu region had already made a name for itself for well over a decade with a wide range of injection-moulded plastic figures.

Together with the co-operation with Rokuhan to support Japanese sales in Europe, the decisive impulse seemed to have been given to also produce important figure sets in 1:220 scale.

Noch's 3D printed figures have been just released and announced for this year appear to be the next evolutionary stage. They allow a very intricate reproduction of the smallest details and also of colour gradients and patterns in the clothing, something that was previously not economically feasible on our scale.



The comparison of two plastic injection-moulded conductor figures (photo left) from Noch (left) and Preiser (right) reveals different shapes, but also subtleties in body parts and posture. The new, multi-coloured 3D printed figures from Noch (photo right), on the other hand, represent a completely new generation of human miniatures.

3D printing, although not in colour, has now even arrived in small volume production. The pioneer Trafofuchs was joined here by Klingenhöfer Miniaturen, both of which specialise in special themes and thus play a firm and reliable role in our scale.

We currently categorise Artitec between large and small volume production. In addition to its direct sales platform, the Dutch supplier also has a dealer network and impresses with extremely credibly reproduced miniatures, but they are still so new on the market that they first have to find their way onto the layouts. We have no doubt that they will do so!

NoBa-Modelle is not really a manufacturer of figures, but we would also like to briefly mention this company. At least there were a few exceptions, because their petrol lawnmower also wanted to be pushed by somebody.

KoMi-Miniaturen deserves a historical mention. When this specialist first started out, it also focussed heavily on figures. We remember golfers, who also characterised the brand image, a nativity scene or a pig butchering scene.

Wherever we go, nothing works on the model railway without figures. Everyone recognised this early on. However, the decisive factor is how we now know which scenes we define, select, and, perhaps, also adapt.



Special situations can also be reproduced with the lifelike figures from Noch's new products: This shunting locomotive driver with a radio remote control stands on the platform of the class 361 and moves along with the model.

We should also think carefully about how and where we place them. Because in their final location, they should tell us stories from life as we know it. And they always do this in silence. This is where the old adage comes true: "Photo is worth a thousand words."

Current figure suppliers for Z gauge:

<https://artitec.nl>
<https://www.faller.de>
<https://www.klingenhoefer.com>

<https://www.noch.de>
<https://www.preiserfiguren.de>
<http://www.trafofuchs.de>

Image gallery of the Eisenbahnstiftung:

<https://eisenbahnstiftung.de/bildergalerie>,

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Alternative building construction (Part 2)

Deep-set Windows

Buildings with windows that are almost or completely flush with the wall are quite prototypical. This applies not only to modern architecture, but also to some older, even historic, buildings. However, there are many buildings where the windows are considerably deeper in the wall. We now show how vinyl film can also be used in such situations.

By Reinder Rutgers. The basis for our work today is a sheet of opaque polystyrene: we choose a 0.8 mm thick, white version. It should be larger than the part to be produced, as with the method described in the last part 1.

This panel is covered with vinyl or painted with modelling paint or spray-painted. Then a layer of foil in the colour of the beams or wall details is glued over it to create the framework or, in the case of plastered buildings, the wall elements. This can be done in the same way as with method 1 / 1A (see part 1), which is the simplest method and recommended for beginners.



As I don't want to deprive readers of an alternative, I will now describe how to produce one or more layers of vinyl independently of the background material. In this case, the vinyl remains on the backing paper and is then transferred to the base material using an intermediate carrier.

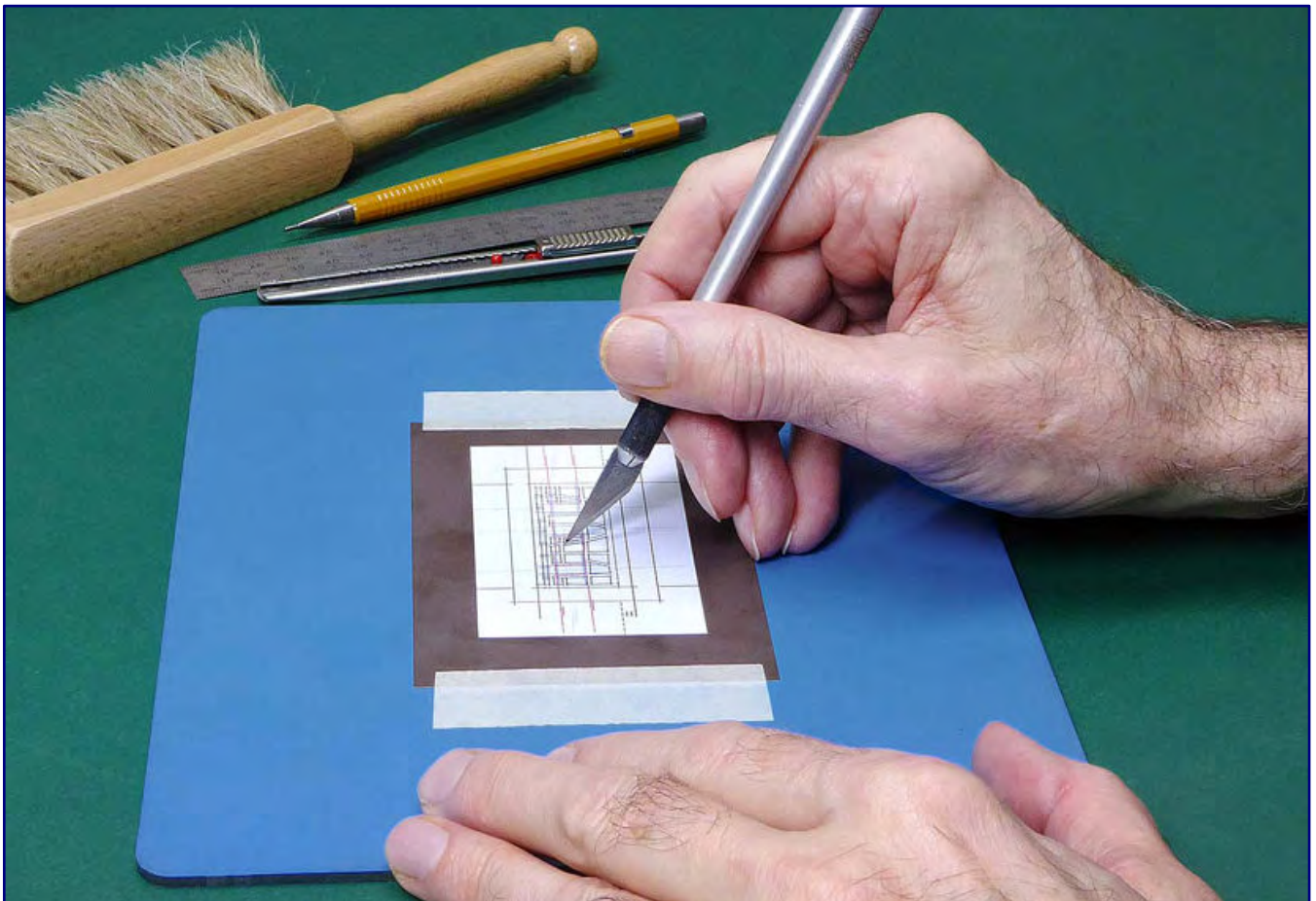
In principle, this is the standard procedure for transferring lettering to cars, advertising signs, windows, etc., on a 1:1 scale. Even if we work with a CNC cutting plotter for modelling purposes, it is a normal

procedure. It is less relevant when cutting by hand in Z scale, as described here; the reasons for this will become clear below.

Getting to work

A copy of the drawing is glued onto vinyl film with “Rubber Cement.” It is best to fix the whole thing to a small board with adhesive tape. In principle, you could also work without a board, but in this case, there is a risk that the vinyl will not lie flat, which could affect the dimensional accuracy of the workpiece.

In this case, the model is a romantic timber frame with crooked beams, as we see in rural buildings or historical half-timbered buildings – in this case, the model comes from the Palatinate. Therefore, with the exception of the outer lines, the beams are cut freehand. A pointed knife with an interchangeable blade is used for this purpose.



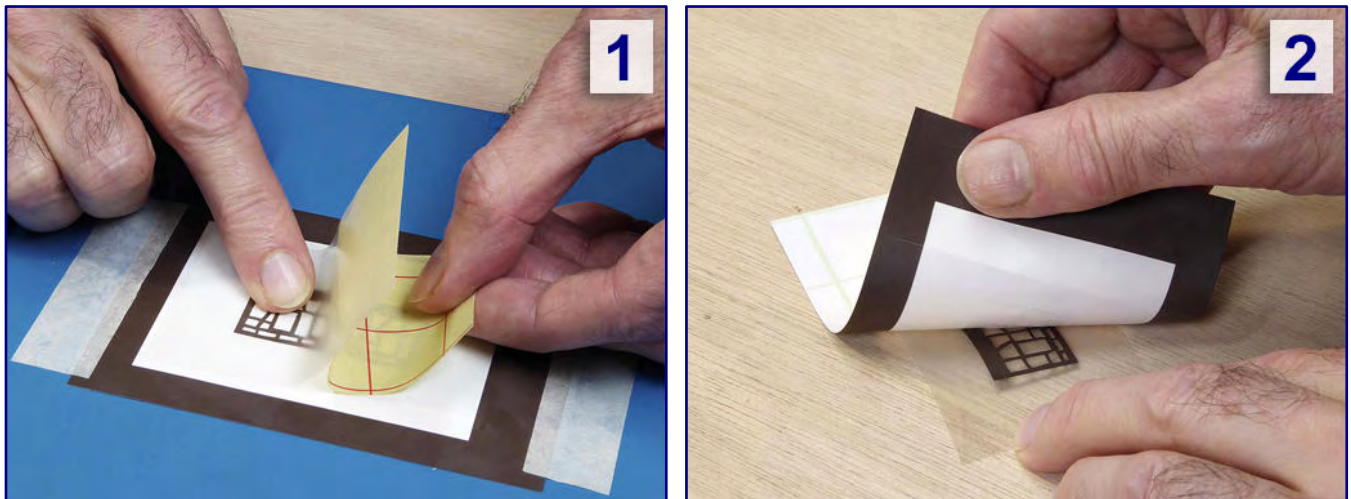
With the exception of the outer lines, the framework of our current template is cut freehand.

The drawing and remnant pieces are then removed. The part is exposed by removing the edge area. We then stick on the transfer film. Then carefully and slowly peel off the background paper from the vinyl film. It is best to unroll the paper, which is easy with DC fix, whereas, professional films make it more difficult with their thick background papers.

Important: If something does not detach from the backing paper of the vinyl, stop immediately and carefully try to detach it manually using specific grips. The framework is then located on the transfer foil.

We then stick the combination of these two layers of film onto the substrate. Then carefully remove the transfer film again, as you did when removing the backing paper. If something sticks to the transfer film when sticking it on, immediately roll it back and rework it. This is done by pressing the part firmly, for example with a small screwdriver.

Film should never be pulled directly upwards, but should always be unrolled as flat as possible to one side. This principle applies to all films. It helps if you work diagonally to the main direction of the wall. Finally, a piece of background paper is placed on top and rubbed down well.



For the planned transfer, the transparent transfer film is now applied to the framework (step 1). The transfer film is then carefully removed again, the framework should now adhere to it (step 2). Otherwise, proceed as described in the text.

This all sounds quite simple, but in practice it is unfortunately difficult with very small and filigree parts, as is often the case in Z gauge. Of course, you can also work with several layers with this method. However, this further increases the level of difficulty.

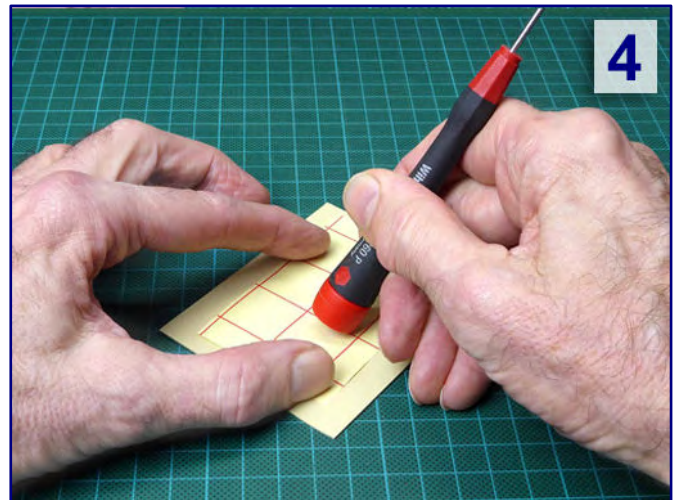
Cutting unglued vinyl

It must be cut more precisely if the vinyl is not firmly glued on. If the vinyl is glued on, for example, it makes little difference if a truss beam is accidentally cut through. Unfortunately, the situation is quite different here. It may then be difficult or even impossible to transfer the workpiece using the transfer film.

As building walls are so tiny and, in the case of timber framing, also very delicate, they easily stick to the backing paper instead of transferring to the film. They can also warp slightly when transferred to transfer film.

Then the dimensions are no longer correct and it is difficult or impossible to use a single base drawing to cut several parts. When transferring transfer film to the base, there is this risk again. If the vinyl surface is coloured, the transfer foil can damage the surface.

However, there are also advantages to cutting unglued vinyl. If a piece of vinyl fails, the substrate remains unaffected. There is also no risk of scratching the substrate when removing the remaining pieces.



If a bar sticks to the transfer foil when transferring the structure to the wall surface, we carefully help with a suitable tool — in this case, the tip of a screwdriver (step 3). In the last step, we place the background paper of the transfer film on top again and ensure good adhesion of the framework and wall surface by rubbing with gentle pressure (step 4).

In theory, you could also use transfer film to cover walls that have already been cut to size or buildings that have already been installed. However, it is difficult to stick them on accurately and can easily lead to errors which are not correctable.

Reworking

Once the framework has been transferred, the window openings are cut or sawn. Cutting thin polystyrene is relatively simple: score all four sides, score two cross lines from corner to corner and press through onto a suitable surface.

I use a board with several rectangular openings for this. It is best to cut them a little and finish them with a needle file. A flat needle file with a so-called 'saving edge' is recommended here, i.e., one side without a profile is used to guide the file in the corners without accidentally removing material in the wrong place.

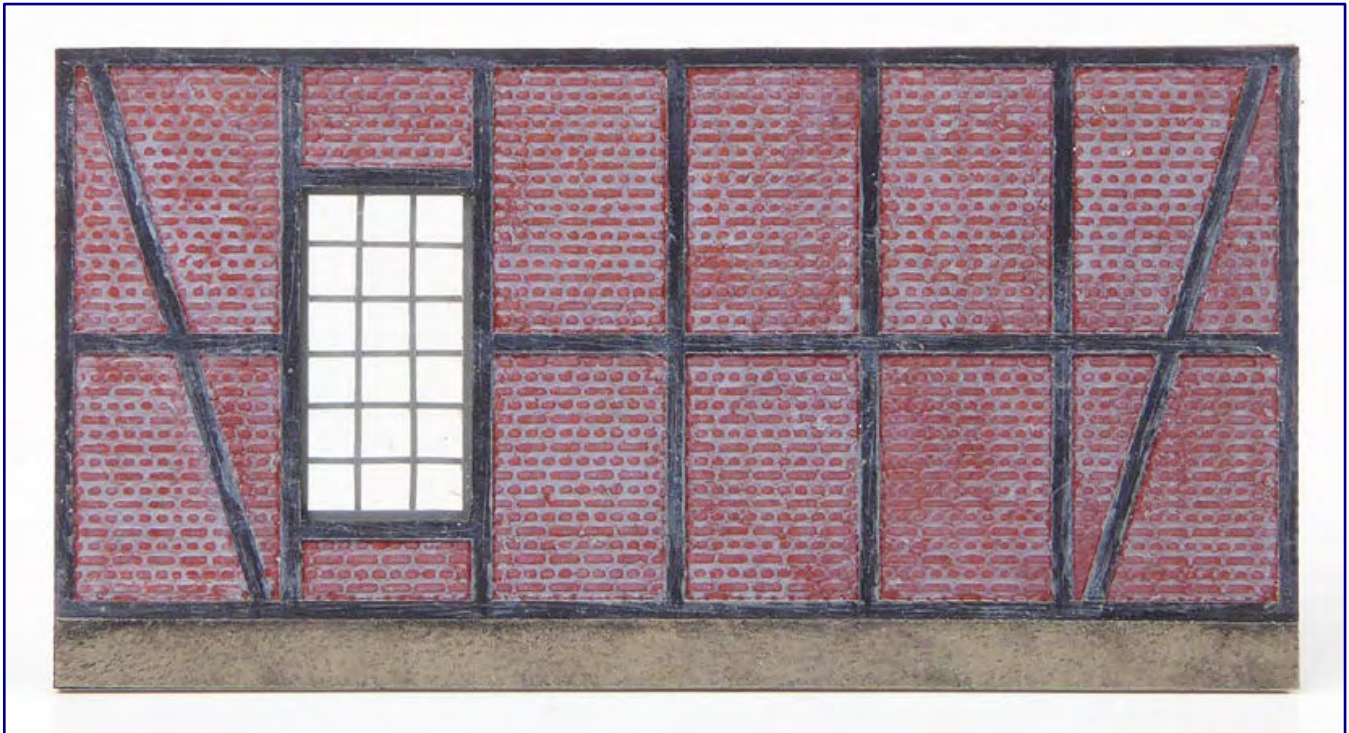


Cutting and clearing (left photo), reworking (centre photo) and painting (right photo) the window openings are the next steps on the way to creating windows deep in the building wall.

The inner sides of the window openings are then painted. Finally, the wall is cut to the final dimensions along the contours of the vinyl (notching and breaking off).

We could also ask ourselves: “Why not just paint the beams on instead of using the film?” The answer is actually quite obvious: Firstly, we have more control over the width of the bars when cutting than when painting.

Secondly, we have an almost infinite number of attempts because we use a copyable drawing, but also because the repositionable film can be removed again. In addition, there is the option of ageing with standard modelling techniques, whereby the small height differences of foil layers and the possibility of 'engraving' or embossing are helpful.



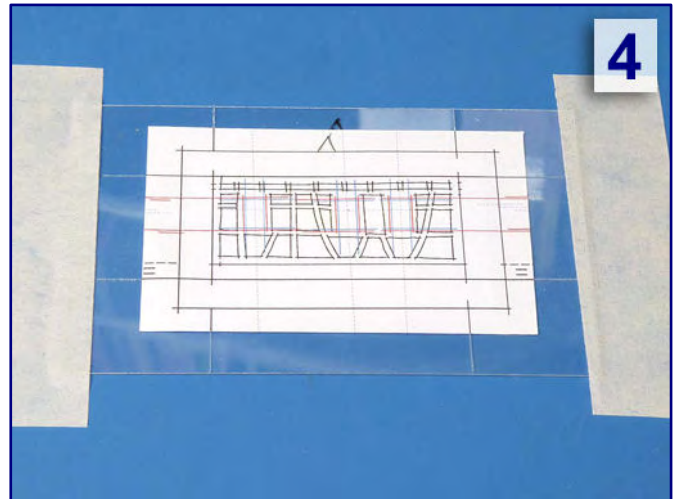
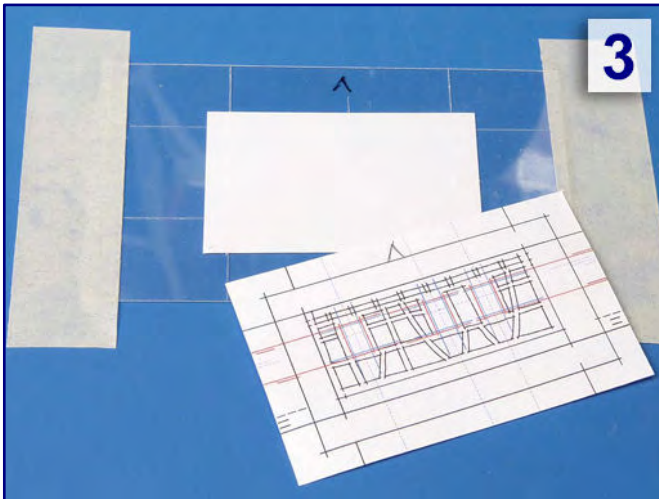
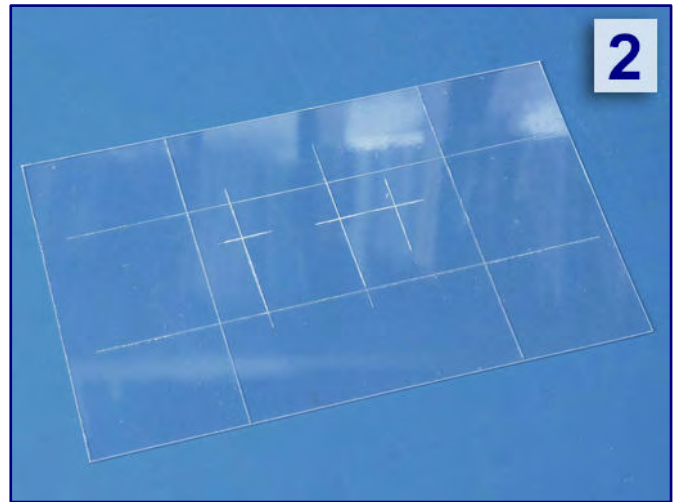
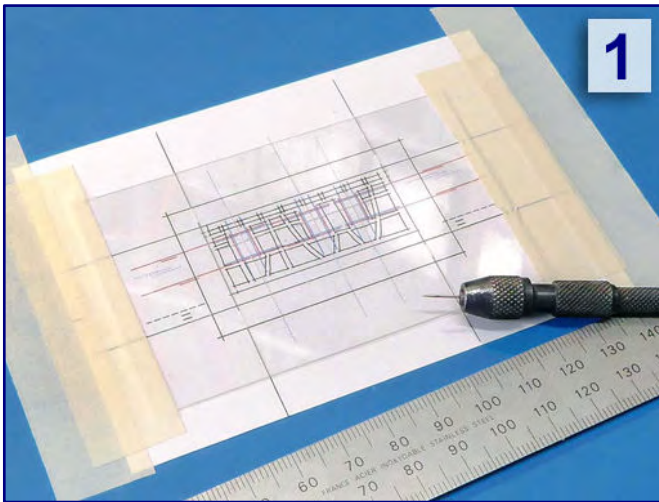
Commercial buildings often featured brick-filled half-timbering. The polystyrene sheet required for this comes from Slaters and was filled with grey acrylic paint. The half-timbered beams are cut from highly adhesive film because the contact surface to the embossed background is small. The window was created from separate vinyl strips 0.3 mm wide on 2 mm thick, transparent polystyrene.

And last but not least, thanks to these common techniques, it also looks more realistic, but at least that could also be a matter of personal taste. If we use textured brick panels instead of smooth polystyrene to create a lined timber frame, painted beams are not an alternative anyway.

Making the windows

Our windows are made separately. There are two alternatives. The first is a thick, transparent panel, as described in method 1 / 1A. This results in very stable buildings. However, if the wall already made is stable enough, we can also use a thin material.

In this case, I use a 0.4 mm thick polystyrene board from Evergreen. Together with the 0.8 mm thick truss wall, this results in a total thickness of 1.2 mm, which is sufficiently stable.



We place the material for the window reproduction over the drawing and attach both with adhesive tape (Step 1). The window divisions (window crosses) are then scratched in and coloured white, if necessary. It is important to apply reference lines, which can also be found on the drawing, for alignment on the plastic (Step 2).

Then we stick white vinyl over it (Step 3) - this piece should be larger than the wall, but smaller than the drawing. If thicker window frames are required, two layers of vinyl can also be glued on top of each other. Now cut out a copy of the drawing. Then align the reference lines on the drawing and on the glass and fix the drawing with rubber cement (Step 4). The openings in the window frames can then be cut into the vinyl. Using the same drawing every time ensures that windows and frames fit exactly.

A copy of the drawing is attached to a surface. Mildly adhesive masking tape, which is sold as masking tape for painting work, is recommended here. The transparent material is placed over the drawing and also attached with adhesive tape. The window divisions are scratched in and coloured white if necessary.

It is important to scratch in reference lines. These should also be recorded on the drawing. Then stick white vinyl over, or any other desired colour. The piece should be larger than the wall, but smaller than the drawing. If you want thicker window frames, stick two layers of white on top of each other. A copy of the drawing is cut out.

Now, the reference lines of the drawing and the glass can be aligned. The drawing is then glued on with rubber cement and aligned precisely. If there are only a few openings, tape is sufficient.

The openings of the window frames are then cut into the vinyl. By always using the same drawing, the glass distributions and window frames will match after removing the remaining pieces.

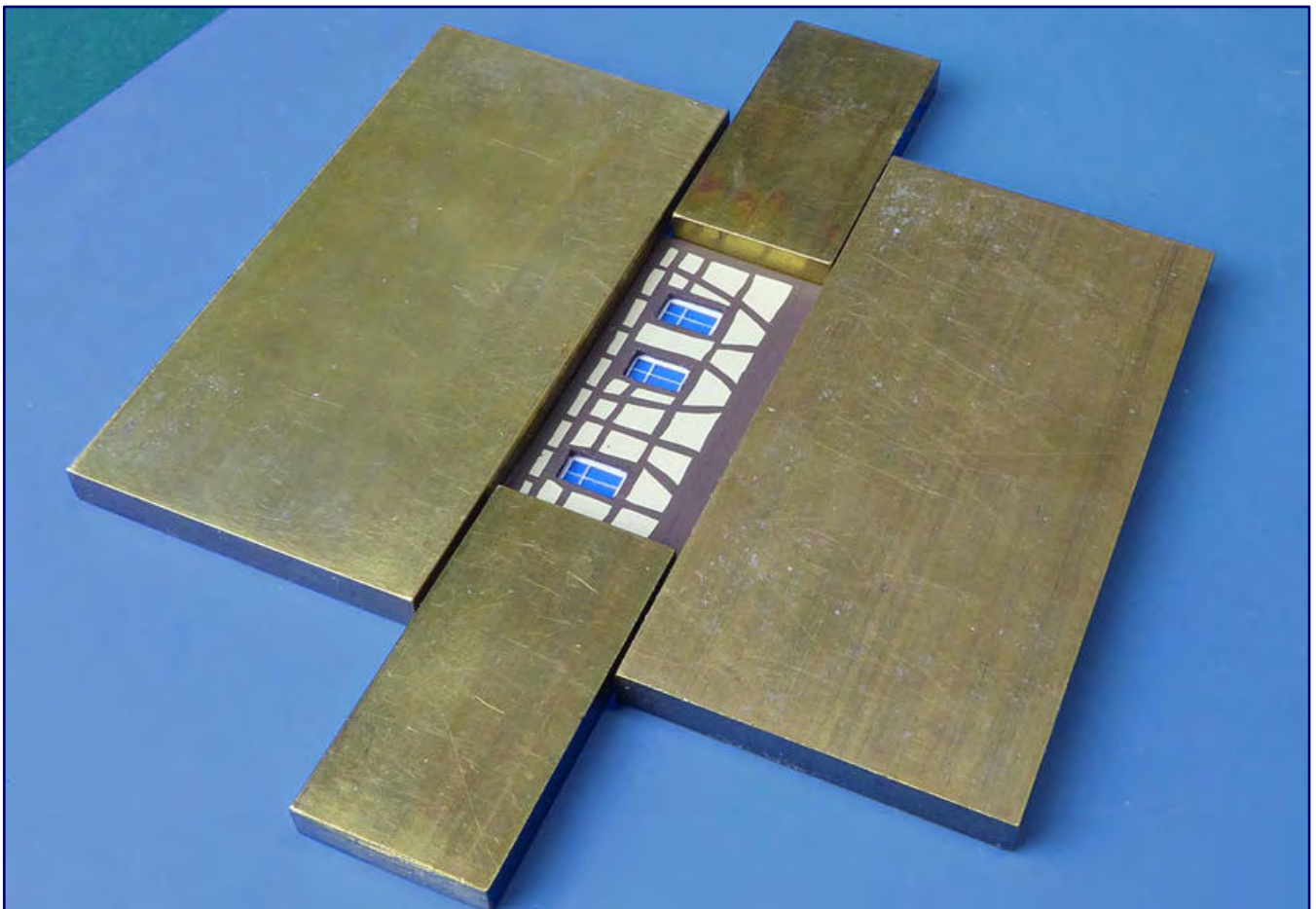
One difference to method 1 is that the inside lines of the window frames must be indicated on the drawing. In method 1, the frames are created almost 'automatically' by using a knife for cutting. This is not the case here.

In this model, two of the three windows are not horizontal, but slanted in the wall, as is often the case in old houses. For this reason, the reference lines for cutting the window frames are shown in colour on the drawing. In this way, we avoid confusion with the dividing lines of the window openings.

Assembling wall sections

The end result is a piece of white vinyl with a window. This now needs to be glued behind the finished polystyrene wall and this raises the question: which adhesive would be suitable for this because very few adhesives stick to soft vinyl.

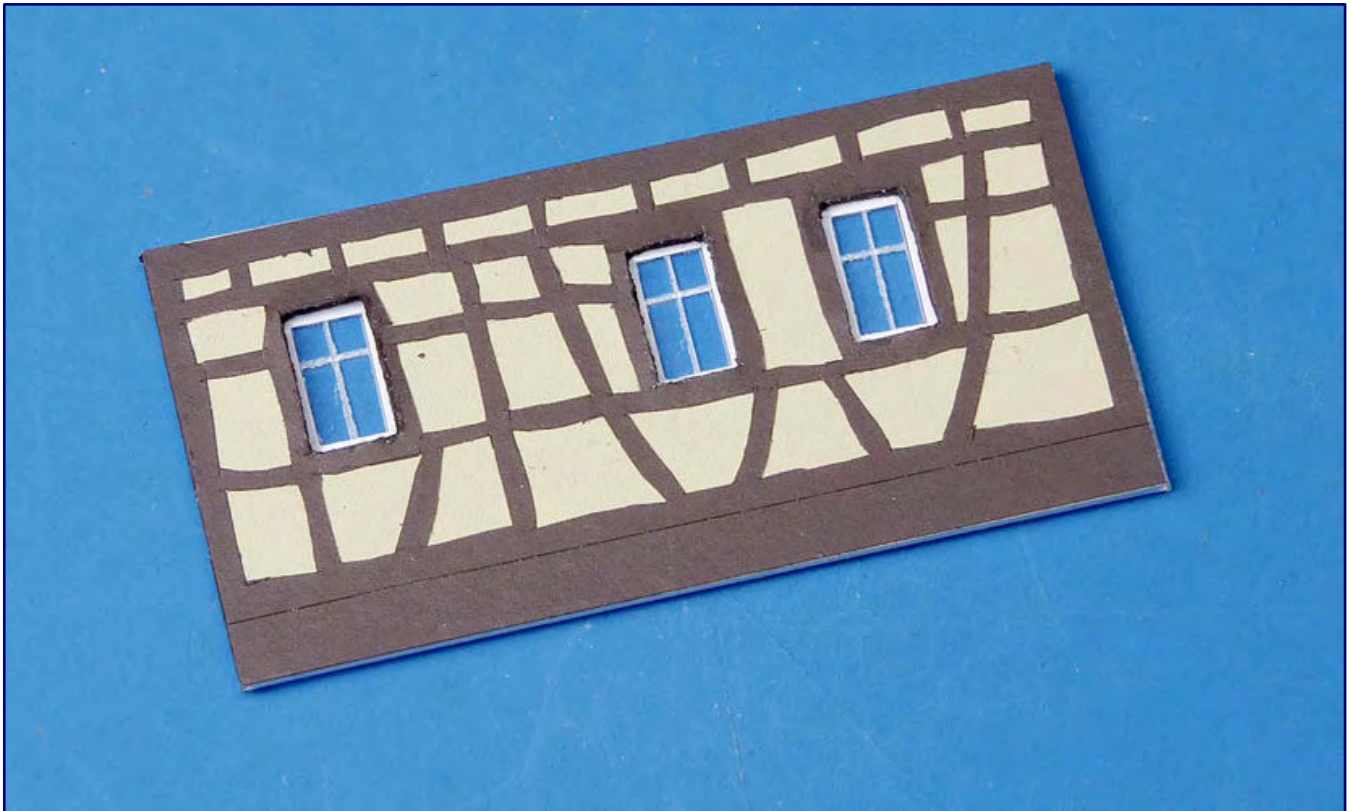
I tested 17 adhesives in detail. What didn't work: Hard PVC adhesive, polystyrene adhesive, two-component epoxy resin, hard plastic adhesive, soft plastic adhesive, white glue ("wood glue") and plastic levelling compound.



During drying, the joined parts are weighted down so that the building wall and window parts hold together permanently.

“Loctite Super Glue-3 Power Gel” worked excellently. Caution: Do not get confused and confuse it with other Loctite gel instant adhesives, always look at the label! With this adhesive, you have about 30 seconds to align the workpiece. Please do not inhale the vapours.

“Pattex Multi Universal Adhesive” also proved to be suitable. Strangely enough, the supposedly stronger “Pattex Repair Extreme” did not work. The “Velpon Universal Adhesive” delivered satisfactory results. The advantage here is the large timeframe for alignment.



This photo shows the result of the work described so far: The white window frames are significantly lower than the half-timbered wall. The white window crosses also look good.

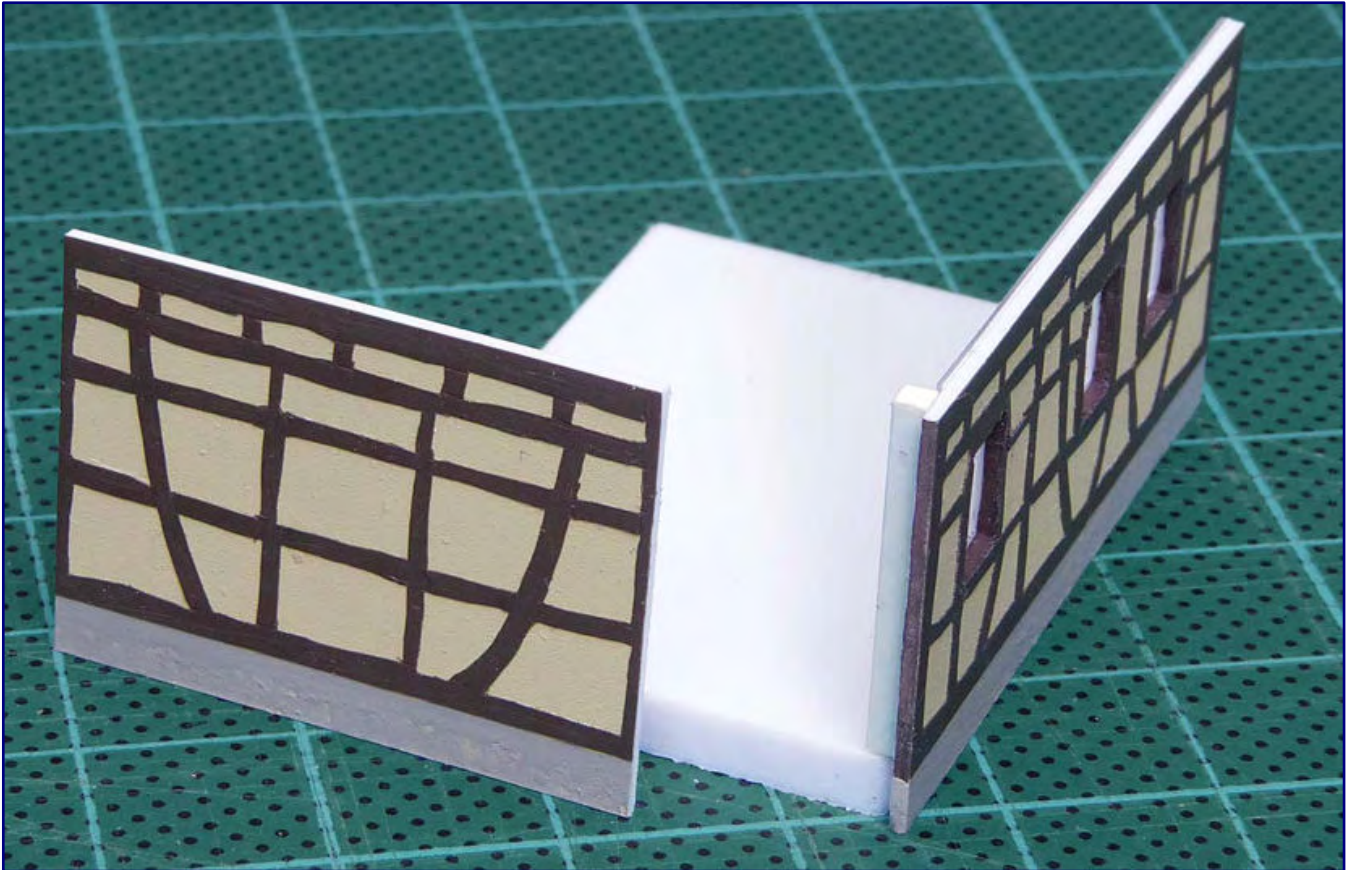
The disadvantage, however, is the very long drying time of around one day. Two other well-known universal adhesives did not work at all during testing. Obviously, it is not possible to predict which products will be usable, so the only option is to carry out the test series described above.

Low-viscosity superglue of the Bison brand (identical to Uhu, Griffon or Bostik) worked very well technically, but there is no time to align it, it has to sit correctly immediately, otherwise the part is lost for good.

As the window section and the wall were created using the same drawing, they should now fit together exactly. However, you want to avoid the parts “floating” when they lie on top of each other with an intermediate layer of liquid adhesive.

The danger is great if the drying time is several hours, as is the case with universal adhesive. To prevent this, I place heavy pieces of metal around the parts to be glued during the drying process. As the external dimensions of the wall and window panel are identical, this helps to achieve the desired exact match.

Finally, the wall sections are mitred again at 45°. This is necessary for method 1 because the walls are so thick. However, there is an alternative for method 2. Because the wall sections are relatively thin, a simple blunt corner joint is also one of the possible options.



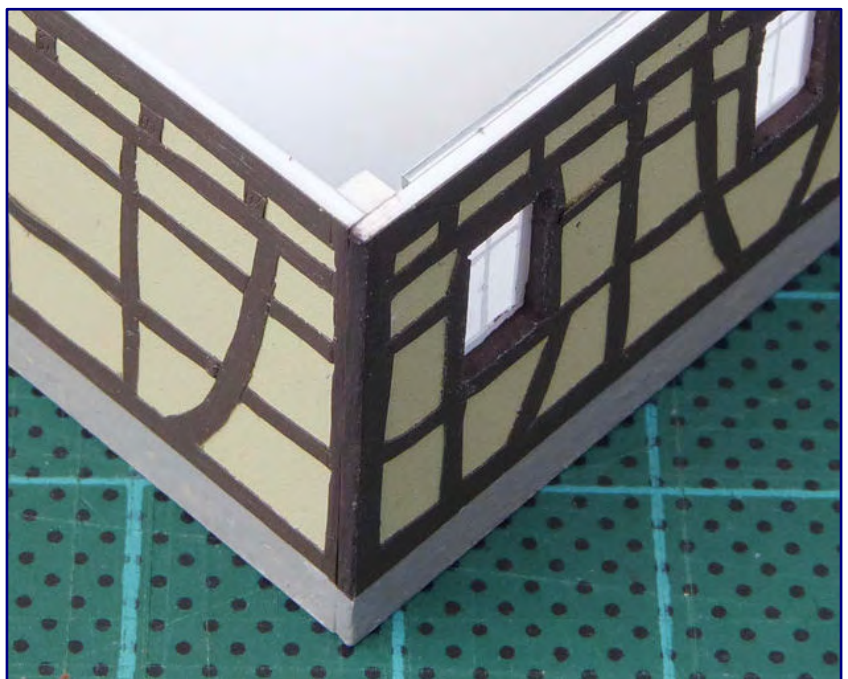
As the walls of the sample building are not too thick, a blunt corner joint is sufficient (photo above). One wall head side was therefore painted brown and grey so that the adhesive joint does not stand out due to a difference in colour after gluing (photo bottom right).

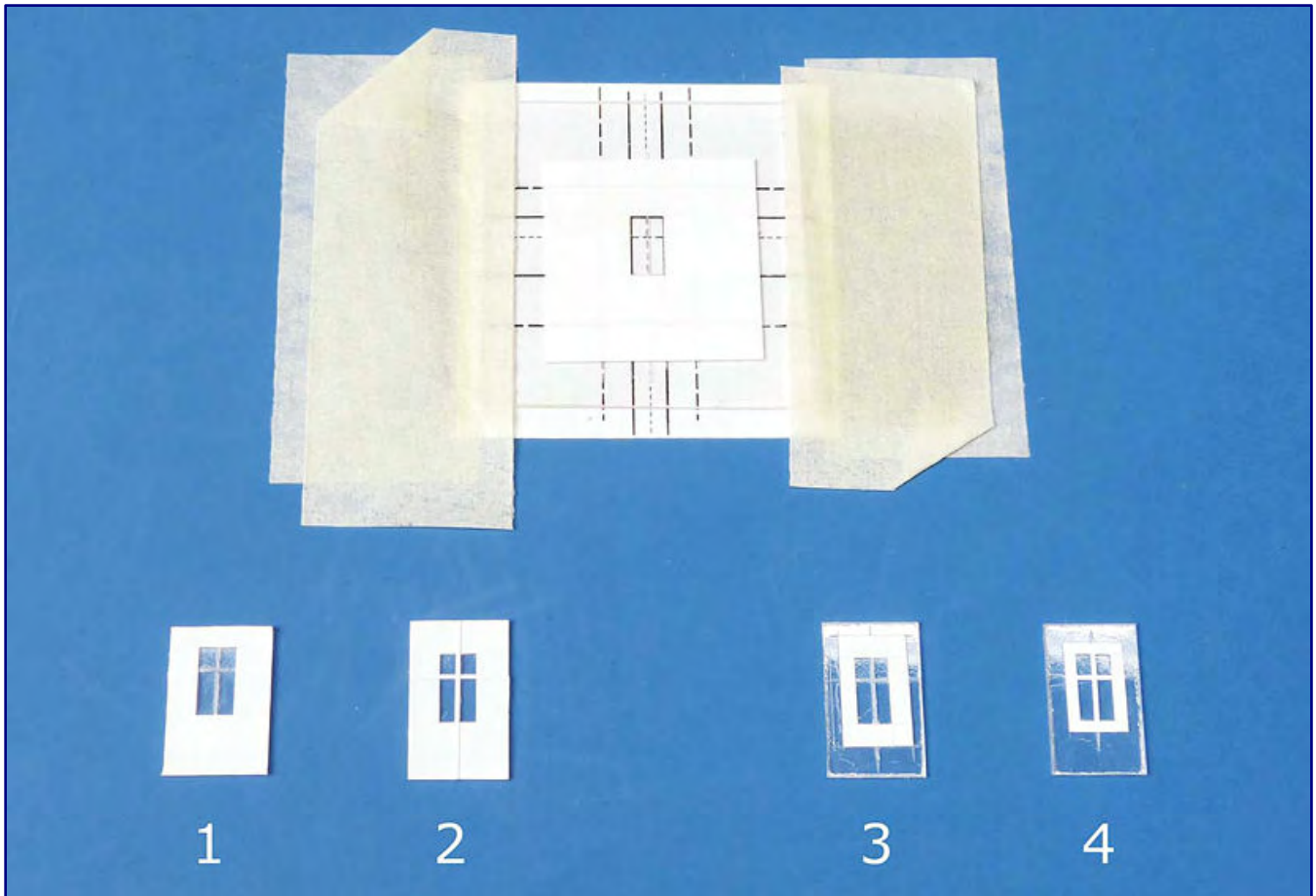
The seam can be concealed very well with a little paint, especially in timber-framed or wooden buildings.

If it is too rough, putty would be used first. For vinyl, we recommend a more flexible filler, similar to an acrylic filler.

The disadvantage, however, is that the glass plate must then be shorter than the front wall section. This means that both elements no longer fit exactly on top of each other.

Of course, there are some clever tricks. For example, the remaining strip of the window section could be kept to temporarily fill the gap when gluing them together. I leave it to the readers to find further solutions.





gluing them together. I leave it to the readers to find further solutions.

Incidentally, many people will already have realised that it is not absolutely necessary to use polystyrene as a substitute for glass in this construction method. As the glazing material does not play a role in connecting the walls, other transparent materials are just as suitable.

Separate windows

If a wall is already sufficiently stable, we can also produce the windows separately. Precisely fitting preparatory work on two larger parts is then not necessary. This method can also be used to customise commercially available kits, as their window frames and transoms are almost always too coarse.

The easiest way to make separate windows is to use a copy of the drawing again. The working method is basically the same as for the wall glazing in one piece. But here we work with several pieces of clear polystyrene.

I show here a template with a separate drawing for a window. As all the reference lines are now horizontal or vertical, it is not necessary to re-glue the drawing to the vinyl each time the frame opening is cut. We simply leave it glued to the substrate and make as many windows as we need using the same pattern.

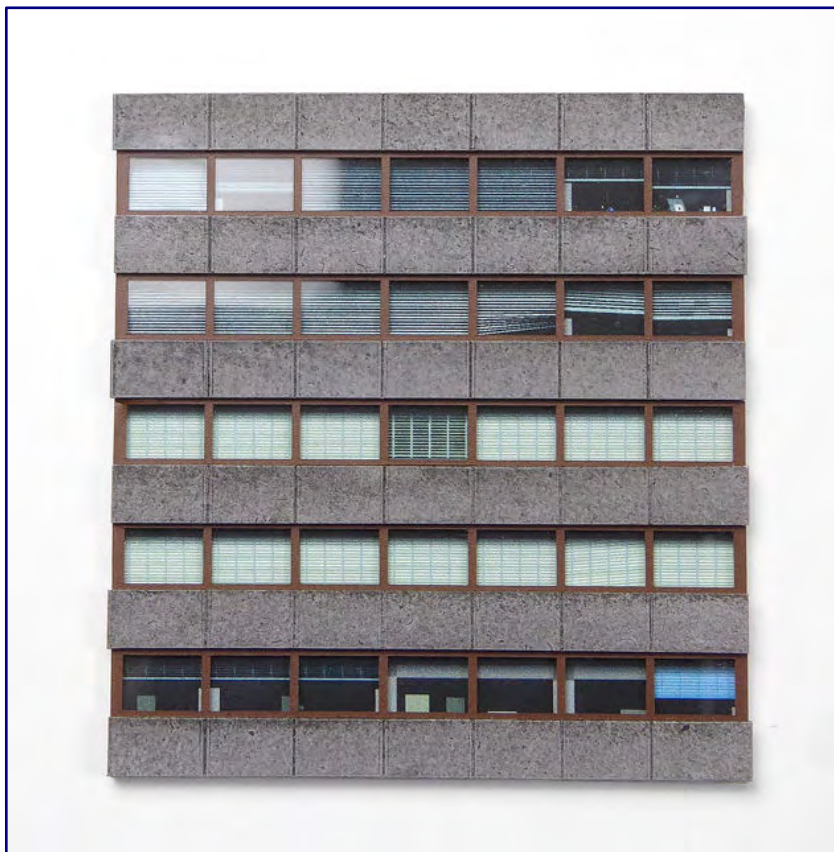
Window 1 in the accompanying photo (see page 38) is the same as the template for the half-timbered house: background completely covered with vinyl, window division scratched in, possibly coloured. Win-

dow 2 has vinyl strips for the window division, but these are not cut to size but simply stuck on. Pay attention to their length.

They therefore cross over each other and also overlap the window frame. This is not for purists, but it is not noticeable from a distance. The vinyl does not lie flat when glued in this way, but both universal adhesives that I rated as 'good' have no problems with this; they hold excellently despite the small distance between the wall and the glazing. The advantage is that the vinyl strips do not need to be fixed in place.

Another alternative is to make the window frames slightly larger than the window opening, just enough so as not to risk a gap in the end (windows 3 & 4 in the photo) and remove the edge film. This gives us the chance to glue the windows with polystyrene adhesive.

High-viscosity polystyrene adhesive is only just able to bridge the small space of 0.07 mm between the wall and the window glass caused by the film. However, the drying time is long, about one day.



The film method is also suitable for modern buildings, as they have a lot of glass. The building wall in the photo has only been given one layer of vinyl on a transparent panel to which 6 mm wide polystyrene strips have been glued. Engravings, colour, and a printed interior do the rest.

Faller Expert was tested here. The advantage is its precise dosing option and the long time for correcting the window position. Vinyl strips that are longer than the window frame (window 2) do not allow this procedure.

We would therefore have to cut the strips to size again and fix them with clear varnish or cover the gap between the glazing and the wall with small strips of polystyrene glued outside the window. The thinnest dimension is 0.13 mm (Evergreen sheet no. 9009 – sold by Faller).

I can imagine that mounting the windows with adhesive tape sounds temptingly simple, but, in Z gauge, it is tricky and often doesn't hold really well, so I would always prefer reliable gluing.

In the third and final part of this short series, I would like to introduce you to modern buildings and a few tricks. I will also give you some models to try out what you have learnt.

Sources of supply for tools and materials:

<https://www.amazon.de>
<https://www.conrad.de>
<https://www.faller.de>

<https://www.gerstaecker.de>
<https://www.modulor.de>
<http://www.peter-post-werkzeuge.de>

All photos: Reinder Rutgers

Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Funktionsweise einer Traktionsart erklärt **Hauptbaugruppen der Diesellok**

Wie funktioniert die Diesellok, fragt sich bestimmt so mancher Eisenbahnfreund. Außer dem Motor hat sie nicht viele Komponenten mit einem Auto gemein, das die meisten sicher noch recht überblicken. Unabhängig von den technischen Beschreibungen in Baureihenbänden ist ein grundsätzlicher Überblick zur Funktionsweise der einzelnen Komponenten sicher sinnvoll. Das Ergebnis solcher Überlegungen stellen wir nun vor.

Stefan Alkofer
So funktioniert die Diesellok

Transpress Verlag
Stuttgart 2024

Taschenbuch mit Klebebindung
Format 17,0 x 24,0 cm
184 Seiten mit 20 S/W- & 118 Farbbildern sowie 9 Zeichnungen

ISBN 978-3-613-71715-2
Titel-Nr. 71715
Preis 39,90 EUR (Deutschland)

Erhältlich direkt ab Verlag
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Das vorliegende Buch ist kein brandneuer Titel, sondern die aktualisierte Wiederauflage eines 2005 im gleichen Verlag erschienenen Werks. Autor ist damals wie heute Stefan Alkofer, heute Chefredakteur des Magazins Modelleisenbahner.

Da das Vorgängerwerk noch nicht bei uns besprochen wurde, bot sich die aktuelle Auflage für eine Rezension an. „So funktioniert die Diesellok“ ist ein sprechender Titel, aber je nach persönlichem Wissen und persönlicher Perspektive wird ihn wohl jeder mit abweichenden Erwartungen belegen.

Denkbar wäre etwas ein „Fahrschulbuch“ im Sinne von Anleitungen, wie eine Diesellok aufgerüstet, gestartet und gefahren wird. Ebenso möglich wäre ein Buch, das tief in die technischen Prozesse dieser Traktionsart eintaucht und Hauptmotor wie auch andere Aggregate und Hilfseinrichtungen im kleinsten Detail beschreibt.

Genau das birgt beim Spontanerwerb das Risiko eines Fehlkaufs, vermag zugleich aber auch wegen falscher Erwartungen vom Kauf abzuschrecken. So setzen wir uns nun mit den Beschreibungen des Verlags, dem Inhalt des Werks und auch seiner Zielgruppe auseinander.

Festzuhalten ist zunächst, dass sich die Lektüre nicht auf Fahrzeuge deutscher Eisenbahnen beschränkt, es ist also nicht baureihenspezifisch. Weltweit ist die Dieseltraktion die meistverwendete und außerhalb von Deutschland steht bei ihr die elektrische Leistungsübertragung im Mittelpunkt. Aber auch hierzulande zeigte sich die Entwicklung nach der Bahnreform sehr dynamisch.



Will der Autor also umfassend berichten, muss er alle denkbaren Antriebsspielarten berücksichtigen. Zunächst taucht Stefan Alkofer daher in die Geschichte ein. Es wird herausgearbeitet, was die Diesellok kennzeichnet und beschreibt das grundlegende Problem, dass ein Dieselmotor nicht unter Last gestartet werden kann.

Der weitere Abriss führt dann zu Klassifizierungen der Maschinen, technische Bezeichnungen in Deutschland (aber auch anderswo), wirft dabei aber auch einen Blick auf private Unternehmen und das Ausland. Nicht vergessen werden dürfen Dieseltriebwagen als besonderer Aspekt und auch Meilenstein der Geschichte.

Angeschlossen wird ein Kapitel zu physikalischen Grundlagen. Hier geht es um grundlegende Begriffe und deren Definition sowie die Einheiten, in denen hier das Messen erfolgt. Mechanik und Dynamik der Gase und Wärme in ihrer Wirkung wollen verstanden werden, um zu verstehen, wie ein effizient arbeitender Dieselmotor beschaffen sein muss.

Wegen der vorherrschenden elektrischen Leistungsübertragung werden auch die Elektromotoren in ihrer Vielfalt und Schaltungsmöglichkeiten mit Vor- und Nachteilen erklärt. Ebenso abgehandelt werden Generatoren, die moderne Drehstromtechnik und Strömungsmaschinen (Turbinen und Pumpen).

Auf Basis dieser Grundlagen werden dann Zwei- und Viertaktmotor erläutert und die einzelnen Bauteile sowie Betriebsstoffe beschrieben. Es folgen ähnlich aufgebaute Kapitel zur mechanischen, elektrischen und hydrodynamischen Kraftübertragung.

Beschreibungen der Nebeneinrichtungen, des mechanischen Teils und der Nebeneinrichtungen einer Diesellok schließen sich an, bis eine beliebige Lok in allen Bestandteilen erklärt wurde. Der Leser wird in die Lage versetzt, Sinn und Zweck der vielen Bauteile und -gruppen zu verstehen und ahnt, warum eine solche Maschine heute so komplex aufgebaut ist.

Ein Ingenieursstudium sind dabei weder Voraussetzung zum Verstehen noch Ziel der Wissensvermittlung. Die fachliche Tiefe wurde so gewählt, dass wichtige Entwicklungsstufen einzelner Komponenten nachvollziehbar werden und verstanden wird, worauf künftige Veränderungen zielen.

Dabei berücksichtigt der Verfasser auch die internationale Entwicklung, bis hin zu einer kleinen Typenkunde amerikanischer Dieselloks am Ende dieses Buchs. Zuvor ist der Leser aber dabei, wie in einer kurzen Reportage eine Vertreterin der Baureihe 218 binnen neun Minuten startklar gemacht wird und wann diese Zeit sehr knapp für den Lokführer wird.

Bildlich wird das Geschriebene recht umfangreich und geeignet unterstützt: Die gewählten Aufnahmen zeigen zwar teilweise vollständige Lokomotiven – nicht nur aus Deutschland – im Streckendienst, aber der Schwerpunkt richtet sich stets auf die jeweils im Kapitel behandelten Teile der Maschinen. So bleibt gewährleistet, dass die Ausführungen nachvollziehbar und zuordbar sind.

Dies führt uns zur Zielgruppe des Titels. Der Verlag selbst umschreibt diese wie folgt: „Damit bietet dieses Buch fundiertes Wissen für jeden, der sich für Technik und Funktion dieser wichtigen Fahrzeuge interessiert.“

Das ist sehr treffend zusammengefasst, denn hier kommen nicht nur Technikverliebte zum Zuge und zugleich ist es kein trockenes Fachbuch mit einem übermäßig hoch gewählten Niveau, dem nur die wenigsten folgen könnten. Es ist schlicht das, was wir in diesem Sachgebiet der Eisenbahn als massentauglich bezeichnen möchten.

Zweiter Bildband von Udo Kandler **Neue Sichten auf die Bundesbahn**

Reinhold Palm fotografierte für die Werbeabteilung der Deutschen Bundesbahn. Dies und seine Passion für die Eisenbahnfotografie brachten eine unglaubliche Vielfalt und Abwechslung an Lichtbildern hervor. Um Haaresbreite verloren gegangen, freuen wir uns heute über den Erhalt der Sammlung. Udo Kandler hat einen zweiten Bildband daraus zusammengestellt.

Udo Kandler
Die Bundesbahn
Fotoraritäten der 50er- und 60er-Jahre von Reinhold Palm

Geramond Media GmbH
München 2024

Gebundenes Buch
Format 26,8 x 28,9 cm
240 Seiten mit ca. 260 Abbildungen

ISBN 978-3-98702-001-8
Preis 59,00 EUR (Deutschland)

Erhältlich direkt ab Verlag
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Das an dieser Stelle vorgestellte Buch haben wir nicht grundlos ausgewählt: Es passt einfach bestens zum heutigen Teil unserer Reihe „Randerscheinungen“, in der wir uns mit wirkungsvollen Figurenszenen beschäftigen.

Um glaubhafte Szenen erreichen zu können, ist immer das Vorbild die Vorlage für unsere Modellwiedergaben. Doch woher bekommen wir historische Szenen, auf denen Menschen und nicht Eisenbahnfahrzeuge im Mittelpunkt stehen?

Reinhold Palm ist einer der wenigen Fotografen, die berufsbedingt eine sehr breite und bunte Palette an Themen abzuarbeiten hatten, weshalb nicht immer nur Lokomotiven und Triebzüge in seinem Fokus standen. Dazu gehört er zu den frühen Farbbildpionieren – ein Glücksfall für unseren Antritt. Reinhold Palm war ein Eisenbahnfotograf aus Passion, der nicht einfach aus Pflichtbewusstsein gehandelt hat.

Den Schatz seines Nachlasses hat die Eisenbahnstiftung im Dezember 2018 gehoben, sonst wäre er um Haaresbreite vernichtet worden. Udo Kandler hat den wohl einmaligen Bildbestand ausgiebig gesichtet und geeignete Aufnahmen zu einem zweiten Bildband zusammengestellt.

Den ersten hatten wir bereits in **Trainini®** 4/2022 rezensiert und auch den durch viele Publikationen eigentlich gut bekannten Autor kurz vorgestellt. Wiederholt wusste er Bildarchive auszuwerten und das gefundene Material durch einen roten Faden sinnvoll zu verbinden.

So schuf er auch Bücher über den bekannten Hamburger Direktionsfotografen Walter Hollnagel, quasi ein Kollege Reinhold Palms, wenngleich auch in anderer Form und viel weiter im Norden wirkend. Gewiss haben er und seine Kollegen aber auch Kunst und Können des hier bildlich Geehrten beeinflusst. Arbeitskollege und Direktionsfotograf Paul Trost war auf jeden Fall sein Lehrmeister.



Sinnvoll erscheint es deshalb, wenn Udo Kandler zunächst den beruflichen Weg von Reinhold Palm nachzeichnet und eben auch in einem separaten Kapitel Paul Trost und einige seiner Werke in angemessener Form vorstellt.

Ein Querschnitt verschiedener Bundesbahn-Impressionen widmet sich portraitierten Schienenfahrzeugen. Dies stellt nur einen Ausschnitt aus dem Schaffen vor, besticht aber durch das eingesetzte Farbmaterial, das sich bei kaum einen anderen zeitgenössischen Fotografen findet. Heute sind das wertvolle Geschichtsdokumente.

Ausgewählt haben wir diesen Titel aber, weil er eben auch abseits der Gleise Geschichte dokumentiert. Oft haben auch die Menschen jener Zeit hier eine wichtige Funktion. Da Reinhold Palm in seiner beruflichen Funktion Zutritt zu den Orten der DB hatten, die öffentlich nicht zugänglich waren, entstanden viele Aufnahmen, die sie bei der Arbeit oder anderswo im Dienst zeigen.

Berücksichtigt wurden auch Messen und Ausstellungen mit DB-Beteiligung, darüber hinaus sind Reisende oft mit auf Zelluloid verewigt worden. Auch seine eigene Familie bekam oft eine Statistenrolle zugewiesen. Selbst Kinder dienten gezielt als Motiv.

So erfüllt dieses Buch viele Zwecke: Für uns ist es ein Sammelband wertvoller Momentaufnahmen, unter denen wir den Fokus heute mal auf Menschen gerichtet haben, Bahngeschichtsinteressierte freuen sich über viele überraschende Motive und besondere Ansichten, mit denen die Entwicklung der Deutschen Bundesbahn für alle Zeit festgehalten wurde.

Die zeitgeschichtlich wertvollen Bilddokumente stammen übrigens aus den Jahren vor 1970. Reinhold Palms Schaffen war auch zu seinen Lebzeiten schon anerkannt: Bei Fotoausstellungen im In- und Ausland errang er zahlreiche Preise.

So besticht die getroffene Auswahl neben einer guten Reproduktion auch mit einer hervorragenden technischen Qualität. Wir können auch ohne gezielte Suche nach bestimmten Objekten oder Szenen diesen Band nicht einfach aus der Hand legen.

Schon das Blättern und „Aufsaugen“ der kurzweiligen Erläuterungen von Udo Kandler, mit denen die Kapitel eingeleitet und vorgestellt werden, fesselt uns in atemberaubender Weise. Fast schon wie eine Strafe wirkt es da, dass der Autor gleich zu Beginn seines Vorworts mitgibt, dass dies der zweite und letzte Palm-Bildband sei.

Zustimmen müssen wir ihm aber, wenn er festhält, dass man aufhören sollte, wenn es am schönsten ist. Das mag zunächst bedauerlich wirken, reitet eine solche Reihe aber nicht perspektivisch tot, weil das beste Material längst verarbeitet ist und die Qualität irgendwann nachlassen muss – wie dann auch das Kaufinteresse.

Und deshalb meinen wir, dass hier Zugreifen angesagt ist statt zu warten. Wenn das Beste bis zum Schluss aufgehoben wird, dann ist dieser Moment nun erreicht. Irgendwann ist auch dieses Werk mal ausverkauft. Wir würdigen die tolle Arbeit und Qualität mit dem Nominieren dieses Titels für die Neuerscheinungen des Jahres 2024 in der Kategorie Literatur.

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Z gauge weekend in Altenbeken

A Scale in Danger?

The Z gauge weekend in Altenbeken is a highlight for our scale every two years. In May 2024, there was once again plenty to see that catered to a wide variety of tastes. Exhibitors and participants once again justified the international standard. So, was it a complete success? Not quite, because we will also have to point out a few downsides.

“Imagine it's Altenbeken weekend and nobody goes there.” Unbelievable? Yes, fortunately it's not like that today, because the biennial meeting is known worldwide in our scene and attracts Zetties from near and far to the Eggegebirge.

Some visitors travel many hundreds of kilometres to get here, some of them even travelling well into the four-digit kilometre range. There is no clearer way of expressing how important this meeting is within our scene.

It was no different on 4 and 5 May 2024 for the 9th edition of the event. However, the official total of around 900 visitors is anything but a record, as it exceeded the edition held under pandemic conditions by just 12.5 %.



The Altenbeken memorial locomotive 044 389-5, which Märklin has also announced as a miniature as part of the museum locomotive series, has become a trademark of the town, and, thus, also of the Z gauge weekend.

In comparison, similar club exhibitions have returned to their former strength or have even been able to score with a significant increase in popularity. The recent desire to finally attend such events again has obviously bypassed Altenbeken. At the end of this report, we will ask why this was the case and analyse the figures.

Before that, we would like to take a tour of the exhibition hall. It was freshly renovated and partially remodelled. Those who suffered from the changes were the participants from ZIST1 from Berlin, who traditionally set up their layout and showpieces in the adjoining room.

As the access route has been changed and is now through a narrow door, which does not provide a view of this room, many guests did not notice this presentation. We can say that they missed something in this case.

As always, there were beautiful showpieces to see and with various trams based on Berlin models, this club also enriched the display with exhibits that are far too rarely included in layouts.



This arrangement by Thomas Heß, on which the Z gauge was assigned the role of a garden railway, spread a leisurely atmosphere. The reduced-length shorty of the "Glass Train" fits in perfectly. Photo: Thomas Heß

The large layout traditionally consists of club members modules. However, this group skilfully overcomes the crucial dilemma of a lack of depth: The narrow parts would otherwise also allow a ride on a pub table.

The oval, which must be made larger for exhibition purposes, is simply filled with trackless elements in the interior, which are designed in such a way that a different layout is always created and can be seen. We think this is a good way to demonstrate the basic idea to visitors without missing out on souvenir photos with a depth effect.

In terms of subject matter, this brings us to Thomas Buchner, the inventor of this tiny module idea. He was seated in the first aisle near the entrance area, where he successfully promoted his idea. His table neighbour Harald Hieber, also from Stammtisch Bayern, had discovered the ageing of locomotives and carriages for himself and demonstrated his skills with the spray gun.

Thomas Heß was particularly creative and dared to take a completely different approach after various layouts: based on a data carrier that has dominated the market for almost 30 years, he created a drivable circle of the same diameter, put it in the typical transparent cover and called his work "Zett-D", already presented in version 2.0.



To commemorate the event, the organisers again ordered a special car from Märklin (item no. 8617.147), which had different printing on both sides.

But the barely larger oval on a 1-gauge stake wagon was also well received, as the Z-gauge garden railway appeared here in front of larger figures, some of whom were enjoying beer. Thomas thus impressively demonstrated that there must be room for a bit of humour in today's world – let's call it just barely serious.

Elephant meeting and Shinkansen

Peter Sturm was only present on Saturday, filling a gap in the row of tables at short notice with a suitcase layout. This showed a small town with a winter theme, on the slopes of which the “Elephant Meeting” takes place – a winter gathering of enthusiastic motorcyclists in a large tent camp that really happens annually.

We almost walked past Hagen Sroka and Ronald “Larry” Schulz, two other creative pillars of our nominal scale. Hagen likes to keep things simple when he devotes himself to his many own and conversion projects. The results could be seen in operation on his “Draxlried” layout.

Larry preferred not only to present his exhibits in the display case, but also to demonstrate his work. As was to be expected, this was very well received, as many people are always wondering how he manages to produce his unusual delicate models and small series.



Ronald “Larry” Schulz demonstrated precision mechanical work. The customers were able to see the necessary reworking of cast replacement roofs for Märklin’s V 60 (see picture), where the cast burrs were removed.

On the opposite side of the booth, Ulrich Günther once again invited visitors to play shunting games and enter dialogue with him. Märklin, as a neighbour on the booth, was available to answer questions, demonstrated the exhibition layout and presented samples of current innovations. These included a hand sample of the V 60 for the first time, which is apparently made from series parts and not only for us increases the excitement for delivery.

Ralf Junius was present with his “Kistrath” layout and demonstrated quite modern railway operation. At the same time, he was also flying the flag for our magazine as editor. At least three editors and three translators were present in Altenbeken.

In addition to all the preparatory work for this report and our new episode on **Trainini TV**, this also gave us the opportunity to engage in dialogue with our readers. That makes us very happy, and we would like to take this opportunity to thank you for the many impressions and ideas you have given us.

Pantographs in our scale have been and remain a hot topic for months. Most of our readers would like to see much more delicate, but robust parts. In addition to the lack of scale, the non-flat support of the original Märklin parts is repeatedly criticised. Reader Michael Fuhry showed us another home-made solution using the class 111 model.



Our reader has reworked this class 111 locomotive so that the single-arm pantograph, which is not extended, lies flat on the roof and does not look too oversized. Photo: Michael Fuhry

Dieter Heckl from Heckl Kleinserien who returned as exhibitor will have been delighted. At his booth, he presented eight samples of different versions of the SBS 9 and SBS 10 pantograph types as a 2024 novelty. The HISE types and the SBS 39, which was widely used after 1945 and was mainly found on the E 18, E 19, E 44 and E 94 series, are also to follow.

The duo from KoMi-Miniaturen were probably not interested in any of this, as they usually focus on the years leading up to Era III. A small layout attracted the spectators, who then took a look at the extensive and perfectly designed modelling and equipment material.

We do not wish to list the many dealers with new and used material in full, as we are focussing on the exhibits on display. Christine Specht and Roland Kimmich also travelled from Stuttgart to display a colourful repertoire of the former Railex assortment. Of course, the sale of existing leftovers continues unchanged.

And the 1zu220 shop is also always significantly reducing sales space and is presenting many beautiful layouts and dioramas to inspire its customers again. The diorama “Am Lietwerk” provided by Markus Schöne and the Shinkansen diorama personally presented by Christoph Maier, which played a leading role in episode 21 of **Trainini TV**, were on display for the first time this year.

From the Taunus to northern Germany

Next door, we experienced Swiss train operation on the presentation diorama “Kesswil railway station on Lake Constance”, built and operated by Karl Sinn, which was constructed according to the peep-box concept. This also applies to Jürgen Tuschick’s “Heinzenberg”, modelled on the former Weiltalbahn in

the Taunus. Jürgen also had a soft spot for children and offered them their own hands-on layout to control on their own.



Markus Schöne presented the diorama "Am Lietwerk" at the 1zu220-Shop booth, where it was well received.

Two digital suppliers had also placed their booths, in the area at the end of the hall. Oleksiy "Alex" Mark presented his latest digital decoder developments, and, at the corner of the next aisle, Michael Boost presented his remodelling range including the latest developments. By the way, for the first time, Velmo was not present in Altenbeken for personal reasons.

Azar Models celebrated a premiere in Altenbeken. A large selection from the current programme was on display on a layout based on southern French motifs, while a preview of the announced models was given in the showcases. There was a sample of the TGV made from 3D printed parts on display, as well as examples of the new models announced in the spring that are already well advanced.

Eckhard König presented his fine and customised track solutions, for which he had also brought along a large demonstration display. Rainer Tielke's double-decker bridge in Bullay was also very busy. He had brought along his familiar showpiece, as there was something new to see on the car system installed on it: An ambulance was now also doing its rounds non-stop, while on the Moselle an eight-man scull was demonstrating its rowing skills.

Booth neighbour Torsten Schubert is also well known for his colourful light show. He had a new small diorama in his luggage, which picks up on a current trend with "forgotten places" ("lost areas"). He has placed great emphasis on detail and has created a convincing showpiece.

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Unfortunately, we were only able to photograph the “Kesswil station on Lake Constance” (photo above) from the Spur-Z-Atelier without any rolling stock, as the dismantling of this layout began two hours before the end of the event. We had more success in “Heinzenberg” (photo below) by Jürgen Tuschick: Who can spot the escaped parrot, a bright red macaw, on this layout?

Trainini® *International Edition*

German Magazine for Z Gauge

The Z Gauge Fans Hamburg presented a large modular layout. There were many well-known motifs here, of which the large railway station was certainly one of the most impressive.

Hans-Jörg Ottinger ("Otti") had also installed his new, gated level railway crossing at this end of the large layout, whose control system was now ready for demonstration after much effort and delighted the visitors. A first sample of the EL-16 locomotive from the Barkvieren studio was also on display.



This is also important to Jürgen Tuschick: his layout invites children to play.

What fixed images cannot say...
Many of the exhibits mentioned in this report can hardly or not at all be illustrated in order not to go beyond the scope of this edition.
We therefore refer you to episode 28 of **Trainini TV**, which also shows these aspects in motion pictures. You will find the link and a QR code to our magazine channel at the end of this report.

The Z Club Vienna once again made eyes light up with many small layouts, from club member's table layouts and in suitcases, and a wide variety of themes. The works of Manfred Forst, Günther Falkus and Hermann Kammler are also predominantly well-known. Nevertheless, the Magnorail track system, for example, which is used to move cars and ships, was well worth a visit.



A first example of the Norwegian EL-16 from the Barkvieren studio was on display on a platform of Hans-Jörg Ottinger's station.

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Peter Sturm's winter case (photo above) is dominated by the "Elephant Meeting", whose tent camp here extends right up to the railway line. Torsten Schubert, meanwhile, has focussed on forgotten places (photo below) and thus staged an abandoned burned-out ruin.



At the Azar Models booth, there were many samples of upcoming new products on display, which we would like to present separately. Among them was a model of the 1st class Corail carriage (photo above), which will be delivered from August. Other new products could also be discovered on the layout based on southern French themes (photo below): several examples of an Azar-Model special edition wagon can be seen here, which was offered as part of the financing campaign for the TGV, as well as various versions of a four-axle, covered SNCF goods wagon behind the diesel locomotive.

On one of his layouts Hermann had gone to great lengths to have a tipper lorry loaded at one place and unloaded at another. He knew how to create illusions with a train. The many movements on the showpieces mesmerised visitors for several minutes.

The Trafofuchs booth, on the other hand, was quiet. However, this is probably only due to the fact that figures are still part of the equipment that can only rarely be moved. Birgit Foken-Brock showed some recent and current innovations, including the Olsen gang, a children's playground, and various sales booths and vehicles for the weekly market, including matching figures.



JMC Scale Models exhibited a wide range of different road vehicles from different countries and eras, so there should have been something for everyone. One of the exotic vehicles was the streamlined Texaco tanker lorry from Great Britain, which had not made it beyond prototype status in the original.

JMC Scale Models was on site for the second time and had a sufficient stock of various car models in its luggage. The vehicles that we were able to announce in the February edition as spring 2024 novelties were also on display here.

Along the exhibition hall wall

We now begin our journey through the third hall aisle, always along the wall opposite the entrance. Before we enter this aisle, we come to a booth run jointly by Mr. and Mrs. Scholz from Heidenheim (Modellbahn Scholz) and Christian Ribatzky (CRZ Ulm).

Scholz presented the Westmodel ready-to-run model of the 93⁰⁻⁴ series on the current and ready-to-deliver state. The Prussian tender steam locomotive has now been fitted with the latest Märklin running gear, which offers both detailed valve gear and a bell-type armature motor. This means that the gem is now state of the art and has become even more attractive.

Meanwhile, Christian Ribatzky presented a selection of his locomotive circuit boards, which can be fitted with almost any decoder, for all those who are not yet familiar with his product range. This means that many Märklin models can be individually digitised by replacing the standard circuit board.

Just a few steps further on, Raffele Picollo was waiting for us. He had his Wuppertal layout with suspension railway in his luggage so that he could demonstrate some of his work. As he now also offers models in small series under the name "Mr Ztrax", he also had his own house lighting solution and various new car miniatures on display.



Modellbahn Scholz presented its Westmodel novelty of the 93⁰⁻⁴ series with full valve gear (photo above). At Mr Ztrax we found a wide range of new products, including two special edition models (photo below) for the meeting in Altenbeken.

Booth neighbour Avantgarde Modellbau (Phillip Meyer) impressed with perfectly and realistically designed dioramas that illustrate the full repertoire of his skills and products, which are depicted in his small series programme. As a novelty for this year, he also showed samples of his wagon detailing and patination announced under "AVA Modellbahnen," which should also raise the rolling stock to this level.

Oliver Kessler had travelled all the way from Poland to demonstrate his Z-Car system once again. He was able to illustrate his technology using an enlarged model and various courses that were travelled by chassis. These included two new ones, one of which is intended to help tractor models to drive in a prototypical manner.



Phillip Meyer (Avantgarde Modellbau) presented his landscaping material on beautiful dioramas, but also knew how to present his detailing of freight cars sold under AVA Modellbahnen.

As always, the Werstatt / Hafermalz duo had a great time, and that can usually be taken literally. Small trains, which are designed to inspire children in particular, run on strawberry pies, simple circles or even in an old television set. It's not too serious, because the focus here is on childish fun.

The new products that Klingenhöfer Miniaturen had to present certainly serve a different target group. In the middle of the current product range, there was also a new set of figures here, which is labelled "piggishness" (see report in **Trainini**® 5/2024).

The "Boischoot Statie" layout by Pieter Willems and Jan van Casteren is always nice to see. Colourful SNCB trains, often with a "round nose", could also be seen on this jewel at this year's event.

In addition, Werner Wendel presented a layout on which things often went up and down. Here, loads were lifted and swivelled with a crane, or the main line of the railway was interrupted when the lifting bridge stretched upwards over the canal.

Hans-Georg Müller (Z-Freunde Sachsen) had a whole ring binder folded up. This was even more surprising as it contained a complete Z-gauge landscape. It was designed in such a way that no opposing details got in each other's way.

The passionate paper modeller's layout showed a Project Eva 500 series Shinkansen in a rather abstract setting made of metal folding kits. Wilfried Pflugbeil was also represented at this booth, having built a T-gauge layout for his window, which will be on display there at Christmas time. It also found a place in Altenbeken because many of the buildings and parts of the landscape in 1:450 scale are based on his well-known Z gauge layout "Wiesenthal".

Trainini® *International Edition*

German Magazine for Z Gauge

Michael Hering from Wilhelmshaven is a true artist of 3D printing.

He had set up a large display case showing the colourful range of rail vehicle and car constructions that he has created in the meantime.

Particularly impressive were classic Büssing lorries such as the LU11 with truck cab and the large Menck excavator M152 with high bucket.



The Büssing LU11 with long-distance lorry drivers cab (top right) and, above all, the Menck M152 cable excavator with high bucket are impressive vehicle models that Michael Hering designed, printed, and painted himself.

The Stammtisch Untereschbach e.V. always takes up a lot of space and always presents itself with a broad line-up. HOS Modellbahntechnik was also represented at the event, where, among other things, the pressure-tight coach crossovers for Intercity coaches were on display, which are a prerequisite for running on high-speed lines in the prototype, but were not reproduced on Märklin coaches in the model, with just one exception.



SIG coach crossovers (photo above) are accessories manufactured by HOS Modellbahntechnik, which can always be seen at the booth of the Z-Stammtisch Untereschbach e.V. The desert layout (photo below right) was also exhibited there, but we would not expect it to be located in North America.

New showpieces included a desert layout with camels and other inhabitants of hot sandy areas. A railway line runs through the landscape, on which rolling stock built in North America runs, as we could also experience in the USA.

A large fire station festival with an exhibition at the same booth not only offered the unique opportunity to show a large selection of available or previously offered fire engines, but also to stage them very effectively and present them to the visitors. Even without a railway siding, this diorama turned out to be an effective eye-catcher.



Another member of this group is Heribert Tönnies, who did not set up his layout on the long side, but across the corner. We are talking about "Bw Herilingen." A steam locomotive festival was held within the walls of the historic steam locomotive depot, which already looked rather dilapidated and in need of renovation, right next to the locomotive depot, which is still in regular operation.

continued on page 61



Photo above:
The fire station festival presents itself as a large exhibition with the participation of the Federal Agency for Technical Relief (THW). And the Untereschbach club will be providing refreshments with a food truck.

Photo below:
An old 57- tonne steam crane from Ardelt is demonstrated at Herilingen depot. **Trainini TV** is also there and films the event from the roof of the roundhouse.

An Ardelt 57-tonne steam crane can also be seen in operation there. To capture this for the public, a certainly not unknown duo from **Trainini TV** positioned themselves on the roofs of the roundhouse to document the manoeuvre in the best possible way on film. A sign has been placed to warn viewers to be quiet and not disturb the filming.

We experience a journey through the Höllental valley on Jürgen Walther's "Hirschsprung" layout. When it was still under construction, it was already shown once in Altenbeken, now visitors can experience it in its finished state and watch the German "potato beetle" and ancestor of the "eggheads", the VT 92⁵, on test runs.

A class 601 diesel multiple unit also travelled through the sometimes narrow valley areas so that DB could test its suitability for steep lines for the travel agency special service. So, this layout had much more to offer than just the uniquely beautiful Black Forest landscape, which incidentally is not characterised by the fir trees that are otherwise so common in this section.



At Jürgen Walther's Hirschsprung we meet a class 601 diesel multiple unit, which is on a test run for planned special services.

Jürgen Wagner, who was located right next door, also had a well-known showpiece in his luggage. His "Middle Earth" layout had not been on display for a long time and yet immediately revealed its builder's personal passion for the "Lord of the Rings" book and film trilogy. A factory neighbouring the station also bears a fitting name.

The train operation is extremely varied thanks to the hidden track helix and large hidden trainyard and captivates visitors for many minutes, during which they can also explore the landscape and details. This showpiece is also one of the most successful in the exhibition.

Maciej Gawecki invited us to his native Poland. The landscape and appearance of his beautiful layout still look quite familiar, but it is the rolling stock that makes the difference. The pride and joy of its owner is the PKP's OI49 steam locomotive with a 1'C1' wheel arrangement, similar to the German class 23.



The view across the railway station is of the Saruman toy factory: welcome to Jürgen Wagner in Middle Earth! Which book trilogy is his favourite?

Manufactured by Bahls Modelleisenbahnen and in co-operation with Aspenmodel which were located right next door, they also had their own new product to show us, which can also be ordered by all other interested customers.

This is the streamlined steam turbine locomotive T09 001 from 1944, which Michael had to research extensively to decide on an external condition; the locomotive was destroyed by the effects of the war before it was put into service. The second new item Michael Bahls showed us was the Focke DR 1 triplane, the "Red Baron" from the First World War.

Stefan Schulz and Hans-Joachim Rüssel flew the flag for the Blomberg regulars' table and presented fine rolling stock, much of it from small series production, on the "Lidbachtal" layout, where Jochen also taught his car models to drive. Here, too, we could hardly get enough of it, but unfortunately the tour of the hall came to its end.

Event review

At the beginning of this report, we already mentioned that we wanted to take another critical look at the visitor numbers. This seems right and important to us if we first consider the significance of this meeting regarding the entire model railway sector.

This is exactly what we did within the editorial team and with all translators beforehand. The subsequent analysis revealed alarming findings that may well jeopardise the future of the meeting.

We have not only reduced the purpose of the Z gauge weekend to the desire to come together as a largely closed community and, shielded from the public, to find out about new layouts, dioramas, products, and developments.



Maciej Gawecki's pride and joy is the Polish PKP passenger steam locomotive Ol49, which he presented on his layout in Altenbeken.

Rather, this meeting fulfils the same function for us as comparable meetings for 1 and now also 0 gauges in Sinsheim, Speyer, Lauingen or Giessen: a showcase for the scales on display and effective publicity for the entire model railway sector. While these meetings also receive an appropriate response in the trade press, the situation for Z gauge is poor, except for the anniversary year 2012, not 2022.

Only Märklin-Magazin, Märklin TV, and we report from Altenbeken, and there was no interest elsewhere in actively offering articles. Such decisions are made by editorial offices from the readers' point of view, because sales figures and expectations determine that.

We believe that the reason behind this is a lack of public relations work or a lack of targeted public relations work. Altenbeken did not appear in the published event calendars outside of the above-mentioned media. With one single exception, we also found no advertising references from traders. Even on a tour of Altenbeken town centre, there was no information at all in the shops.

Due to a lack of advertising and advance reports in the daily press, there was no chance for families and occasional visitors in the immediate catchment area who were interested in model railways to even take note of an event in the Eggelandhalle. Not even the website of Z-Freunde International e.V. mentioned this meeting! Unfortunately, this knowledge was reserved for internet forum visitors.



Bahls Modelleisenbahnen presented the streamlined steam turbine locomotive T9 001 from 1944 as a new product (photo above). The Fokker DR 1 triplane is also a new product from the same company (photo below).

It is laudable that the ZFI board has taken the initiative to regularly provide Altenbeken kindergartens with model railway material and to publicise this in the local press afterwards. After the end of the event, however, this no longer influences visitor numbers.

And since nothing is older than yesterday's news, it won't have a positive effect on the next event either. Wouldn't it be better and smarter to get the kindergarten children in particular, who are supposed to play with the start-up material, "hooked" on the model railway hobby before the donations are handed over?

And if, after at least six fundraising runs (2012 to 2024), not all local kindergartens are saturated by any means, then it would make sense to turn the beneficiaries into participants, in advance, by distributing flyers or putting up posters. After all, they also benefit from raffle or flea market earnings because of high visitor numbers.



Märklin presented a first sample of the newly designed V 60 from series parts. When asked about extending the event by another day, we were told that this would not make economic sense.

We conclude that this is precisely where any future potential is being left untapped. The world's largest Z-gauge meeting is still a great success within the community, but outside it is perceived as meaningless. There is simply a lack of awareness of the meeting and, unfortunately, of our nominal size.

We would like to prove this with the visitor numbers that we have been informed of edition by edition: In the years from 2010 to 2016 (four editions; from 2012 under ZFI organisation), the number of guests was always estimated at around 1,000 visitors.

Originally, no admission fee was charged, so it was not possible to count the exact number for a long time. But even in the more recent editions, club members receive free admission and are therefore not recorded. Comparable measurement errors therefore existed and still exist each time and cannot explain variations.

In 2018, the Spur-Z weekend set an all-time record with 1,200 visitors (+ 20 %). Anyone who was there will never forget the constant cramped conditions in the aisles. The coronavirus pandemic then meant a hard break. The event only resumed with restrictions in 2022 and it remained unclear for a long time whether it would actually take place.

Despite the 50th anniversary of the Z gauge, which was actually a very high-profile event, only 800 visitors responded to the call to Altenbeken. Whether and to what extent the effects of the pandemic restrictions and the golden anniversary cancelled each other out or influenced each other cannot be proven with certainty.



The Berlin regulars' table ZIST1 has a long tradition of self-manufactured models. Modern and historic Berlin trams running on Shorty bogies or parts thereof are the latest results, which were demonstrated in Altenbeken.

As other events also experienced a similar situation, it was even more crucial to see how things would pick up again after the end of this pandemic. Elsewhere, this succeeded very impressively; what remained was the realisation that people would like to meet in person again, want to get out and show a need to catch up.

Unfortunately, this was not the case at the 9th Spur-Z weekend, as the 900 visitors mentioned represent a 10% drop in the long-term average and even a 25 % drop compared to the visitor record. And it is exactly this that we would like to use as a benchmark, because this reference value can also be interpreted to mean that the event has established itself in Altenbeken, the date at the end of March is marked in the diary and the meeting has generally become sufficiently well known.

The sudden change of date in 2022 to a warmer and less infectious springtime, which was not voluntary at the time, can then provide an additional explanation for the decline in visitor numbers, or at least hint at it.



The "Goldbroiler", the V 60 of the Deutsche Reichsbahn, could also be seen on the large Berlin club member's layout.

When the temperatures rise outside and it is light for longer, people are known to be drawn back outdoors after the winter months. Random indoor visits outside the core target group then only take place in bad weather if there is no alternative. The figures from 2024 prove this very impressively: while Saturday was well attended and recognisably mainly by Zetties, we experienced largely empty aisles on Sunday, which is generally a work-free day.

To our regret and the annoyance of paying guests, this also led to the first installations being dismantled and exhibits removed at around 3:30 pm, a full hour and a half before the end of the event, which also explains some of the gaps in our photo collection.

Problem misjudged?

The question therefore remains whether success or failure will also be analysed and scrutinised by the organiser to then act in a future-oriented manner based on the results. After all, we are writing here about a central task of the association in accordance with its statutes. But that is precisely what we are unable to recognise.

Shortly after the end, we heard that the next edition had been set for the Ascension weekend in 2026. So, the ZFI want to stay true to May and apparently even believe that they can capitalise on the busiest days of the year. Anyone travelling with their family during the bridging days or wandering through the fields with a handcart and beer will certainly find their way to Altenbeken. Don't all roads lead to Rome (or Altenbeken)?

Incidentally, a discussion we initiated on this topic in the Z-Freunde International forum met with some approval, but not from the board. The board only announced that the set-up would be opened on Thursday (Ascension Day) and that Friday, the previous set-up day, would be extended to become an internal exhibition day. This would be possible without any significant increase in costs.

But this is exactly what the commercial exhibitors already experience! In the run-up to the event, we had already received isolated reports that they would not be exhibiting in Altenbeken because the booth prices were too high.

Some of the suppliers of used goods complained on site that they were unable to recoup their costs because the range of goods on offer was too large.



Märklin also made it clear to our editorial team that the additional costs for another overnight stay and loss of regular working hours were not economically viable. While Intermodellbau in Dortmund was shortened by one day a few years ago for this reason, the self-confident Z fans are now taking the opposite approach.

Finally, let's take a look at the Lidbachtal, with which the club Blomberg had lined up.

Let's hope that the future Friday visitors don't get in the way of the trades people who want to set up at the same time...

We probably won't notice because the **Trainini®** team will also be on holiday and recovering on the chosen bridging days, and it is unlikely that anyone will be able to be there. We will (hopefully) report back here in four years' time.



Exhibitor selection with Z-gauge relevance:

<http://www.1-220-modellbahn.de>
<https://www.1zu220-shop.de>

<https://avantgarde-modellbau.beepworld.de>
<https://azar-models.com>
<http://www.bahls-modelleisenbahnen.de>
<https://bestagernet.de>

<https://www.crz-ulm.de>
<https://www.die-minilok.de>
<http://forstberg.de>
<http://www.hos-modellbahntechnik.de>

<https://www.klingenhoefer.com>
<http://www.komi-miniaturen.de>
<https://www.maerklin.de>
<http://www.michas-moba.de>

<http://modscholz.de>
<http://www.mrztrax.com/Zcatalogue.html>
<https://www.rainer-tielke-modellbau.com>
<https://stammtisch-untereschbach.de>

<https://www.rainer-tielke-modellbau.com>
<http://weichenlaterne.de>
<http://z-car-system.de>
<https://www.z-freunde-international.de>

<http://www.z-lights.de>
<https://zotti.lena-johannson.de/shop>
<https://www.z-spur.de/>
<https://z-stammtisch-bayern.de>

To episode 28 of Trainini TV:

<https://www.youtube.com/TraininiTV>

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Praise and reader questions from Iceland:

I have been following your magazine with interest and what an excellent magazine it is! As a Zettie since 1976 I have been pleasantly surprised by the slow and steady improvement in the quality, accuracy and detail of the models (Märklin and others) which seem to get better and better. Today's standards are excellent, perhaps with the exception of the pantographs, which are in desperate need of a viable replacement option, such as those offered by Sommerfeldt GmbH.

I also particularly enjoy your professional reviews of the latest models as well as the history and development of the models, e.g., the history of the BR 151 in **Trainini®** 12/2022 and the Swiss Re 6/6 in your current issue 5/2024.

You recently published an article on the various colours used by DB and DB AG (**Trainini®** 6/2023), in particular the so-called "pop colours".

This brings me to my question: has anyone you know published a list of the different RAL colour numbers used, the range of use and the period of use? I have started to compile my own list, but it is incomplete.

Thank you very much and keep up the good work!

Sverrir Ragnars Arngrimsson, Reykjavik (Iceland)

Editor's reply: In addition to good specialist literature, above all the books on the paintwork and designations of German railway vehicles by Wolfgang Diener, there are also sources on the Internet that have dealt with this area very well and present it clearly.

As an example, we would like to mention an address that we feel has been very carefully prepared: <https://www.bahnstatistik.de>.

Customised solution for current collectors:

Thanks you all for the work you do on **Trainini®**! There was a recent discussion on pantographs.

I live in New Zealand and (among many scales and prototypes!), I have been (very!) slowly building a small model of Wassen (Gotthard Pass). One of the first things that scared me were the Märklin pantographs, so I designed a set of etched ones suitable for the SBB locos. You can see the results here.





The single-arm pantographs, which were designed in-house and realised using the etching process, are functional, but perfect the prototypical appearance of a locomotive model, here the superimposed Re 4/4^{II} from Märklin. Photo: Darryl Bond

(...) The pantographs (blog): <https://zwassen.blogspot.com/2019/11/marklins-re-44-improving-looks-3.html>

The model also has a 'front' pilot replacing the coupler at one end, decal air conditioning unit, zettzeit.ch interior, gps aerials, roof painted silver, front step painted, UIC plug painted, etc. Cannot wait to see the new Re 6/6!

Darryl Bond (Neuseeland), per E-Mail

Many new locomotives at AZL:

The EMD SD70M-2 of the Canadian National is another locomotive model in a historic livery, this time based on the Grand Trunk scheme (item no. 63121-7). The EMD SD40-2 of the B&O for the Chessie system (64219-1 to -3) and the Chicago Northwestern (64203-1 to -3) are travelling in standard livery. Both show the individual detailing of their originals for these railway companies.

The GE ES44AC is at home with the UP at the largest company in the USA (62414-1). It has a pink ribbon on its sides to promote the fight against breast cancer. The ALCO RS-3 (63314-1 to -3) also returns to the programme with a version of the New York Central.





The UP's GE ES44AC has a pink bow on the sides (item no. 62414-1; page 70, photo below left), while the B&O's EMD SD40-2 for the Chessie system (64219-1; page 70, photo below right) stands out with its individual details. The Ortner bulk goods wagons for the Chessie system, set by C&O (905352-1; photo bottom right), are a perfect match. The closed wagons with compressed air unloading (916030-1; photo below left) appear in a new design, initially for the ATSF. Photos: AZL / Ztrack

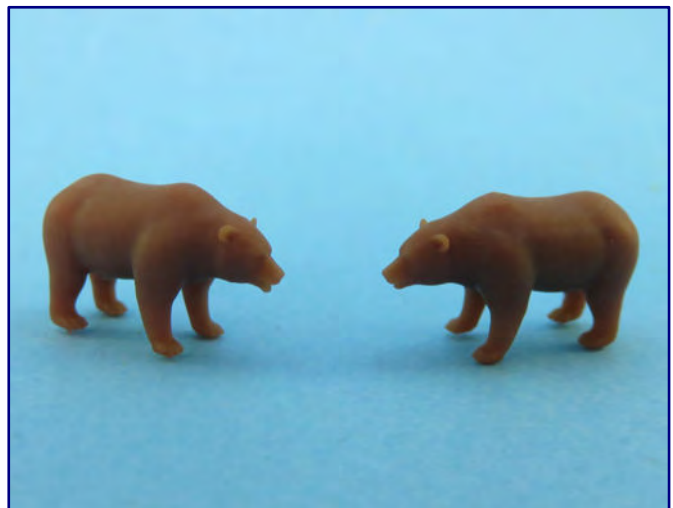
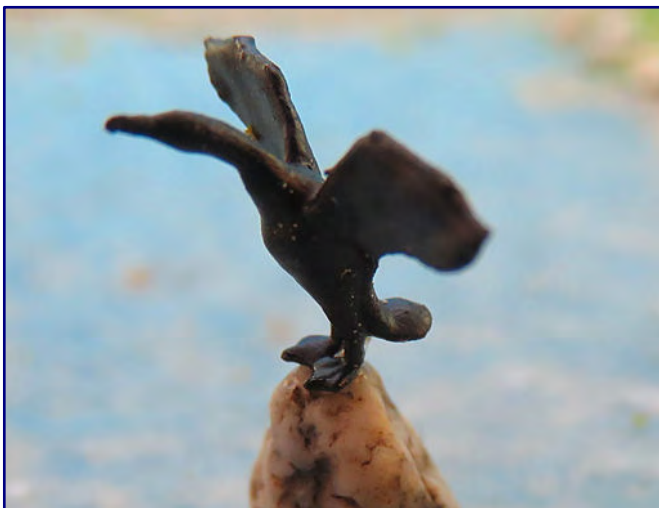
The covered bulk goods wagon 4180, which uses an air stream for loading and unloading and is therefore intended for powdery goods, is a new moulded item delivered for the first time. It appears in the first edition for the ATSF and is available in two sets of four (906000-1 / -2) and one double pack (916030-1).

The 40-foot Ortner bulk goods wagons are also offered in sets of four (905302-1) and two (905352-1) and run for the C&O in the Chessie system. The yellow-green colour scheme of the Reading (921028-1 /-2) wide-view goods train escort wagon stands out).

Photos of all new products can be viewed at <https://www.americanzline.com>.

New animal figures from Klingenhöfer:

Figures from Klingenhöfer Miniaturen (<https://www.klingenhoefer.com>) have been available since 2008. And over the years, a very special focus has developed here on a wide variety of animal figures. Surprising new products can be found here time and again.



Cormorants (item no. Z-TK08; photo left) and unpainted brown bears (Z-TB08; photo right) are the latest new products from Klingenhöfer Miniaturen, which were only launched after the meeting in Altenbeken. Photo: Klingenhöfer Miniaturen

This is also the case in May/June: The two cormorants (item no. Z-TK08) can be used to populate not only seashores, as these medium-sized waterfowl sometimes also colonise inland waters, where they certainly make a welcome change.

Two brown bears (Z-TB08) are supplied unpainted as brown blanks. Depending on the era depicted, they can represent zoo and former circus animals, but can also be used as wild animals in some Alpine regions. Here, too, there are a number of possible uses beyond the everyday. They can certainly also be used credibly on layouts based on North American motifs.

Rainy summer break:

As the start of the announced summer break turned out to be rainy, NoBa-Modelle (<https://www.noba-modelle.de>) already had a new design, which was actually only planned for the last quarter of 2024.

The new standardised water crane (item no. 5511R) is sure to meet with great demand, as a model of this once widely used supply part has not been commercially available for years.



The standard water crane (item no. 5511R) is an indispensable item on a layout with steam locomotive operation. Photo: NoBa-Modelle

In well-lit areas, the mounted lantern can also be dispensed with if the customer removes it himself before painting. A blank is supplied under this item number, which is assembled from three parts. With sufficient tinkering skills, this also opens up options for a rotating bracket, possibly even with a motorised drive.



The Convoy Rubber Duck (6928R; photo left) and Peterbilt 281 Duel Truck (6929R; photo right) are canvas heroes. Photos: Noba-Modelle

Two new lorry models with tanker trailers will enrich North American roads in future: Convoy Rubber Duck (6928R) and Peterbilt 281 Duel Truck (6929R), both supplied as resin blanks for self-painting, based on historical film models.

Open door day at Noch:

Noch invites you to its company headquarters in Wangen (Allgäu) for an open day on 6 July 2024. There will be plenty on offer there to allow visitors to experience the hobby of model landscaping up close. The highlights listed by the manufacturer include factory tours during ongoing production, workshop and handicraft demonstrations and a large flea market with remaining stock and discontinued items

The company museum “Noch-Modellbau-Welt” will also be open on this day, and Thorsten Rodriguez Fernandez will be exhibiting his 16m² funfair as a guest. There will also be an age-appropriate programme for children with arts and crafts and face painting, and food and drink will also be provided.

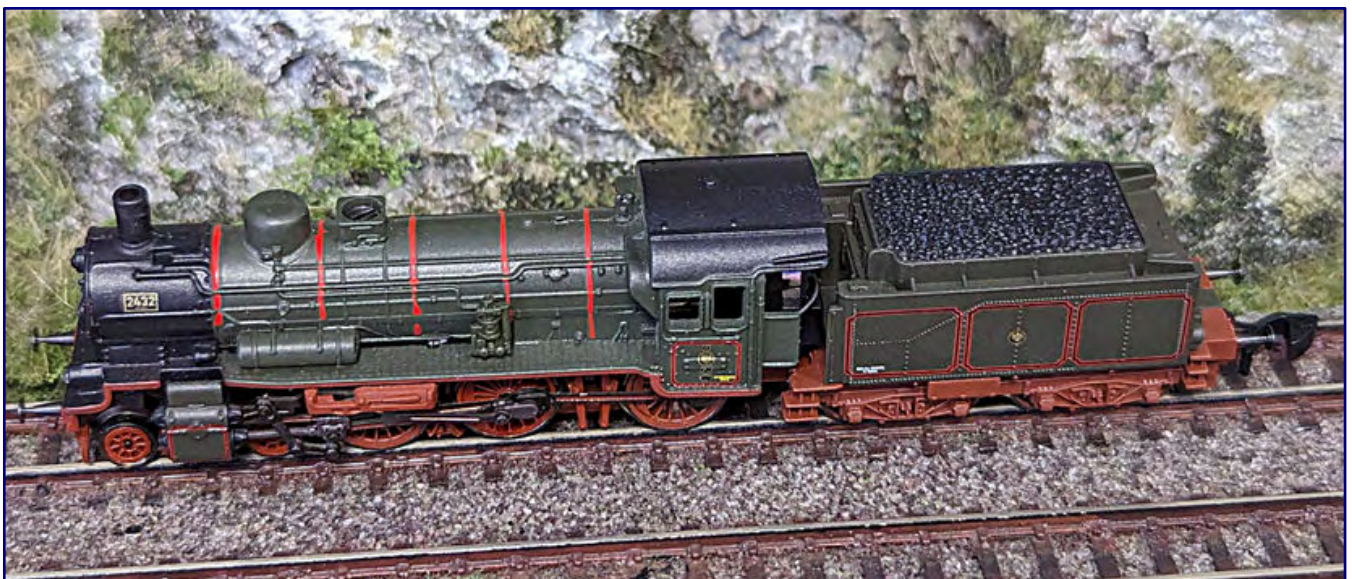
It is not necessary to register in advance; this is possible on site with different starting times for the company tours. If you are travelling by car, please use public parking spaces in the surrounding area. Current information will be published continuously at <https://www.noch.de/events-seminare/>.

The most important facts in brief:

Address	Lindauer Str. 49, 88239 Wangen im Allgäu
Date	Saturday, 6 July 2024
Time	9:00 - 16:00

Märklin deliveries in early summer:

With the steam locomotives of the class P 8 of the Prussian-Hessian State Railways (item no. 88995) for Era I and the class 01⁵ of the DR for Era IV (88018), two interesting models have been delivered, which technically correspond to their predecessors and are therefore not described in more detail.



The Prussian class P 8 steam locomotive for Era I (item no. 88995) is one of the current Märklin deliveries. Photo: 1zu220-Shop

In the case of the most powerful steam locomotive of the former Reichsbahn in the GDR, it should only be noted that the error of the first models delivered from the first edition has of course been corrected and that it has a mixing preheater box in front of the chimney ex works.

Also from the former DR is the “Rüganer Bäder-PmG” (87503) wagon set, which somewhat awkwardly describes the place of use on the Baltic Sea island of Rügen. It consists of three 2nd class Bi 341 thunder coaches, a G-04 boxcar and a Zd food tank car with the advertising lettering “Aiga.”



Car set “Rüganer Bäder-PmG” (87503; photo above) and “Spezial-Prellböcke” (89013; photo left). Photos: 1zu220-Shop (photo above) / Märklin (photo left)

The class 78 locomotive with Witte wind deflectors, which was only upgraded for use on this island, is intended as a suitable locomotive. The reason for this was the strong Baltic Sea winds, which were not supposed to obstruct the driver's view through the vapour vane.

The “Special buffer stop” kit (89013) supplied by Modellbau Laffont is probably not self-explanatory. The scaffolding built from this kit was not used to catch and stop free-rolling freight wagons, but was used specifically to bring slipping loads back into the correct position (and to be able to secure them).

The plank superstructure, therefore, only reaches into the transported goods above the loading area. The wagon floor and the chassis, therefore, rolled underneath, as far as necessary.

Deliveries from Busch:

Busch has now made some of the announced design material new products available. These are mainly the “mixed forest” for the H0 (art. no. 6488) and N / TT (6588) gauges. However, trees can also be used without hesitation for Z gauge, because they are far too small. The typical prototype tree heights can be found in Wikipedia and can easily be converted and adjusted to the 1:220 scale.

With the modelling snow paste (7173), a white, glittering landscape shines in the bright morning light as soon as it has been applied to a diorama or a well-tended layout. The paste is easy to mould and can be used to create snowmen, hills, and piles of snow.

The transparent modelling water (7580) for pouring in thin layers is also new on the market and is therefore also suitable for trickles. The modelling water colour set (7581) provides optional blue accents in various shades. The aim is to reproduce a depth effect or to create the typical blue water of the South Seas and some icy waters near the Alps in the model.

Rhein-Neckar-Stammtischwagen 2024:

As every year, the Stammtisch Rhein-Neckar has once again endeavoured to create an exclusive Stammtisch car based on a special model. The chosen design comes from Atelier Barkvieren and appears as a model from FR.



The Rhine-Neckar club car 2024 (item no. 49.342.41) was certainly a good idea: The model of the G1m 207 required changes to the etching templates for the UIC ventilation sliders and the sheet metal roof compared to the earlier FR model.

A DB type G1m 207 boxcar with a UIC number and additionally labelled with the former type designation Gm 35 (item no. 49.342.41) was selected. This places it in the transition period between model railway eras III and IV.

The prototype for the wagon was rebuilt on the chassis of the wartime model Bremen and as a prototype has colour-contrasting UIC ventilation sliders on the left long side. It is a special feature in the Z nominal scale, required design changes to the basic model and provides a colourful photo together with covered wagons of other types. There was a one-off production run of 45 units, which were sold out shortly before the editorial deadline.

Classic bulk goods wagon from WDW Full Throttle:

WDW Full Throttle (<http://www.wdwfullthrottle.com>) is launching a classic bulk goods wagon as a two-pack (item no. FT-2061): This is an open wagon with a prototypical length of 33 feet and external box struts. It is labelled on a brown background with the striking Santa Fe lettering of the ATSF.



The open wagon with two discharge hoppers (item no. FT-2061) is of an older design, but has been extensively refurbished and thus reached a considerable operating age in numbers worth seeing. Photo: WDW Full Throttle

The prototypes of these cars were older models that were refurbished and repainted due to the high demand in order to provide several more years of reliable service. With the ATSF, the supplier has selected one of the most popular railway companies in North America, which existed until 1996 and then merged with the Burlington Northern to form the BNSF.

Exclusive model for the 1zu220 shop:

The Modellbahn-Union provided the idea for the model, as they had published their own product for the H0 scale, which Jörg Erkel also liked for the 1:220 scale. After a brief exchange, which did not give rise to any reservations, he decided to order such a wagon based on an FR design for Z gauge as his own exclusive model.



With two side windows on each of the side walls, the workshop equipment trolley 649 (item no. 49.343.101) has been given an unmistakable look. The chrome oxide green painted special model from the 1zu220-Shop is therefore certainly a good addition to the crane train from NoBa-Modelle.

The Uwe Schuster assembly service has now delivered the desired workshop equipment trolley 649 (item no. 49.343.101) in chrome oxide green paint to RAL 6020, making it available from the 1zu220 shop (<https://www.1zu220-shop.de>).

The original was created on the basis of a surplus Pwghs 54 goods train luggage wagon by minor conversion and insertion of a suitable interior. As a service goods wagon, it is no longer used for public transport, but is used in auxiliary, construction or crane trains.

The model already has a UIC number and can therefore be used in colour well into the seventies. There it contrasts excellently with those coaches that were painted in the new RAL 5020 ocean blue colour scheme from 1974 onwards. It certainly also looks good in the crane train from NoBa-Modelle, to which it also matches in terms of time and colour.

Detailed information on the prototype will be provided in the book "Bahndienst- und Dienstgüterwagen - Band 2: Bauen und Wohnen" by Stefan Carstens, which has been announced for 2025.

News from the house Noch:

A replica of the N'EX Shiosai in three-part form appears as a new body from Rokuhan's Shorty series. With the motor chassis (item no. 7297903) and two non-driven bogies (7297905), the miniature can be made drivable. The beautiful chassis should also be available as a starter pack and make getting started as easy, as possible.

Many of Rokuhan's products were modelled on N-gauge products from the Japanese market leader Kato. For almost 30 years, its products were distributed in Europe by Noch. After the management founded a company in Denmark for distribution in Europe, this long-standing and successful co-operation between the two companies recently came to an end.

Herpa's new products for the high summer:

Shortly before its summer festival, Herpa announced the new products for July and August 2024. Several 1:200 scale models will appear in the Wings range, which we would like to list here.

The Goldhofer AST-2 aircraft tractor now appears with company imprints in the Lufthansa LEOS version (item no. 573238). It is joined by the following modern aeroplanes:



The widely used Goldhofer AST-2 aircraft tractor appears in the Lufthansa LEOS version (item no. 573238). Photo: Herpa

Binter Canarias Embraer E195-E2 "Islas Canarias" (573009) and Croatia Airlines Bombardier Q400 "Zagorje" (573108).

The Douglas C-54 "Skymaster" (573177) sultana bomber of the U.S. Air Force is destined for history buffs. The original of this model is on display today on the grounds of the former Rhein-Main Air Base, now Frankfurt Airport, from where a large proportion of the supply flights to Berlin took off.



The model of the Douglas C-54 "Skymaster" commemorates the "Raisin Bombers" and the 75th anniversary of the end of the Berlin Airlift. Illustration: Herpa

At the time, well over 200 aircraft were in operation around the clock to supply Berlin from the air during the Soviet-Russian blockade. The reason for the limited edition of this aircraft is the end of the airlift 75 years ago on 30 September 1949.

Imprint

ISSN 2512-8035

Bibliographic information of the German National Library: The German National Library lists this publication in the German National Bibliography. Detailed bibliographical data and editions can be found in the DNB catalogue at <https://portal.dnb.de>.

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International Edition

Free,
electronic magazine
for railroad enthusiasts
in the scale 1:220
and Prototype

www.trainini.eu

Published monthly
no guarantee

ISSN 2512-8035

Trainini

German Magazine for Z Gauge



All Creatures great and small

Improved Rail Transport
Summer Anniversary Party in Kamen

Introduction

Dear Readers,

In this edition, we can finally conclude a series of topics that we had designated as our annual focus for 2023. We decided early on what we wanted to cover and to what extent we thought it was right and appropriate.

However, postponed delivery dates and problems in the production of new products, which should and had to find their place in here in any case, required a rethink.

We didn't want to ignore innovative products that already grabbed our attention as samples in Nuremberg and promised to be exceptional. After all, this series was and is the perfect setting for them and they came closer to our expectations of coherent realisations than previous offers from the large-scale production.

We're talking about Noch, of course, as you've probably already realised. Figures with such a lifelike appearance have never been available from model railway dealers before. And figures, and today we are turning our attention to animals, are an essential part of any layout.

All too often they are placed carelessly and fail to have the desired impression. The geometrically minded mathematician or technician unconsciously places them according to a fixed grid spacing to fill as large an area as possible with the expensive accessory.

We want to break through such static thought patterns by looking at nature, internalize the behaviour of animals and then place them in such a way that they too have a story to tell us. No matter how small they may be, they will not fail to make an impact.

This is an approach "from prototype to model", which is also characteristic of our reader Michael Böttcher. As a former operation service teacher with the German Federal Railways, he has a completely different view of the prototype action and is fascinated by incorporating what he has seen into a story and telling it frankly on the model railway. It will be interesting to see what he has come up with for us.

Perhaps you will feel similarly inspired after reading this edition's gleaning of new products? We have put together some great products that we had already discovered in Altenbeken but were unable to include in the event report.

And finally, when our June edition was in the final stages of publication, there was a great celebration not far from the editorial office: Modellbahn-Union in Kamen turned 25 years old and used its biennial summer festival to commemorate the occasion appropriately.

We were also there to congratulate, celebrate and actively participate. We were able to show our own products in action, give explanatory modelling demonstrations and contribute an attractive children's programme too. Today we look back on these two days.

Now all of us from "Team **Trainini®**" hope you enjoy reading and wish you a great summer.

Sin-Z-erely,

Holger Späing



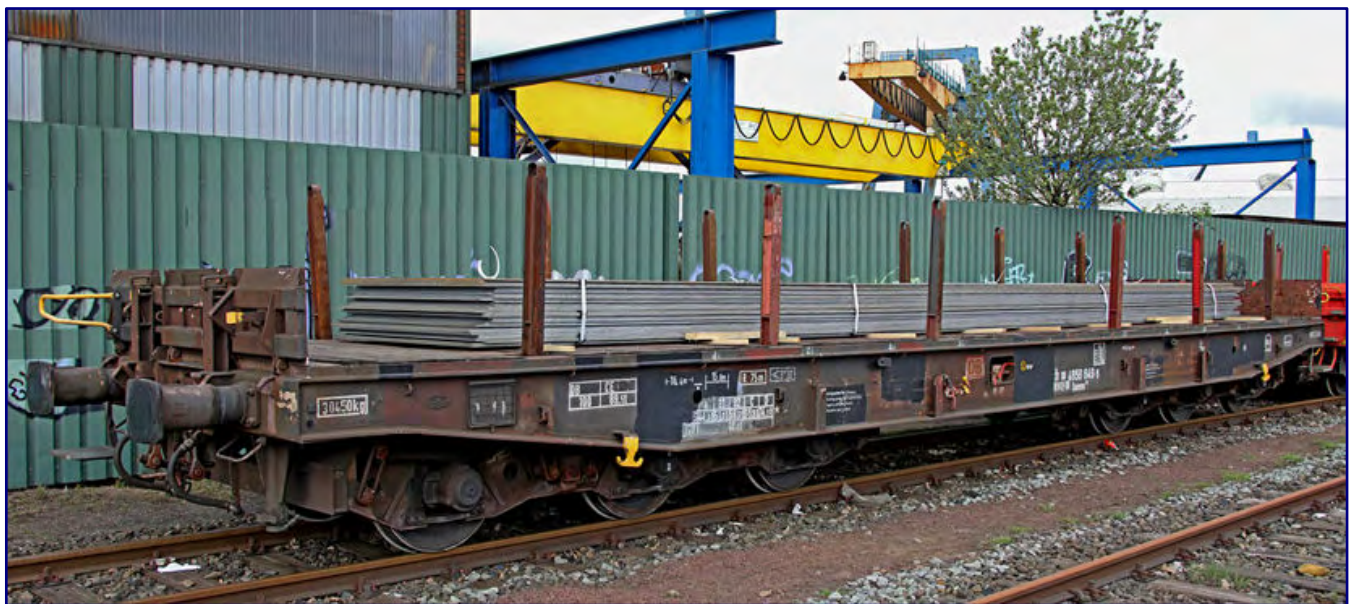
Holger Späing
Editor-in-chief

The Federal Railway around 1982 Special Steel Plate Transport

Michael Böttcher favours model railway operations that are as close to the original as possible. However, he is not interested in the exact reproduction of a specific prototype situation, but rather in inventing stories and freely realising them, for which he then uses the service regulations and timetables of the former German Federal Railways as a guide.

By Michael Böttcher. What drives us in the model railway hobby? The motivations are certainly as varied as they are justified. My main focus, for example, is on recreating prototypical events as realistically as possible in the model – in our case, of course, on a scale of 1:220.

I was recently triggered by an interesting incident that I came across in Hemelingen harbour in Bremen: several heavy goods cars loaded with steel plates were being unloaded using a forklift truck. I found that really interesting. Now it was “only” a matter of constructing a suitable story around the experience and making the necessary accessories.



Viewing this Samms 709 flat wagon loaded with steel sheets provided the idea for today's model contribution. On page 5, the detailed photo (top) shows the load securing, and below it the unloading of a more modern Samms 489 with the help of the forklift truck after opening the securing devices (bottom photo).

The framework conditions were quickly defined, “difficult” circumstances always require flexible solutions, and this was already the case at the beginning of the 1980s. Terms such as “just in time,” communication, teamwork, flexibility, “making a call,” etc., were not yet on everyone's lips; it was simply done.

It tells the story of an unscheduled transport of steel plates from the Bremen steelworks, which became necessary due to the coincidence of various adversities that unfortunately occurred at the same time.

The background of this transport is examined from the point of view of the operations officer. Which regulations, documents, and timetables are relevant? What does something like this look like and what decisions can, should and must be made on the basis of the regulations and documents? Which departments are involved, what is the content of the communication and how is the information channel to those involved? A lot comes together.

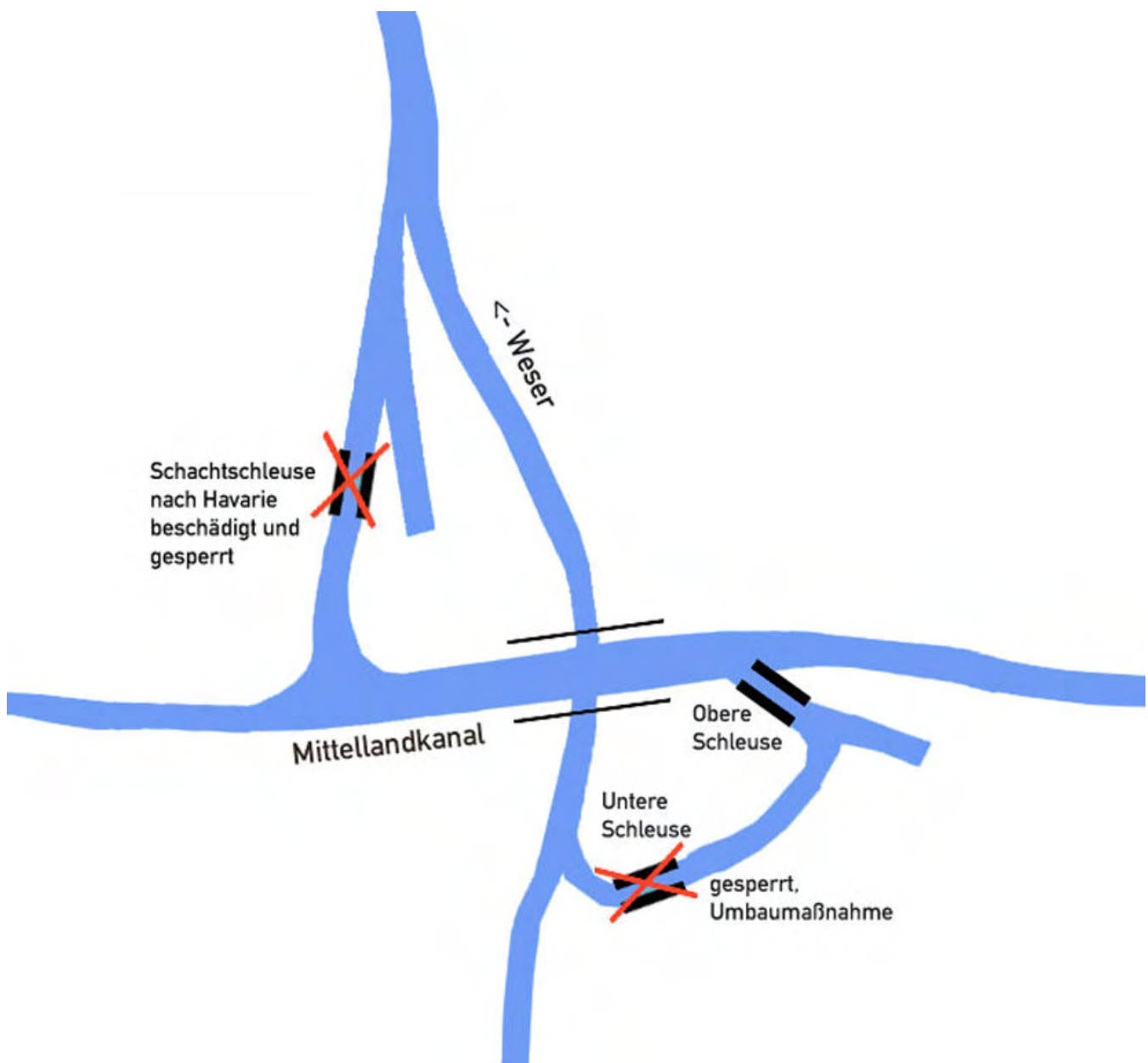


I would also like to point out that this is a purely fictitious story, which will always have more or less large logical and technical gaps, but, in any case, a large portion of tongue-in-cheek. Some of the company documents have also been supplemented or altered by quite freely interpreted facts!

The (pre-)history

A high-ranking employee of the Federal Ministry of Transport has personally called the manager of the Bremen steelworks: "The steel plates must leave your factory for Minden immediately. The train must be on its way by Wednesday 30 June at the latest. Let them attach one more wagon to any train – that's it!"

This brought the conversation to an abrupt end. "Just add one more wagon", was what the railway dispatcher at the steelworks received a little later from the very top. What had happened?

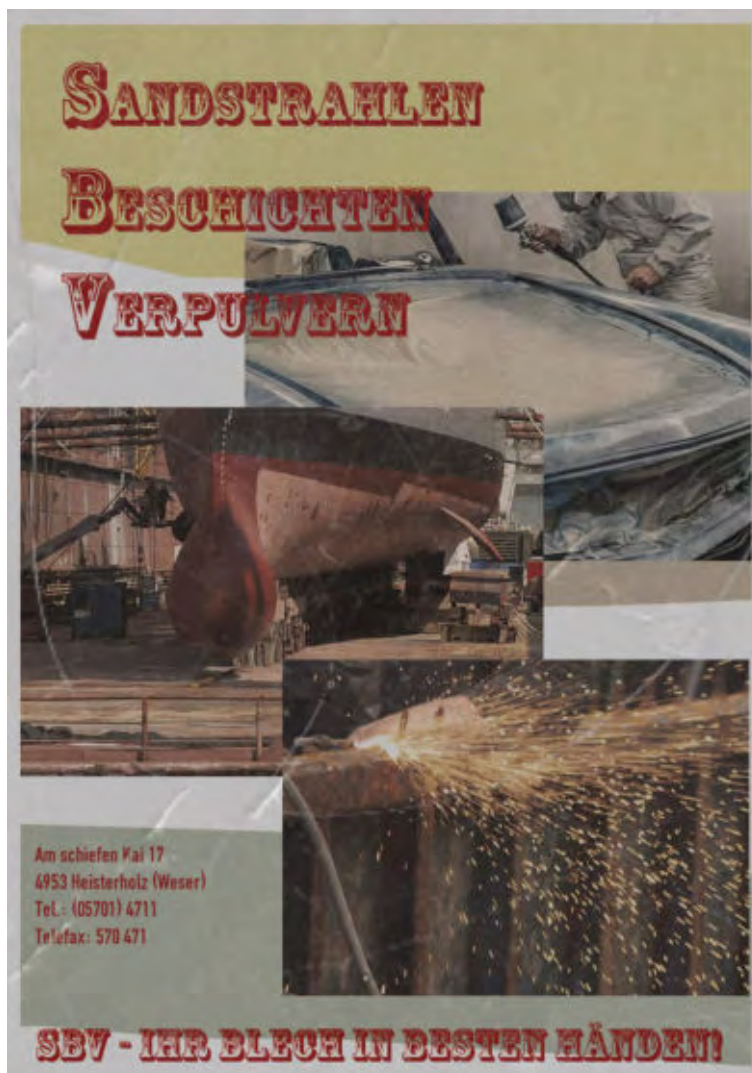


On 23 June 1982, a serious accident occurred in the Minden shaft lock. The large motor vessel GMS Edeltraut crashed almost unchecked into the lock exit gate as a result of a short-term engine failure and damaged it severely.

The consequences were far-reaching. As another lock in Minden ("lower lock") was undergoing renovation work, it could not be used either. The connection between the Weser and the Mittelland Canal was disrupted, not to say interrupted. A real disaster for inland shipping on the Weser and Mittelland Canal federal waterways.

What a mishap! But how to get out of this mess? The lock gate now has to be completely replaced. We are talking about several tonnes of 20 mm thick steel plates that had to be used to create a stable new construction for the lock gate.

Another problem is that the steel plates from the steelworks in Bremen cannot be installed in the same way as they come from there. As water is part of the nature of a lock gate, the steel plates must first be given a waterproof coating.



And this is where the company SBV in Heisterholz (Weser), a few kilometres from Minden, comes into play. The steel plates have to be pre-treated there.

I found an old trade fair brochure for the company in the documents. Well, and as life goes: "Sometimes you're not lucky, and then, you're unlucky too."

Of course, SBV also has a railway siding, but it is currently undergoing a long-planned track renovation that will take at least another week to complete.

This means that the company can only be reached by water (on the Weser).

Due to the damage to the battle lock in Minden, the entire upper Weser is closed to shipping traffic to prevent a backlog of ships. Quick solutions must now be found.

So, the powerful officials of the Hannover operations management (based at the Hannover BD) came up with the following plan:

The steel plates were to be transported on a suitable wagon on a scheduled train from the Bremen steelworks to Minden. There, the wagon was to be abandoned and

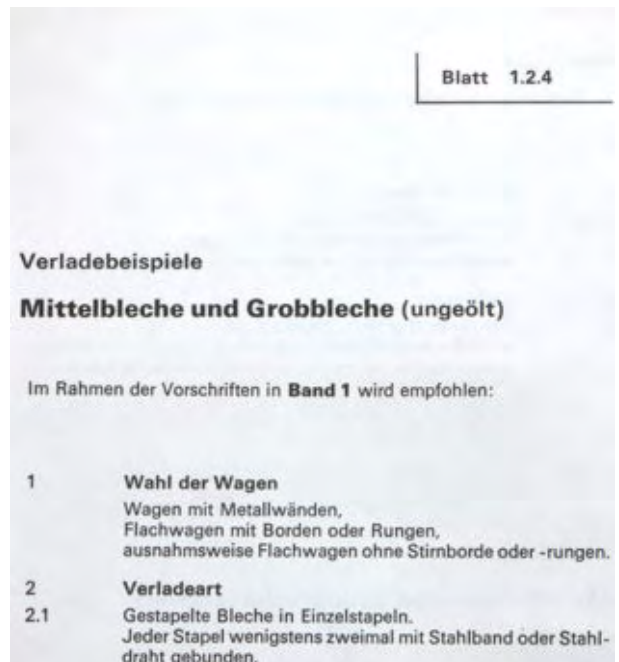
transported to the Hankenberge stop on the loading line by means of a special transfer. From there, it continues by road roller to the company SBV in Heisterholz, 4 kilometres away.

Meanwhile, the DB official at the steelworks is still thinking about the extra wagon. "He's got a clue, the foreman, as if there were any wagons just standing around in the neighbourhood."

This is where the problem starts, because a Samms wagon would actually have been needed for the 79 tonnes of steel plates, but after consultation with the management wagon service, such a wagon was not available at short notice.

There is only one Sa 705 heavy goods vehicle left at the factory, which is not currently in use. And it has neither end walls nor stanchions. So, we first have to see if that's even possible. Exactly the exception we need is quickly found, and that fits the bill.

Let's have a look for a suitable train and consult the GZV (goods train formation regulations). 52740, it looks good, as it is scheduled to run via Nienburg – Minden, so the Bremen – Osnabrück runway should not have had a free timetable path at that time.



Gag 57 237 W Sa B	Neubeckum—Löhne (Westf)—Nienburg— Bremerhaven Seehafen Bremen Zollausschluß
Bremerhaven Kaiserhafen oder Bremen Zollausschluß	Zement. 610 m
Gdg 57 240 S/nS	Bremen Klöckner-Hütte—Nienburg—Löhne (Westf)— Westerholt
Westerholt	Leere Fad. Verk T monatlich durch Erzprogramm 610 m
Gdg 57 244 S/nS	Bremen Klöckner-Hütte—Osnabrück Hbf— Marl-Sinsen—Westerholt
1. Westerholt oder Obereving 2. Leere Torpedowg Hasbergen aus 57279 B bzw 57281 B	Leere Fad. Bedarfs-Flügelpläne Münster—Obereving. Verk T monatlich durch Erzprogramm 610 m

A block train consisting of empty Fad wagons, the so-called “Empty Coaler”. A look at the goods train formation regulations reveals the following: “Deviations must be agreed with the management and the departments involved.” The number is instructed by the Federal Ministry of Transport, so it's a gift.



Finally, a special transfer has to be inserted. Due to the immense time pressure, this was done by telex. This telex was quite something. Let's see what has to be communicated to whom and how. Here are a few points:

- Which railway stations and offices should be notified? Who is responsible for dropping off the wagon in Minden?
- Which department provides the locomotive and the shunting staff for the special transfer? Important: It must be a V locomotive (diesel locomotive) because the loading line in Hankenberge is not spanned with contact wire.

- Which department provides the Culemeyer road scooter and its tractor unit? This department, Kbw Minden, also has to produce a support frame for the steel plates on the Culemeyer at short notice.
- Which timetable is required for the special transfer to Hankenberge?

So here is the telex:

64 minden
sam
sam
26 han db

b 1527 von hannover 27 06 1657

bf bw kbw ma gv ozl ztl
von nienburg (weser) ueber leese-stolzenau bis minden (weser)
bw minden
kbw minden
gv minden
nachr. : ozl essen ztl mainz

betreff apl. ueberf schwerlastwagen

am 30.06.82 ueberfuehrt dgd 57240 einen bel. schwerlastwagen sa705
mit stahlplatten von bremen stahlwerke ueber minden gbf zur
weiterbefoerderung nach hankenberge.
nach entladung hankenberge weitertransport der stahlplatten per
strassenroller zur firma sbv heisterholz (weser).

ankunft minden gbf 14,47. sa705 (reihung spitze) absetzen.

um 15,27 verkehrt ngs 86061 von minden gbf nach hankenberge
fahrplan minden gbf 15,27 hankenberge an 15,33.
hg 80 km/h, last 99t, mbr 64, fzt nach buch 162 seite 148.
rueckfahrt wird besonders geregelt.

bf windheim erteilt bef ad apl halt 57240 in minden gbf.

bf minden stellt rg fuer absetzen sa705 in minden gbf und ueberf
nach hankenberge (mitfahrt auf tfz).
ladestrasse hankenberge freihalten 30.06.82 10-19 uhr.

bw minden stellt vlok und tf br211/212.

kbw minden stellt strassenroller, zugmaschine und -bediener.
strassenroller ist mit tragegestell f stahlplatten auszuruesten.

entladung sa705 durch fa sbv.
ladegeschaeft ist bis 18 uhr zu beenden und ladestr anschliessend zu
raeumen.

beteiligte verstaendigen.

bd hannover bf 13 bfg +

25 han db 1712

64 minden
64 minden

Bohnhof
Minden (West.)
Eing. 28. JUNI 1982
AA

A1 A2 A4 Aufg Gest Fdc Rg
GV Bw Kbw

At another level, the confirmation of the agreement between the transport service provider Deutsche Bundesbahn and the customer / freight recipient SBC must be regulated. First of all, it is agreed by telephone that the SBV will provide the corresponding unloading device and its operator. Everything must be in official order! Here is the corresponding confirmation telex from the Minden general agency to SBV:

570471 sbv d
311224 db d

fs 1436 von minden (weser) lfsst 28 06 1134

firma sbv
metallveredelung gmbh u co.kg
telex-nr.570 471
4953 heisterholz (weser)

telefonat zwischen ihrem herrn oelgemeier und unserem kundenberater,
herrn boettinger, vom 27.06.82, 15.30 uhr

sehr geehrte damen und herren,

zur schriftlichen bestaetigung des gestern stattgefundenen telefongespraechs
unserer mitarbeiter wird folgendes zusammengefasst:

die dringlichkeit des stahlplattentransportes zur reparatur der
schachtschleuse steht ausser frage, das bundesministerium fuer verkehr (bmv) hat
aeusserste prioritaet und einen entsprechenden aktionsbedarf festgestellt.

wegen der derzeit laufenden instandsetzungsarbeiten an ihrem gleisanschluss
muss die ueberfuehrung der stahlplatten (79 tonnen) von hankenberge in ihr werk
im wege des strassenrollerbetriebes abgewickelt werden. strassenroller,
zugmaschine und fahrer werden durch die bundesbahn gestellt.
sie sorgen fuer eine rechtzeitige bereitstellung des entsprechend geeigneten
entladegeraetes einschliesslich eines befahigtgen bedieners an der
ladestrasse der haltestelle hankenberge. ankunft des uebergabezuges, bestehend
aus einem beladenen schwerlastwagen sa705 von minden, ist am 30.06.82 um
15.33 uhr.

das ladegeschaeft muss bis 18 uhr abgeschlossen sein.

mit freundlichen gruessen

dr. schrullenstock, generalvertreter

generalvertretung minden (weser) +

311224 db d 1673+++
570471 sbv d

The journey begins

Now let's just imagine it's 30 June 1982 and everything is ready. The special transfer of the steel plates on the Sa 705, lined up at the head of train Gdg 57240, can start at the Bremen steelworks.

23

Gdg 57240 Bremen Klöcknerhütte - Nienburg - Löhne Gbf (-Westerholt)
Ttz 140 Last 1600 t Mar 75

		57240					
1	2	3a	3b	4	5	4	5
	40	Klöcknerhütte	2,5		12,15		
		Abzw Bremen-	0,3		20		
		Inlandfl II	2,5				
			2,3				
		- ZBF A 73 -	2,3				
		Bremen Rbf Bve	0,0		24		
			12,1				
10,8		Bremen Rbf Bvl	11,2	+12,25	39		
	60	Bremen Rbf Bvp	10,1		42		
9,1		Asig	9,1				
	40	Bremen Rbf Bal	8,9		44		
		Bremen Hbf Bgw	7,4		48		
6,7		Zsig	6,7				
	60	Brm Hbf Bgn/Bgm	6,5		51		
		Brm Hbf Bgo	5,8				
		Brm Hbf Bhw	5,4		53		
		Bremen Hbf	5,1				
4,5		Bremen Hbf Bhl	4,7		54		
	80	Abzw Vahr	2,1		57		
			2,0				
			2,0				
0,4		Abzw Hasledt	0,4		13,01		
	50						

27

		57240					
1	2	3a	3b	4	5	4	5
	80	- ZBF A 67 -					
		Nienburg		0,0		14,01	
		Langendamm	E 60	4,4		07	
			A 60				
		Schäterhof		7,7		10	
		Estorf		11,1		13	
		Landesbergen		14,9		16	
		Leese-Stolzenau		20,7		21	
		Schlüsselburg Hp		24,9			
		Heimsen		28,9		28	
		Döhren Hst		33,0			
		Windheim		36,3		34	
		Lahde	A 60	40,8		38	
		Hankenberge Hst		45,0		42	
		Minden Gbf		51,6		47	
				51,7			
				63,1			
		- G-Bahn -					
		Minden Pbf		64,4		48	
65,6							
	100	Porta Gbf		68,5		52	
		Sbk 171		69,9			
		Sbk 173		71,2			
		Vennebeck		73,9		14,56	

Book timetable for Gdg 57240 (photo left) and the relevant section from Nienburg to Minden (photo right).





We encounter Gdg 57240 for the first time shortly after the junction to the “Nato railway” (photo on page 12 below) and at the intermediate stop in Windheim (photo above), where the driver accepts the Ad command.

Befehl A
 Zug / Sperrfahrt S 7240

a) fährt ohne Ausfahrtsignal aus

b) fährt vorbei am – Halt zeigenden – gestörten –
 Einfahrtsignal Sperrsignal /
 Zwischensignal Sperrsignal /
 Ausfahrtsignal Sperrsignal /
 Deckungssignal der Deckungsstelle
 Blocksignal der Block-Abzweig- stelle
 Selbstblocksignal Nr. / / /

c) fährt ohne Signal – ein – weiter

d)
 hält außerplanmäßig in
 Minden Gbf. 1 Wagon
 absetzen

Windheim, den 30. 06. 1982, 14 Uhr 40 Min
 Der Fahrdienstleiter Erhalten (1 mal)
 Bos Schulte Borna Tf
 (Name, Dienststellung)

Gültiges unter Benutzung der Querslinien umrahmen!
 Nichtzupassendes ins umrahmten Teilschlag streichen!

405 14 Befehl A 95 x 340 mm Bk 100 5c50
 405 14.1 desgleichen 95 x 210 mm Bk 25 5c50

01

The weather conditions are similar to the real summer of 2024: mixed, a bit of everything. Here we meet the train just after the junction with the “Nato railway” (Nienburg – Minden), the Nienburg – Hannover line can still be seen in the background.

In the meantime, Gdg 57240 has reached Windheim, where it is briefly called Hp0 - train stop. There, the driver accepts command A for the unscheduled stop at Minden Gbf to set down the steel plate wagon, as specified in the insert telex. We would not like to withhold a copy of the document that the Windheim dispatcher has kept from our readers (photo left).

After a short stop, it's “bon voyage” again, the two railwaymen say goodbye, the dispatcher pulls the exit signal and the journey can continue.

A short time later, our goods train passes the Hankenberge stop, where the Sa 705 with the steel plates is supposed to go. But there is no chance of setting it down here yet. There is still absolutely nothing going on in the area of the loading line. Gdg 57240 continues towards Minden.

Please do not be alarmed that the block signal for the opposite direction shows “drive” (photo page 13 above). In the meantime, it has been switched through and therefore only signals passage.



Gdg 57240 passes Hankenberge (photo above), but is unable to set down the flat wagon destined for this station. A little later, both the Culemeyer road roller and the “unloading device” from SBV have arrived at the loading line (photo below).

But a little later, there is a hint of activity in Hankenberge. Both the Culemeyer team and the “unloader” from SBV have now arrived. A brief discussion ensues and the DB driver is a little taken aback.

“Moin. A forklift truck? What do you want with it?” “Moin too, loading steel plates, what else? That's what we always do at the factory.” “Well, if you say so. Can I leave the trailer like this? Otherwise, say now that I want to go back later...” “Right, leave it like that.”

That takes care of that as well. Shortly before Minden, a last look at 57240, here it rolls past a loading point that has been “left open” in the meantime, which the local goods train with the single wagonload of steel plates will pass again later on its way to Hankenberge.



Gdg 57240 passes a loading ramp that has now been left open at a so-called alternative connection point (Awanst).

At around the same time, the 212 that will later be required in Minden is still travelling on a branch line with a transfer. This consignment will also reach Minden goods station shortly. Now that both trains have arrived in Minden, things have to move quickly. The 140 of 57240 sets the steel plate wagon down and back onto the train. Brake test, update the paperwork – on we go.

The 212 then accepts the Sa 705. Brake test, the train dispatcher hands over the wagon list and the brake note, the driver opens the book timetable with the comparison plan and the train is ready to go. The exit signal shows “drive,” the driver presses the hand wheel of the driving switch, the train starts to move and a few minutes later passes a familiar spot.

There, the colleague from the rubber track is also travelling quite quickly. The two drivers meet briefly, greet each other and continue on their respective journeys. Finally, the short journey of the local goods train draws to a close as it reaches the entrance to Hankenberge.

continued on page 17



A class 212 diesel locomotive is travelling with a transfer on a branch line (photo above) and accepts the Sa 705 when it arrives in Minden. As it also passes Awanst, now in the opposite direction, it encounters a railway bus (photo below)

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Now the points still have to be unlocked. The shunter collects the necessary key from the signal box. After the shunting department has pulled up to the points, the train is pushed to the loading line. The shunter is at the head, more or less in accordance with the regulations, and gives the Ra2 “come here” signal.



Shortly afterwards, the entrance to Hankenberge (photo above) is reached. After unlocking the points, the heavy goods vehicle is pushed past the road roller and onto the loading road (photo below).

A short time later, the department is ready for the forklift driver to start work. In the meantime, the three railwaymen, engine driver, shunter and Kaelble driver, have gone off for a pot of coffee: “Do you need any more help? We’re off, then...”



The unloading process is routine. The forklift picks up three sheets at a time and transports them to the road roller, on which they are then deposited.

“Bullshit” he thinks to himself and just gets going. The steel plate unloading routine obviously seems to work. After all, it’s not the first time his colleague at the stacking fork has done this. The end of the work

is in sight. The forklift carefully drives up to the trolley, picks up three plates, “shuffles” them into position and places them on the Culemeyer. Precision work, skilful is skilful!

Half an hour later, the work is actually finished, the Sa 705 is unloaded and the Culemeyer is loaded with the steel plates. The load is not stacked as neatly as on the railway wagon, but for the few kilometres on the road at crawl speed, it should be fine.

The load is lashed down and the Kaelble starts to move. The forklift truck is already on its way, after all, the steel plates have to be unloaded at the SBV plant. The railwaymen from the goods train also want to get going again, it's time for the end of the working day. With the forklift truck, he has pretty much ripped up the wooden frame on the heavy goods wagon, but the nails are still in it, which should still fit for the few kilometres back to Minden.



The passing of D 739 still has to be waited for before the shunting unit can pull out of the loading siding to start its return journey. However, the diesel locomotive will only be able to switch to the other end of the train in Windheim.

In the background, D 739 from Bielefeld to Hamburg-Altona is rushing towards its next stop at Leese-Stolzenau. The driver wants to get going, after all they still have to go to Windheim to change trains: “So, we can get on track behind that one. Is the wood on the back still solid? When the express train is through, unlock the points and I'll pull ahead.”

“Yes, master.” Behind the express train, the two of them finally pull away and everyday life in Hankenberge is back to normal. An eventful working day draws to a close.

All photos and illustrations: Michael Böttcher

Information on the Minden waterway junction:
https://dewiki.de/Lexikon/Wasserstra%C3%9Fenkrenz_Minden
https://gewaesser.rudern.de/_detail/minden-11.png?id=wasserstrassenkreuz_minden

New products review

New Products Show in May

At the beginning of May in Altenbeken, several important new products for our gauge were showcased that could not be included in the report from the event. We therefore want to give them the space they deserve in this section and present them to our readers in more detail after the event.

The French manufacturer Azar Models celebrated an exhibition premiere in Germany. Owner Moïse Rogez already has extensive experience in modelling, having previously manufactured and sold drones. As a supplier of model railways, however, he is still quite young, which is all the more reason for him to make a name for himself with spectacular models and innovative ideas.

His financing project for the French high-speed TGV train, which was launched at the end of 2023 and successfully completed in February 2024, attracted a great deal of attention. Having such a large project financed not by banks but by customers and other private lenders deserves respect. It was not easy to expect that it would be successful because it was a premiere.

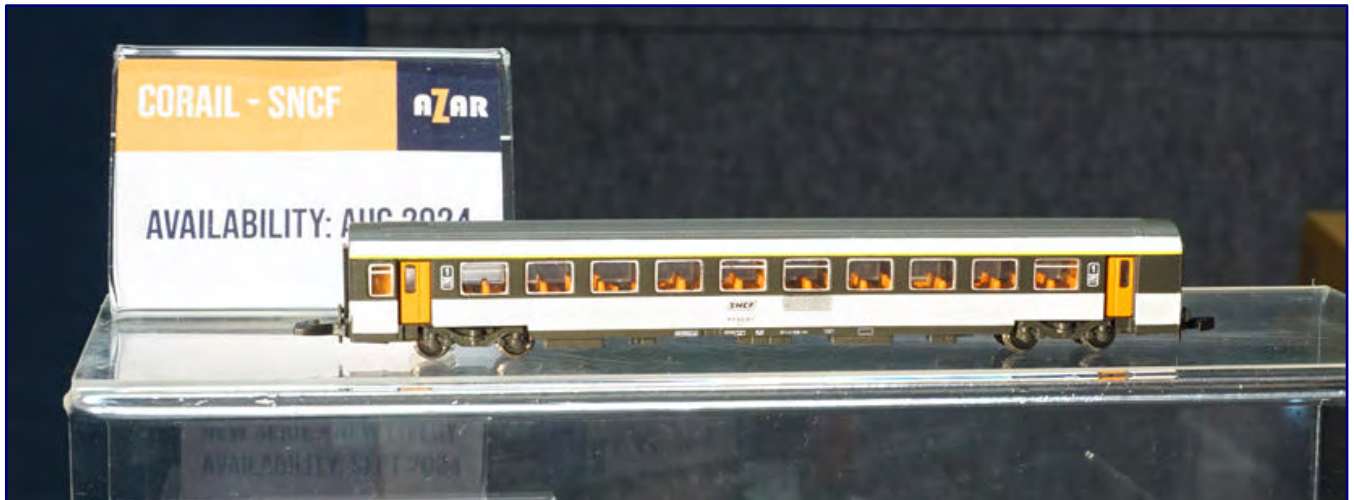
Of course, there is now great interest in the progress of this project, as it will probably be some time before the first moulded samples are ready. The French world record train was given its own display case in Altenbeken, in which it was presented in seven prototypical parts.



With the BB 67400 in the "Isabelle" livery, one of the announced novelties was already running on the Azar Models demonstration layout. The covered wagons on the coupling are also the first samples of a novelty presented for the first time in Altenbeken.

It was hard to believe that the hand samples on show were still 3D printed examples! Delivery is scheduled for 2025, and at that time two units of this train can optionally be coupled. Analogue and digital versions will be offered, and both versions have fine pantographs made of injection-moulded plastic in common – they were still missing on the sample shown.

The product features listed also include electrically connected bogies, although this has not yet been described in more detail in relation to the Jacob bogies of the intermediate wagons: Does it serve to draw



It could be available shortly after this issue is published: The SNCF Corail coach also makes a perfect addition to the German class 181², which Rokuhan realised almost ten years ago.

current across all axles or should current be drawn at the front depending on the direction of travel in order not to run over disconnected stopping sections in analogue operation?

There are no questions about the interior design of all intermediate carriages, which also includes the bar car, which is effectively emphasised by interior lighting. The head and tail lights on the power cars shall also alternate depending on the direction of travel.

The Corail coach, which was presented in the 1st class version, needs no explicit explanation at this point. The interior fittings were clearly recognisable and the paintwork and printing definitely demonstrated its status as a production release sample.



This is the current status of the French TGV project at Azar Models. In this case, all components still come from the 3D printer.

These wagons, which will be offered as wagon collection, are also being injection moulded and should be available next month. In view of their (also cross-border) significance, we will then also present them in detail in **Trainini®**.

However, two further models that are no longer new construction models will be available in July. We already presented the SNCF diesel locomotive BB 67400 in **Trainini®** 7/2022. Two years later, the previous versions are now being reissued using a production technique with plastic injection moulding and a changed road number. The “Capitole” and “Isabelle” colour variants are completely new.



Three livery variants of the BB 67400 were presented (top row in the photo above), including “Isabelle” on the far right, which had not previously been offered. One row below, are the CC 65000 with newly designed single-arm pantographs mentioned in the text and two versions of the CC 72000 diesel locomotive. Two further designs of the 67400 are already scheduled for delivery (photo below), including the “Capitole” (on the right in the photo) for pulling a historic set for the first time.

The six-axle diesel locomotive of the CC 72000 series was shown as a 3D printed sample with complete paintwork but is also expected to be available soon.

The “Logserv” and “FRET SNCF” livery variants of the tarpaulin-covered wagons are planned for September. They differ from Märklin's design, which will certainly only be noticed immediately by good prototype experts but is certainly relevant. Finally, the CC 65000 is expected at the end of 2024 and has been announced in various versions – we have already been able to show factory photos of this six-axle locomotive in a monthly edition.

Samples of models that are part of the project schedule for next year were also presented. First and foremost is the French-built NS Series 1600 locomotive, which is travelling for the foreign subsidiary of Deutsche Bahn in the livery shown. Azar Models had indicated to us that production would depend on interest and possible pre-orders to be able to determine the economic viability of this variant in advance.



The NS 1600 series, which has been transferred to Deutsche Bahn's foreign subsidiary (photo above), will only be produced if there is sufficient interest. Another project for 2025 is the four-axle, covered SNCF freight wagons in four different designs (bottom row in the photo below).

The last project not yet mentioned by Azar Models for the coming year is four-axle, covered SNCF freight wagons, of which four variants are planned: the historical version in the usual brown freight wagon livery, in yellow for horse transport, in the green of the FRET and a grey private version from Sernam.

Such wagons are eye-catchers in every freight train, as they can be found internationally and their length is rather unusual for German eyes, because the Deutsche Bundesbahn had relied exclusively on two-axle wagons. Things were different in France and Italy, where the large area was appreciated and significantly extended in this way.

Creative products from Trafofuchs

It was mentioned several times in new product presentations that Trafofuchs is gradually converting well-known figures to 3D printing production, but new ideas are also constantly being hatched. There were also some of these to be seen in Altenbeken.

Among them was the Olsen gang, a well-known film trio that also encountered the railway. A collection of travellers not only complements the great demand for different figures for busy platforms, but also featured designs that are not part of the usual stock of the large-scale series.



The travellers (above) stand out clearly from the familiar Preiser figures in terms of their outfitting and posture and thus vivify the platform even more. The Olsen gang (below) also has a real railway connection from the movies.

The dark blue dressed rider of a red trike is still one of the latest new products, even if it is no longer quite “dew-fresh”. Birgit Foken-Brock, thus, offers one more figure for modern layout themes.

Children's playgrounds are one of the neglected layout themes that can, however, create beautiful eye-catchers. These include not only the otherwise neglected children's figures, but also play equipment such as slides, seesaws and climbing frames with swings. At Trafofuchs, everything is part of an item that was also shown in Altenbeken.

Central squares form a visual centrepiece on many layouts, both with village and small-town motifs. They are often brought to life and filled with a weekly market. Many booths can be found in the Trafofuchs programme for this purpose, but new products are also constantly being added.

Pushed sausage and ice cream carts can certainly be found elsewhere, but a mobile service counter, i.e., a sales trolley on a delivery van chassis, is simply too typical and commonplace for the popular market stands. Two new models with a rear double axle were on show.



Photo above:
The children's playground with figures (photo above) closes a gap in the range that has existed again for many years. Once upon a time, etched play equipment was offered by 1zu220-Modelle.

Photo below:
A wide range of products vitalise beach promenades, holiday resorts, fairgrounds and the weekly market (top row). Typical sales trolleys for fish (bottom row left) and bread and sausage products (bottom row right) are not to be missed.

One of them replicates a fish store, while the other is a meat and sausage shop. They look great among the fruit and vegetable stands and the many shoppers with bags in their hands that are part of the accompanying product range.

Car models close gaps

Only a few suppliers currently have car models in their programme. They often sell rather small quantities, and there are also high postage costs due to shipping from abroad. It is therefore to be welcomed if further gaps are closed and there are alternatives that also allow purchases at exhibitions.

This is the case with Raffele Picollo (Genoa), who has been active for some time under the brand name "MrZtraX". He has been taking part in the events in Altenbeken for years, and now also has an impressive line-up that is very broadly based.



The 1965 version of the VW Beetle makes its way into the MZX (MrZtraX) lineup, complemented by the "VW Baja Bug" as a pimped-up beach buggy (right in photo). The Volkswagen Transporter T 3, which is also offered in various versions (left in photo), is likely to be in demand, too. All car models are 3D-printed from transparent resin.

He started with a few movie cars in particular, which brought him a lot of attention. This was followed by everyday cars of the present and past, which are missing from many layouts. He also presented some of them in May 2024.

We were particularly excited about the VW Transporter T 3, the last one with a rear engine. Although this concept seemed outdated when it first appeared, it was very successful because it was a good-tempered car that proved itself in many forms in everyday life.

Thus, one- and two-colour Caravelle versions, a camper van and two panel vans with company advertising are reminiscent of this. Raffaele presented the predecessor T 2 in the form of two special models for the "Puppenkönig Bonn" (closed panel van) and as a platform lorry with an oversized squeaky duck.

The VW Beetle has probably already been part of every car model supplier's range, so the Type 1 from 1965 was also introduced here in various colours. The road vehicles Baja Bug were supplemented by various versions with long exhaust pipes and on high wheels, called the VW Baja Bug.

He also had the Japanese market in mind, as several new products were shown that are almost unknown in Europe: Honda Z360, Honda S-MX, Toyota Crown as a police car, Honda City Turbo II, Toyota FJ



Mr. Bean's Austin Mini in this famous film scene (photo above left) has long been a classic in MrZtraX's range, but is shown here in close-up to illustrate the high detail reproduction and effect of the clear windows. Brand new and not yet delivered are the "Beetles under tarpaulin" for prototypical loading of the new Off 52 car transport wagons from Märklin (photo above right). In Japan, the Zetties will certainly be pleased to see typical motor vehicles (photo below) that are part of their everyday scene. Illustration top right: Mr ZtraX / Raffaele Picollo

Cruiser and the Mitsubishi Fuso small lorry. If you want to design your layout according to Far Eastern motifs and address Shinkansen traffic, for example, you can't go past this offer.

It is explicitly worth taking a look at the pages of MrZtraX and also at his current delivery listing (with product images), which contains many more models and also announcements for the near future. We obviously still have a lot to expect from this supplier!

The webpages of the three manufacturers:
<https://azar-models.com>
<http://www.mrztrax.com>
<http://www.trafofuchs.de>

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Layout details (part 8)

All Creatures great and small

A model railway layout should and must look alive, and well-placed figures play a decisive part in this. We have already dealt with this in Part 7 of our series on layout details and learned that it is not only the number of figures that matter, but also their careful selection and correct positioning. The same applies to animal figures.

What would life be like without animals? We encounter them every day, even in the concrete jungles of poorly planned cities, be it pigeons on church towers and in public squares, dogs on a leash, or feral cats.



Up close and personal with the animal world: If you want to depict wildlife, pets or domestic animals on the layout, you have to learn to observe and understand our animal contemporaries.

We will leave out even smaller creatures at this point, because one thing should already be clear: a model railway without animals is almost inconceivable. If we change from urban environments to the more frequently seen branch line themes, animal figures become even more relevant.



Two bright red macaws (top right) and a grey heron (left) cavort in this Cuban motif, while a worker fights off two alligators (centre) with an iron bar.

In the countryside, we encounter far more wild animals in the woods and meadows, but also farm animals that play a central role in the food supply. It is clear therefore that far more and different animal figures need to be included in scenes set in the countryside compared to a city theme.

Animals all behave very differently towards the observer. In any case, their size, their characteristics as herd or solitary animals and the colour of their fur or feathers are major features to take into account.

Many animals camouflage themselves and do not want to be discovered in their habitat, while others need to stand out from their conspecifics or wear markings with warning colours. Just think of the fire salamander, which we rarely spot, but it stands out when we do.

We can therefore use our figures skilfully and purposefully to draw attention to a scene or specific area of our choice. Similarly, we can also deliberately create hide-and-seek games that invite our layout viewers to take a closer look at detailed areas and entice them to go on a discovery tour.

The diversity of what is nowadays available on the market also allows us certain freedoms. For example, with the variety of available figures it has long been possible to include, if one wanted to, an entire zoo or wildlife park on a layout. Landscapes outside the European or North American continent with their fauna can also be depicted.

continued on page 32



Photo above:

A photo safari to the dried-up waterhole on the back of an elephant cow is a rather unusual theme. At the end of the expedition, we encounter African herd animals looking for water. The wild elephants are all cows whose herd is led by a lead cow. The bulls live as loners.

Photo below:

We may encounter the dromedary in a Z gauge zoo, but it is also suitable as a “ship of the desert” caravan animal in the Arabian desert, as show in this skilfully executed scene.

For example, elephants, dromedaries and even crocodiles are part of the existing product range. Some were perhaps initially intended as circus animals after the company Luetke Modellbahn added a circus tent and wagon to its range at an early stage, thus covering a very popular theme for eras III and IV. At the same time, however, they also opened up new scope for creativity.

Some Z scale enthusiasts also like to use these to misappropriate animals or to reproduce unpleasant everyday events. The raccoons from the Klingenhöfer miniatures range serve as an example here.

Who offers animal figures in 1:220 scale?

With a little skill, some animals can certainly be converted from existing figures. For example, the unicorn as a fantasy and fairy tale animal could certainly be derived from a horse.

A snake or slow worm is made from thin enamelled copper wire that is slightly thickened in the head area and recoloured if necessary.

Deer, sheep, horses and cows found their way into programme of large volume producers early on. Some of them had already been reissued several times and in different versions at Faller, Merten, Noch, or Preiser, but are currently not consistently available.

This is why small volume producers have been quite active in this area of the market. Here too, not all creations from the past are available from manufacturers. KoMi-Miniaturen and Rolfs Laedchen should be mentioned here, and Trafofuchs also has some animal figures in its range.

The most important supplier here is Klingenhöfer Miniaturen, which also produces animals that are not to be found in the domestic environment but belong into zoos or also serve foreign themes that are located in other climate zones.

Therefore, are not easy to hunt and eradicate. If to appear authentic on a European layout, they should, therefore, be placed discreetly.

Raccoons surely do not look out of place on North American scenes, but escaped zoo and farm animals have also settled in Germany and been able to move into sustainable habitats.

This eye-catching animal can therefore be used as a detail that is sure to attract the attention of many a spectator.

However, many people are not even familiar with this animal and not aware that it has been around in Europe for many decades. This is probably because raccoons are nocturnal, and,

Miniature birds

Other exotic birds on layouts are parrots. These colourful and eye-catching birds are perfectly at home in South Sea motifs with palm trees, but the macaw available at Rolfs Laedchen is actually at home in the South American rainforest. There they live in large flocks, with the birds deliberately keeping a greater distance from each other. Only pairs tolerate greater proximity to each other and then like to engage in grooming each other.



The ring-necked pheasant, which is also native to Germany (in two nominate forms), is still missing as a 1:220 scale figure. Photo: MakaraSK (CC-BY-SA)

A South Sea motif therefore tends to depict ornamental birds, which are also popular in our latitudes. Every now and then, some of these intelligent animals escape captivity and cannot find their way back in a completely strange environment.

In the city of Düsseldorf, a whole colony of collared parakeets has evolved from escaped captive birds. It's hard to not notice them as they have learnt to look for suitable food and to survive a German winter.

The event on which this is based can be used to create an interesting layout scene. Jürgen Tuschick has equipped the station of his latest layout Heinzenberg, which is located in Hesse, with a bright red macaw.

There is no doubt that this magnificent specimen is now missing somewhere and anyone who discovers the scene will instinctively look for the owner, who is perhaps following his escaped favourite bird with a bird cage.

Other very exotic birds can also help us to attract attention. The blue peacock, which originally comes from South Asia and belongs to the pheasant family, is a very popular bird. Roosters are particularly popular as park birds, but such a figure is not yet available on a scale of 1:220 – it is certainly not easy to realise either.

The situation is different with the pink flamingo, which originates from Africa. They are always found in large groups and usually stand at regular intervals in the shallow banks of standing water, often on just one leg.

Their bright feather colour, ranging from white to pale pink to light red, makes them stand out so clearly against the dark green of the water that they become a visual focal point. This can also be used deliberately, as they are certainly not out of place in a well-kept park.

But there is also a suitable market offer for native waterfowl species of which the grey heron and the white stork are the two largest representatives. The heron is solitary and can remain motionless in one position for a long time while it lies in wait for prey. It can be placed well in the planted margins of a standing body of water, where it is not immediately recognised, which also presents viewers with another challenge.



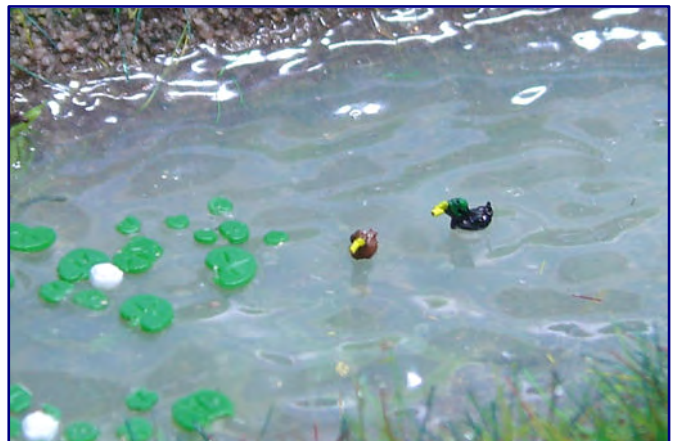
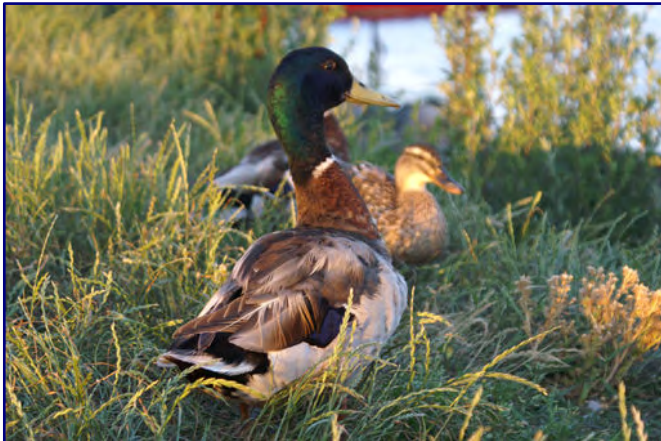
This grey heron has settled on the top of a gable in its typical perching posture. When approach by people this shy bird will fly away immediately.

Placing them on fields is also a good way to add some variety to an otherwise monotonous looking patch of land. As they do hunt for mice (and presumably hamsters too) in spring and summer, putting several specimens on a field at some distance from each other will make for a realistic looking scene.

The stork prefers wet meadows as well as riparian areas, but otherwise has a similar food spectrum, which includes frogs and other small vertebrates. However, it tends to roam its territory when it is looking for food. Stork nests are also popular on model railway layouts and can be put on top of chimneys and church towers where they are clearly visible.

As long as there are no eggs to incubate, these birds can always be found in pairs on the nest. Typical is the “clattering” of their beaks with their heads tucked back on their necks. Often there is not just one breeding pair in a village. Where storks settle, whole colonies can often be found.

This also applies to the mallard, our most common representative of this waterfowl family. Occasionally we find such specimens along a small stream, and they are often found in large numbers on the slow-flowing banks of rivers and on lakes and ponds. During the breeding season, pairs can often be clearly



Comparison between the real thing and the model: For both the mallard ducks (top row) and the white swans (bottom row) the correct posture as well as the distance between the animals should be taken into account when using them on a layout.

recognised within these larger groups because the distance between them is noticeably smaller than the rest.

With their snow-white feathers, the majestic mute swans immediately catch everyone's eye. They also rarely appear alone and usually travel in pairs. A few figures are sufficient here, otherwise viewers will soon no longer know where to look. They do not have to be placed in the water, but also look sufficiently appealing in the grass by the shore.

Forest and field animals

Forest birds hardly need to be modelled because they often live in hiding and are rarely visible. For example, owls sit motionless in trees during the day and are perfectly camouflaged due to their colouring.

However, it is certainly attractive to give their largest representative, the eagle owl, a place on the layout. It can be perfectly fixed on one of the highest branches of a fir or spruce tree. A cursory glance over the treetops will not spot it, just like reality. But its silhouette stands out recognisably from the branches of the needles when viewed from the right angle or when the light falls. And this is moment that you see it.



Owls can be easily identified by their typical physique and sitting posture, as shown by the male snowy owl (photo left), which incidentally belongs taxonomically to eagle owls. The eagle owl itself is only slightly larger and can be recognised, even at a scale of 1:220, by its typical feathered ears (photo right).

Suddenly the observer realises that there is life in the (model) forest after all. This also draws attention to other forest dwellers, which we will come to later. But first we would like to talk about pigeons, crows and seagulls.

Ring doves and turtle doves are also originally forest birds, but followed humans into the cities and discovered new habitats there that their natural predators did not dare to enter for a long time.

As scale models, they are small and grey and therefore hardly contrast with the road surface. However, when they are set up in sufficient numbers, they can no longer be overlooked, and are just as much a part of our lives.

In the fields, rooks and ravens do the same. They usually appear in large flocks, with their black colour making for a visual impact.

Only the buzzard-sized common raven is a solitary bird, which is no less conspicuous, but easier to reproduce due to its size.

Gulls are to the sea what crows are to inland areas. The largest native gull species, the herring gull, is available



The track barrier signal clearly shows how it is appreciated by herring gulls as a perch. Although mainly found in flocks on the coast, gulls are also a quite common sight inland.

as a 1:220 scale model. They are also frequently found far from the coast, so that they can also be used to populate a harvested field, even mixed with crows. One thing is certain: without these two bird species, rural scenes can hardly be depicted in a believable way.

Let's now switch from the coast to the high Alps. As mountains and tunnels are also very popular layout themes, the production of matching animal figures was only a matter of time. Chamois and ibex are typical mountain goat species that will breathe life into the rocky terrain.



Herring gulls in a salt marsh on the coast (photo above) and pigeons at the railway station (photo below) in the model show how important it is to also include relatively small animals on a layout for achieving a coherent picture

It should be noted that chamois live in herds of up to 30 animals, while an old male ibex is a solitary animal. The brown golden eagle may be circling above, clearly recognisable by its fingered wing tips and majestic wingspan of over two metres.

The brown bear can also move into a modern layout. Here, excitement could be added to the layout by recreating what we would not want to encounter in real life: The predator is attracted by a group of hikers having a sandwich break and being unaware of the danger sneaking up to them.



Alpine ibex (photo above) and brown bear (photo below) are used on layouts based on Alpine motifs, unless a zoo is the theme. Photo (below): Klingenhöfer Miniaturen

Squirrels look downright cute. And although they are sometimes on the ground, the slightest disturbance will send them up the nearest trunk. When used in meadows and fields or in the greenery of a settlement, there is no problem in making them recognisable to onlookers. After all, the reddish-brown fur provides some contrast to the darker wood colour.

When building a diorama a few years ago, we were amazed at how well camouflaged a fox is in the undergrowth. Even its fur, which is almost identical in colour, is far from inconspicuous. Nevertheless, it is hardly noticeable in the forest, especially as the forest floor is not covered in green throughout.



Spot the animal: The squirrel shown here is so skilfully placed in the tree that children are more likely to find it from their perspective than their parents. The ensuing discussions about such details will keep people glued for longer to a layout.

If this loner is placed in the layout, it should contrast sufficiently with its surroundings. This is best achieved by choosing green areas where the fibre length of grass blades is not too high.

The theme of stags is also likely to gain popularity now that Noch has ensured wide availability with the release of its 3D Master Figures range. Unfortunately, its injection moulded predecessors by Merten have not been available for over twenty years, meaning that for a long time this theme could only be covered by small volume production.

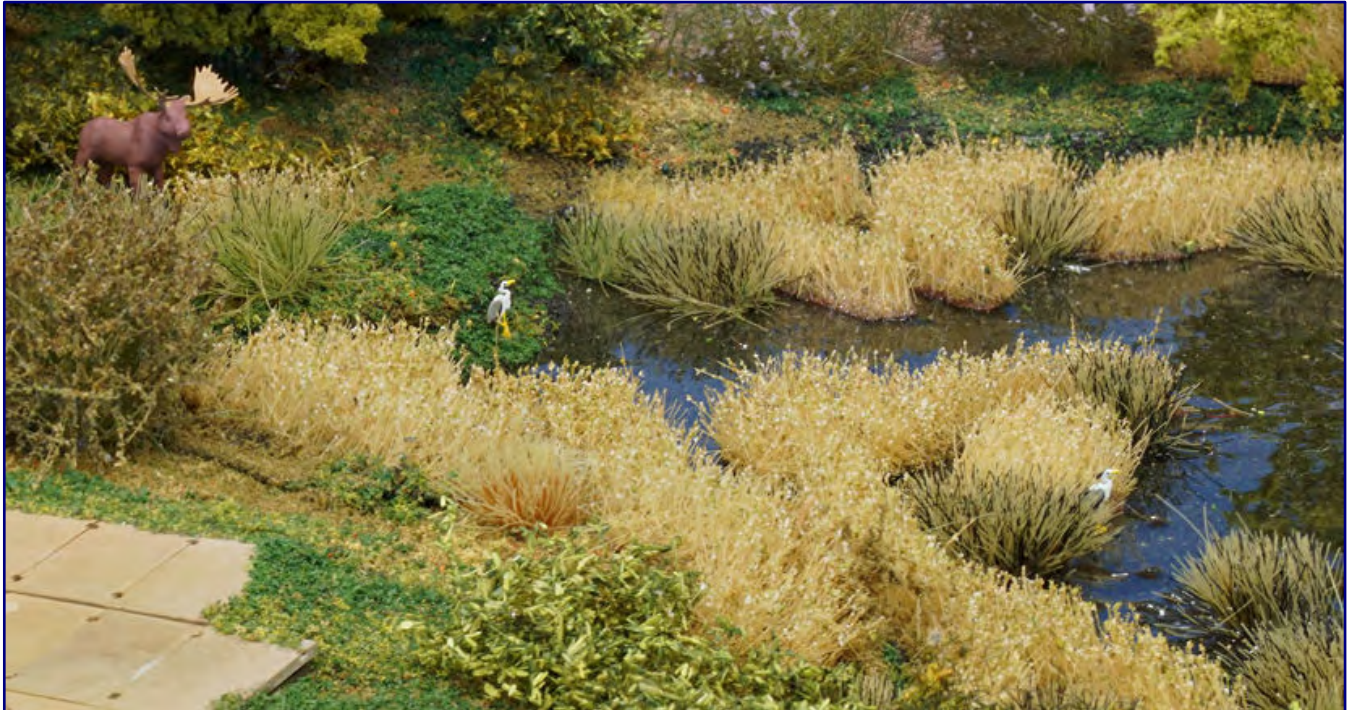
Roe deer, like stags, are herd animals. Where one animal is found, the others are usually not far away. This is especially true for the females with their fawns. They only venture out of the forest at dusk and can then be spotted in lighter areas, usually in tall grass, but also in fields.

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A proud fallow deer observes its surroundings (photo above). As these animals also like to spend time in open areas during the day, they are relatively easy to photograph. Red deer (photo below), which tend to favour the forest, were recently released by Noch. A comparison between a real stag and the scale model standing on the upper slope of the photo below, shows how lifelike the new figures have become.

Depending on the species, bull deer are solitary animals or live in smaller, all-male groups close to the company of females. This is particularly the case with fallow deer, which are ideal for model railway purposes, because they rarely seek the protection of the forest and are also more likely to be found in open pastureland during the day.



Here, we see an elk and a grey heron in their natural habitat, on a scale of 1:220. It is well worth giving such a biotope sufficient space on a layout.

In contrast, moose, which are the largest deer species in the world and are native to Scandinavia and North America, are always solitary animals. We must not forget wild boar, which live in packs in the forest. Fortunately, the wild form of our domestic pigs tends to avoid humans, as these animals can be very dangerous with their powerful tusks.

Cattle and “miners' cows”

With the focus on branch lines and agriculture, the classic domestic animals have been a focal point of the Z scale figures ranges since its inception. Cow and horse figures have always been available, together with sheep.

We certainly don't need to give hints for modelling stables, because here people determine the space and distance between the individual animals. The same applies to the use of carriage and riding horses.

But it gets interesting when we look at the pastures, because there the animals are left to their own devices. Each species has its own typical behaviour, which should also be recognisable in the model. If this is not done correctly, people will immediately notice that something is not quite right, perhaps without even knowing for what reason exactly.

While the intelligent domestic pigs seem to have a life of their own and sometimes lie next to each other or wallow individually, almost all other domestic animals (meaning farm animals as opposed to pets) are classic flight animals. If they sense danger, they take flight.

The only difference is whether they tend to do this alone because they are sufficiently fast or seek the protection of the herd, which makes it more difficult for a predator to recognise a single animal as prey and attack it.

Sheep often stand closely together in larger groups, but with groups within the overall herd maintaining some distance from each other. In the event of danger, the animals seek close proximity and try to move away. Around the perceived danger point, often the sheepdog leading the flock when moving, a conspicuously large distance can be seen radially – a sight that is particularly impressive when seen from above.



From a bird's-eye view (photo above) it is easy to see how sheep are spread out in the pasture: They tend to gather in larger groups, but with some distance between them. Harald Hieber has successfully realised the typical movement of a guided flock of sheep on his layout (photo below). The location of the sheepdog can be recognised immediately from the direction of movement.

A common modelling mistake is to use far too few sheep figures, which is due to the usual packaging sizes containing only a few specimens. Preiser has reacted to this in its 2024 new products programme and announced a set of 60 sheep – without the dog and shepherd. It remains to be seen how many years will pass before this set will actually become available.



Horses as flight animals keep a greater distance from each other in the pasture (photo above). Noch (photo below) currently has the best models of these animals on the market and their 3D printed miniatures could hardly look more lifelike.

Until then, we note that a flock of these wool donors needs to be sufficiently large, and that their direction of movement and flight must be clearly recognisable from the observer's perspective, in order to make an authentic looking scene. This is completely different with warm-blooded horses, which can reach a high running speed. Their behaviour on the pasture clearly shows a hierarchy, at the top of which sits the lead mare.

The distances between the horses are usually quite large, making it much harder to recognise groups. This ensures that each specimen does not get in the way of its neighbours with its need to feed on grasses and meadow herbs.

They are less likely to flee because, unlike sheep, they do not have to fear attacks from other animals. Lambs in particular are at risk from attacks by crows and birds of prey, which also explains the fearful and cautious behaviour of sheep.

The donkey, once a very popular carrier of loads, also has a pronounced sense of danger. Cheaper than a horse, but strong and persistent, it still has a reputation for being particularly stubborn. In fact, this behavioural pattern manifests itself when the intelligent animal has identified a danger and wants to avoid it.

A single one of these grey animals is typically enough on the layout, as today they are kept almost exclusively by enthusiasts, often alongside ponies. The donkey's role as a source of meat is hardly relevant any more, but has not completely disappeared. As with horses, however, they are only rarely used for food production.



Nowadays, the intelligent, but stubborn, donkey is usually kept as a single pet or together with ponies.

Cows are also a special case. We usually encounter smaller herds in the pasture, as the large farms regularly restrict themselves to keeping the animals indoors. In addition, only females with their calves and young bulls that are not sexually mature are allowed outside.

While a calf is always close to its nursing mother, the cows are otherwise spread out over large parts of the pasture. During feeding breaks or in the evening, however, they are also closer together unless they are led back into the barn overnight.



Cows are very curious animals (photo above) and, as grazing animals, show no fear of humans if they are used to their presence. The best Z scale models to date have been from Faller and Preiser (photo below).

Close contact with conspecifics is negligible in daylight, because the cow, which appears so sedate, is by no means slow when it has to flee or decides to attack in defence of its offspring.

With regard to passers-by lingering at the pasture fence, it should be noted that cows that are used to the sight of humans do not fear them at all. They are gripped by curiosity and will even seek to get close to them.

For the model, this means a differentiated approach: For eras III and IV, when agriculture was not yet so industrialised and grazing animals were still the norm, the miniatures can also be seen to be familiar with humans. If we switch to the present day, many cows keep a recognisable distance because encounters with humans are a new experience for them.

At the end of this section, we come to another domestic animal that is one of the most intelligent in the human environment: the domestic goat. When many households were still considered self-sufficient, even away from farms, it was a valued pet.

It gave milk and meat, but cost considerably less to buy and maintain than a cow. Alongside pigeons, chickens and rabbits, it was the typical animal kept in the small stables behind the houses in miners' settlements. It is not surprising, therefore, that it was colloquially referred to as the "miner's cow" in the Ruhr region.



A clever creature: the "miner's cow" (photo above) is one of the most intelligent domesticated animals! Klingenhöfer Miniaturen offers authentic looking models of them (photo below).

Today we mostly know them as petting animals from zoos, where they are bred to provide meat for predators. Only rarely are they kept without a specific purpose, where sufficient meadow areas are part of the property.

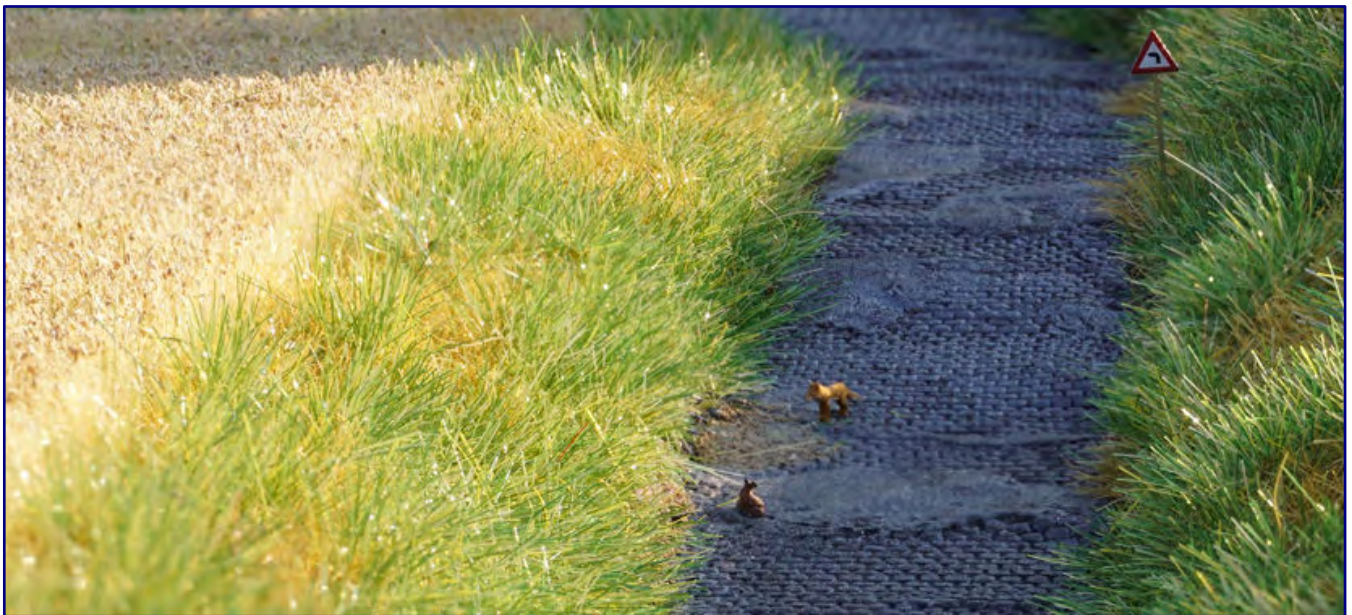
On the model railway, they can be used alone or in smaller groups, depending on the era. When grazing, they spread out over a larger area, like to climb on whatever they get the chance to and immediately come running to crowd at the fence as soon as a person approaches.

Small animals and exotic species

Rabbits and chickens have just been mentioned. Kept indoors, they will hardly catch the eye of the observer. A built-up outdoor enclosure next to the house, in which some dwarf rabbits hop about and attract attention with their different fur patterns, certainly add interest to the environment on a scale of 1:220.

The model railroader should know that rabbits are not solitary animals but live in packs and exhibit pronounced social behaviour. The individual "hutch rabbit" from earlier times, which also belonged to a higher weight class and was only kept for slaughter, should not be used as a model here.

Rabbit figures are unlikely to stand out in the open. Wild animals tend to be inconspicuously marked so that they are not noticed close to the ground. As martens, foxes and birds of prey are among their predators, they always try to avoid detection from a distance or from above.



Fox and hare say goodnight to each other makes for an idyllic scene on the layout. In real life, however, such an encounter is likely to play out differently.

The larger brown hare is somewhat different, recognisable by its significantly longer hind legs and spoon. They live in fields and are solitary animals. Bucks can be seen fighting over females in the spring, and after the harvest they are easier for us to see as they sprint quickly across the field.



The camera is rarely at hand when a hare runs past you so calmly and seemingly fearlessly.

A figure positioned in this way might be noticed only at second sight, but then arouses curiosity and evokes a familiar experience. The same applies to the male ring-necked pheasant, which is also striking and beautiful to look at. It is not yet available in Z scale, but therefore offers an opportunity for a new model for small volume production.

Another noteworthy species are the snow-white domestic geese, which were a typical sight on Era III village ponds or backyards or even the village street, a time when motorised private transport was rare enough to pose a real danger to them and vice versa.



The domestic goose is a useful modelling theme due to its sufficient size and striking white plumage. However, most animals are only granted a very short life.

Today, domestic geese are often kept in flocks in free-range farms. They appear there as young birds in spring, grow up over the summer and end their lives early as St. Martin's or Christmas geese. During these nine months, however, we can also enjoy the sight of them on the model railway.



No farm scene is complete without geese and chickens! Even if at least the geese are quite hidden here, they quickly draw the attention of the observer.

The lake frog that we mention here is unusual. Produced to scale by Trafofuchs for one of our projects from summer 2022, it is at best perceived as a speck of dust at first glance. And yet it has what it takes to be an eye-catcher on a pond. The frog amphibians like enjoy the sun on a water lily leaf.

They can also be used as models in exactly the same way. Depending on the size of the body of water, there may only be a few specimens or an entire frog concert may be organised. The observer will notice them in rising or setting sunshine because they then stand out from the flat surface of the water and floating leaves.

But this is precisely the moment that captivates our viewers and inspires them to search for interesting details. Once we reach that point, people are sure to spend more time at our layout. The decisive factor here is always the mixture of quickly discovered animal scenes and casually guiding of the eye to more extraordinary hidden details.

We would like to end this article with a trip to the German North Sea coast. Seagulls, which are plentiful and impossible to miss on the coast, were already a topic at the beginning. But there are also two other animal species that are an integral part of the Wadden Sea fauna and are favourite among so many holidaymakers: Harbour seals and grey seals.

While do not know of any grey seal figures, harbour seal miniatures are available as unicoloured grey specimens. Recoloured in brown, they can also serve as American sea lions, because most animals are similar in size and obvious differences do not matter at a scale this small.

continued on page 50



Did he realise that he was going to get a 1:220 scale mini-me? Wide awake, but apparently patient, the pond frog (photo above) observes the photographer. As a result, two specimens, a large female and a smaller male, can be seen on the diorama (photo below) surrounded by water lilies.

The different ways of moving on land should also not play a role in the model, as the animals are depicted lying down. Adult harbour seals always keep a fixed distance of at least a body's width from others. On sandbanks, however, they like to line up at a fixed parallel distance in order to rest.

If this theme is taken up for a coastal layout, there is one particular detail that matters: A distinguishing feature of harbour seals is the black spots on their back. And similar to a zebra, each animal has its own unique pattern which should be taken into account when painting your harbour seal figures.



On a sandbank somewhere in the North Frisian Wadden Sea: a group of harbour seals resting after hunting, while two specimens compete for the best spot in the sun.

We very much hope that this last part of our 2023 annual focus topic series on layout details has provided you with a lot of interesting information and will inspire you to add those little but easily overlooked things to your layout.

However, it is precisely these details that are so often forgotten or incorrectly executed during layout construction. If the latter happens, it might disrupt the overall coherence of a layout, albeit without one quite being able to recognise the problem.

If we can contribute even just a little to avoid such mistakes in the future and to help the average modelers to improve the authenticity of their layout, then we will all have achieved a lot! As a supplementary contribution, we are also planning an episode on layout details for **Trainini TV**.

This will pick up on suitable real-life situations, capture them on video and primarily take a bird's-eye perspective, which should allow us to take a critical look at our layout and ask ourselves what we can do to improve it. But this we should know by now.

Suppliers of animal figures (Z scale):
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Note for English readers: The literature section that follows is not translated into English because the original texts of the books involved are in the German language. The original German is left here for information purposes only.

Durchs Watt und auf die Inseln **Kleinbahnen der Nordsee**

Es ist wieder Urlaubszeit und wir haben für Sie einen dazu passenden Titel gefunden. Schauen wir auf Eigenbauprojekte, die in unserem Magazin vorgestellt wurden, sowie kommerzielle Angebote, finden wir auch einen engen Bezug zu unserer Baugröße. Warum nicht ein bisschen in der Sonne träumen und das Urlaubsgefühl dann mit in den Modellbahnkeller nehmen?

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Das vorliegende Buch erschien bereits 2012 mit identischer Buchdeckelabbildung, aber einer abweichenden ISBN. Das schicken wir unserer Besprechung voraus, um Irritationen bei unseren Lesern zu vermeiden, um das Abgrenzen der beiden äußerlich gleich wirkenden Bücher zu erleichtern.



Vermutlich ging es dem Verlag auch nur um die Neuauflage in aktualisierter Form, doch Autor Reiner Preuß, der auch für viele weitere Titel verantwortlich zeichnete, starb bereits 2015. So stand der ausgewiesene Schmalspurkenner für diese Aufgabe nicht mehr zur Verfügung.

Auf den aktuellen Stand zu bringen war überwiegend das verwendete Bildmaterial, denn erforderliche Modernisierungen der Kleinbahnen, die hier behandelt werden, sind bereits vor der Jahrtausendwende aus Regionalisierungsmitteln erfolgt. Die verbliebenen und bis heute betriebenen Inselbahnen sind also bis heute auf einem zeitgemäßen Stand.

Damit haben wir den wesentlichen Unterschied zur ursprünglichen Auflage bereits herausgearbeitet: Das Bildmaterial wurde grundlegend geprüft und mit solchen Aufnahmen, die erst nach 2012 entstanden sind, grundlegend aktualisiert. Die textlichen Inhalte sind deutlich weniger betroffen, aber ebenso nicht einfach blind übernommen worden.

Das wird besonders an jüngeren Ereignissen deutlich, die kurz vor und zum Teil auch erst nun nach der Corona-Zeit stattfanden: Die Inselbahn von Borkum und der „Molli“ in Bad Doberan (Ostsee), die beide auf der seltenen Spurweite von 900 mm fahren, hatten einen temporären Fahrzeugtausch vereinbart. Damit sollte der Tourismus in beiden Regionen gefördert werden.

2019 reiste deshalb eine der Dampflokomotiven auf die Nordseeinsel, um dort ein Technikdenkmal vorzuführen, das dort sonst kaum eine Rolle spielt. Erst im letzten Jahr war der Gegenbesuch eines Wismarer Schienenbusses an der mecklenburgischen Küste möglich.

Und damit sind wir auch schon mitten im Thema: Inselbahnen waren einst eine Lebensader für die Eilandbewohner in Ost- und Nordfriesland. Heute haben sie diese Bedeutung weitgehend verloren, die verbliebenen Bahnen sind viel mehr eine Touristenattraktion.

Das vorliegende Werk widmet sich der Geschichte und Gegenwart der Inselbahnen zwischen Borkum und Sylt. Eingeschlossen sind Lorenbahnen zu den Halligen und auch der Personen- und Autotransport über den Hindenburgdamm nach Westerland.

Was vielleicht nicht gleich jedem Interessenten präsent sein mag, aber wichtig zu wissen ist: Der Buchtitel verweist darauf, dass hier nur diejenigen Bahnen behandelt werden, die an der Nordseeküste zu finden sind. Mangels Gezeiten gibt es in der Ostsee kein Wattenmeer, entsprechend fehlen die Küsten- und Inselbahnen von Bad Doberan bis Stralsund und Rügen.

Dafür beschränken sich die Inhalte nicht auf öffentliche Strecken in schmaler Spur: Thematisiert werden, soweit ausreichend Informationen vorlagen, auch militärische Bahnen und eben die Normalspurverbindung auf die Insel Sylt.

Die meisten Inselbahnen sind längst Vergangenheit und wecken folglich eher Erinnerungen oder sprechen nostalgisches Empfinden an. Sehr interessant ist immer auch der geschichtliche Aspekt, der im Rückblick abgearbeitet wird und erklärt, warum die Bahnen überhaupt gebaut wurden.

Deutlich wird so auch, mit welchen Problemen die Insulaner angesichts von Versandung und Sturmfluten zu kämpfen hatten oder welche große Bedeutung einer Kleinbahn für das Entwickeln eines erfolgreichen Tourismus zukam.

Bedroht waren sie von der zunehmenden Individualmotorisierung, sofern auf den Inseln erlaubt, wie auch vom Bau neuer und geschützter Häfen oder Anleger näher am Hauptort der Inseln. Im Bereich der Halligen blieben die Feldbahnen hingegen unentbehrlich, weil sie dort vor allem dem Küstenschutz dienen, der eine staatliche Aufgabe ist.

Dort sind diese Bahnen kein Mittel für den Personentransport, sehen wir von Sonderrechten für die Bewohner mal ab, die mit Privatvehikeln auf Schienen aufs Festland (und zurück) gelangen. Spürbar wird nun hoffentlich, welche Spannung in diesem sonst kaum beachteten Thema stecken kann.

Das zum Erhalt der Urlaubsgefühle nach dem Sommerurlaub erforderliche Umsetzen von Impressionen ins Modell, sollte Anlass und kein Hindernis sein. Schließlich gibt es auch im Maßstab 1:220 zumindest gebrauchte verfügbare Modelle für Feldbahnen. Und die typischen Inselbahnen sollten im Eigenbau auf Basis von Spur-T-Material auch möglich sein – in **Trainini®** wurde schließlich schon gezeigt, wie es funktioniert!

Lassen Sie sich inspirieren von aktuellen und historischen Aufnahmen und wertvollen wie fachkundigen Beschreibungen der verbliebenen und verbliebenen Bahnen, die sich bis heute ihren Charme bewahrt haben.

Beispielsweise auf Langeoog können sie heute noch zwischen Fähranleger und Inselhauptort mit der Bahn pendeln – und das sogar rollstuhlgerecht. Dabei weht eine frische Nordseebrise durch die Nase, die vielleicht auch Erinnerungen an so populäre Bahnen wie die Sylter Verkehrsgesellschaft mit Wismarer Schienenbus und Borgward-Leichttriebwagen weckt. Leidenschaft geweckt?

Publishing pages:
<https://www.motorbuch-versand.de>

Dampf-Erinnerungen **Bitte einsteigen!**

Bereits fünf Mal zuvor hat Ton Pruisen bewiesen, was er altem Filmmaterial – 50 Jahre und älter – technisch zu entlocken weiß. Aber auch die Auswahl der verarbeiteten Sequenzen hat uns schon das eine oder andere Mal verblüfft. Alles schon mal dagewesen? Keinesfalls, wie Sie auch hier im sechsten Teil der DB-Reihe schnell feststellen werden. Unsere Reise geht los!

Ton Pruisen
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Dampf an Rhein und Mosel

Nord Süd Express GmbH
Hattenhofen 2023

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Ende 2023 erschien diese Folge 6 der DB-Filmreihe von Ton Pruisen. Dieses Mal nimmt der begeisterte Dampflokker seine Zuschauer mit auf eine Reise, die in Köln beginnt, den Rhein flussaufwärts führt und dann dem Moselverlauf folgt.

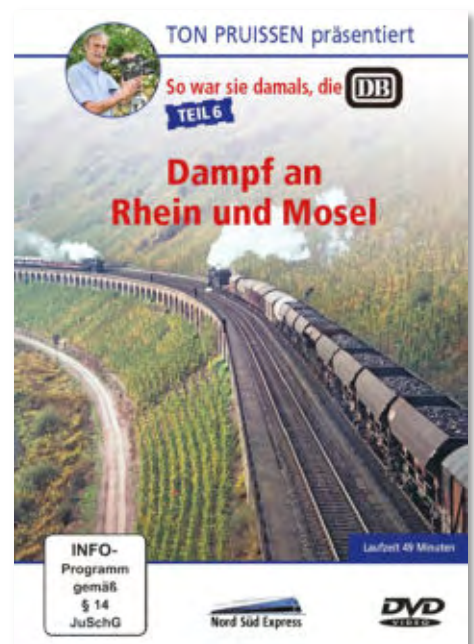
Spannende Filmmomente sind dabei wieder garantiert, denn ausgewertet wurde nicht nur eigenes Material, sondern, konsequent den vorherigen Teilen folgend, auch das von befreundeten Filmern, die auch mal in den Sequenzen auftauchen, die bei gemeinsamen Ausflügen entstanden sind. Geschickt eingebaut wurden auch Fotografien aus passender Zeit von den behandelten Orten, die dank der Zoom-Funktion lebendig werden.

Zeitlich reicht die zeitliche Spanne des Gezeigten von der ersten Hälfte der fünfziger Jahre bis in den Anfang der Siebziger. Gerade die ältesten Aufnahmen sind wohl unwiederbringlich, auch wenn unter ihnen auch diejenigen sind, die qualitativ etwas aus der Reihe fallen.

Verblüffend ist in der Gesamtsicht aber wieder einmal, wie sehr sich der hohe Digitalisierungs- und Nachbearbeitungsaufwand bezahlt gemacht hat. Immerhin sind auch die jüngsten Aufnahmen schon über fünfzig Jahre alt, doch wirklich anzusehen ist es ihnen allenfalls am überwiegenden Schwarz-Weiß-Teil.

In dieser Qualität haben wir zuvor noch nie historische Eisenbahnfilme vorgelegt bekommen und hier bestechen sie jetzt auch noch mit einem größeren Anteil an Farbaufnahmen, als es sonst üblich ist. Alle wurden sie in einem einzigartigen wie aufwändigen Prozess mit modernster Technik gescannt, digital bearbeitet und gereinigt sowie professionell vertont.

Explizit hervorzuheben ist auch, dass hier nicht altbekannte Dokumente wiederaufgelegt wurden, sondern Unbekanntes aus den privaten Archiven geholt wurde. Das macht den gesamten Film gleich doppelt wertvoll.



Widmen wir uns im Überblick nun dem Inhalt der filmischen Reise: Der holländische Eisenbahnfilmer fährt mit uns vom Rhein an die Mosel, schaut dabei unterwegs aber auch nach rechts und links. So sind auch die Hohenzollernbrücke in Köln, das rechtsrheinische Köln-Deutz mit dem Bw Deutzerfeld und die Rheinuferbahn der Köln-Bonner-Eisenbahn wichtige Filmthemen. Es folgt das Mittelrheintal, wo auch die damals noch alltägliche Dampfschiffahrt festgehalten wurde.

Unsere Fahrt setzt sich fort über Koblenz Hbf, wo wir den Laufweg ändern und auf die Moselstrecke einbiegen. Zuvor gibt es aber noch einen Ausflug auf die Lahntalbahn und nach Dillenburg sowie nach Mayen. Auch hier konnten tolle Schmankerl festgehalten werden.

Auf dem weiteren Weg nach Trier Hbf, wo unsere Reise enden wird, erleben wir einen weiteren Schwerpunkt der Dampftraktion in den sechziger und siebziger Jahren. Den erleben wir auch im Bw Ehrang, einer damals wichtigen Betriebsstätte.

Ein Erlebnis ist auch der Abschnitt der Moselstrecke, auf dem wir nacheinander den Kaiser-Wilhelm-Tunnel, die Doppelstockbrücke Bullay, den Prinzenkopftunnel und das Hangviadukt Pünderich passieren. Lesern, die das zweijährige Adventstreffen in Zell (Mosel) besuchen, sollte dabei warm ums Herz werden.

Gewiss machen nicht nur die durchfahrenen Landschaften, die wir unter anderem aus dem Führerstand einer Baureihe 44 genießen, den Wert dieser DVD aus. Dampflokomotiven der Baureihen 01, 03, 03¹⁰, 18³, 23, 38, 42⁹⁰, 44, 50, 86 und 94 erfreuen uns. Besonders die Neubaulok der Baureihe 23 ist zahlreich in diesem Film anzutreffen.

Die Baureihe 03¹⁰ der DB mit Neubaukessel wird uns in Köln Hbf schon in ihren letzten Dienstjahren begegnet sein. Sie macht sich auf den Weg in Richtung Hagen, wo sie in jenen Jahren vollständig beheimatet war. Bewegte Aufnahmen von ihr sind selten.

Noch schwieriger dürfte es sein, solche Dokumente von der Franco-Crosti-42⁹⁰ zu finden. Ton Pruisen hat beide Exemplare dokumentieren können, wobei wir in einem Fall ein größeres Zugeständnis an die Filmqualität machen müssen – deshalb darauf verzichten, wollten wir gewiss nicht! Und die letzte Sequenz mit einer solchen Lok ist auch phantastisch anzuschauen und entschädigt das sogar.

Gleichermaßen besonders sind Aufnahmen des ausfahrenden Rheingold-Express, bespannt mit der Baureihe 01. Der Zug verlässt Koblenz Hbf im Jahre 1956 und erinnert uns Modellbahner an eine frühere Zugpackung von Märklin. An Stellen wie diesen wird der große Nutzen für die Modellbahn deutlich, der sich beispielsweise auch an den Beladungen der vielen Güterzüge zeigt.

Teilweise auch beim Bekohlen und Wasserrasseln erleben wir König Dampf. Besonders hervorheben müssen wir 001 150-2, die wir im Zugdienst erleben: Sie war ab 1935 bei allen großen Eisenbahnjubiläen dabei und blieb bis heute erhalten. 041 334-4 kommt die Rolle der letzten Lok ihrer Baureihe mit Altbaukessel zu.

Doch nicht nur Dampflokomotiven sind hier zu sehen: Willkommene Ausnahmen sind der ET 85 in Koblenz, ein roter Brummer VT 95⁹ und ein betagter ETA 177 im Alltagsdienst. Einen Rahmen für das letzte Filmdrittel spannt eine DGEG-Sonderfahrt mit 18 316 aus Minden, einer ehemaligen badischen IVh. Sie taucht an verschiedenen Unterwegshalten mehrfach auf und wäre für sich schon einen Kauf wert.

Publishing pages:
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25 Years of Modellbahn-Union Let's have a Party today

In 2019, the Modellbahn-Union celebrated its first summer festival in Kamen, Westphalia. This was supposed to be the start of a biennial in-house exhibition, but the pandemic forced a cancellation. Fortunately, this did not happen in June 2024, as the occasion was also the 25th anniversary of the distributor. We were there.

The current branch in Gutenbergstraße, just a short walk from Kamen railway station on the Dortmund - Hamm (Westf.) line, opened in 2019. Its predecessors were shops in the pedestrian zones of Kamen and later Dortmund.

However, after the merger with N-gauge retailer DM-Toys at the latest, these shops would probably have been bursting at the seams, as the nationwide mail-order business required more and more storage space. And this seemed to have been found in the industrial zone close to the city centre. The company celebrated the successful move with a summer festival, which was also attended by several manufacturers.



Our magazine also took part in this year's summer festival, at which the Modellbahn-Union celebrated its 25th anniversary, with a children's handicraft programme, exhibits and model building demonstrations. Editor-in-chief Holger Späing, Kristin Späing, Hendrik Späing and editor Ralf Junius (from left to right) looked after the interested visitors.

This was supposed to be a prelude to inviting customers from near and far to the south of the small Westphalian town every two years. However, the coronavirus pandemic caused an unplanned



The actual exhibition area was formed by a tent city, while the shop took centre stage and it also invited customers to make purchases.

cancellation of the second edition. The event was only able to resume in summer 2022, but heavy rainfall dampened the mood here, too.

On 28 and 29 of June 2024, everything finally fell into place: many manufacturers had responded to the call to Kamen, the Modellbahn-Union had a lot to offer and **Trainini®** also expressed its thanks for the good cooperation with a children's craft station and model building demonstrations.

The Summer Festival 2024 thus developed into a meeting place and venue for good technical discussions and exchanges between model railway fans. We were also able to meet and get to know some readers over the two days, who were happy to give us feedback and suggestions. This is also the occasion to look back on the event in more detail here.

Although the focus was not on Z gauge, it was worth a visit because manufacturers and products relevant to our scale were also represented. It was also worthwhile to think outside the box and return home with new ideas.

Visitors were able to pick up suitable inspirations from the very first stand they passed: Markus Schiavo, a well-known and much appreciated "niner," had set up here. On a scale of 1:160, he presented his "Cinema City" layout, which was modelled on an American theme.

It keeps its viewers busy for almost hours, as they continuously discover new scenes from famous Hollywood movies, children's and adult series, comic books, and the world of rock and pop. At no point is it kitschy, because all the figures are so well integrated that they don't impose themselves on the viewer but are there to be discovered.

continued on page 59



Well-known comic and film stars came together in Kamen, at least on a scale of 1:160 on Markus Schiavo's "Cinema City" layout.

As the festival was also well supported by the manufacturers, we are pleased to take a brief look at those of them who are also active in or for Z gauge. The market leader Märklin, who had travelled to the event with a small H0-gauge show layout, has to be mentioned.



Märklin presented a sample of its pre-series locomotive of the V 200⁰ series, which clearly differs from the series machines in some details: Compressed air whistle in front of the driver's cab instead of a macrophone (train air horn) behind a cover, round head and taillights at the bottom and a different roof equipment. Such a model would also be extremely interesting in Z gauge.

The sales representatives did not have anything in 1:220 scale in their exhibits, but the hand sample of a pre-series locomotive of the V 200⁰ series certainly sparked desire for our nominal size. Fuller as much as Kibri / Viessmann / Vollmer reminded us with their presence of the suitable range of products, which could also be purchased on site in the shop.

Conversation and demonstrations

It was worth visiting the Weinert-Modellbau booth, as many of the products on display there are also available for Z scale. This led to a dialogue about the filigree Bundespost telephone boxes that Rolf Weinert himself had built as test pieces.

This was always the case, he explained, as no products were sent to customers, if he had not first convinced himself of their function and utility value in this way. Incidentally, the telephone boxes were soon delivered.

The visit to the RTS booth was also very promising for us. In addition to high-performance turfing ("RTS-Greenkeeper"), the manufacturer's programme also includes design materials and seminars. Grass fibres with a length of only 1.0 mm appear to be very helpful for our scale when short-cut meadows are to be reproduced in the gardens of the miniature figures.

To apply needles to fir and spruce trees, the lengths on offer are even shorter: the range also includes fibres just 0.5 mm long, which naturally raised the question (still to be clarified) as to whether this could be an even better alternative for an English lawn.

After training are completed, RTS seminars are now also held in Kamen. Participants learn about working techniques and use RTS products and tools. We were invited to attend such a seminar in October and will report on it to our readers.



RTS presented its product range and gave demonstrations with the Greenkeeper (picture above), behind it you can see the Faller representation. The brochures and books by Stefan Carstens were also on sale (picture below), with Joachim Reinhard from Wagenwerk providing expert advice. At the back left you can see a preprint of "Güterwagen Band 10."

The company "Wagenwerk" was also represented by Joachim Reinhard as a replacement for Stefan Carstens, who was sick. Located close to our editorial office, it was almost a matter of course to have a nice chat with him.

On display at the booth was an advance print sample of “Güterwagen Band 10”, which author Stefan Carstens has announced for autumn 2024. The topic will be the German low-loader wagon types, a very exciting and still underrepresented group of wagons in Z nominal scale. We will present this book in detail once it is published, but this much can already be revealed, the content will absolutely appeal to our readers!

The call to Kamen was not only answered by Modellbahn-Union customers from the local area. Many wanted to take advantage of the opportunity to obtain information on site, exchange expertise and then make their purchases in person. As a result, long queues formed in front of the shop throughout both days, which is why those waiting in the high summer temperatures were also provided with mineral water free of charge.



Our booth neighbour was the rail transport company National Express. In addition to providing information on job and career paths in this company, the booth staff also sweetened the day for visitors with free popcorn, biscuits and natural energy drinks.

Outside, there were additional booth areas where a raffle, special offers and in-house products were presented. We would just like to remind you that there are also many exclusive items for Z scale that are produced using light-cutting technology or 3D printers. They can be used to design landscapes or buildings.

It may actually be possible to complete rolling stock very soon, but we won't reveal any details at this point. Instead, we refer you to our interview with Head of Marketing Marcel Hagemann in episode 29 of **Trainini TV**.

Incidentally, the big role model was also represented at the summer festival: National Express, a company whose RRX train services are provided virtually on the doorstep, was present. Visitors who travelled by train should have already made the acquaintance of National Express on the way.

The matching models of the Desiro HC multiple unit from Siemens were presented by the Modellbahn-Union at one of its own booths. However, the German branch of the railway company from the United Kingdom had come here to promote job profiles on the railway and, in particular, its company.



Modellbahn-Union itself presented a fully painted and printed sample of its own H0-gauge novelty, the Siemens Desiro HC. The prototype of this RRX model train (Rhine-Ruhr Express) is also in service with National Express and in this case runs on the RE 6 line to Cologne / Bonn Airport.

The good wage conditions, which were actively supported rather than boycotted, were emphasised to attract and retain skilled workers. The talks were sweetened with popcorn, biscuits, and a ginger energy drink. Here, too, it became apparent to us that a look inside the company with a subsequent report could be quite useful.

In dialogue with visitors

Our own booth was also intended as a thank you to the organiser for his quick and generous help last year, when we were virtually overrun by children at the EHEH's All Saints' Day exhibition in Erkrath-Hochdahl and ran out of craft materials after the first of three days of the event.

We therefore wanted to offer the popular coaster-making activity here as well. As this was also actively highlighted, the response was particularly good on the Saturday. Many children took advantage of the free offer at the **Trainini®** booth to create a path on the surface of such a paper coaster, to grass over the surrounding area and to design it with their own selection of Modellbahn-Union and Noch products.

Right next to our booth, children could also colour in templates or create a badge ("button"). During the breaks when the glue had to dry, Ralf Junius invited the children to steer a train (rail bus with mouse motifs) on his regulars' table photo diorama and to trigger various noises during the journey.

This demonstration piece was decorated with Modellbahn-Union's half-relief houses made of hard cardboard, creating a close link to the organiser and its product range. On a second club layout, Ralf promoted an idea that goes back to Thomas Buchner and presented various options for how they can be individually filled and enhanced with content.



At **Trainini®**, children were guided through the process of designing a coater themselves. Many creative ideas were born in the smallest of spaces over the two days of the event.

But as expected, adults made up most of the visitors. That's why we had planned early on to do some modelling demonstrations and to push ahead with our riverside diorama.

We received many questions from visitors about the impressively realistic-looking water on the unfinished presentation and photography display. Holger Späing patiently explained the transfer from reality, for example in the choice of colours, the basis of the water surface, the creation of the water layers and the use of an illusion technique.

The new 3D printed figures from Noch, other design materials from this manufacturer, working with the Gras-Master 3.0 Profi and creating flower areas with products from Woodland Scenics were also demonstrated.

As a result, we were always sure to attract a great deal of interest. In addition to praise for our magazine, for which we would like to take this opportunity to express our sincere thanks, the many dialogues in both directions were also very helpful. In this way, we also learn about the wishes of our readers so that we can incorporate them into future annual planning.



On Ralf Junius' club photo diorama, the children were allowed to operate the rail bus themselves, past the backdrop of half-relief houses from the Modellbau-Union.

In two years' time, when the next edition of the summer festival is due, we will certainly be there again, because we definitely enjoyed it and the dialogue with the other exhibitors that followed on both evenings was also very valuable to us.



Webpages and offer of the organiser:

<https://www.modellbahnunion.com>

Exhibitors with Z gauge relevance:

<https://www.faller.de>

<https://www.maerklin.de>

<https://www.noch.de>

<https://www.preiserfiguren.de>

<https://www.rts-greenkeeper.de>

<https://www.stefancarstens.de>

<https://viessmann-modell.com>

<https://weinert-modellbau.de>

<https://woodlandscenics.woodlandscenics.com>

Trainini TV Episode 29:

<https://www.youtube.com/TraininiTV>

märklin



Tag der offenen Tür

Einlass ins Werk von 9.00 - 15.00 Uhr.
Buspendelverkehr zwischen 8.30 - 18.30 Uhr
von Parkplatz EWS-Arena.

13. und 14. September 2024 in Göppingen

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Märklin öffnet in Göppingen die Türen zur gläsernen Produktion. Für alle, die schon immer hinter die Kulissen schauen wollten. Erleben Sie Werksführungen durch die Fertigung, die Entstehung und den Baufortschritt der neuen Märklineumsanlage und der Sonderpräsentation 40 Jahre Digital. Für die ganze Familie ist etwas geboten.

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Alle weiteren Infos finden Sie unter www.maerklin.de

Readers' letters and messages

Zetties and Trainini in Dialogue

Thank you for each letter to the editor and all the feedback that reaches us. Write us (contact details are in imprint) - Trainini® lives from dialogue with you! Of course, this also applies to all suppliers in Z gauge, who would like to introduce innovations here. A representative sample is our goal. Likewise, here we note any events or meetings with significance to Z gauge reference, if we are informed in time.

Error correction to the article in Trainini® 6/2024:

I have a small comment on the Altenbeken contribution: The streamlined Texaco tanker lorry from JMC Scale Models is an American vehicle.

The prototype is the Diamond T tanker "Doodlebug" and was built in 1933 by the Diamond T Motor Car Company for Texaco and presented at the 1933 World Exhibition in Chicago. Six vehicles were built, none of which still exist today.



We made a mistake when labelling this unusual-looking vehicle.

Manfred Forst, by E-Mail

Editor's reply: The comment is correct and justified, so we would like to thank you very much. When writing, the author had confused this tanker lorry with a British vehicle that was as unusual as it was special. In that case, however, it was a post-war design that only led to a single prototype.



Our reader from Japan would soon like to present his own SNCF TGV.
Photo: Goushi Koike

A fantastic offer from Japan:

I am one of your magazine's readers in Japan. I have made a TGV Sud-Est (south east) model by using 3D printed body, so I would like to share the information with all of you. (...)

In case of you allow to publish this article. I have understood and allowed your editing and agreed to cut or add for any portion or all. Please let me know if you have any questions and/or need any other information. Danke!

Goushi Koike, Cyuo-ku Tokyo (Japan)

Editor's response: We are very pleased about this offer and, above all, the information and photos provided. We are sure that our readers will be delighted. That is why we have already contacted them to prepare this construction report and incorporate it into our further planning.

Praise for customised figure designs:

In the report "Marginal phenomena (part 7)," reference is made to the company Trafofuchs, which manufactures (miniature) people according to individual wishes. I can only confirm what the report says, that Mrs. Birgit Foken-Brock responds to personal wishes and then realises what the customer wants. If further figures are desired or needed, Trafofuchs is my first port of call.

Hartmut Schnittjer, by E-Mail

Ztrack relocation underway:

Ztrack is not only an American trade magazine for our gauge, but also a sales platform for new and used Z gauge goods as well as the wholesaler and distributor for all AZL products, including Rokuhan in the USA.



This is what the new building that Ztrack is currently moving into looks like.
Photo: Ztrack

Owner Rob Kluz reported an enormous lack of space due to the many new products taking up storage space, as well as customers. It was therefore clear that the company's own shop had to be expanded quickly.

This process was completed at the end of June with the acquisition of a new property, which is now also the correspondence address for dealers. The building should be ready for occupancy by the start of the new model railway season.

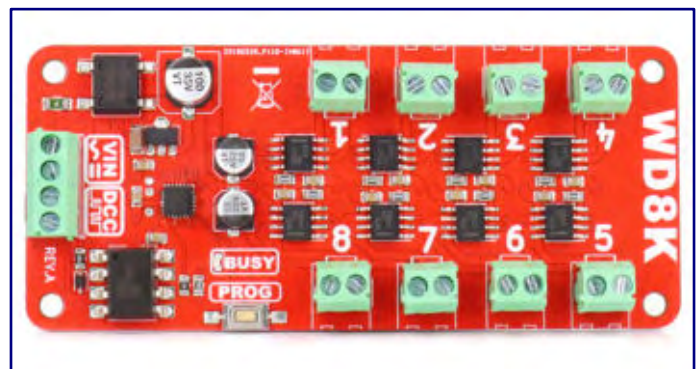
Compared to the previous office, the space has almost doubled and there is room for further growth.

Point decoder at the Modellbahn-Union:

Inexpensive and powerful DCC points decoders were developed and produced for Modellbahn-Union (<https://www.modellbahnunion.com>) in Germany. They are offered as eight-fold copies in three versions.

They are available for WD8D double-coil point machine drives with screw connection (item no. MU_J00003) and for WD8K two-pole drives (as with Rokuhan) with plug (MU_J00001) and screw terminal (MU_J00002).

The decoder is addressed conveniently at the touch of a button. Complicated CV programming is, therefore, not necessary.



This DCC turnout decoder with screw terminal (item no. MU_J00002) can switch eight Rokuhan turnouts digitally. Photo: Modellbahn-Union

The outputs are assigned consecutively, starting from the freely selectable start address. Thanks to linear addressing, there is no need to divide the outputs into blocks of four.

The outputs of the decoder are protected against overload and overvoltage. In addition to the decoder, the scope of delivery includes a mounting and fastening set for circuit boards with 4 screws and spacers as well as the operating instructions.

We would also like to take this opportunity to draw your attention to the landscaping seminars in Kamen, which can be booked via the website. They will take place on the following dates from 8:30 to 17:00: 7 September, 5 October, 2 November, and 2 December 2024 (Model Railway Day).

A club and the fire brigade:

The Stammtisch Untereschbach e.V. is always good for a surprise. In Altenbeken and at other exhibitions, it presented a detailed replica of the Untereschbach fire station.

Built within three years by association chairman Dietmar Stähler and equipped with vehicles from the 3D printer, it was now time for a presentation in the real fire station in front of firefighters and representatives of the town of Overath.



Presentation of the fire brigade diorama at the volunteer fire brigade in Untereschbach by association chairman Dietmar Stähler (far left). Photo: Stammtisch Untereschbach e.V.

Not only did they marvel at the impressive replica with more than 1,000 figures, but they were also amazed at the idea of choosing an open day as the theme. This made it possible to combine both historical and current fire service and THW vehicles and to integrate a showcase of modern firefighting. The attractions shown on the diorama form a fitting backdrop for such an event on a 1:1 scale.

Five Märklin deliveries in July:

When the customers of the specialist dealers are on holiday, Märklin fills their shelves. Let's hope that the following models will soon find their way to the Zetties and make room for everything that is still to be expected later in the year.

A few days before the European Championship final in Berlin, the class 185² electric locomotive (item no. 88488), intended as a suitable special model, went on sale and is sold out at the factory. As a special feature, this locomotive without a road number and inscriptions has genuine gold-plated single-arm pantographs.



The special locomotive for the 2024 European Championships (item no. 88488) went on sale just in time and is sold out at the factory.

The "Carbide Bottle Wagon" (82092) set is intended for Era IV and can be used to recreate an eye-catching and prototypical transport between the SKW Trostberg plants. The five freight wagons differ from the first edition from 2002 in that they are fully equipped with a brakeman's platform, lettering with UIC numbers and a slightly different colour of the carbide bottles.

The car type used for this new product is also used for the Märklin 2024 (80834) magazine car. The lettering was adapted to a fictitious private car and in this case the loaded container shines out at the observer in the blue identification colour of the magazine.

With the eight-part "Sgns container wagon" freight wagon display, a new shape from the previous year can be usefully extended to form a modern KLV block train. The included carrier wagons are assigned to different railway administrations, therefore also have different colours and are also loaded very differently to match: Container types, owners and lengths vary. This means that not all of the wagon's stands are always utilised, which further breaks up the train's appearance.

continued on page 56



The carbide bottle wagons (82092; photo above) for Era IV, which go very well with a V 90, are very interesting for connecting journeys. The new display (86241; photo centre) consists of a total of eight different and differently loaded container wagons, from which we would like to show the SNCF wagon (photo below) individually, as an example.

The pack with six pairs of conversion wagons (item no. 87061) is now also available in the display. These are well-known pairs of three-axle conversion wagons, which are offered individually packaged twice each in the arrangement AB3yge 756 / B3yge 761, B3yge 761 / B3yge 761, and B3yge 761 / BD3yge 766.

The three compartment cars of the Prussian-Hessian State Railways (87568) offer a small special feature. The two brown-painted models labelled for 3rd class as well as the brown-green designed car with 1st / 2nd / 3rd class have prototypical spoked wheels, which are hardly recognisable as such in the Prussian standard bogies.



Shortly before the editorial deadline, the wagon packs with two Off-52 pairs each (86235) for Era III arrived at the dealers.

Shortly before the editorial deadline, one of this year's new models based on the Omm 52 open car followed with the two pairs of Off 52 (86235) unloaded car transporters. They are the successors to the 86221 pack from 1999, which, like their base model at the time, were clearly too long.

Both pairs of wagons are closely and firmly coupled in the centre, the upper deck is completely red-brown in contrast to the previous range and has a railing in an early design. We will take a closer look at this new product as part of a test and also compare it with its predecessor model.

The last new product for this month is the hard cardboard kit "Jagstzell Station" (89708), exclusively designed and manufactured by Modellbau Laffont for Märklin. The model is based on a typical Württemberg station building, which has been used in a similar form at many stations.

New items from GM Modelli Torino:

This model making supplier from Turin has further expanded its still young website (<https://www.gmmodelli.com>). Pages for the current catalogue, used goods and a picture gallery have been added. The range can now also be browsed in German and English.

Exclusive model in Westheim:

The Uwe Schuster assembly service has produced a new version of the Eanos open goods wagon from FR Freudenreich Feinwerktechnik exclusively for the 1zu220 shop (<https://www.1zu220-shop.de>). The model (item no. 352.337.01), which was produced in 45 units, has a brown paint scheme and is labelled for the CFL. However, the model originating from Luxembourg can be seen throughout Europe.



The special model of the Eanos open goods wagon for the 1zu220 shop (item no. 352.337.01) is in service with the CFL. Photo: 1zu220-Shop | Jörg Erkel

A model was selected here without a handbrake and without a protective bracket for the brake system changeover levers on the car floor.

AZL locomotives and a new design:

At AZL, July brings some new locomotive variants, but also a new freight wagon. We start with the models of diesel locomotives: The EMD SD40-2 arrives with railway-specific equipment in the colours of L&N (item no. 64205-1 / -2). As the SD40T tunnel locomotive, it also appears at the same time for Southern Pacific with a “bloody nose” and the “Speed logo” (64109-1 / -2).



The EMD SD60 (item no. 65023-1; photo left) required a new body, the Union Pacific EMD SD70ACE (63109-9; photo right) is travelling in a special livery. Photos: AZL / Ztrack

A new body was required for the EMD SD60, which now runs for the SOO Line (65023-1 / -2). In contrast, the EMD SD70ACE of the Union Pacific is offered with only one road number in the historic livery of the Western Pacific (63109-3B). Following the template, this also applies to the “Powered by our People” version (63109-9) from the same stable.

The ALCO RS-3, which is now returning to the programme, can be selected with two operating numbers and is on the road for the Spokane, Portland & Seattle (63335-1 / -2). Please note that only a few units are available from the manufacturer and that a further locomotive is expected shortly.

The Ortner bulk freight cars are painted blue with GATX lettering. They are available in packs of two (905356-1 / -2). The Ontario Northern freight car with a wide-view cab is very nice to look at (921025-1 / -2).



The 4180 bulk freight cars with compressed air unloading (916034-1; photo left) are a new design, the Ontario Northern wide-view goods train escort car (921025-2; photo right) impresses with its harmonious colours. Photos: AZL / Ztrack

The closed 4180 bulk goods wagons with compressed air unloading for dust goods are now being delivered for the first time. This first version appears for the CB&Q. The models now available are offered in a pack of four (906004-1) and a pack of two (916034-1).

According to the manufacturer, these are its most detailed freight wagons to date, which primarily relates to the wagon floor. According to the manufacturer, this leads to the models swaying when travelling, which is to be prevented by replacing an enclosed bolt.

Photos of all new products can be viewed at <https://www.americanzline.com>.

Fascination Modelling in Friedrichshafen:

Faszination Modellbau in Friedrichshafen will open its doors on 70,000 m² from 1 to 3 November 2024. Many branches of model making will be represented there, with the large Hall A1 and the smaller A2 reserved for model railways. The trade fair would then like to once again adorn itself with the brand "International Model Railway Exhibition."

In addition to functional model making, model railways and steam engines, the real steam hall meeting can also be found there again. Visitors to BodenSee (Lake Constance) can expect demonstrations and exhibits as well as hands-on activities.

Once again, layouts of all gauges are promised, as well as stands from all leading model railway manufacturers. However, participation from the small series has recently been meagre, with just as little information currently available as on the expected Z gauge exhibits.

First Atlas wagon model:

Atlas Model Railroad (<https://shop.atlasrr.com>) entered the Z gauge market a few years ago with a new flex track, followed by turnouts and a double crossover. We also reported on this in detail at the time.

In the meantime, this supplier has expanded its commitment and delivered its first wagon model. The model was the 53-foot-long, covered goods wagon with double revolving doors from Evans Products. These doors opened up a large area for loading and unloading. Manufactured in the sixties and seventies,

the wagons were popular for transporting timber and paper. But tinned food and other foodstuffs were also typical loads.



Still available from the factory are these two versions of the Atlas wagon novelty from British Columbia Railways (item no. 75000001; photo left) and Union Pacific (75000008; photo right). Photos: Atlas Model Railroad

Only four of the announced variants are still available from the manufacturer, all others are sold out but can still be found in specialised shops: British Columbia Railways (item no. 75000001), Northwest Hardwoods (75000006), and Union Pacific (75000008 / -9).



Z gauge club car 2024 from Schorndorf:

For the second time, the Schorndorf Z-Stammtisch in Baden-Württemberg has had its own car produced and printed by Märklin. A Swiss sliding wall wagon was selected, which was printed with a motif from EDV-Schuler and a reference to the regulars' table and the calendar year, i.e., it has no prototype.

Schorndorf club car 2024 (item no. 8657.056). Photo: Z-Stammtisch Schorndorf.

remaining items can be purchased from Manfred Frey. Contact should then be made via [m.frey2\[at\]arcor.de](mailto:m.frey2@arcor.de).

The Märklin item number is 8657.056, any

New mould for Rokuhan:

Ztrack and Rokuhan have worked together to bring Rokuhan's first US model to the world, according to a statement from the US.

A USRA tender steam locomotive for light shunting and transfer service with the C axle arrangement (US axle formula: 0-6-0).

The first version of the planned new product is labelled with three different road numbers (item numbers T040-1 to -3) for the Baltimore & Ohio. The American standard locomotive according to USRA standards was built in a total



USRA-Rangierdampflok 0-6-0 der B&O (Art.-Nr. T040-2). Foto: Rokuhan

number of 255 units and equipped with different tenders. The model depicts the condition at the time of commissioning.

The replica has claw couplings compatible with AZL, Micro-Trains, and Full Throttle, and a functional headlight. A minimum radius of 195 mm is recommended. Delivery is expected for September 2024.

Open house at Märklin:

Visitors can look forward to many impressions of the creation of a model railway vehicle at will be open from 9:00 to 15:00 on both days.



Märklin is once again inviting visitors to an open house at its main factory in Göppingen in September.

The most important departments then show what they have to offer. From the die-casting department to post-processing, electroplating, painting and final assembly, all the steps that a new model goes through until it is ready for delivery can be experienced. The employees also explain all the work steps and answer visitors' questions.

The supporting programme includes various children's games with and around the railway, and food and drink will also be provided. The Märklineum, which tells the story of the Märklin company, is also open. The latest information will be updated on the manufacturer's website.

Wood chip transport with WDW:

The model for the current collector's set (item no. FT-COL69) at WDW Full Throttle was provided by the transport of wood chips. It contains two open bulk goods wagons with raised sides.

Their prototypes ran on the Green Bay and Western Railroad (GBW) and the Maine Central Railroad Company (MEC), which was known as the "Pine Tree Route". With their yellow and green paintwork, the two wagons have a striking appearance that immediately makes them stand out and places them in the northern forests of America.



Wood chip trolley in a pack of two (item no. FT-COL69). Photo: WDW Full Throttle

We should know the following about the prototypes: When the railways recognised wood chippings and bark as lucrative transport goods, they converted older hopper wagons with box bodies in order to be able to transport larger quantities of this light load. Wooden superstructures, steel extensions and parts recovered from defective wagons were used. The manufacturer can be contacted at <http://www.wdwfullthrottle.com>.

Imprint

ISSN 2512-8035

Bibliographic information of the German National Library: The German National Library lists this publication in the German National Bibliography. Detailed bibliographical data and editions can be found in the DNB catalogue at <https://portal.dnb.de>.

The publication of **Trainini German Magazine for Z Gauge** is voluntary and non-commercial. **Trainini German Magazine for Z Gauge** does not aim for any sources of income. This publication is governed exclusively by German law.

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